

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) enclose a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. 1 DATED 7/19/2005 ADDENDUM NO. _____ DATED _____
ADDENDUM NO. _____ DATED _____ ADDENDUM NO. _____ DATED _____

Number	Description	TOTAL ADDENDA
1	Revised NTB 512, replaces same; Proposal Sheet 2-2, replaces same; Revised Project Disk Required.	<u> 1 </u> (Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

Signature

TITLE _____

ADDRESS _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 512

CODE: (SP)

DATE: 07/18/2005

SUBJECT: Scope Of Work

PROJECT: MP-6026-55(012) / 301763302 -- Pearl River County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings." All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following:

SR 26
WIDEN AND OVERLAY APPROXIMATELY 11.5
MILES OF SR 26 FROM APPROXIMATELY 250' EAST
OF THE JUNCTION OF SR 53, TO THE STONE COUNTY LINE.

(A) Prior to the overlay, centerline alignment shall be determined by the contractor by measuring the existing roadway at 500 feet intervals in tangent sections, and 100 feet intervals in horizontal curves. The existing pavement edge shall be cut to a smooth and near vertical face with an approved cutting device (Not to be measured for separate pay). The foundation for widening the base shall be built according to the Typical Section(see typical drawing). Material excavated while building the foundation for widening shall be used to raise the existing shoulder to match the new pavement elevation. Surplus material shall be spread along the edge of the shoulders, foreslopes, or other adjacent areas as directed by the Project Engineer, and will be an absorbed item.

(B) Cold mill the roadway at the B.O.P., E.O.P., and bridge approaches as designated by the Project Engineer to ensure smooth transition of new overlay with existing grade. (See Typical Drawings)

(C) Overlay SR 26 with 1" and variable (ST) asphalt 9.5-mm mixture from 250' East of the Jct. of SR 53, East to the Stone Co. Line. Prior to the overlay, a leveling course of ¾" & variable of 9.5mm ST asphalt will be required in both lanes. The foundation for widening the roadway shall be built according to the Typical Section. The asphalt for the widening shall be placed at the same time with the ¾" leveling course. No bridge decks shall be overlaid. Remove any failed areas on the main facility and repair by backfilling with 19.0mm ST asphalt as directed by the Project Engineer. Approximately 150 tons of 19.0 mm ST asphalt will be used for base repair. Publicly maintained roads or streets shall be surfaced to the existing R.O.W.; Privately owned entrances shall be surfaced a distance of 10 feet & var. from edge of pavement. Any site grading at local roads or drives will not be measured for separate payment but will be considered an

absorbed item. A paved apron shall be placed around each guardrail location as directed by the Project Engineer. The overlay shall correct the roadway lane cross-slope to 2% minimum. The existing superelevation in the horizontal curves is to be maintained as a minimum. Any work to determine the existing superelevation rate as well as any work to control the laydown equipment for proper placement of the asphalt in the superelevated curves shall be absorbed by the contractor at no additional cost to the state. The Contractor will mill a 12" rumble strip along the shoulders. The Contractor will place the traffic stripe on the inside 6" of the rumble strip.

(D) Raise the existing shoulders to match the new pavement elevation by placing 1 ¾" inches and variable depth borrow excavation material on the shoulders. Placement of the borrow excavation material on the finished surface course shall not be permitted. The existing shoulder shall be scarified prior to the placement of new borrow material on the shoulder to ensure bonding of the new shoulder material. The material shall be bladed, rolled and compacted to a finished slope of 4%. Shoulders with existing adequate shoulder material in place shall be bladed to a slope of 4%, the cost of which shall be included in the prices of other items bid.

(E) Temporary striping shall conform to finished stripe specifications for alignment, neatness, reflectivity, and straightness. All permanent pavement markings are to be hot thermoplastic. Edge lines will be placed so as to maintain the original lane width. Glass beads applied to thermoplastic shall conform to Section Number - 720.01(Beads shall be double dropped Class B, High-Visibility first, and then Class A High-Visibility). On concrete bridges, old traffic stripe shall be removed and replaced with High Performance Cold Plastic.

