

**SECTION 905 -- PROPOSAL (CONTINUED)**

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) enclose a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.   1   DATED   1/14/2011   ADDENDUM NO.            DATED             
 ADDENDUM NO.            DATED            ADDENDUM NO.            DATED           

Number	Description
1	Table of Contents, replace same; Revised NTB 3332, replaces same; Add NTB 3367; Revised Supplement 907-401-2, replaces same; Revised Bidsheets, replace same; Amendment EBS Download Required.

TOTAL ADDENDA:   1    
 (Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE \_\_\_\_\_

\_\_\_\_\_  
 Contractor

BY \_\_\_\_\_  
 Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.

Revised 09/21/2005

MP-5080-51(022) / 304022301

Newton County(ies)

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

## TABLE OF CONTENTS

**PROJECT: MP-5080-51(022) / 304022301 -- Newton County**

901--Advertisement

904--Notice to Bidders:

- Governing Specs. - # 1
- Final Cleanup - # 3
- On-The-Job Training Program - # 777
- Payroll Requirements - # 883
- Rumble Stripe - # 1312
- Errata and Modifications to 2004 Standard Specifications - # 1405
- Advancement of Materials - # 1546
- Safety Apparel - # 1808
- Federal Bridge Formula - # 1928
- Petroleum Products Base Price - # 2858
- Standard Drawings - # 2862
- Reduced Speed Limit Signs - # 2937
- Alternate Asphalt Mixture Bid Items - # 3039
- Temporary Traffic Paint - # 3131
- Warm Mix Asphalt (WMA) - # 3242
- Scope of Work, W/Attachments - # 3332
- Contract Time - # 3333
- Specialty Items - # 3334
- Shoulder Wedge - # 3367

907-101-4: Definitions

907-102-4: Bidding Requirements and Conditions, W/Supplement

907-103-8: Award and Execution of Contract

907-105-3: Cooperation By Contractors, W/Supplement

907-107-7: Legal Relations and Responsibility to Public, W/Supplement

907-108-18: Prosecution and Progress, W/Supplement

907-109-4: Measurement and Payment, W/Supplement

907-304-12: Granular Courses

907-401-2: Hot Mix Asphalt (HMA), W/Supplement

907-401-4: Warm Mix Asphalt (WMA), W/Supplement

907-403-4: Hot Mix Asphalt (HMA), W/Supplement

907-403-9: Warm Mix Asphalt (WMA), W/Supplement

907-407-1: Tack Coat

907-413-2: Sawing & Sealing Transverse Joints in Asphalt Pavement

907-501-3: Price Adjustment for Thickness

907-618-1: Additional Signing Requirements, W/Supplement

907-618-4: Placement of Temporary Traffic Stripe

907-626-15: Thermoplastic Traffic Markings

907-703-8: Aggregates, W/Supplement

907-707-2: Joint Material

**-CONTINUED ON NEXT PAGE-**

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 - NOTICE TO BIDDERS NO. 3332**

**CODE: (SP)**

**DATE: 12/14/2010**

**SUBJECT: Scope of Work**

**PROJECT: MP-5080-51(022) / 304022301 – Newton County**

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings". All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following:

Mill and overlay approximately 3.5 miles of existing composite pavement on U.S. Hwy 80 in Newton County from the end of the 4-lane east of SR15 (Station 21+88) to just east of the Veteran's Memorial Cemetery (Station 214+88).

### **From the BOP at Station 21+88 to 197+74 and from 206+60 to EOP at Station 214+88**

Work in this area shall consist of repairing failed areas in the right and left lanes, widening the travel lanes, milling and overlaying the travel lanes with 2" and variable 12.5 mm asphalt pavement, MT. The pavement shall be milled 1-1/2" and overlaid with 2" at centerline and 1-1/2" at the edge of the travel lane. The existing lanes are 10' in width and are to be widened 2' to provide a 10' lane and a 2' paved shoulder. The existing shoulder shall be excavated 8-1/2" below the existing pavement surface and widened 2' with 7" (2 @ 3-1/2") of 19.0mm asphalt pavement, MT, Trench Widening prior to the mill and overlay. The excavated material shall be retained and used to raise the existing shoulder to match the new pavement elevation. The cost of blading will be an absorbed item and is not to be included in the price of pay items bid. Material which cannot be placed and blended in adjacent areas and deemed to be excess excavation by the Engineer shall be removed under Pay Item No. 203-G Excess Excavation. Some areas of the widening may contain variable depth bituminous material originally used to patch low spots in the shoulder. This material shall be excavated and removed prior to the paving operation and will not be measured for separate payment. A 150' milling transition (1-1/2" to 0") as shown on the attached detail shall be used from station 196+24 to 197+74 and 206+60 to 208+10 to transition from the mill & overlay section to the overlay section.

### **From Station 197+74 to Station 206+60**

Work in this area shall consist of overlaying the existing pavement with 2" of 12.5mm asphalt pavement, MT. Subsequent to the overlay, the shoulders shall be constructed using granular material as required by the typical section.

Prior to the milling and paving operations, failed areas in the existing pavement shall be removed and backfilled with 19.0mm MT asphalt pavement as per the attached typical sections and details. A maximum lift of 3.5" is to be used for the backfilling. Any granular base material deemed unsuitable by the Engineer shall be removed as directed. Payment for the excavation of base material will be made using the 203-G Excess Excavation pay item. A list of the failed areas is shown in the attached table. Pavement repairs shall be completed as a continuous operation in order to minimize the traffic impact. Lane closures shall be in place until the failed area has been completely repaired. Should overnight closures be necessary, they shall be maintained by the contractor's personnel and in accordance with a lighting plan approved by the Engineer.

The concrete pavement joints in the travel lanes not repaired as failed areas are to be cleaned and filled. Joints in the underlying concrete pavement are to be cleaned to depth of 3" in the concrete pavement and filled with 3" of compacted asphalt pavement, 12.5mm MT. Cracked and broken pieces of existing HMA to be removed within 1' of each side of joint during joint cleaning, and replaced with 12.5mm compacted asphalt pavement after joint is filled or as directed. Locations are to be determined by the Engineer.

Following the overlaying operation the transverse joints in the pavement shall be sawed and sealed within 7 days. The details for sawing and sealing transverse joints for this section are as shown in the attached drawings. The width of the sawing and sealing operation will be 11' on each side of centerline, unless otherwise directed, to prevent "sympathy cracking."

**General Notes:** \* These general notes are applicable to all sites.

Guardrails are to be replaced at the bridge approaches near station 100+37 and 143+45. The quantities are listed in the attached table. All guardrail removed is to be replaced the same day and prior to reopening the adjacent lane of traffic. Voids created by the removal of posts, concrete anchors, footings, etc. shall be backfilled and tamped in accordance with Section 203 of the Standard Specifications. Object markers at the bridge approaches are also to be replaced. The guardrail pads are to be excavated and paved with 3" of 12.5mm asphalt pavement as per the attached drawings. The excavated material shall be retained and used to raise the existing shoulder to match the new pavement elevation. The cost of blading will be an absorbed item and is not to be included in the price of pay items bid. Material which cannot be placed and blended in adjacent areas and deemed to be excess excavation by the Engineer shall be removed under Pay Item No. 203-G Excess Excavation. All existing guardrail sections are to remain property of the MDOT and stockpiled at the Newton Maintenance Office.

Potholes that may exist are to be patched in a timely manner and prior to beginning the asphalt overlay. Patching of potholes shall be considered an absorbed item.

Traffic may be allowed to run on the milled surface up to 3 calendar days. Temporary striping shall be placed as required prior to opening the roadway to traffic. Overnight lane closures will not be allowed for this operation. Milling shall be performed in accordance with the attached drawings at all city streets, mainline tie-ins, crossovers, etc. Transitions at BOP, EOP, and local roads may be milled and left open if properly maintained. Temporary pavement joints (paper joints) shall be at least 3 paper-widths long shall be used at all milled tie-ins and shall be adequately maintained.

The Reclaimed Asphalt Pavement (RAP) material removed by the milling operation shall become the property of the contractor with the exception of 10,000 tons or 50% of the total anticipated RAP tonnage, whichever is less, to be stockpiled at the MDOT maintenance office yard at the Newton District Office. Unless the Contractor desires otherwise, the Contractor's milled material will be obtained first. The Contractor will be required to coordinate efforts with the maintenance office to effectively stockpile the milled material as directed by the Engineer. All costs associated with the hauling, placing, and stockpiling the state retained material shall be included in the price bid for cold milling

Publicly maintained roads and streets should be paved to the existing right-of-way. Privately owned entrances shall be paved to the shoulder line per the included typical drawing. Pads shall be shaped horizontally and vertically to prevent excessive drop-offs. Granular material (Class 5, group 'C') shall be provided around the pads to prevent shoulder drop-offs as directed and shall be placed in a timely manner. Drop-offs exceeding 2.5" shall be corrected within 2 calendar days of the placement of the pad. Stabilizer aggregate shall be used as directed by the Engineer.

It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the engineer, any structures damaged during the life of the contract. No payment will be made for replacement or repair of damaged items.

Any signs, mailboxes, etc. that are in conflict with construction of this project shall be removed and relocated by the Contractor as directed by the Engineer, the cost of which is to be absorbed in other items bid.

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost is to be included in the price bid for Pay Item No. 618-A, Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background. Standard roadside construction signs and barricades will be paid for using the appropriate pay items.

Roadside construction signs, barrels, etc. shall be placed in accordance with the attached drawings or as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed.

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes

first. No direct payment will be made for the debris removal; the cost is to be included in the prices of items bid. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor.

Milling operations shall be in accordance with the contract documents and the MDOT Standard Specifications for Road and Bridge Construction. Variable and length transitions will be used for local roads.

Where applicable the existing shoulders are to be raised to match the new pavement elevation by placing variable depth Granular Material (Class 5, Group C) on the existing shoulders. Placement of the granular material on the finished asphalt course shall not be permitted. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%). Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%). The cost of blading will be an absorbed item and is not to be included in the price of pay items bid.

Removal of the existing shoulder material shall be coincident with the milling/overlaying operation to prevent the possible ponding of water. No payment will be made for blading or removal of the existing shoulder material. Any material excavated from the existing shoulder shall be used to raise the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, foreslopes, or other adjacent areas as directed by the Engineer and will be an absorbed item. Material which cannot be placed in adjacent areas and deemed to be excess excavation by the Engineer shall be removed under Pay Item No. 203-G Excess Excavation.

Existing raised pavement markers are to be removed prior to beginning the overlay operation. No measurement will be made for separate payment; the cost is to be included in the prices for other items bid. Permanent pavement markers are to be placed in accordance with the attached drawings and Standard Drawings. Two-way yellow markers are to be placed on two-way roads and county roads. Red-clear markers are to be placed on divided roadways. Two-way clear markers are to be placed on county roads.

Temporary traffic stripe will be required immediately after the required milling & overlay and prior to opening area to traffic. Temporary stripe is to be placed in the same location and configuration as the permanent stripe.

All permanent striping will be thermoplastic. Edge lines shall be placed to accommodate the lane widths shown on the applicable typical sections unless prevented by field conditions. Rumble stripe will be placed throughout the project limits in accordance with the attached detail. Rumble bars will be replaced at intersections as per the attached drawing.

