



SM No. CSTP2170000231

# PROPOSAL AND CONTRACT DOCUMENTS

FOR THE CONSTRUCTION OF  
(STATE DELEGATED)

5

Mill and Overlay approximately 5 miles of US 11 from the Lamar County Line to Lincoln Road, known as Federal Aid Project No. STP-2170-00(023) / 106222301, in Forrest County.

Project Completion: November 30, 2011

## NOTICE

**BIDDERS MUST PURCHASE A BOUND PROPOSAL FROM MDOT CONTRACT ADMINISTRATION DIVISION TO BID THIS PROJECT.**

Electronic addendum updates will be posted on [www.gomdot.com](http://www.gomdot.com)

**SECTION 900**  
**OF THE CURRENT**  
**(2004) STANDARD SPECIFICATIONS**  
**FOR ROAD AND BRIDGE CONSTRUCTION**  
**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**  
**JACKSON, MISSISSIPPI**

**BIDDER CHECK LIST  
(FOR INFORMATION ONLY)**

- \_\_\_\_\_ All unit prices and item totals have been entered in accordance with Subsection 102.06 of the Mississippi Standard Specifications for Road and Bridge Construction.
- \_\_\_\_\_ If the bid sheets were prepared using the Electronic Bid System, proposal sheets have been stapled and inserted into the proposal package.
- \_\_\_\_\_ First sheet of SECTION 905--PROPOSAL has been completed.
- \_\_\_\_\_ Second sheet of SECTION 905--PROPOSAL has been completed and signed.
- \_\_\_\_\_ Addenda, if any, have been acknowledged. Second sheet of Section 905 listing the addendum number has been substituted for the original second sheet of Section 905. Substituted second sheet of Section 905 has been properly completed, signed, and added to the proposal.
- \_\_\_\_\_ DBE/WBE percentage, when required by contract, has been entered on last sheet of the bid sheets of SECTION 905 - PROPOSAL.
- \_\_\_\_\_ Form OCR-485, when required by contract, has been completed and signed.
- \_\_\_\_\_ The last sheet of the bid sheets of SECTION 905--PROPOSAL has been signed.
- \_\_\_\_\_ Combination Bid Proposal of SECTION 905--PROPOSAL has been completed for each project which is to be considered in combination (See Subsection 102.11).
- \_\_\_\_\_ Equal Opportunity Clause Certification, when included in contract, has been completed and signed.
- \_\_\_\_\_ The Certification regarding Non-Collusion, Debarment and Suspension, etc. has been executed in duplicate.
- \_\_\_\_\_ A certified check, cashier's check or bid bond payable to the State of Mississippi in the principal amount of 5% of the bid has been included with project number identified on same. A bid bond has been signed by the bidder and has also been signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent for the Surety with Power of Attorney attached.
- \_\_\_\_\_ **ON FEDERAL FUNDED PROJECTS, the Notice To Bidders regarding DUNS Requirements has been completed and included in the contract documents.**
- \_\_\_\_\_ Non-resident Bidders: ON STATE FUNDED PROJECTS ONLY, a copy of the current laws regarding any preference for local Contractors from State wherein domiciled has been included. See Subsection 103.01, Mississippi Standard Specifications for Road and Bridge Construction, and Section 31-7-47, MCA, 1972 regarding this matter.

Return the proposal and contract documents in its entirety in a sealed envelope. **DO NOT** remove any part of the contract documents; exception - an addendum requires substitution of second sheet of Section 905. A stripped proposal is considered as an irregular bid and will be rejected.

Failure to complete any or all of the applicable requirements will be cause for the proposal to be considered irregular.



# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

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(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET  
OF SECTION 905 AS ADDENDA)

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

## SECTION 901 - ADVERTISEMENT

Sealed bids will be received by the Mississippi Transportation Commission in the Office of the Contract Administration Engineer, Room 1013, Mississippi Department of Transportation Administration Building, 401 North West Street, Jackson, Mississippi, until 10:00 o'clock A.M., Tuesday, July 26, 2011, and shortly thereafter publicly opened on the Sixth Floor for:

Mill and Overlay approximately 5 miles of US 11 from the Lamar County Line to Lincoln Road, known as Federal Aid Project No. STP-2170-00(023) / 106222301, in Forrest County.

The attention of bidders is directed to the Contract Provisions governing selection and employment of labor. Minimum wage rates have been predetermined by the Secretary of Labor and are subject to Public Law 87-58 1, Work Hours Act of 1962, as set forth in the Contract Provisions.

The Mississippi Department of Transportation hereby notifies all bidders that it will affirmatively insure that in any contract entered into pursuant to this advertisement, disadvantaged business enterprises will be afforded full opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of race, color, sex, age, disability, religion or national origin in consideration for an award.

**The award of this contract will be contingent upon the Contractor satisfying the DBE requirements.**

The specifications are on file in the offices of the Mississippi Department of Transportation.

Bid proposals must be acquired from the MDOT Contract Administration Division. These proposal are available at a cost of Ten Dollars (\$10.00) per proposal. Specimen proposals are also available at the MDOT Contract Administration Division at a cost of Ten Dollars (\$10.00) per proposal, or can be viewed or downloaded at no cost at [www.gomdot.com](http://www.gomdot.com).

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached or on file with the Contract Administration Engineer of the Department, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

MELINDA L. MCGRATH  
INTERIM EXECUTIVE DIRECTOR

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 - NOTICE TO BIDDERS NO. 1**

**CODE: (IS)**

**DATE: 05/03/2004**

**SUBJECT: Governing Specifications**

The current (2004) Edition of the Standard Specifications for Road and Bridge Construction adopted by the Mississippi Transportation Commission is made a part hereof fully and completely as if it were attached hereto, except where superseded by special provisions, or amended by revisions of the Specifications contained herein. Copies of the specification book may be purchased from the MDOT Construction Division.

A reference in any contract document to controlling requirements in another portion of the contract documents shall be understood to apply equally to any revision or amendment thereof included in the contract.

In the event the plans or proposal contain references to the 1990 Edition of the Standard Specifications for Road and Bridge Construction, it is to be understood that such references shall mean the comparable provisions of the 2004 Edition of the Standard Specifications.

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 - NOTICE TO BIDDERS NO. 3**

**CODE: (SP)**

**DATE: 05/03/2004**

**SUBJECT: Final Clean-Up**

Immediately prior to final inspection for release of maintenance, the Contractor shall pick up, load, transport and properly dispose of all litter from the entire highway right-of-way that is within the termini of the project.

Litter shall include, but not be limited to, solid wastes such as glass, paper products, tires, wood products, metal, synthetic materials and other miscellaneous debris.

Litter removal is considered incidental to other items of work and will not be measured for separate payment.

## **SUPPLEMENT TO NOTICE TO BIDDERS NO. 696**

**DATE: 11/06/2009**

The goal is 2 percent for the Disadvantaged Business Enterprise. The low bidder is required to submit Form OCR-481 for all DBEs. Bidders are advised to check the bid tabulation link for this project on the MDOT website (<http://www.gomdot.com/applications/bidsystem/currentletting.aspx>) for results. Bid tabulations are usually posted by 3:00 pm on Letting Day.

Form OCR-481 is available at [http://www.gomdot.com/Divisions/CivilRights/Resources/Forms/pdf/MDOT\\_OCR481.pdf](http://www.gomdot.com/Divisions/CivilRights/Resources/Forms/pdf/MDOT_OCR481.pdf) or by calling 601-359-7466.

Subparagraph (2) under Award on page 6 indicates that the OCR-481 form is to be submitted to Contract Administration Division. Instead of submitting this form to Contract Administration Division, all OCR-481s must be returned within 10 days following the bid letting to the MDOT Office of Civil Rights, P.O. Box 1850, Jackson, MS 39215-1850.

For answers to questions, contact the MDOT Office of Civil Rights at (601) 359-7466.

The bidder's execution of the signature portion of the proposal shall constitute execution of the following assurance:

The bidder hereby gives assurance pursuant to the applicable requirements of "Safe, Accountable, Flexible, Efficient Transportation Equity Act, A Legacy For Users (SAFETEA-LU)" and "Part 26, Title 49, Code of Federal Regulation" that the bidder has made a good faith effort to meet the contract goal for DBE participation for which this proposal is submitted.

A pre-bid meeting will be held in Amphitheater 1 & 2 of the Hilton Jackson located at I-55 and County Line Road, Jackson, Mississippi at 2:00 P.M. on the day preceding the date of the bid opening.

This meeting is to inform DBE firms of subcontracting and material supply opportunities. Attendance at this meeting is considered of prime importance in demonstrating good faith effort to meet the contract goal.

A list of "Certified DBE Contractors" which have been certified as such by the Mississippi Department of Transportation and other Unified Certification Partners (UPC) can be found on the Mississippi Department of Transportation website at [www.gomdot.com](http://www.gomdot.com). The DBE firm must be on the Department's list of "Certified DBE Contractors" that is posted online at the time the job is let and approved by MDOT to count towards meeting the DBE goal.

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 696

CODE: (IS)

DATE: 12/20/2005

**SUBJECT: DISADVANTAGED BUSINESS ENTERPRISES IN FEDERAL-AID HIGHWAY CONSTRUCTION**

This contract is subject to the [Safe, Accountable, Flexible, Efficient Transportation Equity Act, A Legacy For Users \(SAFETEA-LU\)](#) and applicable requirements of "Part 26, Title 49, Code of Federal Regulations." Portions of the Act are set forth in this Notice as applicable to compliance by the Contractor and all of the Act, and the MDOT DBE Program, is incorporated by reference herein.

The Department has developed a Disadvantaged Business Enterprise Program that is applicable to this contract and is made a part thereof by reference.

Copies of the program may be obtained from:

Office of Civil Rights  
Mississippi Department of Transportation  
P. O. Box 1850  
Jackson, Mississippi 39215-1850

## **POLICY**

It is the policy of the Mississippi Department of Transportation to provide a level playing field, to foster equal opportunity in all federally assisted contracts, to improve the flexibility of the DBE Program, to reduce the burdens on small businesses, and to achieve that amount of participation that would be obtained in a non-discriminatory market place. In doing so, it is the policy of MDOT that there will be no discrimination in the award and performance of federally assisted contracts on the basis of race, color, sex, age, religion, national origin, or any handicap.

## **ASSURANCES THAT CONTRACTORS MUST TAKE:**

MDOT will require that each contract which MDOT signs with a subrecipient or a Contractor, and each subcontract the Prime Contractor signs with a Subcontractor, includes the following assurances:

“The Contractor, subrecipient or Subcontractor shall not discriminate on the basis of race, color, national origin, or sex in the performance of this contract. The Contractor shall carry out applicable requirements of 49 CFR 26 in the award and administration of federally assisted contracts. Failure by the Contractor to carry out these requirements is a material breach of this contract, which may result in the termination of this contract or such other remedy as MDOT deems appropriate.”

**DEFINITIONS**

For purposes of this provision the following definitions will apply:

"Disadvantaged Business" means a small business concern: (a) which is at least 51 percent owned by one or more socially and economically disadvantaged individual(s) or in the case of any publicly owned business, at least 51 percent of the stock of which is owned by one or more socially and economically disadvantaged individual(s); and (b) whose management and daily business operations are controlled by one or more of the socially and economically disadvantaged individual(s) who own it. It is important to note that the business owners themselves must control the operations of the business. Absentee ownership or title ownership by an individual who does not take an active role in controlling the business is not consistent with eligibility as a DBE under CFR 49 Part 26.71.

**CONTRACTOR'S OBLIGATION**

The Contractor and all Subcontractors shall take all necessary and reasonable steps to ensure that DBE firms can compete for and participate in the performance of a portion of the work in this contract and shall not discriminate on the basis of race, color, national origin, religion or sex. Failure on the part of the Contractor to carry out the DBE requirements of this contract constitutes a breach of contract and after proper notification the Department may terminate the contract or take other appropriate action as determined by the Department.

When a contract requires a zero percent (0%) DBE goal, the Contractor still has the responsibility to take all necessary and reasonable steps to ensure that DBE firms can compete for and participate in the performance of the work in the contract. **In this case**, all work performed by a certified DBE firm is considered to be a "race neutral" measure and the Department will receive DBE credit towards the overall State goals when the DBE firm is paid for their work. If the Prime Contractor is a certified DBE firm, the Department can receive DBE credit only for the work performed by the Prime Contractor's work force or any work subcontracted to another DBE firm. Work performance by a non-DBE Subcontractor is not eligible for DBE credit.

**CONTRACT GOAL**

The goal for participation by DBEs is established for this contract in the attached Supplement. The Contractor shall exercise all necessary and reasonable steps to ensure that participation is equal to or exceeds the contract goal.

The percentage of the contract that is proposed for DBEs shall be so stated on the last bid sheet of the proposal.



The apparent lowest responsive bidder shall submit to the Contract Administration Division Form OCR-481, signed by the Prime Contractor and the DBE Subcontractors, no later than the 10th day after opening of the bids.

FORMS ARE AVAILABLE FROM THE CONTRACT ADMINISTRATION DIVISION

The OCR-481 Form must contain the following information:

The name and address of each certified DBE Contractor / Supplier;

The Reference Number, percent of work and the dollar amount of each item. If a portion of an item is subcontracted, a breakdown of that item including quantities and unit price must be attached, detailing what part of the item the DBE firm is to perform and who will perform the remainder of the item.

If the DBE Commitment shown on the last bid sheet of the proposal, does not equal or exceed the contract goal, the bidder must submit, with the proposal, information to satisfy the Department that adequate good faith efforts have been made to meet the contract goal.

Failure of the lowest bidder to furnish acceptable proof of good faith efforts, submitted with the bid proposal, shall be just cause for rejection of the proposal. Award may then be made to the next lowest responsive bidder or the work may be readvertised.

The following factors are illustrative of matters the Department will consider in judging whether or not the bidder has made adequate good faith effort to satisfy the contract goal.

- (1) Whether the bidder attended the pre-bid meeting that was scheduled by the Department to inform DBEs of subcontracting opportunities;
- (2) whether the bidder advertised in general circulation, trade association, and minority-focus media concerning the subcontracting opportunities;
- (3) whether the bidder provided written notice to a reasonable number of specific DBEs that their interest in the contract is being solicited;
- (4) whether the bidder followed up initial solicitations of interest by contacting DBEs to determine with certainty whether they were interested;
- (5) whether the bidder selected portions of the work to be performed by DBEs in order to increase the likelihood of meeting the contract goal;
- (6) whether the bidder provided interested DBEs with adequate information about the plans, specifications and requirements of the contract;

- (7) whether the bidder negotiated in good faith with interested DBEs and did not reject them as unqualified without sound reasons based on a thorough investigation of their capabilities; and
- (8) whether the bidder made efforts to assist interested DBEs in obtaining any required bonding or insurance.

### **DIRECTORY**

Included with this Bid Proposal is a list of "Certified DBE Contractors" which have been certified as such by the Mississippi Department of Transportation and other Unified Certification Partners (UCP).

The DBE firm must be on the Department's list of "Certified DBE Contractors" that is attached to this proposal and approved by MDOT to count towards meeting the DBE goal.

### **REPLACEMENT**

If a DBE Subcontractor cannot perform satisfactorily, and this causes the OCR-481 commitment to fall below the contract goal, the Contractor shall take all necessary reasonable steps to replace the DBE with another certified DBE Subcontractor or submit information to satisfy the Mississippi Department of Transportation that adequate good faith efforts have been made to replace the DBE. The replacement DBE must be a DBE who was on the Department's list of "Certified DBE Contractors" when the job was awarded, and who is still active. All DBE replacements must be approved by the Department.

Under no circumstances shall the Prime or any Subcontractor perform the DBE's work (as shown on the OCR-481) without prior written approval from the Department. See "Sanctions" at the end of this document for penalties for performing DBE's work.

When a Contractor proposes to substitute/replace/terminate a DBE that was originally named on the OCR-481, the Contractor must obtain a release, in writing, from the named DBE explaining why the DBE Subcontractor cannot perform the work. A copy of the original DBE's release must be attached to the Contractor's written request to substitute/replace/terminate along with appropriate Subcontract Forms for the substitute/replacement/terminated Subcontractor, all of which must be submitted to the DBE Coordinator and approved, in advance, by MDOT.

### **GOOD FAITH EFFORTS**

To demonstrate good faith efforts to replace any DBE that is unable to perform successfully, the Contractor must document steps taken to subcontract with another certified DBE Contractor. Such documentation shall include no less than the following:

- (a) Proof of written notification to certified DBE Contractors by certified mail that their interest is solicited in subcontracting the work defaulted by the previous DBE or in subcontracting other items of work in the contract.
- (b) Efforts to negotiate with certified DBE Contractors for specific items shall include as a minimum:
  - (1) The name, address, and telephone number of each DBE contacted;
  - (2) A description of the information provided about the plans and specifications for those portions of the work to be subcontracted; and
  - (3) A statement of why agreements were not reached.
- (c) For each DBE contacted that was rejected as unqualified, the reasons for such conclusion.
- (d) Efforts made to assist each DBE that needed assistance in obtaining bonding or insurance required by the Contractor.

Failure of the Contractor to demonstrate good faith efforts to replace a DBE Subcontractor that cannot perform as intended with another DBE Subcontractor, when required, shall be a breach of contract and may be just cause to be disqualified from further bidding for a period of up to 12 months after notification by certified mail.

**PARTICIPATION / DBE CREDIT**

Participation shall be counted toward meeting the goal in this contract as follows:

- (1) If the Prime Contractor is a certified DBE firm, only the value of the work actually performed by the DBE Prime can be counted towards the project goal, along with any work subcontracted to a certified DBE firm.
- (2) If the Contractor is not a DBE, the work subcontracted to a certified DBE Contractor will be counted toward the goal.
- (3) The Contractor may count toward the goal a portion of the total dollar value of a contract with a joint venture eligible under the standards of this provision equal to the percentage of the DBE partner in the joint venture.
- (4) Expenditures to DBEs that perform a commercially useful function may be counted toward the goal. A business is considered to perform a commercially useful function when it is responsible for the execution of a distinct element of the work and carries out its responsibilities by actually performing, managing, and supervising the work involved.

- (5) The Contractor may count 100% of the expenditures for materials and supplies obtained from certified DBE suppliers and manufacturers that produce goods from raw materials or substantially alters them for resale provided the suppliers and manufacturers assume the actual and contractual responsibility for the provision of the materials and supplies. The Contractor may count 60 percent of the expenditures to suppliers that are not manufacturers, provided the supplier performs a commercially useful function in the supply process. Within 30 days after receipt of the materials, the Contractor shall furnish to the DBE Coordinator invoices from the certified supplier to verify the DBE goal.
- (6) Any work that a certified DBE firm subcontracts or sub-subcontracts to a non-DBE firm will not count towards the DBE goal.
- (7) Only the dollars actually paid to the DBE firm may be counted towards the DBE goal.

### **AWARD**

Award of this contract to the low bidder will be contingent upon the following conditions:

- (1) Concurrence from Federal Highway Administration, when applicable.
- (2) Bidder must submit to the Contract Administration Division for approval, Form OCR-481 (DBE Commitment) no later than the 10th day after opening of the bids, or submit information with the bid proposal to satisfy the Department and that adequate good faith efforts have been made to meet the contract goal.
- (3) Bidder must submit **with the bid proposal** a list of all firms that submitted quotes for material supplies or items to be subcontracted. This information must be submitted on form OCR-485 in the back of the contract proposal.

Prior to the start of any work, the bidder must notify the Project Engineer, in writing, of the name of the designated "DBE Liaison Officer" for this project. This notification must be posted on the bulletin board at the project site.

### **DEFAULT**

The contract goal established by MDOT in this proposal must be met to fulfill the terms of the contract. The Contractor may list DBE Subcontractors and items that exceed MDOT's contract goal, but should unforeseen problems arise that would prevent a DBE from completing its total commitment percentage, the Contractor will meet the terms of the contract as long as it meets or exceeds MDOT's Contract Goal. For additional information, refer to "Replacement" section of this Notice.

## **DBE REPORTS**

- (1) OCR-481: Refer to 'CONTRACT GOAL' section of this Notice to Bidders for information regarding this form.
- (2) OCR-482: At the conclusion of the project the Contractor will submit to the Project Engineer for verification of quantities and further handling Form OCR-482 whereby the Contractor certifies to the amounts of payments made to each Contractor / Supplier. The Project Engineer shall submit the completed Form OCR-482 to the DBE Coordinator (Office of Civil Rights). Final acceptance of the project is dependent upon Contract Administration Division's receipt of completed Form OCR-482 which they will receive from the Office of Civil Rights.
- (3) OCR-483: The Project Engineer/Inspector will complete Form OCR-483, the Commercially Useful Function (CUF) Performance Report, in accordance with MDOT S.O.P. No. OCR-03-09-01-483. Evaluations reported on this form are used to determine whether or not the DBE firm is performing a CUF. The Prime Contractor should take corrective action when the report contains any negative evaluations. DBE credit may be disallowed and/or other sanctions imposed if it is determined the DBE firm is not performing a CUF. This form should also be completed and returned to the DBE Coordinator (Office of Civil Rights).
- (4) OCR-484: Each month, the Contractor will submit to the Project Engineer OCR-484 certifying payments to all Subcontractors.
- (5) OCR-485: The bidder must submit **with the bid proposal** a list of all firms that submitted quotes for material supplies or items to be subcontracted.
- (6) OCR-487: Only used by Prime Contractors that are certified DBE firms. This form is used in determining the exact percentage of DBE credit for the specified project. It should be returned to MDOT with the OCR-481 form, or can also be returned with the Permission to Subcontract Forms (CAD-720 or CAD-725).

## **SANCTIONS**

The Department has the option to enforce any of the following penalties for failure of the Prime Contractor to fulfill the DBE goal as stated on the OCR-481 form or any violations of the DBE program guidelines:

- (1) Disallow credit towards the DBE goal
- (2) Withhold progress estimate payments
- (3) Deduct from the final estimate an amount equal to the unmet portion of the DBE goal

- (4) Recover an amount equal to the unmet contract goal
- (5) Debar the Contractor involved from bidding on Mississippi Department of Transportation projects.
- (6) Deduct from the Contractor's final estimate all or any combination of the following.

<u>Offense</u>	<u>Percentage of the monetary amount disallowed from (1) above</u>	<u>Lump Sum</u>
# 1	10%	\$ 5,000 or both
# 2	20%	\$ 10,000 or both
# 3	40%	\$ 20,000 & debarment

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

| SECTION 904 - NOTICE TO BIDDERS NO. [777](#)

CODE: (IS)

| DATE: [04/13/2006](#)

**SUBJECT: On-The-Job Training Program**

| Payment for training hours will be handled as outlined in Special Provision 906-6. A pay item for trainees will not be included in individual construction projects. Payment for training individuals will be processed in accordance with the conditions in MDOT's ON-THE-JOB TRAINING PROGRAM (Special Provision 906-6).

| On Federal-Aid projects, failure on the part of the Contractor to carryout the terms of the Alternate Training Special Provision (Special Provision 906-6) will be considered grounds to preclude the Contractor from participating in the Alternate On-The-Job Training Program. In the event the Department is required to preclude the Contractor from participating in the program, the Contractor will be required to adhere to the requirements of the Training Special Provision (Special Provision 906-3), for which purpose the special provision is also made a part of this proposal.

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 - NOTICE TO BIDDERS NO. 883**

**CODE: (IS)**

**DATE: 04/28/2006**

**SUBJECT: Payroll Requirements**

Bidders are hereby advised that the Contractor and Subcontractor(s) are required to submit payroll information to the Project Engineers on a weekly basis.

On Federal-Aid Projects, CAD-880, CAD-881 and certified payroll submissions are required each week the Contractor or a Subcontractor performs work on the project. This is addressed in Section V, page 6 of Form FHWA-1273.

On State-Funded Projects, CAD-880 is required each week the Contractor or a Subcontractor performs work on the project.

When no work is performed on either Federal-Aid and State-Funded Projects, the Contractor should only submit CAD-880 showing no work activities.

The Contractor shall make all efforts necessary to submit this information to the Project Engineer in a timely manner. The Engineer will have the authority to suspend the work wholly or in part and to withhold payments because of the Contractor's failure to submit the required information. Submission of forms and payrolls shall be current through the first full week of the month for the estimate period in order for the Project Engineer to process an estimate.

Bidders are advised to review the requirements regarding payroll submissions in Section 110 of the Standard Specifications.



**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 1312**

**CODE: (SP)**

**DATE: 01/22/2007**

**SUBJECT: Rumble Stripe**

Bidders are hereby advised that when edge lines are placed over rumble strips, the pavement marking stripe must be applied using the atomization method instead of extrusion / ribbon method. The thickness of the stripe will be 60-mils, unless otherwise noted in the plans/proposal or pay item description. To ensure the proper alignment of the rumble stripes, the Contractor will be required to place a layout line to be followed during installation of the edge lines over the rumble strips.

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1405

CODE: (IS)

DATE: 03/15/2007

SUBJECT: ERRATA AND MODIFICATIONS TO THE 2004 STANDARD SPECIFICATIONS

<u>Page</u>	<u>Subsection</u>	<u>Change</u>
101	201.01	In the second sentence of the first paragraph, change “salvable” to “salvageable”.
107	202.04	In the fourth sentence of the fourth paragraph, change “yard” to “feet”.
107	202.05	In the list of units measurements for 202-B, add “square foot”.
132	211.03.4	In the second sentence of the second paragraph, change “planted” to “plated”.
192	306.02.4	In the first line of the first paragraph, delete the word “be”.
200	307.03.7	In the fourth sentence of the second paragraph, change “lime-fly ash” to “treated”.
236	401.01	Change the header from “Section 403” to “Section 401”.
242	401.02.3.2	In the first sentence of the third full paragraph, add “1/8” in the blank before the inch mark.
250	401.02.6.3	In the second sentence of the first paragraph on page 250, change “rutting over ”” to “rutting over 1/8” ”.
253	401.02.6.4.2	In the paragraph preceding the table, change “91.0” to “89.0”.
259	401.03.1.4	In the first paragraph, change “92.0 percent” to “the specified percentage (92.0 or 93.0)”.
269	403.03.2	In the table at the top of page 269, change the PI requirement from “ = ” to “ ≤ ”.

- 278 404.04 In the second sentence, change the subsection from “401.04” to “403.04”.
- 283 409.02.2 Change “PG 64-22” to “PG 67-22”.
- 294 413.02 In the first sentence of the second paragraph, change “707.02.1.3” to “Subsection 707.02.1.3”.
- 340 511.04 In the second sentence of the second paragraph, change “412” to “512”.
- 349 601.03.3 In the first sentence, change “804.03.2” to “804.03.5”.
- 355 603.02 Change the subsection reference for Joint mortar from “707.03” to “714.11”.
- 369 604.04 In the first sentence, change “601.04” to “Subsection 601.04”.
- 427 619.04 Delete the second paragraph.
- 442 625.04 In the third paragraph, change “626.04” to “Subsection 626.04”.
- 444 626.03.1.2 Delete the third sentence of the first paragraph.
- 464 631.02 Change the subsection reference for Water from “714.01.0” to “714.01.1”.
- 570 682.03 Change the subsection number from “682-03” to “682.03”.
- 575 683.10.4 Change the subsection number from “683.10.4” to “683.04”.
- 575 683.10.5 Change the subsection number from “683.10.5” to “683.05”.
- 596 701.02 In the table under the column titled “Cementations material required”, change Class F, FA” to “Class F FA,”.
- 603 702.11 In the first sentence, change “702.12” to “Subsection 702.12”.
- 612 703.04.2 In the fifth paragraph, delete “Subsection 703.11 and”.
- 616 703.07.2 In the Percentage By Weight Passing Square Mesh Sieves table, change the No. 10 requirement for Class 7 material from “30 - 10” to “30 - 100”.

- 618 703.13.1 In the first sentence of the first paragraph, change “703.09” to “703.06”.
- 618 703.13.2 In the first sentence, change “703.09” to “703.06”.
- 671 712.06.2.2 In the first sentence, change “712.05.1” to “Subsection 712.05.1”.
- 689 714.11.2 In the first sentence, change “412” to “512”.
- 709 715.09.5 In the first sentence of the first paragraph, change “guage” to “gauge”.
- 717 717.02.3.4 In the top line of the tension table, change “1 1/2” to “1 1/8” and change “1 1/8” to “1 1/2”.
- 741 720.05.2.2 In the last sentence of this subsection, change “720.05.2.1” to “Subsection 720.05.2.1”.
- 827 803.03.2.3.7.5.2 In the first sentence of the second paragraph, change “803.03.5.4” to “803.03.2.3.4”.
- 833 803.03.2.6 In the first sentence, change “803.03.7” to “803.03.2.5”.
- 854 804.02.11 In the last sentence of the first paragraph, change “automatically” to “automatic”.
- 859 804.02.13.1.3 In the last sentence, change Subsection “804.02.12.1” to “804.02.12”.
- 879 804.03.19.3.2 In the first sentence of the third paragraph, change “listed on of Approved” to “listed on the Approved”.
- 879 804.03.19.3.2 In the last sentence of the last paragraph, change “804.03.19.3.1” to “Subsection 804.03.19.3.1”.
- 962 814.02.3 In the first sentence, change “710.03” to “Subsection 710.03”.
- 976 820.03.2.1 In the first sentence, change “803.02.6” to “803.03.1.7”.
- 976 820.03.2.2 In the first sentence, change “803.03.9.6” to “803.03.1.9.2”.
- 985 Index Change the subsection reference for Petroleum Asphalt Cement from “702.5” to “702.05”.

985	Index	Change the subsection reference for the Definition of Asphaltic Cement or Petroleum Asphalt from “700.2” to “700.02”.
985	Index	Change the subsection reference for Automatic Batchers from “501.03.2.4” to “804.02.10.4”.
986	Index	Delete “501.03.2” as a subsection reference for Batching Plant & Equipment.
988	Index	Change the subsection reference for the Central Mixed Concrete from “501.03.3.2” to “804.02.11”.
988	Index	Change the subsection reference for the Concrete Batching Plant & Equipment from “501.03.2” to “804.02.11”.
999	Index	Delete “501.03.3.3” as a subsection reference for Truck Mixers.
1001	Index	Change the subsection reference for Edge Drain Pipes from “605.3.5” to “605.03.5”.
1002	Index	Change the subsection reference for Metal Posts from “713.05.2” to “712.05.2”.
1007	Index	Change the subsection reference for Coarse Aggregate of Cement Concrete Table from “703.3” to “703.03”.
1007	Index	Change the subsection reference for Composite Gradation for Mechanically Stabilized Courses Table from “703.8” to “703.08”.
1009	Index	Delete “501.03.3.3” as a subsection reference for Truck Mixers and Truck Agitators.
1010	Index	Delete reference to “Working Day, Definition of”.

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1808

CODE: (IS)

DATE: 09/09/2008

SUBJECT: Safety Apparel

Bidders are advised that the Code of Federal Regulations CFR 23 Part 634 final rule was adopted November 24, 2006 with an effective date of November 24, 2008. This rule requires that **"All workers within the right-of-way of a Federal-Aid Highway who are exposed either to traffic (vehicles using the highway for the purposes of travel) or to construction equipment within the work area shall wear high-visibility safety apparel"**. High-visibility safety apparel is defined in the CFR as **"personnel protective safety clothing that is intended to provide conspicuity during both daytime and nighttime usage, and that meets the Performance Class 2 or 3 requirements of the ANSI/ISEA 107-2004 publication entitled American National Standard for High-Visibility Safety Apparel and Headwear"**. All workers on Mississippi State Highway right-of-way shall comply with this Federal Regulation. Workers are defined by the CFR as **"people on foot whose duties place them within the right-of way of a Federal-Aid Highway, such as highway construction and maintenance forces, survey crews, utility crews, responders to incidents within the highway right-of-way, and law enforcement personnel when directing traffic, investigating crashes, and handling lane closures, obstructed roadways, and disasters within the right-of-way of a Federal-Aid Highway"**.

You can access this final rule at the following link:

<http://a257.g.akamaitech.net/7/257/2422/01jan20061800/edocket.access.gpo.gov/2006/pdf/E6-19910.pdf>

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

| SECTION 904 - NOTICE TO BIDDERS NO. 1928

CODE: (IS)

| DATE: 04/14/2008

SUBJECT: Federal Bridge Formula

Bidders are hereby advised that Federal Highway Administration Publication No. FHWA-MC-94-007, **BRIDGE FORMULA WEIGHTS**, dated January 1994, is made a part of this contract when applicable.

Prior to the preconstruction conference, the Contractor shall advise the Engineer, in writing, what materials, if any, will be delivered to the jobsite via Interstate route(s).

Copies of the **BRIDGE FORMULA WEIGHTS** publication may be obtained by contacting:

Federal Highway Administration  
400 7<sup>th</sup> Street, SW  
Washington, DC 20590  
(202) 366-2212

or

| [http://ops.fhwa.dot.gov/freight/sw/brdgcalf/calcul\\_page.htm](http://ops.fhwa.dot.gov/freight/sw/brdgcalf/calcul_page.htm)

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 - NOTICE TO BIDDERS NO. 2239**

**CODE: (SP)**

**DATE: 01/06/2009**

**SUBJECT: Department of Labor Ruling**

On December 19, 2008 the U.S. Department of Labor issued a final rule revising their regulations in 29 CFR Parts 3 and 5. This rule takes effect for all Federal funded contracts awarded after January 19, 2009.

The primary change in the rule is a provision that requires Contractors to limit the amount of personal information on the weekly payroll submissions. Personal addresses and full social security numbers may no longer be used. Contractors must use an ". . . individually identifying number for each employee (e.g., the last four digits of the employee's social security number)." Form FHWA-1273 - "Required Contract Provisions Federal-aid Construction Contracts" will eventually be revised to reflect this change.

Until the revised is made to FHWA-1273, bidders are advised to disregard any requirement in FHWA-1273 regarding the use of personal addresses and full social security numbers, such as in Section V, Paragraph 2b.

Bidders are also advised that the requirement for maintaining and submitting form FHWA-47, as referenced in FHWA-1273 Section VI, is no longer required on construction projects.



## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

| SECTION 904 - NOTICE TO BIDDERS NO. [2382](#)

CODE: (IS)

| DATE: [02/12/2009](#)

| SUBJECT: **Status of Right-of-Way**

Although it is desirable to have acquired all rights-of-way and completed all utility adjustments and work to be performed by others prior to receiving bids, sometimes it is not considered to be in the public interest to wait until each and every such clearance has been obtained. The bidder is hereby advised of possible unacquired rights-of-way, relocatees and utilities which have not been completed.

| The status of right-of-way acquisition, utility adjustments, [encroachments](#), potentially contaminated sites [and asbestos containment](#) are set forth in [the following](#) attachments.

In the event right of entry is not available to ALL parcels of right-of-way and/or all work that is to be accomplished by others on the date set forth in the contract for the Notice to Proceed is not complete, the Department will issue a restricted Notice to Proceed.

**STATUS OF RIGHT-OF-WAY**  
**STP-2170-00(023)**  
**106222-301000**  
**US 11 FROM I-59 TO LINCOLN ROAD**  
**FORREST COUNTY**  
**May 12, 2011**

All rights of way and legal rights of entry have been acquired except:

**NONE.**

STATUS OF POTENTIALLY CONTAMINATED SITES

STP-2170-00(023)

106222-301000

Forrest

May 12, 2011

THERE IS NO RIGHT OF WAY REQUIRED FOR THIS PROJECT. NO INITIAL SITE ASSESSMENT WILL BE PERFORMED. IF CONTAMINATION ON EXISTING RIGHT OF WAY IS DISCOVERED, IT WILL BE HANDLED BY THE DEPARTMENT.

ASBESTOS CONTAMINATION STATUS OF BUILDINGS  
TO BE REMOVED BY THE CONTRACTOR  
STP-2170-00(023)  
106222-301000  
Forrest County  
May 12, 2011

Reference is made to notices to bidders entitled "Asbestos National Emission Standards for Hazardous Air Pollutants (NESHAP)" and "Removal of Obstructions".

The following pertinent information is furnished concerning asbestos containing materials (ACMs), if any, found in buildings to be removed by the Contractor.

There is no Right of Way required for this project. There are no buildings to be removed by the contractor.

**UTILITY STATUS REPORT**

STP-2170-00(023)

106222301

FORREST COUNTY(IES)

May 12, 2011

This is to certify that the above captioned project has been inspected and there are no known utilities in conflict with the project.

**ENCROACHMENT CERTIFICATION**

STP-2170-00(023)

106222301

FORREST COUNTY(IES)

May 12, 2011

This is to certify that the above captioned project has been inspected and no encroachments were found.

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2596

CODE: (IS)

DATE: 05/13/2009

SUBJECT: DBE Forms, Participation and Payment

Bidders are hereby advised that the participation of a DBE Firm can not be counted towards the Prime Contractor's DBE goal until the amount being counted towards the goal has been paid to the DBE.

Form OCR-482 has been developed to comply with this requirement. Bidders are hereby advised that at the end of the job, the Prime Contractor will submit this form to the Project Engineer before the final estimate is paid and the project is closed out. This form certifies payments to all DBE Subcontractors over the life of the contract.

Form OCR-484 has also been developed to comply with this requirement. Bidders are hereby advised that each month, the Prime Contractors will submit this form to the Project Engineer no later than the last day of each month. This form certifies payments to all Subcontractors and shows all firms even if the Prime Contractor has paid no monies to the firm during that estimate period (negative report). The Project Engineer will attach this form to the monthly estimate before forwarding the estimate to the Contract Administration Division for processing.

Bidders are also advised that Form OCR-485 will be completed by ALL BIDDERS submitting a bid proposal and must be signed and included in the bid proposal package. Failure to include Form OCR-485 in the bid proposal package will cause the Contractor's bid to be considered irregular.

DBE Forms, including Forms OCR-482, OCR-484 and OCR-485, can be obtained from the Office of Civil Rights Division, MDOT Administration Building, 401 North West Street, Jackson, MS, or at [www.gomdot.com](http://www.gomdot.com) under *Business, Disadvantaged Enterprise, Applications and Forms for the DBE Program, MDOT Forms*.

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 - NOTICE TO BIDDERS NO. 2858**

**CODE: (SP)**

**DATE: 11/12/2009**

**SUBJECT: Petroleum Products Base Prices**

Bidders are advised that the Notice To Bidders entitled “Monthly Petroleum Products Base Prices” previously included in the proposal documents will no longer be a printed part of the proposal beginning with the January 2010 letting. Monthly petroleum products base prices will be available at the web site listed below. Current monthly prices will be posted to this web site on or before the 15<sup>th</sup> of each month. Bidders are advised to use the petroleum base prices on this web site when preparing their bids. The current monthly petroleum products base prices will become part of the contract during the execution of the contract.