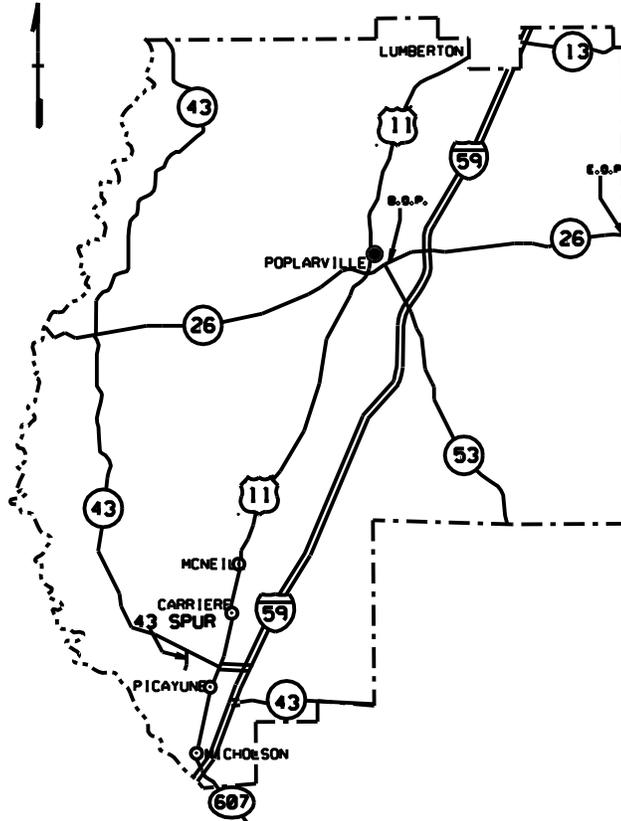
(F) Raised pavement markers will be placed at 80 feet intervals in tangents and 40 feet intervals in curves, and in urban limits along the centerline of roadway. Any removals of existing raised pavement markers or rumble bars shall be considered an absorbed item.

The contractor shall erect and maintain construction signing, and provide all signs and traffic handling devices in accordance with the Manual Uniform Traffic Control Devices (MUTCD)(the cost is to be included in the price bid for pay item No. 618-A, Maintenance of Traffic).

Incidental work such as removing vegetation, shaping and compaction of shoulder, removing excess asphalt material, project clean-up, and other incidental work necessary to complete the project will not be measured for separate payment, but will be included in other bid items.

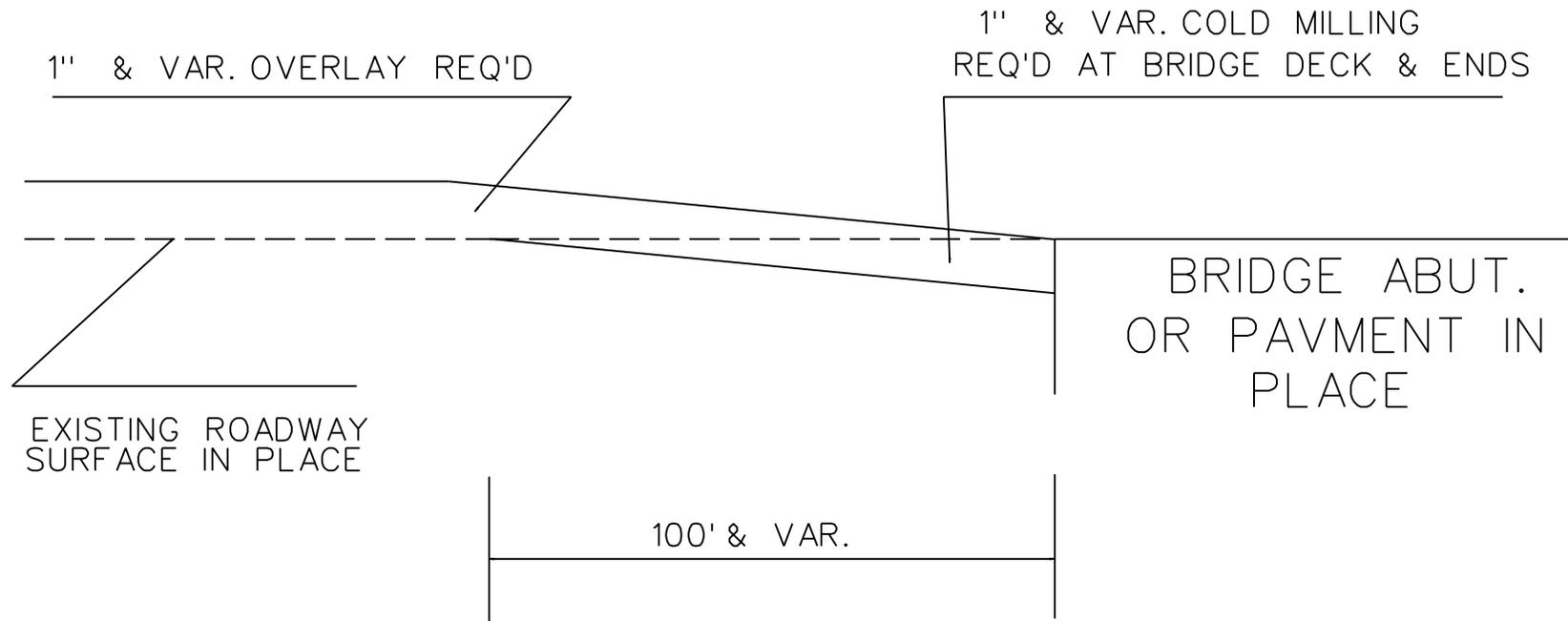
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MP-6026-55(012)
PEARLRIVER COUNTY

