

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) enclose a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. 1 DATED _____ ADDENDUM NO. _____ DATED _____
ADDENDUM NO. _____ DATED _____ ADDENDUM NO. _____ DATED _____

Number	Description
1	Revised NTB 3124; Wage Rate, replaces same; Amendment EBS Download Required.

TOTAL ADDENDA: 1
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.

HSIP-0054-02(023) / 105874301 HSIP-0020-01(194) / 105874302 Hinds County(ies)

Revised 09/21/2005

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS 3124

CODE: (SP)

DATE: 07/20/2010

SUBJECT: Scope Of Work

PROJECT: HSIP-0054-02(023) & HSIP-0020-01(194) / 105874301 & 302 - Hinds County

The contract documents do not include an official set of construction plans, but may by reference include some Standard Drawings when so specified in a Notice To Bidders entitled, "Standard Drawings". All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Bidders are advised that work under this contract consists of the following 2 locations and that the first order of work shall be the completion of the overlay of I20 (Location 1). Work on Location 2 (SR27) shall not commence prior to the placement of all required HMA/WMA on I20.

Location 1

HSIP-0020-01(194) / 105874-302000 - I20 IN HINDS COUNTY

The work shall consist of overlaying approximately 1,300 feet of existing asphalt pavement on I-20 Eastbound in Hinds County, for the purpose of correcting the superelevation rate of the roadway and the adjacent I20 entrance ramp from Norrell Road, and to provide an overall 1 lift overlay of the roadway, entrance ramp, and paved shoulders after correction of the cross slopes and superelevation rates (See Typical Section).

Prior to overlay, the Department will determine the existing cross slopes and superelevation rates of the eastbound lanes of I20 and the I20 entrance ramp from Norrell Road. The Department will determine the overlay grade profile required to correct the mainline lanes and the entrance ramp to the required cross slopes and superelevation rates. The Department will provide all staking necessary for the establishment of the required superelevation rates and grade profile in the field for all required courses of HMA/WMA.

The Contractor shall place that total thickness of HMA/WMA leveling course, 12.5mm Mix, HT, Polymer Modified, which is necessary in order to bring the I20 lanes and the I20 entrance ramp from Norrell Road to the required cross slopes, superelevation rates, and grade profile as determined and staked by the Department. Maximum single lift thickness for the leveling course(s) is 2 ½". After placement of the leveling course(s), an overall lift of HMA/WMA, 9.5mm Mix, HT, Polymer Modified, 1 ½" thick will be placed across the I20 lanes and the I20 entrance ramp from Norrell Road. The existing paved shoulders along I20 and the entrance ramp from Norrell Road will also be overlaid. The thickness and course designations for the overlay of the shoulders are to match that of the adjacent main line or ramp. The break over slope

between the right edge (high side) of the mainline or ramp and the paved shoulder shall be -7% minus the superelevation rate, not to exceed - 4%.

Potholes that may exist are to be patched as directed by the Engineer prior to beginning the asphalt overlay. Patching of potholes shall be considered an absorbed item.

Cracks of significant depth or depressions in the existing surface which, in the opinion of the Engineer, may cause reflective cracking shall be filled with HMA immediately prior to overlay operations. No separate payment will be made for this operation.

Ground in rumble strips are to be placed on the mainline paved shoulders.

Existing raised pavement markers are to be removed prior to beginning the overlay operation. No measurement will be made for separate payment; the cost is to be absorbed in the prices for other items of work.

All permanent striping will be thermoplastic. Edge lines will be placed so as to maintain the original lane width. In addition to these markings, raised pavement markers will be placed at 80 foot intervals in tangents, 40 foot intervals in curves or as directed by the Engineer.

Temporary traffic stripe will be required immediately after placement of each lift of HMA and prior to opening to traffic. Temporary stripe is to be placed in the same location and configuration as the permanent stripe. Temporary raised pavement markers are required, and are to be placed as soon as the overlay is completed.

The Contractor shall provide all traffic control devices in accordance with the Standard Specifications, special provisions and drawings included elsewhere in the contract. Payment shall be made under pay item number 618-A001, Maintenance of Traffic.

The Contractor shall erect and maintain construction signing and provide all signs and traffic handling devices necessary to safely maintain traffic around and through the work areas in accordance with the Standard Drawing and the MUTCD. The cost is to be included in the price bid for Pay Items . 619-D1001 & 619-D2001.

Incidental work such as removing vegetation, removing and resetting signs, shaping and compacting shoulders, removing excess asphalt material, project clean-up, and other incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

It shall be the responsibility of the Contractor to protect existing structures such as pipes, aprons, bridges, etc., from damage occurring during construction. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged during the life of the contract. No payment will be made for replacements and or repairs resulting from such damages.

Location 2

HSIP-0054-02(023) / 105874-301000 - SR27 IN HINDS COUNTY

The work shall consist of milling and overlaying approximately 23.4 miles of the existing asphalt pavement on MS 27 in Hinds County, from the Copiah County Line to the intersection of MS 27 and MS 18 at Utica (approximately 11.2 miles), and from the intersection of MS 27 and MS 18 just north of Utica to the Warren County Line (approximately 12.2 miles).

The width of cold milling is 24 feet and variable and the depth is 2" and variable. The thickness of the HMA/WMA overlay is 2" and variable.

Publicly maintained roads or streets are to be milled and overlaid to the existing right-of-way. Currently paved privately owned driveway entrances or pads shall be milled and overlaid to the shoulder line per the typical drawing in the contract. Currently un-paved privately owned driveway entrances or pads shall be paved to the shoulder line per the typical drawing in the contract. Pads shall be shaped horizontally and vertically to prevent excessive drop-offs. Granular material (Class 5, Group 'C') shall be provided around the pads to prevent shoulder drop-offs as directed and shall be placed in a timely manner. Drop-offs exceeding 2.5" shall be corrected within 2 calendar days of the placement of the pad. Stabilizer aggregate shall be used to maintain egress and ingress as directed by the Engineer.

Prior to beginning overlay operations, preliminary leveling of the existing roadway in selected short locations will be required to correct vertical alignment as directed by the Engineer.

Potholes are to be patched prior to paving operations as directed by the Engineer. Patching of potholes shall be considered an absorbed item.