Monthly Petroleum Products Base Prices can be viewed at:

<http://www.gomdot.com/Applications/BidSystem/Home.aspx>



**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 – NOTICE TO BIDDERS NO. 2862**

**CODE: (SP)**

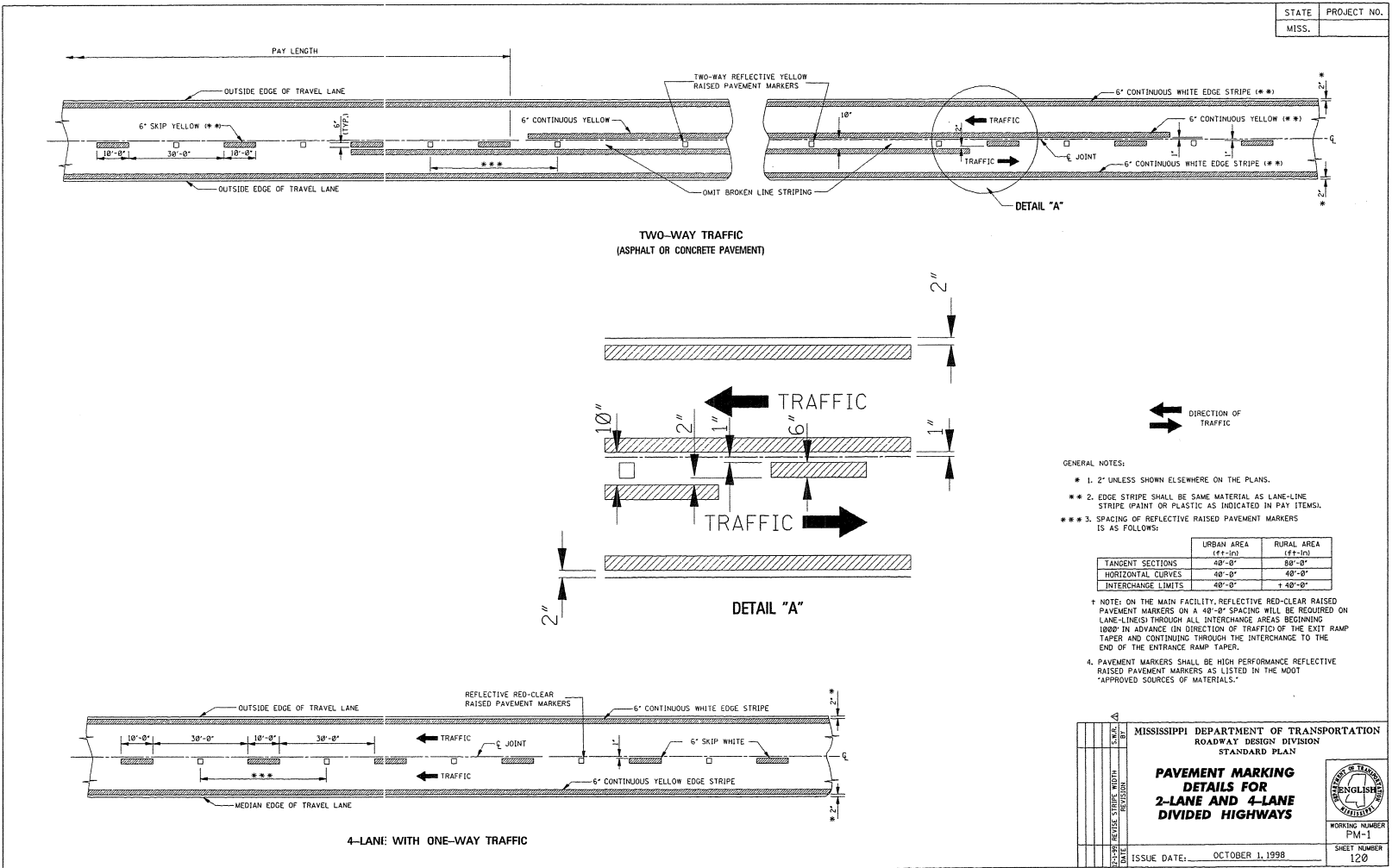
**DATE: 11/24/2009**

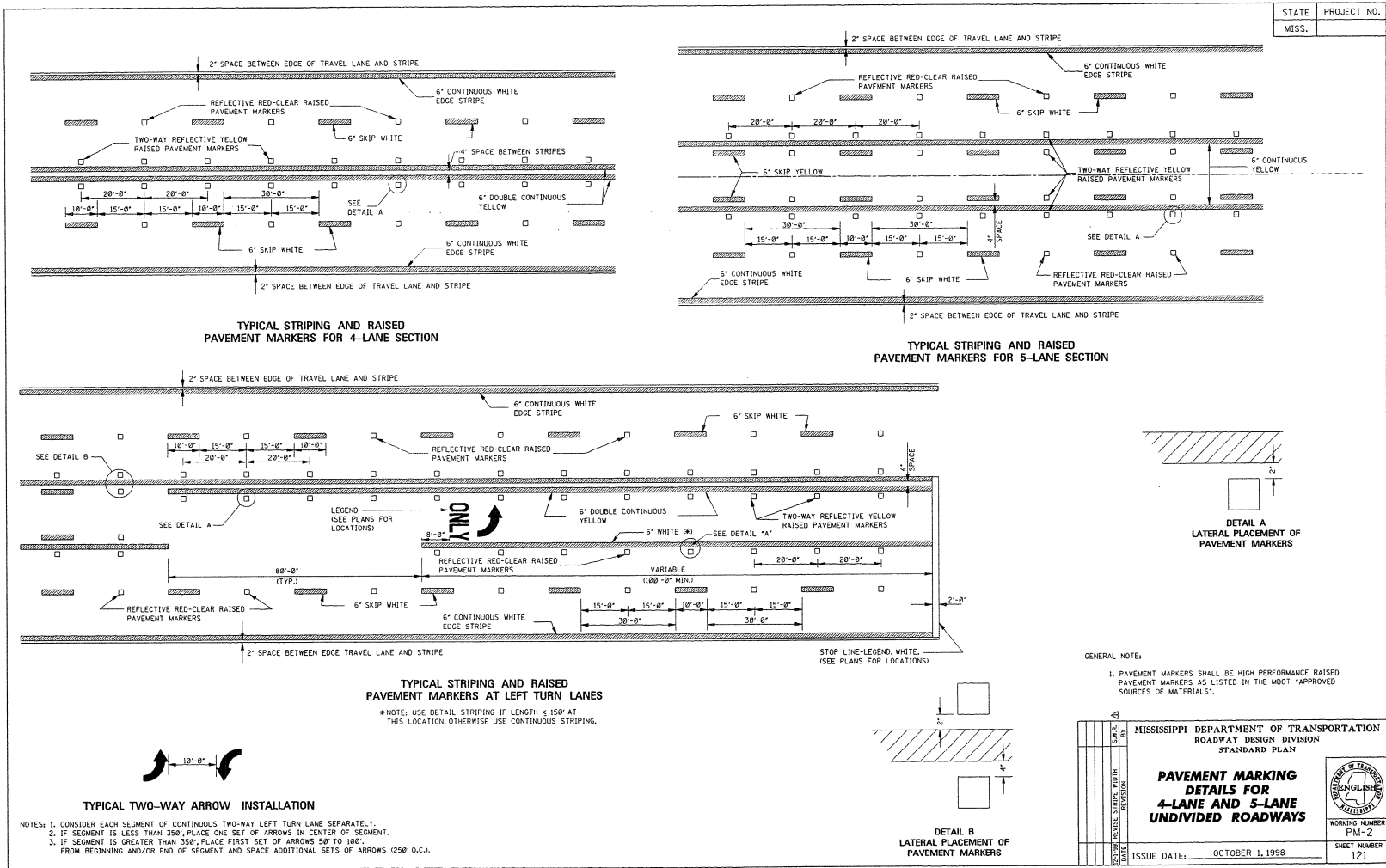
**SUBJECT: Standard Drawings**

Standard Drawings attached hereto shall govern appropriate items of required work.

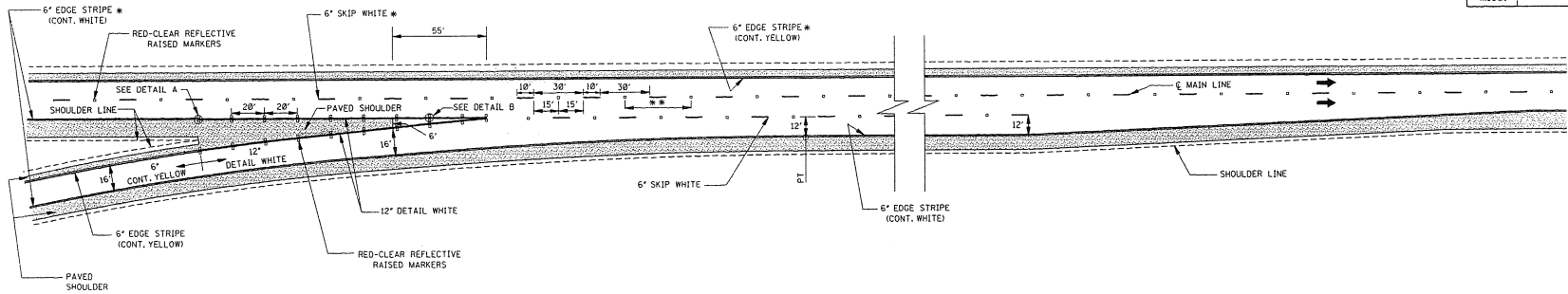
Larger copies of Standard Drawings may be purchased from:

MDOT Plans Print Shop  
MDOT Shop Complex, Building C, Room 114  
2567 North West Street  
P.O. Box 1850  
Jackson, MS 39215-1850  
Telephone: (601) 359-7460  
or FAX: (601) 359-7461  
or e-mail: [plans@mdot.state.ms.us](mailto:plans@mdot.state.ms.us)

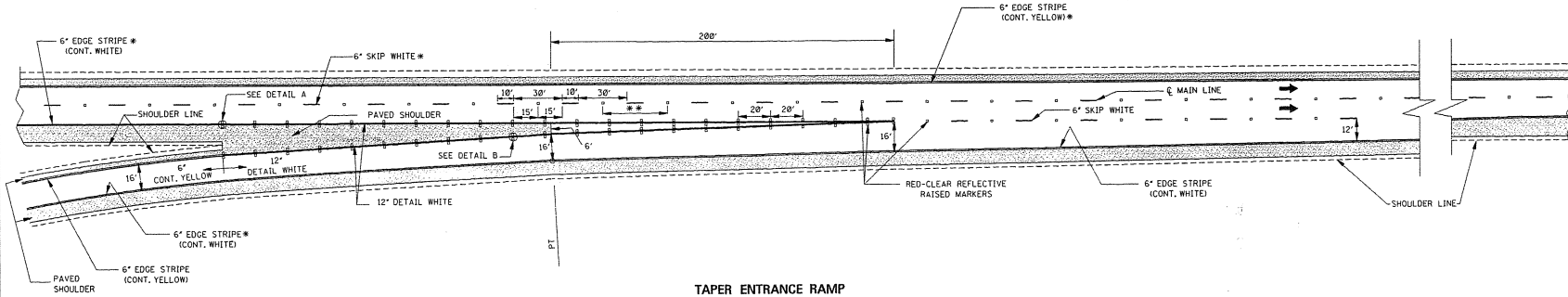




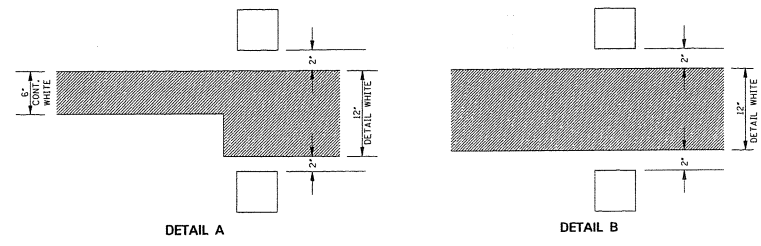
STATE	PROJECT NO.
MISS.	



PARALLEL ENTRANCE RAMP



TAPER ENTRANCE RAMP



- GENERAL NOTES:
- SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELED WAY.
  - ON THE MAIN FACILITY, PLACE REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS AT 40' SPACING ON ALL LANE-LINE(S) THROUGHOUT THE INTERCHANGE AREA BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
  - PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED SOURCES OF MATERIALS."

DESIGNED BY	4
CHECKED BY	
DATE	
REVISIONS	
DESCRIPTION	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

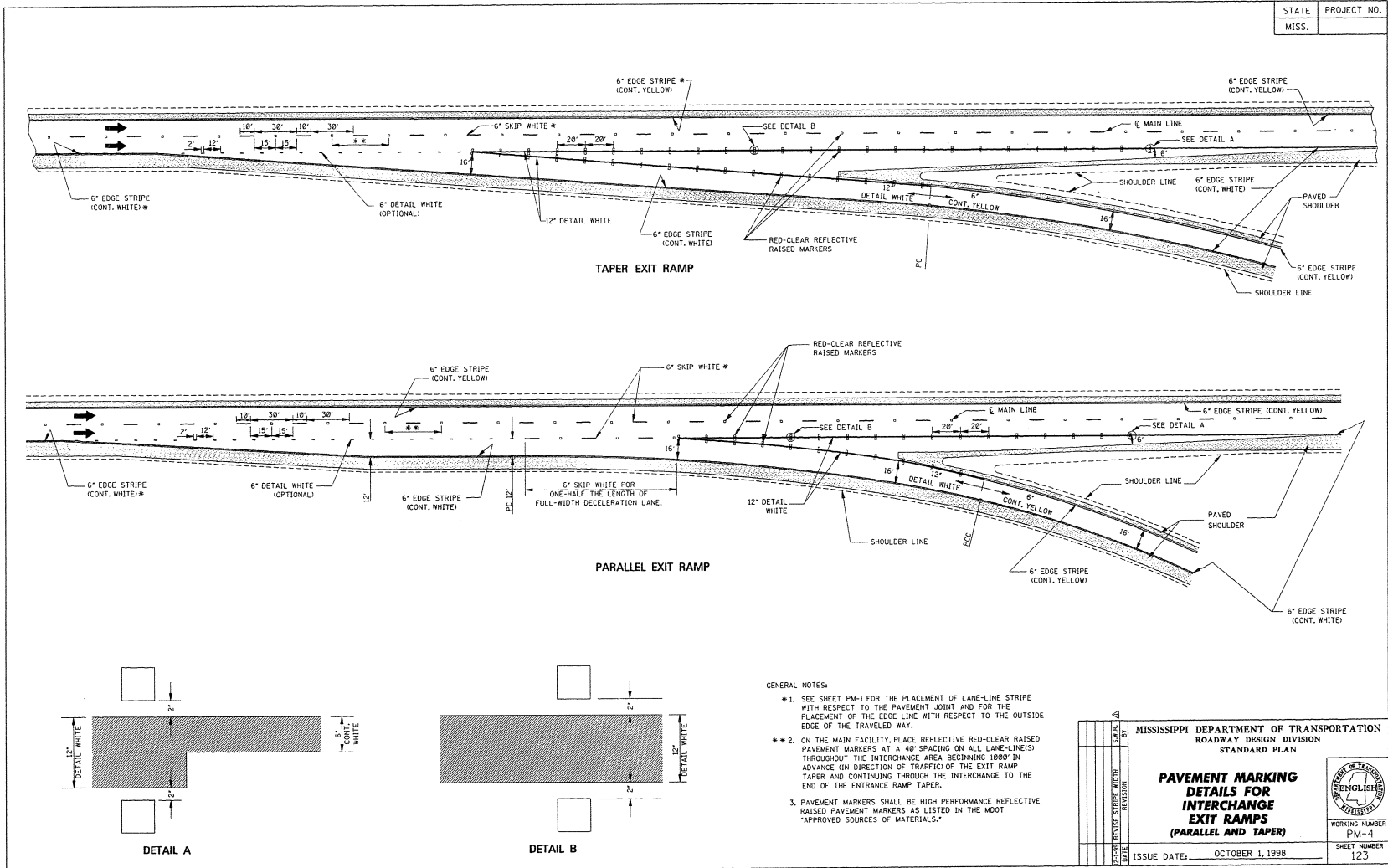
**PAVEMENT MARKING  
DETAILS FOR  
INTERCHANGE  
ENTRANCE RAMPS  
(PARALLEL AND TAPER)**



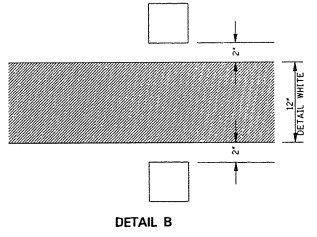
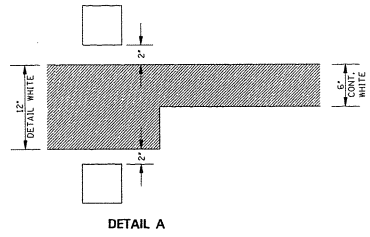
ISSUE DATE: OCTOBER 1, 1998

WORKING NUMBER: PM-3  
SHEET NUMBER: 122

STATE	PROJECT NO.
MISS.	



- GENERAL NOTES:
- \* 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELED WAY.
  - \*\* 2. ON THE MAIN FACILITY, PLACE REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS AT A 40' SPACING ON ALL LANE-LINE(S) THROUGHOUT THE INTERCHANGE AREA BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
  - 3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED SOURCES OF MATERIALS."



MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN		
<p align="center"><b>PAVEMENT MARKING DETAILS FOR INTERCHANGE EXIT RAMP (PARALLEL AND TAPER)</b></p>		
WORKING NUMBER PM-4	ISSUE DATE: OCTOBER 1, 1998	SHEET NUMBER 123

	STATE	PROJECT NO.
	MISS.	

STOP

RIGHT

LEFT

TURN

LANE

AHEAD

YIELD

EXIT

SIGNAL

SCHOOL

**GENERAL NOTES:**

- UNLESS OTHERWISE SHOWN ON THE PLANS, ALL PAVEMENT MARKING LEGENDS SHALL BE APPLIED USING HIGH PERFORMANCE MATERIALS.
- TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTORS) OF 1/8" OR LESS AND EXTENDING THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
- FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

PAY QUANTITIES	
LEGEND	AREA (Ft <sup>2</sup> )
STOP	24.6
RIGHT	26.6
LEFT	19.5
TURN	27.3
LANE	22.7
AHEAD	32.3
YIELD	26.8
EXIT	16.5
SIGNAL	32.5
SCHOOL	35.5

	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	<b>PAVEMENT MARKING LEGEND DETAILS</b>	
DATE	ISSUE DATE:	OCTOBER 1, 1998	WORKING NUMBER PM-5 SHEET NUMBER 124

STATE PROJECT NO.  
 MISS.

**ONLY**

**TURN ARROW**

**THRU ARROW**

**COMBINATION ARROW**

**1-WAY ARROW**

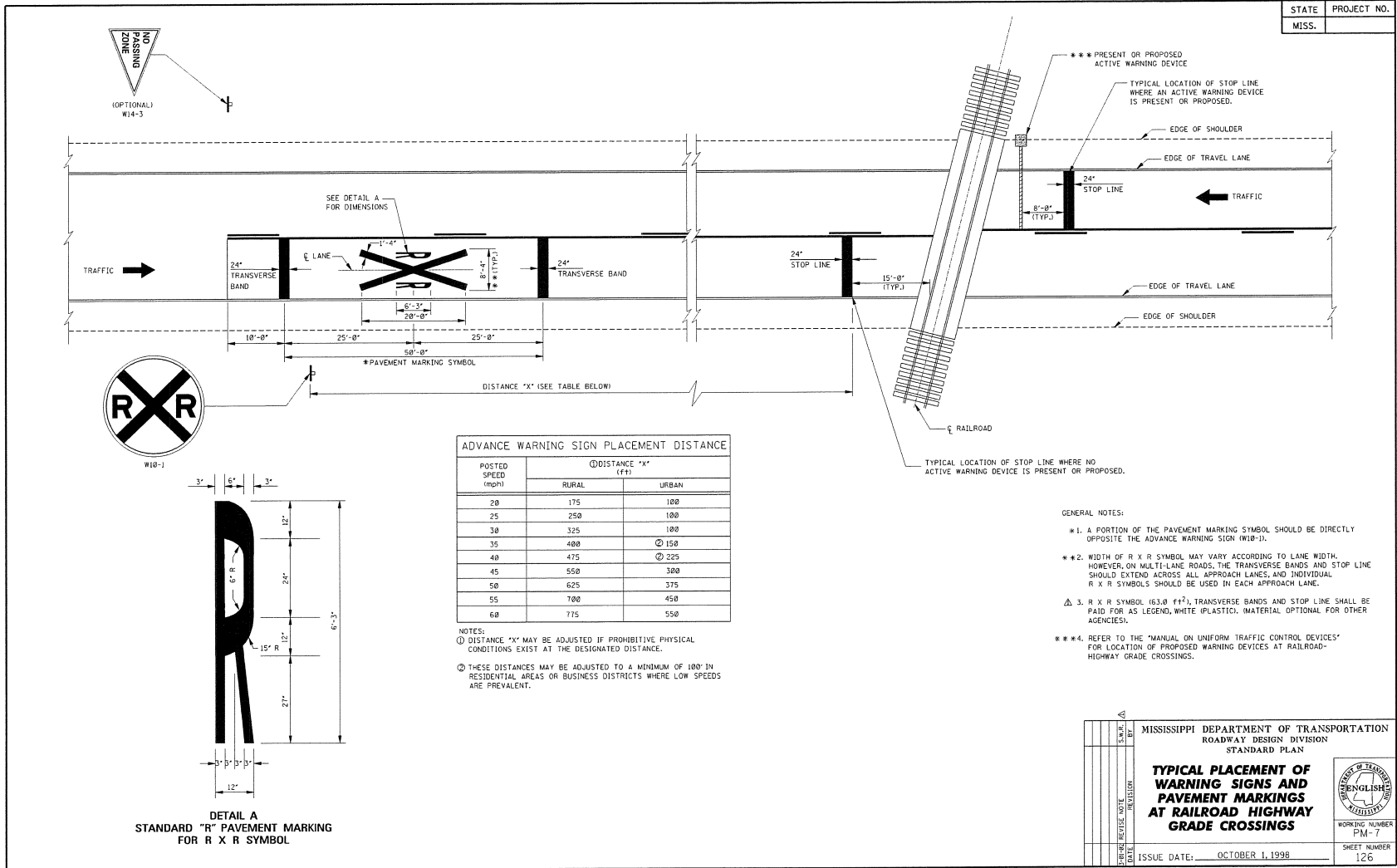
**GENERAL NOTES:**

- UNLESS OTHERWISE SHOWN ON THE PLANS, ALL PAVEMENT MARKING LEGENDS, INCLUDING TURN ARROWS, SHALL BE APPLIED USING HIGH PERFORMANCE MATERIALS.
- TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTORS OF 1/2" OR LESS AND EXTENDING THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
- FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

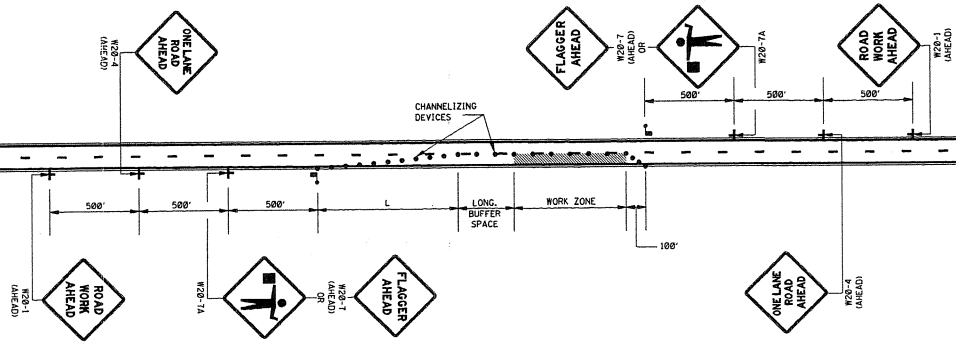
PAY QUANTITIES	
LEGEND/SYMBOL	AREA (±±²)
ONLY	22.0
TURN ARROW	15.4
THRU ARROW	12.3
COMB. ARROW	27.5
1-WAY ARROW	24.3

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION
REVISION	ROADWAY DESIGN DIVISION
	STANDARD PLAN
<b>PAVEMENT MARKING LEGEND DETAILS</b>	
DATE	ISSUE DATE: OCTOBER 1, 1998

WORKING NUMBER  
PM-6  
SHEET NUMBER  
125







LEGEND  
 FLAGGER  
 CHANNELIZING DEVICES

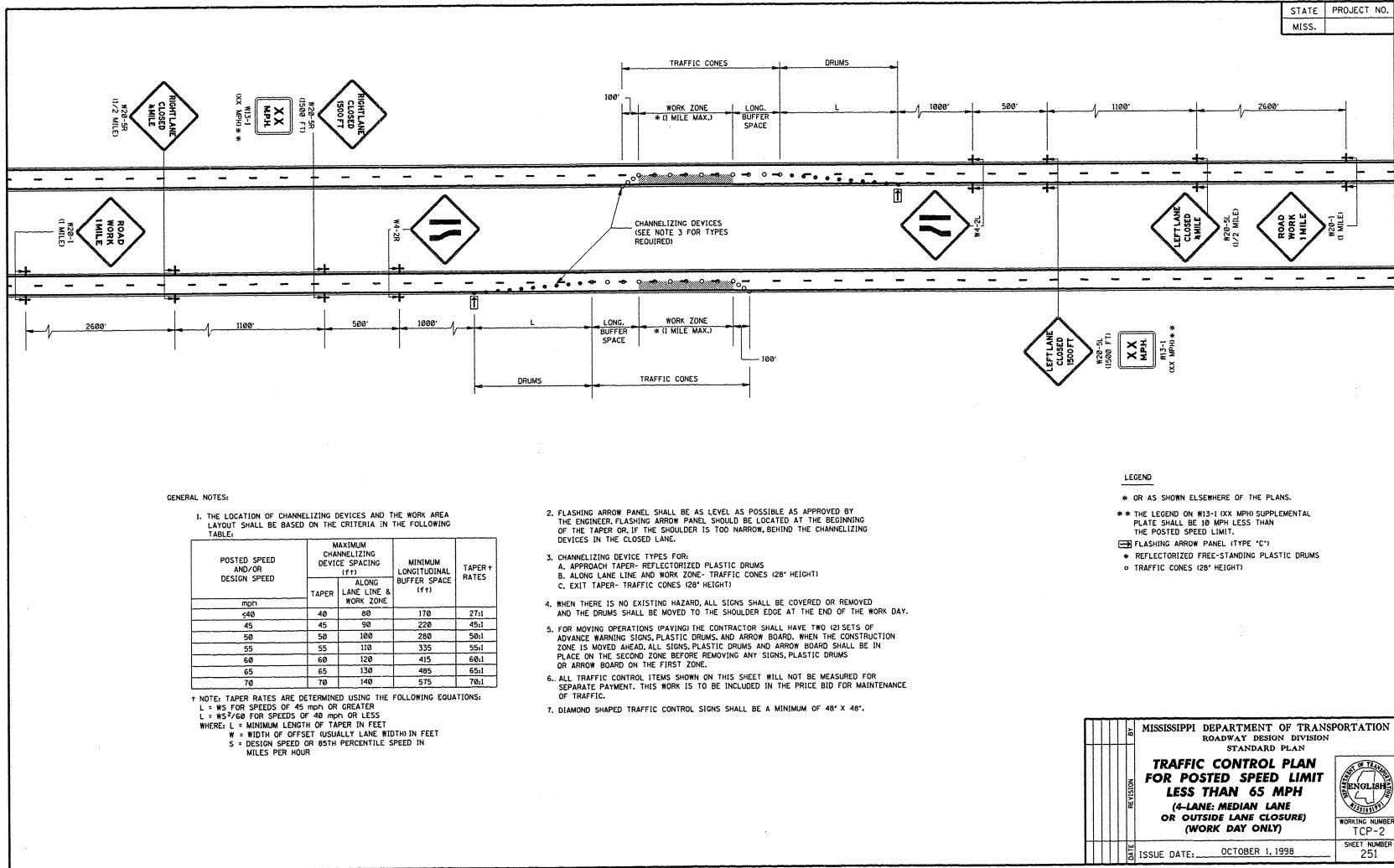
GENERAL NOTES:  
 1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (FT)		MINIMUM LONGITUDINAL BUFFER SPACE (FT)	TAPER † RATES
	TAPER	ALONG LANE LINE & WORK ZONE		
≤40	40	80	170	27:1
45	45	90	220	45:1
50	50	100	280	58:1
55	55	110	335	55:1
60	60	120	415	68:1
65	65	130	485	65:1
70	70	140	575	78:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:  
 $L = WS$  FOR SPEEDS OF 45 mph OR GREATER  
 $L = WS^2/60$  FOR SPEEDS OF 40 mph OR LESS  
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET  
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET  
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

2. ALL CHANNELIZING DEVICES SHALL BE A MINIMUM OF 24" IN HEIGHT.
3. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
4. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" x 48".
5. WHEN THERE IS NO EXISTING HAZARD OR AT THE END OF THE WORK DAY, ALL SIGNS SHALL BE COVERED OR REMOVED AND ALL CHANNELIZING DEVICES SHALL BE MOVED TO THE SHOULDER EDGE.
6. WHERE THE WORK ZONE IS STATIONARY, THE W20-7 (500 FT.) SIGN OR THE W20-7A SIGN TOGETHER WITH THE W20-7 (500 FT.) SUPPLEMENTAL PLATE SHOULD BE USED TO INDICATE THE DISTANCE TO THE FLAGGER.

DATE	REVISION	BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
<p><b>TRAFFIC CONTROL PLAN WITH FLAGGER (ONE-LANE CLOSURE OF TWO-WAY TRAFFIC)</b></p>			
ISSUE DATE:	OCTOBER 1, 1998		



STATE PROJECT NO.  
MISS.

GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		MINIMUM LONGITUDINAL BUFFER SPACE (ft)	TAPER † RATES
	TAPER	ALONG LANE LINE & WORK ZONE		
20	40	80	170	27:1
45	45	90	220	45:1
50	50	100	280	50:1
55	55	110	335	55:1
60	60	120	415	60:1
65	65	130	485	65:1
70	70	140	575	70:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:  
 $L = WS$  FOR SPEEDS OF 45 mph OR GREATER  
 $L = WS^2/60$  FOR SPEEDS OF 40 mph OR LESS  
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET  
 W = WIDTH OF OFFSET USUALLY LANE WIDTH IN FEET  
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

- FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
- CHANNELIZING DEVICE TYPES FOR:  
 A. APPROACH TAPER- REFLECTORIZED PLASTIC DRUMS  
 B. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT)  
 C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT)
- WHEN THERE IS NO EXISTING HAZARD, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
- FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
- DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".

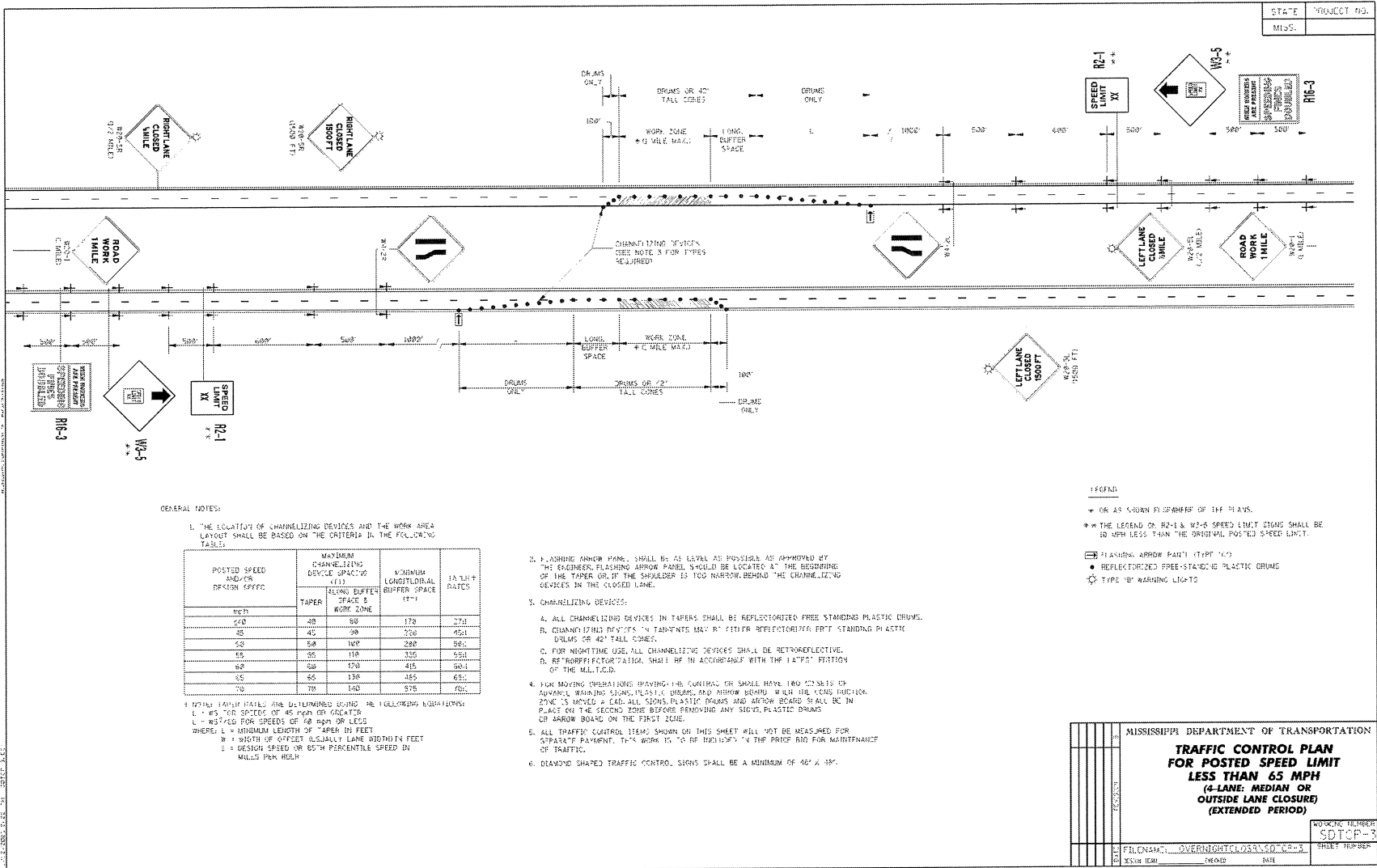
LEGEND

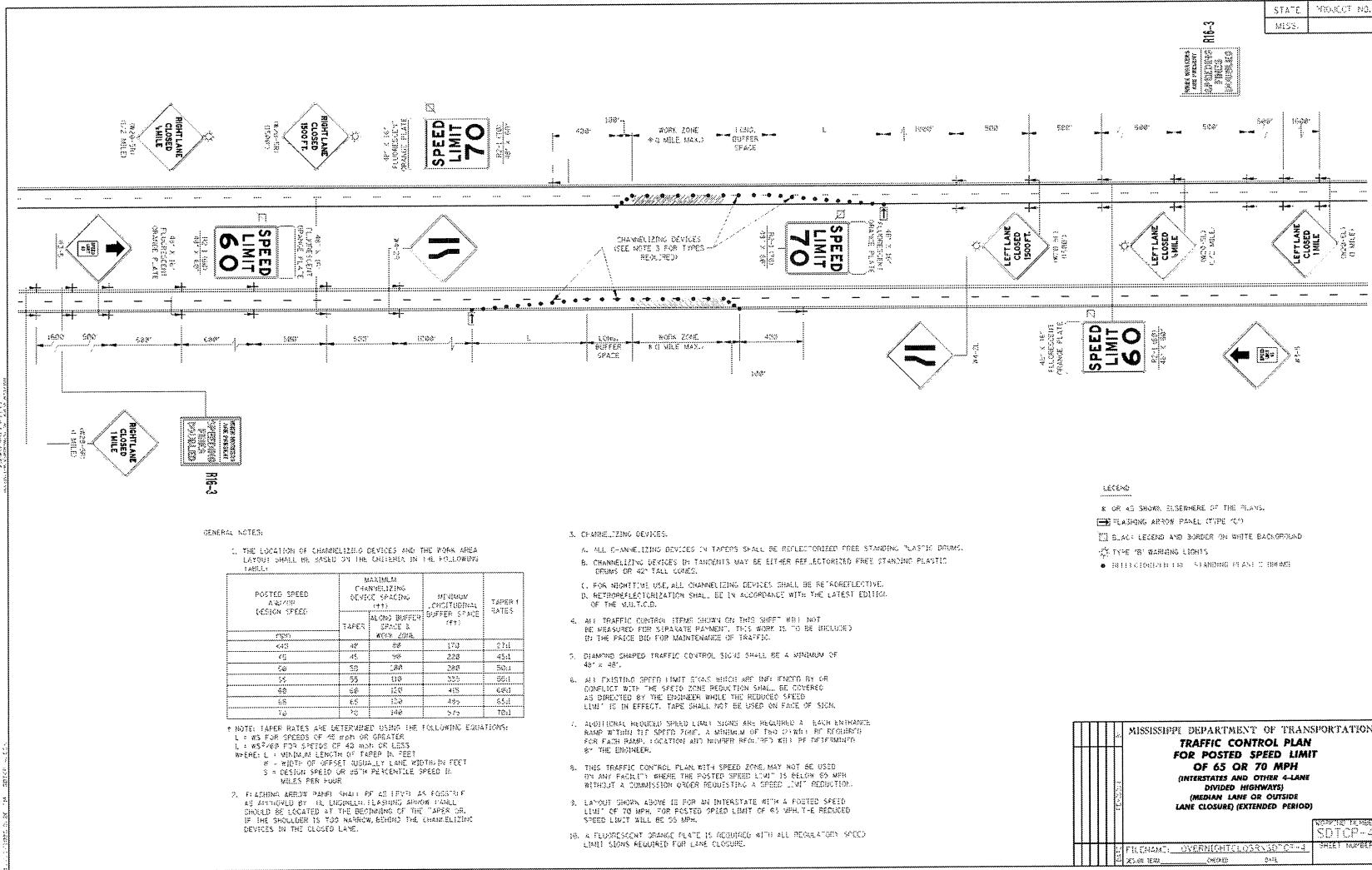
- OR AS SHOWN ELSEWHERE OF THE PLANS.
- THE LEGEND ON W13-1 0XX MPH SUPPLEMENTAL PLATE SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT.
- FLASHING ARROW PANEL (TYPE "C")
- REFLECTORIZED FREE-STANDING PLASTIC DRUMS
- TRAFFIC CONES (28" HEIGHT)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
 ROADWAY DESIGN DIVISION  
 STANDARD PLAN  
**TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT LESS THAN 65 MPH**  
 (4-LANE MEDIAN LANE OR OUTSIDE LANE CLOSURE)  
 (WORK DAY ONLY)

ISSUE DATE: OCTOBER 1, 1998

WORKING NUMBER TCP-2  
 SHEET NUMBER 251





GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (FT)		MINIMUM LONGITUDINAL BUFFER SPACE (FT)	TAPER RATES
	TAPER	ALONG BUFFER SPACE & WORK ZONE		
40	40	40	170	27:1
45	45	45	220	45:1
50	50	50	280	50:1
55	55	55	330	66:1
60	60	60	410	85:1
65	65	65	490	111:1
70	70	70	570	143:1

NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:  
 $L = WS$  FOR SPEEDS OF 45 MPH OR GREATER  
 $L = WS^2/60$  FOR SPEEDS OF 40 MPH OR LESS  
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET  
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET  
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

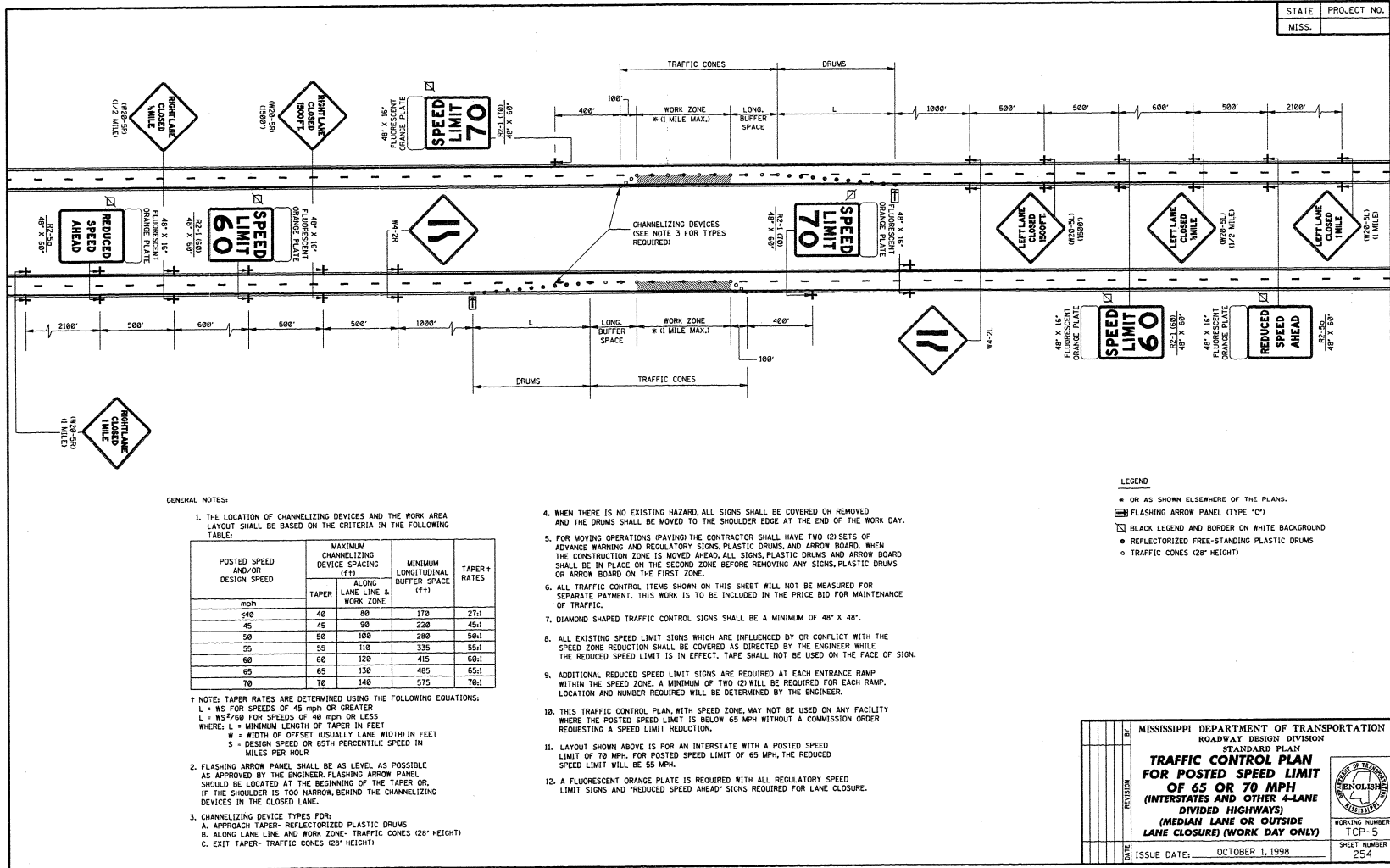
2. FLASHING ARROW PANELS SHALL BE AS FOLLOWS: A POSTERIOR VIEW AUTHORIZED BY THE ENGINEER. FLASHING ARROW PANELS SHOULD BE LOCATED AT THE BEGINNING OF THE WORK ZONE IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.

