

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) enclose a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u>1</u>	DATED <u>2/11/2011</u>	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. _____	DATED _____	ADDENDUM NO. _____	DATED _____

Number	Description
1	Revised Table of Contents, replace same; Delete NTB No. 1546; Revised NTB No. 3361, replaces same; Revised wage rates; Bidsheets, replace same; Amendment EBS Download Required.

TOTAL ADDENDA: 1
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.

Revised 09/21/2005

STP-0039-02(046) / 105895301

Rankin County(ies)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

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SECTION 905 - PROPOSAL,
PROPOSAL BID SHEETS,
COMBINATION BID PROPOSAL,
CERTIFICATION OF PERFORMANCE - PRIOR FEDERAL-AID CONTRACTS,
CERTIFICATION REGARDING NON-COLLUSION, DEBARMENT AND SUSPENSION,
SECTION 902 - CONTRACT FORM, AND SECTION 903 - CONTRACT BOND FORM,
OCR-485.

(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET OF SECTION 905
AS ADDENDA)
DENDA)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS 3361

CODE: (SP)

DATE: 02/11/2011

SUBJECT: Scope Of Work

PROJECT: STP-0039-02(046) / 105895301 -- Rankin County

The contract documents do not include an official set of construction plans. Applicable Standard Drawings have been included and should be referenced when so specified in a Notice To Bidders entitled, "Standard Drawings". All other reference to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on this project shall consist of milling and overlaying approximately 7.41 miles of SR 18 in Rankin County from the BOP (Station 617+28.70 BK = 617+37.44 AH) near Rock Hill Road to the EOP (Station 1010+80) near the Brandon City Limits.

The work shall consist of milling the existing asphalt pavement to approximately a 2% cross-slope, and overlaying with one (1) 2" lift of HMA/WMA, MT, 12.5mm Mix in tangent sections. Super-elevated sections, turn lanes and county roads shall be milled 2" on the existing slope and overlaid with one (1) 2" lift of HMA/WMA, MT, 12.5mm Mix. HMA/WMA, MT, 12.5 mm, Leveling will be required as directed to correct any grade distortions.

Traffic will **not** be allowed to run on the milled surface; therefore, any area milled shall be paved prior to opening the lane to traffic. Overnight lane closures will not be allowed for this operation. Milling shall be performed in accordance with the attached details at all county roads and mainline tie-ins. Milling operations shall be in accordance with the contract documents. Temporary pavement joints (paper joints) shall be at least 3 paper-widths long and shall be used at all milled tie-ins and shall be adequately maintained. Milling ties at concrete pavement or bridge joints shall be performed in such a manner as to provide a perpendicular joint face. The concrete pavement shall not be damaged by the milling operation; any damage shall be repaired at the Contractor's expense.

Unpaved shoulders shall be widened to provide a 2-foot wide paved shoulder. The existing shoulder shall be excavated 2½" & variable below the existing pavement grade and paved to a depth of 4½" with 12.5mm HMA/WMA, MT. The shoulder widening shall be placed concurrently with the mainline overlay operations.

Material excavated shall be used to raise the existing shoulder to match the pavement elevation. Surplus material shall be spread along the edge of the shoulders, fore-slopes, or other adjacent areas as directed by the Engineer and will be an absorbed item.

Prior to beginning the overlay operations, failed areas of the existing roadway are to be removed full depth and back filled with HMA/WMA, 12.5mm, MT mixture. Potholes are to be patched prior to the asphalt overlay, the cost of which is to be included in the price for items bid.

Where applicable the existing shoulders are to be raised to match the new pavement elevation by placing variable depth Granular Material (Class 5, Group C) on the existing shoulders. Placement of the granular material on the finished asphalt course shall not be permitted. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%). Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%). The cost of blading will be an absorbed item and is not to be included in the price of pay items bid.

The Reclaimed Asphalt Pavement (RAP) material removed by the milling operation shall become the property of the contractor with the exception of 10,000 tons or 50% of the total anticipated quantity, whichever is less, to be stockpiled at the Whitfield Maintenance Office located at 3769 Hwy 468, Pearl, MS. Unless the Contractor desires otherwise, the Contractor's RAP shall be obtained first. The Contractor shall be required to push the milled material into one stockpile. The Contractor shall be responsible for placing and operating equipment necessary to "push up" the milled material. All costs associated with the hauling, placing, and stockpiling the RAP shall be included in the price bid for cold milling.

All existing guard rail, terminal end sections, and bridge-end sections shall be removed and replaced (non-flared installation). Voids created by the removal of posts, concrete anchors, footings, etc., shall be backfilled and tamped in accordance with Section 203 of the 2004 Mississippi Standard Specifications for Road and Bridge Construction. Terminal end sections are to be installed per manufacturer's specifications.

The Contractor shall be responsible for the removal of existing raised pavement markers and cold plastic tape prior to placement of the new raised pavement markers and thermoplastic striping. Cost of removing existing raised pavement markers is to be included in the cost of other items of work. The rumble strips (edge line) and pavement markings shall be installed in accordance with the Standard Specifications, special provisions and enclosed drawings.

The Contractor is responsible for all pavement surface cleaning or other preparation necessary to insure a proper bond prior to placement of the pavement markings.

All permanent striping will be plastic. Edge lines will be placed so as to maintain the original lane width. In addition to these markings, raised pavement markers will be placed at 80 foot intervals in tangents, 40 foot intervals in curves and as directed by the Engineer.

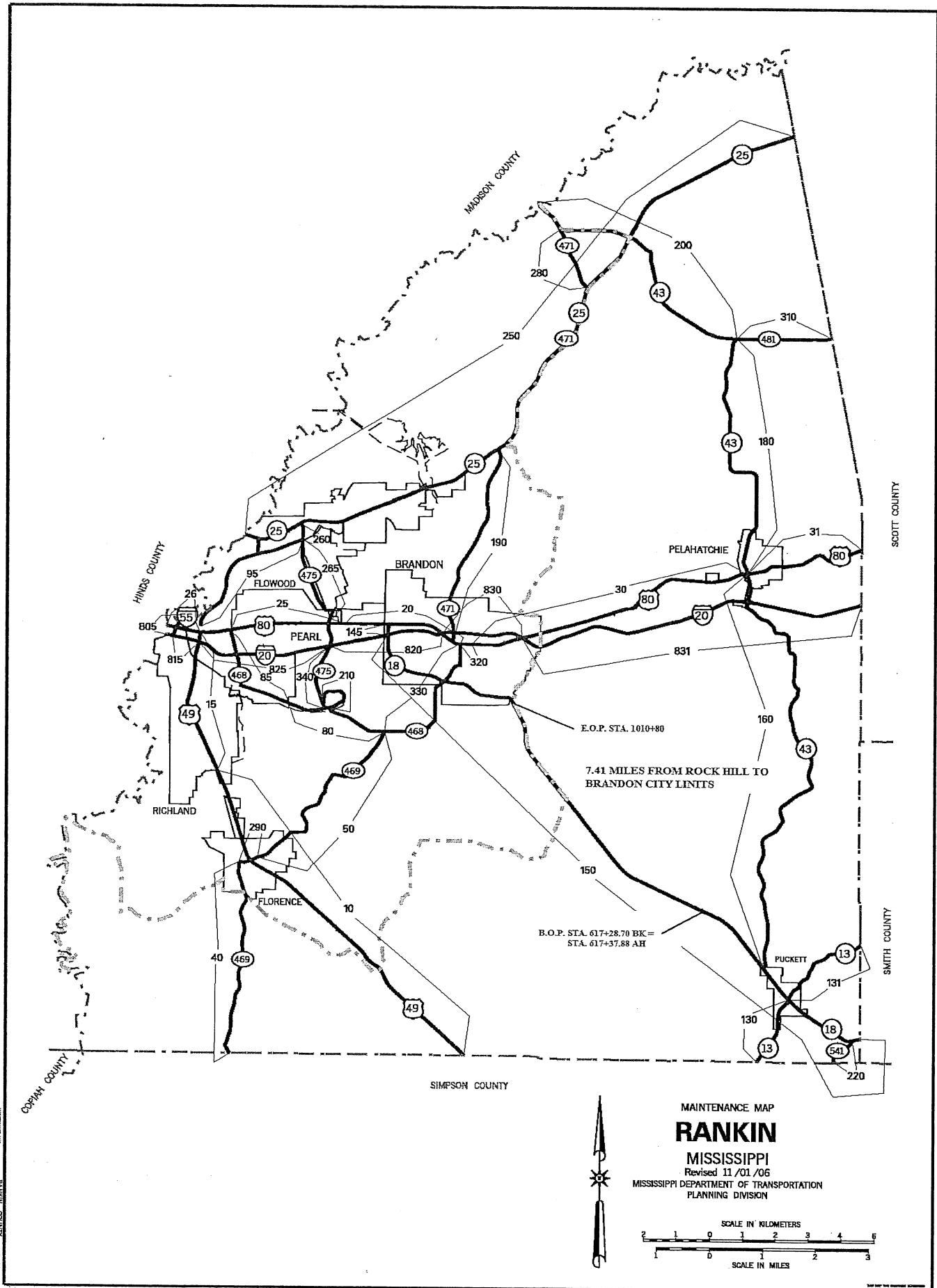
Temporary striping shall conform to finished stripe specifications for alignment, neatness, reflectivity and straightness. Temporary stripe, either paint or tape, will be required immediately after overlaying and prior to opening area to traffic. All permanent pavement markings are to be hot thermoplastic. Edge lines will be placed so as to maintain the original lane width. Glass beads applied to thermoplastic shall conform to Section Number - 720.01