Unpaved shoulders are to be paved two (2) feet wide on each lane in locations where existing shoulder widths allow. It is estimated that 50% of the project length has shoulders sufficiently wide to accommodate the 2' pavement widening. Locations to be widened will be determined by the Engineer. Prior to placement of the pavement widening, the shoulders are to be bladed 2 ½ inches below the milled surface. The cost of blading will be an absorbed item and is not to be included in the price of pay items bid. The widening will consist of 4.5" HMA/WMA, 12.5mm Mix, HT, Polymer Modified, and will be placed in one lift with the placement of the overlay.

All existing guard rail, terminal end sections, and bridge-end sections shall be removed and replaced. Voids created by the removal of posts, concrete anchors, footings, etc., shall be backfilled and tamped with suitable soil obtained from the adjacent shoulder. Terminal end sections are to be installed per manufacturer's specifications. (See Table)

The Contractor shall be responsible for the removal of existing raised pavement markers on the roadway and bridges, as well as for the removal of non-paint traffic striping, and any painted traffic striping which will not be covered by the new permanent thermoplastic striping on existing bridge deck surfaces prior to placement of the new raised pavement markers and new

thermoplastic striping. Cost of removing existing raised pavement markers is to be included in the cost of other items of work. Cost of stripe removal to be paid under Pay Item 202-B076.

The ground in rumble strips (center line and edge line) and all pavement markings shall be installed in accordance with the Standard Specifications, special provisions and enclosed drawings.

The Contractor shall provide all traffic control devices in accordance with the Standard Specifications, special provisions and enclosed drawings. Payment shall be made under pay item number 618-A001, Maintenance of Traffic. Incidental work that is necessary to complete the work shall not be measured for separate payment, and the cost shall be included in the items bid.

Temporary stripe, either paint or tape, will be required immediately after overlaying and prior to opening any area to traffic. Payment will be made for temporary traffic stripe, under Pay Item Nos. 619-A1002 through 619-A6001. Temporary stripe is to be placed in the same location and layout as permanent stripe. Temporary raised pavement markers are required, and are to be placed as soon as a section of overlay is completed.

All permanent striping will be thermoplastic. Edge lines will be placed so as to maintain the original lane width. In addition to these markings, raised pavement markers will be placed at 80 foot intervals in tangents, 40 foot intervals in curves and as directed by the Engineer.

The Contractor shall erect and maintain construction signing and provide all signs and traffic handling devices necessary to safely maintain traffic around and through the work areas in accordance with the STANDARD DRAWINGS and the MUTCD.

The cost is to be included in the price bid for Pay Items 619-D1001 & 619-D2001.

Where applicable the existing shoulders are to be raised to match the new pavement elevation by placing variable depth Granular Material (Class 5, Group C) on the existing shoulders. Placement of the granular material on the finished asphalt course shall not be permitted. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%). The quantity of material placed shall be the minimum amount required to provide a finished shoulder to match the existing width. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%). The cost of blading will be an absorbed item and is not to be included in the price of pay items bid.

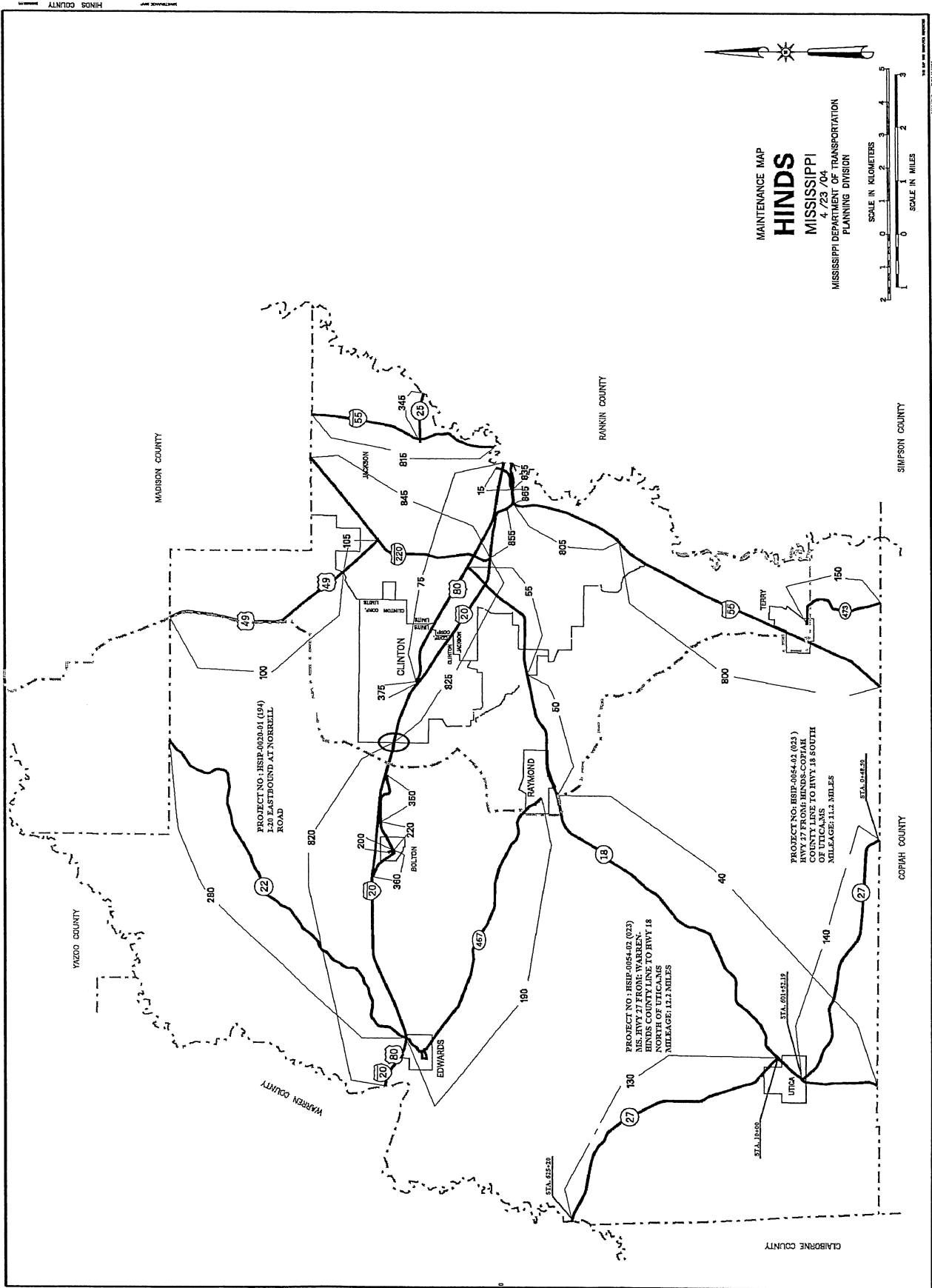
All material excavated from the existing shoulder shall be used to raise the existing shoulder to match the new pavement elevation where needed, and any surplus material shall be spread along the edge of the shoulders, foreslopes, or other adjacent areas as directed by the Engineer and will be an absorbed item