3. CHANNELIZING DEVICES:

- A. ALL CHANNELIZING DEVICES IN TAPERS SHALL BE REFLECTORIZED FREE STANDING "LASTIC" DRUMS.
- B. CHANNELIZING DEVICES IN TAPERS MAY BE EITHER REFLECTORIZED FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.
- C. FOR PERMITTEE USE, ALL CHANNELIZING DEVICES SHALL BE "REFLECTIVE".
- D. RETROREFLECTORIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE M.S.T.C.D.
- E. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
- F. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" x 48".
- G. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE IMPAIRED BY OR COMPLECT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON FACE OF SIGN.
- H. ADDITIONAL REDUCED SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP TO THE SPEED ZONE. A MINIMUM OF TWO (2) WILL BE REQUIRED FOR EACH RAMP. LOCATION AND NUMBER INDICATED WILL BE DETERMINED BY THE ENGINEER.
- I. THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 60 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
- J. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH THE REDUCED SPEED LIMIT SHALL BE 50 MPH.
- K. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS REQUIRED FOR LANE CLOSURE.

- LEGEND
- \* OR AS SHOWN ELSEWHERE ON THE PLANS.
  - REFLECTORIZED FREE STANDING PLASTIC DRUMS
  - FLASHING ARROW PANEL (TYPE "C")
  - BLACK LEGEND AND BORDER ON WHITE BACKGROUND
  - TYPE "B" WARNING LIGHTS
  - REFLECTORIZED FREE STANDING PLASTIC DRUMS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
<b>TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (EXTENDED PERIOD)</b>	
PROJECT NO.	OVERNIGHT/LOSS/3107-4
FILE NAME	OVERNIGHT/LOSS/3107-4
DATE	DATE
SHEET NUMBER	SHEET NUMBER



STATE PROJECT NO.  
MISS. \_\_\_\_\_

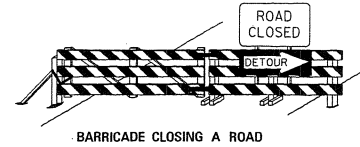
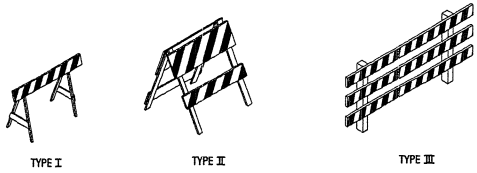
GENERAL NOTES:

- THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:
- | POSTED SPEED AND/OR DESIGN SPEED | MAXIMUM CHANNELIZING DEVICE SPACING (ft) |                             | MINIMUM LONGITUDINAL BUFFER SPACE (ft) | TAPER RATES |
|----------------------------------|--|-----------------------------|--|-------------|
|                                  | TAPER                                    | ALONG LANE LINE & WORK ZONE |  |             |
| 40                               | 40                                       | 80                          | 170                                    | 27:1        |
| 45                               | 45                                       | 90                          | 220                                    | 45:1        |
| 50                               | 50                                       | 100                         | 280                                    | 50:1        |
| 55                               | 55                                       | 110                         | 335                                    | 55:1        |
| 60                               | 60                                       | 120                         | 415                                    | 60:1        |
| 65                               | 65                                       | 130                         | 485                                    | 65:1        |
| 70                               | 70                                       | 140                         | 575                                    | 70:1        |
- † NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:  
 L = WS FOR SPEEDS OF 45 MPH OR GREATER  
 L = WS<sup>2</sup>/600 FOR SPEEDS OF 40 MPH OR LESS  
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET  
 W = WIDTH OF OFFSET USUALLY LANE WIDTH IN FEET  
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
- FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
  - CHANNELIZING DEVICE TYPES FOR:
    - APPROACH TAPER- REFLECTORIZED PLASTIC DRUMS
    - ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT)
    - EXIT TAPER- TRAFFIC CONES (28" HEIGHT)

- WHEN THERE IS NO EXISTING HAZARD, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
- FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING AND REGULATORY SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
- DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".
- ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON THE FACE OF SIGN.
- ADDITIONAL REDUCED SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. A MINIMUM OF TWO (2) WILL BE REQUIRED FOR EACH RAMP. LOCATION AND NUMBER REQUIRED WILL BE DETERMINED BY THE ENGINEER.
- THIS TRAFFIC CONTROL PLAN, WITH SPEED ZONE, MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
- LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.
- A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS AND "REDUCED SPEED AHEAD" SIGNS REQUIRED FOR LANE CLOSURE.

- LEGEND
- OR AS SHOWN ELSEWHERE OF THE PLANS.
  - FLASHING ARROW PANEL (TYPE "C")
  - BLACK LEGEND AND BORDER ON WHITE BACKGROUND
  - REFLECTORIZED FREE-STANDING PLASTIC DRUMS
  - TRAFFIC CONES (28" HEIGHT)

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION
DATE	ROADWAY DESIGN DIVISION
REVISION	STANDARD PLAN
	<b>TRAFFIC CONTROL PLAN</b>
	<b>FOR POSTED SPEED LIMIT</b>
	<b>OF 65 OR 70 MPH</b>
	<b>(INTERSTATES AND OTHER 4-LANE</b>
	<b>DIVIDED HIGHWAYS)</b>
	<b>(MEDIAN LANE OR OUTSIDE</b>
	<b>LANE CLOSURE) (WORK DAY ONLY)</b>
ISSUE DATE:	OCTOBER 1, 1998
WORKING NUMBER	TCP-5
SHEET NUMBER	254



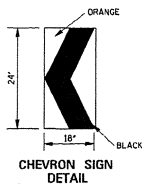
- STANDARD BARRICADES**
1. A TYPE I BARRICADE CONSISTS OF ONE (1) HORIZONTAL RAIL SUPPORTED BY A DEMOUNTABLE FRAME OR A LIGHT "A" FRAME. A TYPE I BARRICADE NORMALLY WOULD BE USED ON CONVENTIONAL ROADS OR URBAN STREETS AND ARTERIALS.
  2. A TYPE II BARRICADE CONSISTS OF TWO (2) HORIZONTAL RAILS ON A LIGHT "A" FRAME. TYPE II BARRICADES ARE INTENDED FOR USE ON EXPRESSWAYS AND FREEWAYS AND OTHER HIGH-SPEED ROADWAYS.
  3. TYPE I AND TYPE II BARRICADES ARE INTENDED FOR USE WHERE THE HAZARD IS RELATIVELY SMALL AS, FOR EXAMPLE, ON CITY STREETS, OR FOR THE MORE OR LESS CONTINUOUS DELIMITING OF A RESTRICTED ROADWAY, OR FOR TEMPORARY DAYTIME USE.
  4. A TYPE III BARRICADE CONSISTS OF THREE (3) HORIZONTAL RAILS SUPPORTED BY FIXED POSTS, A RIGID SKID, A HEAVY DEMOUNTABLE FRAME OR A HEAVY, HINGED "A" FRAME.
  5. TYPE III BARRICADES ARE INTENDED FOR USE ON CONSTRUCTION AND MAINTENANCE PROJECTS AS WING BARRICADES AND AT ROAD CLOSURES, WHERE THEY MUST REMAIN IN PLACE FOR EXTENDED PERIODS.
  6. THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE ISLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS.
  7. DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
  8. FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.

**BARRICADE CHARACTERISTICS**

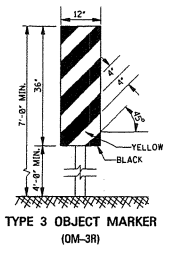
	I	II	III
WIDTH OF RAIL **	6" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE #	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF REFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS
TYPE OF FRAME	LIGHT	LIGHT "A" FRAME	POST OR SKID

\* 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.  
 \*\* 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 IN<sup>2</sup> OF REFLECTIVE AREA FACING TRAFFIC.

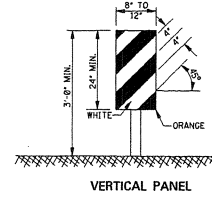
- WING BARRICADES**
1. WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAYMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
  2. WING BARRICADES SHOULD BE USED:
    - A. IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
    - B. IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.



- CHEVRON SIGN DETAIL**
1. A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
  2. THE CHEVRON SIGN SHALL BE MOUNTED ON FIXED POST OR RIGID SKID.
  3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHALL BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.

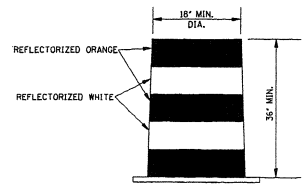


- TYPE 3 OBJECT MARKER (OM-3R)**
1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
  2. THE OM-3R IS SHOWN. THE OM-3L IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
  3. THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



- VERTICAL PANEL**
1. VERTICAL PANELS CONSIST OF AT LEAST ONE PANEL 8" TO 12" IN WIDTH AND A MINIMUM OF 24" IN HEIGHT.
  2. THE DIAGONAL STRIPES SHALL SLOPE DOWNWARD IN THE DIRECTION THAT TRAFFIC IS TO PASS THE PANEL. THE PANELS SHALL BE MOUNTED WITH THE TOP A MINIMUM OF 36" ABOVE THE ROADWAY ON A SINGLE LIGHT MASS POST.
  3. VERTICAL PANELS USED ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH-SPEED ROADWAYS SHALL HAVE A MINIMUM OF 270 IN<sup>2</sup> OF RETROREFLECTIVE AREA FACING TRAFFIC.
  4. FOR TWO-WAY TRAFFIC OPERATIONS, BACK-TO-BACK PANELS SHALL BE USED.

- GENERAL NOTES:**
1. MARKINGS ON ALL DEVICES SHOWN ON THIS SHEET SHALL BE HIGH INTENSITY REFLECTIVE SHEETING.
  2. THE TRAFFIC CONTROL PLAN WILL LIST THE VARIOUS TRAFFIC CONTROL DEVICES REQUIRED FOR EACH PROJECT.



- PLASTIC DRUM STRIPING DETAIL**
1. PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT COLOR ON DRUMS SHALL BE ORANGE WITH FOUR (4) REFLECTORIZED, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
  2. DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
  3. WHERE PRACTICAL PLASTIC DRUMS SHALL BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

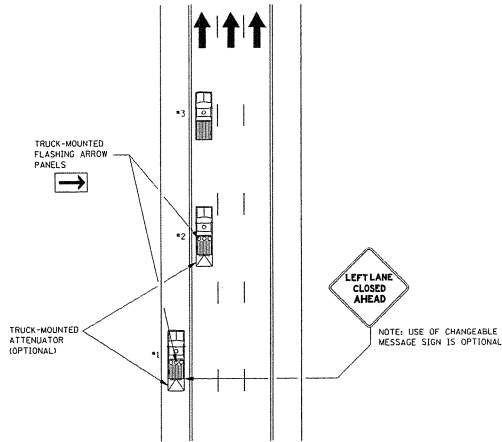
MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

**HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION**

ISSUE DATE: OCTOBER 1, 1998

WORKING NUMBER TCP-10  
SHEET NUMBER 259

MOBILE OPERATIONS ON MULTILANE ROAD Δ

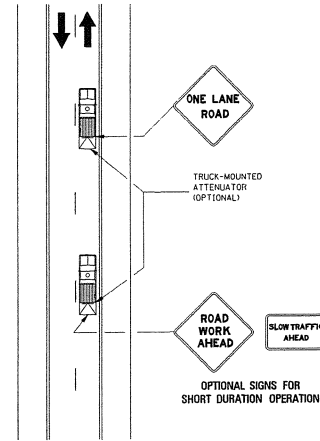


MOBILE OPERATIONS ON MULTILANE ROAD

NOTES:

1. VEHICLES USED FOR THESE OPERATIONS SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT, SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLAGS, SIGNS, OR ARROW PANELS.
2. PROTECTION VEHICLE #1 SHOULD BE EQUIPPED WITH AN ARROW PANEL. AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON PROTECTION VEHICLE #1 SO AS NOT TO OBSCURE THE ARROW PANEL.
3. PROTECTION VEHICLE #2 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK-MOUNTED ATTENUATOR (TMA).
4. PROTECTION VEHICLE #1 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
5. WHEN ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, PROTECTION VEHICLE #1 SHOULD BE ELIMINATED.
6. ON HIGH-SPEED ROADWAYS, A THIRD PROTECTION VEHICLE SHOULD BE USED (I.E., VEHICLE #1 ON THE SHOULDER IF PRACTICAL, VEHICLE #2 IN THE CLOSED LANE, AND VEHICLE #3 IN THE CLOSED LANE).
7. ARROW PANELS SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
8. WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
9. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MOBILE OPERATIONS ON TWO-LANE ROAD



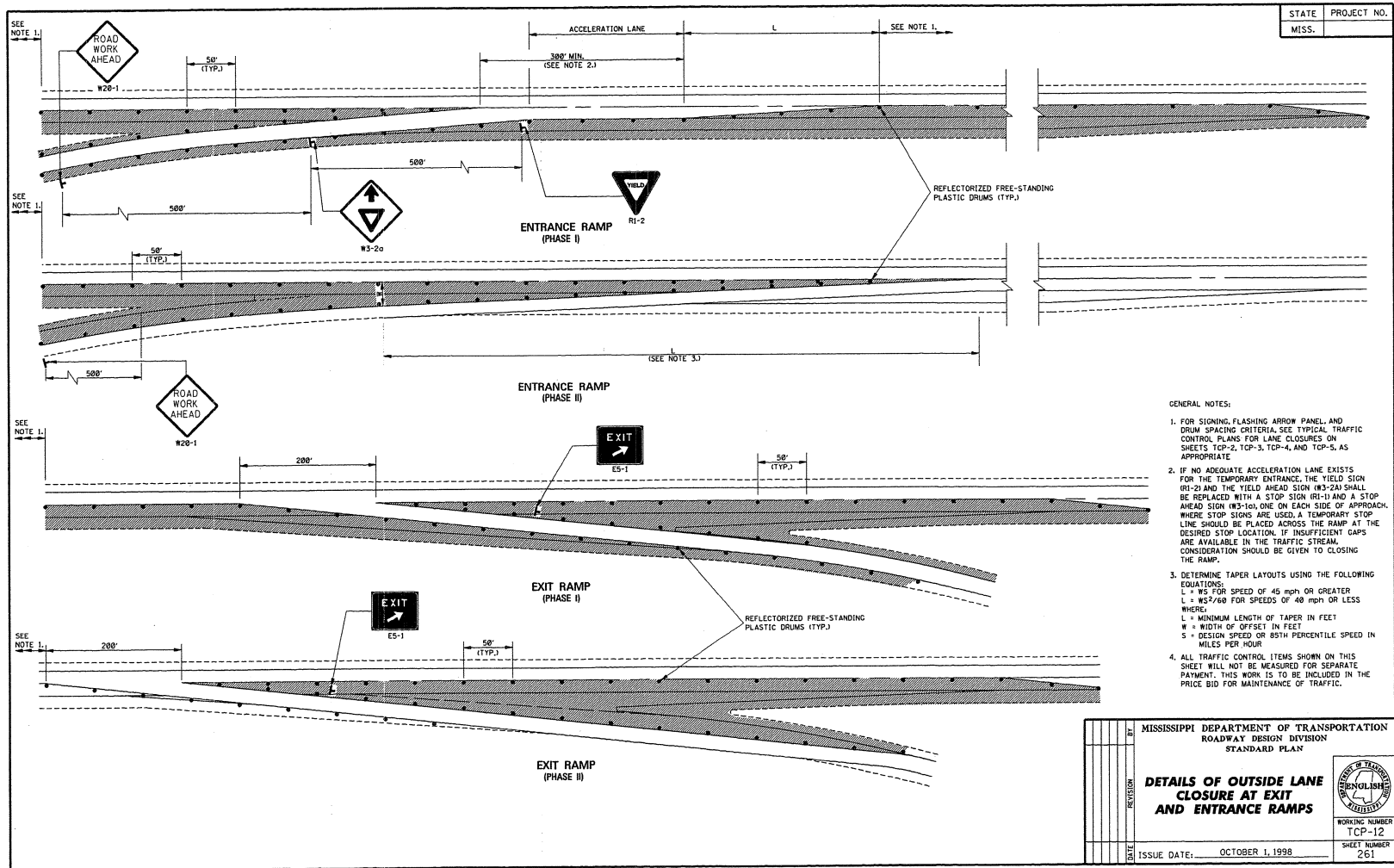
MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES:

1. WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND PROTECTION VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY, AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
2. THE DISTANCE BETWEEN THE WORK AND PROTECTION VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT, DRIVING TIME, AND OTHER FACTORS. PROTECTION VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD, WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, THE PROTECTION VEHICLE SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE PROTECTION VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
3. ADDITIONAL PROTECTION VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
4. A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE PROTECTION VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
5. THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE PROTECTION VEHICLES SHALL BE EQUIPPED WITH TWO HIGH-INTENSITY FLASHING LIGHTS MOUNTED ON THE REAR ADJACENT TO THE SIGN. PROTECTION AND WORK VEHICLES SHOULD DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
6. VEHICLE-MOUNTED SIGNS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAYMENT. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
7. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

STATE	PROJECT NO.
MISS.	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO-LANE ROADS	
ISSUE DATE: OCTOBER 1, 1998	



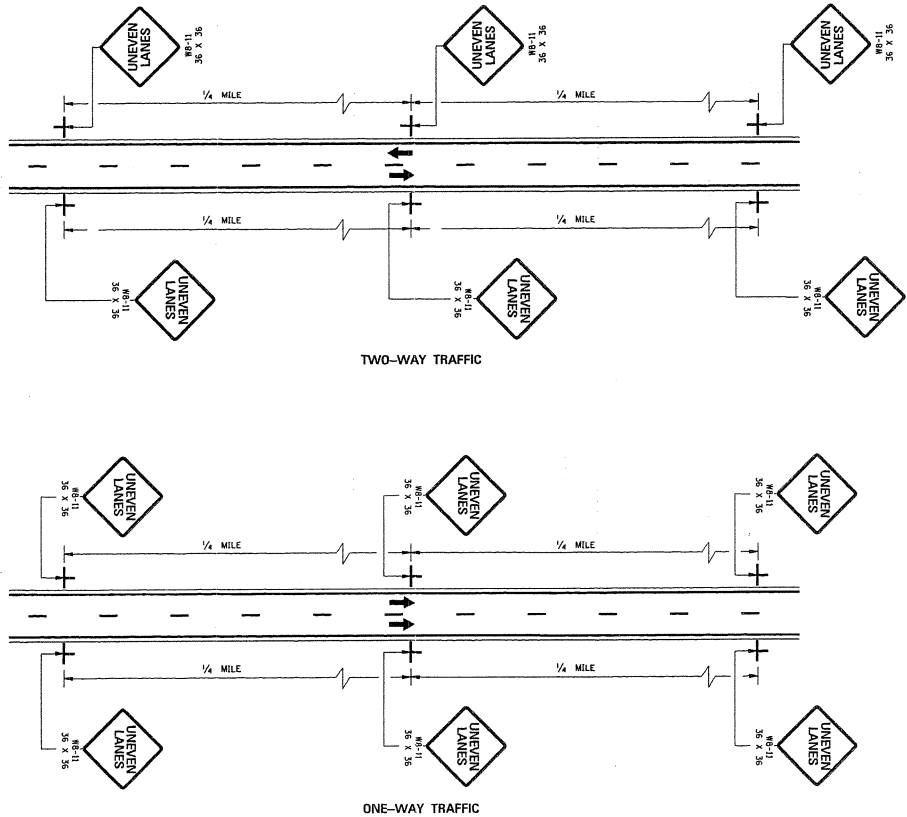
STATE PROJECT NO.  
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- GENERAL NOTES:
1. FOR SIGNING, FLASHING ARROW PANEL, AND DRUM SPACING CRITERIA, SEE TYPICAL TRAFFIC CONTROL PLANS FOR LANE CLOSURES ON SHEETS TCP-2, TCP-3, TCP-4, AND TCP-5, AS APPROPRIATE.
  2. IF NO ADEQUATE ACCELERATION LANE EXISTS FOR THE TEMPORARY ENTRANCE, THE YIELD SIGN (RI-2) AND THE YIELD AHEAD SIGN (#3-2A) SHALL BE REPLACED WITH A STOP SIGN (RI-1) AND A STOP AHEAD SIGN (#3-1A), ONE ON EACH SIDE OF APPROACH. WHERE STOP SIGNS ARE USED, A TEMPORARY STOP LINE SHOULD BE PLACED ACROSS THE RAMP AT THE DESIRED STOP LOCATION. IF INSUFFICIENT GAPS ARE AVAILABLE IN THE TRAFFIC STREAM, CONSIDERATION SHOULD BE GIVEN TO CLOSING THE RAMP.
  3. DETERMINE TAPER LAYOUTS USING THE FOLLOWING EQUATIONS:  
 $L = WS$  FOR SPEED OF 45 mph OR GREATER  
 $L = WS^2/500$  FOR SPEEDS OF 40 mph OR LESS  
 WHERE:  
 $L$  = MINIMUM LENGTH OF TAPER IN FEET  
 $W$  = WIDTH OF OFFSET IN FEET  
 $S$  = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
  4. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

DATE	REVISED	BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
DETAILS OF OUTSIDE LANE CLOSURE AT EXIT AND ENTRANCE RAMP			 WORKING NUMBER TCP-12 SHEET NUMBER 261
ISSUE DATE: OCTOBER 1, 1998			



STATE	PROJECT NO.
MISS.	



**GENERAL NOTES:**

1. UNEVEN LANE LINE:
  - A. IF LESS THAN OR EQUAL TO 1/4", NO SIGNS REQUIRED.
  - B. IF GREATER THAN 1/4" AND LESS THAN OR EQUAL TO 2/4", PLACE SIGNS AS SHOWN ON THIS SHEET.
  - C. IF GREATER THAN 2/4", TRAFFIC SHOULD NOT BE ALLOWED TO CROSS UNEVEN LANE LINE.
2. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.
3. THE W8-11 SIGNS SHALL BE SPACED AT 1/4-MILE INTERVALS THROUGHOUT UNEVEN LANE LINE LIMITS.

REVISION	DATE	BY

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

**TRAFFIC CONTROL PLANS  
UNEVEN PAVEMENT  
DETAILS**

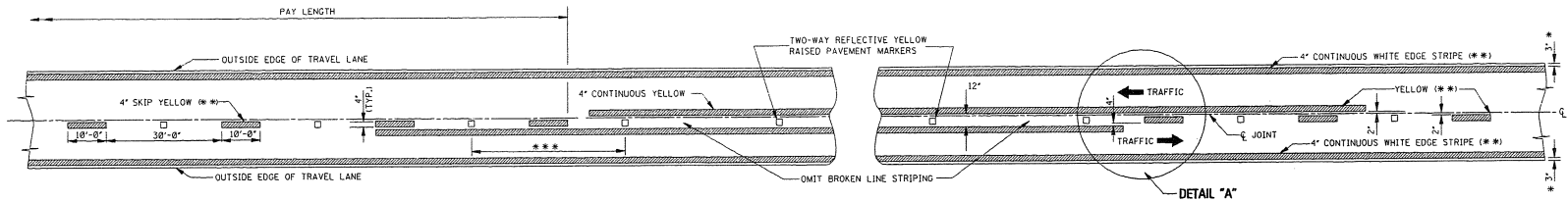
ISSUE DATE: OCTOBER 1, 1998

WORKING NUMBER  
TCP-14

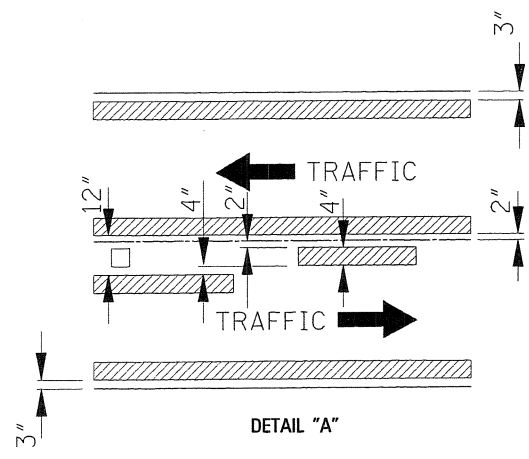
SHEET NUMBER  
263



STATE	PROJECT NO.
MISS.	



**TWO-WAY TRAFFIC**  
(ASPHALT OR CONCRETE PAVEMENT)



**DETAIL "A"**

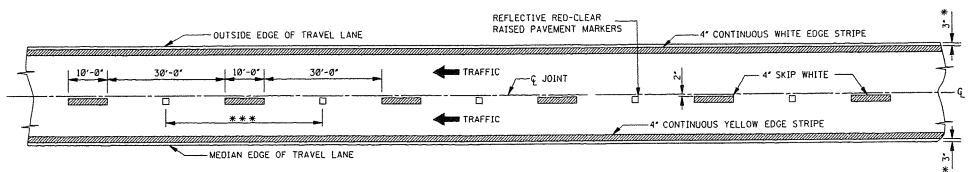


GENERAL NOTES:


- \* 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- \*\* 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
- \*\*\* 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

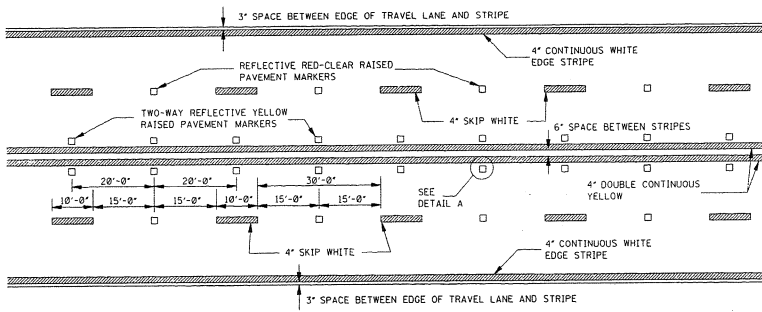
	URBAN AREA (11'-0" TO 11'-11")	RURAL AREA (12'-0" TO 11'-11")
TANGENT SECTIONS	40'-0"	80'-0"
HORIZONTAL CURVES	40'-0"	40'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

- † NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- 4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED SOURCES OF MATERIALS."
- 5. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS.

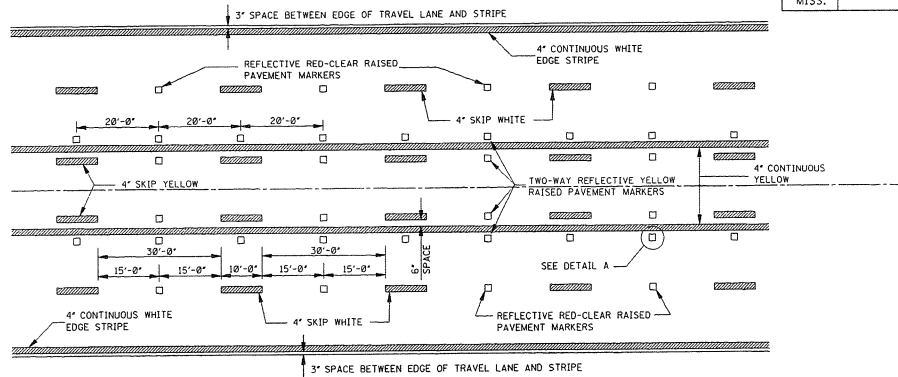


**4-LANE WITH ONE-WAY TRAFFIC**

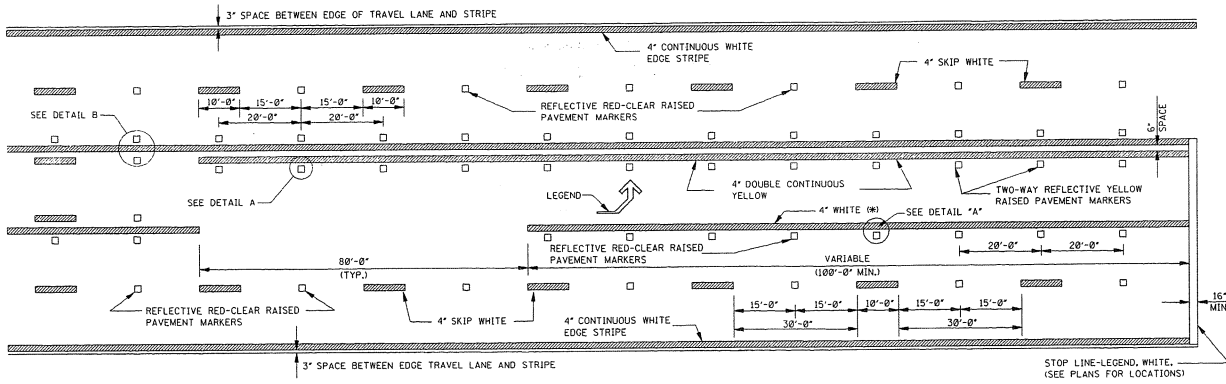
DATE	REVISION	BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
			<b>TEMPORARY STRIPING FOR TRAFFIC CONTROL</b> <b>2-LANE AND 4-LANE DIVIDED HIGHWAYS</b>
			 WORKING NUMBER TCF-15 SHEET NUMBER 264
			ISSUE DATE: DECEMBER 1, 1999



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION

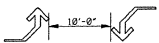


TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 5-LANE SECTION



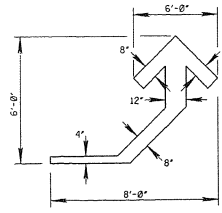
TYPICAL STRIPING AND RAISED PAVEMENT MARKERS AT LEFT TURN LANE

\*NOTE: USE DETAIL STRIPING IF LENGTH ≤ 150' AT THIS LOCATION, OTHERWISE USE CONTINUOUS STRIPING.

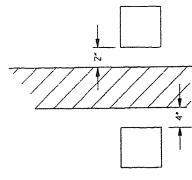


TYPICAL TWO-WAY ARROW INSTALLATION

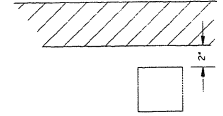
- NOTES:
1. CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY.
  2. IF SEGMENT IS LESS THAN 350', PLACE ONE SET OF ARROWS IN CENTER OF SEGMENT.
  3. IF SEGMENT IS GREATER THAN 350', PLACE FIRST SET OF ARROWS 50' TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS (250' O.C.).



DETAIL OF TEMPORARY TURN ARROW



DETAIL B LATERAL PLACEMENT OF PAVEMENT MARKERS



DETAIL A LATERAL PLACEMENT OF PAVEMENT MARKERS

GENERAL NOTE:

1. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED SOURCES OF MATERIALS".
2. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS.
3. TEMPORARY TURN ARROW TO BE PAID FOR AS TEMPORARY TRAFFIC STRIPE (LEGEND), ESTIMATED AT 10.9 SQ. FT. PER ARROW.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
<b>TEMPORARY STRIPING FOR TRAFFIC CONTROL 4-LANE AND 5-LANE UNDIVIDED ROADWAYS</b>	
REVISION	WORKING NUMBER TCP-16
DATE	SHEET NUMBER 265
ISSUE DATE: DECEMBER 1, 1999	

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 2937**

**CODE: (SP)**

**DATE: 01/11/2010**

**SUBJECT: Reduced Speed Limit Signs**

Bidders are advised that all black and white speed limits signs that are used to reduce the speed limit through construction zones shall be covered or removed during times when the Contractor is not performing work. If the Contractor has a routine daytime operation and is not working at night, the signs shall be covered or removed during the nighttime when there is no work activity.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 3039**

**CODE: (SP)**

**DATE: 03/23/2010**

**SUBJECT: Alternate Asphalt Mixture Bid Items**

Bidders are advised that the asphalt mixture used on this project will be bid as an alternate pay item: Hot Mix Asphalt (HMA) or Warm Mix Asphalt (WMA). Bidders must select one of the alternates at the time of bid. **The Contractor must use the selected asphalt mixture, HMA or WMA, throughout the entire project.**

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 3131**

**CODE: (SP)**

**DATE: 06/24/2010**

**SUBJECT: Temporary Traffic Paint**

Bidders are hereby advised that the temporary traffic paint for this project can be waterborne paint as specified in the 2004 Mississippi Standard Specifications For Road and Bridge Construction or fast dry solvent traffic paint meeting the requirements set out in 907-710-1 (Fast Dry Solvent Traffic Paint).

Payment for all temporary traffic paint shall be paid under the appropriate 619 pay items.

When using fast dry solvent traffic stripe, no paint can be sprayed or placed on the ground during set-up or clean-up.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 3242**

**CODE: (SP)**

**DATE: 09/21/2010**

**SUBJECT: Warm Mix Asphalt**

Bidders are advised that MDOT approved products and processes for the production of Warm Mix Asphalt is available at the following MDOT website.

<http://www.gomdot.com/Divisions/Highways/Resources/MPL/Home.aspx>

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 3414**

**CODE: (SP)**

**DATE: 02/16/2011**

**SUBJECT: DUNS Requirement for Federal Funded Projects**

Bidders are advised that the Prime Contractor must maintain current registrations in the Central Contractor Registration ( <http://www.ccr.gov> ) at all times during **this project**. A Dun and Bradstreet Data Universal Numbering System (DUNS) Number ( <http://www.dnb.com> ) is one of the requirements for registration in the Central Contractor Registration.

Bidders are also advised that the following information needs to be completed and included in the bid documents:

DUNS: \_\_\_\_\_

Company Name: \_\_\_\_\_

Company e-mail address: \_\_\_\_\_

By: \_\_\_\_\_



**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 3425**

**CODE: (SP)**

**DATE: 03/01/2011**

**SUBJECT: Questions Regarding Bidding**

Bidders are advised that all questions that arise regarding the contract documents or plans on this project shall be directed to the Construction Division at 601-359-7301.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 3558**

**CODE: (SP)**

**DATE: 6/8/2011**

**SUBJECT: Contract Time**

**PROJECT: STP-2170-00(023) / 106222301 – Forrest County**

The calendar date for completion of work to be performed by the Contractor for this project shall be **November 30, 2011** which date or extended date as provided in Subsection 907-108.06 shall be the end of contract time. It is anticipated that the Notice of Award will be issued no later than **August 9, 2011** and the effective date of the Notice to Proceed / Beginning of Contract Time will be **September 8, 2011**.

Should the Contractor request a Notice to Proceed earlier than **September 8, 2011** and it is agreeable with the Department for an early Notice to Proceed, the requested date will become the new Notice to Proceed / Beginning of Contract Time date.

*A progress schedule as referenced to in Subsection 907-108.03 will not be required for this contract.*

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 3559

DATE: 06/20/2011

SUBJECT: Specialty Items

PROJECT: STP-2170-00(023) / 106222301 - Forrest County

Pursuant to the provisions of Section 108, the following work items are hereby designated as "Specialty Items" for this contract. Bidders are reminded that these items must be subcontracted in order to be considered as specialty items.

### CATEGORY: GUARDRAIL, GUIDERAIL

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Line No	Pay Item	Description
0120	606-E003	Guard Rail, Terminal End Section, Non-Flared

### CATEGORY: PAVEMENT STRIPING AND MARKING

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Line No	Pay Item	Description
0200	627-J001	Two-Way Clear Reflective High Performance Raised Markers
0210	627-K001	Red-Clear Reflective High Performance Raised Markers
0220	627-L001	Two-Way Yellow Reflective High Performance Raised Markers
0310	907-626-A003	6" Thermoplastic Traffic Stripe, Skip White
0320	907-626-C003	6" Thermoplastic Double Drop Edge Stripe, Continuous White
0330	907-626-D003	6" Thermoplastic Traffic Stripe, Skip Yellow
0340	907-626-E004	6" Thermoplastic Traffic Stripe, Continuous Yellow
0350	907-626-F003	6" Thermoplastic Double Drop Edge Stripe, Continuous Yellow
0360	907-626-G004	Thermoplastic Detail Stripe, White
0370	907-626-G005	Thermoplastic Detail Stripe, Yellow
0380	907-626-H004	Thermoplastic Legend, White
0390	907-626-H005	Thermoplastic Legend, White

### CATEGORY: TRAFFIC CONTROL - PERMANENT

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Line No	Pay Item	Description
0230	630-F001	Delineators, Guard Rail, White
0240	630-F002	Delineators, Guard Rail, Yellow
0250	630-F003	Delineators, Flexible Post Mounted, Crossover, Type I, Green
0260	630-F004	Delineators, Flexible Post Mounted, Crossover, Type I, Yellow
0270	635-A001	Vehicle Loop Assemblies

### CATEGORY: TRAFFIC CONTROL - TEMPORARY

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Line No	Pay Item	Description
0150	619-A1002	Temporary Traffic Stripe, Continuous White
0160	619-A2002	Temporary Traffic Stripe, Continuous Yellow
0170	619-A3006	Temporary Traffic Stripe, Skip White
0180	619-A4006	Temporary Traffic Stripe, Skip Yellow

CATEGORY: TRAFFIC CONTROL - TEMPORARY

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Line No	Pay Item	Description
0190	619-A6001	Temporary Traffic Stripe, Legend

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 -- NOTICE TO BIDDERS NO. 3560**

**CODE: (SP)**

**DATE: 6/21/2011**

**SUBJECT: Scope of Work**

**PROJECT: STP-2170-00(023) / 106222301 – Forrest County**

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings”. All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following:

**US HIGHWAY 11  
WIDEN AND OVERLAY APPROXIMATELY 4.8 MILES OF US HWY 11  
FROM THE LAMAR/FORREST COUNTY LINE NORTH TO LINCOLN ROAD.**

**PLACEMENT OF ASPHALT:**

Widen and overlay the northbound and southbound lanes from (B.O.P.) Station 494+80 to (E.O.P.) Station 677+00 (See Typical Sections).

**Typical Section No. 1: Station 494+80 (B.O.P.) to Station 627+27**

- Cold mill 100’ transition at the B.O.P. from 1” to 0” and 50’ transition from 1” to 0” at local roads.
- Trench widen the shoulders two feet (2’) and variable at a depth of three inches (3”) below the existing asphalt roadway edge. The existing material removed from the trench shall be windrowed along the shoulder and later bladed and incorporated into the shoulder material used to place against the asphalt shoulder edge.
- Overlay each lane with ¾-inch and variable 9.5-mm, MT, Asphalt, Leveling, to correct the cross slope to 2% (See Notes) or superelevation rate in curves. The placement of the leveling asphalt shall include placing asphalt in the trench. The lanes shall be placed at 12-foot widths and the shoulders at a width of two feet (2’) for a total roadway width of 28 feet for the section from I-59 (Station 494+80) to the curb and gutter section (Station 627+27).
- Overlay each lane with 1½ inches of 9.5-mm, MT, Asphalt to a cross slope to 2% (See Notes) or superelevation rate in curves. The placement of the asphalt shall include the

overlay of the previously placed paved shoulder. The lanes from I-59 (Station 494+80) to the curb and gutter section (Station 627+27) shall be placed at 14-foot width for a total roadway width of 28 feet.

**Typical Section No. 2: Station 627+27 to Station 677+00**

- Mill the existing roadway 1½ inches from gutter to gutter and overlay the roadway with 1½ inches of 9.5-mm, MT, Asphalt. The roadway shall be milled full depth and overlaid to the end of curb and gutter at local roads.

**Typical Section No. 3: From the Lamar County line to I 59**

- Cold mill a 100-foot transition at the B.O.P. from 1" to 0", 50-foot transition from 1" to 0" at local roads, bridge approaches, and high joints that do not require removal and replacing shall be milled to the same elevation as the roadway prior to the leveling course.
- Overlay each lane with ¾-inch and variable 9.5-mm, MT, Asphalt, Leveling to correct the cross slope to 2% (See Notes) or superelevation rate in curves.
- Overlay each lane with 1½ inches of 9.5-mm, MT, Asphalt to a cross slope to 2% (See Notes) or superelevation rate in curves.

**ASPHALT NOTES:**

- Cross slopes shall be increased where practical with contract quantities in an effort to achieve a uniform cross slope of 2%.
- All crossovers, turn lanes, local roads, and driveways shall be overlaid with 9.5-mm, MT, asphalt.
- Publicly maintained roads or streets shall be surfaced to the existing R.O.W.; privately owned entrances shall be surfaced a distance of 10 feet & variable from the edge of pavement.
- Any site grading at local roads or drives will not be measured for separate payment but will be considered an absorbed item.
- A paved apron shall be placed around each guardrail location as directed by the Project Engineer.
- Approximately 80 tons of 19-mm, MT, asphalt will be used for Base Repair. The following failed areas will be repaired. NB Mainline 1 failed @ 1.285 Miles North of the BOP, 2 failed @ 3.895 miles North of the BOP, and 1 failed @ 3.810 miles North of the BOP. SB Mainline 2 failed @ 3.380 miles North of the BOP, and 1 @ 4.220 miles North of the BOP.

- Remove and replace failed concrete slabs just South of the I-59 overpass on US 11 as directed by the Project Engineer (See Typical Drawings).