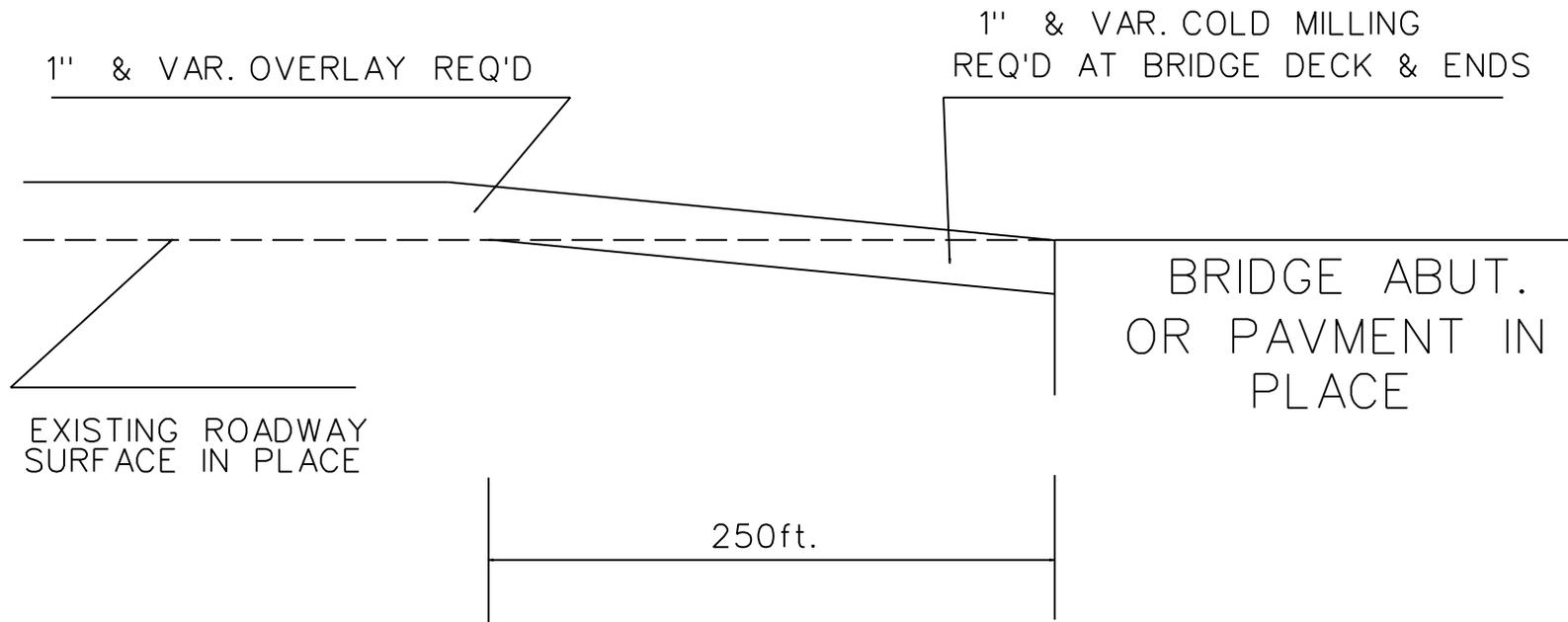


WIDEN & OVERLAY APPROXIMATELY
11.5 MILES OF SR 26 FROM APPROXIMATELY
250' EAST OF THE JCT OF SR 53, EAST TO
THE STONE CO. LINE

