

FED. ROAD REG. NO.	STATE	PROJECT NO.	SHEET NO.
4	MISS.	HSIP-0331-00(012)	1

STATE OF MISSISSIPPI

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE OF PROPOSED STATE HIGHWAY FEDERAL AID PROJECT NO. HSIP-0331-00(012)

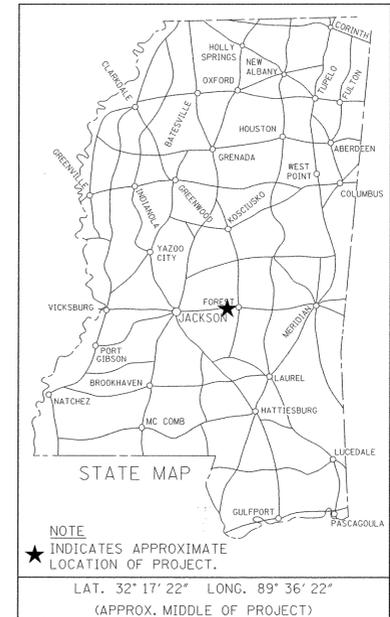
①8-12-08

MS 481 REALIGN CURVE SOUTH OF I-20
APPROXIMATELY 2 MILES SOUTH
SCOTT COUNTY

CONST FMS 104898 /301000

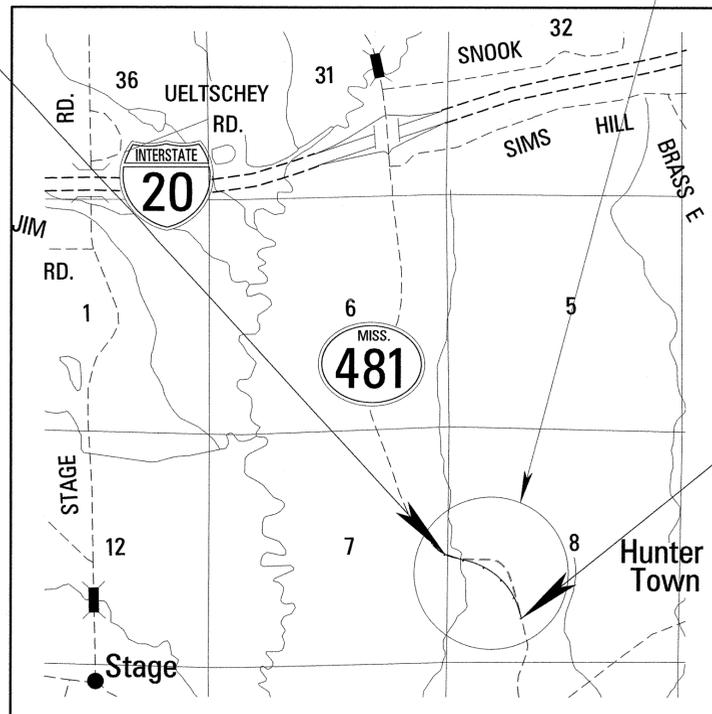
SCALES

PLAN	1 IN. = 100 FT.
PROFILE	HOR. 1 IN. = 100 FT.
	VERT. 1 IN. = 10 FT.
LAYOUT	1 IN. = 2000 FT.



STA. 15+00 E.O.P.

BRIDGE STRUCTURES REQ'D.



STA. 0+00 B.O.P.

BOX BRIDGES REQ'D.

