

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) enclose a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. 1 DATED 6/20/2011 ADDENDUM NO. DATED
 ADDENDUM NO. DATED ADDENDUM NO. DATED

Number	Description
1	Revised NTB No. 3527, replaces same; Amendment EBS Download Required.

TOTAL ADDENDA: 1
 (Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

 Contractor

BY _____
 Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.

Revised 09/21/2005

NH-0009-03(050) / 106189301

Bolivar County(ies)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 3527

CODE: (SP)

DATE: 06/20/2011

SUBJECT: Scope of Work

PROJECT: NH-0009-03(050) / 106189301 – Bolivar County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings.” All other references to plans in the contract documents and Standard Specification for Road and Bridge Construction are to be disregarded.

Highway 61
Overlay from 0.1 mile South of the intersection of Highway 61/Highway 446 to end approximately at Clark Street on the Southern End of Cleveland

In general, the work to be accomplished using the pay items and corresponding specifications set forth in this contract is to rehabilitate approximately 2.42 miles of Highway 61 in Bolivar County beginning at Log Mile 13.500 and ending at Log Mile 15.920 at or near Clark Street.

Two areas require specific procedures in the project limits. The outside lanes both Northbound and Southbound will have details for its specific nature of work. Also, the curb and gutter within the median and material within will have details for its specific nature of work.

- 1) The Contractor shall erect and maintain construction signing, and provide all signs and traffic handling devices in accordance with the Traffic Control Plan (the cost is to be included in the price bid for pay item No. 618-A, Maintenance of Traffic). All traffic control devices on this project should comply with Part VI of the MUTCD (Latest Edition). Fluorescent orange sheeting shall be used on all construction and traffic control signs except for those designated in plans to be black legend and border on white background. Cones shall be narrow profile with a minimum height of 28 inches and a minimum weight of ten (10) pounds. Cones used in speed zones equal to or greater than 45 miles per hour shall be narrow profile with a minimum height of 28 inches and a minimum weight of fifteen (15) pounds. All cones shall be approved by the Engineer prior to use.

First Sequence

For the purpose of this project, work known as the Outside lanes will consist of work being done both Northbound and Southbound in the outermost lane of Highway 61 which is adjacent to the 10-foot shoulder on one side and the median lane on the other side. The termini for this work will be from the Beginning of Project (BOP) to the End of Project (EOP).

- 1) Work will be done in a manner to maintain traffic flow at all times. This is to be paid for in the maintenance of traffic pay item.
- 2) Cold Mill the roadway at the B.O.P., E.O.P, local road curb and gutter sections and other areas as designated by the Project Engineer to ensure smooth transitions of new overlay with existing grade.
- 3) Traffic will not be allowed to go across a drop off of more than 1½ inches at any time.
- 4) All asphalt laid for this portion of the work will be required to have PG 76-22 polymer modified asphalt cement.
- 5) The Contractor will mill 5½ inches of asphalt in each lane for a width not to exceed 14 feet. The Contractor will place four (4) inches of 19-mm Asphalt, HT, Polymer Modified, in the same lane closure while the milling operation is taking place. 92% density will be required on this asphalt with no exceptions for low densities. This operation will put the asphalt 1½ inches lower than the existing asphalt on both adjacent sides. During the milling operation the Contractor will encounter petro mat ranging in depth from 3½ inches to five (5) inches and variable. Traffic will not be allowed on milled surfaces in this area.
- 6) In the event a breakdown occurs in the operation, the Contractor shall have means to fill any voids created with crushed limestone or stockpiled milled material. This shall be used to maintain traffic until operations can continue. Then the material will be cleaned out of the void and construction will continue. This will be done at no additional cost to the state.
- 7) All work in this area will be done at night. Milling may commence at 10:00 P.M. and all paving operations must be complete and out of the Highway by 6:00 A.M. the following morning. During the time periods listed above, no excuses will be accepted by the MDOT, and the Contractor will be charged a fee of **\$300.00** per lane for each full or partial five (5) minute period during which a closure of any type is in the urban area.
- 8) The milled material in the Northbound lane will be retained by the MDOT. The Contractor will be responsible for hauling and delivering this material to the Cleveland Maintenance Barn.
- 9) The final 1½ inch lift in this area can be done during daylight hours. This lift will be 1½ inches of 12.5-mm Asphalt, HT, polymer modified.
- 10) This portion of work on this project must be complete before the Contractor starts the next sequence of work.