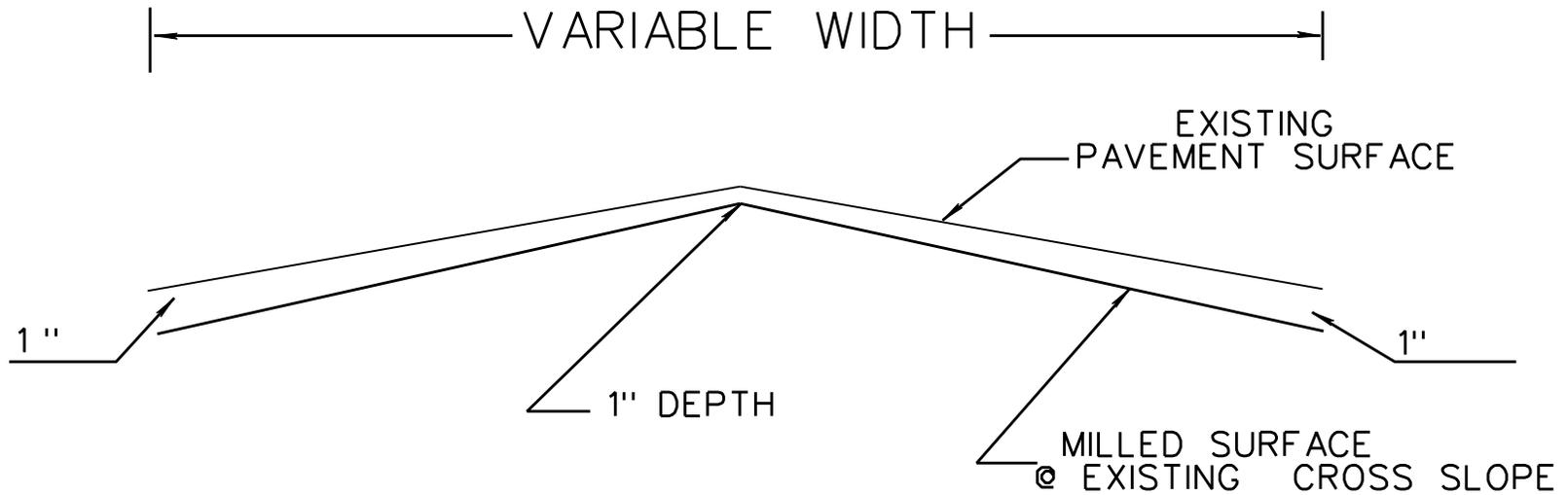
TYPICAL MILLED TRANSITION AT BRIDGE ABUT. OR PAVEMENT IN PLACE