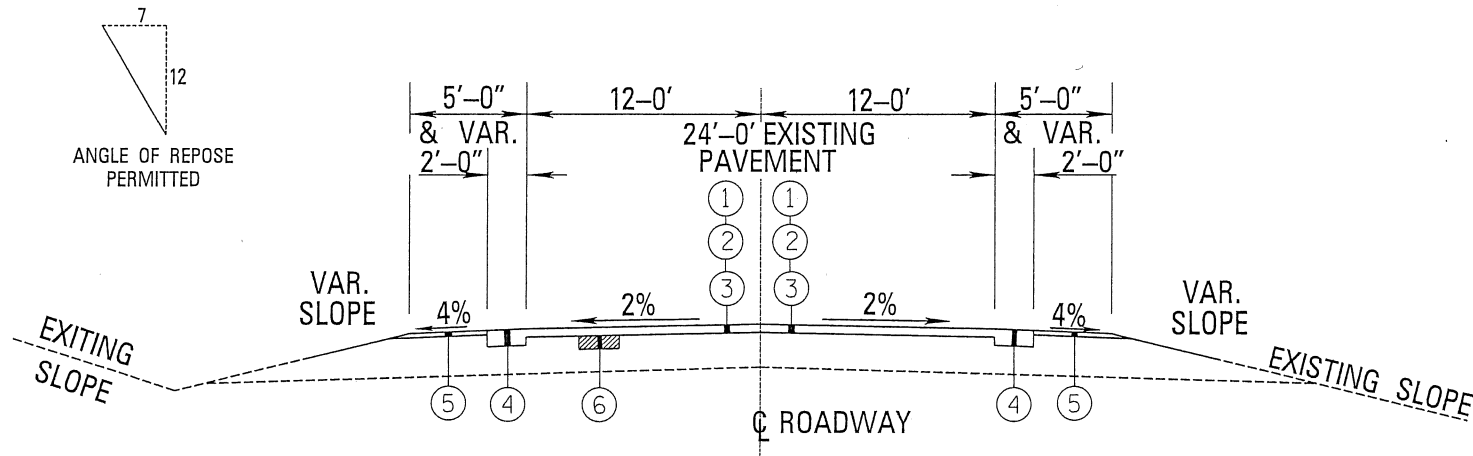
The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost is to be included in the price for Pay Item No. 618-A, Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend on white background. Standard roadside construction signs and barricades will be paid for using the appropriate pay items.

Incidental work such as removing vegetation, removing and resetting signs and mailboxes, shaping and compacting shoulders, removing excess asphalt material, project clean-up, and other incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

It shall be the responsibility of the Contractor to protect existing structures such as pipes, aprons, bridges, etc., from damage occurring during construction. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged during the life of the contract. No payment will be made for replacements and or repairs resulting from such damages.

Litter shall be removed from the project ROW and properly disposed of by the Contractor before the project will be released to maintenance. Litter shall include solid waste such as glass, paper products, tires, wood products, metal, synthetic materials and other miscellaneous debris.

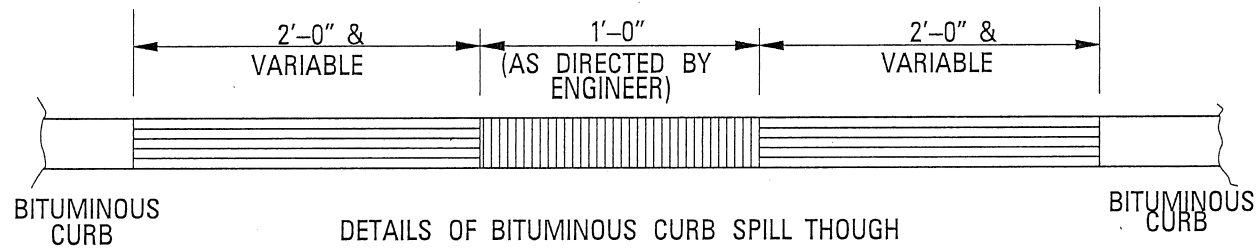




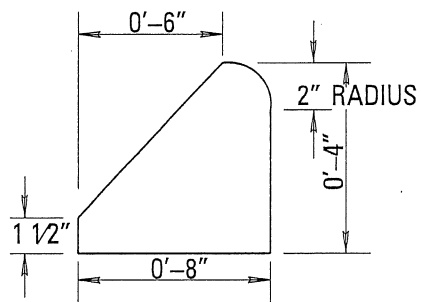
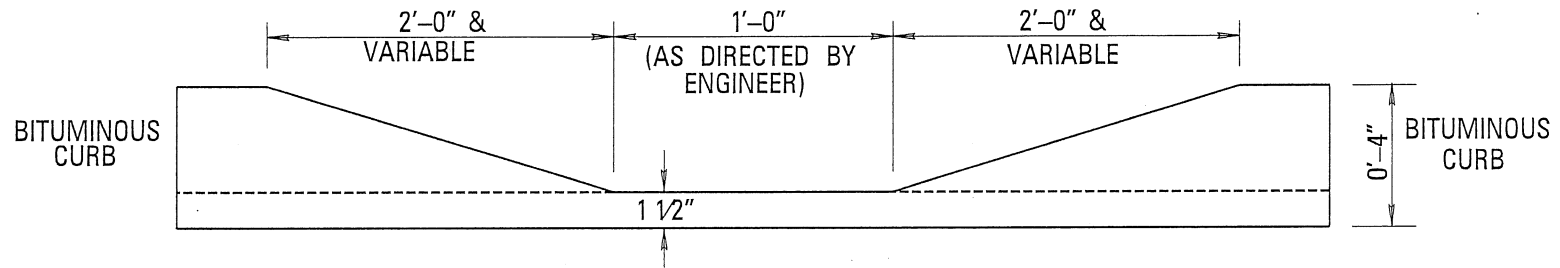
TYPICAL SECTION (HWY 18)

(B.O.P.) STA. 617 + 28.70 BK = STA. 617 + 37.88 AH TO (E.O.P.) STA. 1010 + 80
(N.T.S.)

- ① MILLING REQUIRED (1 1/2" AT CENTERLINE AND 2" AT EGDE OF PAVEMENT)
- ② VARIABLE DEPTH HOT OR WARM MIX ASPHALT, 12.5mm MIXTURE, MT. REQ'D. (FOR GRADE DISTORTION) (AS DIRECTED BY THE ENGINEER)
- ③ 2" AND VAR. HOT OR WARM MIX ASPHALT, 12.5-mm MIXTURE, MT. REQ'D.
- ④ 4.5" AND VAR. HOT OR WARM MIX ASPHALT, 12.5-mm MIXTURE, MT. REQ'D. (TO BE PLACED MONOLITHIC WITH TOP COURSE)
- ⑤ VARIABLE DEPTH GRANULAR MATERIAL (CLASS 5, GROUP "C") REQ'D. (TO BRING SHOULDER TO GRADE)
- ⑥ FAILED AREA (FULL DEPTH HOT OR WARM MIX ASPHALT MT, 12.5mm MIXTURE, REQ'D.)