**CONCRETE REPAIR NOTES:**

- Remove and replace sections of concrete pavement on Hwy 11 as per the attached chart.
- Traffic to be detoured on the shoulders during the removal and replacement. Limits of temporary detour to be determined by Contractor to provide safe movement of traffic and safety to workers. Shoulders to be milled as necessary and overlaid using the asphalt mixes provided in the Contract. Removal and replacement curb at drives will be necessary to provide an adequate temporary detour (costs to be absorbed in concrete repair items). Trimming of limbs, removal and resetting of permanent signs, traffic control signs, drums, cones, arrow boards, changeable message signs, removal of traffic stripe and other incidental items not specified in this scope are to be absorbed in concrete repair items. Detour shall be Contractor designed and meet the standards of the Manual on Uniform Traffic Control Devices and shall be submitted to the Project Engineer for review prior implementation. Temporary Striping to be used in detour to be paid with Contract unit prices for temporary stripe and shall be Type II Tape.
- Concrete repair to be in accordance with Standard Drawings PR-1 and PR-2. Any items not specified for payment are to be absorbed in other concrete paving items.
- New joints in concrete pavement to be sawed and sealed as per the joint repairs shown in Standard Drawing PR-2 (costs to be absorbed in other concrete paving items).
- Concrete mix shall have a compressive strength of 2,600 psi in 18 hours.

**GENERAL NOTES:**

- Prior to the overlay, centerline alignment shall be determined by the contractor by measuring the existing roadway at 500-foot intervals in tangent sections, and 100-foot intervals in horizontal curves. The existing pavement edge shall be cut to a smooth and near vertical face with an approved cutting device (Not to be measured for separate pay). The foundation for widening the base shall be formed by trenching or excavating to the required depth, width, and constructing a smooth, firm compacted foundation using 9.5-mm, MT, asphalt when placing the leveling (See typical drawing). Material excavated from the trench shall be used to raise the existing shoulder to match the new pavement elevation. Surplus material shall be spread along the edge of the shoulders, foreslopes, or other adjacent areas as directed by the Project Engineer, and will be an absorbed item.
- The Contractor will mill a 12-inch rumble strip along the shoulders. The Contractor will place the traffic stripe on the inside six inches (6") of the rumble strip (See Typical Drawings).

- All Transverse Joints shall be sawed and sealed after the overlay (See Typical Drawing). The Contractor shall locate and mark all joints requiring sealing for proper location of sawing.
- The installation of non-flared terminal end sections, installed as per manufactures recommendations, shall be National Cooperative Highway Research Program (NCHRP) Report 350 Test Level 3 (TL-3) approved. The flared Terminal End Section or equivalent shall be used at each of the bridge locations on this project. The Contractor shall furnish the Project Engineer two (2) copies of the manufacture's installation instructions prior to beginning guardrail operations. Any site grading and all fill material necessary at the guardrail location will not be measured for separate payment but will be absorbed in other guardrail pay items. Seeding, fertilizer and vegetative mulch are required on all disturbed areas at the guardrail site and will be considered an absorbed item. Application rate for bermuda grass seed will be 25 pounds per acre. Application rate for combination fertilizer (13-13-13) will be 1,000 pounds per acre. Application rate for mulch shall be two (2) tons per acre.
- Raise the existing shoulders to match the new pavement elevation by placing Size 825 Crushed Stone Base on the shoulders where necessary. Prior to the placement of shoulder material, the existing vegetation and material excavated from the trench shall be windrowed on the shoulder to be incorporated with crushed stone material by mechanical mixing. Crushed stone shall only be placed where necessary to have an adequate shoulder as per the typical section. The existing material removed from the shoulders should be used where possible. Placement of the crushed stone Material on the finished surface course shall not be permitted. The material shall be bladed, rolled and compacted to a finished slope of 4%.
- Temporary striping shall conform to finished stripe specifications for alignment, neatness, reflectivity, and straightness. All permanent edge pavement markings on asphalt are to be hot thermoplastic. Edge lines will be placed on the inside edge of the rumble strips with the inside edge of the rumble strip being at the edge of the original lane. Glass beads applied to thermoplastic edge lines shall conform to Subsection 720.01. Beads shall be double dropped Class B High-Visibility first and then Class A High-Visibility.
- Raised pavement markers will be placed at 80-foot intervals in tangents and 40-foot intervals in curves, and in urban limits along the centerline of roadway. Any removal of existing raised pavement markers shall be considered an absorbed item.
- The Contractor shall erect and maintain construction signing, and provide all signs and traffic handling devices in accordance with Manual Uniform Traffic Control Devices (MUTCD) (the cost is to be included in the price bid for pay item No. 618-A, Maintenance of Traffic).
- Incidental work such as removing vegetation, shaping and compaction of shoulder, removing excess asphalt material, project clean-up, and other incidental work necessary to

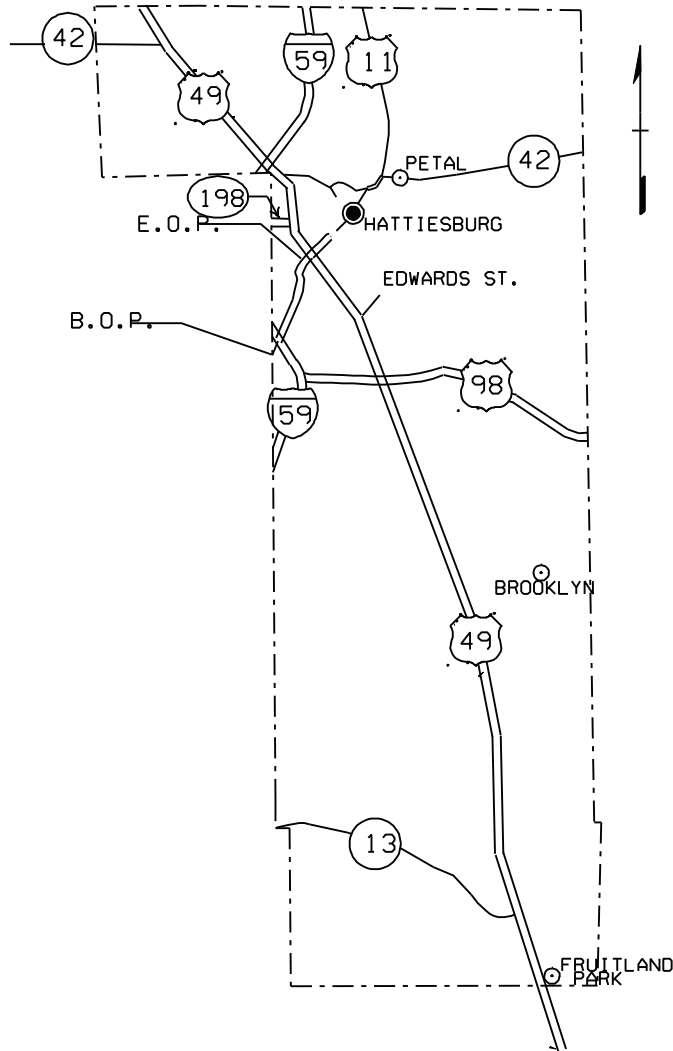


complete the project will not be measured for separate payment, but will be included in other bid items.

- After cold milling through the Lincoln Road intersection and the placement of leveling through WSF Tatum Blvd., replace the vehicle loop assemblies for each intersection as per the chart below. All loop assemblies shall be installed prior to the placement of the surface course.

<b>Phase</b>	<b>Size</b>	<b>Quantity</b>
1	50' x 6'	162 LF
2A	6' x 6'	24 LF
2B	6' x 6'	24 LF
3	50' x 6'	162 LF
4	50' x 6'	162 LF
5	50' x 6'	162 LF
6A	6' x 6'	24 LF
6B	6' x 6'	24 LF
7	50' x 6'	162 LF
8	50' x 6'	162 LF
<b>Total =</b>		<b>1,068 LF per intersection</b>

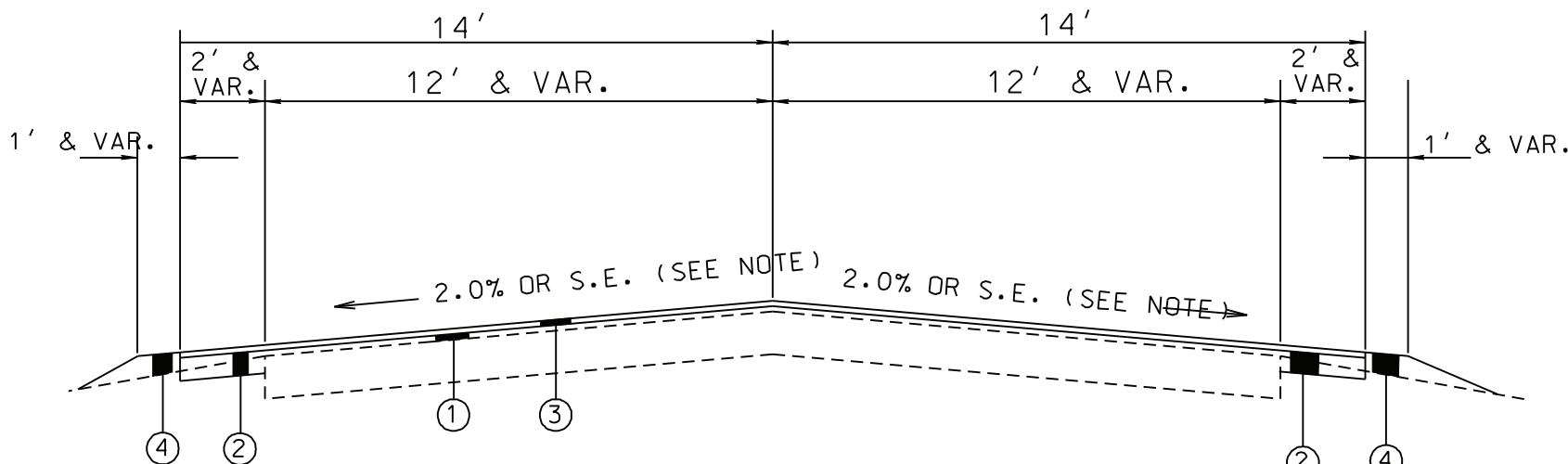
STP-2170-00(023)  
US 11 FORREST CO.



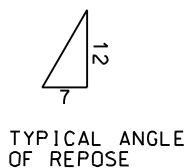
THE ABOVE REFERENCED PROJECT IS FOR PAVING  
4.8 MILES OF US HWY 11, FROM THE LAMAR C/L TO  
LINCOLN ROAD

TYPICAL SECTION  
 U.S. Hwy 11 494+80 (B.O.P.) To 627+27

CL IN DIRECTION OF TRAVEL



NOTE: CROSS SLOPES SHALL BE INCREASED WHERE PRACTICAL WITH CONTRACT QUANTITIES IN AN EFFORT TO ACHIEVE A UNIFORM CROSS SLOPE OF 2%



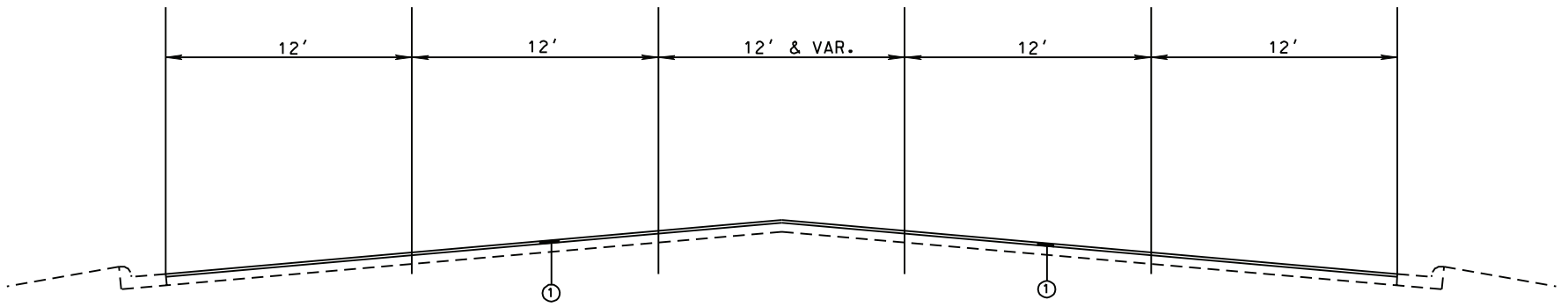
- ① 3/4" & VAR. 9.5mm MT HMA
- ② 3" TRENCH WIDENING REQUIRED (9.5mm MT HMA) TO BE PLACED WITH LEVELING
- ③ 1 1/2" 9.5mm MT HMA
- ④ SIZE 825 Crushed Stone Base 1 3/4" & VARIABLE  
 (To be incorporated with existing trench and vegetative material)

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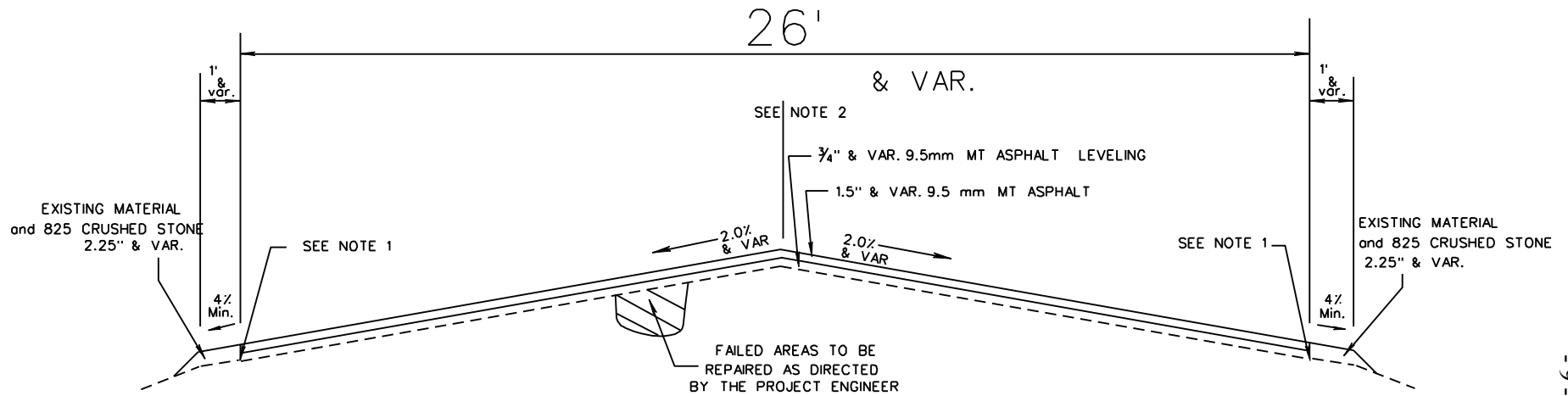
Notice To Bidders No. 3560--Cont'd.

TYPICAL SECTION NO. 2: STA. 627+27 TO STA. 677+00 (E.O.P.)



① COLD MILL 1 1/2" AND OVERLAY WITH 1 1/2" OF 9.5mm MT HMA

# Typical Section #3



NOTES:

- (1) THE EXISTING PAVEMENT EDGE SHALL BE CUT TO A SMOOTH AND NEAR VERTICAL FACE WITH AN APPROVED CUTTING DEVICE. (NOTO MEASURED FOR SEPARATE PAY.)
- (2) TRUE CENTERLINE PAVEMENT ALIGNMENT SHALL BE DETERMINED BY THE CONTRACTOR BY MEASURING THE EXISTING ROADWAY AT 500ft. INTERVALS IN TANGENT SECTIONS, AND 100ft. INTERVALS IN HORIZONTAL CURVES.
- (3) 9.5mm MT (3/4" LEVELING)
- (4) 9.5mm MT (1.5" & VARIABLE SURFACE COURSE)

# Concrete Repair Locations on U.S. Hwy 11

Station	202-B030 Removal of Conc. Pav't.	503-A009 9" Reinf. Conc. Pav't.	501-D001 Exp. Joint w/ Dowels	501-I001 Joint Filler Wooden Board	503-B001 Saw Cut Long. Joints	503-C006 Saw Cut 9"	503-E001 Tie Bars No. 4 Deform	503-E002 Tie Bars No. 5 Deform	503-F001 Smooth Dowels	Remarks
<b>North Bound Lane</b>										
483+29 - 484+34 RL	175 SY	175 SY	60 LF	27 LF	105 LF	30 LF	17 EA	52 EA	11 EA	Width = Var. 12' - 18'
484+34 - 484+76 RL/RS	56 SY	56 SY	12 LF	12 LF	42 LF	24 LF	11 EA	20 EA	11 EA	Width = 12'
484+94 - 485+21 RL/RS	36 SY	36 SY	24 LF	0 LF	27 LF	24 LF	22 EA	13 EA	0 EA	Width = 12'
<b>South Bound Lane</b>										
483+50 - 484+35 LL	137 SY	137 SY	43 LF	12 LF	85 LF	29 LF	16 EA	42 EA	11 EA	Width = Var. 12' - 17'
484+56 - 484+85 LL/LS	39 SY	39 SY	0 LF	12 LF	29 LF	24 LF	11 EA	14 EA	11 EA	Width = 12'
485+06 - 485+91 LL/LS	113 SY	113 SY	36 LF	12 LF	85 LF	24 LF	11 EA	42 EA	11 EA	Width = 12'
<b>TOTALS</b>	<b>556 SY</b>	<b>556 SY</b>	<b>175 LF</b>	<b>75 LF</b>	<b>373 LF</b>	<b>155 LF</b>	<b>88 EA</b>	<b>183 EA</b>	<b>55 EA</b>	

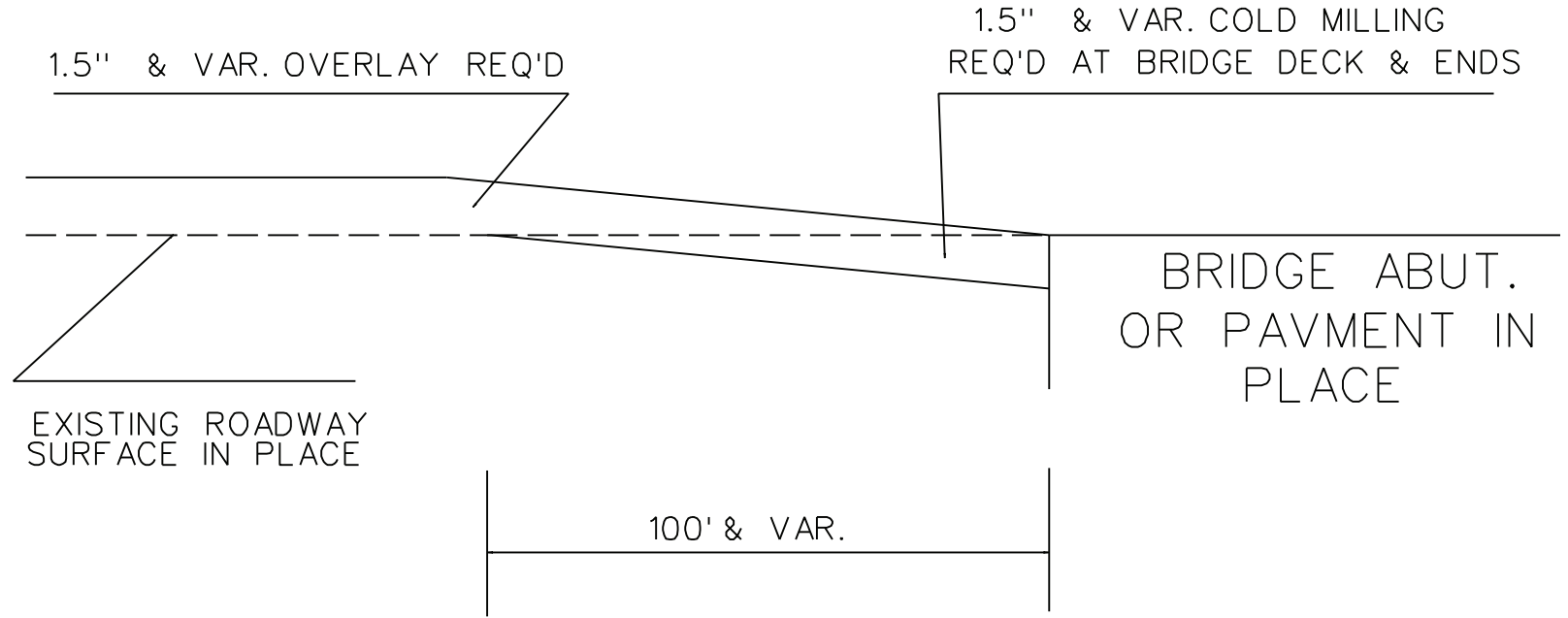
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- 10 -

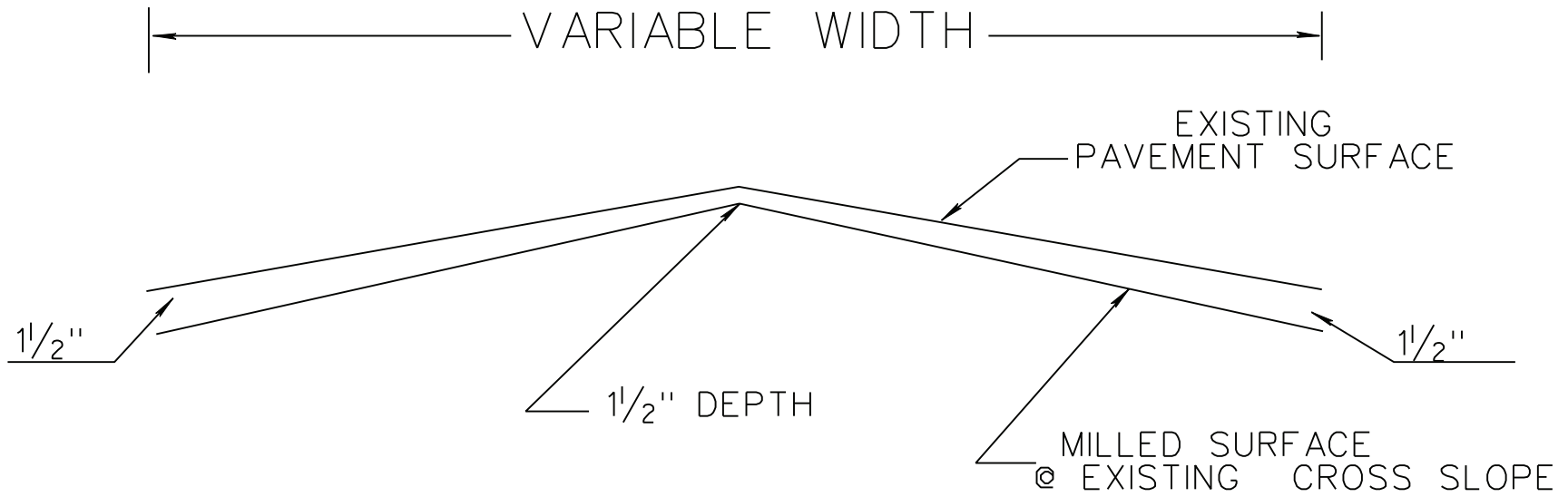
Notice To Bidders No. 3560-- Cont'd.

# TYPICAL MILLED TRANSITION AT BRIDGE ABUT. OR PAVEMENT IN PLACE

71

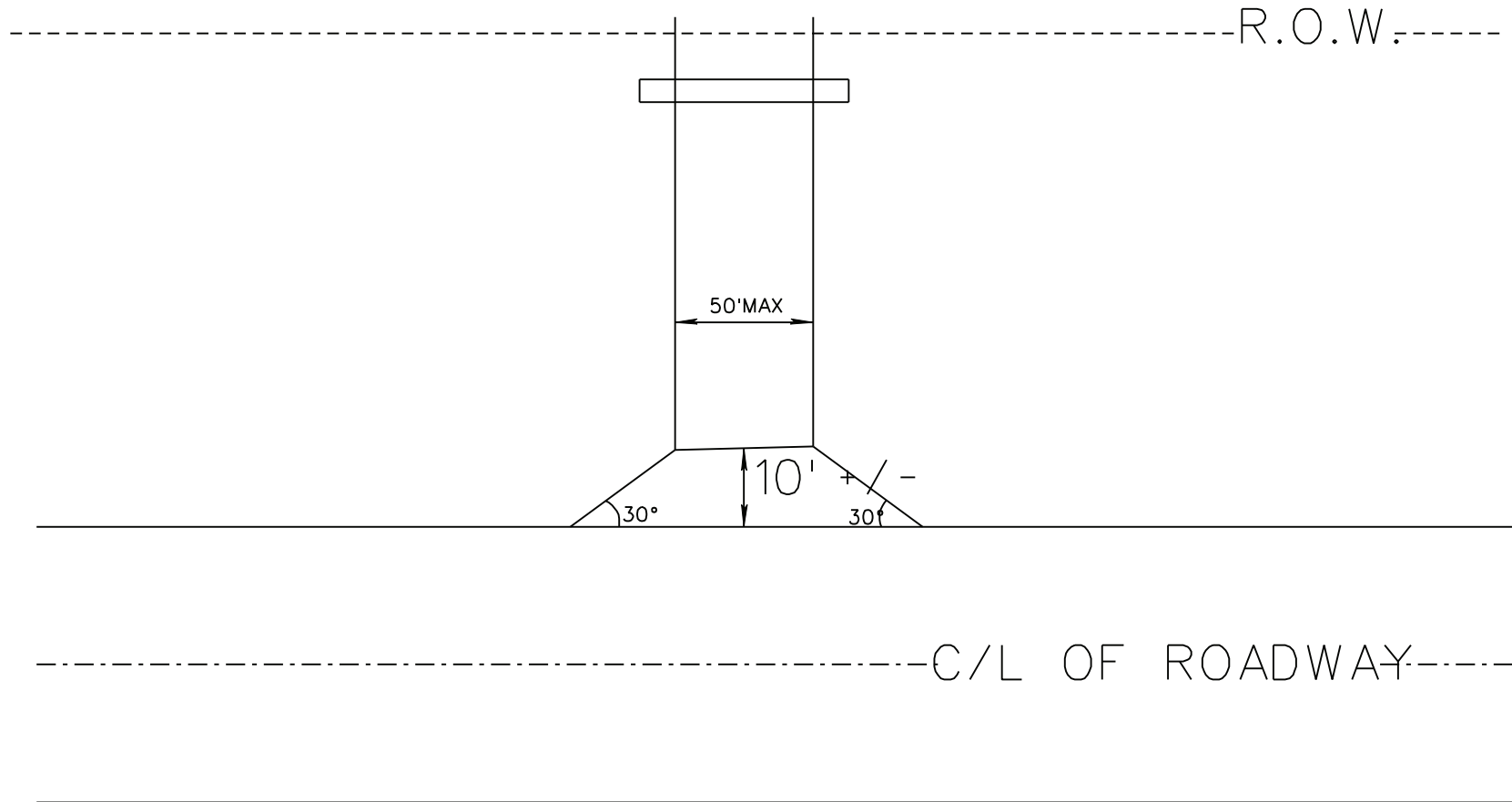


# TYPICAL MILLING DIAGRAM

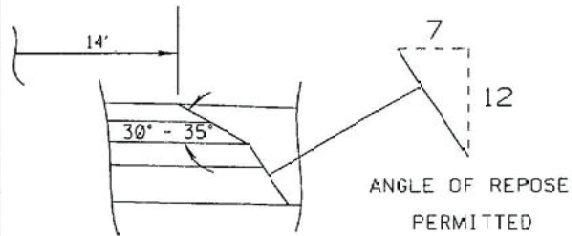
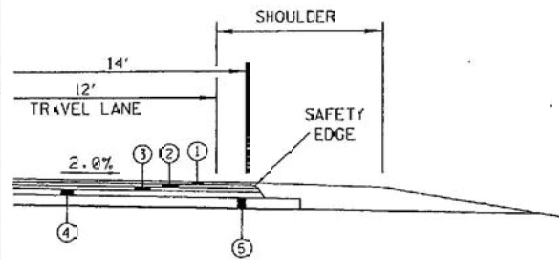




# PRIVATE DRIVEWAY DETAIL

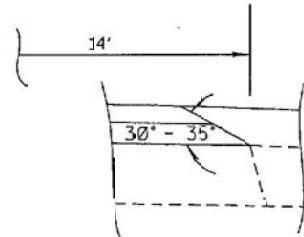
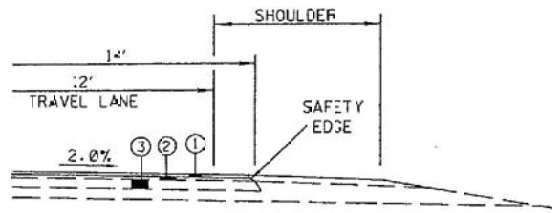


73



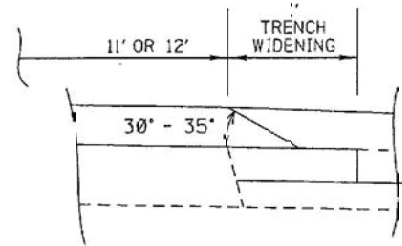
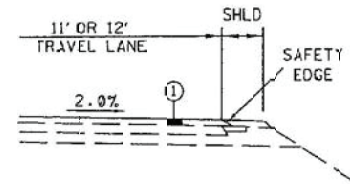
SAFETY EDGE REQ'D  
TOP 2 LIFTS ONLY  
(NOT A PAY ITEM)

**NEW CONSTRUCTION**



SAFETY EDGE REQ'D  
TOP 2 LIFTS ONLY  
(NOT A PAY ITEM)

**OVERLAY  
14' PAVEMENT**

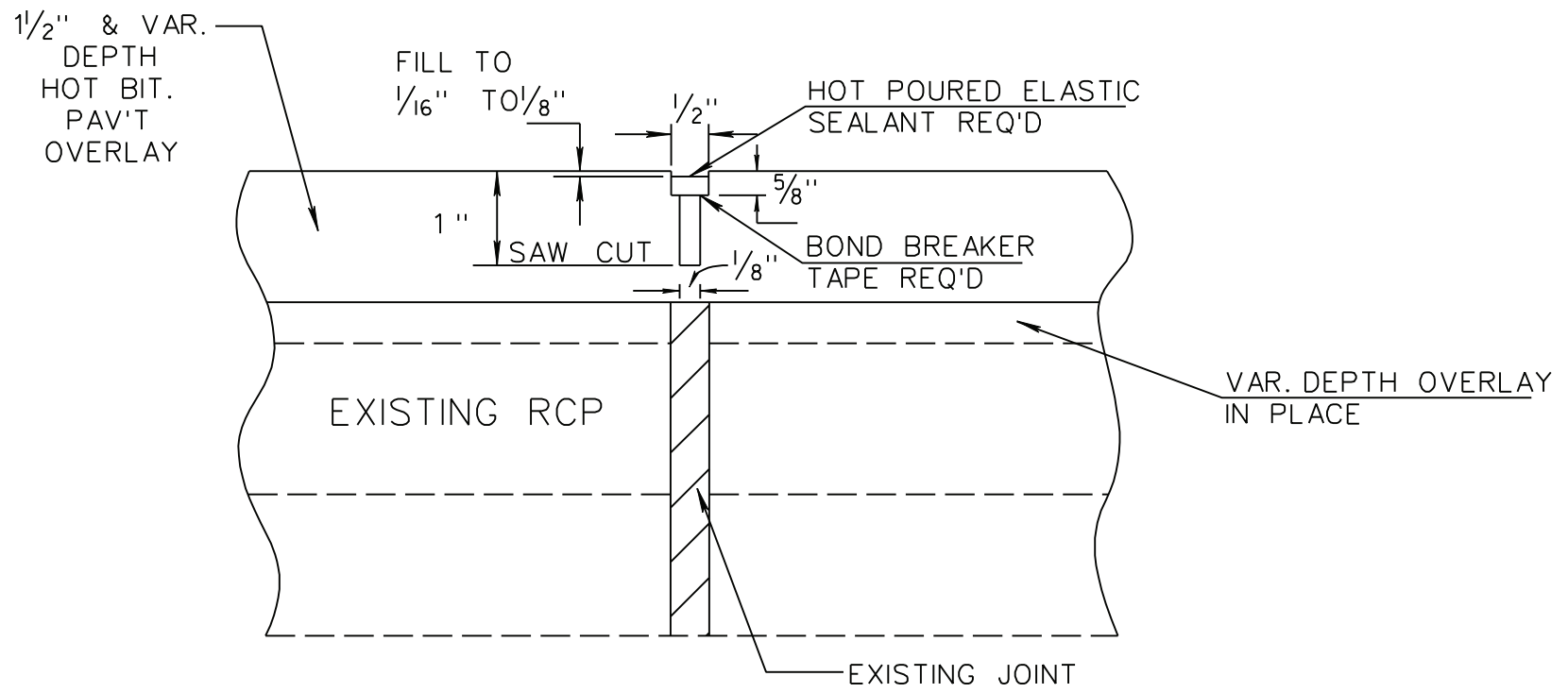


SAFETY EDGE REQ'D  
(NOT A PAY ITEM)

**OVERLAY  
11' OR 12' PAVEMENT**

**SAFETY EDGE DETAILS**

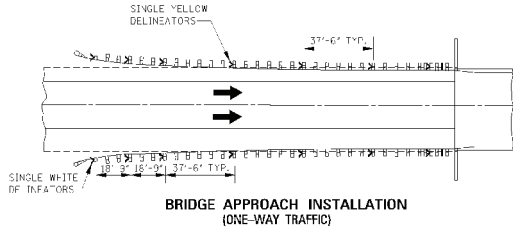
**3/31/2011**



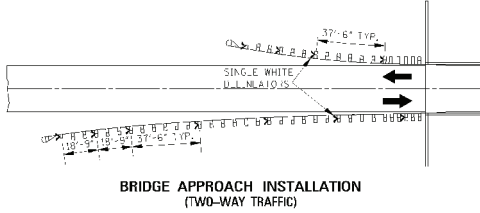
DETAIL OF SAWING AND SEALING TRANSVERSE JOINTS

US HWY 11

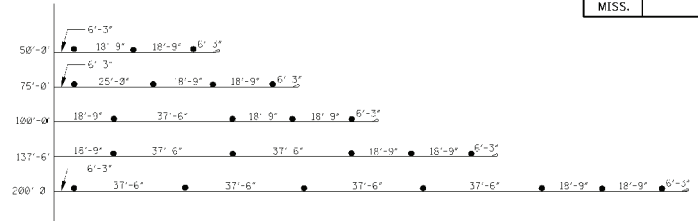
STATE	PROJECT NO.
MISS.	



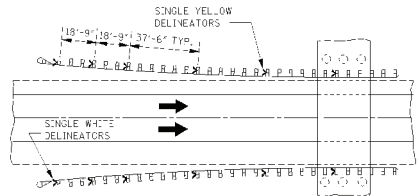
**BRIDGE APPROACH INSTALLATION  
(ONE-WAY TRAFFIC)**



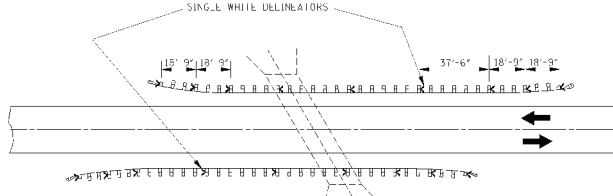
**BRIDGE APPROACH INSTALLATION  
(TWO-WAY TRAFFIC)**



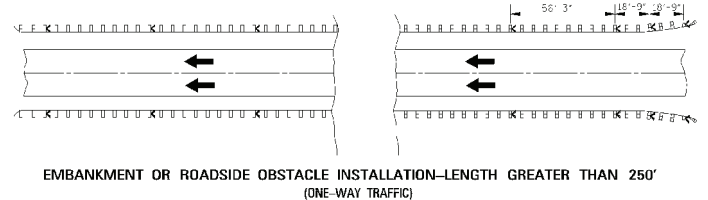
**GRAPHIC SHOWING SPACINGS OF GUARDRAIL DELINEATORS  
AT SOME COMMONLY USED BRIDGE APPROACHES**



**ROADSIDE OBSTACLE INSTALLATION—LENGTH 250' OR LESS  
(ONE-WAY TRAFFIC)**



**ROADSIDE OBSTACLE INSTALLATION—LENGTH 250' OR LESS  
(TWO-WAY TRAFFIC)**

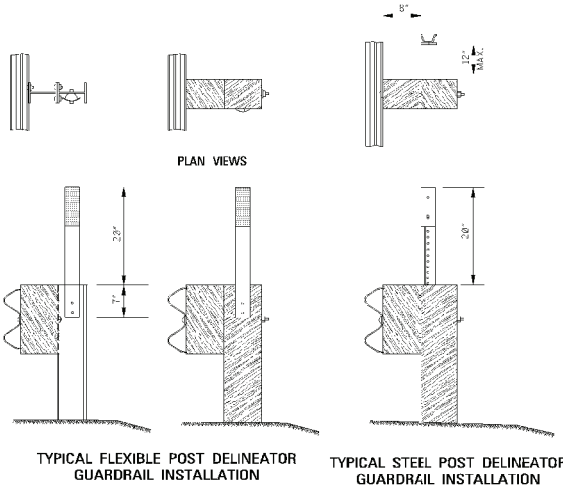


**EMBANKMENT OR ROADSIDE OBSTACLE INSTALLATION—LENGTH GREATER THAN 250'  
(ONE-WAY TRAFFIC)**

NOTE: ONE-WAY TRAFFIC SHOWN. DELINEATOR SPACING FOR TWO-WAY TRAFFIC SIMILAR. DELINEATOR COLOR WILL BE THE SAME AS THE ADJACENT PAVEMENT EDGE MARKING. THE FIRST THREE (3) MARKERS WILL FACE TRAFFIC IN OFF-LANE FOR TWO-WAY TRAFFIC AS SHOWN IN DRAWING FOR OBSTACLE INSTALLATION FOR TWO-WAY TRAFFIC.

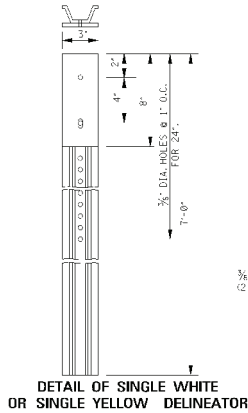
**GENERAL NOTES:**

1. THE UNIT PRICE OF DELINEATOR INCLUDES: COST(S) OF DELINEATOR FACE(S), POST, HARDWARE AND INSTALLATION.
2. DELINEATOR FACE WILL BE ENCAPSULATED LENS REFLECTIVE SHEETING.
3. DELINEATORS FOR GUARDRAIL SHALL BE MOUNTED ON STEEL POSTS ON FLEXIBLE POSTS AS FOLLOWS:
  - 3A. DELINEATORS ON STEEL POSTS:
    1. DELINEATOR FACE SHALL BE PLACED ON 0.080" THICK SHEET ALUMINUM OR 1/4" GAUGE GALVANIZED SHEET STEEL.
    2. DELINEATOR POSTS SHALL BE GALVANIZED STEEL U-SECTION POSTS (2.0 LB/FT) TO 2.5 LB/FT. THE POSTS ARE TO BE FABRICATED BEFORE THE METAL IS GALVANIZED. RADII IN BENDS OF POST CROSS SECTION NOT TO EXCEED 1/4" FOR 90 DEGREE SECTION.
    3. WHEN COVER OVER A DRAINAGE STRUCTURE IS LESS THAN THE NORMAL DELINEATOR POST DEPTH, THE DELINEATOR POSTS WILL BE FIELD CUT, DRILLED AND FASTENED TO THE BACK OF THE GUARDRAIL POSTS WITH (2) 1/4" DIA. LAG SCREWS.
  - 3B. DELINEATORS ON FLEXIBLE POSTS:
    1. THE DELINEATOR POSTS WILL BE FROM THE DEPARTMENT'S "APPROVED SOURCE OF MATERIALS" AND WILL BE FASTENED TO GUARDRAIL POST IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION.