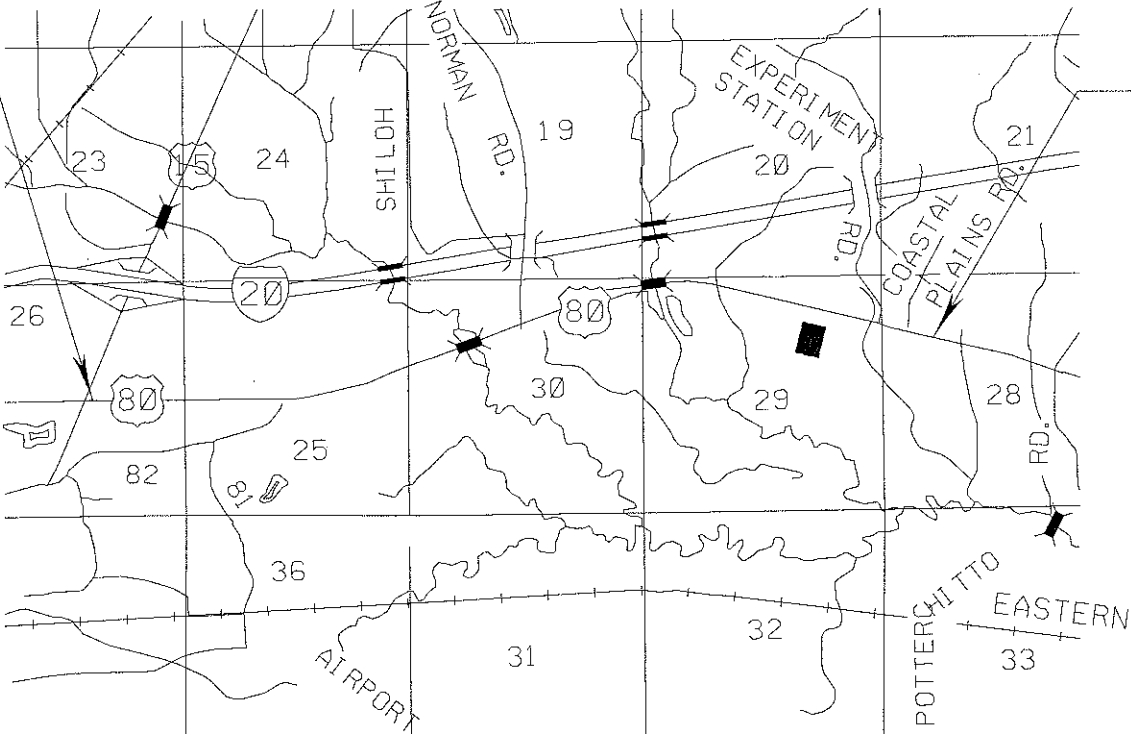
Asphalt curbs are to be placed at the locations shown in the attached table. The curbs shall be placed a minimum of 5 feet from the edge of the travel way at county roads or as directed by the Engineer.

MP-5080-51(022) 304022/301000

NEWTON COUNTY

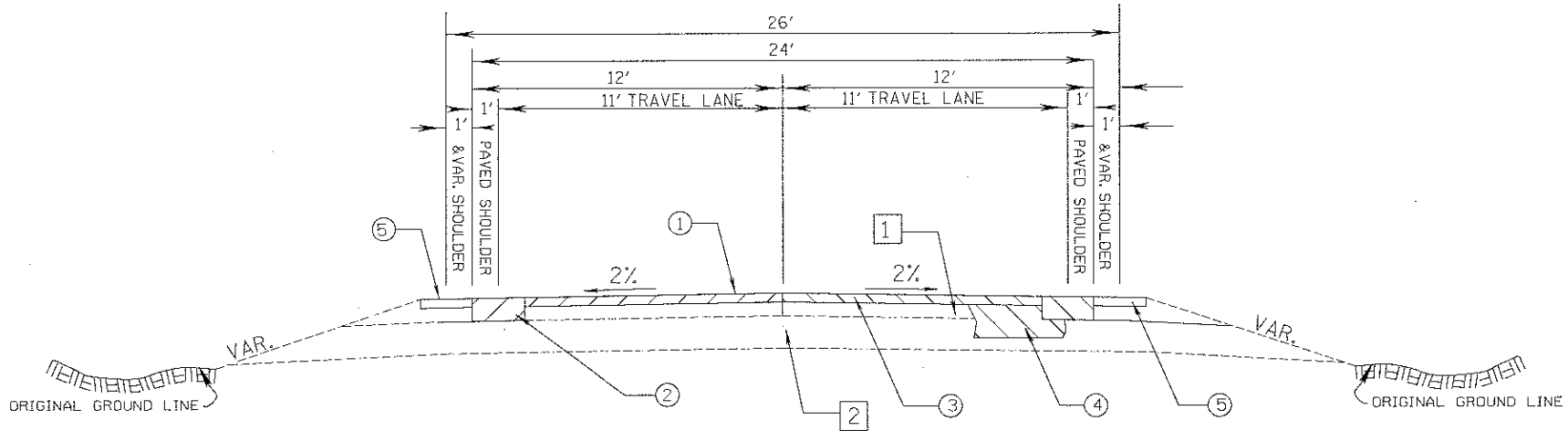
BOP  
STA: 21+88

EOP  
STA: 214+88



MP-5080-51(022) 304022/301000  
 NEWTON COUNTY

TYPICAL SECTION - WIDENING & OVERLAY



BOP (Sta 21+88) to 197+74  
 206+60 to EOP (Sta 214+88)

EXISTING MATERIALS

- ① 3" AND VARIABLE ASPHALT OVER 6" OF JRCP PAVEMENT
- ② 10" GRANULAR MATERIAL

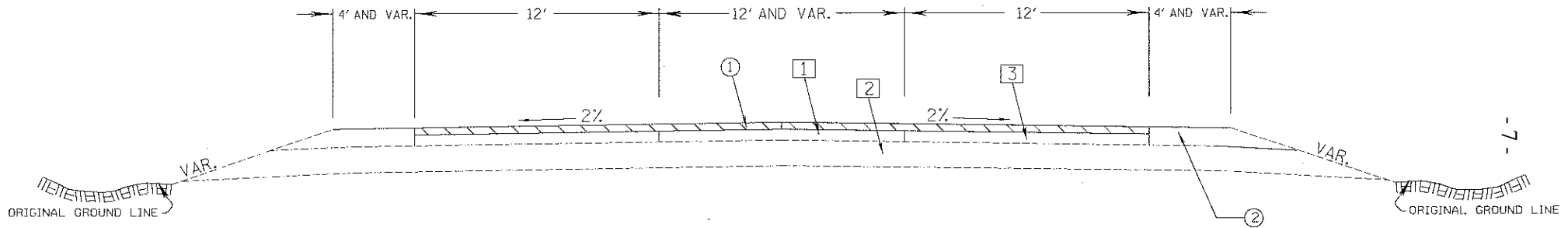
PROPOSED

- ① OVERLAY WITH 2" ASPHALT PAVEMENT, 12.5mm MIX, MT. MODIFY EXISTING CROSS-SLOPE BY PLACING 2" AT CENTERLINE AND 1-1/2" AT EDGE OF PAVEMENT, TO ACHIEVE 2% SLOPE
- ② PRIOR TO OVERLAY, WIDEN EXISTING 10' LANES TO 12' BY TRENCH WIDENING. EXCAVATE 8 1/2" DEEP X 2' WIDE AND PLACE 7"(2 @ 3 1/2") ASPHALT PAVEMENT, 19.0mm MIX, MT.
- ③ MILL EXISTING ASPHALT PAVEMENT 1-1/2".
- ④ REPAIR FAILED AREAS FULL DEPTH. REPAIR WITH 19.0mm ASPHALT PAVEMENT, MT.
- ⑤ MATERIAL EXCAVATED DURING TRENCH WIDENING TO BE USED TO BRING SHOULDERS TO GRADE. PROVIDE ADDITIONAL QUANTITY (IF REQUIRED) OF CLASS 5, GROUP "C" GRANULAR MATERIAL TO BRING SHOULDERS TO GRADE AFTER OVERLAY



MP-5080-51(022) 304022/301000  
 NEWTON COUNTY

TYPICAL SECTION - OVERLAY



Station 197+74 to 206+60

EXISTING MATERIALS

- ① 3" AND VARIABLE ASPHALT OVER 6" OF JRCP PAVEMENT
- ② 10" GRANULAR MATERIAL
- ③ 9" AND VARIABLE ASPHALT

PROPOSED

- ① OVERLAY WITH 2" ASPHALT PAVEMENT, 12.5mm MIX, MT.
- ② 2" & VARIABLE DEPTH CLASS 5, GROUP "C" GRANULAR MATERIAL TO BRING SHOULDERS TO GRADE AFTER OVERLAY

619-D Standard Roadside Construction Signs (Less than 10 Sq. Ft.)					
Station	Location	Description	Quantity	Unit	Remarks
	LT	G20-2A	8	SF	500' West of BOP
21+88	RT	R4-1	5	SF	
21+88	LT	W14-3	5.56	SF	
29+72	RT	R4-2	5	SF	
44+86	RT	R4-2	5	SF	
44+86	LT	W14-3	5.56	SF	
51+86	RT	R4-1	5	SF	
58+82	RT	R4-2	5	SF	
142+50	RT	R4-1	5	SF	
142+50	LT	W14-3	5.56	SF	
147+50	RT	R4-1	5	SF	
153+00	RT	R4-2	5	SF	
174+06	RT	R4-1	5	SF	
174+06	LT	W14-3	5.56	SF	
181+58	RT	R4-1	5	SF	
189+06	RT	R4-1	5	SF	
196+56	RT	R4-1	5	SF	
204+06	RT	R4-1	5	SF	
211+56	RT	R4-1	5	SF	
214+88	LT	R4-1	5	SF	
214+88	RT	W14-3	5.56	SF	
206+24	LT	R4-2	5	SF	
195+60	LT	R4-1	5	SF	
195+60	RT	W14-3	5.56	SF	
185+73	LT	R4-2	5	SF	
163+70	LT	R4-1	5	SF	
163+70	RT	W14-3	5.56	SF	
168+70	LT	R4-1	5	SF	
153+00	LT	R4-2	5	SF	
69+32	LT	R4-1	5	SF	
69+32	RT	W14-3	5.56	SF	
61+82	LT	R4-1	5	SF	
54+88	LT	R4-2	5	SF	
39+18	LT	R4-1	5	SF	
39+18	RT	W14-3	5.56	SF	
31+68	LT	R4-1	5	SF	
24+18	LT	R4-1	5	SF	
	RT	G20-2A	8	SF	500' East of EOP
		<b>Total</b>	<b>201.04</b>	<b>SF</b>	

619-D Standard Roadside Construction Signs (10 Sq. Ft. or More)					
Station	Location	Description	Quantity	Unit	Remarks
	RT	G20-1	10	SF	500' West of BOP
35+67	RT	W20-1	16	SF	Thomas Cutoff.
57+94	RT	W20-1	16	SF	Old Hickory Rd.
113+51	RT	W20-1	16	SF	Norman Rd.
197+47	RT	W20-1	16	SF	Experimental Station Rd.
204+53	RT	W20-1	16	SF	Coastal Plains Rd.
	LT	G20-1	10	SF	500' East of EOP
					G20-1
		<b>Total</b>	<b>100</b>	<b>SF</b>	

