

FED. ROAD REG. NO.	STATE	PROJECT NO.	SHEET NO.
	MISS.	IM-0020-02(072)	1

STATE OF MISSISSIPPI

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE OF PROPOSED FEDERAL-AID PROJECT NO. IM-0020-02(072)

MILLING AND OVERLAYING INTERSTATE 20
FROM THE SCOTT/NEWTON COUNTY LINE TO CHUNKY
IN NEWTON COUNTY

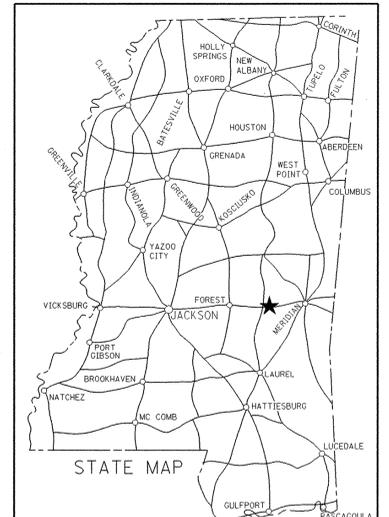
103927/301000



① 01-20-06
RE: ① 01-17-08

SCALES

LAYOUT 1 IN. = 13200 FT.



NOTE
★ INDICATES APPROXIMATE LOCATION OF PROJECT.
LAT. 32° 20' 36" LONG. 89° 01' 55"
(APPROX. MIDDLE OF PROJECT)

BOP 0+02 RT LANE
0+02 LT LANE

EOP 1249+15 RT LANE
1249+91 LT LANE



DESIGN CONTROL

70 MPH = V (SPEED DESIGN)
ADT (2008) = 20000; ADT (2028) = 36000
DHV = 3600 : D = 60 % T = 48 %

PERMITS ACQUIRED BY MDOT

WETLANDS AND WATERS PERMITS (NECESSARY FOR ULTIMATE IMPROVEMENTS ONLY):		
	WATERS	WETLANDS
NATIONWIDE #14	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
NATIONWIDE (OTHER)*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
GENERAL*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
INDIVIDUAL (404)*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
* ACQUISITION OF PERMITS FOR TEMPORARY IMPACTS DURING CONSTRUCTION ARE THE RESPONSIBILITY OF THE CONTRACTOR		
STORMWATER PERMIT <input checked="" type="checkbox"/>		
Y	REQUIRED, CNOI SUBMITTED BY MDOT (DISTRIBUTED AREA = 5 ACRES + (NTB 6484))	
S	REQUIRED, SCNOI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES) (NTB 6483)	
N	NO STORMWATER PERMIT REQUIRED (<1 ACRE)	
APPROVED BY: <i>DBS</i> DATE: <i>1/17/08</i>		

CONVENTIONAL SYMBOLS

- COUNTY LINE
- TOWN CORPORATION LINE
- SECTION LINE
- EXISTING ROAD OR TRAVELED WAY
- PROPOSED ROAD OR TRAVELED WAY
- RAILROAD
- SURVEY LINE
- BRIDGES

EQUATIONS

Right Lane	74 + 90.47 BK = 74 + 35.05 AH	876 + 26.94 BK = 876 + 71.94 AH	213 + 47.81 BK = 211 + 52.95 AH	942 + 62.66 BK = 942 + 75.16 AH
	160 + 50.59 BK = 159 + 80.30 AH	943 + 94.12 BK = 943 + 80.28 AH	269 + 09.04 BK = 297 + 62.80 AH	1198 + 00.00 BK = 1198 + 26.40 AH
	269 + 09.04 BK = 297 + 62.80 AH	1197 + 99.24 BK = 1198 + 26.40 AH	538 + 90.15 BK = 539 + 90.15 AH	
	538 + 90.15 BK = 538 + 90.15 AH		657 + 51.22 BK = 657 + 73.57 AH	
	657 + 51.22 BK = 657 + 73.57 AH		877 + 15.82 BK = 876 + 73.48 AH	
Left Lane				

EXCEPTIONS
NONE

LENGTH DATA

LENGTH OF ROADWAY	FT.	23.653	MI.
LENGTH OF BRIDGES	FT.	0.000	MI.
LENGTH OF PROJECT (NET)		23.653	MI.
LENGTH OF EXCEPTIONS	FT.	0.000	MI.
LENGTH OF PROJECT (GROSS)		23.653	MI.

REVISIONS	DATE	
	DATE	
APPROVED:		
BY	<i>Harry Lee James</i>	1/17/08
	CHIEF ENGINEER	DATE
	<i>Samuel R. ...</i>	1-17-08
	EXECUTIVE DIRECTOR	DATE
MISSISSIPPI DEPARTMENT OF TRANSPORTATION		
APPROVED:		
	DIVISION ADMINISTRATOR	DATE
	FEDERAL HIGHWAY ADMINISTRATION	DEPARTMENT OF TRANSPORTATION

STATE	PROJECT NO.
MISS.	IM-0020-02(072)