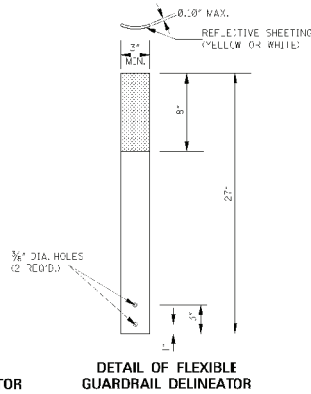


**TYPICAL FLEXIBLE POST DELINEATOR  
GUARDRAIL INSTALLATION**

**TYPICAL STEEL POST DELINEATOR  
GUARDRAIL INSTALLATION**



**DETAIL OF SINGLE WHITE  
OR SINGLE YELLOW DELINEATOR**



**DETAIL OF FLEXIBLE  
GUARDRAIL DELINEATOR**

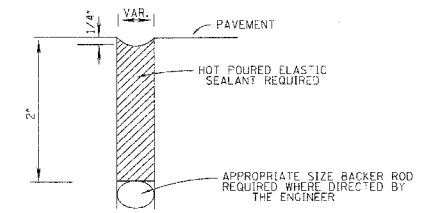
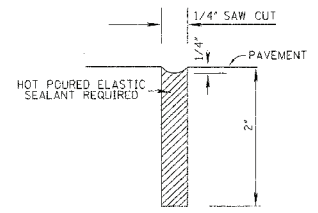
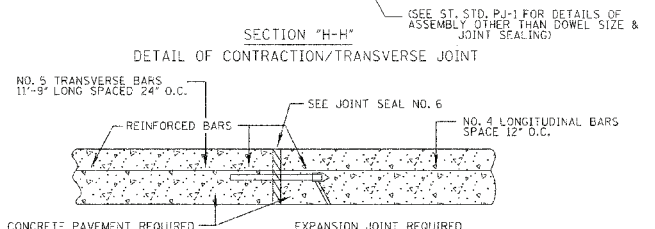
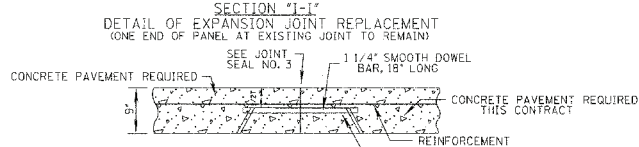
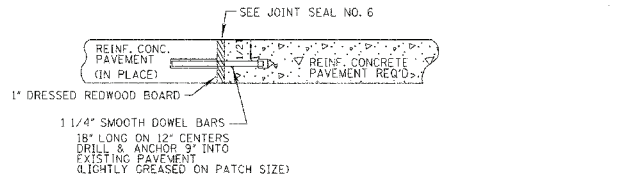
DATE	REVISION	BY	APP'D.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

**TYPICAL GUARDRAIL  
DELINEATOR**

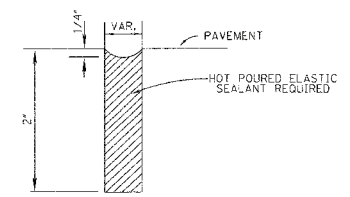
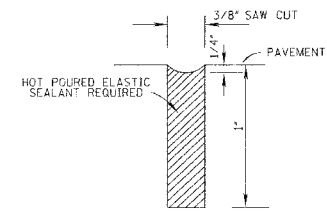
ISSUE DATE: OCTOBER 1, 1998

WORKING NUMBER: S-1-8C  
SHEET NUMBER: 236



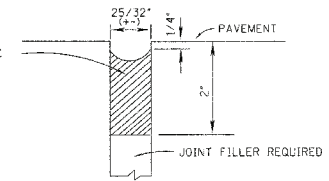
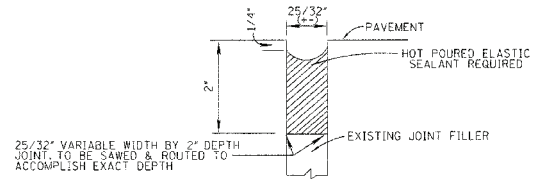
DETAIL OF LONGITUDINAL JOINTS  
 SEALING NEW CONSTRUCTION JOINTS JOINT SEAL NO. 1

CLEANING NEW CONSTRUCTION JOINTS JOINT SEAL NO. 2



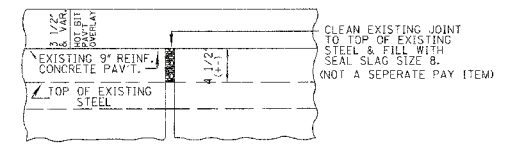
DETAIL OF CONTRACTION JOINTS  
 SEALING NEW CONSTRUCTION JOINTS JOINT SEAL NO. 3

SEALING NEW CONSTRUCTION JOINTS JOINT SEAL NO. 4



DETAIL OF EXPANSION JOINTS  
 CLEAN & SEAL EXISTING EXPANSION JOINTS JOINT SEAL NO. 5

SEAL NEW CONSTRUCTION TRANSVERSE EXPANSION JOINTS JOINT REPAIR NO. 6

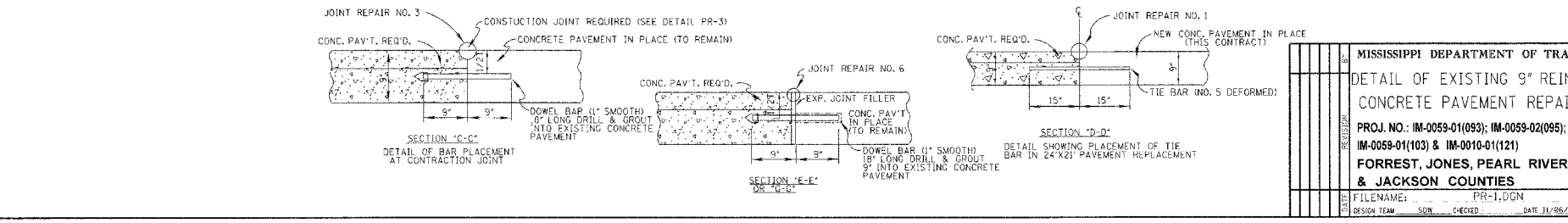
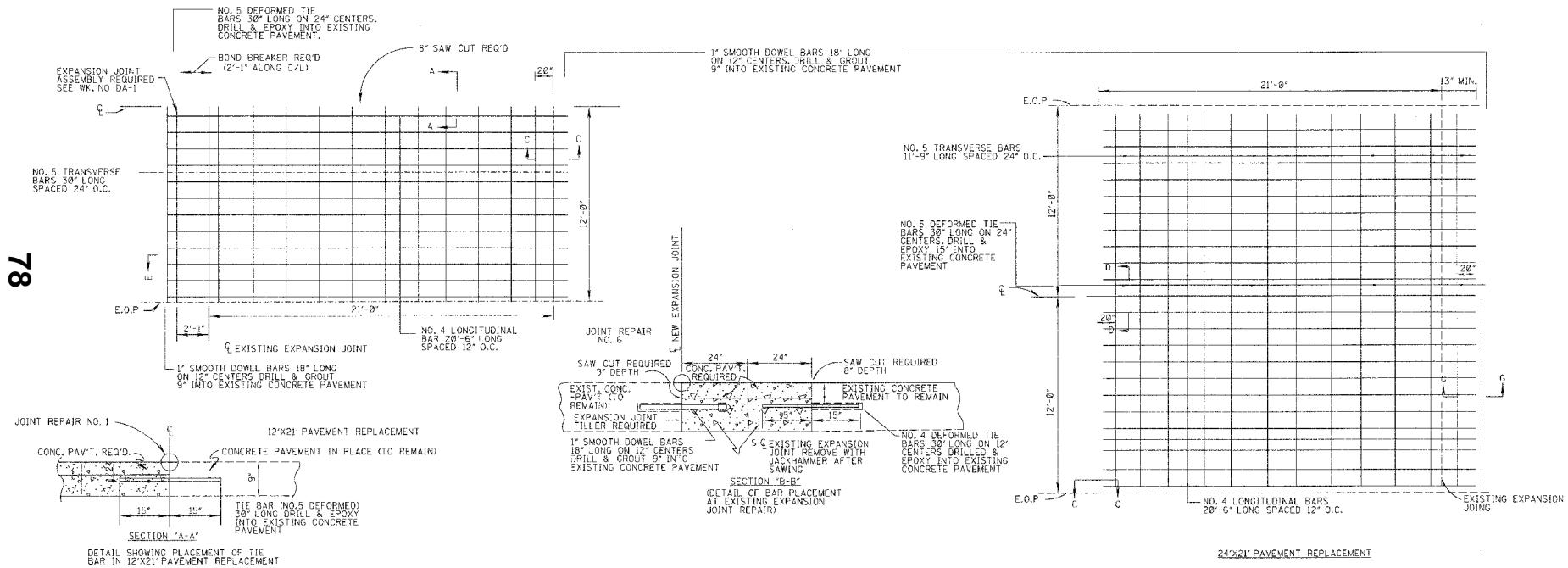
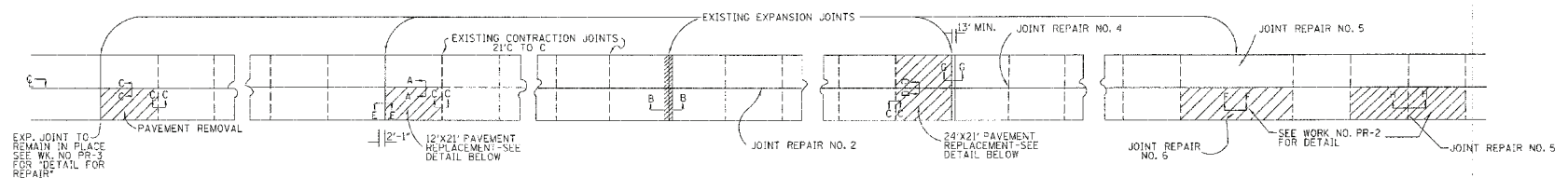


DETAILS OF CLEANING AND FILLING JOINTS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
DETAIL OF EXISTING REINF. CONCRETE PAVEMENT JOINT REPAIR	
PROJ. NO.: IM-0059-01(093); IM-0059-02(095); IM-0059-01(103) & IM-0010-01(121)	
FORREST, JONES, PEARL RIVER & JACKSON COUNTIES	
WORKING NUMBER	2
SHEET NUMBER	2
DESIGN TEAM	SDW
CHECKED	DATE: 11/01/99



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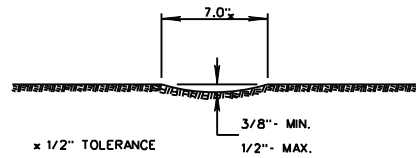
78

- 18 -

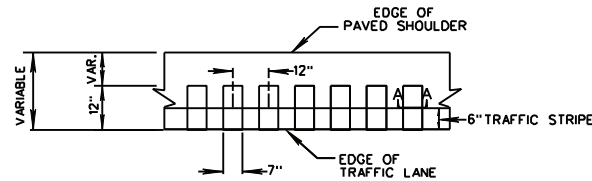
Notice To Bidders  
Mississippi Department of Transportation

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
DETAIL OF EXISTING 9" REINFORCED CONCRETE PAVEMENT REPAIR	
PROJ. NO.: IM-0059-01(093); IM-0059-02(095); IM-0059-01(103) & IM-0010-01(121)	
FORREST, JONES, PEARL RIVER & JACKSON COUNTIES	
WORK NUMBER	1
SHEET NUMBER	1
FILENAME:	PR-1.DGN
DESIGN TEAM:	SDW
CHECKED:	DATE 11/06/03

STATE	PROJECT NO.
MISS.	

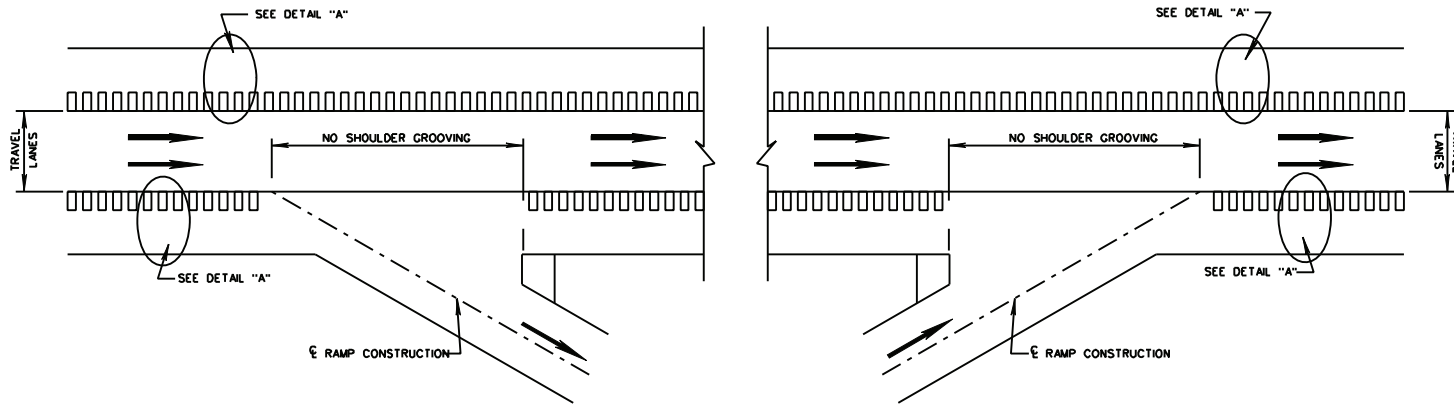


SECTION "A-A"



DETAIL "A"

- GENERAL NOTES
1. GROUND-IN RUMBLE STRIPS SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
  4. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED TO MAINLINE ONLY.



PLAN  
NOT TO SCALE  
DETAILS OF  
RUMBLE STRIPS  
(GROUND-IN)

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
DATE		RUMBLE STRIPE FOR 4 LANE HIGHWAYS	
DATE	FILENAME:	WORKING NUMBER	RS-41
DESIGN TEAM	CHECKED	SHEET NUMBER	
	DATE 03/04/11		

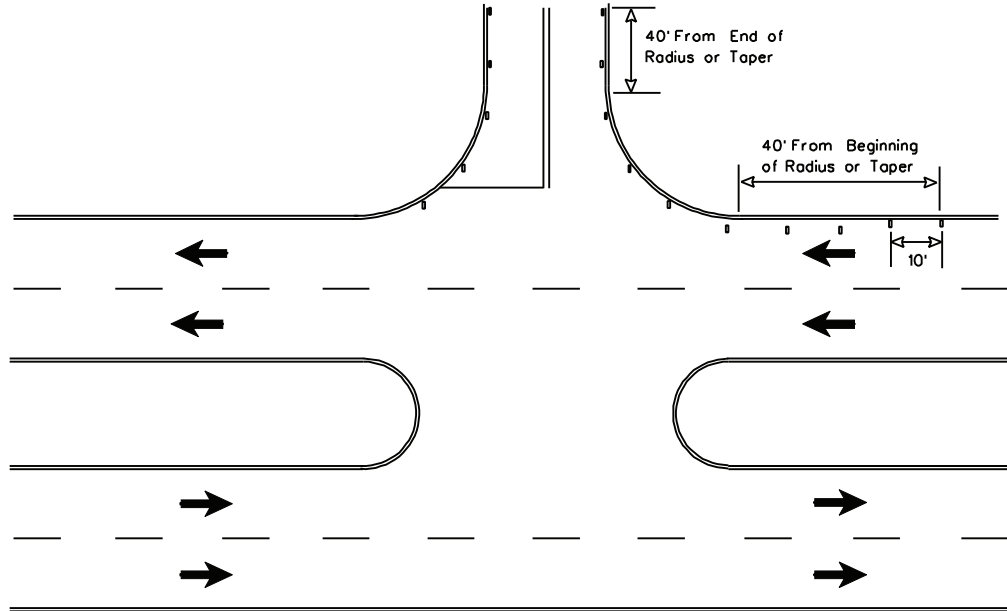
79

- 19 -

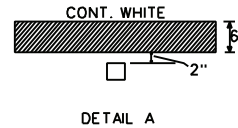
Notice To Bidders No. 3520 - Cont'd.

STATE	PROJECT NO.
MISS.	

## TYPICAL FOR RAISED PAVEMENT MARKERS PLACED ON SIDE ROAD RADIUS



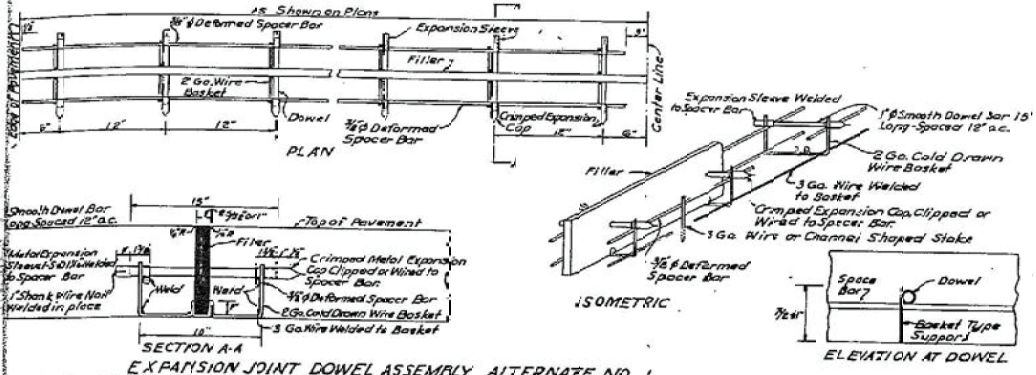
- NOTE 1. MARKERS SHALL BE PLACED EVERY 10 FEET.
- NOTE 2. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
- NOTE 3. MARKERS SHALL BE HIGH PERFORMANCE TWO WAY CLEAR.
- NOTE 4. CHANNELIZED INTERSECTIONS WILL REQUIRE MORE MARKERS THAN STANDARD INTERSECTIONS DUE TO LONGER TAPER.
- NOTE 5. FIVE (5) MARKERS SHALL BE PLACED ON MAINLINE EDGE STRIPE.
- NOTE 6. MARKERS FOR COUNTY ROADS SHALL CONTINUE DOWN THE EDGE STRIPE A DISTANCE OF 40 FEET.



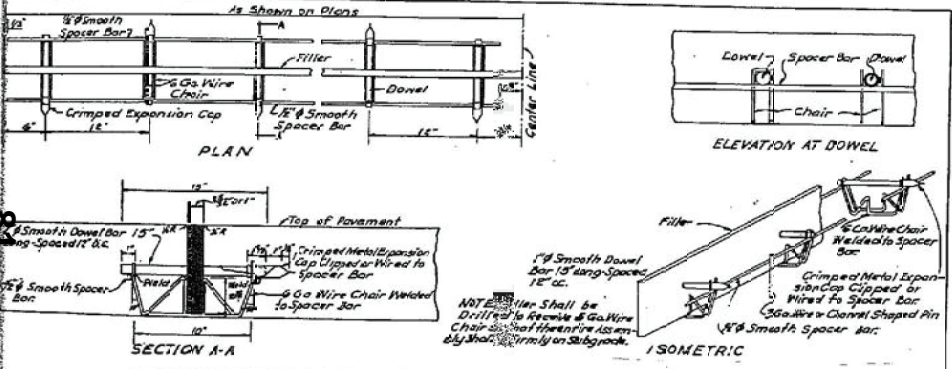
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
4-LANE	
2-WAY CLEAR RAISED PAVEMENT MARKERS PLACED ON SIDE ROADS	
DATE	WORKING NUMBER
FILENAME:	SHEET NUMBER
DESIGN TEAM	CHECKED
	DATE



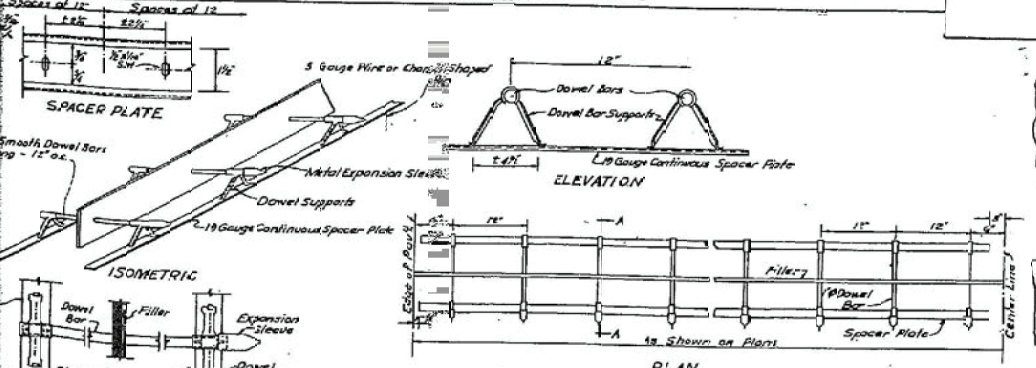
STATE	DATE	BY	REVISION
I-G	12-19-66		
DATE	BY	DATE	BY



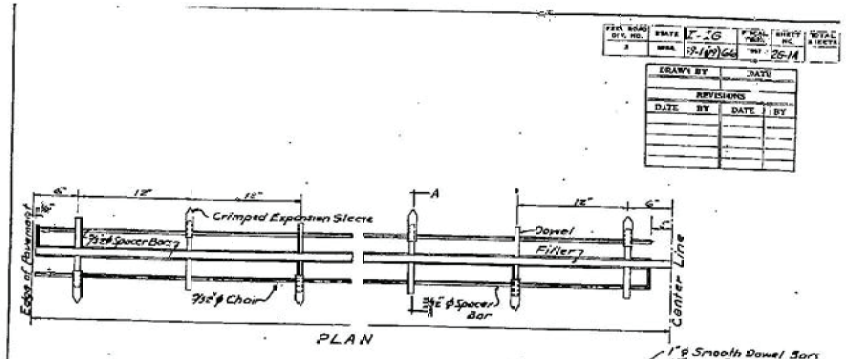
EXPANSION JOINT DOWEL ASSEMBLY ALTERNATE NO. 1



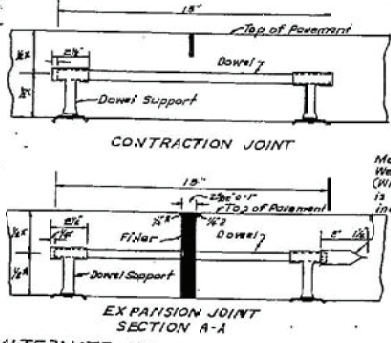
EXPANSION JOINT DOWEL ASSEMBLY ALTERNATE NO. 3



EXPANSION AND CONTRACTION JOINT DOWEL ASSEMBLY ALTERNATE NO. 4



EXPANSION AND CONTRACTION JOINT DOWEL ASSEMBLY ALTERNATE NO. 5

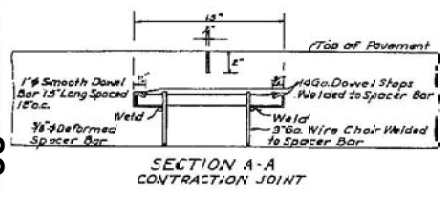
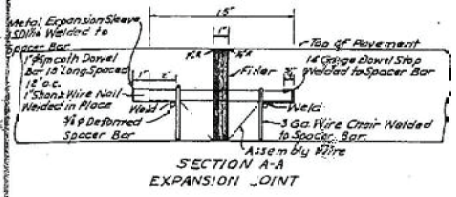
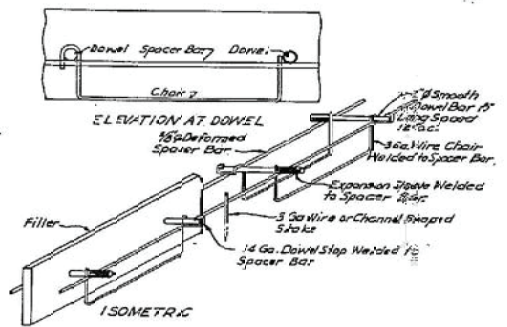
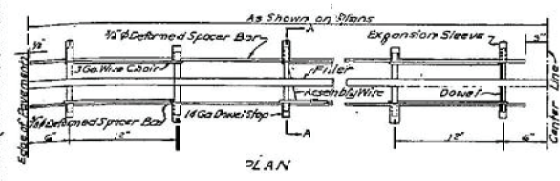


Metal Expansion Sleeve Welded to Dowel Support (When Aluminum Expansion Sleeve is used it shall be Crimped to Dowel Support in Satisfactory Manner with a Standard Crimping Tool)

SPECIAL DESIGN DOWEL ASSEMBLIES FOR TRANSVERSE JOINTS

- 21 - Notice To Bidders No. 3560--Cont'd

DRAWN BY		DATE	
REVISIONS			
DATE	BY	DATE	BY



DOWEL ASSEMBLY ALTERNATE NO. 7

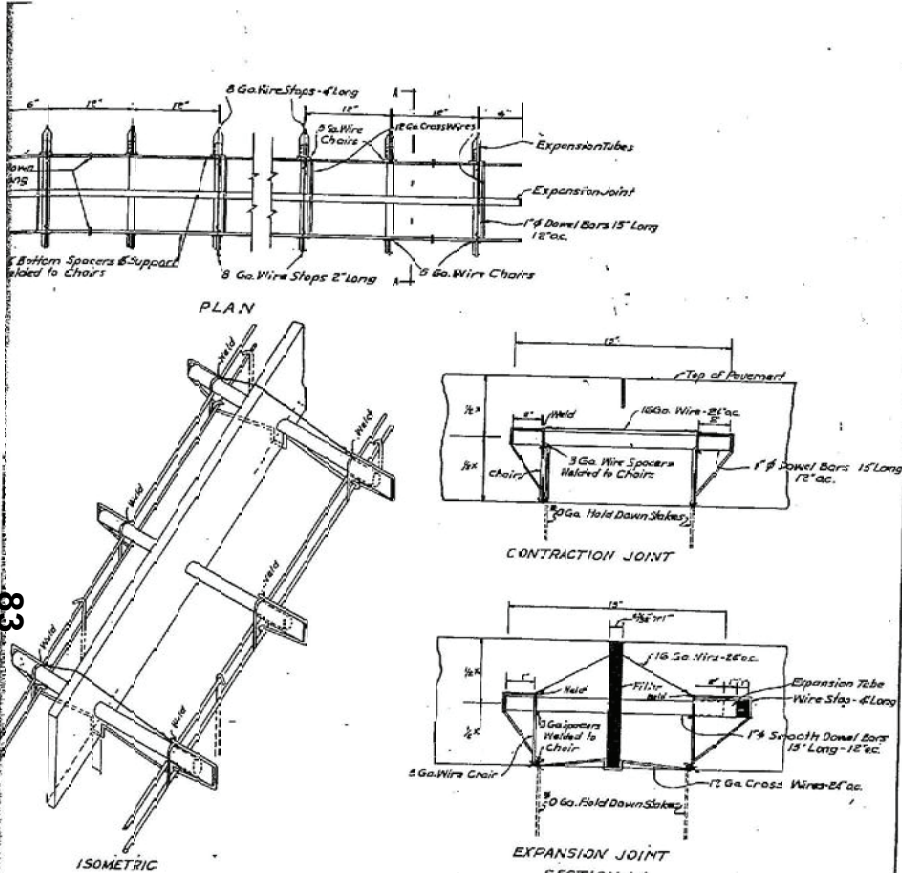
82

SPECIAL DESIGN DOWEL ASSEMBLIES FOR TRANSVERSE JOINTS		DRAWN BY DATE	CHECKED BY DATE
		DESIGNED BY DATE	APPROVED BY DATE
OWNER OF PAVEMENT DIVISION		DATE	

I-IG-53-1(12)46

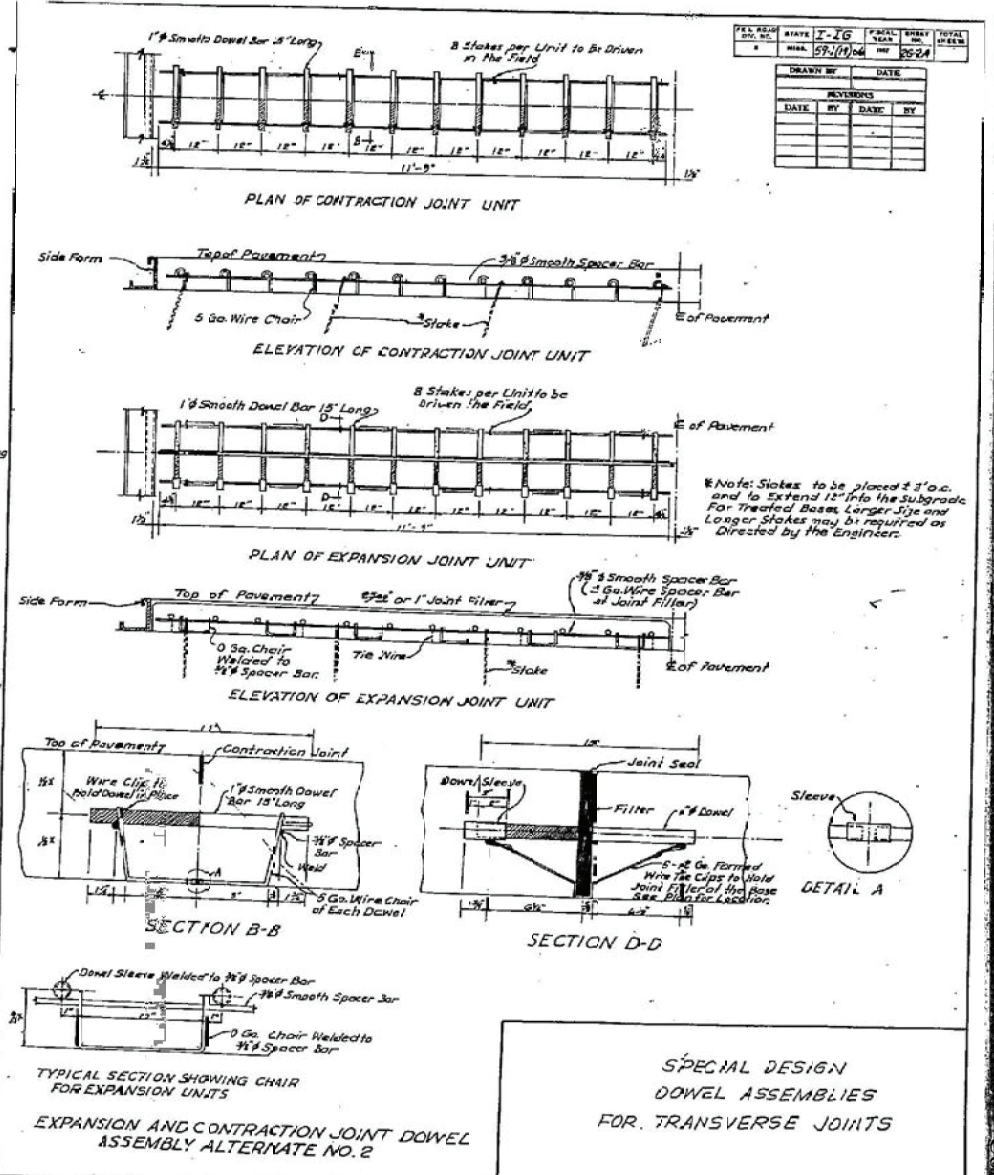
26-4A





NOTE: Slips to be placed 2' oc and to extend 12" into the Subgrade. For Treated Bases, Larger Size and Longer Slips may be Required as Directed by the Engineer.

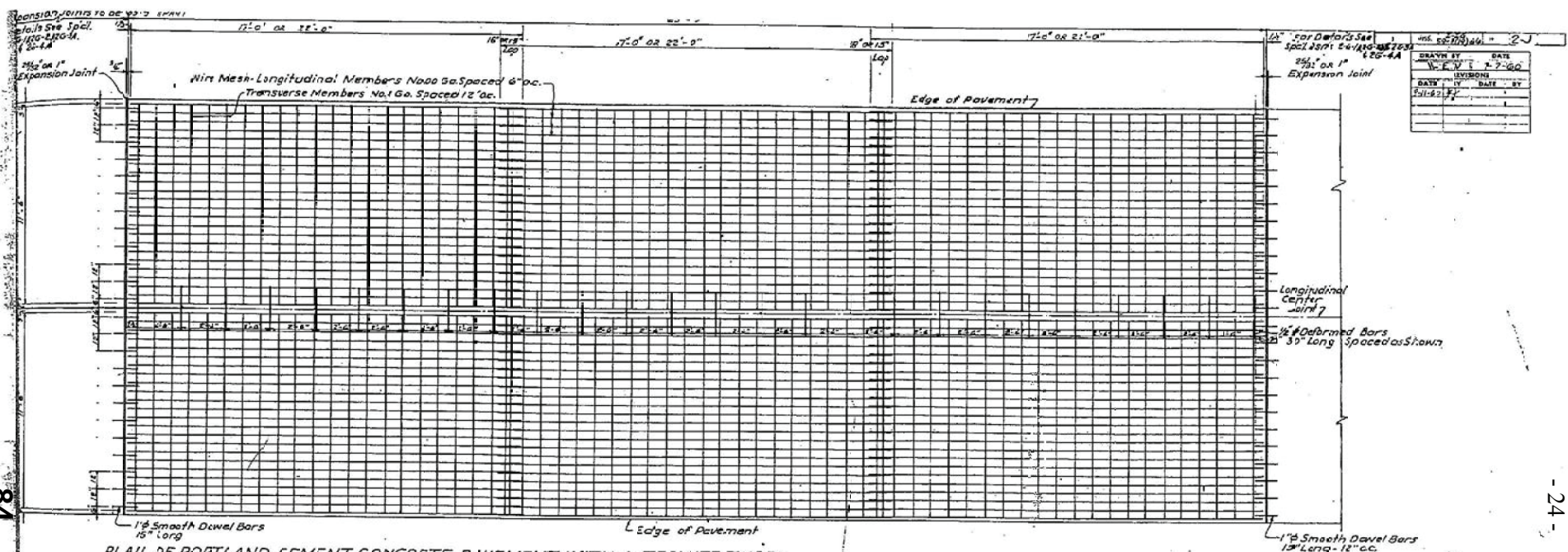
EXPANSION AND CONTRACTION JOINT DOWEL ASSEMBLY  
ALTERNATE NO. 6



TYPICAL SECTION SHOWING CHAIR  
FOR EXPANSION UNITS  
EXPANSION AND CONTRACTION JOINT DOWEL  
ASSEMBLY ALTERNATE NO. 2

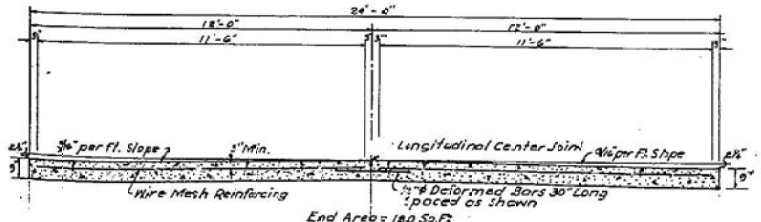
SPECIAL DESIGN  
DOWEL ASSEMBLIES  
FOR TRANSVERSE JOINTS

84

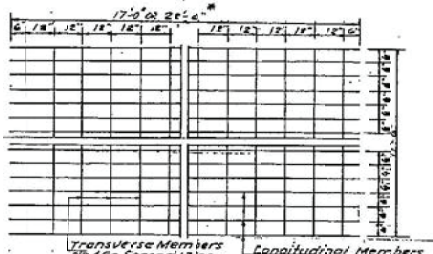


DESIGNED BY	DATE
W. E. T. 12-20-60	
CHECKED BY	DATE

PLAN OF PORTLAND CEMENT CONCRETE PAVEMENT WITH ALTERNATE TYPES OF LONGITUDINAL & TRANSVERSE JOINTS AS SHOWN ON SPECIAL DESIGN 2-G1A, 2-G2A, 2-G3A REINFORCED WITH NO. 006A (LONGITUDINAL) & NO. 4 GA. (TRANSVERSE) WIRE MESH  
SCALE: 3/8" = 1'-0"



SECTION OF PORTLAND CEMENT CONCRETE PAVEMENT (UNIFORM)  
SCALE: 3/8" = 1'-0"



DETAIL FOR SHEARING WIRE MESH  
Weight of Mesh = 78 Lbs.

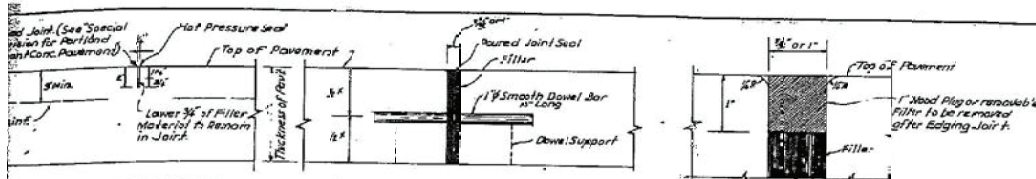
GENERAL NOTES: The Standard Specifications of the Mississippi State Highway Department shall, unless otherwise specified herein, apply to all items on this sheet. Reinforcement shall be electrically welded steel wire fabric (Longitudinal Members No. 006 GA. Spaced 6" oc. - Transverse Members No. 4 GA. spaced 12" oc.)  
A) Longitudinal Joints, Transverse Joints and Dowel Bars shall conform to the requirements of the Standard Specifications and Special Designs 2-G-1A, 2-G-2A, 2-G-3A, 2-G-4A  
Curing shall be performed in accordance with the requirements of Section 110 of the Standard Specifications.  
Weight of Mesh per 100 Sq. Ft. = 78 Lbs.  
\* CONTRACTOR MAY USE FOUR PANELS OF MESH, EACH 17 FEET LONG AND LAPPED 18" OR THREE PANELS, EACH 22 FEET LONG AND LAPPED 15"

SPECIAL DESIGN PAVING DETAIL

T-16-59-1(10)66

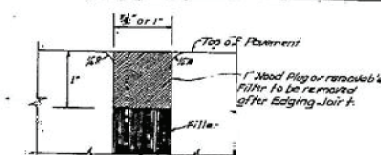
- 24 -  
Newice To Bidders No. 3560-Cont



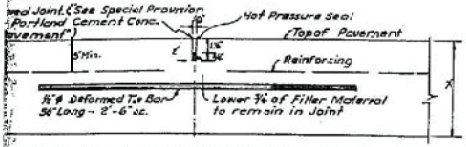


DETAIL OF SAVED TRANSVERSE CONTRACTION JOINT (ALT. I)

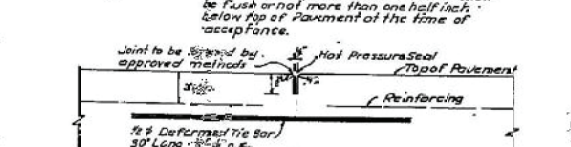
TRANSVERSE EXPANSION JOINT  
 Joints Spaced as indicated on Plans or Directed by the Engineer and at right angles to Center Line of Roadway



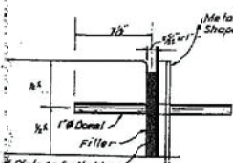
DETAIL AT TRANSVERSE EXPANSION JOINT  
 Note: The Joint shall be Sealed with the Filler prescribed for paved joints. The seal shall be flush or not more than one half inch below top of Pavement at the time of acceptance.



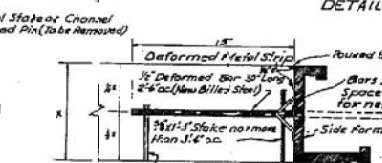
DETAIL OF SAVED LONGITUDINAL JOINT (ALT. I)



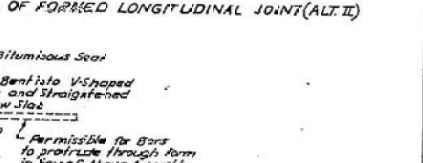
DETAIL OF FORMED LONGITUDINAL JOINT (ALT. II)



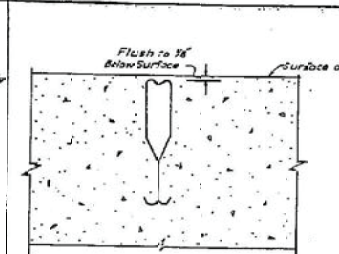
TRANSVERSE CONSTRUCTION JOINT



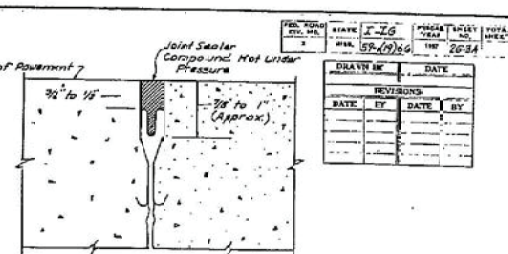
LONGITUDINAL CONSTRUCTION JOINT (ALT. I)



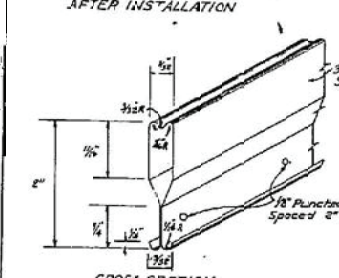
LONGITUDINAL CONSTRUCTION JOINT (ALTERNATE II)



CROSS SECTION IMMEDIATELY AFTER INSTALLATION



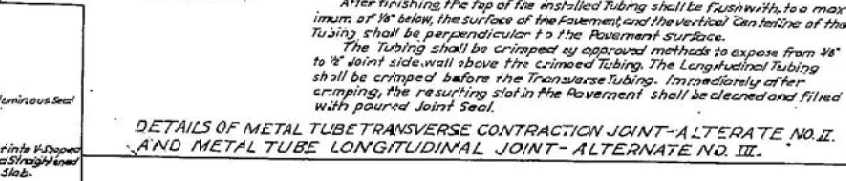
CROSS SECTION AFTER CRIMPING AND SEALING



CROSS SECTION

GENERAL NOTES: All open ends of Tubing shall be plugged with Styrofoam Plugs prior to installation to prevent Grout from entering.  
 Longitudinal Tubing shall be installed parallel to side forms. The matched end of each additional length of Longitudinal Tubing shall be inserted into the open end of the previously installed length, to form a continuous Tube.  
 All Tubing (Longitudinal and Transverse) shall be installed by improved methods immediately following the finishing machinery, and ahead of final finishing.  
 Transverse Tubing shall be installed so that the two lengths on either side of the Longitudinal Tubing are not of alignment by more than 1/8". Styrofoam Plugs inserted into the open ends of Tubing shall be extended to provide a snug fit between the side forms and Longitudinal Tubing.  
 After finishing the top of the installed Tubing shall be flush with, to a maximum of 1/8" below, the surface of the pavement and the vertical centerline of the Tubing shall be perpendicular to the pavement surface.  
 The Tubing shall be crimped by approved methods to expose from 1/8" to 1/2" joint side wall above the crimped Tubing. The Longitudinal Tubing shall be crimped before the Transverse Tubing. Immediately after crimping, the resulting slot in the pavement shall be cleaned and filled with poured joint seal.