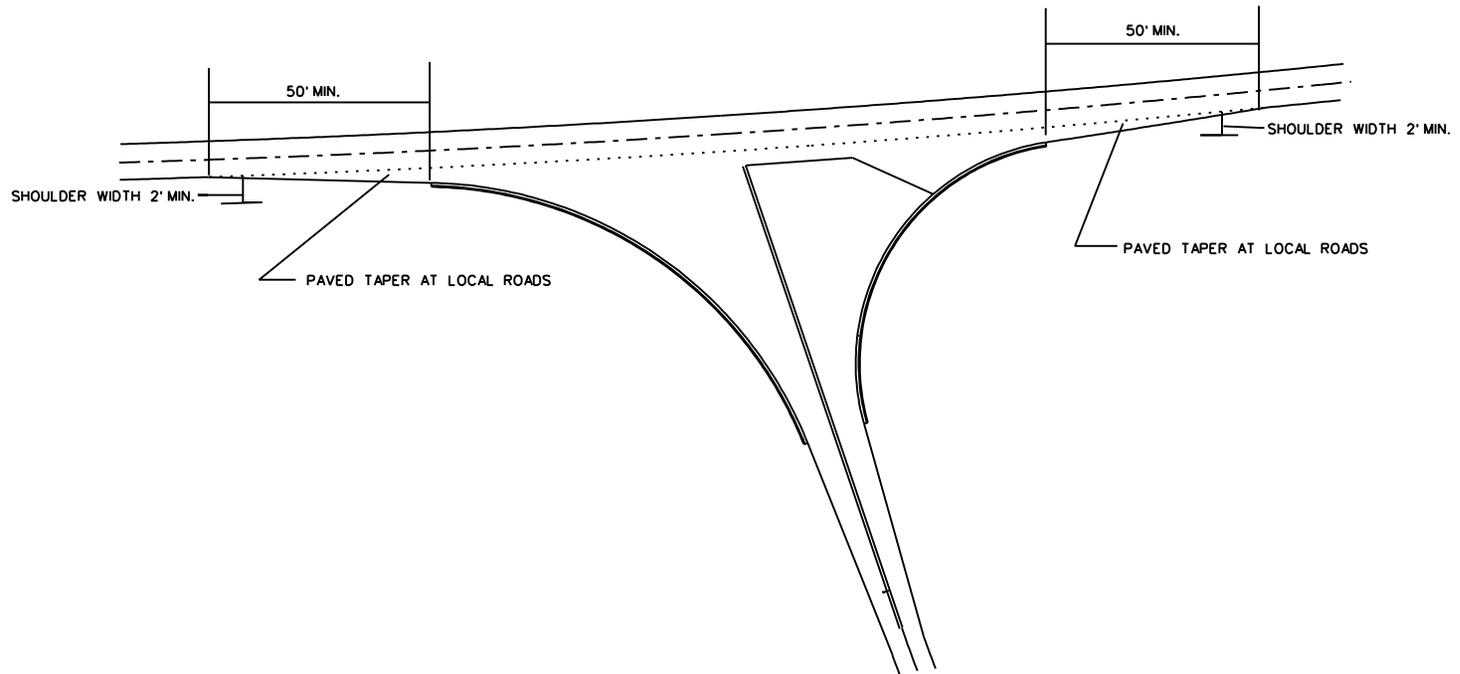
TYPICAL MILLED TRANSITION AT
BRIDGE ABUT. OR PAVEMENT IN PLACE
AT I-59 AND SR 26 INTERSECTION