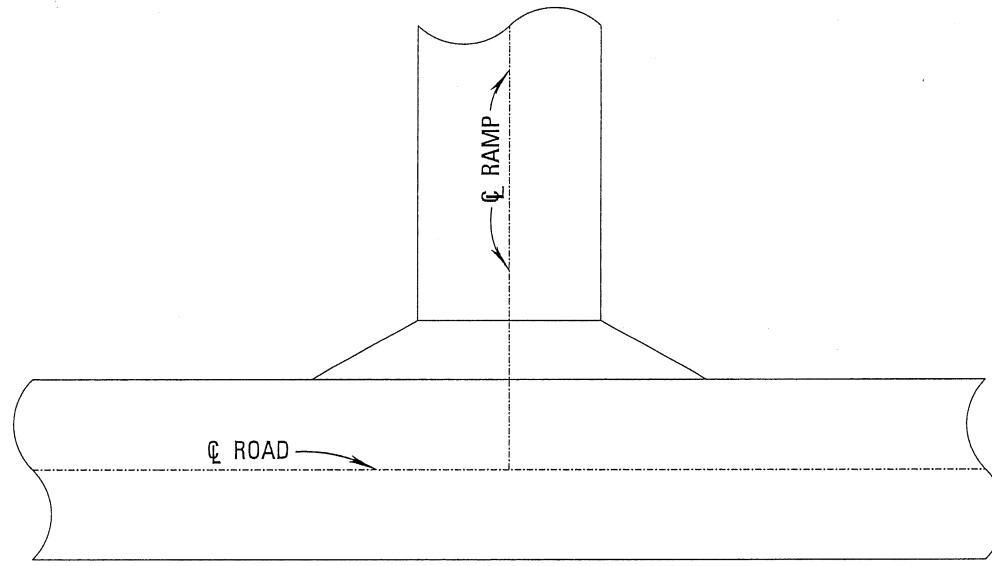


DETAILS OF BITUMINOUS CURB SPILL THOUGH

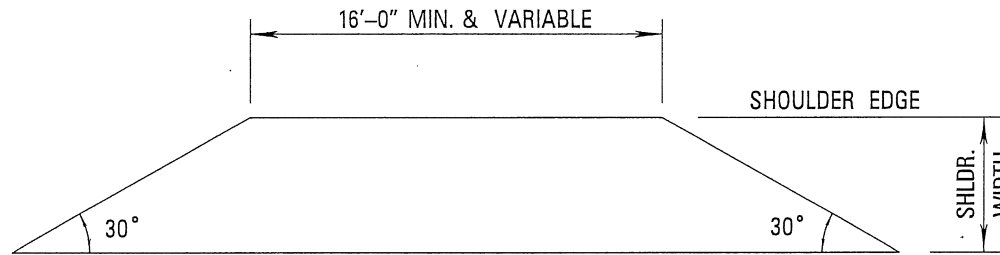


BITUMINOUS CURB CROSS SECTION

DETAILS
BITUMINOUS CURB AND SPILL THROUGH
TYPICAL TURNOUTS

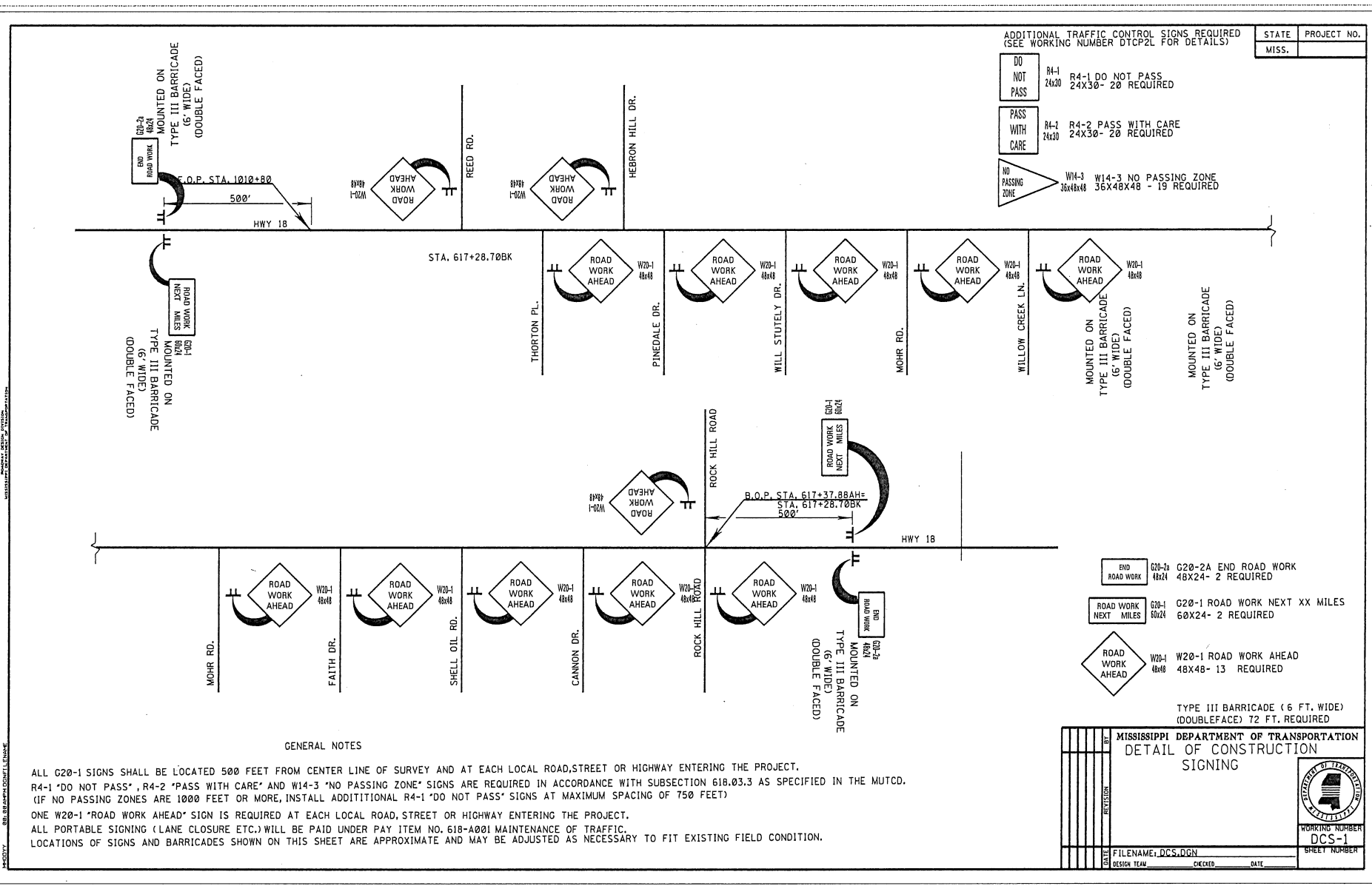


PLAN VIEW OF RAMP

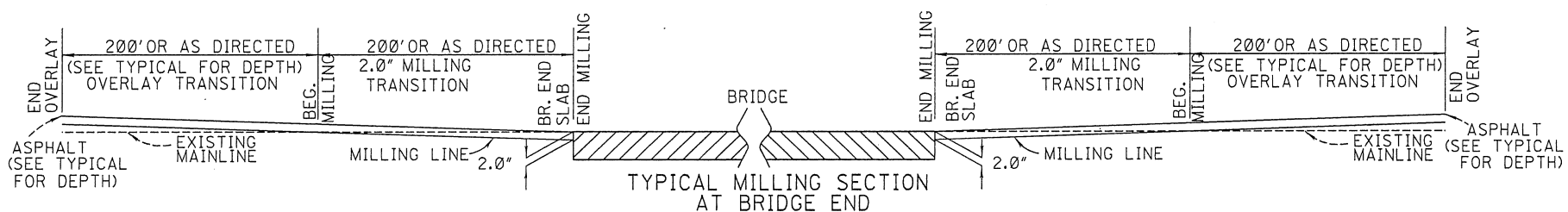


DETAIL OF PAVED APRON

DETAILS
TYPICAL PAVED APRON



STATE	PROJECT NO.
MISS.	



TYPICAL MILLING SECTION
AT BRIDGE END

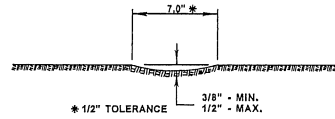
(AS DIRECTED BY THE ENGINEER)

NOTE: (SEE PLAN VIEW SHEETS FOR LOCATIONS)

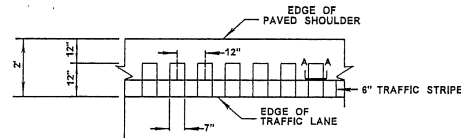
IN ORDER TO PROVIDE ADEQUATE VERTICAL CLEARANCES AT BRIDGES AND UNDERPASSES
MILLING SHALL BE REQUIRED TO THE DEPTH SPECIFIED BY THE ENGINEER.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION			
DETAIL OF MILLING TRANSITION			
PROJECT:		PRELIMINARY	
COUNTY:		NOT FOR CONSTRUCTION	
FILENAME: MILLING.DETAIL.DGN		WORKING NUMBER DMCS-1	
DATE	DESIGN YEAR	CHECKED	SHEET NUMBER

STATE	PROJECT NO.
MISS.	

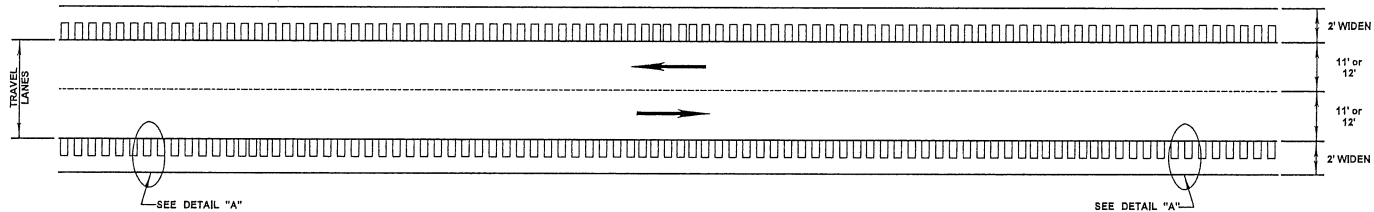


SECTION "A-A"



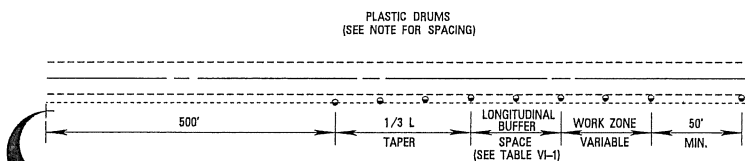
DETAIL "A"

- GENERAL NOTES**
1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
 2. GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
 4. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.
 5. DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.