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

The Reclaimed Asphalt Pavement (RAP) material removed by the milling operation on Hwy 27 only shall become the property of the contractor with the exception of 10,000 tons or 50% of the total estimated quantity, whichever is less, to be stockpiled at the MDOT storage location approximately 300' south of the intersection of south MS 27 and MS 18, in the southwest quadrant. Unless the Contractor desires otherwise, the Contractor's RAP shall be obtained first. At the MDOT storage location, the Contractor shall be required to push the milled material into one stockpile. The Contractor shall be responsible for placing and operating equipment necessary to "push up" the milled material. All costs associated with the hauling, placing, and stockpiling the RAP shall be included in the price bid for cold milling.

Joint sealing and shoulder wedge procedures shall be utilize on both routes of the project in accordance with supplements to Special Provisions 907-401-2 as well as 907-403-4.

It shall be the responsibility of the Contractor to protect existing structures such as pipes, aprons, bridges, etc., from damage occurring during construction. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged during the life of the contract. No payment will be made for replacements and or repairs resulting from such damages.



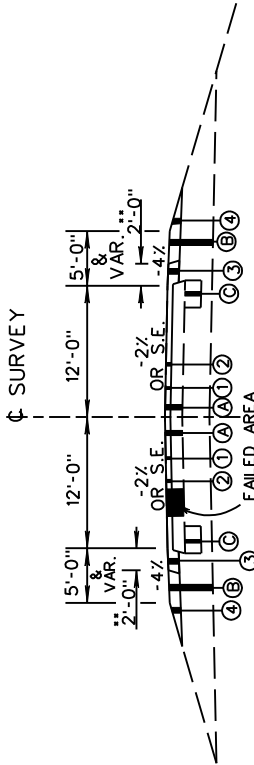
STATE	PROJECT NO.
MSS.	

NOTE:
GROUND IN RUMBLE STRIPS REQUIRED ON THE CENTERLINE AND BOTH EDGES OF PAVEMENT SEE DETAILS ON SHEETS DRS_CL AND D2L_ERS

FAILED AREAS SHALL BE REPAIRED WITH FULL DEPTH HMA/WMA 12.5mm MIX HT, POLYMER MODIFIED

EXISTING PAVEMENT

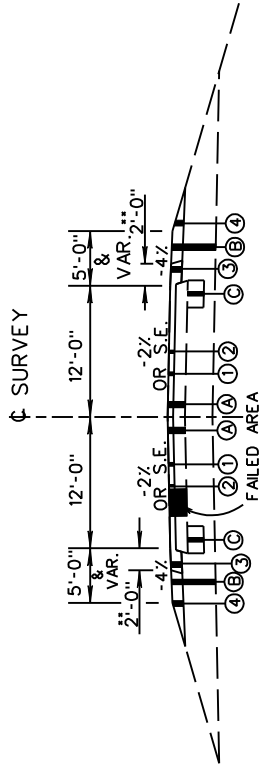
- Ⓐ 9.5" AND VARIABLE DEPTH HOT MIX ASPHALT IN PLACE
- Ⓑ 8.0" AND VARIABLE DEPTH CLAY-GRAVEL BASE IN PLACE
- Ⓒ 10.5" AND VARIABLE DEPTH HMA TRENCH WIDENING IN PLACE



TYPICAL SECTION HWY. 27

B.O.P. STA. 0+48.30 TO STA. 601+52.19
(FROM COPIAH LINE TO HWY 18 AT UTICA)