DETAILS OF METAL TUBE TRANSVERSE CONTRACTION JOINT-ALTERNATE NO. II AND METAL TUBE LONGITUDINAL JOINT-ALTERNATE NO. III.



DIMENSIONS OF SOFTWOOD AND PREMOLED FILLER TRANSVERSE JOINT

GENERAL NOTES: THE STANDARD SPECIFICATIONS ADOPTED BY THE MISSISSIPPI STATE HIGHWAY DEPARTMENT SHALL, UNLESS OTHERWISE SPECIFIED HEREIN, APPLY TO ALL ITEMS.  
 THE SOFTWOOD OR PREMOLED FILLER SHALL BE FURNISHED IN FULL DEPTH AND THICKNESS STRIPS (SEE DIMENSIONS ON PLANS) AND IN LENGTHS EQUIVALENT TO THE DISTANCE BETWEEN LONGITUDINAL JOINTS EXCEPT WHERE EXTRA WIDTH IS REQUIRED.  
 WHERE MORE THAN ONE SECTION IS REQUIRED IN A JOINT, THE SECTIONS SHALL BE SECURELY LAGED OR CLIPPED TOGETHER TO THE SATISFACTION OF THE ENGINEER.  
 THE EXPANSION AND CONTRACTION JOINT DOWEL ASSEMBLIES USED ON THIS PROJECT SHALL BE THE RESPECTIVE EXPANSION AND CONTRACTION JOINTS DOWEL ASSEMBLIES SPECIFIED AT THE TIME THE BID IS SUBMITTED AS INDICATED IN THE PROPOSAL BY THE BIDDER. THE DOWEL BARS IN ASSEMBLIES SHALL BE PAINTED WITH AN APPROVED RED LEAD PAINT (CODE RL) AND SHALL BE GREASED WITH A HEAVY GREASE EQUIVALENT TO 9000 OIL CRIP GREASE TO PREVENT BONDING WITH CONCRETE. THE DOWEL SPACERS AND SUPPORTS SHALL BE ASSEMBLED AS A COMPLETE UNIT PRIOR TO INSTALLATION ON SUBGRADE WHEN USED AT CONSTRUCTION JOINTS. THE DIMENSIONS AND SPECIFICATIONS OF THE DOWEL ASSEMBLIES SHALL JOIN THE SAME.  
 FOR DOWEL ORNATES SEE AASHTO STANDARDS OR SPECIAL DESIGN DRAWING DETAILS.  
 THE 1/2" BOWLS SHALL BE HANG OR SHARPED BULLET STEEL OR RAIL STEEL BARS CONFORMING TO THE REQUIREMENTS OF SECTION 800.03 OF THE STANDARD SPECIFICATIONS AND SHALL BE SHARP OR SHARPED FROM STOCK LENGTHS. THE ENDS OF SHARPED BARS SHALL BE VOID OF SHARP EDGES OR "BURRS". THE PLACEMENT OF BARS SHALL BE IN A LINE PARALLEL TO THE SURFACE OF THE PAVEMENT. THE TOLERANCE OF 1/8" IN THE VERTICAL DISTANCE FROM DOWEL TO FORM GRADE WILL BE PERMITTED.  
 ANY ITEM NOT SHOWN AS PAY ITEM SHALL BE AN ABSORBED ITEM.

SPECIAL DESIGN DETAILS  
 FOR LONGITUDINAL & TRANSVERSE JOINTS

Notice To Bidders No. 3560--Cont'd



**INDEX**

FOR INDEX SEE SHEET NO. 2

SPECIFICATIONS GOVERNING CONSTRUCTION OF THIS PROJECT SHALL BE THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, APPROVED BY THE MISSISSIPPI STATE HIGHWAY COMMISSION NOV. 20, 1956 AND APPROVED BY THE COMMISSIONER OF THE U. S. BUREAU OF PUBLIC ROADS.

**I-SECTION EQUATIONS**

STA 207+48.25 = STA 408+48.25 -100.0 FT

**IG-SECTION EQUATIONS**

NONE

**LENGTH DATA - I SECTION**

LENGTH OF ROADWAY	3242.82 FT	0.140 MI.
LENGTH OF BRIDGES	NONE	0.000 MI.
LENGTH OF PROJECT (NET)	6.14 MI.	
LENGTH OF EXCEPTIONS	1507.68 FT	0.285 MI.
LENGTH OF PROJECT (GROSS)	6.426 MI.	

STA 261+87.85 BEGINNING OF PROJECT  
 STA 261+87.85 BEGINNING OF FED. AID PROJ. NO. 1-IG-59-1(19) 66  
 STA 261+87.85 END OF FED. AID PROJ. NO. 1-IG-59-1(19)

**STATE OF MISSISSIPPI  
 STATE HIGHWAY DEPARTMENT**

**PLAN AND PROFILE OF PROPOSED  
 STATE HIGHWAY  
 FEDERAL AID PROJECT NO. I-IG-59-1(19) 66**

INTERSTATE HWY. NO. 59 BETWEEN  
 LAMAR-FORREST CO. LINE AND FORREST-JONES CO. LINE  
**FORREST COUNTY**

**LENGTH DATA - IG-SECTION**

LENGTH OF ROADWAY	1000.00 FT	0.189 MI.
LENGTH OF BRIDGES	NONE	0.000 MI.
LENGTH OF PROJECT (NET)	0.189 MI.	
LENGTH OF EXCEPTIONS	150.00 FT	0.028 MI.
LENGTH OF PROJECT (GROSS)	0.217 MI.	

COMPUTED FOR RT. LANE ONLY  
 (OMITTED SECTION) - EXCEPTIONS

**I-SECTION**

STA 305+05.25 TO STA 305+13.75	229.35 FT
STA 360+70.00 TO STA 361+50.00	1080.00 FT
STA 527+75.25 TO STA 529+73.25	190.00 FT
<b>TOTAL</b>	1507.68 FT = 0.285 MI.

**IG-SECTION**

STA 317+95.25 TO STA 319+45.25	150.00 FT = 0.028 MI.
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**SCALES**

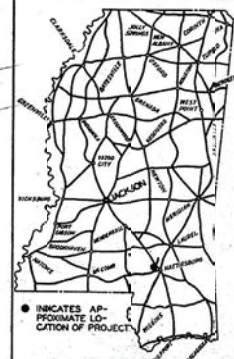
PLAN	1 IN. = 100 FT.
PROFILE (HOR.)	1 IN. = 100 FT.
PROFILE (VER.)	1 IN. = 10 FT.
LAYOUT	1 IN. = 2000 FT.

Contractor Brown & Root  
 (Address) 11111 Highway 59, Hattiesburg, Miss.  
 Dist. Eng. W. J. ...  
 Proj. Eng. ...  
 Work Started on ...  
 Work Accepted on ...

FED. PROJ. NO.	STATE	FEDERAL PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
9	MISS.	1-IG-59-1(19)	66	7	

DATE	BY	
5-9-60	E.R.B.	
REVISIONS	DATE	BY

REV. TO ELIMINATE URBAN SECTION.

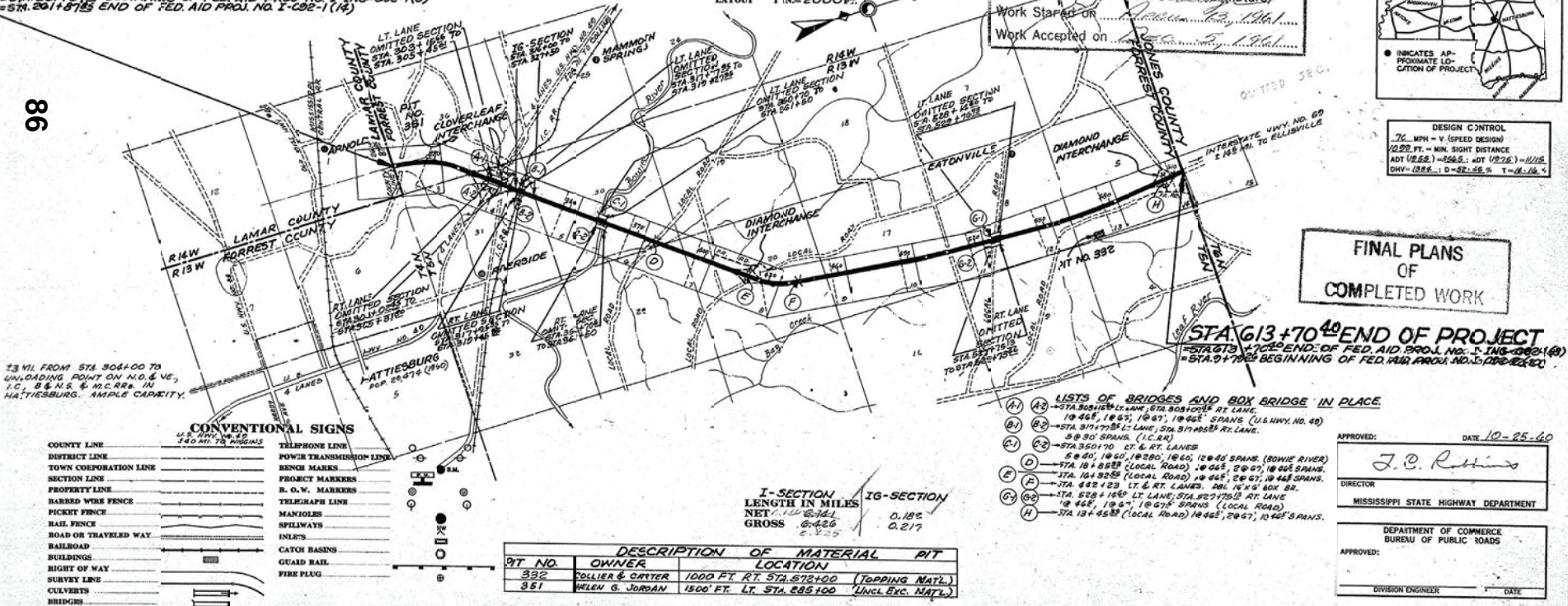


DESIGN CONTROL  
 70 MPH = V (SPEED DESIGN)  
 1050 FT. = MIN. SIGHT DISTANCE  
 ADT (25%) = 2500; ADT (10%) = 1000  
 DIVY (25%) = 0-52-55% T=16.14"

**FINAL PLANS  
 OF  
 COMPLETED WORK**

STA 613+70.40 END OF PROJECT  
 STA 613+70.40 END OF FED. AID PROJ. NO. 1-IG-59-1(19) 66  
 STA 613+70.40 BEGINNING OF FED. AID PROJ. NO. 1-IG-59-1(19) 66

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**CONVENTIONAL SIGNS**

COUNTY LINE	TELEPHONE LINE
DISTRICT LINE	POWER TRANSMISSION LINE
TOWN CORPORATION LINE	RESHO MARKS
SECTION LINE	PROJECT MARKERS
PROPERTY LINE	R. O. V. MARKERS
BARBED WIRE FENCE	TELEGRAPH LINE
PICKET FENCE	MANHOLES
RAIL FENCE	SPELLWAYS
ROAD OR TRAVELED WAY	INLETS
RAILROAD	CATCH BASINS
BUILDINGS	GUARD RAIL
RIGHT OF WAY	FIRE FLUG
SURVEY LINE	
CULVERTS	
BRIDGES	

I-SECTION LENGTH IN MILES	IG-SECTION
NET 0.140	0.182
GROSS 0.426	0.217

**DESCRIPTION OF MATERIAL PIT**

PIT NO.	OWNER	LOCATION
332	COLLIER & CARTER	1000 FT. RT. STA. 613+100 (TOPPING MATL.)
351	HELEN G. JORDAN	1500' FT. LT. STA. 613+100 (UNCL. EXC. MATL.)

- LISTS OF BRIDGES AND BOX BRIDGE IN PLACE**
- (A) STA 305+05.25 TO STA 305+13.75 RT. LANE, 10 465' SPANS, 10 67' 10 465' SPANS (U.S. HWY. NO. 49)
  - (B) STA 317+95.25 TO STA 319+45.25 RT. LANE, 5 30 SPANS, (I.C. RR.)
  - (C) STA 360+70.00 TO STA 361+50.00, 10 465' SPANS, (BOHIE RIVER)
  - (D) STA 10+85.25 (LOCAL ROAD), 10 465' 20 67' 10 465' SPANS.
  - (E) STA 10+85.25 (LOCAL ROAD), 10 465' 20 67' 10 465' SPANS.
  - (F) STA 422+23 LT. & RT. LANES, 16' 10" 4 BOX BR.
  - (G) STA 528+165 LT. LANE, STA 529+73.25 RT. LANE, 10 465' 10 67' SPANS (LOCAL ROAD)
  - (H) STA 13+45.25 (LOCAL ROAD) 10 465' 20 67' 10 465' SPANS.

APPROVED: DATE 10-25-60

J. C. Robbins  
 DIRECTOR  
 MISSISSIPPI STATE HIGHWAY DEPARTMENT

DEPARTMENT OF COMMERCE  
 BUREAU OF PUBLIC ROADS

APPROVED: \_\_\_\_\_  
 DIVISION ENGINEER DATE \_\_\_\_\_

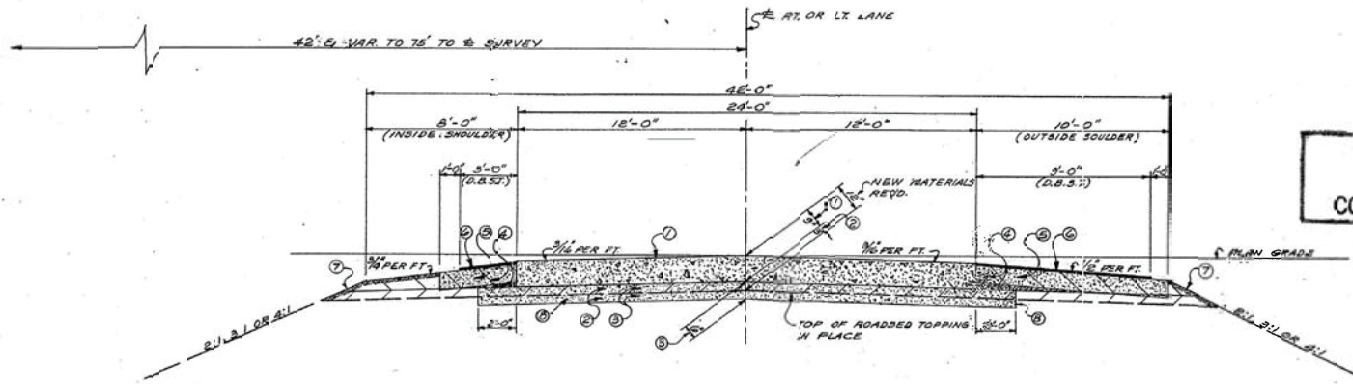
- 26 - Notice To Bidders No. 3560--Cont'd

T-IG-59-1(19) 66



*K.H. Doninger*

NO. 250	STATE	FEEL	SHEET	TOTAL
3	MISSOURI	2	2-A	2
DRAWN BY		DATE		
C.B.C.		8/23/60		
REVISIONS				
DATE	BY	DATE	BY	
11/16/60	Doc			



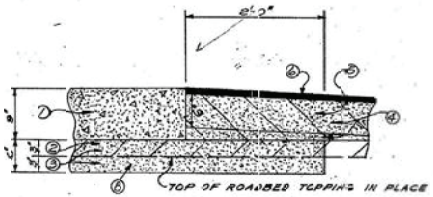
**FINAL PLANS  
OF  
COMPLETED WORK**

**TYPICAL PAVING SECTION**  
ONE 24' ROADWAY (RT. OR LT. LANE) OF INTERSTATE HIGHWAY (NORMAL SECTION)  
STA. 621+87 (D.P.) TO STA. 628+70 (D.P.)

- ① 9" REINFORCED CEMENT CONCRETE PAVEMENT REQ'D (24' WIDE FOR NORMAL ROADWAY) (VARIABLE TO 25'-0" WIDE AT BRIDGE ABUTMENTS OF BRIDGES WITH A 20' CLEAR ROADWAY).
- ② 6" ROADDED TOPPING (SAND CLAY) (D.R.M.) REQ'D FOR WIDTH OF 24'.
- ③ SOIL CEMENT-WATER MIXING REQ'D TO FORM A CEMENT TREATED BASE (6" DEEP X 24'-0" WIDE) [THIS WILL CONSIST OF MIXING THE TOP 6" OF ROADDED TOPPING WITH PORTLAND CEMENT, (3% ESTIMATED BY VOLUME) (PAY ITEMS 401-33-A AND 401-33-B). ASPHALT EMULSION SHALL BE APPLIED AS A CURING AGENT. (NOT A PAY ITEM)].
- ④ SHOULDERS REQ'D (ROADDED TOPPING) (D.R.M.) (6" DEPT)
- ⑤ SOIL CEMENT-WATER MIXING REQ'D OVER THE FULL WIDTH OF THE OUTSIDE SHOULDER (10' WIDE) AND OVER 4' OF THE INSIDE SHOULDER AS SHOWN. [THIS SHALL CONSIST OF THE TOP 6" OF THE SAND CLAY SHOULDER BEING TREATED WITH PORTLAND CEMENT, (3% ESTIMATED BY VOLUME) (PAY ITEMS 401-33-A AND 401-33-B). ASPHALT EMULSION SHALL BE APPLIED AS A CURING AGENT (NOT A PAY ITEM) AND SHALL ALSO SERVE AS THE PRIME COAT FOR THE D.R.M. ON THE SHOULDERS.]
- ⑥ DOUBLE BITUMINOUS SURFACE TREATMENT REQ'D ON SHOULDERS (6'-0" WIDE ON OUTSIDE, 6'-0" WIDE ON INSIDE) [NO PRIME REQ'D UNDER D.B.S.T. SHOULDERS - ASPHALT EMULSION WHICH IS APPLIED AS A CURING AGENT FOR THE SOIL CEMENT WATER-MIXING REQ'D. ON THE SHOULDERS, WILL SERVE AS PRIME].
- ⑦ AREA (Hatched) TO BE SPRIGGED.
- ⑧ TOP 3" OF ROADDED TOPPING IN PLACE TO BE BLADED OUT FOR A WIDTH OF 24'. THIS MATERIAL TO BE DEPOSITED ON SHOULDERS FOR USE AS TOPPING. (ABSORBED ITEM)

**SCHEDULE OF CUT & FILL  
SLOPES IN PLACE**

0' TO 10'-4:1
10' TO 20'-3:1
OVER 20'-2:1



**DETAIL OF EDGE OF PAVEMENT**

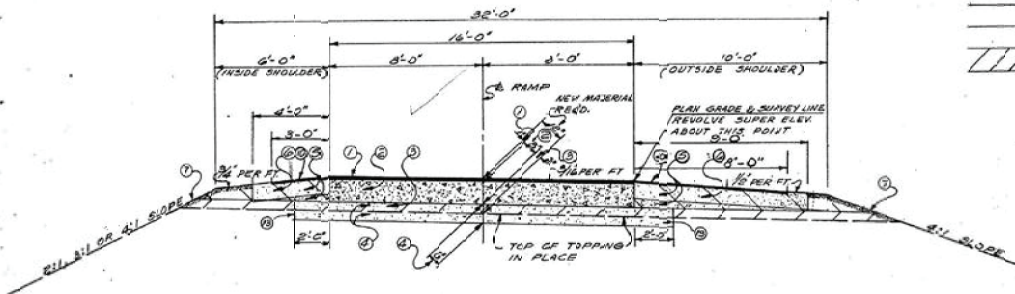
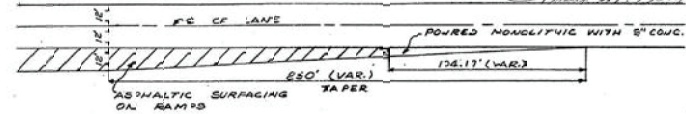
87

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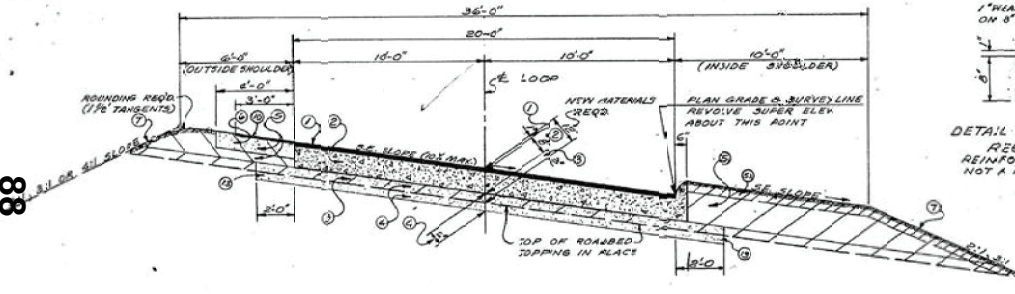
Notice To Bidders No. 3560--Cont'd

DETAIL SHOWING TERMINATION OF ASPHALT SURFACING ON RAMPS

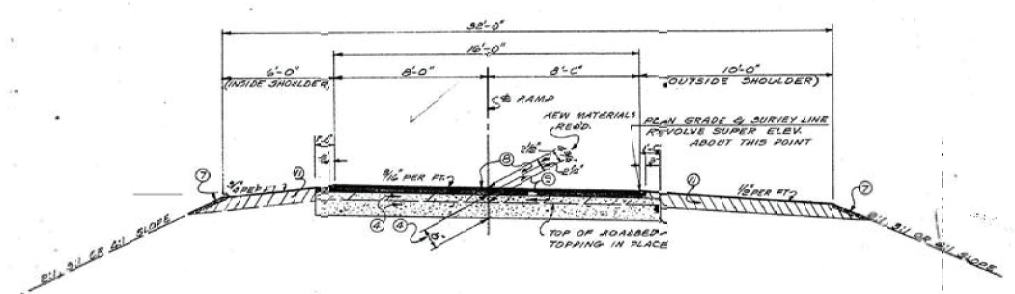
FED. ROAD DIST. NO.	STATE	LOCAL ROAD NO.	SHEET NO.	TOTAL SHEETS
2	MISS.	0000000000	11	25
DRAWN BY		DATE		
C. C. C.		8/20/60		
REVISIONS				
DATE BY	DATE	BY		



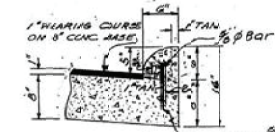
TYPICAL PAVING SECTION  
RAMPS AT CLOVER-LEAF INTERCHANGE WITH U.S. NO. 49



TYPICAL PAVING SECTION  
LOOPS AT CLOVER-LEAF INTERCHANGE WITH U.S. NO. 49



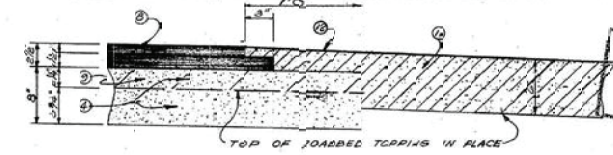
TYPICAL PAVING SECTION  
RAMPS AT DIAMOND INTERCHANGES WITH LOCAL ROADS  
STA. 420+40 AND STA. 504+50



DETAIL OF INTEGRAL CURB  
REQD. ON LOOPS  
REINFORCEMENT IN CURB IS  
NOT A PAY ITEM

- 1 1" DUNGE GRADED HOT PLANT MIX (WEARING COURSE) REQD. ON TOP OF 6" CONCRETE BASE. (16'-0" WIDE FOR RAMPS) (20'-0" WIDE FOR LOOPS)
- 2 8" DIIFORM REINFORCED CONCRETE BASE COURSE REQD. (16'-0" WIDE FOR RAMPS) (20'-0" WIDE WITH INTEGRAL CURB FOR LOOPS)
- 3 3" ROADDED TOPPING (SAND CLAY) (GRA.) REQD. FOR WIDTH OF 10' ON RAMPS & 24' ON LOOPS
- 4 SOIL CEMENT-WATER MIXING REQD. TO FORM A CEMENT BASE (SEE ALSO SCHEDULE BELOW) THIS WILL CONSIST OF MIXING THE TOP 6" OF 5' OF ROADDED TOPPING WITH PORTLAND CEMENT (2% ESTIMATED BY VOLUME) (PAY ITEMS 401-53-A AND 401-53-B) ASPHALT EMULSION SHALL BE APPLIED AS A CURING AGENT (NOT A PAY ITEM)
- 5 ASPHALT EMULSION SHALL SERVE AS A PRIME COAT FOR THE PLANT MIX ON RAMPS TO DIAMOND INTERCHANGES & D.B.S.T. SHOULDERS OF RAMPS & LOOPS
- 6 SAND CLAY SHOULDERS REQD. (ROADDED TOPPING) (GRA.) (5" DEEP) (10" DEEP RIGHT SHOULDER LOOPS)
- 7 PLANT MATERIAL (INC. E.C. L.V.M.) (4" DEEP & 18" WIDE) AT SHOULDER LOOPS
- 8 SOIL CEMENT-WATER MIXING REQD. [FOR RAMPS] FOR A WIDTH OF 9' OF THE OUTSIDE SHOULDER AND OVER 4'-0" OF THE INSIDE SHOULDER IS SHOWN FOR LOOPS; OVER 4'-0" OF THE OUTSIDE SHOULDER AS SHOWN THIS WILL CONSIST OF THE TOP 6" OF THE SAND CLAY SHOULDER MIXED WITH PORTLAND CEMENT (2% ESTIMATED BY VOLUME) (PAY ITEMS 401-53-A AND 401-53-B) ASPHALT EMULSION SHALL BE APPLIED AS A CURING AGENT (NOT A PAY ITEM)
- 9 ARE (MINIMUM) TO BE SPRIGGED.
- 10 2 1/2" ASPHALTIC PAVEMENT REQD. [1" WEARING COURSE - 15'-0" WIDE; 1 1/2" BINDER COURSE - 16'-6" WIDE]
- 11 2 1/4" ROADDED TOPPING (SAND CLAY) (GRA.) REQD. 18" WIDE

INTERCHANGE AT STATION	DESCRIPTION OF INTERCHANGE	LOOP OR RAMP	TOTAL STRUCTURE THICKNESS CONTRACT	ROADDED TOPPING IN PLACE (THIS CONTRACT)	ADDITIONAL ROADDED TOPPING REQD. (THIS CONTRACT)	PAVEMENT REQD. BASE REQD. CONTRACT	DEPTH OF CEMENT WIND PAVEMENT (SEE NOTE @ ABOVE)	WIDTH OF CEMENT TREATS BASE REQD.
304+00	1-LOVER-LEAF WITH U.S. 49	LOOP	21"	2"	3"	PHOTOLAN MIX ON AN 4" CONC. BASE	6"	24'-0"
304+00	1-CLOVER-LEAF WITH U.S. 49	RAMP	21"	2"	3"	HOT PLANT MIX ON AN 4" CONC. BASE	6"	20'-0"
420+40	1-RAMP WITH A LOCAL ROAD	RAMP	18 1/2"	16"	2 1/2"	2 1/2" HOT PLANT MIX	5"	18'-0"
504+50	1-RAMP WITH A LOCAL ROAD	RAMP	18 1/2"	16"	2 1/2"	2 1/2" HOT PLANT MIX	5"	18'-0"



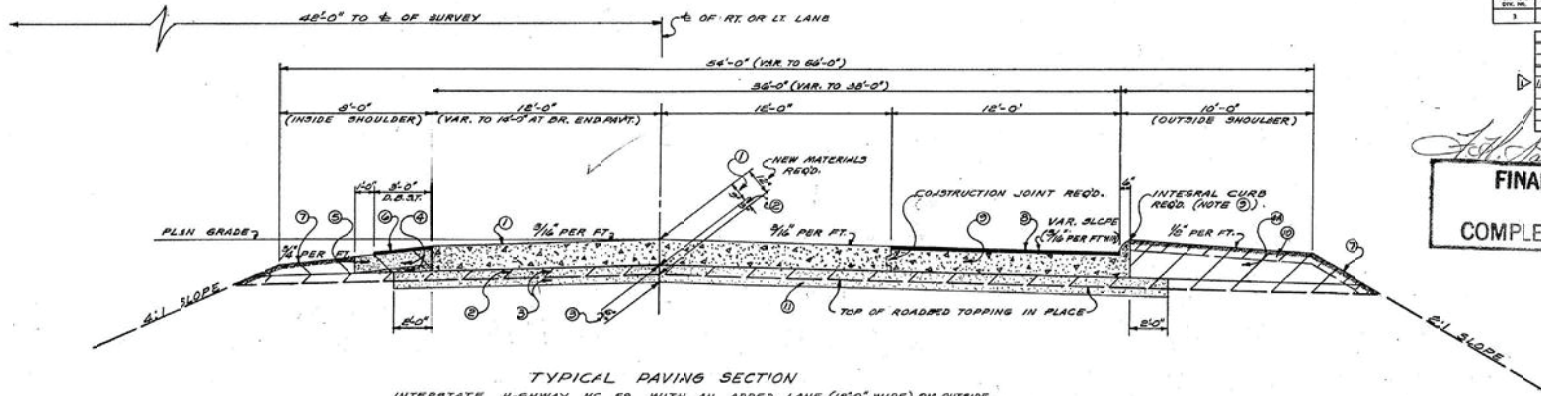
DETAIL OF EDGE OF ASPHALTIC PAVEMENT  
REQD. ON RAMPS AT DIAMOND INTERCHANGES

- 12 D.B.S.T. SHOULDERS REQD. (8" WIDE OUTSIDE SHOULDER & 3" INSIDE SHOULDER OF CLOVERLEAF INTERCHANGE RAMPS) (3" WIDE LT. SHOULDER OF LOOPS) ASPHALT EMULSION CURING AGENT FOR ITEM 12 WILL SERVE AS PRIME (NOT A PAY ITEM)
- 13 2 1/2" GRAVEL SURFACE COURSE (L.V.M.) REQD. 6" DEEP & 10" WIDE RT. SHOULDER & 6" WIDE LT. SHOULDER DIAMOND INTERCHANGE RAMPS
- 14 OMITTED NO.
- 15 TOP 5' OF ROADDED TOPPING IN PLACE TO BE SHAVED OFF FOR A WIDTH OF 20' ON RAMPS & 24' ON LOOPS THIS MATERIAL TO BE DEPOSITED ON SHOULDERS FOR USE AS TOPPING, (ABSORBED ITEM)

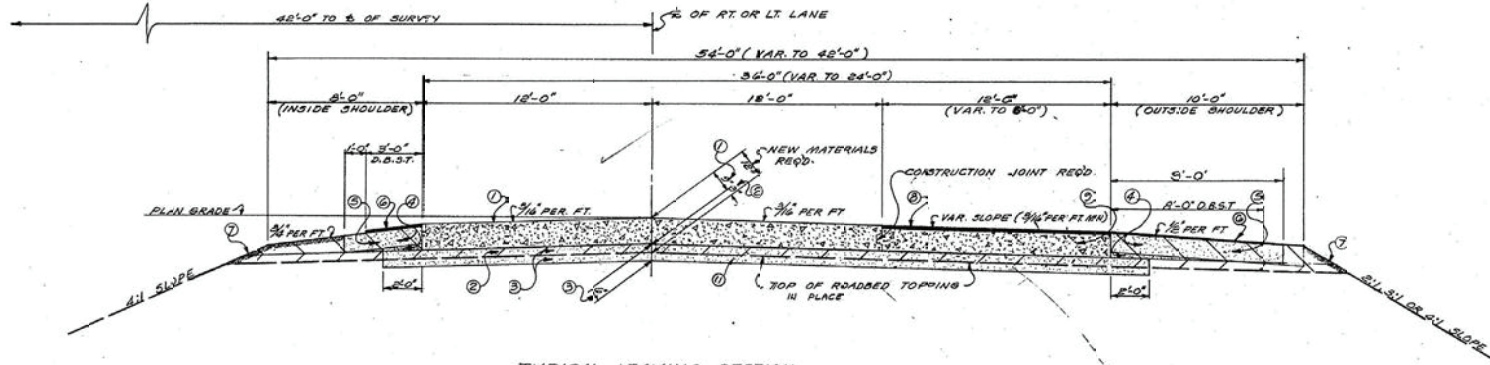
FINAL PLANS  
OF  
COMPLETED WORK

Notice To Bidders No. 3560-Cont'd 1-5





TYPICAL PAVING SECTION  
INTERSTATE HIGHWAY NC 59 WITH AN ADDED LANE (16'-0" WIDE) ON OUTSIDE  
BETWEEN LOOPS OF CLOVER-LEAF INTERCHANGE



TYPICAL PAVING SECTION  
INTERSTATE HIGHWAY NO 59 WITH AN ADDED LANE (16'-0" WIDE) ON OUTSIDE  
BETWEEN RAMPS OF CLOVER-LEAF INTERCHANGE

- ① 5" REINFORCED CEMENT CONCRETE PAVEMENT REQD. (VAR. 24'-0" TO 26'-0" WIDE ON LOOP SECTION) (24'-0" ON RAMP SECTION).
- ② 2" ROADBED TOPPING (SAND CLAY) (CRM.) REQD. FOR A WIDTH OF 4'
- ③ SOIL CEMENT-WATER MIXING REQD. TO FORM A CEMENT TREATED BASE (6" DEEP X 40'-0" WIDE). THIS WILL CONSIST OF MIXING THE TOP 6" OF ROADBED TOPPING WITH PORTLAND CEMENT (3% ESTIMATED BY VOLUME) (PAY ITEMS 401-25-A AND 401-25-B). ASPHALT EMULSION SHALL BE APPLIED AS A CURING AGENT (NOT A PAY ITEM).
- ④ SAND CLAY SHOULDERS REQD. (ROADBED TOPPING) (CRM.) (3" DEEP ON INSIDE) (2" DEEP ON OUTSIDE)
- ⑤ ROADBED TOPPING (SAND CLAY) (CRM.) SHOULDER MATERIAL REQD. BEHIND INTEGRAL CURB ON OUTSIDE SHOULDER (10" DEEP).
- ⑥ SOIL CEMENT-WATER MIXING REQD. FOR A WIDTH OF 8' OF THE OUTSIDE SHOULDER AND OVER 4' OF THE INSIDE SHOULDER AS SHOWN. [THIS WILL CONSIST OF THE TOP 4" OF THE SAND CLAY SHOULDER BEING TREATED WITH PORTLAND CEMENT (3% ESTIMATED BY VOLUME) (PAY ITEMS 401-25-A AND 401-25-B). ASPHALT EMULSION SHALL BE APPLIED AS A CURING AGENT (NOT A PAY ITEM) AND SHALL ALSO SERVE AS THE PRIME COAT FOR THE D.B.S.T. ON THE SHOULDERS].
- ⑦ DOUBLE BITUMINOUS SURFACE TREATMENT REQD. ON THE SHOULDERS. [NO PRIME COAT REQD. UNDER D.B.S.T. SHOULDERS. ASPHALT EMULSION WHICH IS APPLIED AS A CURING AGENT FOR THE SOIL CEMENT-WATER MIXING REQD. ON THE SHOULDERS, WILL SERVE AS PRIME.] (3' WIDE INSIDE & 8' WIDE OUTSIDE)
- ⑧ AREA (WHERE SHOWN) TO BE BRIDGED
- ⑨ 1" DENSE GRADED HOT PLANT MIX (WEARING COURSE) REQD. ON TOP OF 8" CONCRETE BASE (18'-0" WIDE)
- ⑩ 3" UNIFORM REINFORCED CEMENT CONCRETE BASE COURSE REQD. (18'-6" WIDE WITH INTEGRAL CURB AS SHOWN FOR LOOP SECTION) (18'-0" WIDE FOR RAMP SECTION).
- ⑪ 4" PLATING MATERIAL (UNCL. EXC.) (L.V.M.) ± 15' WIDE TOP 3" OF ROADBED TOPPING IN PLACE TO BEADED OUT 40' WIDE THIS MATERIAL TO BE DEPOSITED ON SHOULDERS FOR USE ON SAND CLAY SHOULDERS (ABSORBED ITEM)

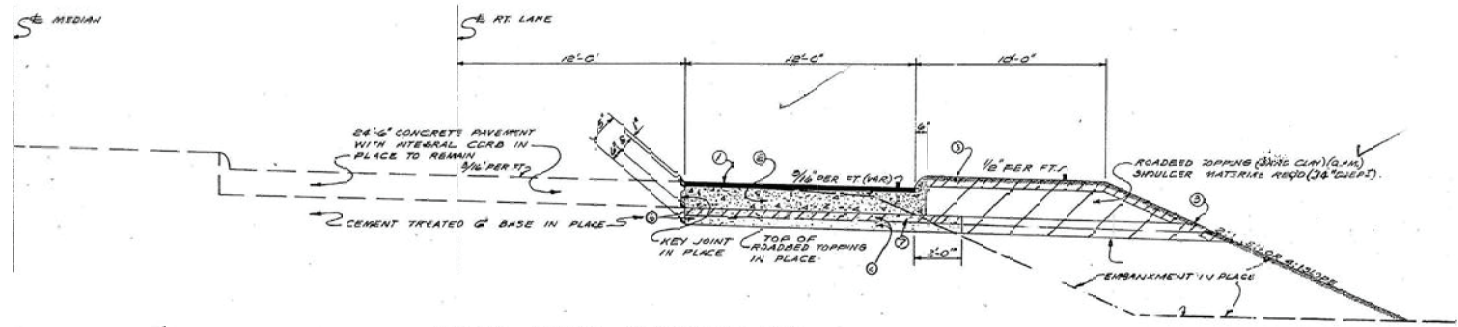
DES. NO.	STATE	SCALE	SHEET	TOTAL
5	NC	1/4" = 1'-0"	20	20
DRAWN BY		DATE		
C.P.G.		8/29/60		
REVISIONS				
DATE	BY	DATE	BY	
11/17/60	Chap			

FINAL PLANS  
OF  
COMPLETED WORK

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DESIGN NO.	STATE	FED. AID DIST.	SHEET NO.	TOTAL SHEETS
2	INDIANA	1A	20	20
DRAWN BY		DATE		
C.P.C.		3/27/60		
REVISIONS				
DATE	BY	DATE	BY	

*John J. ...*  
**FINAL PLANS  
 OF  
 COMPLETED WORK**



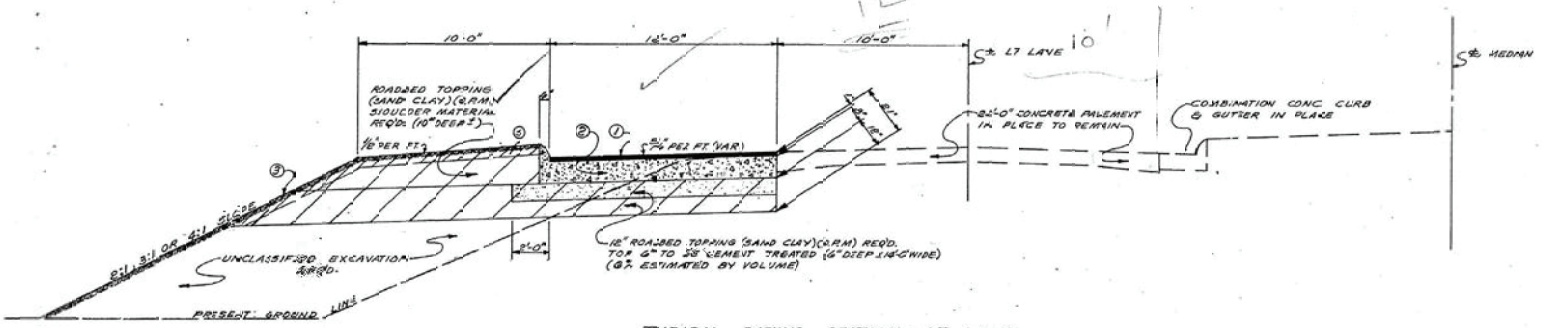
**TYPICAL PAVING SECTION - RT LANE**  
 U.S. HIGHWAY NO. 49 IN PLACE WITH AN ADDED LANE (12'-0" WIDE) ON OUTSIDE BETWEEN LOOPS OF CLOVER-LEAF INTERCHANGE

**SCHEDULE OF CUT & FILL SLOPES ON U.S. HIGHWAY NO. 49**

10'-4"	4:1
4'-10"	3:1
OVER 10'	2:1

90

- ① 1" DENSE GRADED HOT PLANT MIX (WEARING COURSE) REQ'D. ON TOP OF 3" CONCRETE BASE (12'-0" WIDE).
- ② 3" UNIFORM REINFORCED CEMENT CONCRETE BASE COURSE REQ'D. (20'-0" WIDE WITH INTEGRAL CURB).
- ③ AREA ~~TO BE SPAGGED~~ TO BE SPAGGED
- ④ 1/2" ROADBED TOPPING (SAND CLAY) REQ'D.
- ⑤ 1/2" PLATING MATERIAL (UNCL. EXC.) (G.V.M.) WITH 1% FIBER WITH 100 FT. SLOPE
- ⑥ TOP 1" OF ROADBED TOPPING TO BE CEMENT TREATED 1% BY VOLUME
- ⑦ TOP 3" OF TOPPING IN PLACE TO BE SLADED OUT 1/4" WIDE THIS MATERIAL TO BE DEPOSITED ON SHOULDERS FOR USE AS TOPPING. (ABSORBED ITEM)



**TYPICAL PAVING SECTION - LT LANE**  
 U.S. HIGHWAY NO. 49 IN PLACE WITH AN ADDED LANE (12'-0" WIDE) ON OUTSIDE BETWEEN LOOPS OF CLOVER-LEAF INTERCHANGE

30

Notice To Bidders No. 3560--Cont'd



RECAPITULATION OF QUANTITIES

PAY ITEM NO.	ROADWAY ITEMS	X-SECTION		Y-SECTION		TOTAL		UNITS
		PRELIM.	FINAL	PRELIM.	FINAL	PRELIM.	FINAL	
21	RANDOM CLEARING & GRUBBING	0.0	1.287	0.1	2.173	1	1.622	ACRE
23-A	UNCLASSIFIED EXCAVATION (F.M.)	4151	7224	0	0	4151	7224	CU YDS
32-A	HAUL OF EXCAVATION (F.M.)	3,860	1,624	0	0	3,860	1,624	374 YDS
32-B	HAUL OF SELECTED MATERIAL	201,132	29,362	20812	7,660	227,944	23,360	UNITS
32-C	HAUL OF UNCLASSIFIED EXCAVATION (SPECIAL)	1,297	10,224	0	0	1,297	10,224	UNITS
40-A	ROADBED TOPPING (O.R.M.)	20,602	20,537	6,658	3,746	27,260	11,852	CU YDS
32-A	MIXING, SHAPING AND COMPACTION	2,802	774	0	0	2,802	774	SQ YDS
32-B	STABILIZER AGGREGATE - COMMERCIAL COURSE (L.V.M.)	442	121	0	0	442	121	CU YDS
30-B	CLAY GRAVEL SURFACE COURSE (L.V.M.)	2,833	2,751	0	0	2,833	2,751	CU YDS
205-B	CLASS "B" CONCRETE	7	10,567	0	0	7	10,567	CU YDS
211	REINFORCING STEEL	1228	1,234	0	0	1,228	1,234	LBS.
216	PAVED DITCH	0	0	149	4124	149	4124	CU YDS
201-29-A	SOIL CEMENT WATER MIXING (BASE & SHOULDERS)	27,230	27,230	10,208	72,354	89,584	33,212	SQ YDS
201-29-B	PORTLAND CEMENT	27,230	27,230	1,148	6,649	28,378	23,214	BBL.
275-A	AGRICULTURAL LIMESTONE	36	91,076	0	0	36	91,076	TONS
275-C	SUPER PHOSPHATE	18	0	0	0	18	0	TONS
201-279	"SPRIGGING"	36,594	0	2272	0	39,266	0	SQ YDS
215-A	14-13-13 TRAFFIC MIXER	11,160	0	230	0	11,390	0	CU YDS
201-262-A	TRAFFIC STRIPE (SKIP WHITE)	135	13,372	0	0	135	13,372	MILES
201-262-B	TRAFFIC STRIPE (CONTINUOUS WHITE)	5.5	0.237	0	0	5.5	0.237	MILES
201-262-C	TRAFFIC STRIPE (CONTINUOUS YELLOW)	2810	3,169	200	0	3,010	3,169	LIN. FT.
60-75-1	5" UNIFORM REINFORCED CONCRETE CEMENT BASE COURSE	26,184	3,124	743	215	27,042	3,339	SQ YDS
60-75-2	PRIME	1,931	393	0	0	2,324	393	GALS.
60-75-3	ASPHALT CEMENT FOR SURFACE TREATMENT	74,350	1,000	2,000	1,814	78,164	2,814	GALS.
107-A OR 107-E	COVER AGGREGATE (TYPE I OR II UNCRUSHED)	1,583	1,583	63	1,522	1,646	1,585	CU YDS
107-F	SEAL AGGREGATE (TYPE II)	1,028	1,028	28	20	1,056	1,048	CU YDS
171-PA-1	CONCRETE BRIDGE END PAVEMENT	814	8,954	830	3,525	1,644	12,479	SQ YDS
207-C	SPECIAL DESIGN INTEGRAL CURB	808	1,324	0	0	808	1,324	LIN. FT.
201-129-C	DENSE GRADED HOT PLANT MIX PRIMARY COURSE	1,378	1,378	0	0	1,378	1,378	TONS
201-129-D	DENSE GRADED HOT PLANT MIX WEARING COURSE	2,176	3,573	257	2,180	2,433	3,573	TONS
201-170-J-1	EXPANSION JOINTS (WITH DOWELS)	25700	25,213	621	265	26,311	25,478	LIN. FT.
201-170-J-2	EXPANSION JOINTS (WITHOUT DOWELS)	760	775	307	310	1,067	1,085	LIN. FT.
201-170-PA-1	9" UNIFORM REINFORCED CONCRETE PAVEMENT	118,238	112,213	5,120	5,120	123,358	117,333	SQ YDS
220-A-15	15" ROUND CONC. PIPE (CULVERT) - 5500	29580	29580	0	0	29580	29580	TONS
220-A-21	21" ROUND CONC. PIPE (CULVERT) - 5500	20	20	0	0	20	20	TONS
220-C-6	6" CONC. CURB (UNDER DRAIN)	267	267	0	0	267	267	LIN. FT.