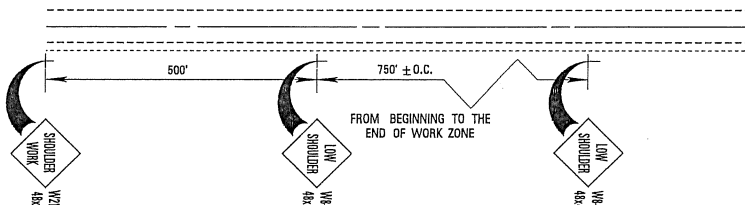
PLAN
NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
SPECIAL DESIGN: RUMBLE STRIPES (GROUND-IN) 2 LANE	
PROJECT NO.:	
COUNTY :	
FILENAME: D2L_ERS.DGN	WORKING NUMBER D2L_ERS
DESIGN TEAM	CHECKED DATE



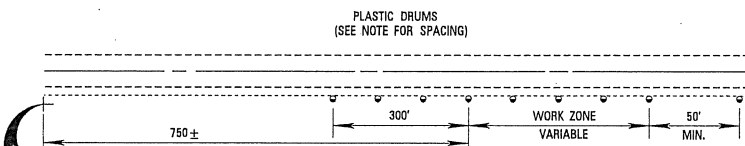
TYPICAL SHOULDER CLOSURE

- (1) TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
- (2) TO BE USED WHEN CONSTRUCTION VEHICLES (EQUIPMENT) ENCOACHES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.



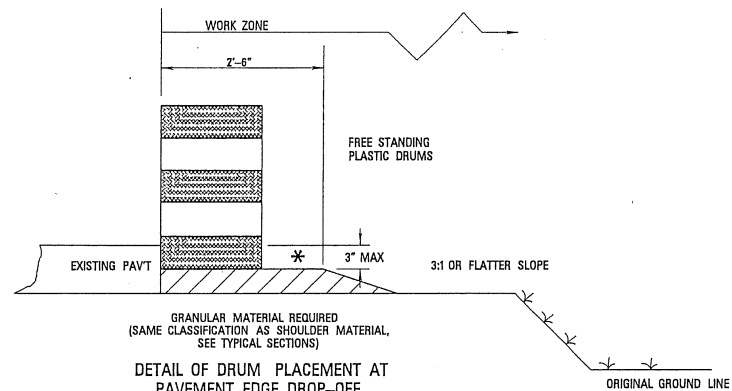
TYPICAL SHOULDER WORK #1

(SEE NOTE A-1 THIS SHEET)



TYPICAL SHOULDER WORK #2

NOTE: WORK OUTSIDE THE (2) FOOT LIMIT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA, OR SEE NOTE A-3 THIS SHEET.



DETAIL OF DRUM PLACEMENT AT PAVEMENT EDGE DROP-OFF

NOTES

* A. PAVEMENT EDGE DROP-OFF

1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES—NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750' ± 0.C.).
2. TWO AND ONE QUARTER TO THREE INCHES—PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. CONES MAY BE USED IN PLACE OF DRUMS, PANELS, AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE M.U.T.C.D. (1/3 L, WHERE L IS THE TAPER LENGTH IN FEET.)
3. GREATER THAN THREE (3) INCHES—POSITIVE SEPARATION OR WEDGE WITH 3:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED. IF CONCRETE BARRIERS ARE USED, SPECIAL REFLECTIVE DEVICES OR STEADY BURN LIGHTS SHOULD BE USED FOR OVERNIGHT INSTALLATIONS.
4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

B. DRUM SPACING

1. TANGENTS = 2 X S
 2. TAPERS = L / 3
- WHERE L = S X W
L = TAPER LENGTH IN FEET
S = SPEED IN MPH (POSTED OR 85 PERCENTILE)
W = WIDTH OF OFFSET IN FEET

C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

TABLE VI-1. GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

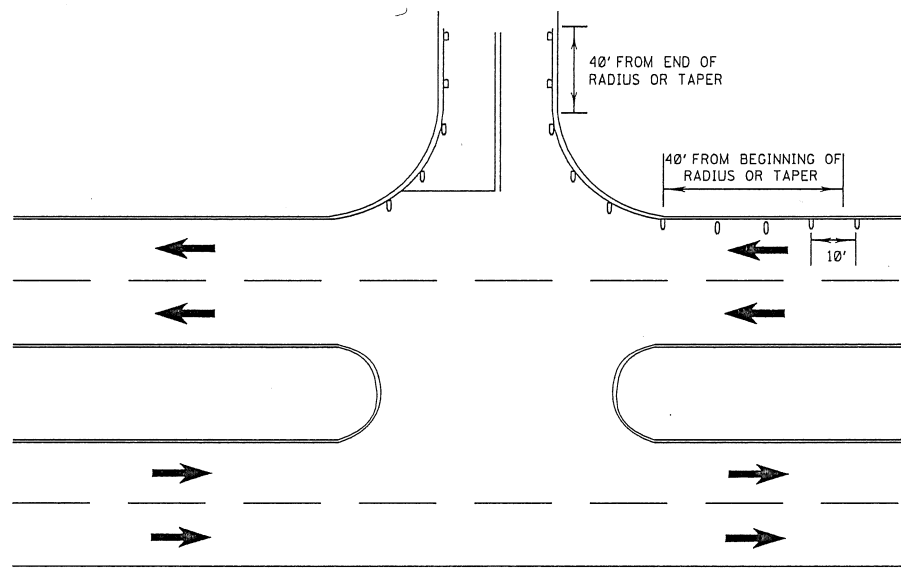
SPEED* (MPH)	LENGTH (FEET)
20	25
25	35
30	45
35	55
40	65
45	75
50	85
55	95
60	105
65	115

* POSTED SPEED, OFF-PEAK 85 PERCENTILE SPEED PRIOR TO WORK STARTING, OR THE ANTICIPATED OPERATING SPEED IN MPH.

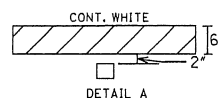
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TRAFFIC CONTROL DETAILS	
DRUM PLACEMENT	
AND	
SHOULDER CLOSURE	
PROJECT NO.:	WORKING NUMBER
COUNTY:	TCP-SC
FILENAME: <u>gans/shldclosure.dgn</u>	SHEET NUMBER
DATE	DATE
DESIGN TEAM	CHECKED
	DATE 12-08-01

STATE	PROJECT NO.
MISS.	