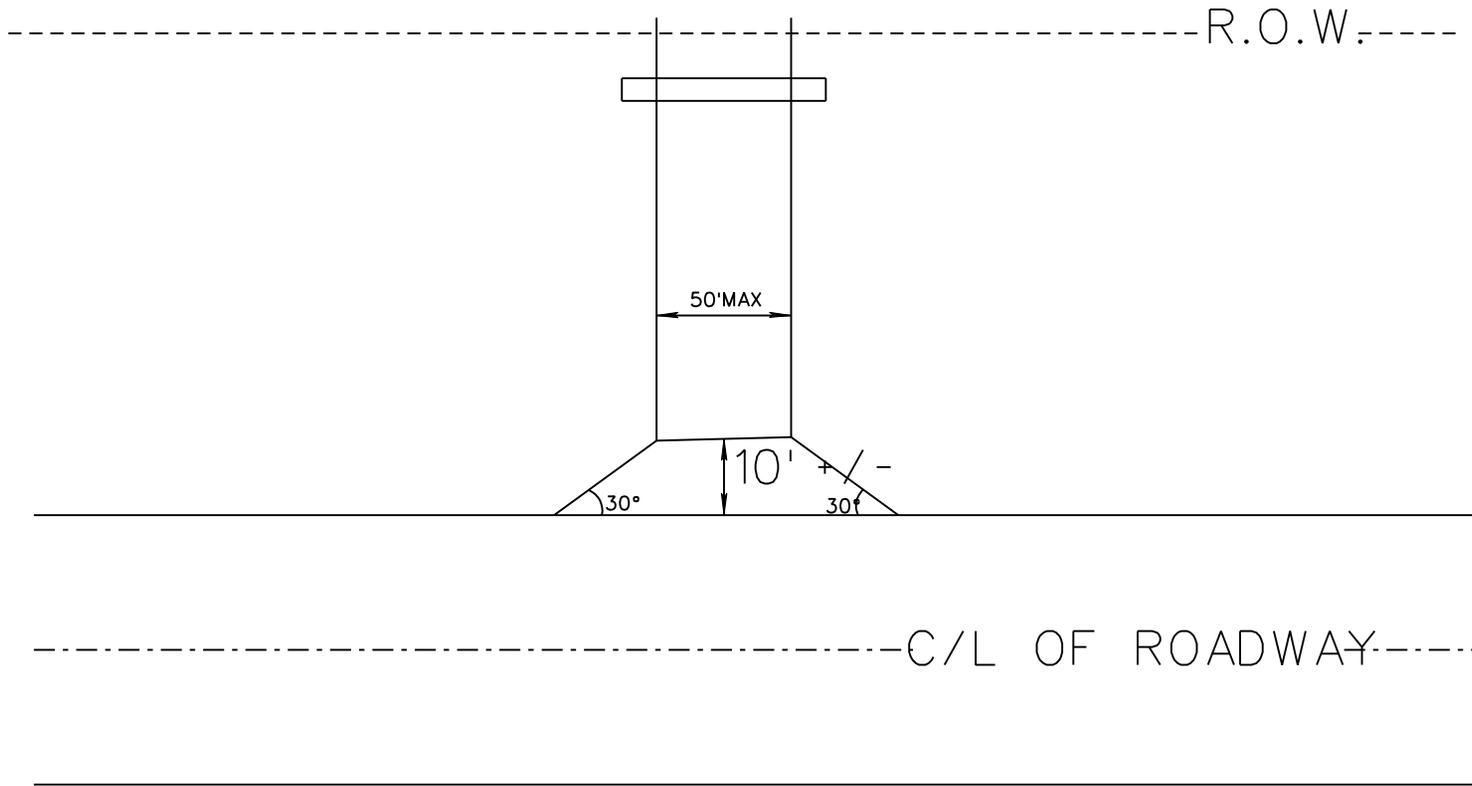
TYPICAL MILLING DIAGRAM

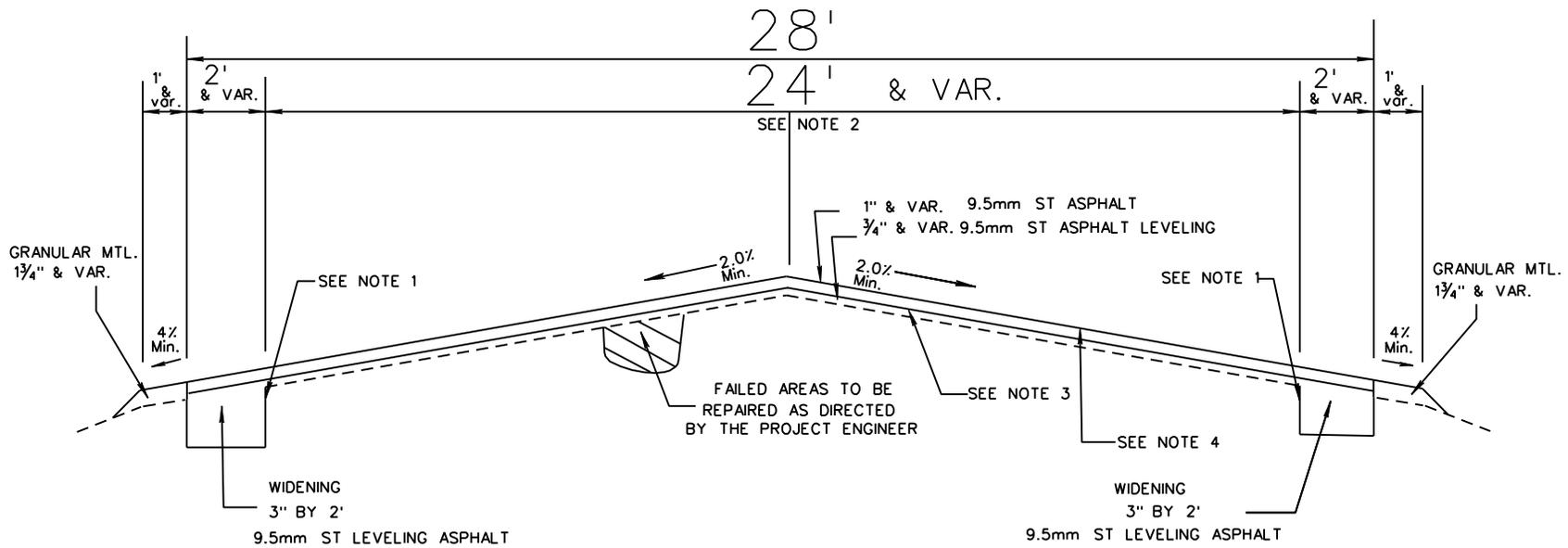


TYPICAL FOR PAVED TAPER AT LOCAL ROADS



PRIVATE DRIVEWAY DETAIL





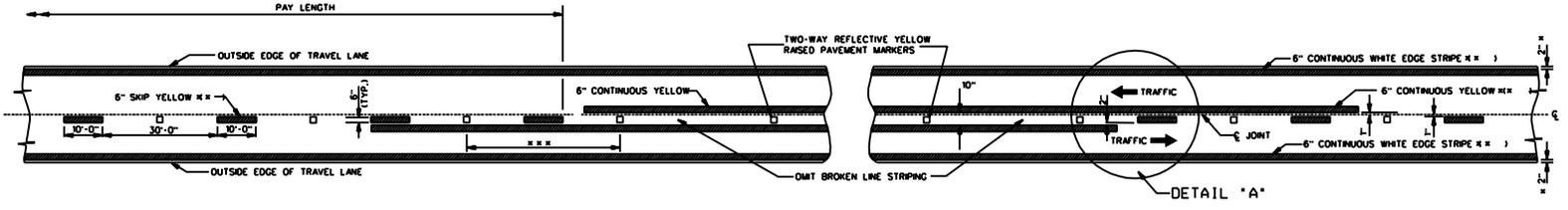
NOTES:

(1) THE EXISTING PAVEMENT EDGE SHALL BE CUT TO A SMOOTH AND NEAR VERTICAL FACE WITH AN APPROVED CUTTING DEVICE. (NOT TO MEASURED FOR SEPARATE PAY.)

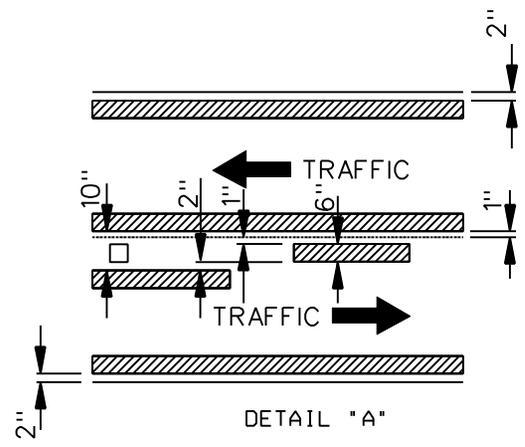
(2) TRUE CENTERLINE PAVEMENT ALIGNMENT SHALL BE DETERMINED BY THE CONTRACTOR BY MEASURING THE EXISTING ROADWAY AT 500ft. INTERVALS IN TANGENT SECTIONS, AND 100ft. INTERVALS IN HORIZONTAL CURVES.