DESIGN CONTROL	
55 MPH = V (SPEED DESIGN)	
ADT (2008) = 930	ADT (2028) = 1,100
DHV = 120	D = 60 % T = 15 %
PERMITS ACQUIRED BY MDT	
WETLANDS AND WATERS PERMITS (NECESSARY FOR ULTIMATE IMPROVEMENTS ONLY):	
NATIONWIDE #14	<input checked="" type="checkbox"/> WATERS <input checked="" type="checkbox"/> WETLANDS
NATIONWIDE (OTHER)*	<input checked="" type="checkbox"/> <input checked="" type="checkbox"/>
GENERAL*	<input checked="" type="checkbox"/> <input checked="" type="checkbox"/>
INDIVIDUAL (404)*	<input checked="" type="checkbox"/> <input checked="" type="checkbox"/>
* ACQUISITION OF PERMITS FOR TEMPORARY IMPACTS DURING CONSTRUCTION ARE THE RESPONSIBILITY OF THE CONTRACTOR	
STORMWATER PERMIT <input checked="" type="checkbox"/>	
Y	REQUIRED, CNOI SUBMITTED BY MDT (DISTRIBUTED AREA = 5 ACRES + INTB 506)
S	REQUIRED, SCNOI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES) (INTB 14)
N	NO STORMWATER PERMIT REQUIRED (<1 ACRE)
APPROVED BY: <i>CEP</i> DATE: 6/13/08	

CONVENTIONAL SYMBOLS

- COUNTY LINE -----
- TOWN CORPORATION LINE -----
- SECTION LINE -----
- EXISTING ROAD OR TRAVELED WAY -----
- PROPOSED ROAD OR TRAVELED WAY -----
- RAILROAD -----
- SURVEY LINE -----
- BRIDGES -----

EQUATIONS

STA. 21+96.10 BK. = 12+60.69 AH.

LENGTH DATA

LENGTH OF ROADWAY	2435.41 FT.	0.461 MI.
LENGTH OF BRIDGES	FT.	MI.
LENGTH OF PROJECT (NET)	FT.	0.461 MI.
LENGTH OF EXCEPTIONS	FT.	MI.
LENGTH OF PROJECT (GROSS)	FT.	0.461 MI.

EXCEPTIONS

APPROVED:	<i>[Signature]</i>	9/12/08
CHIEF ENGINEER		DATE
APPROVED:	<i>[Signature]</i>	9/12/08
EXECUTIVE DIRECTOR		DATE
MISSISSIPPI DEPARTMENT OF TRANSPORTATION		
APPROVED:		
DIVISION ADMINISTRATOR		DATE
FEDERAL HIGHWAY ADMINISTRATION DEPARTMENT OF TRANSPORTATION		

6/11/2008 8:32 AM TITLE.DGN

DESCRIPTION OF SHEET

REVISION DATE WKG. NO. SH. NO.

TITLE SHEET (1)			1
DETAILED INDEX & GENERAL NOTES (2)			
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GENERAL NOTES		GN-1	3
TYPICAL SECTION SHEETS (2)			
TYPICAL SECTION - NEW CONSTRUCTION (STA. 4+50 to STA. 19+70)		TS-1	4
TYPICAL SECTION - WIDEN AND OVERLAY (STA. 0+00 to STA. 4+50) (STA. 19+70 to STA. 15+00)		TS-2	5
QUANTITY SHEETS (7)			
SUMMARY OF QUANTITIES		SQ-1	6
SUMMARY OF QUANTITIES		SQ-2	7
SUMMARY OF QUANTITIES		SQ-3	8
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ESTIMATED QUANTITIES - SIDE DRAINS AND EROSION CONTROL		EQ-2	10
ESTIMATED QUANTITIES - EARTHWORK AND BOX CULVERTS		EQ-3	11
ESTIMATED QUANTITIES FOR TRAFFIC CONTROL SIGNS		TCP-Q	12
PLAN & PROFILE SHEETS (1)			
STA. 0+00 TO STA. 15+00		WK. 3	13
SPECIAL DESIGN SHEETS (6)			
VEGETATION SCHEDULE		VS-1	14
TRAFFIC CONTROL PLANS		TC-1	15
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PAVEMENT MARKING DETAIL		PMD-1	18
PAVEMENT MARKING DETAIL		PMD-2	19
TRAFFIC CONTROL DETAILS - DRUM PLACEMENT AND SHOULDER CLOSURE		TCP-SC	20
STANDARD DRAWINGS - ROADWAY SHEETS (16)			
PAVEMENT MARKING DETAILS FOR 2 & 4-LANE DIVIDED ROADWAYS	12-01-99	PM-1	120
EROSION CONTROL		EC-1	140
TYPICAL TEMPORARY EROSION CONTROL MEASURES (SILT FENCE, HAY BALES, & BRUSH BARRIER)		TEC-1	142
TYPICAL TEMPORARY EROSION CONTROL MEASURES (SLOPE DRAIN AND TYPE A SILT BASIN)		TEC-2	143
DETAILS OF DITCH TREATMENT		DT-1	145

DESCRIPTION OF SHEET

REVISION DATE WKG. NO. SH. NO.