619-G Type III Barricades Double-Faced					
Location		Station	Quantity	Unit	Description
Rt		500' West of BOP	6	LF	
Lt		500' West of BOP	6	LF	
Rt		500' East of EOP	6	LF	
Lt		500' East of EOP	6	LF	
		<b>Total</b>	<b>24</b>	<b>LF</b>	

<u>Pay Item Number</u>	<u>Description</u>	<u>Total</u>	<u>Unit</u>
626-C	6" Thermoplastic Edge Stripe, Continuous White	6.706	Mile
626-D	6" Thermoplastic Traffic Stripe, Skip Yellow	3.082	Mile
626-E	6" Thermoplastic Traffic Stripe, Continuous Yellow	2.539	Mile
626-F	6" Thermoplastic Edge Stripe, Continuous Yellow	0.000	Mile
626-G	Thermoplastic Detail Stripe, White	1633.000	LF
626-G	Thermoplastic Detail Stripe, Yellow	4497.140	LF
626-H	Thermoplastic Legend, White	38.400	SF
626-H	Thermoplastic Legend, White	372.000	LF
627-K	Red-Clear Reflective High Performance Raised Markers	10.000	Each
627-J	Two-Way Clear Reflective High Performance Raised Markers	131.000	Each
627-L	Two-Way Yellow Reflective High Performance Raised Markers	462.000	Each

<u>Pay Item Number</u>	<u>Description</u>	<u>Total</u>	<u>Unit</u>
619-A1	Temporary Traffic Stripe, Continuous White	13.412	Mile
619-A2	Temporary Traffic Stripe, Continuous Yellow	5.078	Mile
619-A4	Temporary Traffic Stripe, Skip Yellow	6.163	Mile
619-A5	Temporary Traffic Stripe, Detail	12260.280	LF
619-A6	Temporary Traffic Stripe, Legend White	744.000	LF
619-A6	Temporary Traffic Stripe, Legend White	76.800	SF

Station	to	Station	Location	6" Cont W Edge (MI)	6" Det W (LF)	6" Det Y (LF)	6" Cont Y (MI)	6" Skip Y (MI)	Legend (LF)	Legend (SF)
21+88	--	102+61		3.086	299	42	1.014	1.306	104	--
102+61	--	214+88		3.620	1334	4455.14	1.525	1.776	268	38.40
				--	--	--	--	--	--	--
				<b>6.706</b>	<b>1633</b>	<b>4497.14</b>	<b>2.539</b>	<b>3.082</b>	<b>372</b>	<b>38.40</b>

**Detail Yellow Gore**

18" Wide Inside length (ft)	6" Detail Yellow (LF)
2.28	6.84
3.61	10.83
4.95	14.85
6.28	18.84
7.62	22.86
8.95	26.85
10.30	30.90
11.62	34.86
12.96	38.88
14.30	42.90
15.63	46.89
16.24	48.72
13.43	40.29
10.46	31.38
7.50	22.50
4.53	13.59
1.50	4.50
16.78	50.34
15.63	46.89
14.30	42.90
12.96	38.88
11.62	34.86
10.30	30.90
8.95	26.85
7.62	22.86
6.28	18.84
4.95	14.85
3.61	10.83
2.28	6.84
0.94	2.82
Subtotal	<b>805.14</b>

6" Wide Border Length (ft)	6" Detail Double Yellow (LF)
1020	2040
740	1480
Subtotal	3520
<b>TOTAL</b>	<b>4325.14</b>

202-B Removal of Curb &/or Curb and Gutter, All Types		
Location	Station	Total (LF)
Thomas Cutoff	35+67	55
Norman Rd.	113+51	60

609-E Bituminous Curb		
Location	Station	Total (LF)
Thomas Cutoff	35+67	55
Norman Rd.	113+51	60

304-A023 Granular Material, LVM, Class 5, Group C									
Station	Location	Width-1 (FT)	Width-2 (FT)	Length (FT)	Taper (SF)	Total (SF)	Depth (FT)	volume (CY)	Remarks
100+00	LT	5	11	76	275	883.000	0.250	10	
100+00	RT	5	11	135	275	1355.000	0.250	16	
100+00	RT	4	4	21	0	84.000	3.000	12	Hole
102+61	LT	5	11	135	275	1355.000	0.250	16	
102+61	RT	5	11	76	275	883.000	0.250	10	
143+45	LT	5	11	51	275	683.000	0.250	8	
143+45	RT	5	11	137	275	1371.000	0.250	16	
145+72	LT	5	11	137	275	1371.000	0.250	16	
145+72	RT	5	11	77	275	891.000	0.250	10	
145+72	RT	2	2	21	0	42.000	2.000	4	Hole
		2	2	2300	0	4600.000	0.333	71	Miscellaneous locations throughout project
				<b>Total</b>				<b>188</b>	

304-A023 Granular Material, LVM, Class 5, Group C									
Station to Station	Location	Width (FT)	Length (FT)	Taper (SF)	Total (SF)	Depth (FT)	volume (CY)	Remarks	
197+74	206+60	LT	4	886	275	3544.000	0.250	41	
197+74	206+60	RT	4	886	275	3544.000	0.250	41	
				<b>Total</b>				<b>82</b>	

**TOTAL**

**270**

627-L001 TWO-WAY CLEAR REFLECTIVE HIGH PERFORMANCE RAISED MARKERS

Station	LOCATION	QUANTITY	INSPECTOR	REMARKS
35+67	Thomas Cutoff	23	R. Hammond	
57+94	Old Hickory Rd.	25	R. Hammond	
113+51	Norman Rd.	26	R. Hammond	
197+47	Experimental Station Rd.	26	R. Hammond	
204+53	Coastal Plains Rd.	31	R. Hammond	
		131		



203-G Excess Excavation, LVM						
Station	Location	Width (FT)	Length (FT)	Total (SF)	Total CY	Remarks
67+77	RT	15	10	150.000	2.778	
68+38	LT	15	10	150.000	2.778	
78+36	RT	10	10	100.000	1.852	
82+55	RT	18	10	180.000	3.333	
85+60	RT	10	10	100.000	1.852	
97+34	LT	18	10	180.000	3.333	
97+34	RT	15	10	150.000	2.778	
142+50	LT	18	10	180.000	3.333	
142+50	RT	10	10	100.000	1.852	
152+48	LT	10	10	100.000	1.852	
152+48	RT	10	10	100.000	1.852	
158+00	LT	15	10	150.000	2.778	
165+73	RT	15	10	150.000	2.778	
179+54	LT	11	10	110.000	2.037	
212+98	LT	10	10	100.000	1.852	
			<b>Total</b>		<b>37.037</b>	

GUARD RAIL REQUIRED												
WK. NO.	STATION	Location	GUARDRAIL	CABLE ANCHOR	TERMINAL END SECTION	BRIDGE END SECTION	BRIDGE END SECTION	SINGLE DELINEATORS		OBJECT MARKERS	Removal of Guardrail	REMARKS
			W-BEAM			TYPE E	SPECIAL SECTION	WHITE	YELLOW			
	100+37	NW Quad	12.5		1		1	4		1	75.0	OM-3L
	100+37	SW Quad	75.0		1		1	5		1	137.5	OM-3R
	100+37	NE Quad	75.0		1		1	5		1	137.5	OM-3R
	100+37	SE Quad	12.5		1		1	4		1	75.0	OM-3L
	143+45	NW Quad	0.0		1		1	3		1	50.0	* Length restriction of 50' / OM-3L
	143+45	SW Quad	75.0		1		1	5		1	137.5	OM-3R
	143+45	NE Quad	75.0		1		1	5		1	137.5	OM-3R
	143+45	SE Quad	12.5		1		1	4		1	75.0	OM-3L
		UNITS	L. F.	EACH	EACH		EACH	EACH	EACH		EACH	
		TOTALS	337.5	0	8		8	35	0		825	

202-B Removal of Concrete Pavement w/ Variable Depth Overlay						
Station	Location	Width (FT)	Length (FT)	Total (SF)	Total (SY)	Remarks
67+77	RT	15	10	150.000	5.556	
68+38	LT	15	10	150.000	5.556	
78+36	RT	10	10	100.000	3.704	
82+55	RT	18	10	180.000	6.667	
85+60	RT	10	10	100.000	3.704	
97+34	LT	18	10	180.000	6.667	
97+34	RT	15	10	150.000	5.556	
142+50	LT	18	10	180.000	6.667	
142+50	RT	10	10	100.000	3.704	
152+48	LT	10	10	100.000	3.704	
152+48	RT	10	10	100.000	3.704	
158+00	LT	15	10	150.000	5.556	
165+73	RT	15	10	150.000	5.556	
179+54	LT	11	10	110.000	4.074	
212+98	LT	10	10	100.000	3.704	
			<b>Total</b>		<b>74.074</b>	

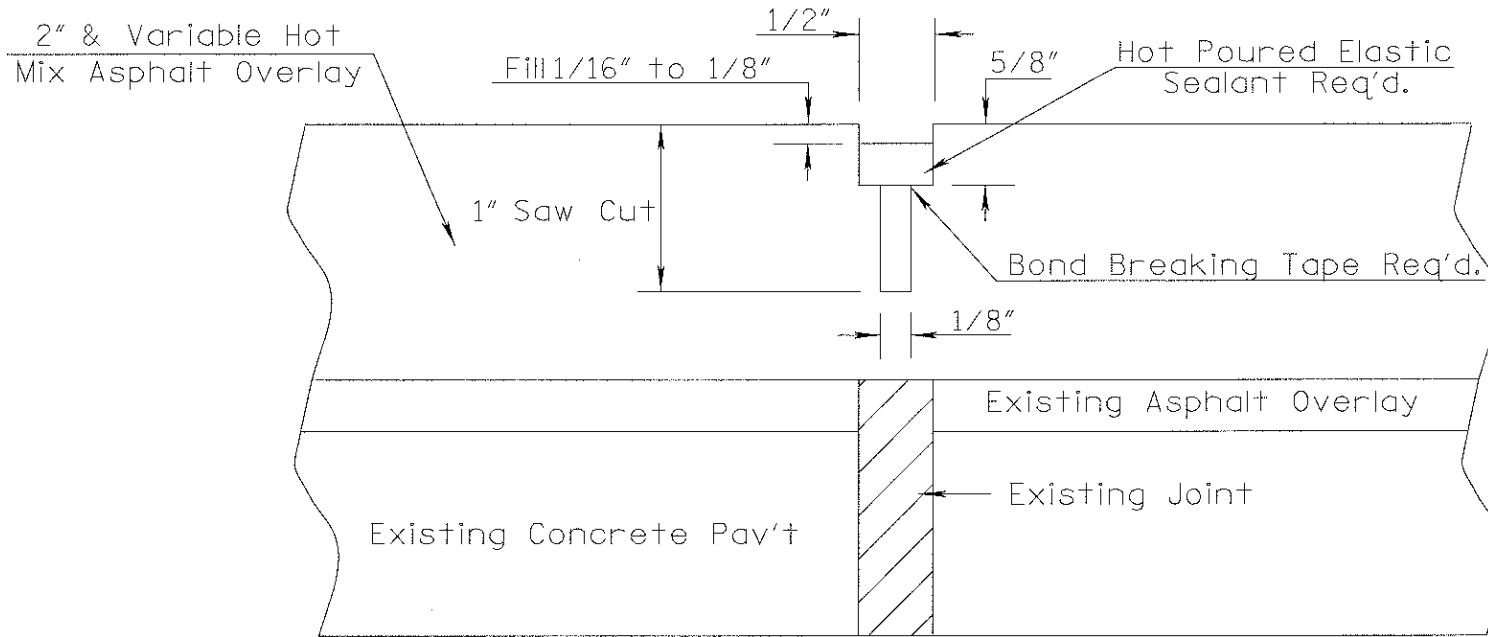
MP-5080-51(022) 304022/301000

NEWTON COUNTY

TRANSVERSE JOINT CLEANING & FILLING DETAIL

DETAIL OF SAWING AND SEALING TRANSVERSE/LONGITUDINAL JOINTS

(PAY ITEM NO. 413-E)