** 2'-0" WIDENING REQUIRED IN ALL LOCATIONS WHERE EXISTING SHOULDERS HAVE SUFFICIENT WIDTH



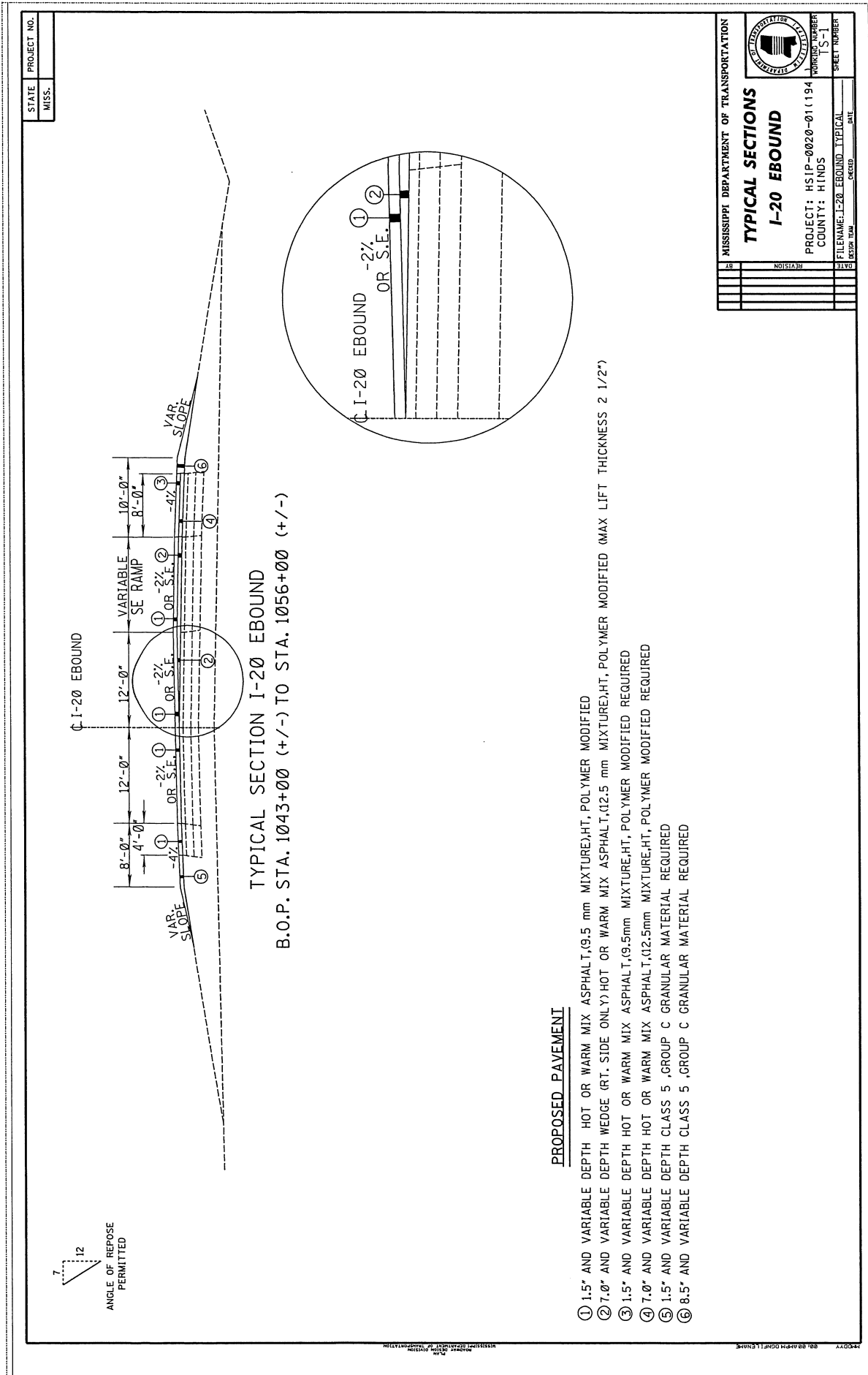
TYPICAL SECTION HWY. 27

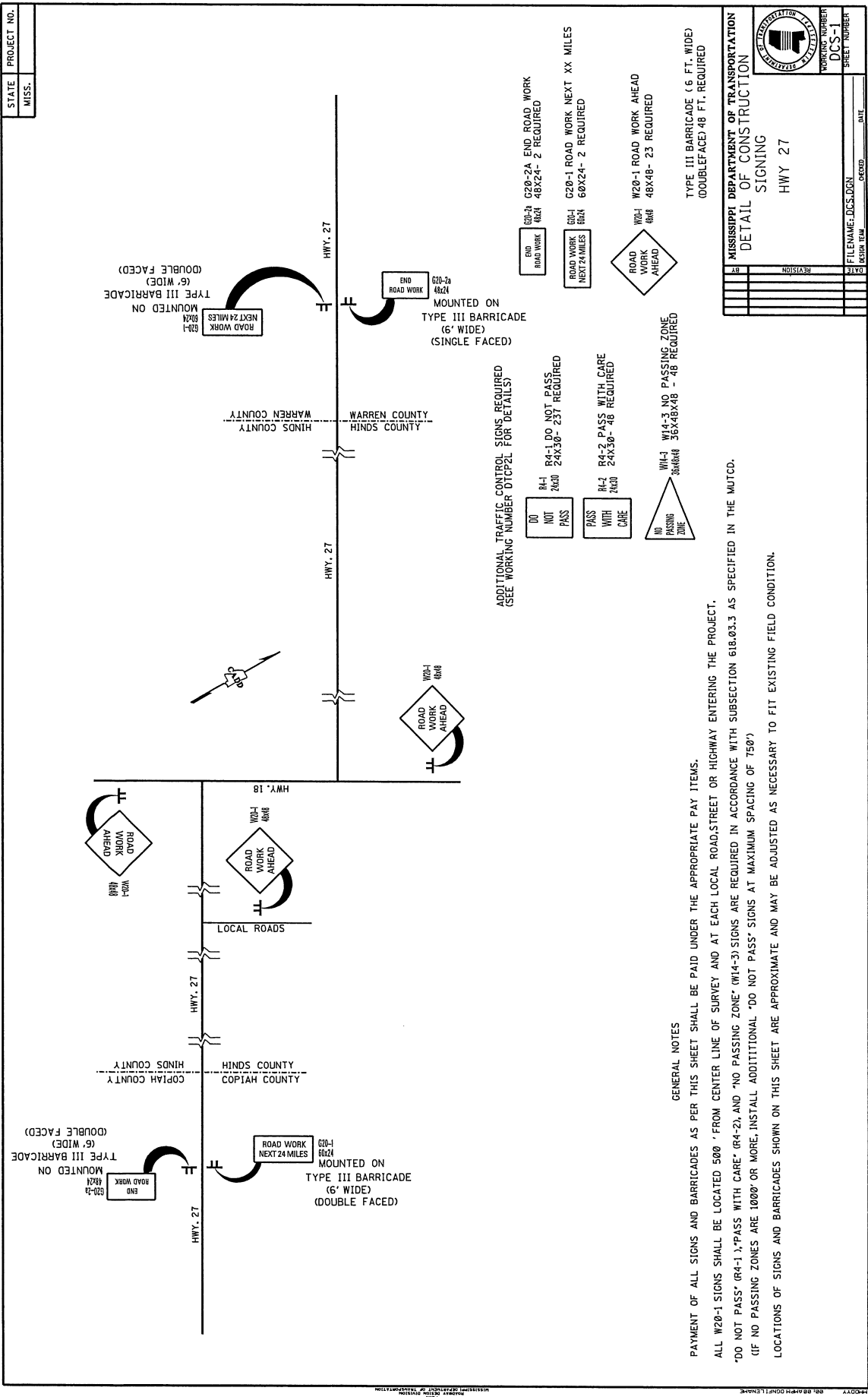
STA. 10+00 TO STA. 625+20
(FROM HWY 18 JUST NORTH OF UTICA TO WARREN COUNTY LINE)

PROPOSED PAVEMENT

- Ⓐ 2.0" AND VARIABLE MILLING REQ'D.
- Ⓑ 2.0" AND VARIABLE DEPTH HOT OR WARM MIX ASPHALT, (12.5 mm MIXTURE), HT, POLYMER MODIFIED
- Ⓒ EXCAVATE 2.5" BELOW MILLED SURFACE AND PLACE 4.5" HOT OR WARM MIX ASPHALT, 12.5mm MIX, HT, POLYMER MODIFIED REQ'D. (1 @ 4.5" WITH SURFACE LIFT)
- Ⓓ VARIABLE DEPTH CLASS 5, GROUP C GRANULAR MATERIAL REQUIRED

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTION	
HWY 27	
PROJECT: HSIP-0064-02(023)	
COUNTY: HINDS	
DATE	DESIGNER
REVISION	CHECKED
	DATE
	FILENAME: HWY27TYPICAL.DGN
	SHEET NUMBER