TRAFFIC CONTROL PLAN WITH FLAGGER (ONE-LANE CLOSURE OF TWO WAY TRAFFIC)		TCP-1	250
HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS		TCP-10	259
TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO-LANE ROADS		TCP-11	260
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TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS		TCP-15	264
RURAL DRIVEWAYS		RD-1	271
TYPICAL GRADING TRANSITION BETWEEN CUTS & FILLS		GT-1	272
SUPERELEVATION TRANSITION - CASE I (2% NORMAL SUBGRADE)	3-01-02	SE-2A	276
PIPE CULVERT INSTALLATION		PI-1	300
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FLARED END SECTION FOR CONCRETE ARCH PIPE		FE-1A	329
STANDARD DRAWINGS - BRIDGE SHEETS (9)			
BOX CULVERT DRAWING - BARREL JOINT LOCATIONS - NORMAL & SKEWED CULVERTS GROUP I DIAGRAMS		IBJL-1	366.1
BOX CULVERT DRAWING - BARREL JOINT LOCATIONS - NORMAL & SKEWED CULVERTS GROUP II DIAGRAMS		IBJL-1	366.2
BOX CULVERT DRAWING - BARREL JOINT LOCATIONS - NORMAL & SKEWED CULVERTS GROUP III DIAGRAMS		IBJL-1	366.3
COLLAR DETAILS FOR BOX STRUCTURES		ICJ-1	367
BASIC CULVERT DRAWING - DOUBLE CELL - HEIGHT 8 FT - SPANS 16-32 FT.		IBD-8-2W	384.1
BASIC CULVERT DRAWING - DOUBLE CELL - HEIGHT 8 FT - SPANS 16-32 FT.		IBD-8-2W	384.2
WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - HEIGHTS 6-12 FT. - SPANS 12-40 FT.		IWD-3	387
WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - HEIGHTS 6-12 FT. - SPANS 12-40 FT.		IWD-3	388.1
WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - HEIGHTS 6-12 FT. - SPANS 12-40 FT.		IWD-3	388.2
CROSS-SECTIONS = 19			901-919
TOTAL SHEETS = 63			

4/15/2009 7:54 AM INDEK.DGN

WALDON

PS & E PLANS-DATE - 8/12/08		
FMS CON. # 104898 / 301000		
REVISIONS		
DATE	SHEET NO.	BY
01/08/09	3	GTW
04/15/09	6 & 7	GTW

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
DETAILED INDEX	
PROJECT NO. : HSIP-0331-00(012)	
COUNTY : SCOTT	
DATE	WORKING NUMBER
INDEX.DGN	DI-1
DESIGN TEAM WALDON CHECKED DATE	SHEET NUMBER
	2

GENERAL NOTES

STATE	PROJECT NO.
MISS.	HSIP-0331-00(012)

FOR LIST OF PUBLIC UTILITIES, SEE WK. NO. 3.