CONCRETE PAVEMENT NOT OVERLAYED WITH ASPHALT

DETAIL OF CLEANING AND SEALING TRANSVERSE JOINTS IN CONCRETE PAVEMENT (> 1/2" WIDE) AND SAWING AND SEALING TRANSVERSE JOINTS IN HMA OVERLAY

(PAYMENT MADE UNDER PAY ITEMS 413-E)

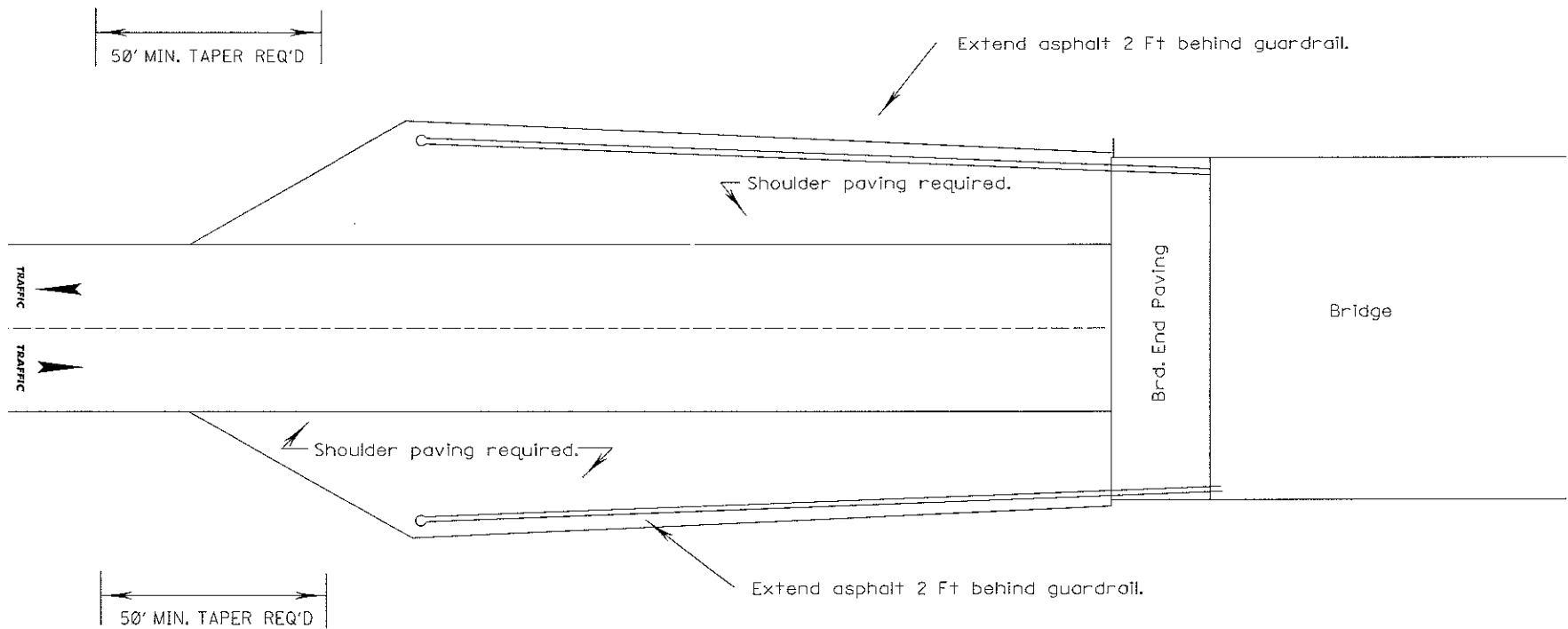
\*NOTE: EXISTING TRANSVERSE JOINTS IN THE CONCRETE PAVEMENT (1/2" OR GREATER IN WIDTH) ARE TO BE ROUTED OR OTHERWISE CLEANED TO A DEPTH OF 3" AND FILLED WITH WELL COMPACTED HMA PRIOR TO OVERLAY

NOTE: FOR JOINTS WHERE THE CONCRETE PAVEMENT IS NOT AND WILL NOT BE OVERLAID, AND IF THE JOINTS ARE 1/2" TO 1 1/2" WIDE, USE BACKER ROD AND SEALANT. (LOCATIONS TO BE DETERMINED BY THE ENGINEER AND PAYMENT MADE UNDER PAY ITEM 508-A.)

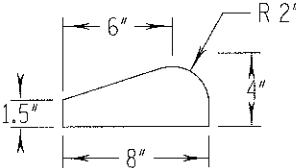
NOTE: HOT POUR (M 324) IS TO BE USED IN LOCATIONS DIRECTED BY THE ENGINEER WHERE CONCRETE PAVEMENT WILL NOT BE OVERLAID AND JOINT WIDTH IS > 1 1/2". (CLEAN JOINT TO 3" DEPTH AND LEAVE SEALANT DOWN ENOUGH TO PREVENT TIRE PICK UP: 3/8" TO 1/2").