- ① INCLUDES 1291 CU YDS. FOR PLATING ON SHOULDERS
- ② 1 UNIT OF HAUL FROM PIT NO. 381 FOR 1291 CU YDS. PLATING MATERIAL ON SHOULDERS.
- ③ 2 TONS PER ACRE EST
- ④ 1 TON PER ACRE EST.

STATE	F.A.R.	DATE	BY
NO.	NO.	NO.	NO.
DATE BY	DATE BY	DATE BY	DATE BY
DATE BY	DATE BY	DATE BY	DATE BY

Δ - REV. TO ELIMINATE URBAN SECTIONS.

*[Signature]*  
**FINAL PLANS OF COMPLETED WORK**

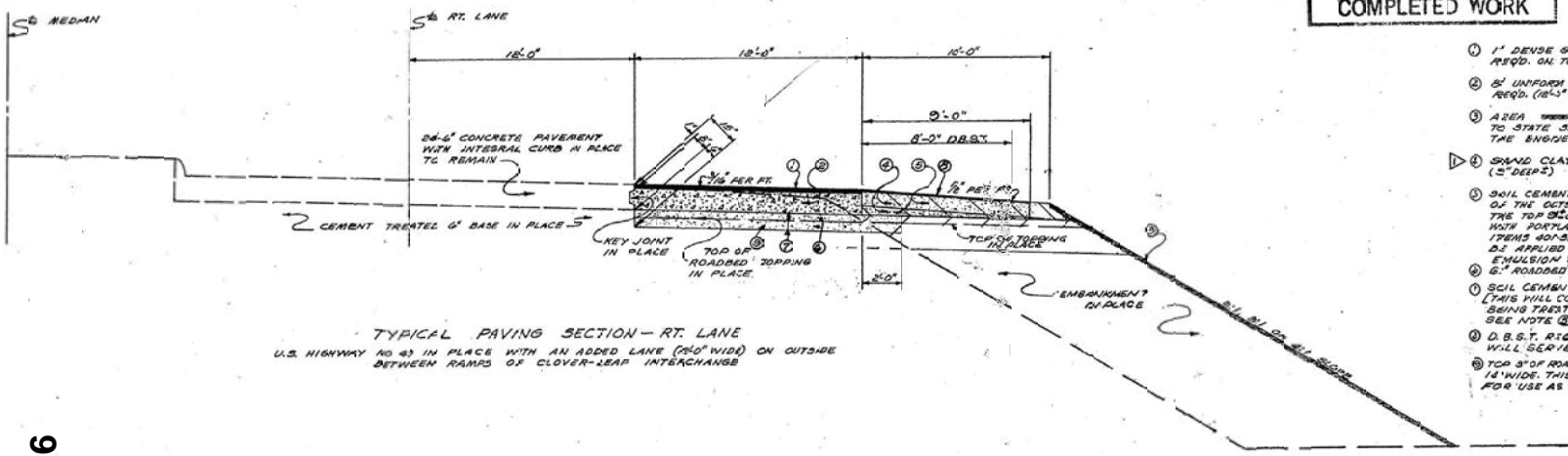
PLAN ASSEMBLY	
SHEET NO.	TITLE
1	TITLE AND LAYOUT
2	QUANTITIES SHEET
BA THRU 2G	TYPICAL SECTION SHEETS
2H	DESIGNATED SOURCE OF SELECTED MATERIAL
2J	STRIPING DETAILS FOR ON AND OFF RAMP
2K, 2L, 2M	STRIPING DETAILS FOR ON AND OFF RAMP
2N, 2O, 2P, 2Q, 2R	SPECIAL DESIGN JOINTS AND DOWELS
2S, 2T	SPECIAL DESIGN PAVING DETAILS
2U, 2V	SPECIAL DESIGN BRIDGE END PAVEMENT
F-21-A	SPECIAL DESIGN BRIDGE END PAVEMENT
PSM-1	STANDARD STRIPING DETAILS
BD-15	STATE STANDARD FOR CURB INLET
SE-1, SEB, SEB	STATE STANDARD FOR SUPERELEVATION
BW-2	STATE STANDARD BARRICADE & WARNING SIGNS
EC-1	STATE STANDARD FOR EROSION CONTROL (2 SHEETS)
E-ON-R	ON RAMP CONTROLS FOR DIAMOND TYPE INTERCHANGE
E-OFF-R	OFF RAMP CONTROLS FOR DIAMOND TYPE INTERCHANGE
34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100	PLAN - PROFILE SHEETS

- 31 - Notice To Bidders No. 3560--Cont'd



STATE	YEAR	SHEET	TOTAL
MISS.	1966	2-E	
DRAWN BY		DATE	
C.B.C.		11/1/66	
REVISION			
DATE	BY	DATE	BY
11/1/66	DEAD		

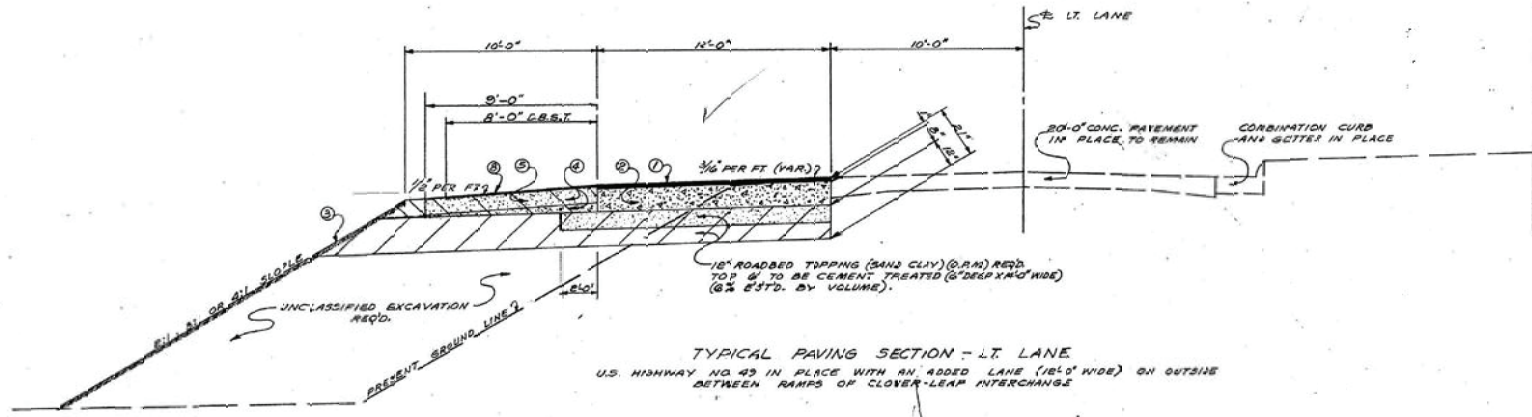
**FINAL PLANS  
OF  
COMPLETED WORK**



TYPICAL PAVING SECTION - RT. LANE  
U.S. HIGHWAY NO 43 IN PLACE WITH AN ADDED LANE (75.0' WIDE) ON OUTSIDE  
BETWEEN RAMPS OF CLOVER-LEAF INTERCHANGE

- ① 1" DEEP SANDY HOT PLANT MIX (WEARING COURSE) REQ'D. ON TOP OF 6" CONCRETE BASE (14'-0" WIDE).
- ② 6" UNIFORM REINFORCED CEMENT CONCRETE BASE COURSE REQ'D. (24'-0" WIDE).
- ③ A 1/2" AREA TO BE SAVED AND MULCHED ACCORDING TO STATE SPECIAL DESIGN EC-2 AND/OR AS DIRECTED BY THE ENGINEER.
- ④ SAND CLAY SHOULDERS REQ'D. (ROADBED TOPPING) (G.R.M.) (5" DEEP).
- ⑤ SOIL CEMENT-WATER MIXING REQ'D. FOR A WIDTH OF 6' OF THE OUTSIDE SHOULDER (9'-0" WIDE). [THIS WILL CONSIST OF THE TOP 6" OF THE SAND CLAY SHOULDER BEING TREATED WITH PORTLAND CEMENT (6% ESTIMATED BY VOLUME) (MAY ITEMS 40-50A AND 40-50B). ASPHALTIC EMULSION SHALL BE APPLIED AS A CURING AGENT (NOT A PAY ITEM).] EMULSION WILL SERVE AS PRIME FOR D.B.S.T.
- ⑥ 6" ROADBED TOPPING (SAND CLAY) (G.R.M.) REQ'D. (5" DEEP).
- ⑦ SOIL CEMENT-WATER MIXING REQ'D. (18" WIDE & 6" DEEP). [THIS WILL CONSIST OF THE TOP 6" OF SAND CLAY TOPPING BEING TREATED WITH PORTLAND CEMENT (6% EST. BY VOLUME) SEE NOTE ② FOR PAY ITEMS & EMULSION NOTE.]
- ⑧ D.B.S.T. 18" WIDE, 8" WIDE [ASPHALT EMULSION (ITEM ②) WILL SERVE AS PRIME.]
- ⑨ TOP 3" OF ROADBED TOPPING IN PLACE TO BE GRADED OUT 18" WIDE. THIS MATERIAL TO BE DEPOSITED ON SHOULDERS FOR USE AS TOPPING ON SHOULDER (ASBOSSED ITEM).

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TYPICAL PAVING SECTION - LT. LANE  
U.S. HIGHWAY NO 43 IN PLACE WITH AN ADDED LANE (18.0' WIDE) ON OUTSIDE  
BETWEEN RAMPS OF CLOVER-LEAF INTERCHANGE

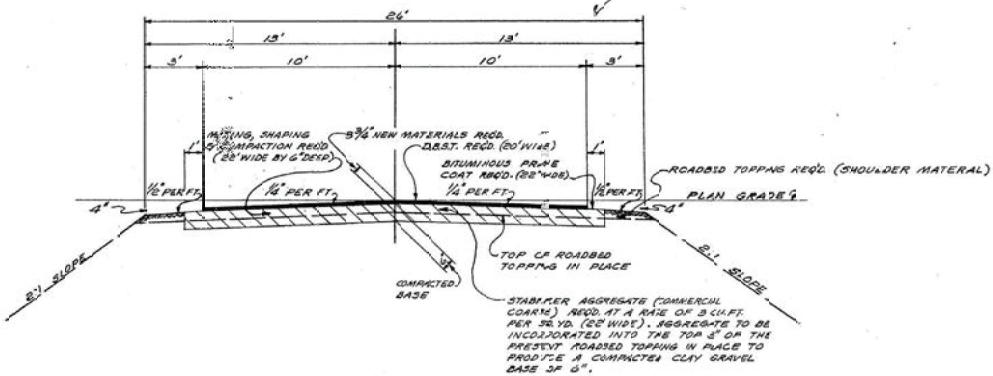
32

Notice To Bidders No. 3560 - Cont'd

REV. NO.	STATE	ROAD	PART	SHEET
3	MIL.	KM10-0244	10	2-F

DATE	BY	DATE	BY

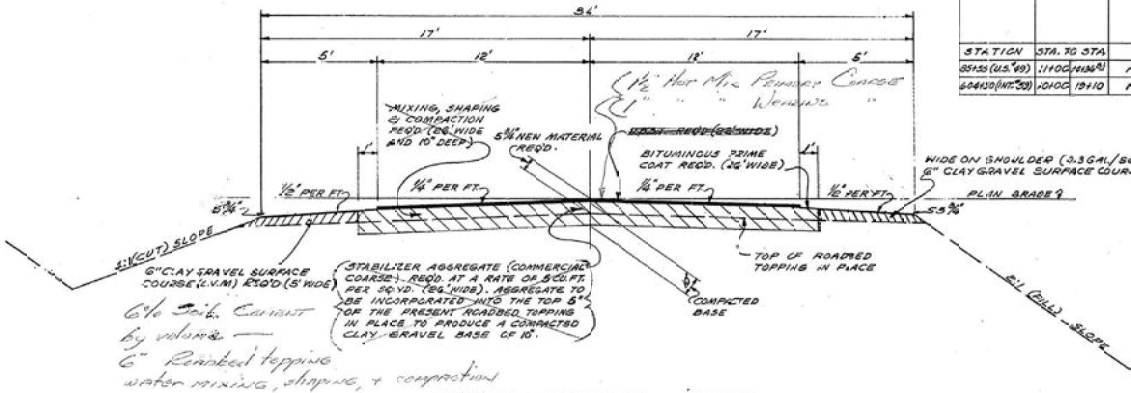
*W. H. ...*  
**FINAL PLANS  
 OF  
 COMPLETED WORK**



**TYPICAL SURFACING SECTION**  
 LOCAL ROAD RIGHT STA. 88155 TO (US. 43)  
 STA. 1100 TO STA. 14+36.83

AREA *XXXXXXXXXXXX* TO BE SPRICED.

LOCAL ROAD CROSSING MAIN FACILITY	LOCAL ROAD LIMITS	TOTAL STRUCTURE THICKNESS REQD.	ROADBED TOPPING THICKNESS IN PLACE THIS CONTRACT (SHOULDING CONTRACT)	STABILIZER AGGREGATE REQD. THIS CONTRACT (COMMERCIAL COURSE) AT A RATE OF	INKING DEPTH	COMPACTED BASE TO PRODUCE A CLAY GRAVEL BASE WITH A COMPACTED THICKNESS OF	TYPE OF SURFACING REQD.
STA. 88155 (US. 43)	1100 TO 1110	12 3/4"	9"	3	3"	6"	3/4" ROADBED TOPPING
STA. 68495 (MT. 50)	10100 TO 10110	10 3/4"	14"	5	5"	10"	ROADBED TOPPING

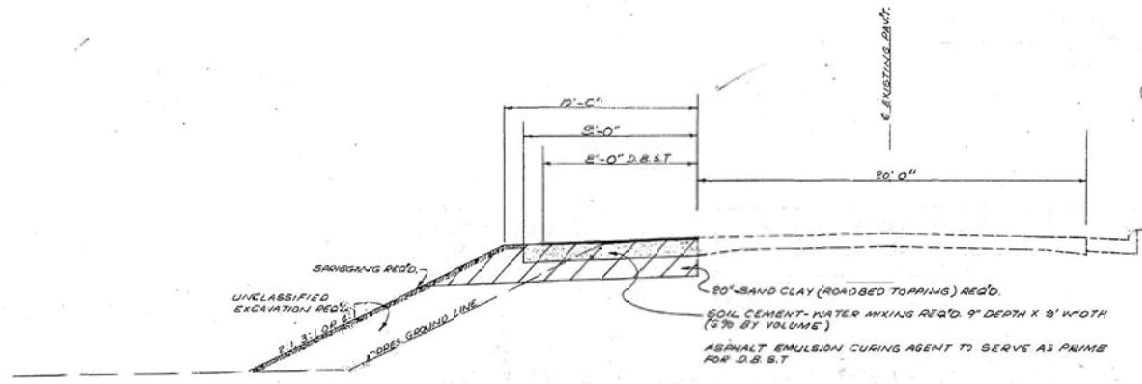


**TYPICAL SURFACING SECTION**  
 LOCAL ROAD CROSSING AT STA. 68495 (MT. 50)  
 STA. 10100 TO STA. 10110

*6% Soil Cement by volume - 6" Roadbed topping water mixing, shaping, & compaction*

REV. NO.	DATE	SCALE	SHEET	TOTAL
1	11-15-66		26	
DRAWN BY		DATE		
REVISIONS				
DATE	BY	DATE	BY	

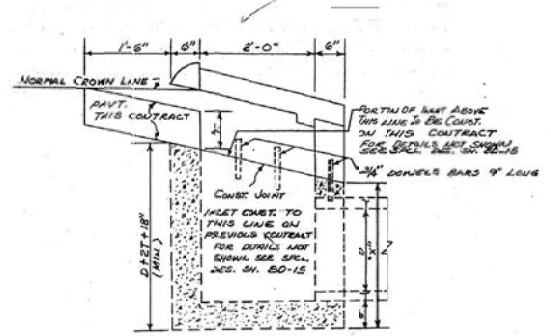
*A. J. ...*  
**FINAL PLANS  
 OF  
 COMPLETED WORK**



**TYPICAL SECTION LT. LANE**  
 GRADING & SURFACING OF OUTSIDE SHOULDER, LT. LANE OF US NO. 49  
 BETWEEN RAMPS & LOOPS OF COVERLET INTERCHANGE

**BC-15 INLET TO BE COMPLETED ON THIS CONTR.**

SHEET NO.	STA.	PIPE SIZE	CLASS OF CONC.	REINFC. STEEL	REMARKS
4-E	18+00	15"	.87	151	S.W. LOOP
4-E	20+00	15"	.87	151	S.W. LOOP
4-E	13+00	15"	.87	151	N.W. LOOP
4-E	18+00	15"	.87	151	N.W. LOOP
4-H	14+00	15"	.87	154	S.E. LOOP
4-H	18+85	15"	.87	154	S.E. LOOP
4-H	18+00	15"	.87	154	N.E. LOOP
4-H	20+00	15"	.87	154	N.E. LOOP
TOTALS			SU. YDS.	ROUNDS	
75110			2.02	170	
19.68			14.76		



**DETAIL SHOWING BC-15 INLET CONSTRUCTION**

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Notice To Bidders No. 3560-Cont'd



CD ROAD DIST. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
3	MISS.	PA-27-0024	24	24

DRAWN BY	DATE
S.L.B.	10-7-60
REVISIONS	
DATE	BY

*S.L.B.*  
**FINAL PLANS OF COMPLETED WORK**

UNIT NO.	RURAL I-SECT.			URBAN RURAL SECT.			RURAL I-SECTION										TOTAL	
	13	12	12	11	11	10	10	9	8	7	6	5	4	3	2	1		1
CU. YDS.	386	5960	637	15,035	232	2226	3672	3604	6098	6669	2313	6098	6098	5721	6098	3782	3788	6632
B.O.P.	20187.50	21.4+20	290+60	2810.0	100.0	100.0	1890.0	2640.0	2610.0	2610.0	2640.0	2610.0	2610.0	2640.0	2610.0	1610.0	1620.0	2530.0
UNITS	8968	71,220	5484	269,388	2552	24,260	34,720	32,436	28,784	40,658	38,058	80,490	24,392	17,172	12,192	3788	3788	18,268
				I-SECTION	IG. SECTION													

1000' AT 1" = 1000'

DFT. NO. 336 EXT. OWNER-COLLIER & CARTER CLEARING & GRUBBING - 1 ACRE STRIPPING QUANTITY: 97000 CU. YD. AREA: 478 ACRES

PAY ITEMS	RURAL I-SECT.	URBAN RURAL SECT.	URBAN RURAL SECT.	TOTAL	UNITS
ROADBED TOPPING	71,110	19,492	2048	93,260	CU. YDS.
HAIL OF SELECTED MATERIAL	886,843	214,869	20,812	1,122,524	1/2 MI. UNITS
RANDOM CLEARING & GRUBBING	0.7	0.2	0.1	1.0	ACRE

95

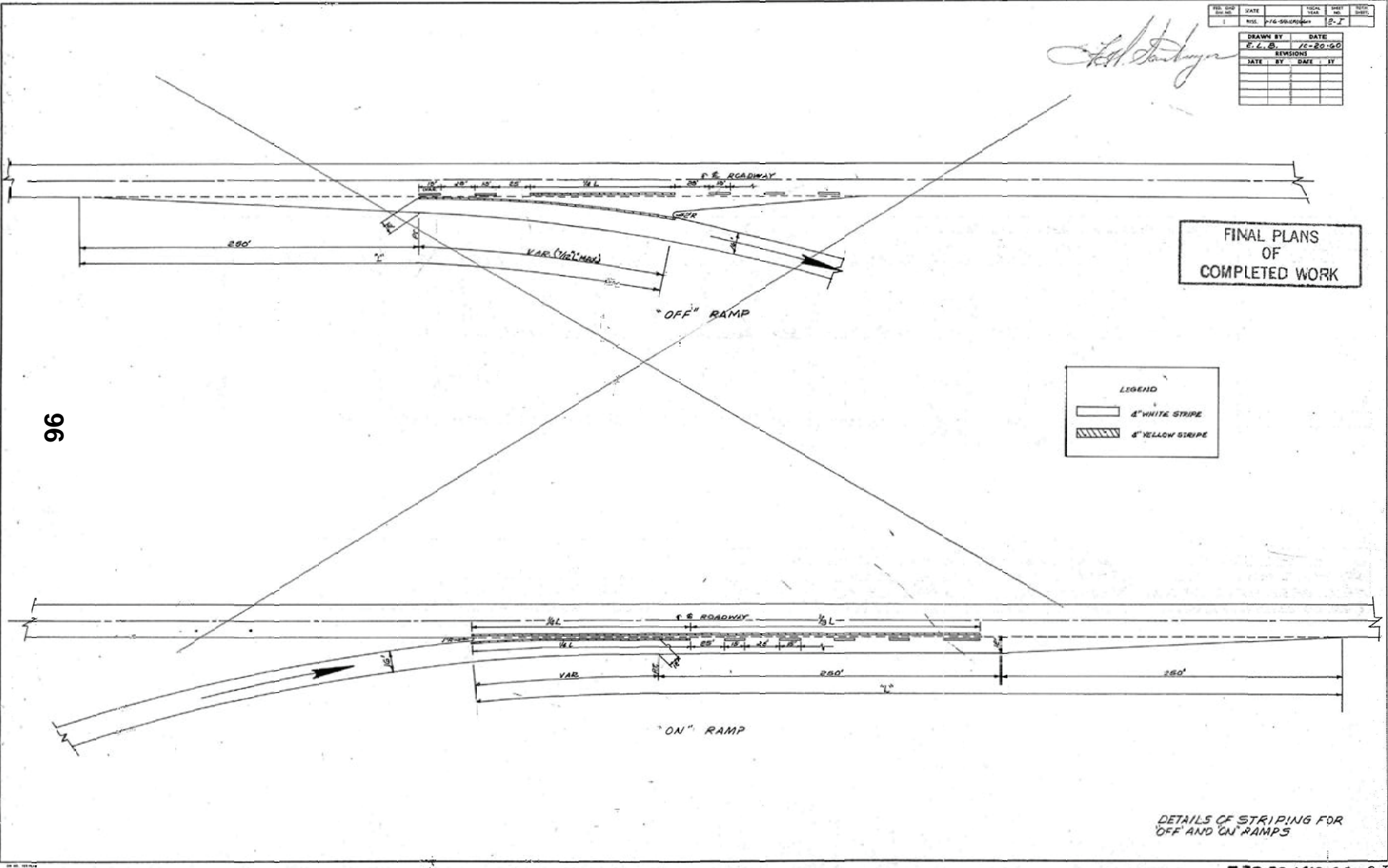
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Notice To Bidders No. 3560--Cont'd

REV. NO.	DATE	ISSUED BY	DATE
1		R.S.S.	7-16-59

*Ed. J. Sandberg*

DRAWN BY	DATE
E.L.B.	7-10-59
REVISIONS	
DATE	BY



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FINAL PLANS  
OF  
COMPLETED WORK

LEGEND

	4" WHITE STRIPE
	4" YELLOW STRIPE

DETAILS OF STRIPING FOR  
OFF AND ON RAMPS

I-16-59-1(19)66 2-1



261+87.25 BEG OF PROJECT 1-16-59-1(19)66  
 STA. 261+87.25 BEGINNING OF FED. AID PROJ. NO. 1-092-1(14)  
 STA. 261+87.25 END OF FED. AID PROJ. NO. 1-092-1(14)

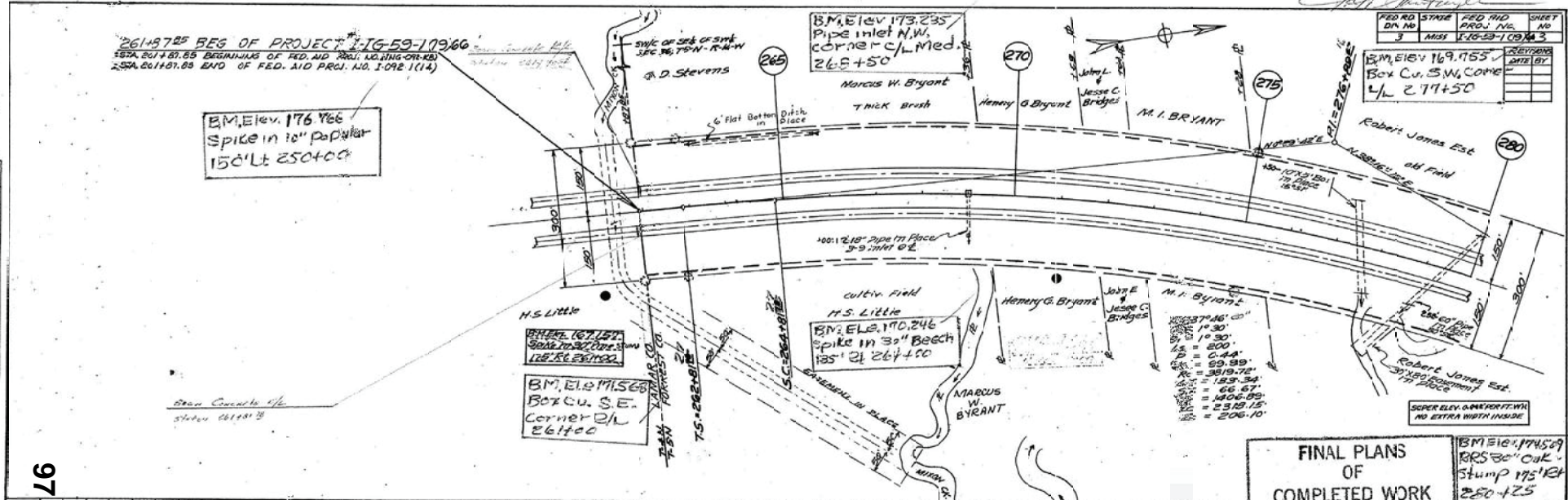
B.M. Elev. 176.766  
 Spike in 10" Diameter  
 150' Lt 250+00

B.M. Elev. 175.825  
 Pipe inlet N.W.  
 Corner C/L Med.  
 265+50

B.M. Elev. 173.235  
 Pipe inlet N.W.  
 Corner C/L Med.  
 265+50

FED. RD. STATE PROJ. NO. SHEET NO.  
 3 MISS 1-16-59-1(19)66 3

B.M. Elev. 169.175  
 Box Cu. S.W. Corner  
 L/L 277+50



FINAL PLANS  
 OF  
 COMPLETED WORK

B.M. Elev. 174.529  
 R/S 30' Oak  
 Stump 175' 1/2"  
 250+25

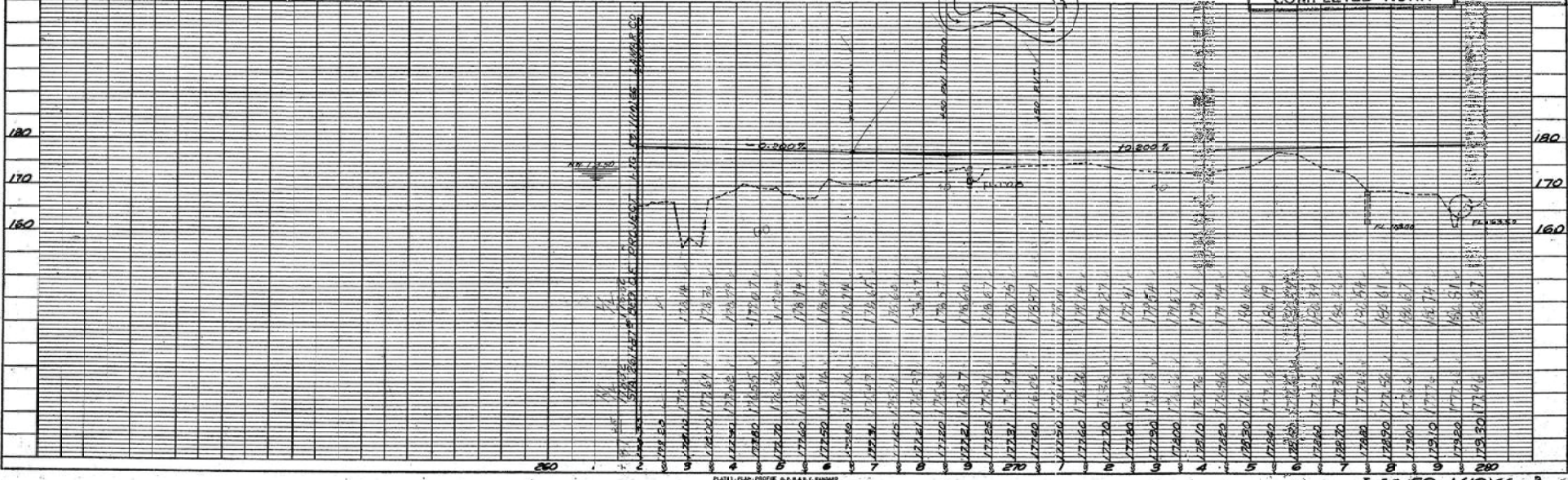


PLATE: PLAN, PROFILE & MARKS BANDS  
 HOFFER & BIRD CO. INC. 1966

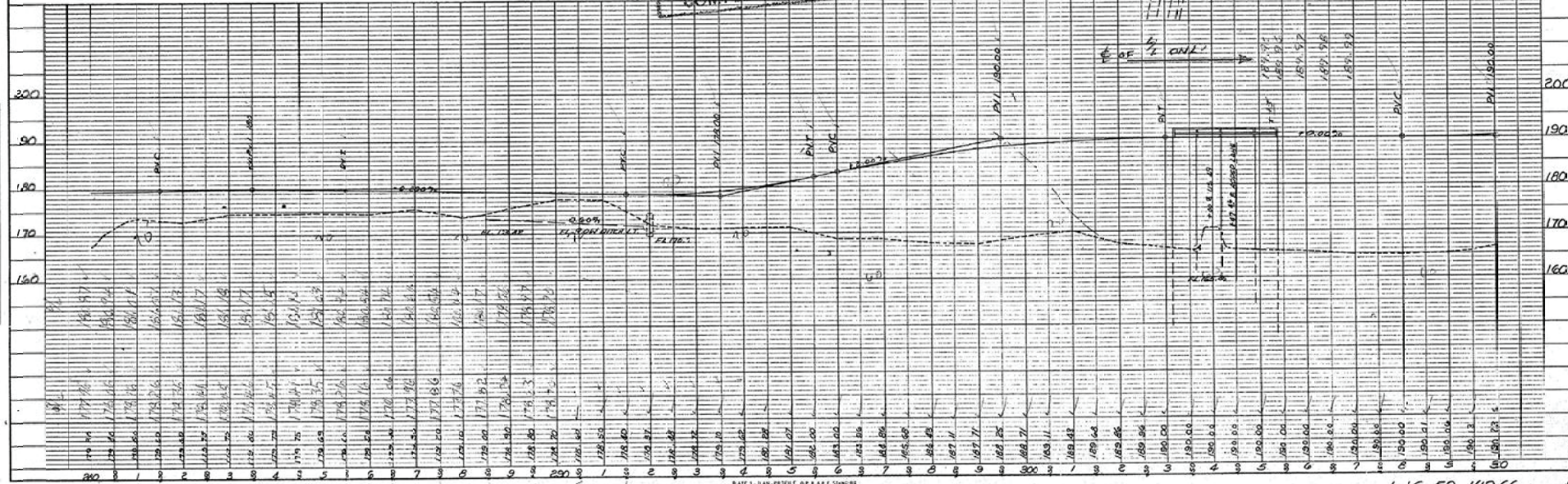
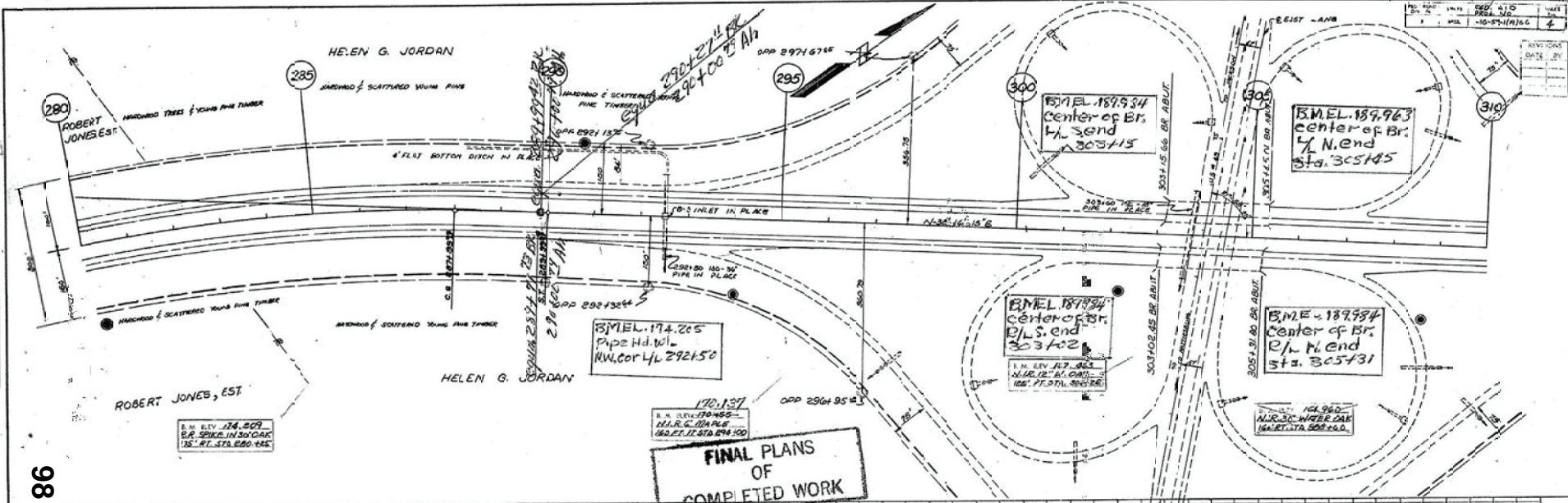
1-16-59-1(19)66 3

37 - Notice To Bidders No. 3560--Cont'd



PLAN  
 DATE  
 DRAWN BY  
 CHECKED BY  
 DATE OF REV. CHECKED  
 DATE OF REV. CHECKED  
 DATE OF REV. CHECKED

PROFILE  
 DATE  
 DRAWN BY  
 CHECKED BY  
 DATE OF REV. CHECKED  
 DATE OF REV. CHECKED  
 DATE OF REV. CHECKED



NOTE 1 - PLAN PROFILE OF A 6 FT. STANDARD  
 WIDTH & 6 INCHES HIGH CURV.