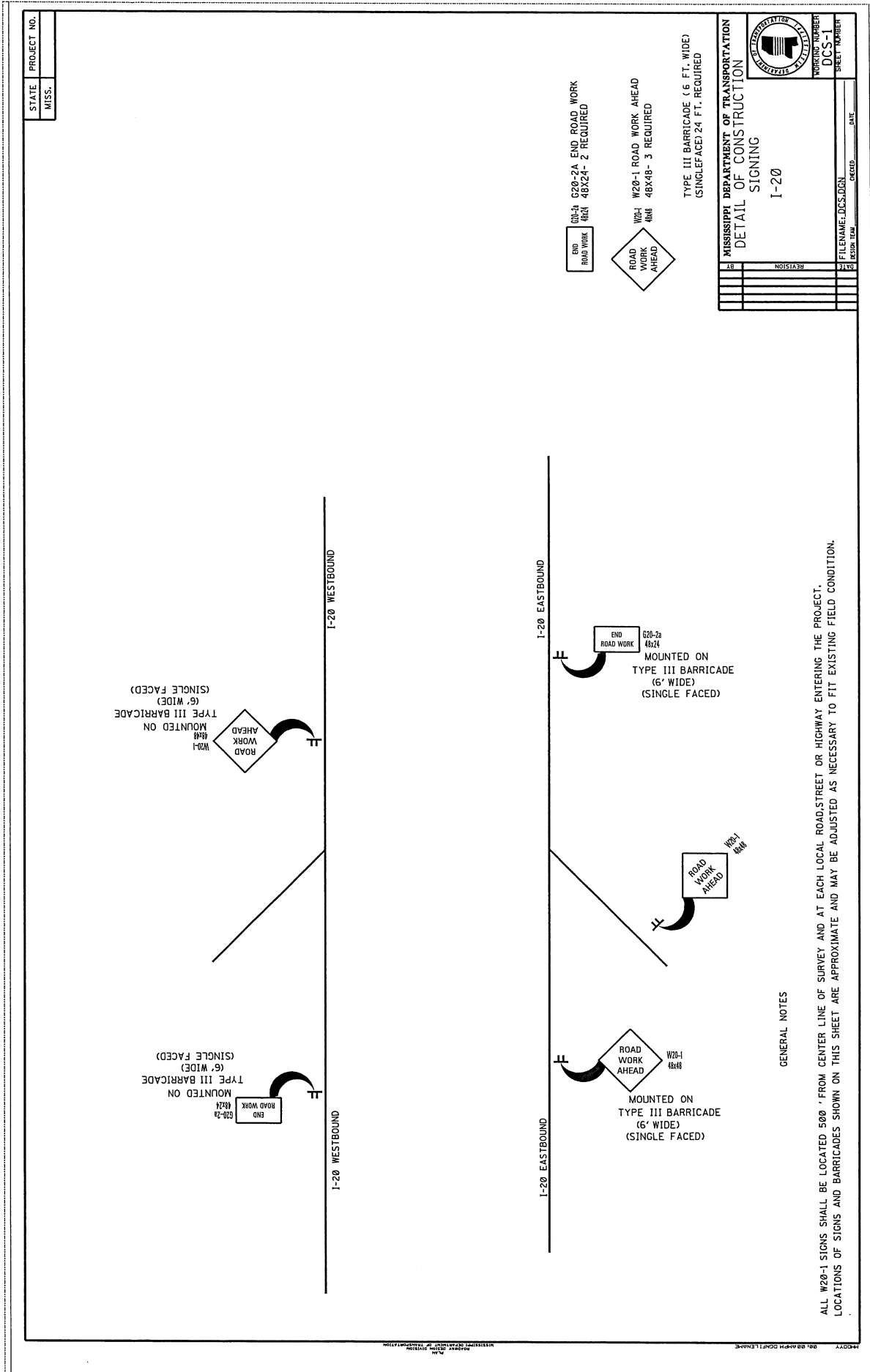
STATE MISS.	PROJECT NO.
MISSISSIPPI DEPARTMENT OF TRANSPORTATION DETAIL OF CONSTRUCTION SIGNING HWY 27	
WORKING NUMBER DCS-1	SHEET NUMBER
FILE NAME: DCS.DGN	DATE
DESIGN TEAM	

ADDITIONAL TRAFFIC CONTROL SIGNS REQUIRED (SEE WORKING NUMBER DTP22 FOR DETAILS)

- DO NOT PASS
- PASS WITH CARE
- NO PASSING ZONE
- R4-1 DO NOT PASS
- R4-2 PASS WITH CARE
- W14-3 NO PASSING ZONE

GENERAL NOTES

PAYMENT OF ALL SIGNS AND BARRICADES AS PER THIS SHEET SHALL BE PAID UNDER THE APPROPRIATE PAY ITEMS.
 ALL W20-1 SIGNS SHALL BE LOCATED 500' FROM CENTER LINE OF SURVEY AND AT EACH LOCAL ROAD, STREET OR HIGHWAY ENTERING THE PROJECT.
 "DO NOT PASS" (R4-1), "PASS WITH CARE" (R4-2), AND "NO PASSING ZONE" (W14-3) SIGNS ARE REQUIRED IN ACCORDANCE WITH SUBSECTION 618.03.3 AS SPECIFIED IN THE MUTCD.
 (IF NO PASSING ZONES ARE 1000' OR MORE, INSTALL ADDITIONAL "DO NOT PASS" SIGNS AT MAXIMUM SPACING OF 750')
 LOCATIONS OF SIGNS AND BARRICADES SHOWN ON THIS SHEET ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT EXISTING FIELD CONDITION.



STATE	PROJECT NO.
MISS.	