MP-5080-51(022) 304022/301000  
NEWTON COUNTY

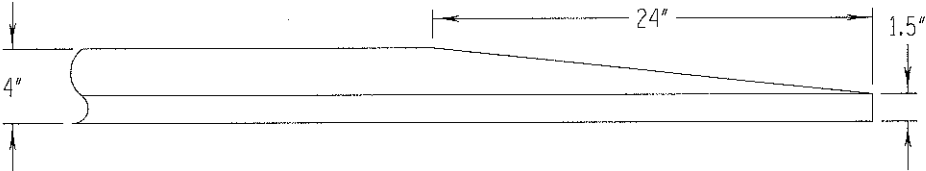
TYPICAL SECTION OF ADDITIONAL SHOULDER PAVING  
REQUIRED AT GUARDRAIL LOCATIONS



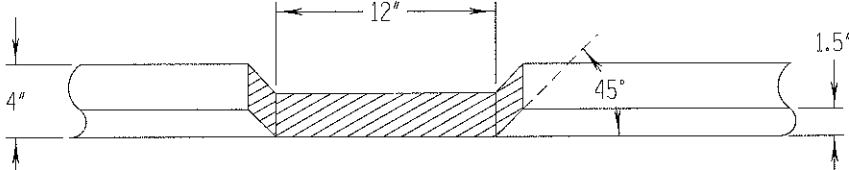
MP-5080-51(022) 304022/301000  
NEWTON COUNTY  
DETAIL OF BITUMINOUS CURB



Detail of Curb Terminus

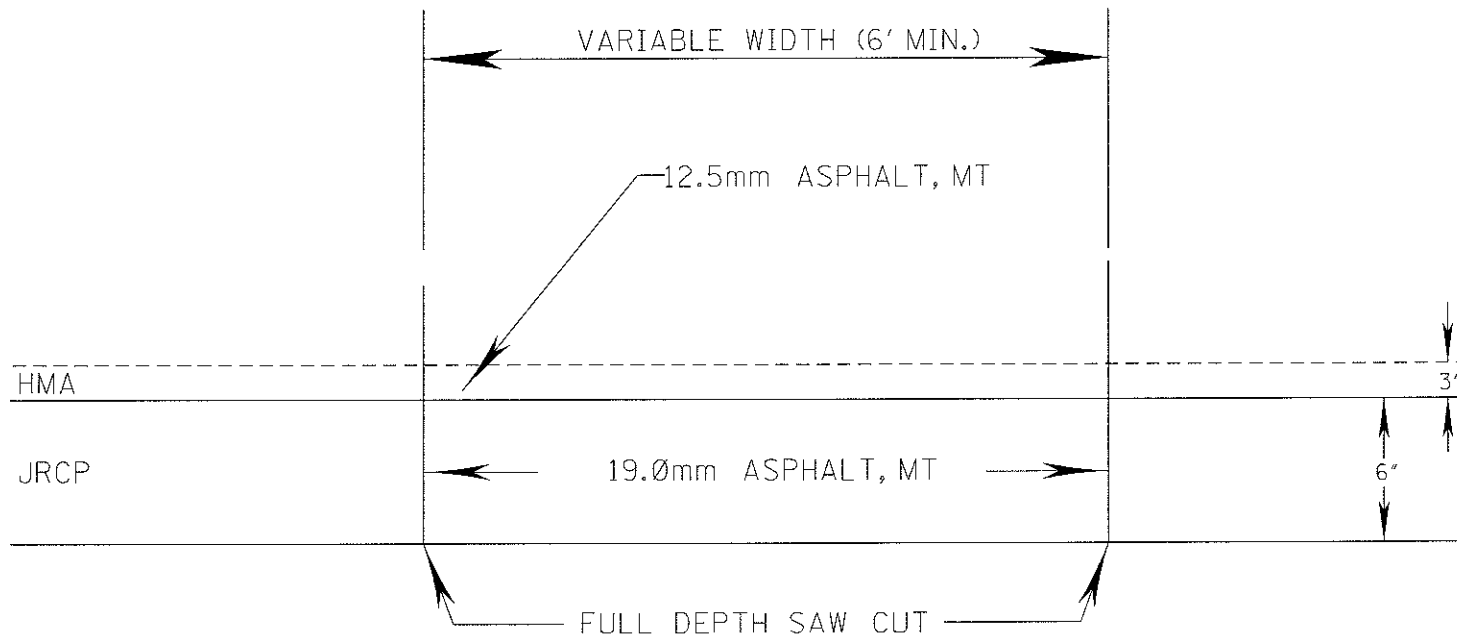


Detail of Spill Thru



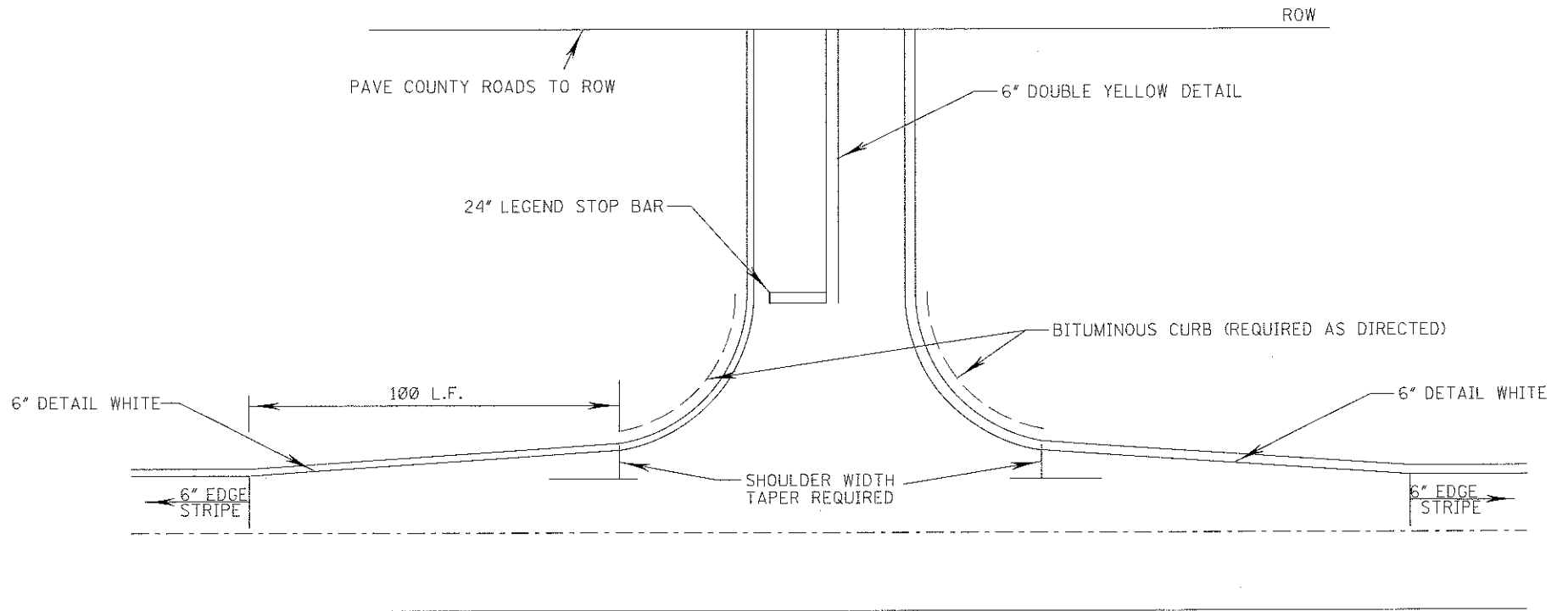
MP-5080-51(022) 304022/301000  
NEWTON COUNTY

CONCRETE PAVEMENT REPAIR DETAIL



MP-5080-51(022) 304022/301000  
NEWTON COUNTY

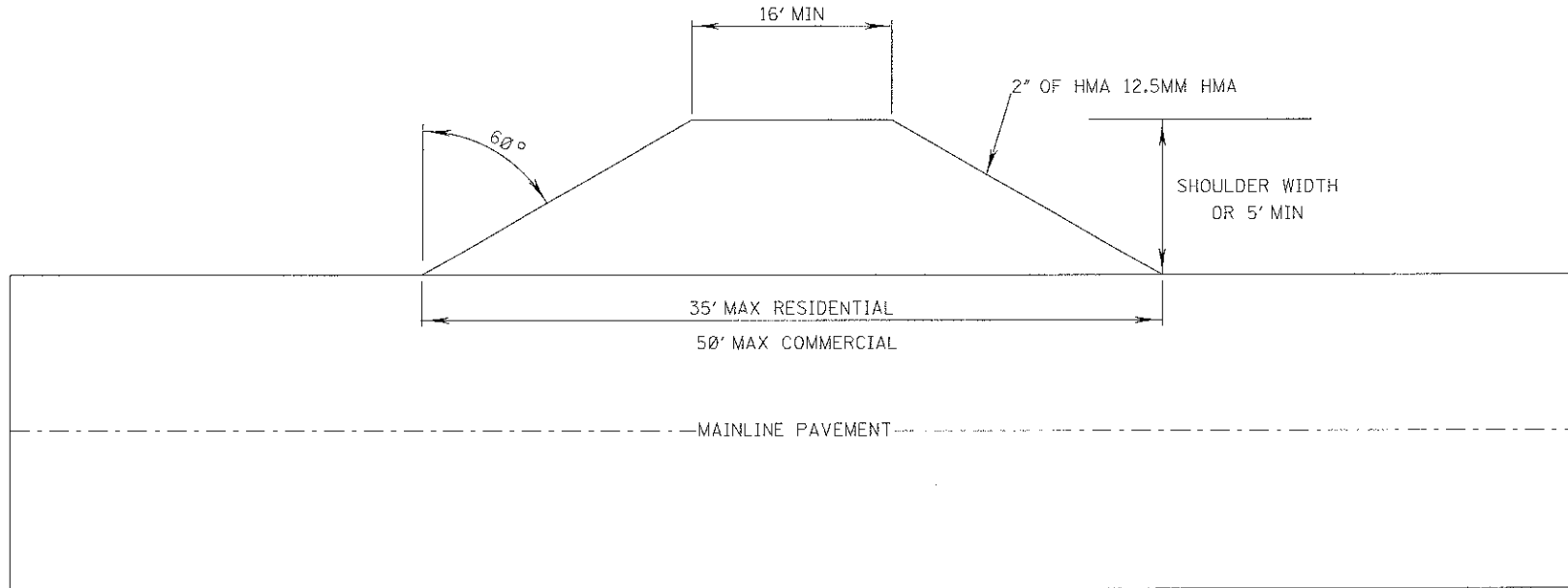
COUNTY ROAD PAVING/STRIPING DETAIL



\*SEE ATTACHED SHEET FOR TWO-WAY CLEAR RPM DETAIL



MP-5080-51(022)304022/301000  
 NEWTON COUNTY  
 TYPICAL DRIVEWAY PAD DETAIL



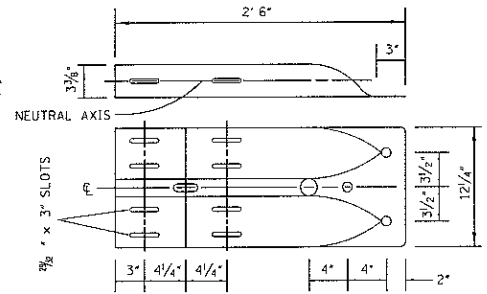
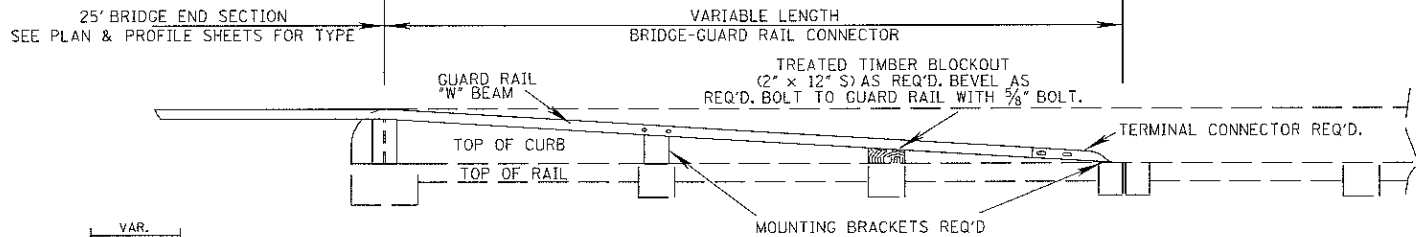
LOCATIONS:

- 29+50 LT LANE
- 33+69 RT LANE (SOUTHERN PINE SUBSTATION DRIVEWAY  
PAD DIMENSION IS 49' X 32' X 5')
- 39+28 RT LANE
- 164+76 LT LANE
- 166+89 LT LANE
- 186+81 LT LANE

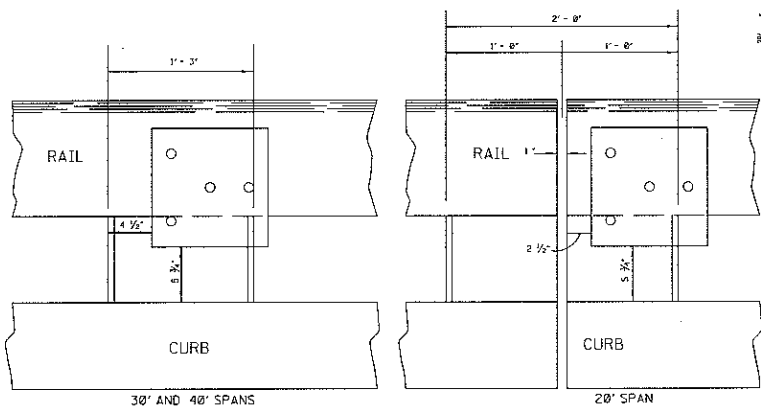
NOTE:

THE HMA ON THE EXISTING DRIVEWAY/RAMP PADS ARE TO REMAIN IN THEIR CURRENT, SIZE, AND LOCATION. IF, IN THE OPINION OF THE ENGINEER, A PAD SHOULD BE MODIFIED OR REPLACED, PAYMENT WILL BE MADE FOR THE WORK USING THE APPROPRIATE PAY ITEMS. GRANULAR MATERIAL AND/OR STABILIZER AGGREGATE SHOULD BE PLACED AROUND THE PADS AS REQUIRED.

STATE	PROJECT NO.
MISS.	



BRIDGE-GUARD RAIL CONNECTOR DETAIL



DETAILS SHOWING PLACEMENT OF HOLES FOR CONNECTOR (VIEWED FROM ROADWAY)

TERMINAL CONNECTOR (AASHTO DESIGNATION : M180, CLASS B, TYPE 2)

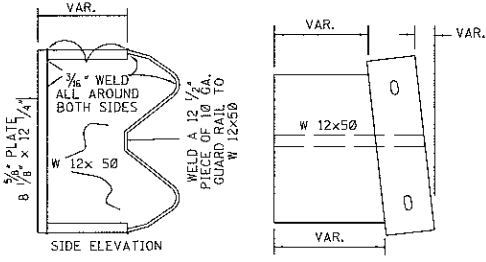
NOTES:

BRIDGE-GUARD RAIL CONNECTOR TO CONSIST OF ONE(1) 25' SECTION OF GUARD RAIL (FIELD CUT, DRILLED, AND GALVANIZED), FABRICATED BRACKETS AND PLATES, TERMINAL CONNECTOR, MOUNTING HARDWARE, AND TIMBER BLOCKOUT (COMPLETE IN PLACE).

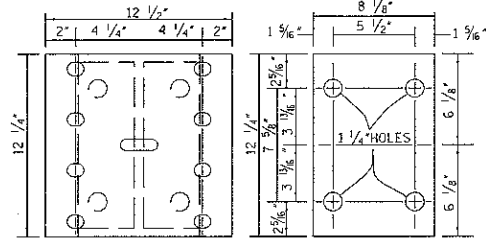
2 BRIDGE-GUARD RAIL CONNECTORS REQ'D ON BRIDGES 24' WIDE, 4 REQ'D ON BRIDGES 26' OR WIDER.

PRIOR TO FABRICATION, ALL DIMENSIONS FOR MOUNTING BRACKETS, CONNECTORS, BOLT LENGTHS AND GUARD RAIL SHALL BE DETERMINED IN THE FIELD BY THE CONTRACTOR. DIMENSION SHOWN MAY VARY FROM ACTUAL FIELD CONDITIONS AT EACH SITE.

PAY ITEM NO. 907-606-D001 SPECIAL SECTION BRIDGE-GUARD RAIL CONNECTOR PER EACH.