GENERAL NOTES

- All permanent striping will be thermoplastic except on concrete surfaces (bridges, bridge approach slabs, etc.). All permanent striping on concrete surfaces will be high performance cold plastic. Removal of existing cold plastic stripe will be required prior to placement, payment for which will be covered under the appropriate pay item. Edge lines will be placed to maintain the original lane widths. Raised pavement markers shall be installed as per standards. Permanent cold plastic detail stripe will be required at all edge drain outlet locations as per plans. All curbs that have previously been painted shall be repainted. The cost of painting the curbs will be absorbed in other items bid.
- Temporary traffic stripe will be required immediately after milling and/or overlay operations and prior to opening the area to traffic. Temporary stripe on the final riding surface shall be placed in the same location and layout as the permanent stripe.
- The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the standard drawings and the MUTCD. The cost of traffic control devices is to be included in the price bid for Pay Item 618-A, Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background. Traffic control signs and devices shall be kept clean and in satisfactory condition throughout the life of the project.
- Grassing will be required at all disturbed areas as directed and shall be performed at the earliest time to minimize the potential for erosion.
- Existing emergency crossovers are to be paved according to the attached drawings.
- Prior to the placement of the final lift of asphalt, failed areas (punchouts) shall be repaired. These locations include, but are not limited to, those shown in the attached tables. Upon placement of new concrete, the concrete shall be placed to a finished elevation so as to coincide with adjacent undisturbed concrete. Following repair of the concrete pavement, the repaired area shall be overlaid with 12.5mm HT, Polymer Modified HMA.
- It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, paved ditches, bridges, fences, etc. from damage which might occur during construction. The Contractor shall replace or repair any structures damaged during the life of the contract. No payment will be made for replacement or repair of damaged items.
- Any signs that are in conflict with construction shall be removed and relocated by the Contractor as directed by the Engineer, the cost of which is to be included in other items bid.
- Incidental work, such as removing vegetation, shaping and compacting shoulders, removing and resetting signs, removing excess asphalt material, project clean-up, and other items, necessary to complete the project will not be measured for separate payment and will be considered included in the prices of other items bid.
- Voids created by the removal of posts, concrete anchors, footings, etc. shall be backfilled and compacted in accordance with Section 203 of the Mississippi Standard Specifications for Road and Bridge Construction. The cost of this work should be included in other items bid.
- Potholes which exist or which may occur in the existing pavement structure are to be patched in a timely manner and prior to paving operations.
- Litter shall be removed from the project right-of-way and properly disposed of by the Contractor at least monthly during the life of the project. Litter shall include, but not be limited to, solid wastes such as glass, paper products, tires, wood products, metal, synthetic materials, and other miscellaneous debris. In addition to monthly litter removal, the Contractor shall on a daily basis remove all debris from within the roadway and a 30-foot clear-zone which, in the opinion of the Engineer, is a hazard to the traveling public. No direct payment will be made for the litter and/or debris removal; the cost is to be included in the other items bid. Failure of the Contractor to remove litter and/or debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment until the work is satisfactorily removed by the Contractor. Prior to acceptance, the existing bridges and curbed sections shall be swept and/or cleaned to remove debris. No separate payment will be made for the clean-up or sweeping activities.
- Site grading will be required at the areas listed or as directed by the Engineer. Topsoil shall be placed as directed by the Engineer.

GENERAL NOTES

- Paving in the milled areas shall begin immediately behind the milling operation. Traffic will not be allowed to run on the milled surface; therefore, any milled surface not paved will require a closure to be left in place.
- Wire backing is required on all silt fence.
- Approximately half of all milled material will be retained by MDOT. The material shall be delivered to the DISTRICT 5 headquarters in Newton. Sufficient advanced notice shall be given to insure that MDOT maintenance is equipped to handle the delivery.
- The included Plan/Profile sheets are attached for information only unless work required is shown in the plan and profile views.
- Some curves will require leveling to correct the superelevation cross-slope prior to the final surface being placed. These areas are as listed on sheet MPD-1 or as directed by the Engineer. When required, the adjacent shoulders shall be overlaid to prevent uneven pavement lines, and rumble strips shall be placed on the paved shoulders.
- Logging, clearing, grubbing, chip/debris/vegetation removed and erosion control operations must be performed during daylight hours. Accumulations of chips and clearing debris shall not be allowed to remain within 30' of the outside edge of the travel lane. Equipment shall be stored outside the 30' clear zone while not in use.
- Existing drainage channels are to be cleared and restored to the lines and grades as directed by the Engineer and as per the applicable Notice-to-Bidders.
- All ditch plugs & authorized crossovers will be brought to current safety standards. The slope will be flattened to a 10:1 slope which may require extending some median pipes, and removal and replacement of some paved ditch, aprons and inlets. Payment will be made using the appropriate pay items.
- Clearing and grubbing will be required on the project. The limits of the clearing will conform to the drawings and guidelines set forth in the plans. Proper erosion control measures shall be taken to prevent siltation of existing structures and loss of materials from the right-of-way. (see section 107.22.2) Burning of waste vegetation, debris, etc. resulting from the clearing operations will not be allowed on the Interstate right-of-way. Burning on adjacent private property that might result in a traffic hazard will be like likewise not be permitted. All dead or dying trees (standing or falling) shall be removed within ROW limits shall be removed as directed by the Engineer throughout the life of the project until the final maintenance release is issued. Price for removal of dead or dying trees is to be included in other items bid.
- Certain slopes within the project are to be extended or flattened to provide a minimum 3:1 foreslope, eliminate washes, or to construct guardrail pads that conform to the current standard of design. These areas are as shown in the plans; however, other areas may be added subsequent to the clearing operation.
- If existing fences fall within the clearing and grubbing limits, they are to be removed and replaced as directed. Payment for removal and replacement of the fence will be made using the appropriate pay items.
- The paved shoulders on the exit & entrance ramps will be widened to the width and length shown on the attached drawings.
- The area beneath existing guardrails is to be graded to restore the appropriate slope and grade. No separate payment will be made for this work; therefore, the cost should be included in other items bid.

PLAN
ROADWAY DESIGN DIVISION, CAROLINA SECTION
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

		BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
		REVISION		GENERAL NOTES	
		DATE		103927/301000	
		DESIGN TEAM		PROJECT NO. IM-0020-02(072)	
		CHECKED		COUNTY : NEWTON	
		DATE		WORKING NUMBER	
				GN-1	
				SHEET NUMBER	
				3	