END ROAD WORK
G20-2a
48x24
48x24

ROAD WORK AHEAD
W20-1
48x48
48x48

TYPE III BARRICADE (6 FT. WIDE)
(SINGLEFACE) 24 FT. REQUIRED

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
DETAIL OF CONSTRUCTION
SIGNING
1-20

NO.	REVISION	DATE

WORKING NUMBER
DCS-1
SHEET NUMBER

FILENAME: DCS.DGN
REVISION TEAM
CHECKED
DATE

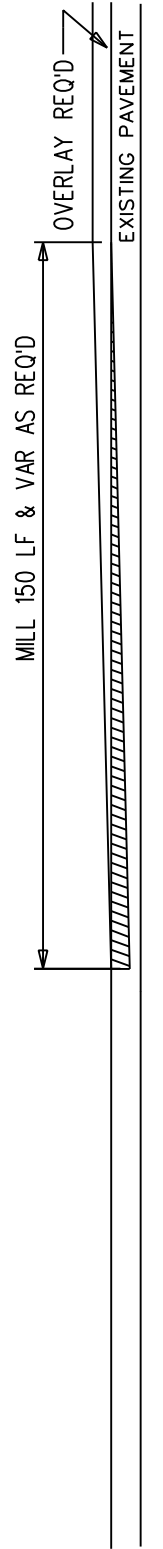
GENERAL NOTES

ALL W20-1 SIGNS SHALL BE LOCATED 500' FROM CENTER LINE OF SURVEY AND AT EACH LOCAL ROAD, STREET OR HIGHWAY ENTERING THE PROJECT. LOCATIONS OF SIGNS AND BARRICADES SHOWN ON THIS SHEET ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT EXISTING FIELD CONDITION.

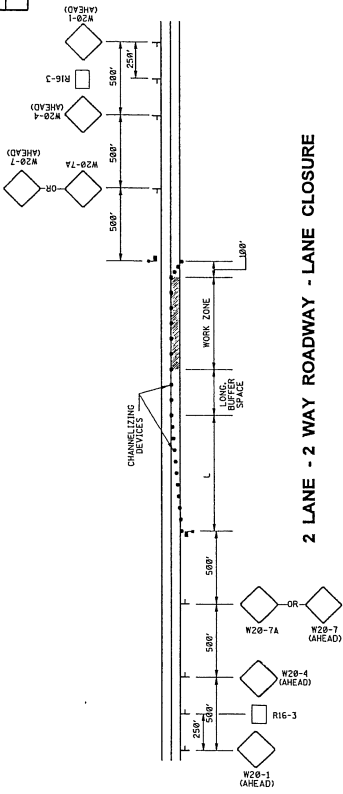
Guard Rail Quantities

STATION	DIRECTION	GUARD RAIL REG.	TERMINAL END SECTION NONFLARED	CABLE ANCHOR TYPE 1	BRIDGE END SECTION TYPE A	DELINEATOR WHITE
625+20	NB	97.5	1		1	4
625+20	SB	37.5			1	
623+39	NB	37.5	1		1	
623+39	SB	97.5			1	4
620+23.5	NB	97.5	1		1	4
620+23.5	SB	37.5			1	
496+65	NB	62.5	1		1	
496+65	SB	112.5			1	4
495+65	NB	112.5	1		1	4
495+65	SB	62.5			1	
200+94	NB	62.5	1		1	
200+94	SB	112.5			1	4
200+18	NB	112.5	1		1	4
200+18	SB	62.5			1	
536+50	NB	42.5	1	1		3
536+50	SB	42.5	1	1		
500-50	NB	62.5	1		1	
500-50	SB	112.5			1	4
498+03	NB	112.5	1		1	4
498+03	SB	62.5			1	
7+44	NB	37.5	1		1	
7+44	SB	112.5			1	4
6+24	NB	112.5	1		1	4
6+24	SB	37.5			1	
	UNITS	LIN.FT.	EACH	EACH	EACH	EACH
		1840	13	2	22	47

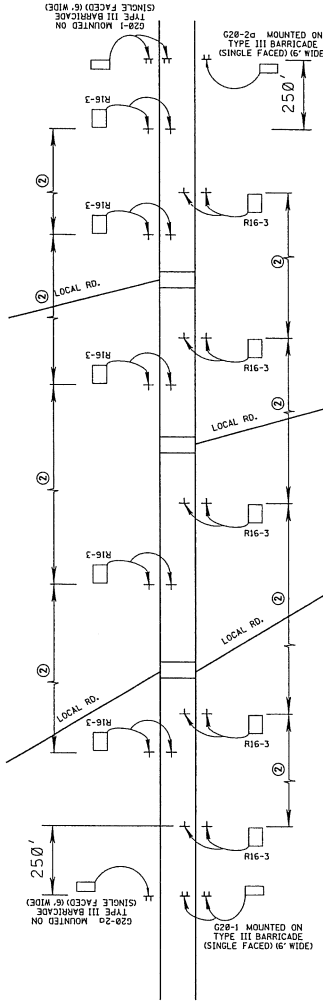
BOP and EOP Paving Detail



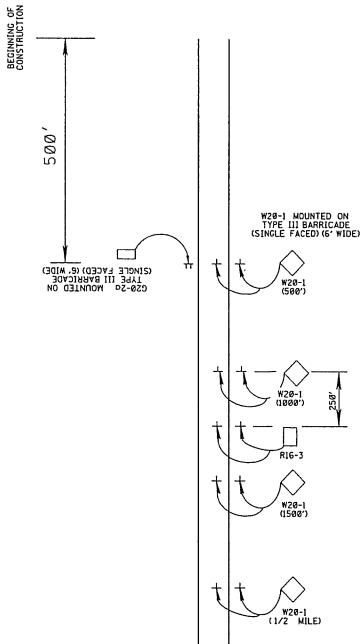
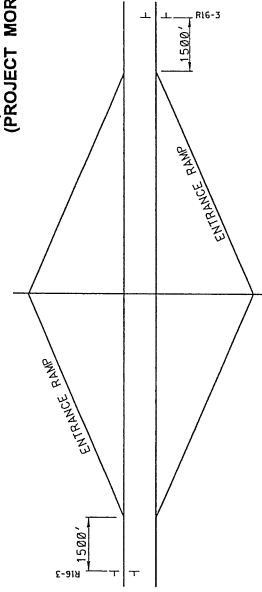
STATE PROJECT NO.
MISS.



2 LANE - 2 WAY ROADWAY - LANE CLOSURE



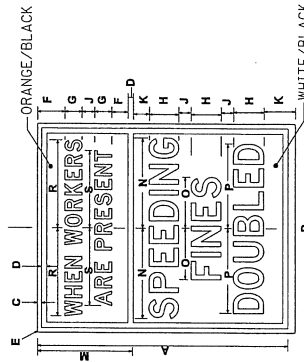
**DIVIDED HIGHWAY SHOWN
(2 LANE - 2 WAY ROADWAY SIMILAR)
(PROJECT MORE THAN 1 MILE LENGTH)**



**DIVIDED HIGHWAY
(PROJECTS LESS THAN 1 MILE LENGTH)**

NOTES

- ⊙ R16-3 SIGN TO BE PLACED AS SHOWN OR AS DIRECTED BY THE ENGINEER.
- ⊙ R16-3 SIGN SHALL BE SPACED AT A MAXIMUM OF 2 MILES THROUGHOUT LENGTH OF PROJECT.