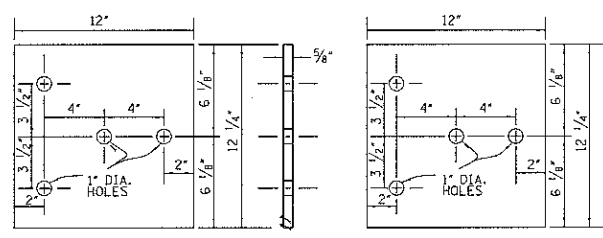


TOP VIEW



FRONT ELEVATION SHOWING 10 GA. GUARD RAIL CONNECTION TO W12x50

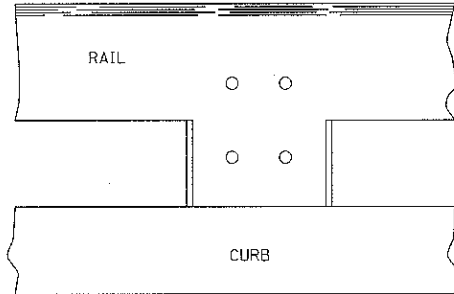
DETAIL OF INTERMEDIATE BRACKET



DETAIL OF FRONT PLATE

DETAIL OF BACK PLATE

DETAIL OF TERMINAL CONNECTOR PLATE



DETAILS SHOWING PLACEMENT OF HOLES FOR INTERMEDIATE BRACKET (VIEWED FROM ROADWAY)

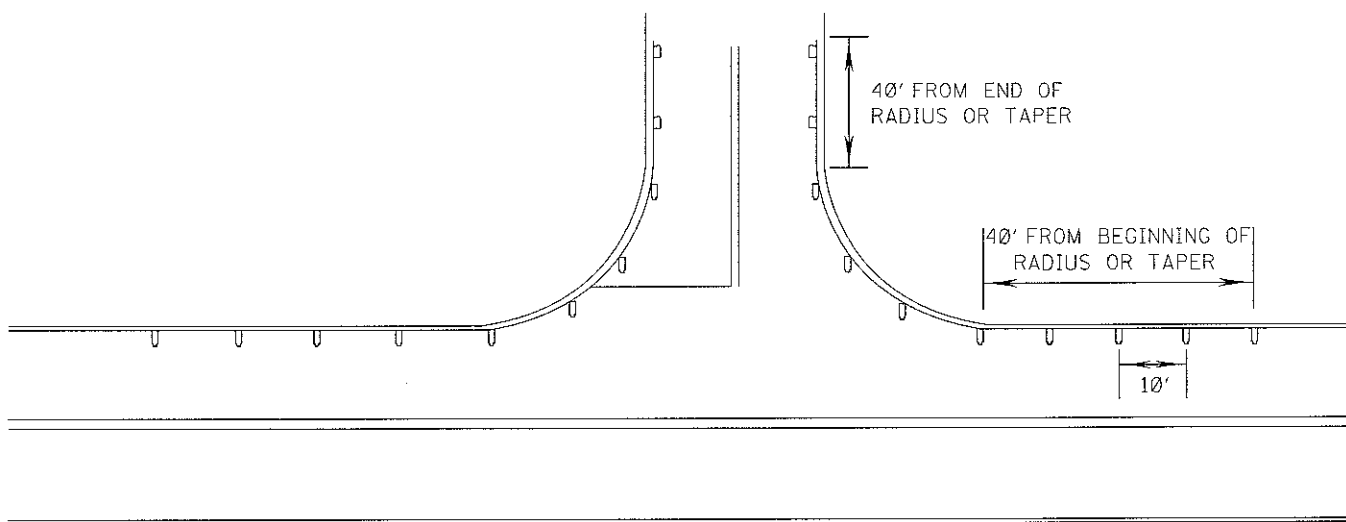
Notice To Bidders No. 3332

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
SPECIAL DESIGN	
BRIDGE-GUARD RAIL CONNECTOR	
WORKING NUMBER	BCRO-1
SHEET NUMBER	
FILENAME: guard/barcl.dwg	
DESIGN TEAM	CHECKED DATE

MP-5080-51(022) 304022/301000

NEWTON COUNTY

TYPICAL FOR RAISED PAVEMENT MARKERS  
PLACED ON SIDE ROAD RADIUS



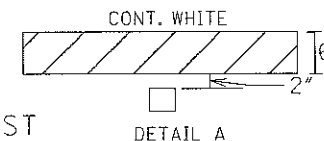
NOTE 1. MARKERS SHALL BE PLACED EVERY 10 FEET.

NOTE 2. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST  
ON STATE DESIGNATED HIGHWAYS.

NOTE 3. MARKERS SHALL BE HIGH PERFORMANCE TWO WAY CLEAR.

NOTE 4. FIVE (5) MARKERS SHALL BE PLACED ALONG MAINLINE EDGE STRIPE.

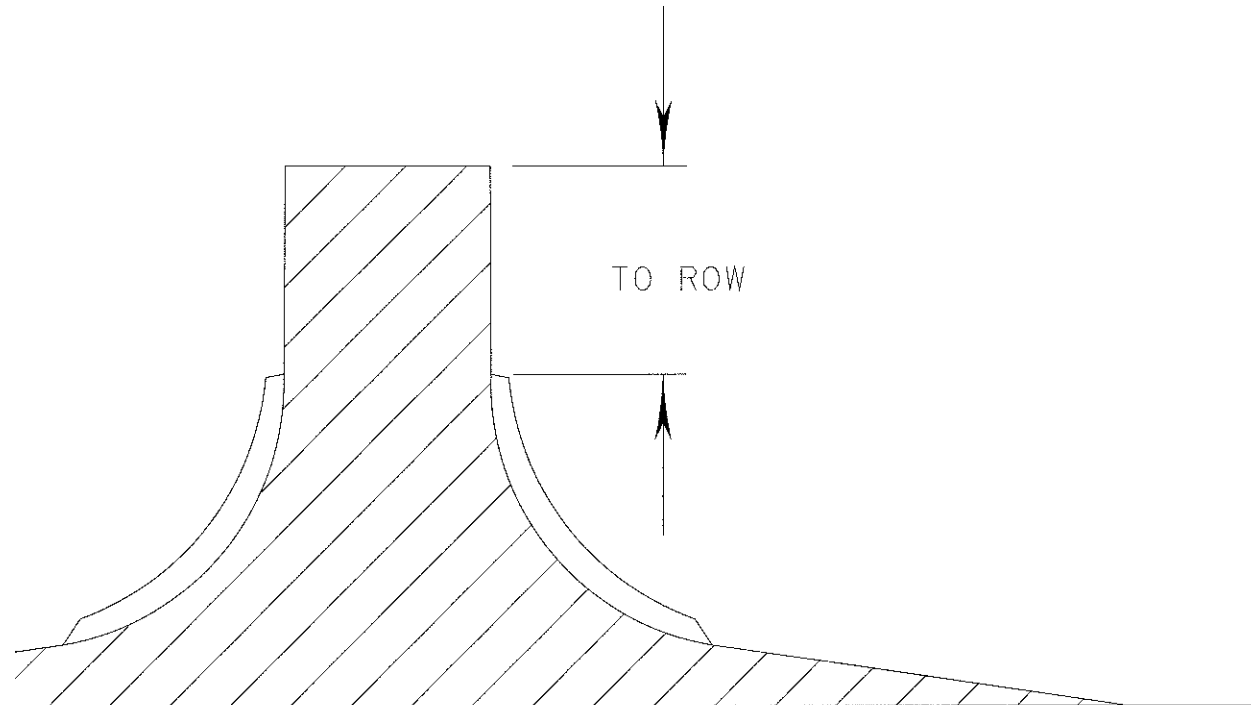
NOTE 5. MARKERS FOR COUNTY ROADS SHALL CONTINUE DOWN THE EDGE  
STRIPE A DISTANCE OF 40 FEET.



MP-5080-51(022) 304022/301000

NEWTON COUNTY

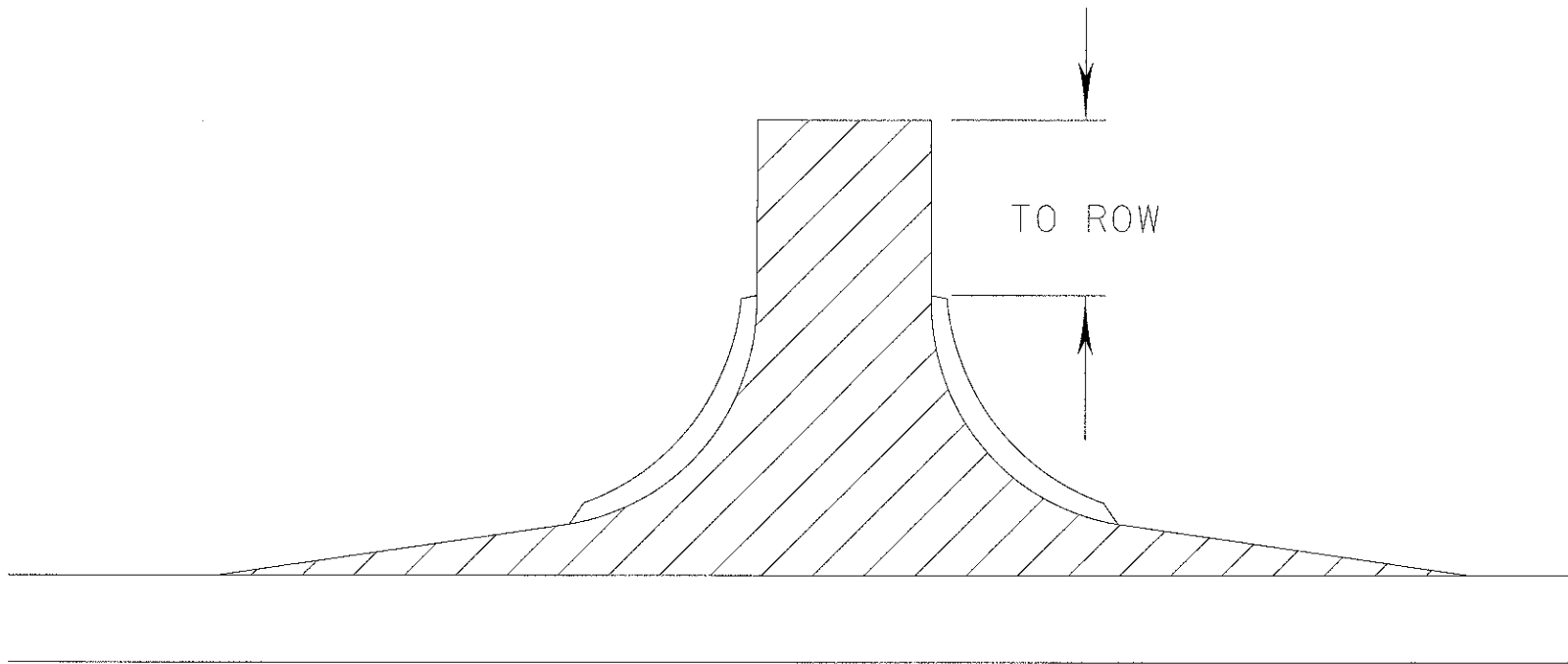
MILLING OF COUNTY ROADS  
WITH CURB AND GUTTER



- MILL FULL DEPTH 1.5"
- PLACE 1.5" HMA TO TIE TO MAINLINE OVERLAY.