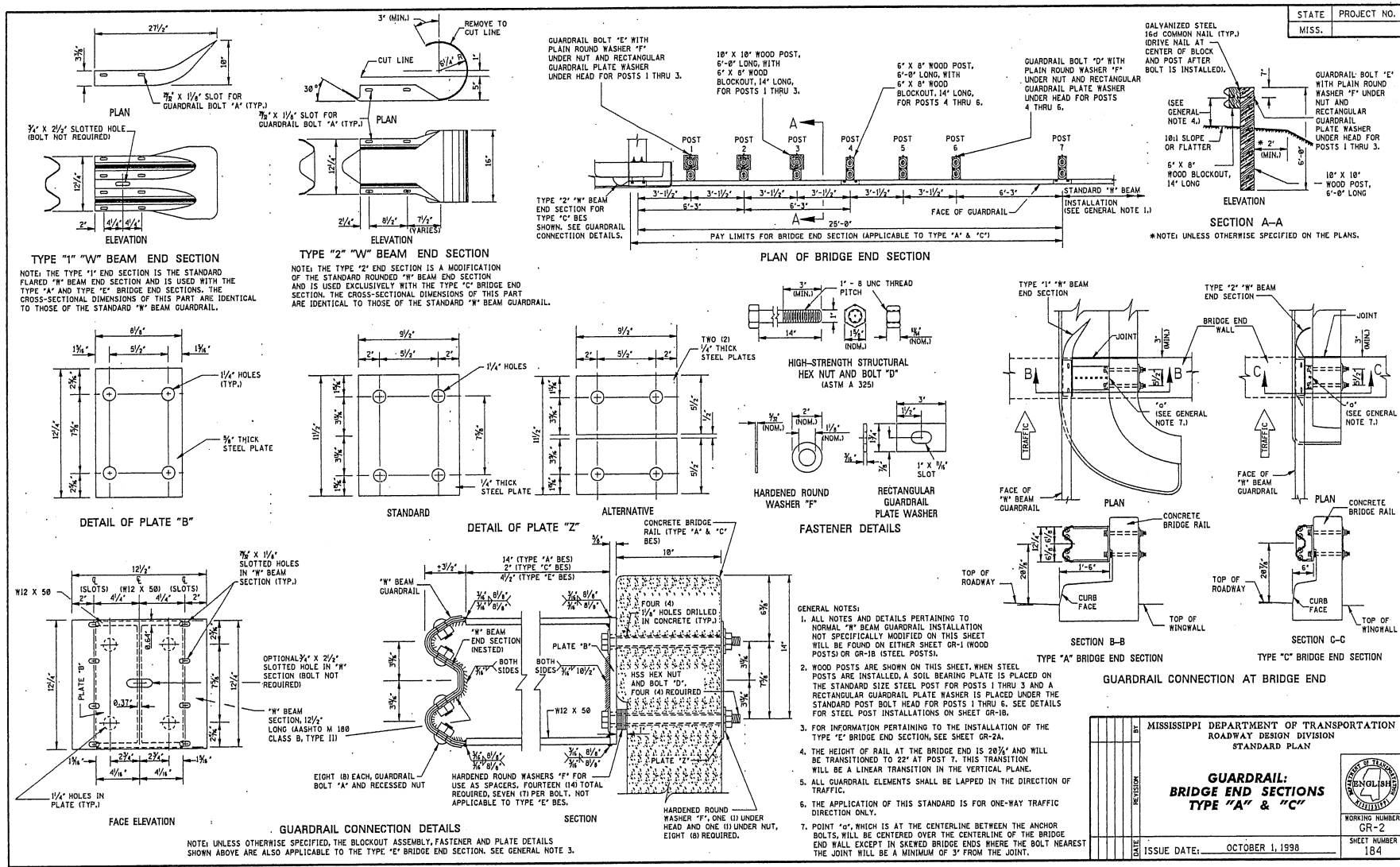
TYPICAL FOR RAISED PAVEMENT MARKERS PLACED ON SIDE ROAD RADIUS

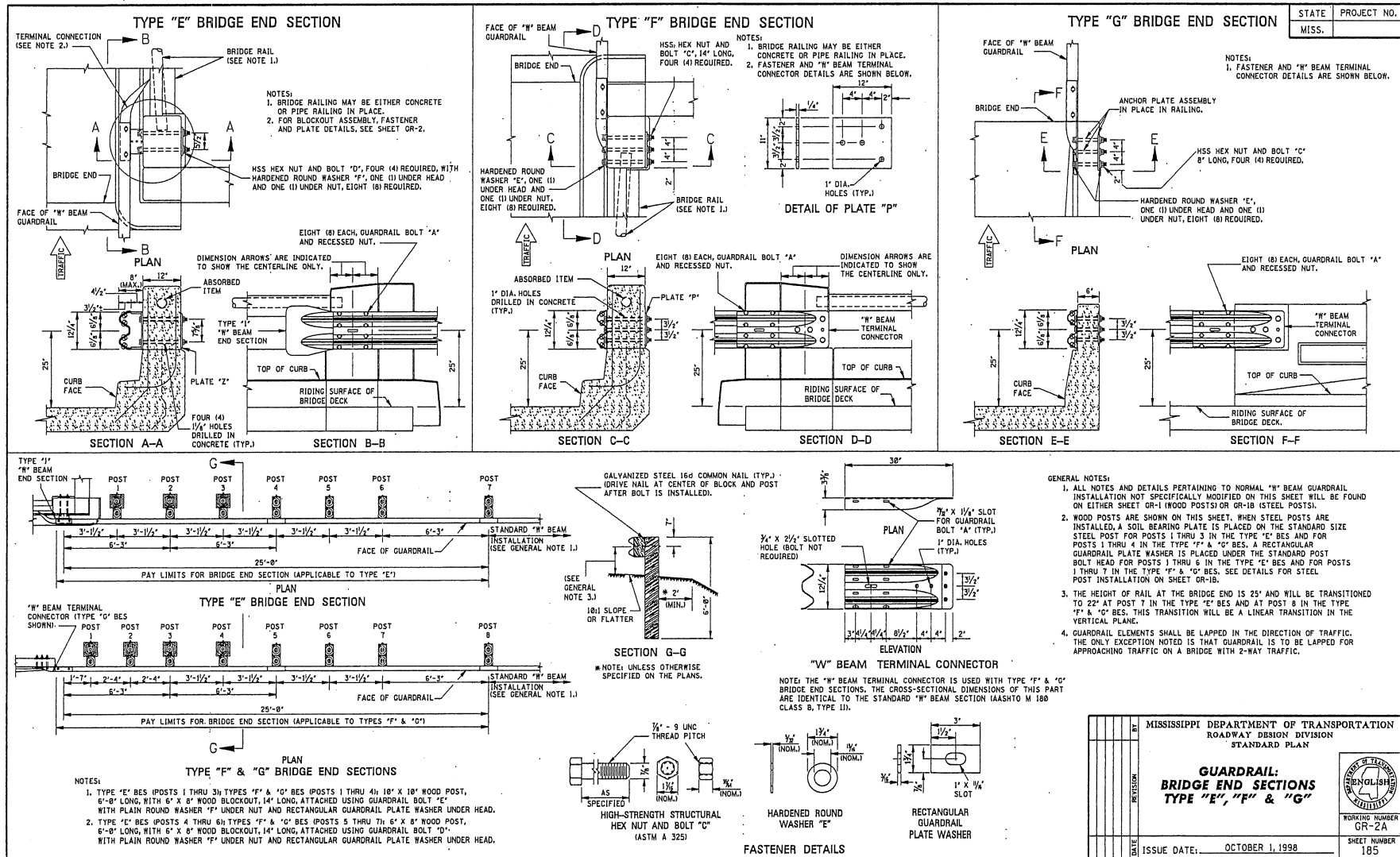


- NOTE 1. MARKERS SHALL BE PLACED EVERY 10 FEET.
- NOTE 2. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
- NOTE 3. MARKERS SHALL BE HIGH PERFORMANCE TWO WAY CLEAR.
- NOTE 4. CHANNELIZED INTERSECTIONS WILL REQUIRE MORE MARKERS THAN STANDARD INTERSECTIONS DUE TO LONGER TAPER.
- NOTE 5. FIVE (5) MARKERS SHALL BE PLACED ALONG MAINLINE EDGE STRIPE.
- NOTE 6. MARKERS FOR COUNTY ROADS SHALL CONTINUE DOWN THE EDGE STRIPE A DISTANCE OF 40 FEET.

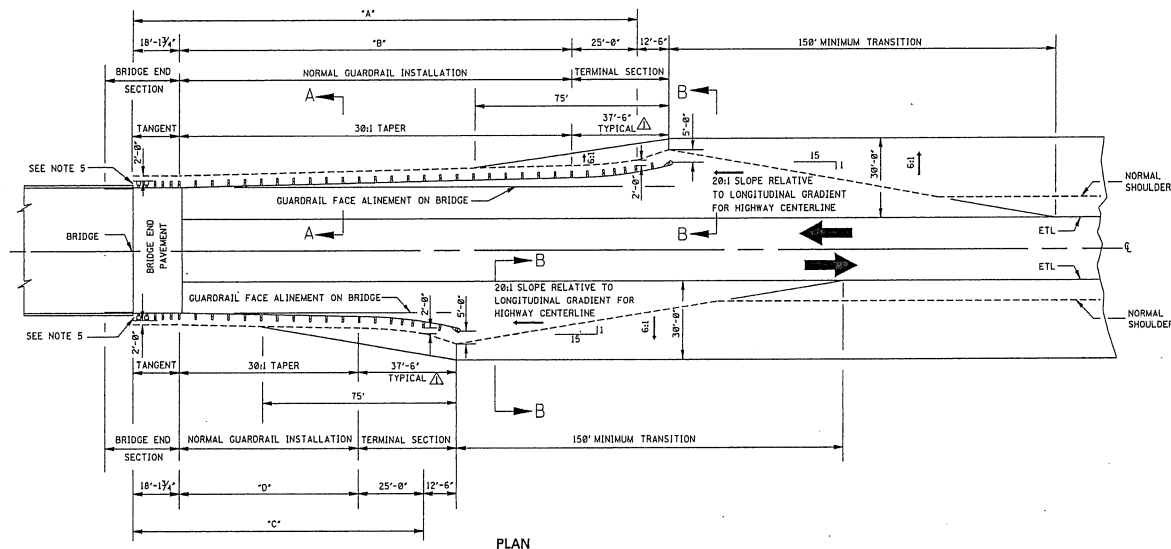


MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
STRIPING DETAILS FOR LOCAL ROADS ON DIVIDED HWY	
PROJECT NO.:	
COUNTY :	
FILENAME: LRD-2.DGN	WORKING NUMBER LRD-2
DESIGN TEAM	CHECKED
DATE	DATE
SHEET NUMBER	

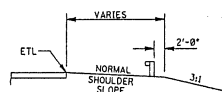




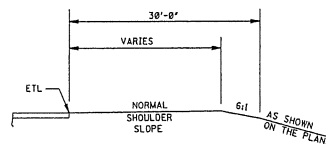
STATE	PROJECT NO.
MISS.	