Second Sequence

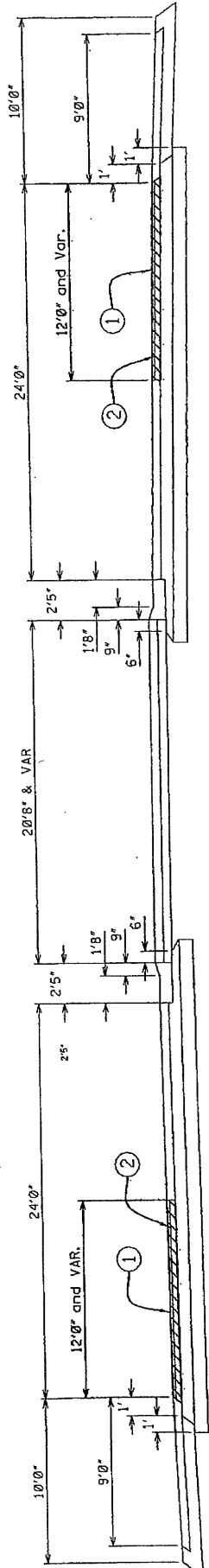
- 1) The Contractor will remove material inside the curb and gutter to a depth of 12 inches or to the old existing concrete roadbed, whichever comes first. Removal of this material will be paid for as Excess Excavation (see 907-203-G003). All signs in the median will be removed at this time and remain the property of the MDOT maintenance division. The contractor will deliver these signs to the MDOT Cleveland Maintenance Barn. All embedded sign post will be cut off flush with the final removal grade. All work accompanied with removal of signs will not be measure for separate pay and should be absorbed in other pay items in the project.
- 2) The Contractor will then remove all curb and gutter in the median for the length of the limits of the project.

- 3) The Contractor will backfill and compact ¾ inch and down crushed stone in all voids left by the removal of this section to within 4 inches of the existing adjacent asphalt grade.
- 4) The contractor will place 4 inches of 19 mm Hot Mix Asphalt, HT, Polymer Modified on the crushed stone base to be level with the existing asphalt. This asphalt will require 92% densities with no exceptions for low densities.
- 5) The contractor will place a wedge of 12.5 mm asphalt from the center of the Highway outward 16 foot and variable to correct any slope imperfections created by the first and second construction sequences as directed by the engineer. This wedge is to be used solely to provide mix necessary to create a new 2% slope. The wedge asphalt will be considered an irregular area and have to be rolled to refusal. The contractor shall provide nuclear gauge readings to the project engineer to ensure the asphalt is being rolled to refusal. There will be at least one rubber tired roller in the paving train.
- 6) This portion of work on this project must be complete before the contractor starts the next sequence of work.

Third Sequence

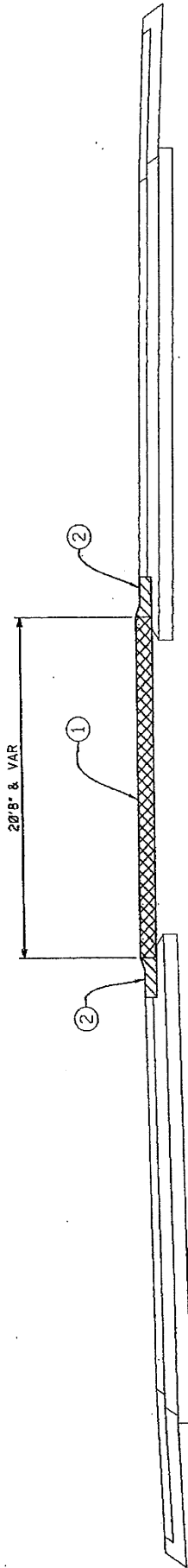
The Contractor shall place two (2) 1½-inch lifts of asphalt on the mainline roadway having a two percent (2%) cross slope or the appropriate super elevation rate in each direction from the centerline. The first lift will be 1½-inch & variable, 12.5-mm, HT, Polymer Modified asphalt mixture and the second lift (surface) will be 1½-inch, 9.5-mm, HT, Polymer Modified asphalt mixture. The asphalt shall be placed in passes and in widths as necessary not to have any longitudinal joint fall in a wheel path. The asphalt shall be placed full width, Ninety-three feet (93) as to create a new 5-lane section with 10-foot shoulders on each side. Place a 1½-inch lift of 9.5-mm, HT, Polymer Modified, asphalt mixture on the intersections, local roads, streets and driveways (as applicable). Local roads are to be paved to the right of way or as directed by the Project Engineer. Aprons shall be constructed at existing ramps that do not have paved aprons by placing three (3) inches of 9.5-mm, HT, Polymer Modified, asphalt mixture, in widths and lengths as directed by the Project Engineer. Existing aprons are to be paved to match final main line grades. Granular material will be place on the shoulders as directed by the Engineer.