MP-5080-51(022) 304022/301000  
NEWTON COUNTY

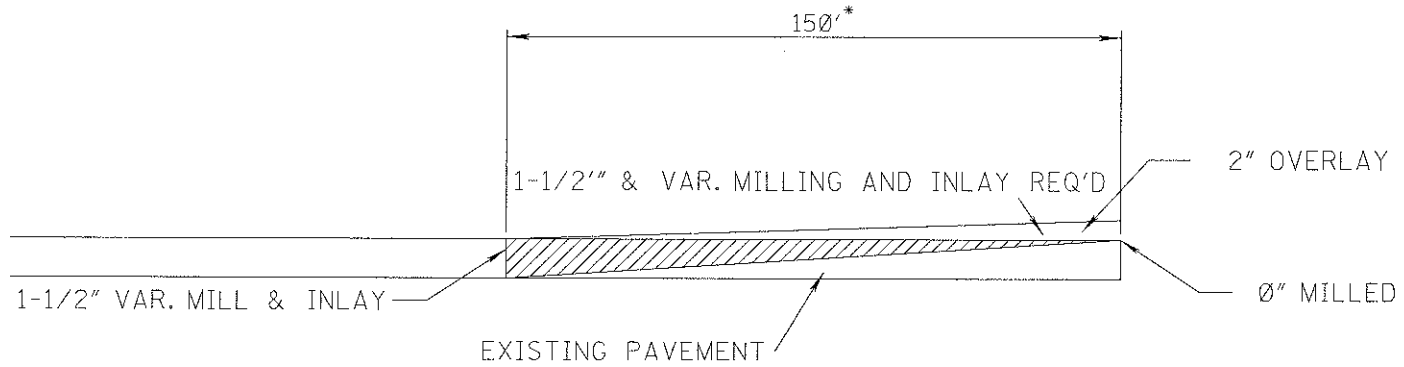
MILLING OF  
EXPERIMENT STATION ROAD  
AND  
COASTAL PLAINS ROAD  
WITH CURB AND GUTTER



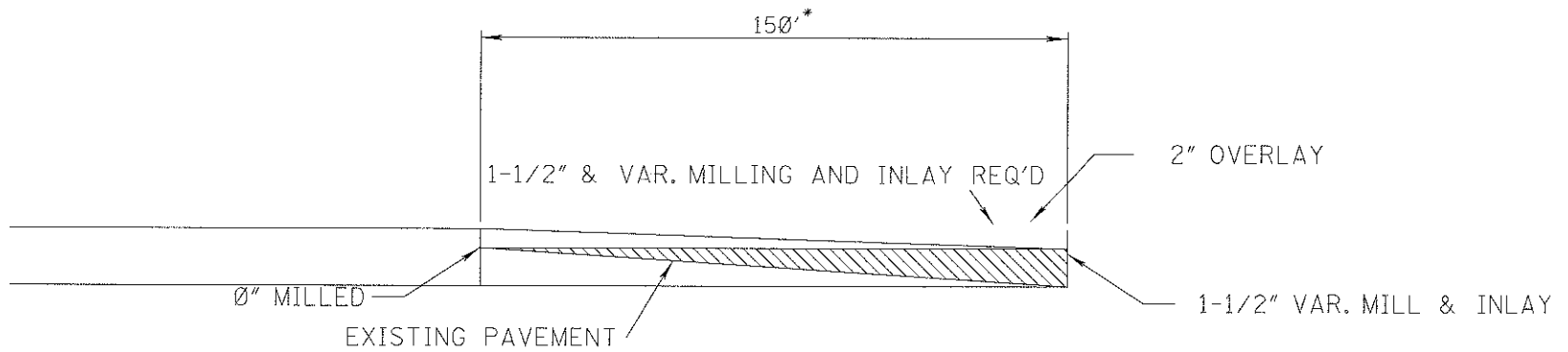
- Mill 0" at EP of mainline  
and 1.5" at curb and gutter.
- Place 1.5" HMA to tie to mainline  
overlay.

MP-5080-51(022) 304022/301000  
NEWTON COUNTY

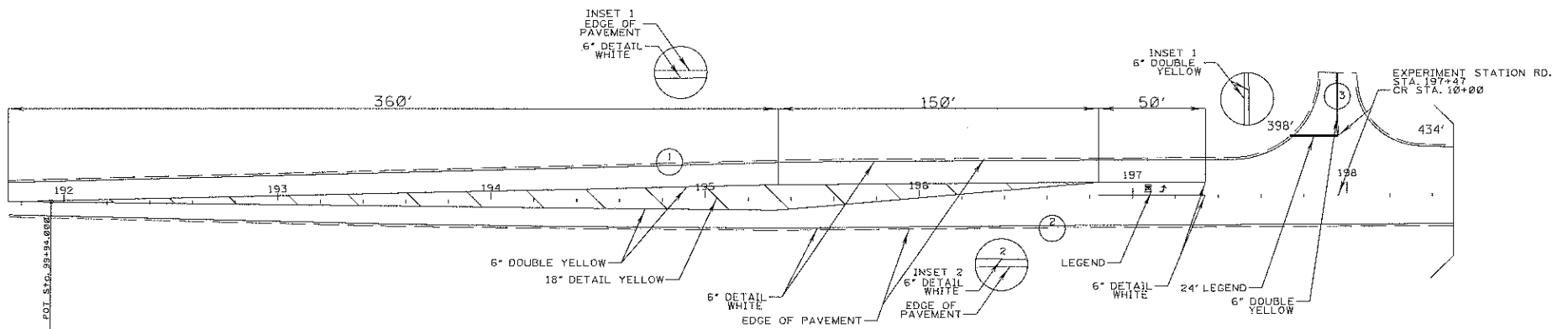
MILLING TRANSITION DETAIL



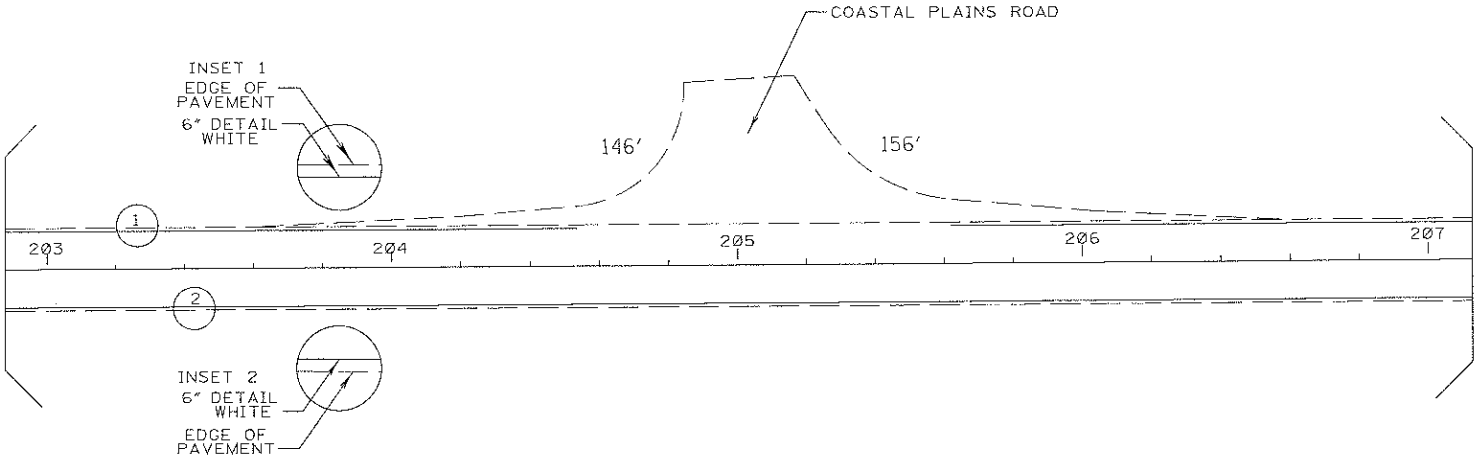
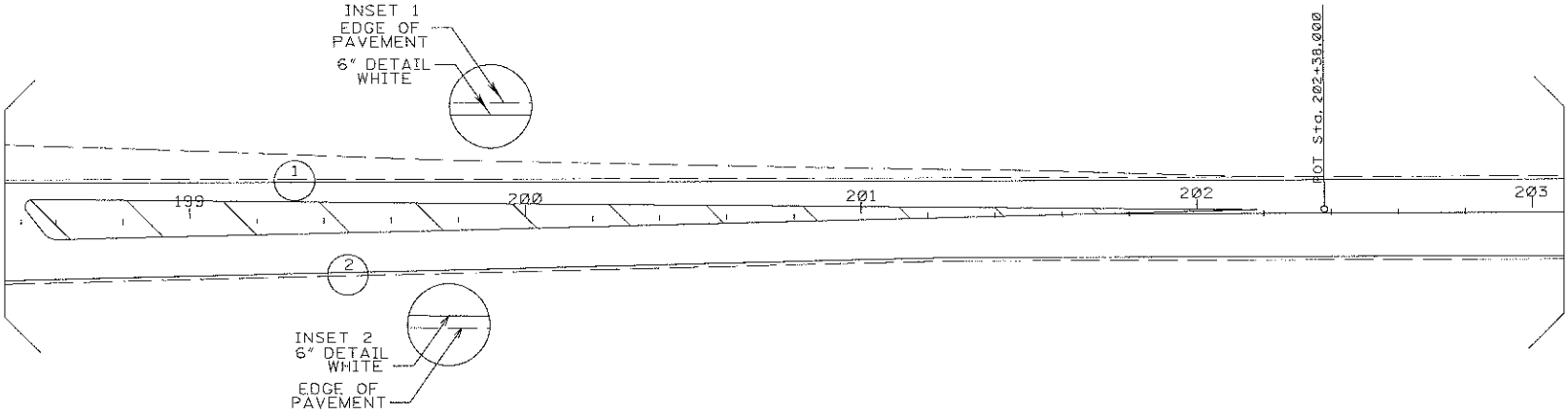
- \* TRANSITION LENGTH MAY BE ADJUSTED FOR LOCAL ROADS, RAMPS, ETC.
- \* MILL FROM BRIDGE END AT BOP THEN START TRANSITION AT END OF GUARDRAIL



# STRIPING DETAIL #1 AT EXPERIMENT STATION



# STRIPING DETAIL #2 AT EXPERIMENT STATION ROAD





**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 3367**

**CODE: (SP)**

**DATE: 01/14/2011**

**SUBJECT: Shoulder Wedge**

**PROJECT: MP-5080-51(022) / 304022301 -- Newton**

Bidders are hereby advised that the Shoulder Wedge specified in the Supplement to Special Provision 907-401-2 shall only apply to the top two (2) lifts of asphalt.

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

## SUPPLEMENT TO SPECIAL PROVISION NO. 907-401-2

**DATE:** 03/2/2010

**SUBJECT:** Hot Mix Asphalt (HMA)

Add the following before 907-401.02.6.2 on page 1.