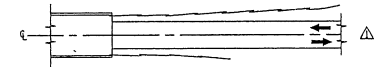
PLAN



SECTION A-A



SECTION B-B

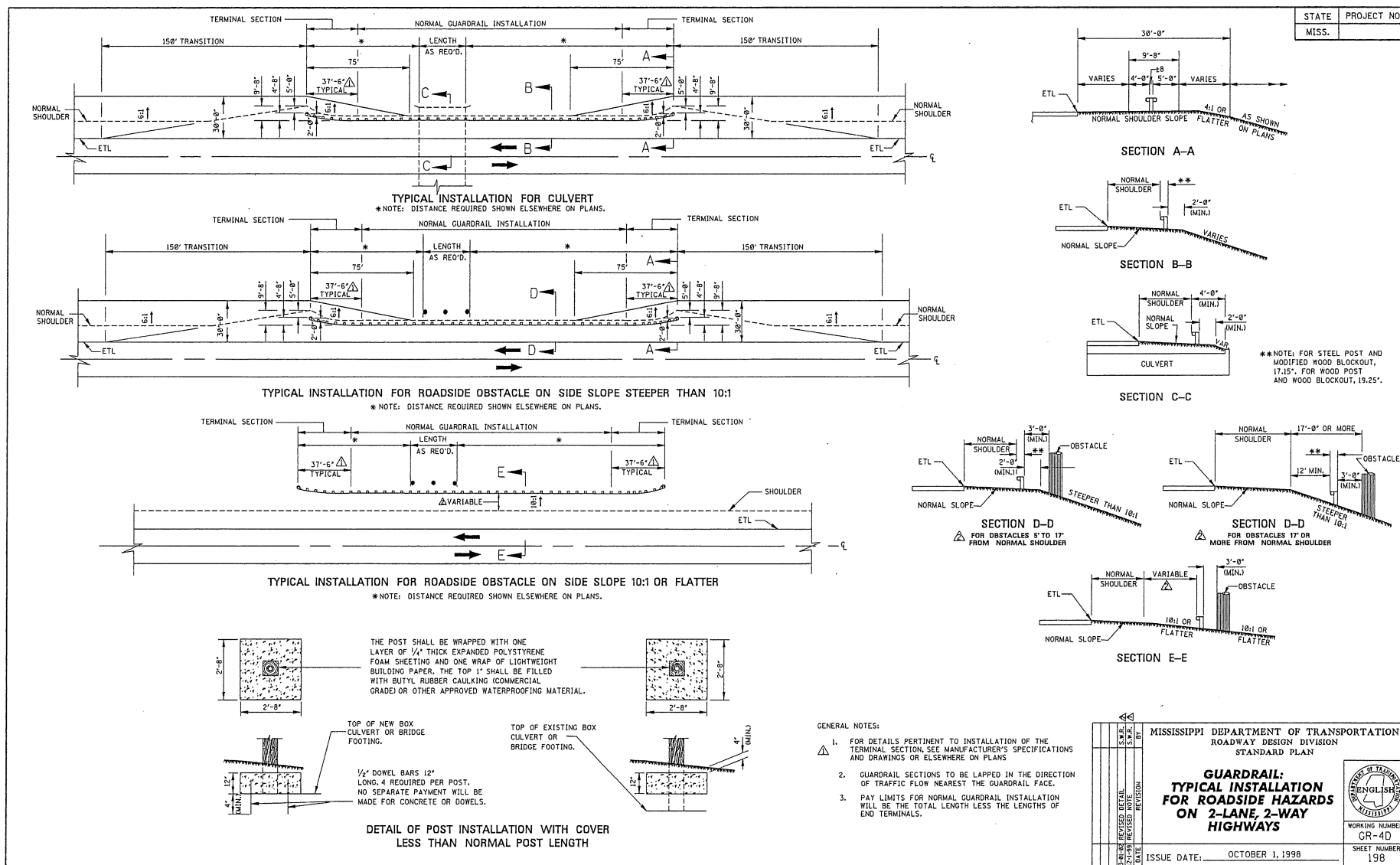


DETAIL OF GUARDRAIL SECTION LAPS

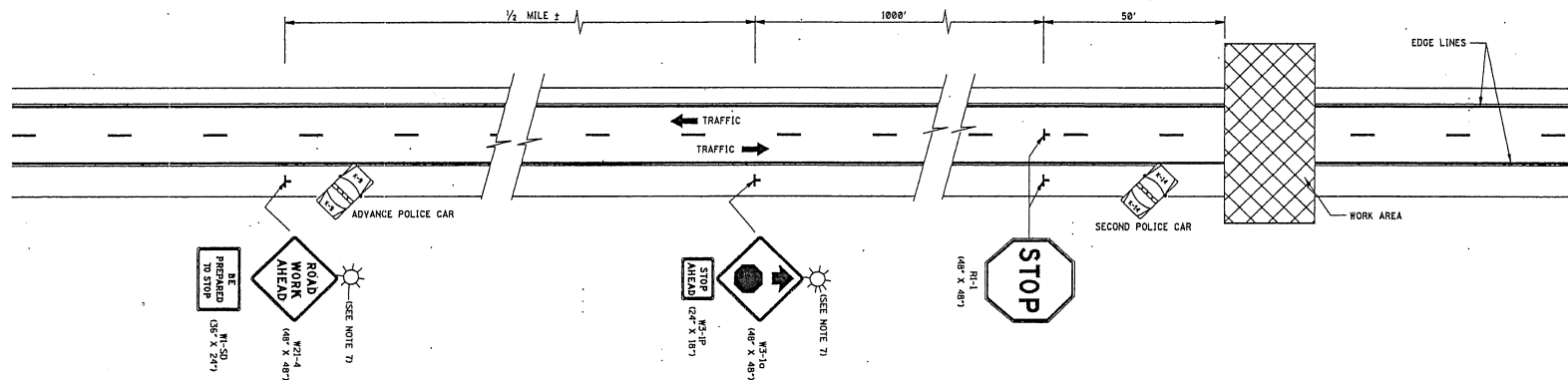
GENERAL NOTES:

1. VALUES FOR "A", "B", "C" AND "D" WILL BE SHOWN ELSEWHERE ON THE PLANS.
2. FOR DETAILS PERTINENT TO INSTALLATION OF THE TERMINAL SECTION, SEE MANUFACTURER'S SPECIFICATIONS AND DRAWINGS OR ELSEWHERE ON PLANS.
3. GUARDRAIL SECTIONS ARE TO BE LAPPED IN THE DIRECTION OF TRAFFIC APPROACHING THE BRIDGE.
4. THE OVERALL LENGTH OF GUARDRAIL IS MEASURED FROM THE CONNECTING END ON THE BRIDGE.
5. THE SHOULDER WIDTH AT THE BRIDGE END SHALL BE SUFFICIENTLY WIDE TO PROVIDE A MINIMUM OF 2'-0" BEHIND THE BACK OF POST BEFORE THE SLOPE BREAK (HINGEPOINT).
6. TYPE, DETAILS AND LIMITS OF GUARDRAIL BRIDGE END SECTION WILL BE SHOWN ELSEWHERE ON THE PLANS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
GUARDRAIL: TYPICAL INSTALLATION AT BRIDGE APPROACHES FOR 2-LANE, 2-WAY HIGHWAY	
ISSUE DATE: OCTOBER 1, 1998	WORKING NUMBER GR-4A
	SHEET NUMBER 195



STATE	PROJECT NO.
MISS.	



GENERAL NOTES:

1. THIS TYPE OF HIGHWAY CLOSURE SHALL ONLY BE USED FOR EMERGENCIES OR FOR CONSTRUCTION OPERATIONS WHEN THE DURATION OF CLOSURE WILL NOT EXCEED 30 MINUTES. AFTER THE HIGHWAY HAS BEEN CLOSED AND REOPENED VIA THIS PROCEDURE, A MINIMUM PERIOD OF 30 MINUTES SHALL ELAPSE BEFORE ANOTHER SHORT DURATION CLOSURE, EXCEPT WITH THE APPROVAL OF THE ENGINEER.
2. AT LEAST TWO POLICEMEN AND TWO POLICE CARS SHALL BE PROVIDED ON EACH APPROACH TO THE CLOSURE. EACH POLICE CAR SHALL HAVE A ROOF MOUNTED ROTATING BLUE LIGHT OR LIGHT BAR.
3. ROAD CLOSURES ARE NOT PERMITTED ON HOLIDAYS OR BETWEEN THE HOURS OF 6 AM TO 9 AM, 11 AM TO 1 PM AND 3 PM TO 6 PM ON MONDAY THROUGH FRIDAY, EXCEPT BY PERMISSION OF THE ENGINEER.
4. THE ADVANCE POLICE CAR AND THE "W21-4 (48" X 48")", "W1-SD (36" X 24")", "W3-10 (48" X 48")", AND "W3-IP (24" X 18")" SIGNS SHALL BE MOVED BACK AS REQUIRED BY THE QUEUING OF STOPPED VEHICLES.

5. TRAFFIC CONTROL FOR THE CLOSURE SHALL BE ACCOMPLISHED IN THE FOLLOWING ORDER:
 - A. ADVANCE POLICE CAR, LIGHTS AND FLASHERS ON.
 - B. "W21-4 (48" X 48")" AND "W1-SD (36" X 24")" SIGNS ERECTED.
 - C. "W3-10 (48" X 48")" AND "W3-IP (24" X 18")" SIGNS ERECTED.
6. SECOND POLICE CAR, LIGHTS AND FLASHERS ON.
7. "R1-1 (48" X 48")" SIGNS ERECTED TO STOP TRAFFIC. THE ORDER OF ERECTION SHALL BE IN THE FOLLOWING ORDER: RIGHT SHOULDER THEN CENTER.