SIGN	DIMENSIONS (INCHES)		DIMENSIONS (INCHES)	
S10	A	B	C	D
S10	68	48	1 1/4	3 3/4
S10	J	K	M	N
S10	3	6 3/4	27 1/2	21
S10	A	B	C	D
S10	48	36	1 1/4	3 3/4
S10	J	K	M	N
S10	3	6 3/4	17 1/2	15 1/2

48" x 60" (INTERSTATE USE)
36" x 48" (ALL OTHER HIGHWAYS)

R16-3

DATE	REVISION	BY

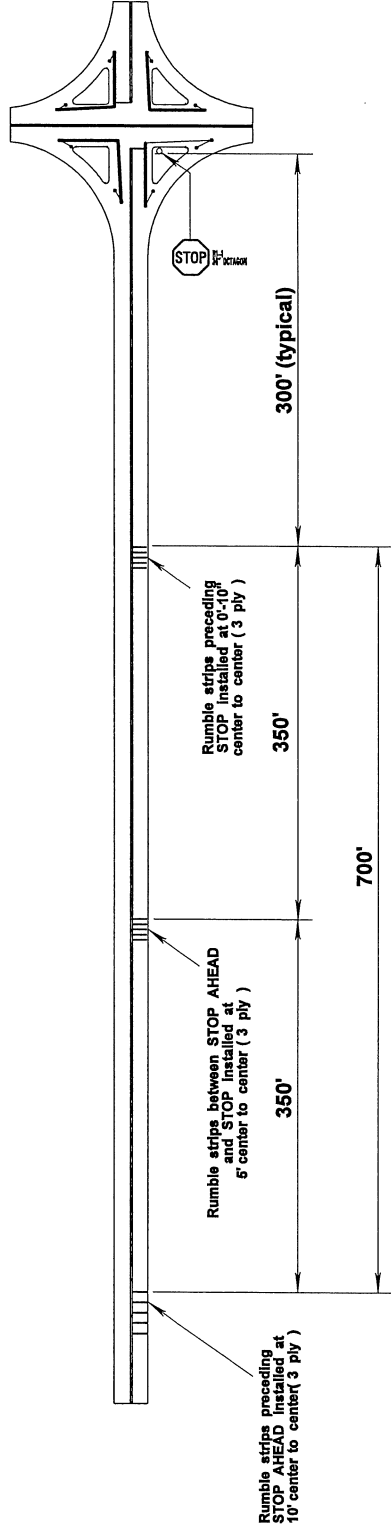
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

LOCATION OF R16-3 SIGNS

WORKING NUMBER: _____ SHEET NUMBER: _____

FILENAME: SPEED SIGN DETAIL.dwg DATE: 04-24-08

STATE	PROJECT NO.
MISS.	



NOT TO SCALE

NOTES:

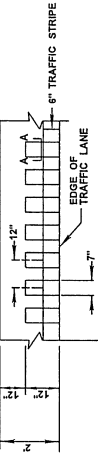
- 1. 1 set of rumble strips approx. 250' from STOP AHEAD
- 2. 1 set of rumble strips approx. 300' from STOP
- 3. 1 set of rumble strips approx. Halfway between first and last set
- 4. Rumble strips to be 6" thermoplastic (120 mill/each ply, 360 mil total)
- 5. 5 rumble strips per set minimum
- 6. Installation may vary due to terrain
- 7. Signs should be 48" for channelized intersection, 36" for non-channelized intersection

MISSISSIPPI DEPARTMENT OF TRANSPORTATION		
DETAILS OF STOP RUMBLE STRIPS		
NO.	DESCRIPTION	DATE

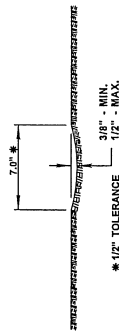
WORKING NUMBER	DATE
SSR-1	
SHEET NUMBER	

FILE NAME:	CHECKED
ISSUED TEAM:	

STATE	PROJECT NO.
MISS.	

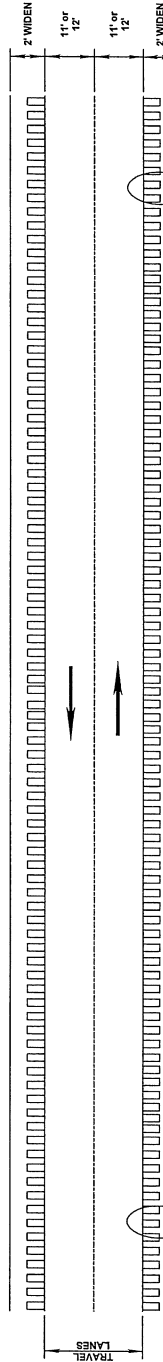


DETAIL "A"



SECTION "A-A"

- GENERAL NOTES**
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
 - GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 - COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.
 - DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.

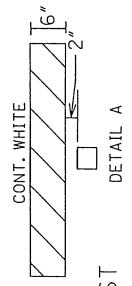
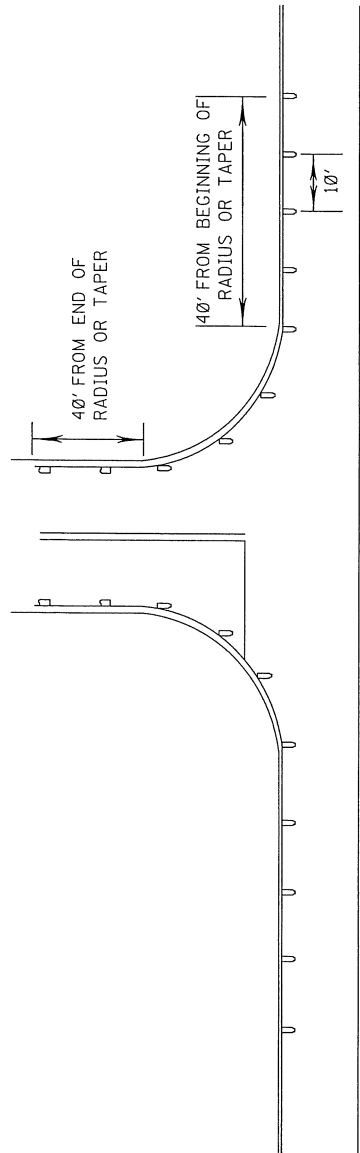


PLAN
NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
SPECIAL DESIGN: RUMBLE STRIPES (GROUND-IN) 2 LANE	
PROJECT NO.:	
COUNTY :	
FILE NAME: D2L ERS.DGN	DATE
DESIGNER: TLM	CHECKED:
PAGE	
REVISION	
BY	
DATE	
WORKING NUMBER D2L ERS	SHEET NUMBER

STATE	PROJECT NO.
MISS.	