EROSION CHECKS: QUANTITY BASED ON THE BASIS OF 4 BALES PER EVERY 25 TO 100 L.F. OF DITCH, 8 BALES PER INLET, AND 4 BALES AT EACH PIPE OUTLET. THIS IS REQUIRED AS A TEMPORARY EROSION CONTROL MEASURE TO MINIMIZE SILTATION UNTIL PERMANENT MEASURES ARE INSTALLED. THE ENGINEER WILL DETERMINE THE ACTUAL LOCATION AND NUMBER OF BALES DURING THE CONSTRUCTION OF THE PROJECT. (SEE WK. NO. TEC-1 FOR DETAILS)

EXISTING PIPES TO BE ABANDONED IN PLACE SHALL BE PLUGGED WITH CONCRETE.(ABSORBED ITEM)

A SOIL PROFILE HAS BEEN PREPARED FOR THIS PROJECT USING SAMPLES TAKEN FROM HOLES AT LOCATIONS INDICATED IN THE TEST REPORTS. THIS SOIL PROFILE IS ON FILE IN THE DISTRICT AND CENTRAL CONSTRUCTION OFFICES, AND IS AVAILABLE FOR EXAMINATION. THE DEPARTMENT DOES NOT GUARANTEE THAT THE MATERIALS AS SHOWN IN THE REPORTS ARE NECESSARILY TO BE FOUND OUTSIDE THE TEST HOLES.

THE EROSION CONTROL DEVICES REFERENCED IN THESE PLANS ARE A MINIMUM REQUIREMENT. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO INSURE THAT SILT DOES NOT LEAVE THE RIGHT OF WAY OR CONTAMINATE WATERS OF THE U.S. DURING CONSTRUCTION. THE CONTRACTOR SHALL SUBMIT AN EROSION CONTROL PLAN PRIOR TO COMMENCEMENT OF WORK AND MAINTAIN THE PLAN DURING CONSTRUCTION.

WIRE BACKING SHALL BE REQUIRED ON ALL SILT FENCE.

IN APPLICABLE AREAS, THE PAVEMENT SHALL BE WIDENED TO PROVIDE A 2 FOOT PAVED SHOULDER. THE EXISTING SHOULDER SHALL BE EXCAVATED 3" AND VARIABLE BELOW THE EXISTING PAVEMENT LEVEL AND PAVED TO A DEPTH OF 4 1/2" WITH 9.5mm HMA, MT. THE SHOULDER WIDENING SHALL BE ACCOMPLISHED WITH THE MAINLINE OVERLAYING OPERATION. THE BLADED MATERIALS SHALL BE HANDLED AS MENTIONED BELOW.

WHERE APPLICABLE THE EXISTING SHOULDERS ARE TO BE RAISED TO MATCH THE NEW PAVEMENT ELEVATION BY PLACING VARIABLE DEPTH GRANULAR MATERIAL (CL. 5, GR. C) ON THE EXISTING SHOULDERS. PLACEMENT OF THE GRANULAR MATERIAL ON THE FINISHED ASPHALT COURSE SHALL NOT BE PERMITTED. THE MATERIAL SHALL BE BLADED ROLLED, AND COMPACTED TO A FINISHED SLOPE OF FOUR PERCENT (4%). PLACEMENT OF THIS MATERIAL SHALL BE PERFORMED TO PROVIDE A UNIFORM AND COMPACTED SHOULDER WITH A MINIMUM DEPTH AND WIDTH OF MATERIAL PLACED. SHOULDERS WITH ADEQUATE SHOULDER MATERIAL IN PLACE SHALL BE BLADED TO A SLOPE OF FOUR PERCENT (4%). THE COST OF BLADING WILL BE AN ABSORBED ITEM AND IS NOT TO BE INCLUDED IN THE PRICE OF PAY ITEMS BID.

REMOVAL OF THE EXISTING SHOULDER MATERIAL SHALL BE COINCIDENT WITH THE MILLING/OVERLAY OPERATION TO PREVENT THE POSSIBLE PONDING OF WATER. NO PAYMENT WILL BE MADE FOR BLADING OR REMOVAL OF THE EXISTING SHOULDER MATERIAL. ANY MATERIAL EXCAVATED FROM THE EXISTING SHOULDER SHALL BE USED TO RAISE THE EXISTING SHOULDER TO MATCH THE NEW PAVEMENT ELEVATION AND ANY SURPLUS MATERIAL SHALL BE SPREAD ALONG THE EDGE OF THE SHOULDERS, FORESLOPES, OR OTHER ADJACENT AREAS AS DIRECTED BY THE ENGINEER AND WILL BE AN ABSORBED ITEM. MATERIAL WHICH CANNOT BE PLACED IN ADJACENT AREAS AND DEEMED TO BE EXCESS EXCAVATION BY THE ENGINEER SHALL BE REMOVED UNDER PAY ITEM NO. 230-G EXCESS EXCAVATION.