8. TRAFFIC CONTROL SHALL BE REMOVED IN THE FOLLOWING ORDER:
 - A. WITH TRAFFIC STOPPED REMOVE THE "R1-1 (48" X 48")" SIGNS TOWARD THE RIGHT SHOULDER IN THE FOLLOWING ORDER: CENTER THEN SIGN ON THE RIGHT SHOULDER.
 - B. AFTER ALL STOPPED VEHICLES HAVE STARTED MOVING, THE "W3-10 (48" X 48")" AND "W3-IP (24" X 18")" SIGNS SHALL BE REMOVED. THESE SIGNS MAY BE COVERED IF RE-USE IS IMMINENT.
 - C. AFTER ALL CARS HAVE RESUMED APPROXIMATELY NORMAL SPEED, THE "W21-4 (48" X 48")" AND "W1-SD (36" X 24")" SIGNS SHALL BE REMOVED. THESE SIGNS MAY BE COVERED IF RE-USE IS IMMINENT.

9. UNILLUMINATED SECTIONS OF HIGHWAYS SHALL NOT BE CLOSED DURING HOURS OF DARKNESS EXCEPT FOR EMERGENCIES OR WITH THE APPROVAL OF THE ENGINEER. WHEN THE HIGHWAY MUST BE CLOSED DURING HOURS OF DARKNESS, A TYPE B HIGH INTENSITY FLASHING BARRICADE WARNING LIGHT SHALL BE USED ON EACH "W21-4 AND "W3-10" SIGN.
10. IF AN ENTRANCE RAMP IS LOCATED BETWEEN THE "W21-4 AND "R1-1, THE "W21-4 (48" X 48")", "W1-SD (36" X 24")", "W3-10 (48" X 48")", AND "W3-IP (24" X 18")" SIGNS SHALL ALSO BE ERECTED ON THE RAMP SHOULDER.
11. SIGN "W1-SD (36" X 24")" IS 4" SERIES "C" BLACK LEGEND AND BORDER ON ORANGE BACKGROUND.
12. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC, INCLUDING SECURING POLICE SERVICES.
13. THE ABOVE DURATION WILL APPLY TO EACH APPROACH TO THE CLOSURE.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN		
SHORT DURATION CLOSING OF TWO-LANE TWO-WAY HIGHWAYS		
WORKING NUMBER TCP-8	SHEET NUMBER 257	
DATE	ISSUE DATE: OCTOBER 1, 1998	

25' BRIDGE END SECTION
SEE PLAN & PROFILE SHEETS FOR TYPE

VARIABLE LENGTH
BRIDGE-GUARD RAIL CONNECTOR

GUARD RAIL
"W" BEAM

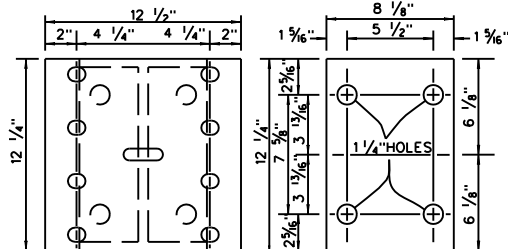
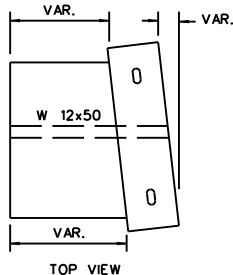
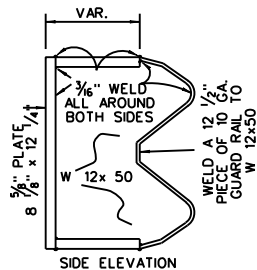
TREATED TIMBER BLOCKOUT
(2" x 12" S) AS REQ'D. BEVEL AS
REQ'D. BOLT TO GUARD RAIL WITH 5/8" BOLT.

TOP OF CURB
TOP OF RAIL

TERMINAL CONNECTOR REQ'D.

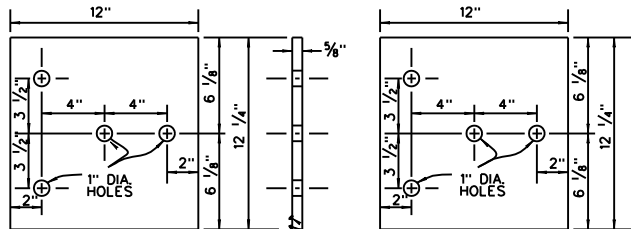
MOUNTING BRACKETS REQ'D

BRIDGE-GUARD RAIL CONNECTOR DETAIL



FRONT ELEVATION SHOWING 10 GA. GUARD RAIL CONNECTION TO W12x50
BACK OF RAIL PLATE & BACK OF BRACKET

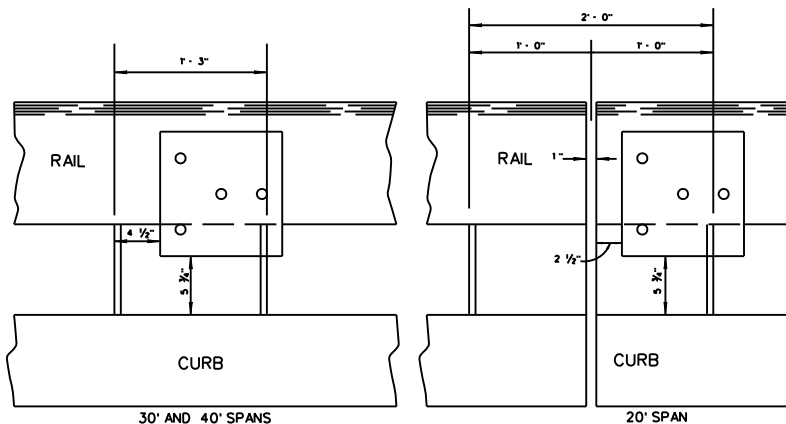
DETAIL OF INTERMEDIATE BRACKET



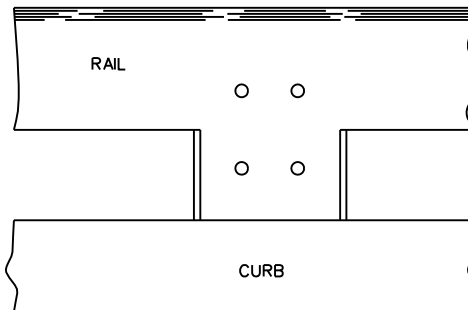
DETAIL OF FRONT PLATE

DETAIL OF BACK PLATE

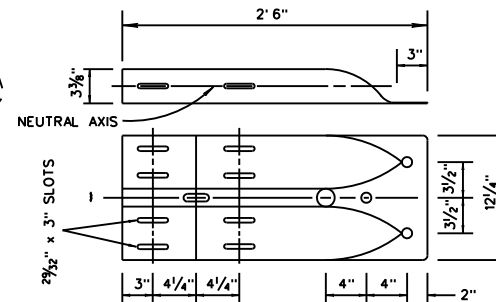
DETAIL OF TERMINAL CONNECTOR PLATE



DETAILS SHOWING PLACEMENT OF HOLES FOR CONNECTOR
(VIEWED FROM ROADWAY)



DETAILS SHOWING PLACEMENT OF HOLES FOR INTERMEDIATE BRACKET
(VIEWED FROM ROADWAY)



TERMINAL CONNECTOR
(AASHTO DESIGNATION : M180, CLASS B, TYPE 2)

NOTES:

BRIDGE-GUARD RAIL CONNECTOR TO CONSIST OF ONE(1) 25' SECTION OF GUARD RAIL (FIELD CUT, DRILLED, AND GALVANIZED), FABRICATED BRACKETS AND PLATES, TERMINAL CONNECTOR, MOUNTING HARDWARE, AND TIMBER BLOCKOUT (COMPLETE IN PLACE).

2 BRIDGE-GUARD RAIL CONNECTORS REQ'D ON BRIDGES 24' WIDE, 4 REQ'D ON BRIDGES 26' OR WIDER.

PRIOR TO FABRICATION, ALL DIMENSIONS FOR MOUNTING BRACKETS, CONNECTORS, BOLT LENGTHS AND GUARD RAIL SHALL BE DETERMINED IN THE FIELD BY THE CONTRACTOR. DIMENSION SHOWN MAY VARY FROM ACTUAL FIELD CONDITIONS AT EACH SITE.

PAY ITEM NO. 907-606-PP002 GUARD RAIL BRIDGE CONNECTOR, PER PLANS (EACH).