**907-401.02.4--Substitution of Mixture.** Delete the table in Subsection 401.02.4 on page 242, and substitute the following:

Mixture	Single Lift Laying Thickness Inches	
	Minimum	Maximum
25 mm	3	4
19 mm	2 ¼	3 ½
12.5 mm	1 ½	2 ½
9.5 mm	1	1 ½
4.75 mm	½	¾

After Subsection 907-401-02.6.2 on page 2, add the following:

**907-401.02.6.4.1--Roadway Density.** Delete subparagraphs 1., 2., & 3. on page 251 and substitute the following:

1. For all leveling lifts, when full lane width and with a thickness as specified in the table in Subsection 401.02.4, the required lot density shall be 92.0 percent of maximum density.
2. For all single lift overlays, with or without leveling and/or milling, the required lot density shall be 92.0 percent of maximum density.
3. For all multiple lift overlays of two (2) or more lifts excluding leveling lifts, the required lot density of the bottom lift shall be 92.0 percent of maximum density. The required lot density for all subsequent lifts shall be 93.0 percent of maximum density.
4. For all pavements on new construction, the required lot density for all lifts shall be 93.0 percent of maximum density.

**907-401.03.1.2--Tack Coat.** Delete the three sentences of Subsection 401.03.1.2 on page 259, and substitute the following:

Tack coat shall be applied to previously placed HMA and between lifts, unless otherwise directed by the Engineer. Tack coat shall be applied with a distributor spray bar. A hand wand

will only be allowed for applying tack coat on ramp pads, irregular shoulder areas, median crossovers, turnouts, or other irregular areas. Bituminous materials and application rates for tack coat shall be as specified in Table 410-A on page 293. Construction requirements shall be in accordance with Subsection 407.03 of the Standard Specifications.

**907-401.03.1.4--Density.** Delete the first sentence of the first paragraph of Subsection 401.03.1.4 on page 259 and substitute the following:

The lot density for all dense graded pavement lifts, except as provided below for preleveling, wedging [less than fifty percent (50%) of width greater than minimum lift thickness], ramp pads, irregular shoulder areas, median crossovers, turnouts, or other areas where the established rolling pattern cannot be performed, shall not be less than the specified percent (92.0% or 93.0%) of the maximum density based on AASHTO Designation: T 209 for the day's production. For all leveling lifts, when full lane width and with a thickness as specified in the table in Subsection 401.02.4, the required lot density shall be 92.0 percent of maximum density.

**907-401.03.9--Material Transfer Equipment.** Delete the paragraph in Subsection 401.03.9 on page 264 and substitute the following:

Excluding the areas mentioned below, the material transferred from the hauling unit when placing the top lift, or the top two (2) lifts of a multi-lift HMA pavement with density requirements, shall be remixed prior to being placed in the paver hopper or insert by using an approved Materials Transfer Device. Information on approved devices can be obtained from the State Construction Engineer. Areas excluded from this requirement include: leveling courses, temporary work of short duration, detours, bridge replacement projects having less than 1,000 feet of pavement on each side of the structure, acceleration and deceleration lanes less than 1,000 feet in length, tapered sections, transition sections for width, shoulders less than 10 feet in width, crossovers, ramps, side street returns and other areas designated by the Engineer.

After Subsection 401.03.13 on page 266, add the following:

**907-401.03.14--Shoulder Wedge.** The Contractor shall attach a device to the screed of the paver that confines the material at the end gate and extrudes the asphalt material in such a way that results in a compacted wedge shape pavement edge of approximately 30 degrees, but not steeper than 35 degrees. The device shall maintain contact between itself and the road shoulder surface and allow for automatic transition to cross roads, driveways, and obstructions. The device shall be used to constrain the asphalt head reducing the area by 10% to 15% increasing the density of the extruded profile. Conventional single plate strike off shall not be used.

The device shall be TransTech Shoulder Wedge Maker, the Advant-Edge, or a similar approved equal device that produces the same wedge consolidation results. Contact information for these wedge shape compaction devices is the following:

1. TransTech Systems, Inc.  
1594 State Street  
Schenectady, NY 12304  
800-724-6306  
[www.transtechsys.com](http://www.transtechsys.com)

2. Advant-Edge Paving Equipment, LLC  
P.O. Box 9163  
Niskayuna, NY 12309-0163  
518-280-6090  
Contact; Gary D. Antonelli  
Cell: 518-368-5699  
email: [garya@nycap.rr.com](mailto:garya@nycap.rr.com)  
Website: [www.advantedgepaving.com](http://www.advantedgepaving.com)

Before using a similar device, the Contractor shall provide proof that the device has been used on previous projects with acceptable results, or construct a test section prior to the beginning of work and demonstrate wedge compaction to the satisfaction of the Engineer. Short sections of handwork will be allowed when necessary for transitions and turnouts, or otherwise authorized by the Engineer.

Mill and Overlay approximately 3.5 miles of U.S. Hwy. 80 from the end of the 4-lane East of SR 15 to just East of the Veteran's Memorial Cemetery, known as State Project No. MP-5080-51(022) / 304022301 in the County of Newton, State of Mississippi.

I (We) agree to complete the entire project within the specified contract time.

**\*\*\* SPECIAL NOTICE TO BIDDERS \*\*\***

**BIDS WILL NOT BE CONSIDERED UNLESS BOTH UNIT PRICES AND ITEM TOTALS ARE ENTERED.  
 BIDS WILL NOT BE CONSIDERED UNLESS THE BID CERTIFICATION LOCATED AT THE END OF THE BID SHEETS IS SIGNED**

**\*\*\*BID SCHEDULE\*\*\***

Line No.	Item Code	Adj Code	Quantity	Units	Description	Unit Price		Item Amount	
						Dollar	Ct	Dollar	Ct
<b>Roadway Items</b>									
0010	202-B033 Changed 01/17/2011		75	Square Yard	Removal of Concrete Pavement w/ Variable Depth Overlay				
0020	202-B038		115	Linear Feet	Removal of Curb, All Types				
0030	202-B102		825	Linear Feet	Removal of Guard Rail				
0040	203-G004	(E)	38	Cubic Yard	Excess Excavation, LVM, AH				
0050	406-A001		15,730	Square Yard	Cold Milling of Bituminous Pavement, All Depths				
0060	503-C007		550	Linear Feet	Saw Cut, Full Depth				
0070	606-B007		338	Linear Feet	Guard Rail, Class A, Type 1, 'W' Beam, Metal Post				
0080	606-D004		8	Each	Guard Rail, Bridge End Section, Type E				

Line No.	Item Code	Adj Code	Quantity	Units	Description	Unit Price		Bid Amount	
0090	606-E001		8	Each	Guard Rail, Terminal End Section				
0100	609-E001		115	Linear Feet	Bituminous Curb				
0110	618-A001		1	Lump Sum	Maintenance of Traffic	XXXXXXXX	XXX		
0120	619-A1002		14	Mile	Temporary Traffic Stripe, Continuous White				
0130	619-A2002		6	Mile	Temporary Traffic Stripe, Continuous Yellow				
0140	619-A4006		7	Mile	Temporary Traffic Stripe, Skip Yellow				
0150	619-A5001 Changed 01/17/2011		8,741	Linear Feet	Temporary Traffic Stripe, Detail				
0160	619-A6001		744	Linear Feet	Temporary Traffic Stripe, Legend				
0170	619-A6002		77	Square Feet	Temporary Traffic Stripe, Legend				
0180	619-D1001		202	Square Feet	Standard Roadside Construction Signs, Less than 10 Square Feet				
0190	619-D2001		100	Square Feet	Standard Roadside Construction Signs, 10 Square Feet or More				
0200	619-F3004		35	Each	Delineators, Guard Rail, White				

Line No.	Item Code	Adj Code	Quantity	Units	Description	Unit Price		Bid Amount	
0210	619-G4005		24	Linear Feet	Barricades, Type III, Double Faced				
0220	620-A001		1	Lump Sum	Mobilization	XXXXXXXX	XXX		
0230	627-J001		131	Each	Two-Way Clear Reflective High Performance Raised Markers				
0240	627-K001		10	Each	Red-Clear Reflective High Performance Raised Markers				
0250	627-L001		452	Each	Two-Way Yellow Reflective High Performance Raised Markers				
0260	630-G001		4	Each	Type 3 Object Markers, OM-3R, Post Mounted				
0270	630-G003		4	Each	Type 3 Object Markers, OM-3L, Post Mounted				
0280	907-304-A001	(GY ) Changed 01/17/2011	270	Cubic Yard	Granular Material, LVM, Class 5, Group C				
0285	907-407-A001	(A2 ) Added 01/17/2011	4,360	Gallon	Asphalt for Tack Coat				
0290	907-413-D001		18,820	Linear Feet	Cleaning and Filling Joints in PCC Pavement				
0300	907-413-E001		20,702	Linear Feet	Sawing and Sealing Transverse Joints in Asphalt Pavement				
0310	907-606-D001		8	Each	Special Bridge Connector				

Line No.	Item Code	Adj Code	Quantity	Units	Description	Unit Price		Bid Amount	
0320	907-626-C004		7	Mile	6" Thermoplastic Edge Stripe, Continuous White				
0330	907-626-D003		4	Mile	6" Thermoplastic Traffic Stripe, Skip Yellow				
0340	907-626-E004		3	Mile	6" Thermoplastic Traffic Stripe, Continuous Yellow				
0350	907-626-G004		1,633	Linear Feet	Thermoplastic Detail Stripe, White				
0360	907-626-G005		4,498	Linear Feet	Thermoplastic Detail Stripe, Yellow				
0370	907-626-H004		372	Linear Feet	Thermoplastic Legend, White				
0380	907-626-H005		39	Square Feet	Thermoplastic Legend, White				
<b>ALTERNATE GROUP AA NUMBER 1</b>									
0390	907-403-A006 (BA1)		6,250	Ton	Hot Mix Asphalt, MT, 12.5-mm mixture Changed 01/17/2011				
0400	907-403-B005 (BA1)		120	Ton	Hot Mix Asphalt, MT, 19-mm mixture, Leveling Changed 01/17/2011				
0410	907-403-C003 (BA1)		3,400	Ton	Hot Mix Asphalt, MT, 19-mm mixture, Trench Widening				
<b>ALTERNATE GROUP AA NUMBER 2</b>									
0420	907-403-M002 (BA1)		6,250	Ton	Warm Mix Asphalt, MT, 12.5-mm mixture Changed 01/17/2011				



Section 905  
Proposal (Sheet 2 - 5)

MP-5080-51(022) / 304022301  
Newton County

Line No.	Item Code	Adj Code	Quantity	Units	Description	Unit Price		Bid Amount	
0430	907-403-N007	(BA1)	120	Ton	Warm Mix Asphalt, MT, 19-mm mixture, Leveling Changed 01/17/2011				
0440	907-403-O003	(BA1)	3,400	Ton	Warm Mix Asphalt, MT, 19-mm mixture, Trench Widening				

\*\*\* BID CERTIFICATION \*\*\*

TOTAL BID.....\$\_\_\_\_\_

\*\*\* SIGNATURE STATEMENT \*\*\*

BIDDER ACKNOWLEDGES THAT HE/SHE HAS CHECKED ALL ITEMS IN THIS PROPOSAL FOR ACCURACY AND CERTIFIED THAT THE FIGURES SHOWN THEREIN CONSTITUTE THEIR OFFICIAL BID.

\_\_\_\_\_  
BIDDER'S SIGNATURE

\_\_\_\_\_  
BIDDER'S COMPANY

\_\_\_\_\_  
BIDDER'S FEDERAL TAX ID NUMBER