The Contractor shall place temporary striping in accordance with Subsection 619.03.2 of the 2004 standard specifications. Temporary striping shall conform to finished stripe specifications for alignment, reflectivity, straightness, and neatness. Temporary striping shall be placed as needed in **every sequence of work throughout the project.** Temporary stripe shall be placed daily as needed for safe movement of traffic. All permanent pavement markings are to be hot thermoplastic. All thermoplastic striping on this project must be placed using an extrusion head. Glass beads applied to thermoplastic shall conform to Subsection 720.01. Raised pavement markers will be placed as per standard drawing for 5-lane sections of roadway in urban areas.

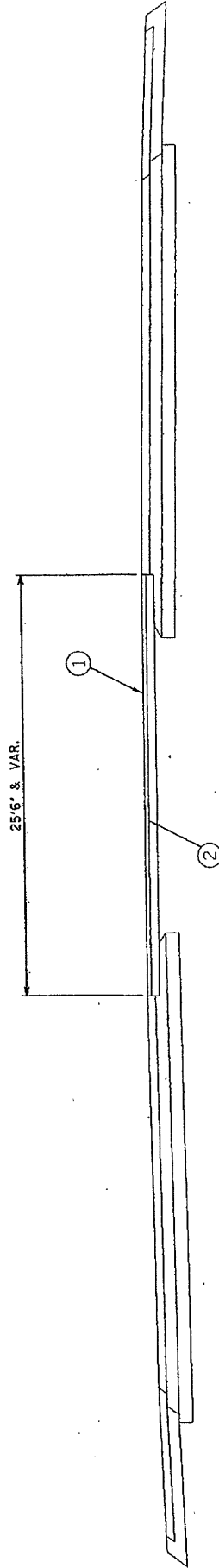


ITEMS ① AND ② TO BE DONE UNDER THE SAME LANE CLOSURE.

- ① REMOVE BY MILLING 5 1/2" AND VARIABLE.
- ② 4" ASPHALT, HT, POLYMER MODIFIED (19 MM MIXTURE)(@ 4")
- ③ 1.5" ASPHALT, HT, POLYMER MODIFIED (12.5 MM MIXTURE)(@ 1.5")

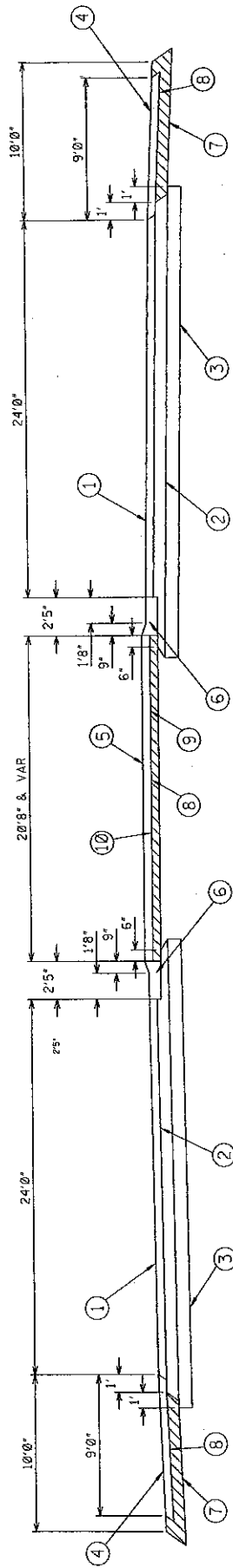


- ① REMOVE MATERIAL TO A DEPTH OF 12" OR UNTIL CONCRETE IS ENCOUNTERED
- ② REMOVE CURB AND GUTTER



- ① 4" ASPHALT, HT, POLYMER MODIFIED (19 MM MIXTURE) @ 4"
- ② 8" AND VAR. 3/4" AND DOWN CRUSHED LIMESTONE (MAXIMUM LIFT THICKNESS OF 6")

FOR INFORMATION ONLY



- ① 4" MIN. HOT BITUMINOUS PAVEMENT REQ'D
1-1" MIN (SURFACE)
2-1.5" MIN (BINDER)
- ② 7" PLANT MIX BITUMINOUS BASE REQ'D
- ③ 8" SOIL-LIME-WATER MIXING REQ'D CLASS B
- ④ 3" HOT BITUMINOUS PAV'T REQ'D, 2 BINDER COURSES @ 1.5" D.B.S.T. REQ'D
- ⑤ COMBINATION CONCRETE CURB AND GUTTER REQ'D SPECIAL DESIGN
- ⑥ 7" & VARIABLE GRANULAR MATERIAL REQ'D CLASS "3", GROUP "D"
- ⑦ BITUMINOUS PRIME REQ'D (DELETED BY PE)
- ⑧ 6" GRANULAR MATERIAL REQ'D CLASS "3", GROUP "D"
- ⑨ 4" PLANT MIX BITUMINOUS BASE REQ'D