(3) THE ASPHALT FOR THE WIDENING SHALL BE PLACED AT THE SAME TIME AS THE 3/4" LEVELING COURSE. 9.5mm ST (3/4" LEVELING)

(4) 9.5mm ST (1" and VARIABLE SURFACE COURSE)



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



DETAIL "A"

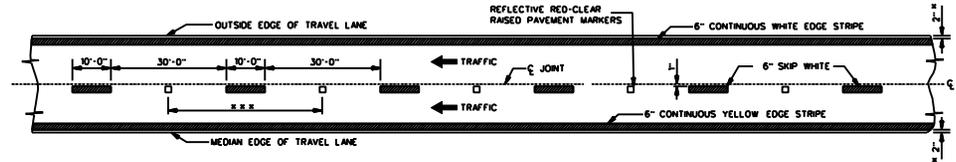


GENERAL NOTES:

- * 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
- *** 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

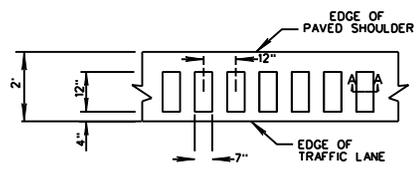
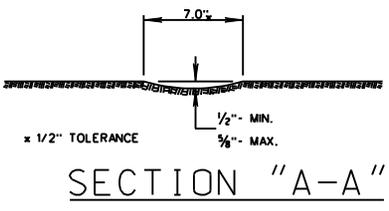
	URBAN AREA (ft-in)	RURAL AREA (ft-in)
TANGENT SECTIONS	40'-0"	80'-0"
HORIZONTAL CURVES	40'-0"	40'-0"
INTERCHANGE LIMITS	40'-0"	140'-0"

- 1. NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- 4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDT "APPROVED SOURCES OF MATERIALS."

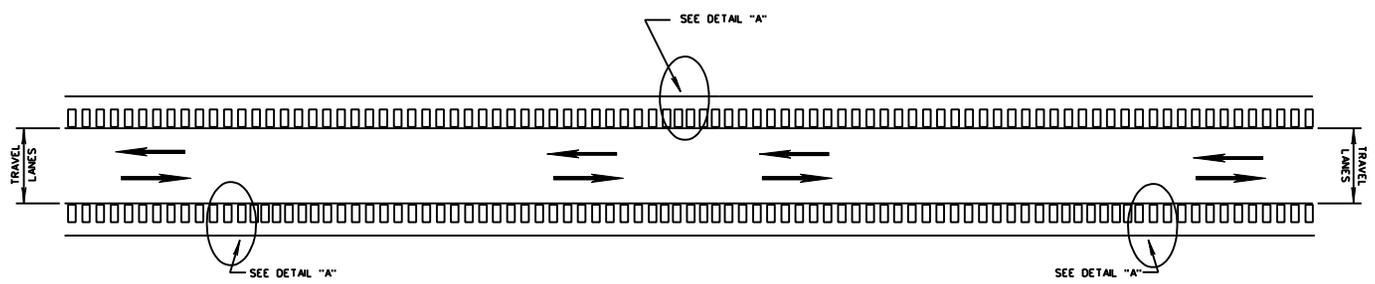


4-LANE WITH ONE-WAY TRAFFIC

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN		
PAVEMENT MARKING DETAILS FOR 2-LANE AND 4-LANE DIVIDED HIGHWAYS		
ISSUE DATE:	OCTOBER 1, 1998	WORKING NUMBER PM-1 SHEET NUMBER 120



- GENERAL NOTES
1. GROUND-IN RUMBLE STRIPS SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 2. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED TO MAINLINE ONLY.



PLAN
NOT TO SCALE
DETAIL OF
RUMBLE STRIPS
(GROUND-IN)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTION	
RUMBLE STRIPS (GROUND-IN)	
FILE NAME:	DATE:
NO. 111	01/08 01
DATE	BY
	RS
	SAFETY REVIEW