THE LOCATION AND SPACING OF SIGNS SHOWN ON THE TRAFFIC CONTROL PLANS ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.

ALL ITEMS THAT CONFLICT WITH REQUIRED CONSTRUCTION, FOR WHICH NO PAY ITEM IS SHOWING, SHALL BE REMOVED AND ABSORBED AS CLEARING AND GRUBBING.

25% SHRINKAGE FACTOR USED IN THE EARTHWORK CALCULATIONS IS FOR DESIGN ESTIMATING PURPOSES ONLY.

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT EXISTING STRUCTURES SUCH AS PIPES, INLETS, APRONS, BRIDGES, ETC. FROM DAMAGE WHICH MIGHT OCCUR DURING CONSTRUCTION. EXTREME CARE SHOULD BE EXERCISED IN UNDERCUT AREAS AND THE UNDERCUT DEPTH MAY BE ADJUSTED AT CROSS-DRAINS, AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL REPLACE OR REPAIR, AS DIRECTED BY THE ENGINEER, ANY STRUCTURES DAMAGED DURING THE LIFE OF THE CONTRACT. NO PAYMENT WILL BE MADE FOR REPLACEMENT OR REPAIR OF DAMAGED ITEMS.

EXISTING UNDERGROUND UTILITY LINES ARE SHOWN ON THE PLANS BASED UPON THE BEST INFORMATION AVAILABLE TO THE ENGINEER. THE ENGINEER CANNOT AND DOES NOT WARRANT THAT THE INFORMATION IS COMPLETE OR ACCURATE. THE CONTRACTOR MUST COORDINATE DIRECTLY WITH THE INVOLVED UTILITY OWNERS TO GET UNDERGROUND UTILITY LINES FIELD LOCATED IN ADVANCE OF CONSTRUCTION.

FULL COLLARS ARE TO BE USED AT ALL BOX CULVERT EXTENSIONS AND AT ALL BOX CULVERT CONSTRUCTION JOINTS. (SEE ICJ-1 FOR DETAILS)

FLOURESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.

WHERE MILLING OF THE ROADWAY LANES IS REQUIRED, THE CONTRACTOR SHALL PROVIDE OUTLETS IN THE EXISTING SHOULDERS AT SUFFICIENT INTERVALS TO PREVENT POOLING OR STANDING WATER ON THE MILLED SURFACE. (ABSORBED ITEM)

PRIOR TO EXCAVATION AND EMBANKMENT CONSTRUCTION, ALL TOPSOIL SHALL BE STRIPPED AND STOCKPILED. AFTER COMPLETION OF EXCAVATION AND EMBANKMENT CONSTRUCTION, ALL SLOPES SHALL BE UNIFORMLY PLATED WITH THE STOCKPILED TOPSOIL. STRIPPING, STOCKPILING, PLACING AND SPREADING OF TOPSOIL WILL NOT BE MEASURED FOR PAY (NOT A SEPARATE PAY ITEM).

⚠ RIP RAP FROM SILT BASINS TO BE PLACED AT DOWNSTREAM END OF BOX CULVERT BEFORE THE PROJECT COMPLETED, AS DIRECTED BY THE ENGINEER. (ABSORBED ITEM)

1/8/2009 10:38 AM INDEX.DGN

				MISSISSIPPI DEPARTMENT OF TRANSPORTATION			
				GENERAL NOTES			
				PROJ. NO.: HSIP-0331-00(012)		WORKING NUMBER	
				COUNTY: SCOTT		GN-1	
				FILENAME: _____ INDEX.DGN			
				DESIGN TEAM: WALDON CHECKED: _____ DATE: _____			
				SHEET NUMBER			
				3			