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
REVISION		SPECIAL DESIGN	
DATE		BRIDGE-GUARD RAIL CONNECTOR	
FILENAME:		guard/bgrc1.dgn	
DESIGN TEAM		CHECKED	
DATE		DATE	

WORKING NUMBER
BGRC-1
SHEET NUMBER

Notice To Bidders No. 364-1-20

General Decision Number: MS100223 01/21/2011 MS223

Superseded General Decision Number: MS20080223

State: Mississippi

Construction Type: Highway

Counties: Copiah, Hinds and Rankin Counties in Mississippi.

HIGHWAY CONSTRUCTION PROJECTS (excluding tunnels, building structures in rest area projects & railroad construction; bascule, suspension & spandrel arch bridges designed for commercial navigation, bridges involving marine construction; and other major bridges).

Modification Number	Publication Date
0	03/12/2010
1	07/02/2010
2	01/21/2011

* ELEC0480-007 01/01/2011

	Rates	Fringes
ELECTRICIAN.....	\$ 23.10	8.12

SUMS2008-141 09/04/2008

	Rates	Fringes
CEMENT MASON/CONCRETE FINISHER...	\$ 12.85	0.39
LABORER: Common or General.....	\$ 8.25	0.00
LABORER: Pipelayer.....	\$ 10.17	0.00
OPERATOR: Backhoe.....	\$ 13.38	0.00
OPERATOR: Broom.....	\$ 8.00	0.00
OPERATOR: Bulldozer.....	\$ 9.00	0.00
OPERATOR: Grader/Blade.....	\$ 11.67	0.00
OPERATOR: Mechanic.....	\$ 13.00	0.00
OPERATOR: Piledriver.....	\$ 12.50	1.23
OPERATOR: Roller.....	\$ 10.00	0.00
OPERATOR: Scraper.....	\$ 10.00	0.00
TRUCK DRIVER.....	\$ 10.00	0.00

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

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Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29 CFR 5.5(a)(1)(ii)).

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In the listing above, the "SU" designation means that rates listed under the identifier do not reflect collectively bargained wage and fringe benefit rates. Other designations indicate unions whose rates have been determined to be prevailing.

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WAGE DETERMINATION APPEALS PROCESS

1.) Has there been an initial decision in the matter? This can be:

- * an existing published wage determination
- * a survey underlying a wage determination
- * a Wage and Hour Division letter setting forth a position on a wage determination matter
- * a conformance (additional classification and rate) ruling

On survey related matters, initial contact, including requests for summaries of surveys, should be with the Wage and Hour Regional Office for the area in which the survey was conducted because those Regional Offices have responsibility for the Davis-Bacon survey program. If the response from this initial contact is not satisfactory, then the process described in 2.) and 3.) should be followed.

With regard to any other matter not yet ripe for the formal process described here, initial contact should be with the Branch of Construction Wage Determinations. Write to:

Branch of Construction Wage Determinations
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

2.) If the answer to the question in 1.) is yes, then an

interested party
(those affected by the action) can request review and
reconsideration from
the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR
Part 7).
Write to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the
interested
party's position and by any information (wage payment data,
project
description, area practice material, etc.) that the requestor
considers
relevant to the issue.

3.) If the decision of the Administrator is not favorable, an
interested
party may appeal directly to the Administrative Review Board
(formerly the
Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

4.) All decisions by the Administrative Review Board are final.

END OF GENERAL DECISION

Mill and Overlay SR 18 from Brandon to Rock Hill, known as Federal Aid Project No. STP-0039-02(046) / 105895301, in the County of Rankin, State of Mississippi.

I (We) agree to complete the entire project within the specified contract time.

***** SPECIAL NOTICE TO BIDDERS *****

**BIDS WILL NOT BE CONSIDERED UNLESS BOTH UNIT PRICES AND ITEM TOTALS ARE ENTERED.
BIDS WILL NOT BE CONSIDERED UNLESS THE BID CERTIFICATION LOCATED AT THE END OF THE BID SHEETS IS SIGNED**

*****BID SCHEDULE*****

Line No.	Item Code	Adj Code	Quantity	Units	Description	Unit Price		Item Amount	
						Dollar	Ct	Dollar	Ct
Roadway Items									
0010	202-B005		250	Square Yard	Removal of Asphalt Pavement, All Depths				
0020	202-B053		750	Linear Feet	Removal of Guard Rail Including Post, Blockouts & Hardware				
0030	202-B076		2,000	Linear Feet	Removal of Traffic Stripe				
0040	406-A001		104,275	Square Yard	Cold Milling of Bituminous Pavement, All Depths				
0050	423-A001		15	Mile	Rumble Strips, Ground In				
0060	606-B007		750	Linear Feet	Guard Rail, Class A, Type 1, 'W' Beam, Metal Post				
0070	606-C002 Deleted 02/11/2011					XXXXXXXXXX	XXX	XXXXXXXXXX	XXX
0080	606-D001		3	Each	Guard Rail, Bridge End Section, Type A				

Line No.	Item Code	Adj Code	Quantity	Units	Description	Unit Price		Bid Amount	
0090	606-D004		3	Each	Guard Rail, Bridge End Section, Type E				
0095	606-E003 Added 02/11/2011		6	Each	Guard Rail, Terminal End Section, Non-Flared				
0100	609-E001		200	Linear Feet	Bituminous Curb				
0110	618-A001		1	Lump Sum	Maintenance of Traffic	XXXXXXXX	XXX		
0120	619-A1002		15	Mile	Temporary Traffic Stripe, Continuous White				
0130	619-A2002		5	Mile	Temporary Traffic Stripe, Continuous Yellow				
0140	619-A4006		8	Mile	Temporary Traffic Stripe, Skip Yellow				
0150	619-A5001		2,000	Linear Feet	Temporary Traffic Stripe, Detail				
0160	619-A6001		2,000	Linear Feet	Temporary Traffic Stripe, Legend				
0170	619-C7001		980	Each	Two-Way Yellow Reflective High Performance Raised Marker				
0180	619-D1001		321	Square Feet	Standard Roadside Construction Signs, Less than 10 Square Feet				
0190	619-D2001		228	Square Feet	Standard Roadside Construction Signs, 10 Square Feet or More				

Line No.	Item Code	Adj Code	Quantity	Units	Description	Unit Price		Bid Amount	
0200	619-G4005		24	Linear Feet	Barricades, Type III, Double Faced				
0210	627-J001		264	Each	Two-Way Clear Reflective High Performance Raised Markers				
0220	627-L001		980	Each	Two-Way Yellow Reflective High Performance Raised Markers				
0230	630-F001		33	Each	Delineators, Guard Rail, White				
0240	630-G002		67	Each	Type 3 Object Markers, OM-3R or OM-3L, Post Mounted				
0250	907-304-A001	(GY)	3,000	Cubic Yard	Granular Material, LVM, Class 5, Group C				
0260	907-407-A001	(A2)	12,640	Gallon	Asphalt for Tack Coat				
0265	907-606-PP002		6	Each	Guard Rail, Bridge Connector, Per Plans				
	Added 02/11/2011								
0270	907-626-B005		1,000	Linear Feet	6" Thermoplastic Double Drop Traffic Stripe, Continuous White				
0280	907-626-C003		15	Mile	6" Thermoplastic Double Drop Edge Stripe, Continuous White				
0290	907-626-D006		8	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip Yellow				
0300	907-626-E005		22,680	Linear Feet	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow				

Line No.	Item Code	Adj Code	Quantity	Units	Description	Unit Price		Bid Amount	
0310	907-626-G006		1,500	Linear Feet	Thermoplastic Double Drop Detail Stripe, White				
0320	907-626-H009		2,000	Linear Feet	Thermoplastic Double Drop Legend, White				
ALTERNATE GROUP AA NUMBER 1									
0330	907-403-A006 (BA1)		19,180	Ton	Hot Mix Asphalt, MT, 12.5-mm mixture				
0340	907-403-B004 (BA1)		300	Ton	Hot Mix Asphalt, MT, 12.5-mm mixture, Leveling				
ALTERNATE GROUP AA NUMBER 2									
0350	907-403-M002 (BA1)		19,180	Ton	Warm Mix Asphalt, MT, 12.5-mm mixture				
0360	907-403-N006 (BA1)		300	Ton	Warm Mix Asphalt, MT, 12.5-mm mixture, Leveling				

*** BID CERTIFICATION ***

TOTAL BID.....\$_____

*** DBE/WBE SECTION ***

Complete item nos. 1, 2, and/or 3 as appropriate. See Notice to Bidders addressing Disadvantaged Business Enterprises in Highway Construction.

- 1. I/We agree that no less than _____ percent shall be expended with small business concerns owned and controlled by socially and economically disadvantaged individuals (DBE and WBE).
- 2. Classification of Bidder: Small Business (DBE)_____ Small Business (WBE)_____
- 3. A joint venture with a Small Business (DBE/WBE): _____

*** SIGNATURE STATEMENT ***

BIDDER ACKNOWLEDGES THAT HE/SHE HAS CHECKED ALL ITEMS IN THIS PROPOSAL FOR ACCURACY AND CERTIFIED THAT THE FIGURES SHOWN THEREIN CONSTITUTE THEIR OFFICIAL BID.

BIDDER'S SIGNATURE

BIDDER'S COMPANY

BIDDER'S FEDERAL TAX ID NUMBER