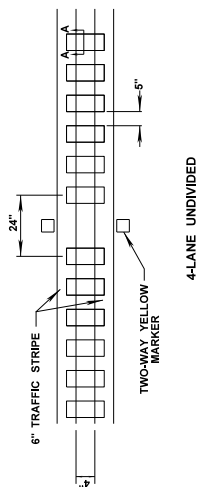
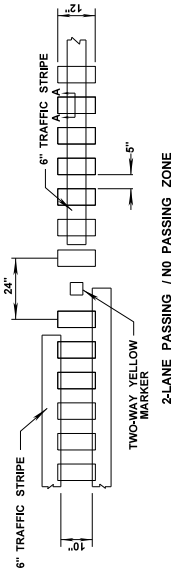
TYPICAL FOR RAISED PAVEMENT MARKERS
PLACED ON SIDE ROAD RADIUS



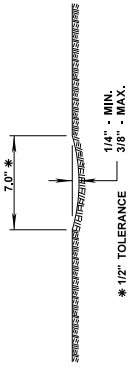
- NOTE 1. MARKERS SHALL BE PLACED EVERY 10 FEET.
- NOTE 2. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
- NOTE 3. MARKERS SHALL BE HIGH PERFORMANCE TWO WAY CLEAR.
- NOTE 4. FIVE (5) MARKERS SHALL BE PLACED ALONG MAINLINE EDGE STRIPE.
- NOTE 5. MARKERS FOR COUNTY ROADS SHALL CONTINUE DOWN THE EDGE STRIPE A DISTANCE OF 40 FEET.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
STRIPING DETAILS	
FOR	
LOCAL ROADS	
PROJECT NO.:	
COUNTY :	
FILE NAME: LRD-1.DGN	
DESIGN TEAM	DATE
REVISION	
DATE	
PROJECT NUMBER	SHEET NUMBER
LRD-1	

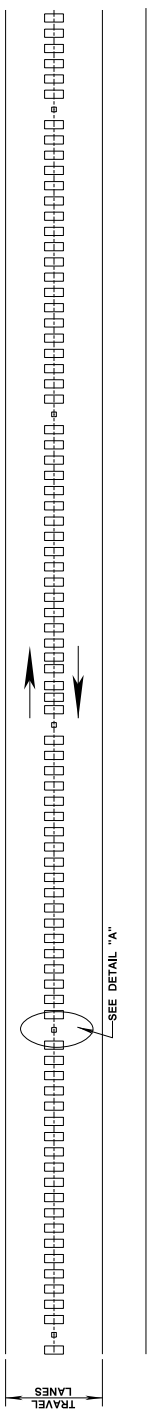
STATE	PROJECT NO.
MISS.	



DETAIL "A"



SECTION "A-A"



PLAN
NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

CENTERLINE RUMBLE STRIPS DETAIL

PROJECT NO.:
COUNTY:

FILE NAME: DBS-CL.DGN
DESIGN TEAM

DATE

BY	REVISION	DATE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

WORKING NUMBER: DBS-CL

SHEET NUMBER: 3

General Decision Number: MS100223 07/02/2010 MS223

Superseded General Decision Number: MS20080223

State: Mississippi

Construction Type: Highway

Counties: Copiah, Hinds and Rankin Counties in Mississippi.

HIGHWAY CONSTRUCTION PROJECTS (excluding tunnels, building structures in rest area projects & railroad construction; bascule, suspension & spandrel arch bridges designed for commercial navigation, bridges involving marine construction; and other major bridges).

Modification Number	Publication Date
0	03/12/2010
1	07/02/2010

* ELEC0480-007 07/01/2010

	Rates	Fringes
ELECTRICIAN.....	\$ 22.60	8.11

SUMS2008-141 09/04/2008

	Rates	Fringes
CEMENT MASON/CONCRETE FINISHER...	\$ 12.85	0.39
LABORER: Common or General.....	\$ 8.25	0.00
LABORER: Pipelayer.....	\$ 10.17	0.00
OPERATOR: Backhoe.....	\$ 13.38	0.00
OPERATOR: Broom.....	\$ 8.00	0.00
OPERATOR: Bulldozer.....	\$ 9.00	0.00
OPERATOR: Grader/Blade.....	\$ 11.67	0.00
OPERATOR: Mechanic.....	\$ 13.00	0.00
OPERATOR: Piledriver.....	\$ 12.50	1.23
OPERATOR: Roller.....	\$ 10.00	0.00
OPERATOR: Scraper.....	\$ 10.00	0.00
TRUCK DRIVER.....	\$ 10.00	0.00

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.
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Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as

provided in the labor standards contract clauses (29 CFR 5.5(a)(1)(ii)).

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In the listing above, the "SU" designation means that rates listed under the identifier do not reflect collectively bargained wage and fringe benefit rates. Other designations indicate unions whose rates have been determined to be prevailing.

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WAGE DETERMINATION APPEALS PROCESS

1.) Has there been an initial decision in he matter? This can be:

- * an existing published wage determination
- * a survey underlying a wage determination
- * a Wage and Hour Division letter setting forth a position on a wage determination matter
- * a conformance (additional classification and rate) ruling

On survey related matters, initial contact, including requests for summaries of surveys, should be with the Wage and Hour Regional Office for the area in which the survey was conducted because those Regional Offices have responsibility for the Davis-Bacon survey program. If the response from this initial contact is not satisfactory, then the process described in 2.) and 3.) should be followed.

With regard to any other matter not yet ripe for the formal process described here, initial contact should be with the Branch of Construction Wage Determinations. Write to:

Branch of Construction Wage Determinations
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

2.) If the answer to the question in 1.) is yes, then an interested party (those affected by the action) can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7).
Write to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.

Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and by any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3.) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

4.) All decisions by the Administrative Review Board are final.

END OF GENERAL DECISION