1-16-59-1119.66



DESIGN NO.	DATE	SCALE	SHEET NO.	TOTAL SHEETS
11-57A	12-20-66	AS SHOWN	11	12

DRAWN BY	DATE
REVISION	
DATE BY	DATE BY

**INDEX (REFERENCE SHEETS)**

- 4 - INTERSTATE HIGHWAY
- 4A - WEST HALF OF INTERCHANGE
- 4AA - EAST HALF OF INTERCHANGE
- 4B - SOUTH LANE OF U.S. #49
- 4B1 - LOCAL ROADS RT. U.S. #49
- 4C - SOUTHWEST RAMP
- 4D - NORTHWEST RAMP
- 4E - SOUTHWEST/NORTHWEST LOOPS
- 5 - INTERSTATE HIGHWAY

LAYOUT OF WEST HALF OF INTERCHANGE  
 REC'D FOR U.S. HWY. NO. 49 AND U.S. HWY.  
 NO. 11 - STA. 304+00

**FINAL PLANS  
 OF  
 COMPLETED WORK**

SOUTHWEST QUADRANT

NORTHWEST QUADRANT  
 SEE INDIVIDUAL Camp & Loop

66

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**R.O.W. DATA**  
 A = 40°29'30" L  
 D = 6' 87'42 1/2"  
 T = 315.10'  
 L = 622.26'  
 R = 84320

**RAMP DATA**  
 A = 40°50'30" L  
 D = 6' 4" 00"  
 T = 332.95'  
 L = 223.10'  
 R = 754.97'

**RAMP DATA**  
 A = 52°34'38" L  
 D = 7'35" 20" 00"  
 T = 372.02'  
 L = 692.73'  
 R = 754.97'

**R.O.W. DATA**  
 A = 30°42'30" L  
 D = 0' 29" 45.47"  
 T = 223.91'  
 L = 334.00'  
 R = 603.37'

**R.C.W. DATA**  
 A = 46°00'30" L  
 D = 12' 5" 3504"  
 T = 212.02'  
 L = 390.74'  
 R = 162.34'

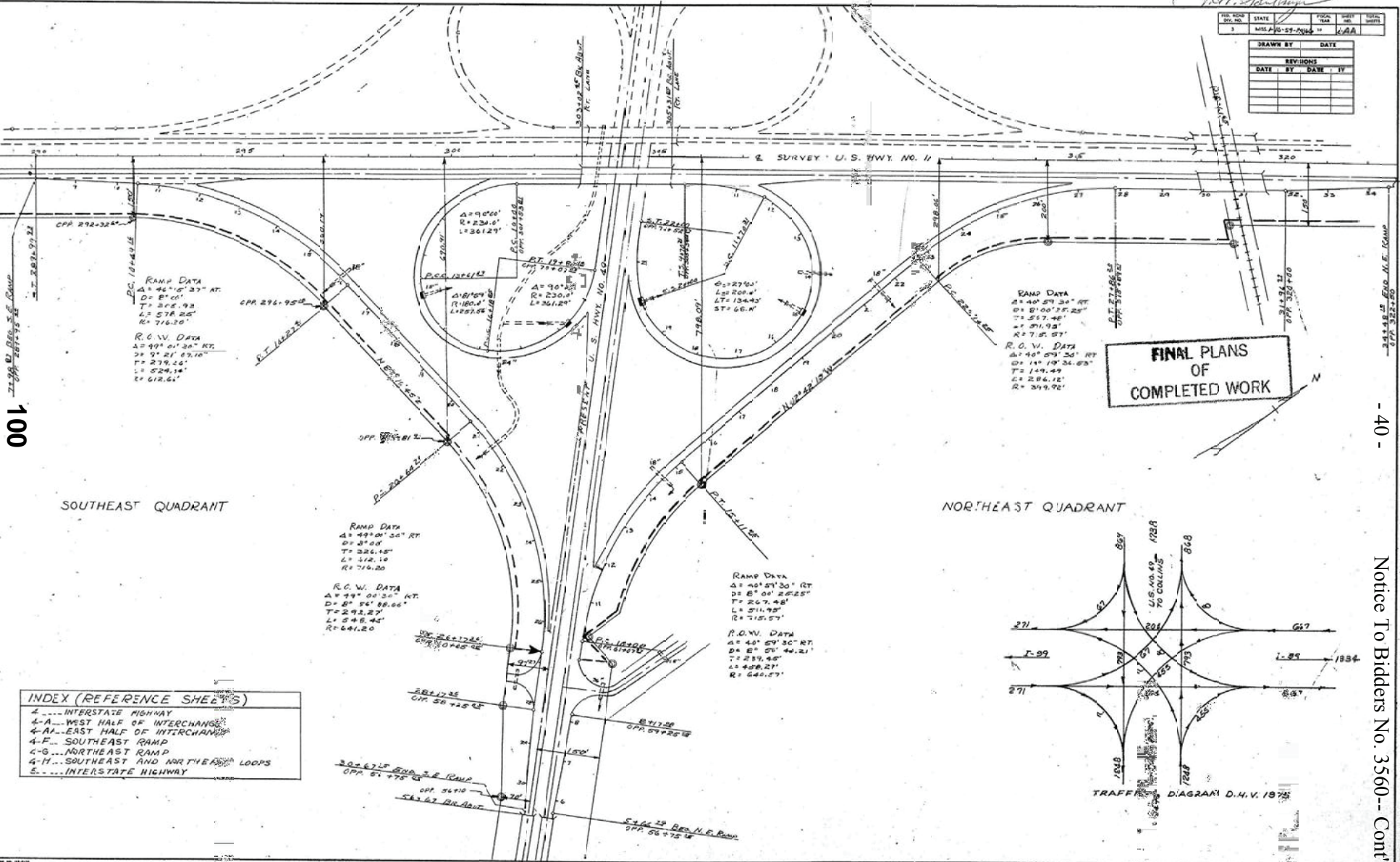
WEST HALF OF INTERCHANGE I-IG-59-1(17)CG

Notice To Bidders No. 3560--Cont'd

FILE NO.	STATE	TITLE	DATE
3	MISS.	10-57-100	11

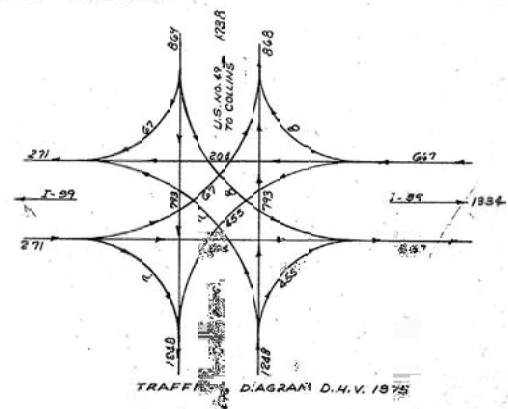
DRAWN BY	DATE
REVISED BY	DATE
DATE BY	DATE



**FINAL PLANS  
OF  
COMPLETED WORK**

**INDEX (REFERENCE SHEETS)**

4	INTERSTATE HIGHWAY
4-A	WEST HALF OF INTERCHANGE
4-A1	EAST HALF OF INTERCHANGE
4-F	SOUTHEAST RAMP
4-G	NORTHEAST RAMP
4-H	SOUTHEAST AND NORTHEAST LOOPS
5	INTERSTATE HIGHWAY



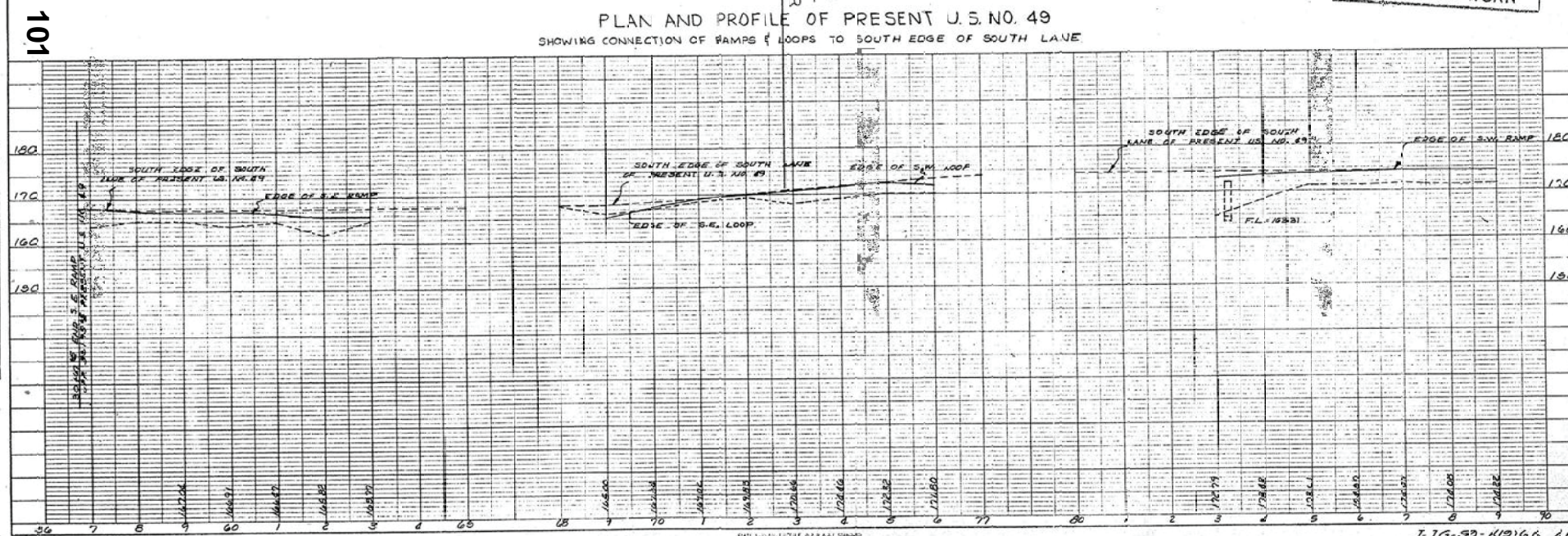
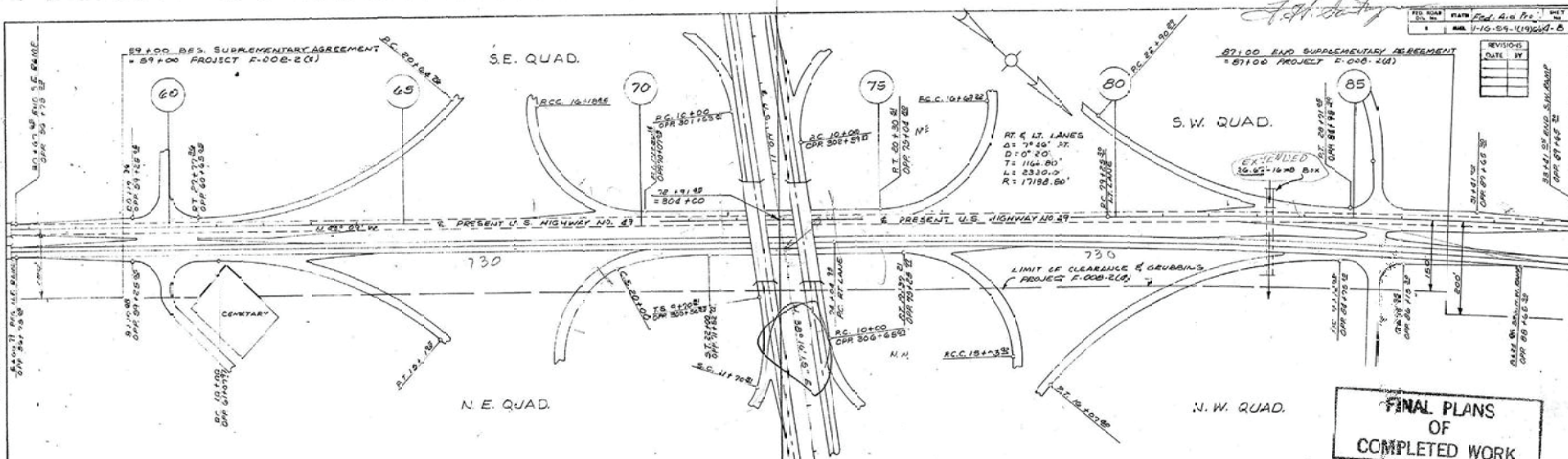
100

- 40 -

Notice To Bidders No. 3560--Cont'd

EAST HALF OF INTERCHANGE I-10-59-100 4-A





FINAL PLANS  
OF  
COMPLETED WORK

REVISED	DATE BY

PLAN

PROFILE

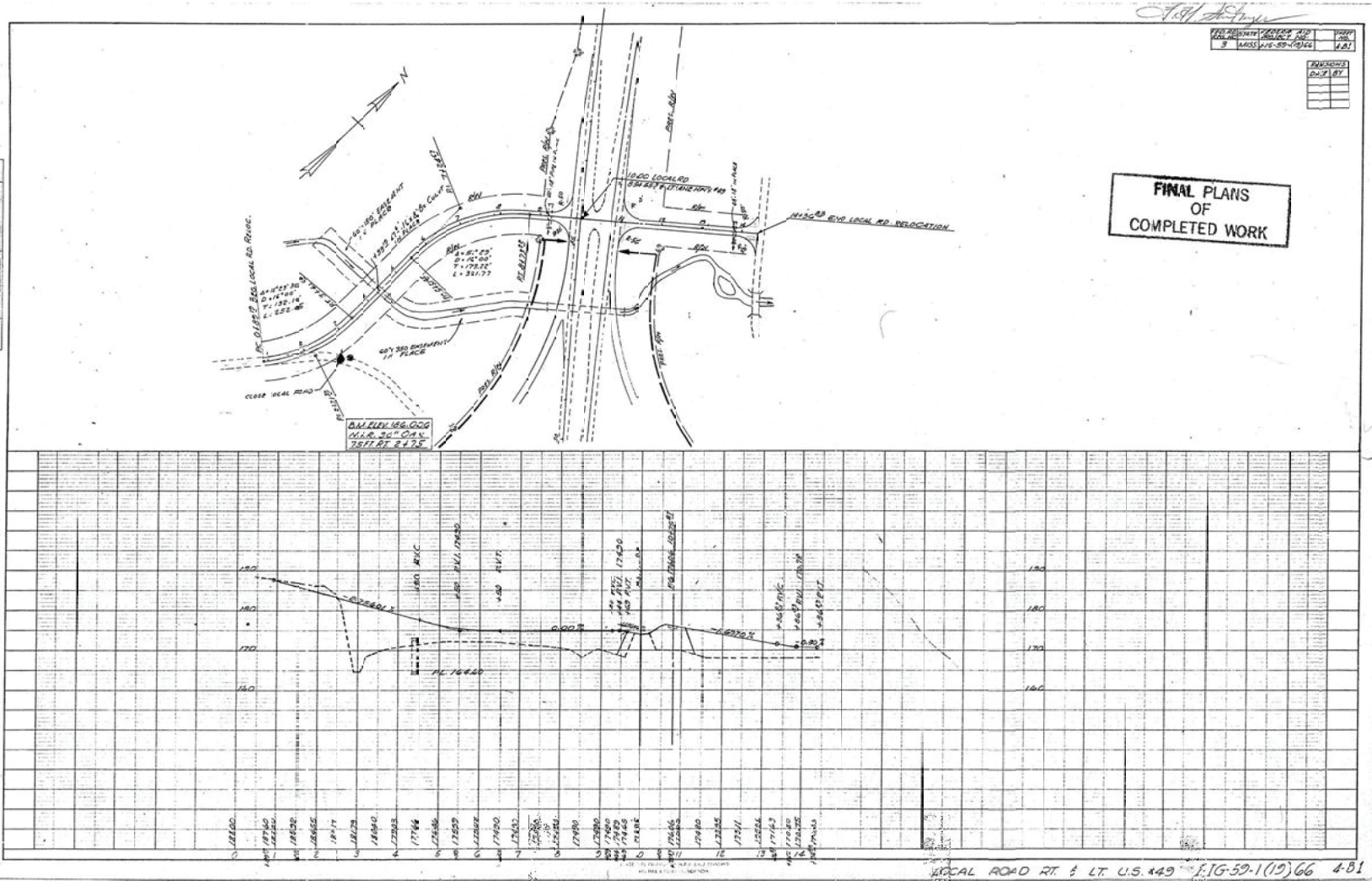
Notice To Bidders No. 3560 - Cont'd



PLAN  
 DRAWN BY  
 CHECKED BY  
 DATE

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LOCAL ROAD RT. & LT. U.S. 449  
 PROJECT NO. 11G-59-1(19)66  
 SHEET NO. 4-81





REV.	NO.	DATE	BY	APP.	DESC.
3		11/59			116-59-1(19)66-C

REVISIONS	DATE	BY

R.O.W. DATE  
 $\Delta = 40^{\circ}29'30''$   
 $D = 6^{\circ}47'45.14''$   
 $T = 315.19'$   
 $L = 603.26'$   
 $R = 943.20'$

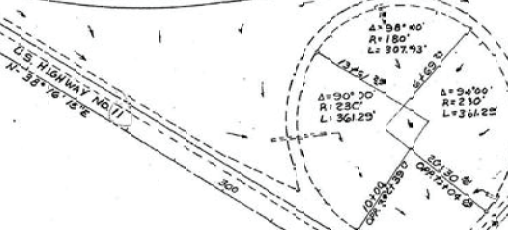
R.O.W. DATE  
 $\Delta = 35^{\circ}17'50''$   
 $D = 2^{\circ}25'22.55''$   
 $T = 175.53'$   
 $L = 329.32'$   
 $R = 771.8'$

$\Delta = 40^{\circ}59'30''$   
 $D = 6^{\circ}00''$   
 $T = 356.95'$   
 $L = 685.19'$   
 $C = 922.91'$

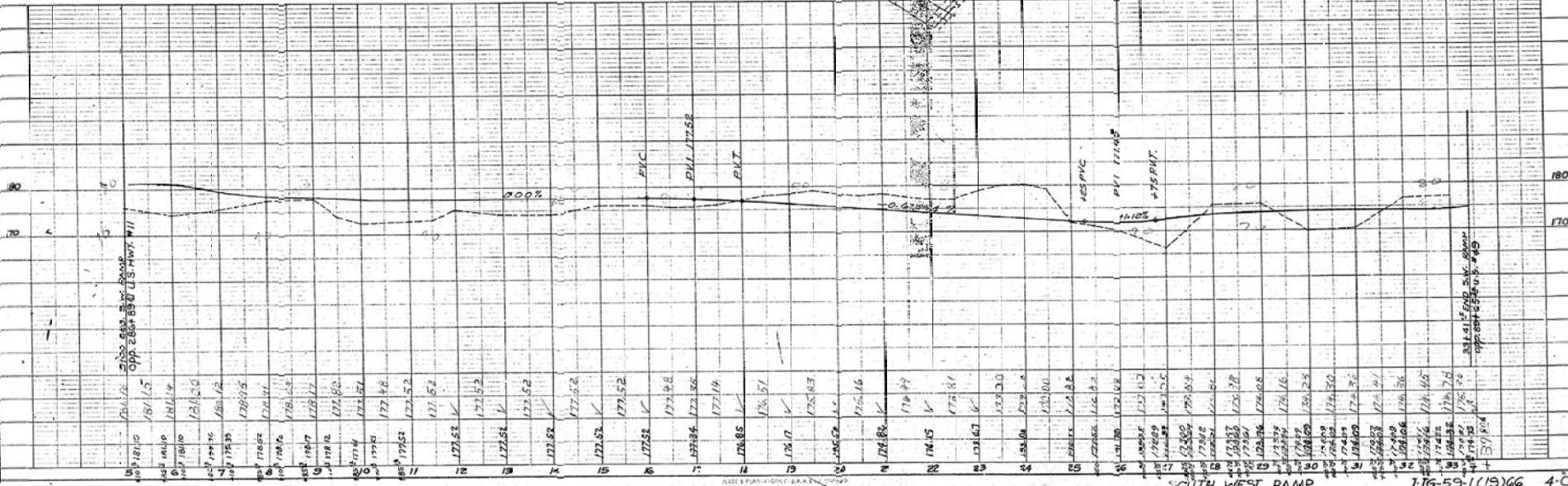
$\Delta = 29^{\circ}13'00''$   
 $D = 5^{\circ}45'56''$   
 $T = 103.15'$   
 $L = 180.76'$

**FINAL PLANS  
 OF  
 COMPLETED WORK**

SOUTH WEST RAMP



103



114.5 MIN. ST. 8.58 +09.2 odd  
 30000 30000 30000 30000

Sta. 10+00 to 30+00  
 30000 30000 30000 30000

SOUTH WEST RAMP 116-59-1(19)66 4-C

Notice To Bidders No. 3560--Cont'd

43

PLAN  
 DATE: 11/59  
 DRAWN BY: [illegible]  
 CHECKED BY: [illegible]

PROFILE  
 DATE: 11/59  
 DRAWN BY: [illegible]  
 CHECKED BY: [illegible]



FEDERAL AID  
 PROJ. NO. 571-59-105(1)G 4-0

FINAL PLANS  
 OF  
 COMPLETED WORK

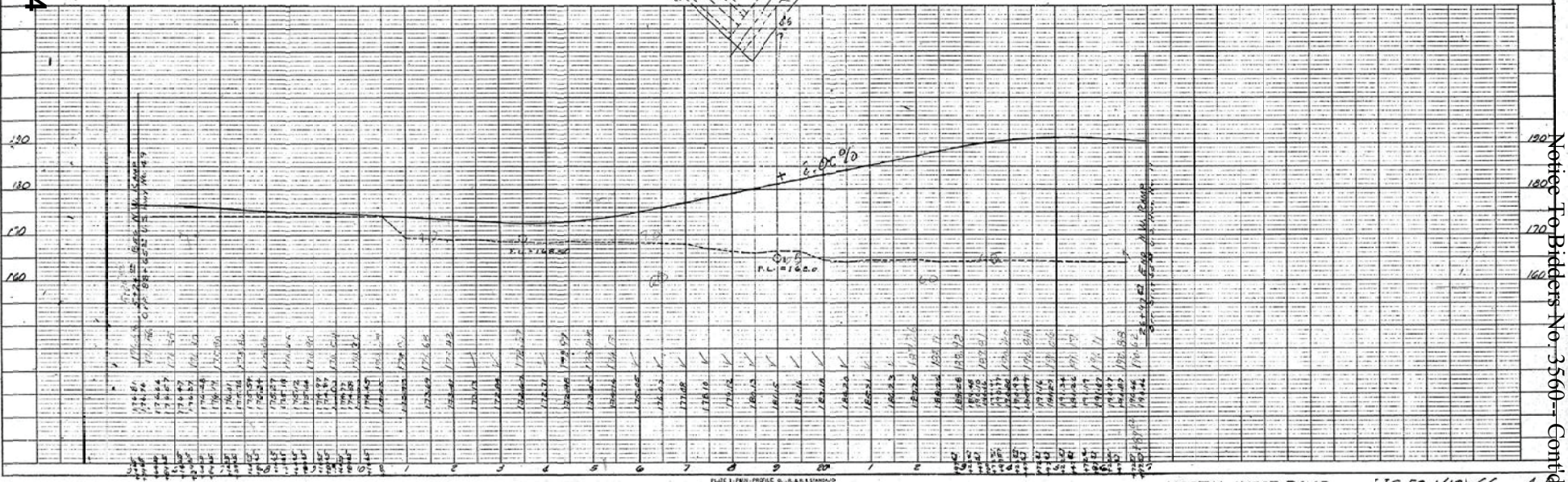
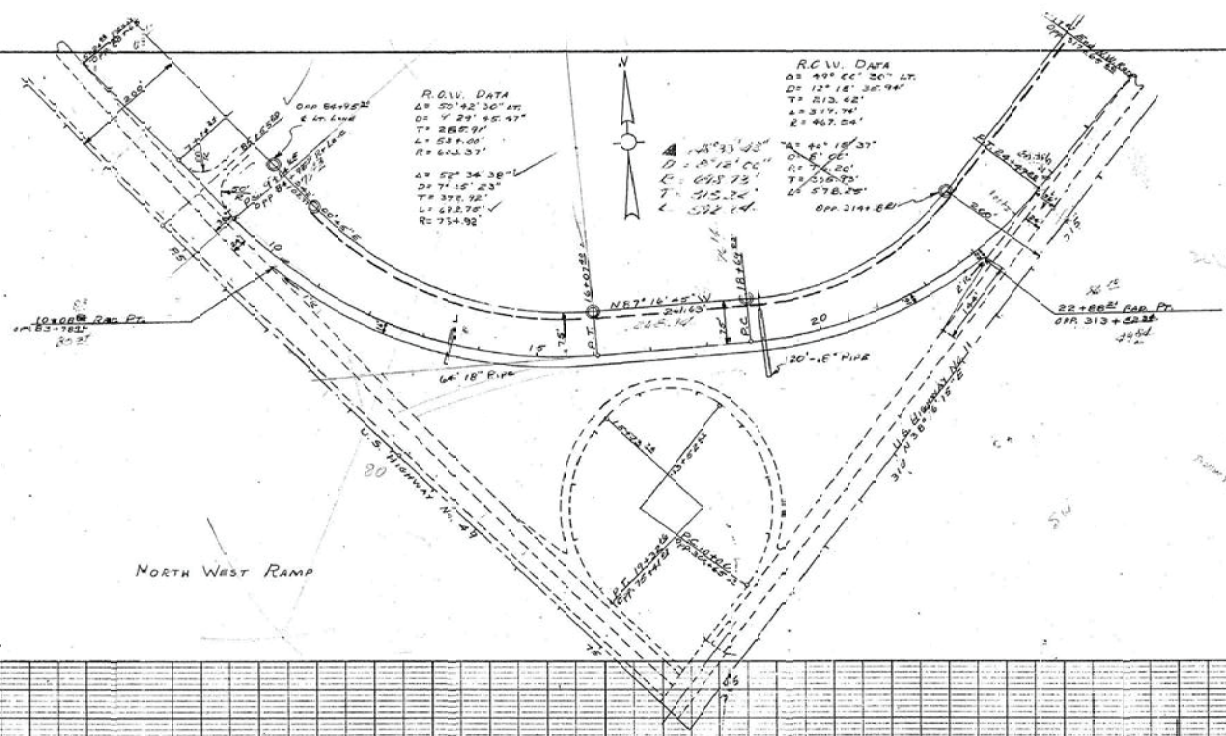


PLATE 1 - PLAN - PROFILE G. H. & R. STANHOPE  
 MAPLE & BIRD CO. NEW YORK

NORTH WEST RAMP 116-59-1(12) 66 4-0

Notice To Bidders No. 3560-Cont'd

104

14

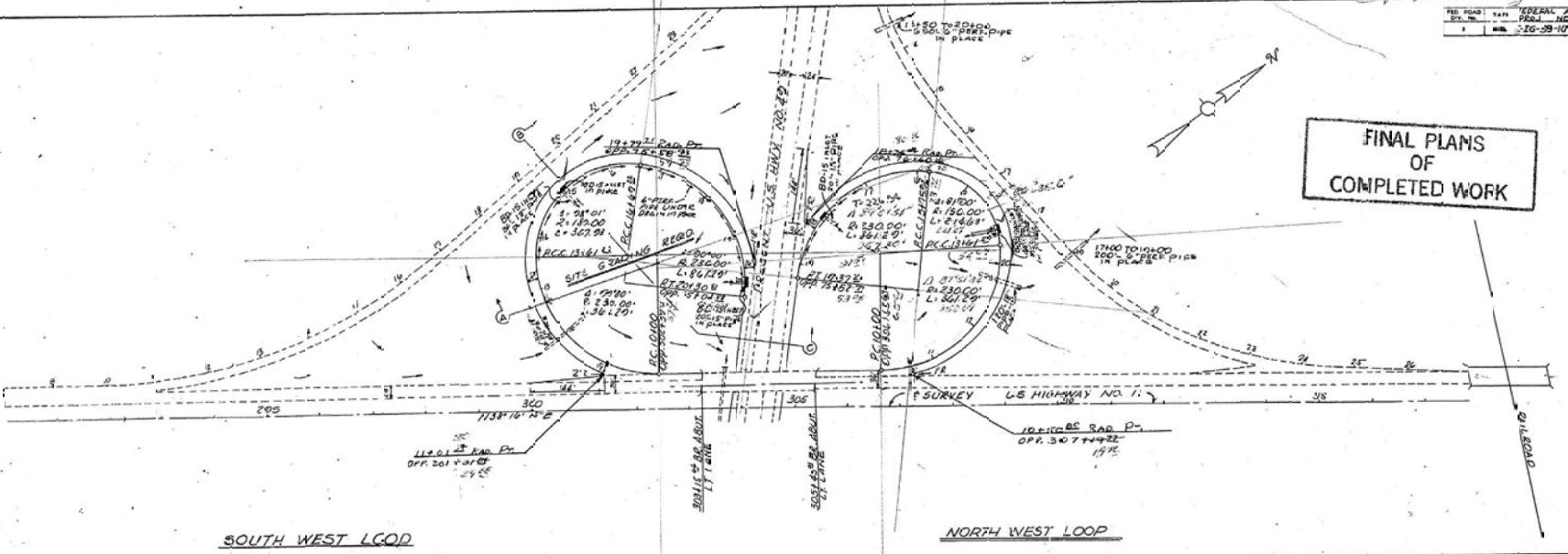
PLAN  
 SHEET NO. 104  
 OF 104 SHEETS

PROFILE  
 SHEET NO. 104  
 OF 104 SHEETS



REVISIONS	DATE	BY

FINAL PLANS  
OF  
COMPLETED WORK



105

60+00 INLETS TO BE COMPLETED.  
SEE 54' 2.0' FOR DETAILS & QUANTITIES  
57+00 & 58+00 SOUTHWEST LOOP  
57+00 & 58+00 NORTH WEST LOOP

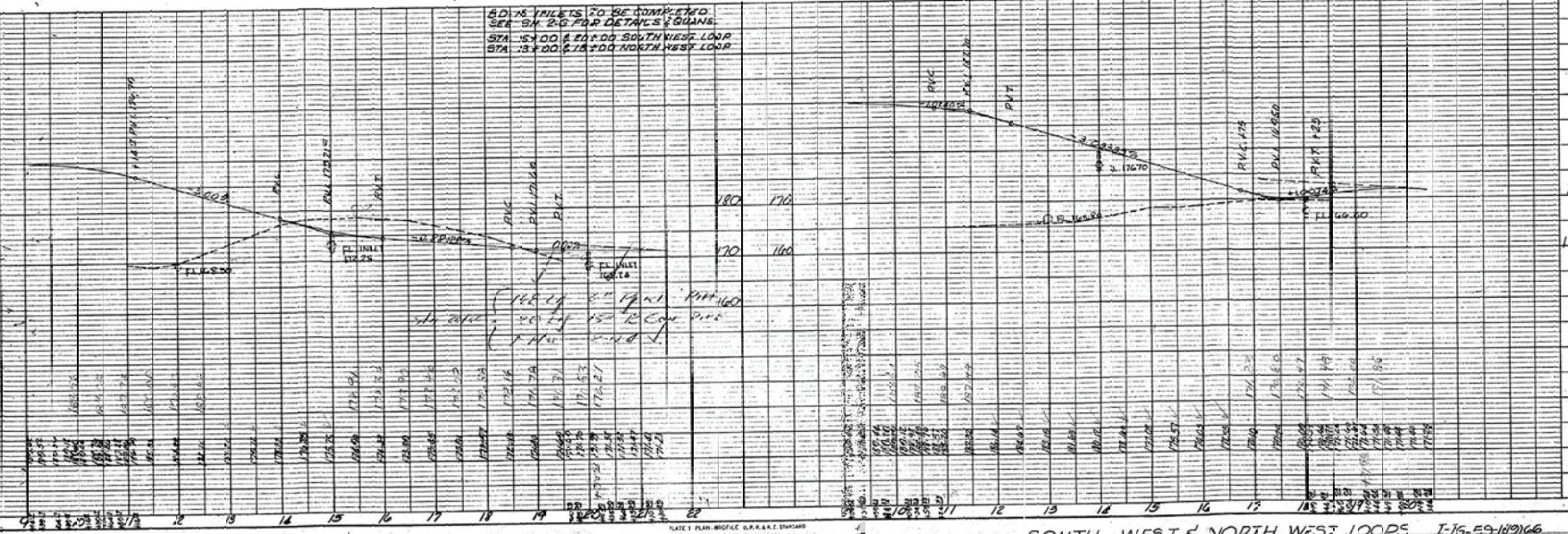


PLATE 1 PLAN, PROFILE, D.P.A. & FINISHED GRADES & ELEVATIONS, NEW YORK

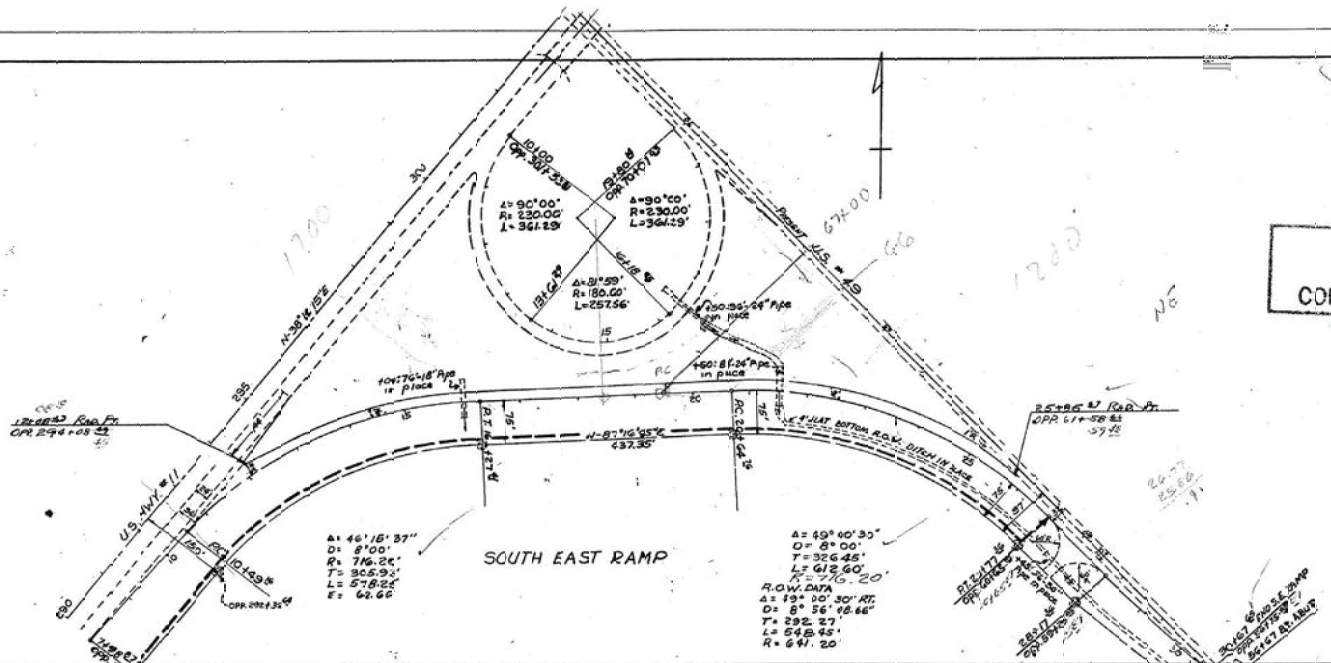
Notice To Bidders No. 3560 - Conn.



NO. 106 PLAN HORIZONTAL AID  
 1-16-59-1(13)66 4-F

REVISED	DATE

FINAL PLANS  
 OF  
 COMPLETED WORK



106

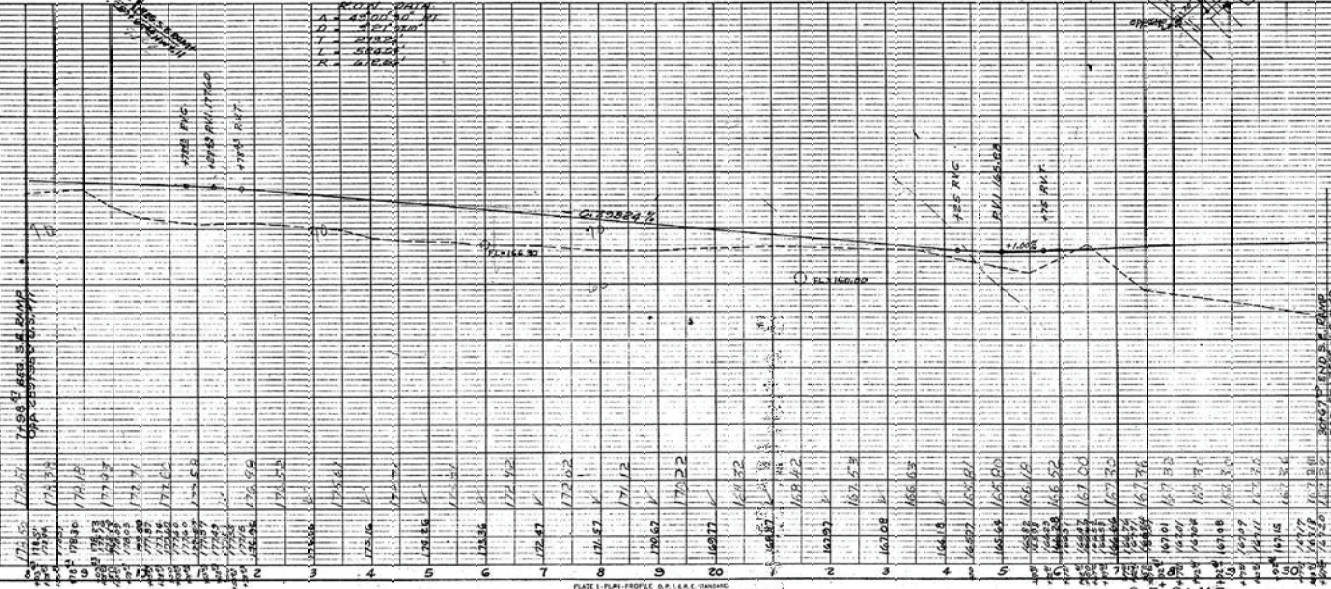


PLATE 1 - PLAN, PROFILE & L&L (L&L) DRAWING  
 HORIZONTAL AID - 106

1-16-59-1(13)66 4-F

106

Notice To Bidders No. 3560 - Cont'd

PLAN  
 HORIZONTAL AID  
 106

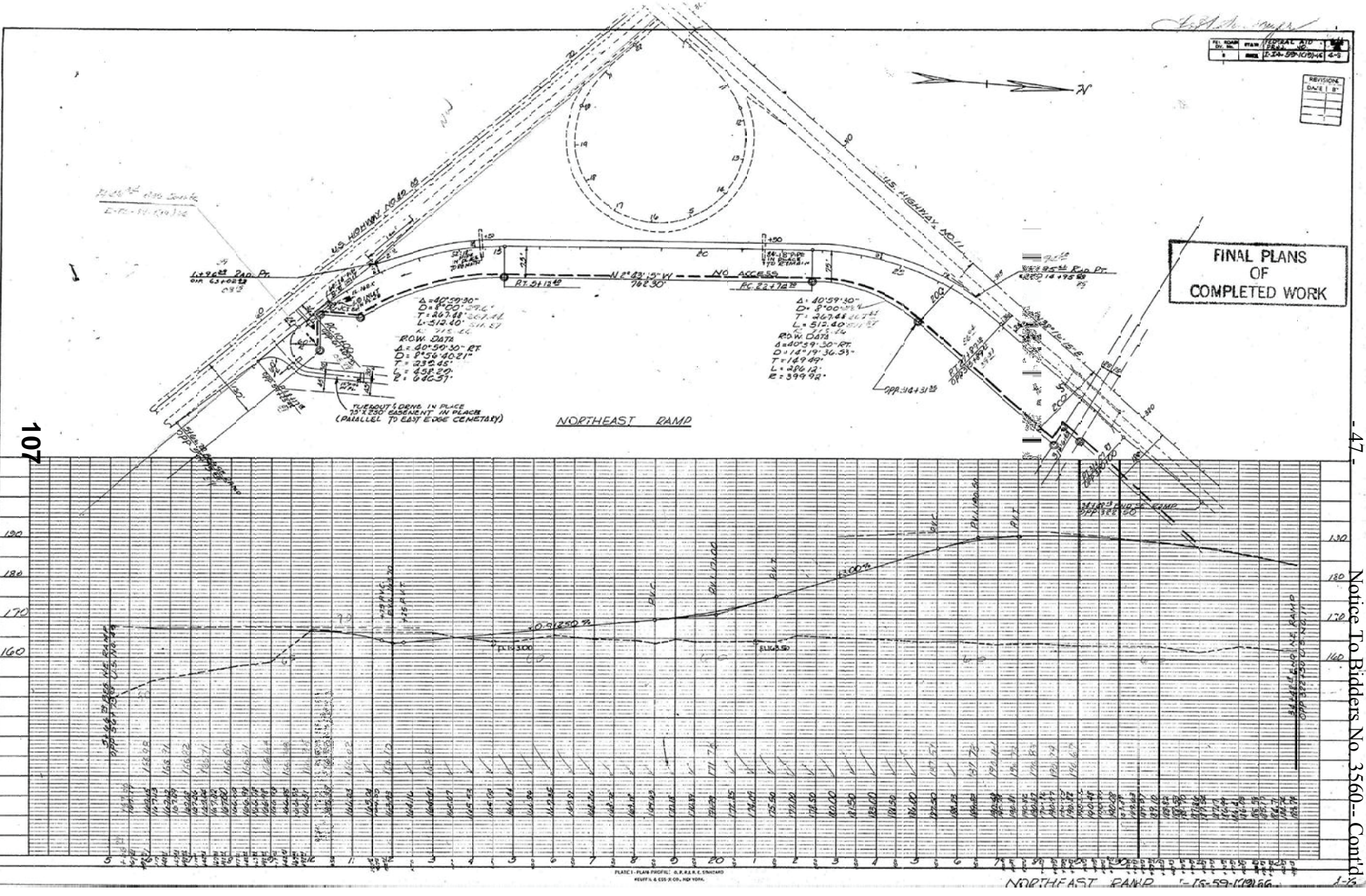
PROFILE  
 106



NO.	DATE	REVISION	BY
1	12-24-59	100%	4-3

REVISION	DATE	BY

**FINAL PLANS  
OF  
COMPLETED WORK**



**NORTHEAST RAMP**

**NORTHEAST RAMP**

PLATE 1 - PLAN PROFILE: S.P. R.E. & S. CHICAGO  
HEAVY & LIGHT & CO., INC. ILL.

PLAN  
DATE: 12-24-59  
BY: [Signature]

PROFILE  
DATE: 12-24-59  
BY: [Signature]

107

47

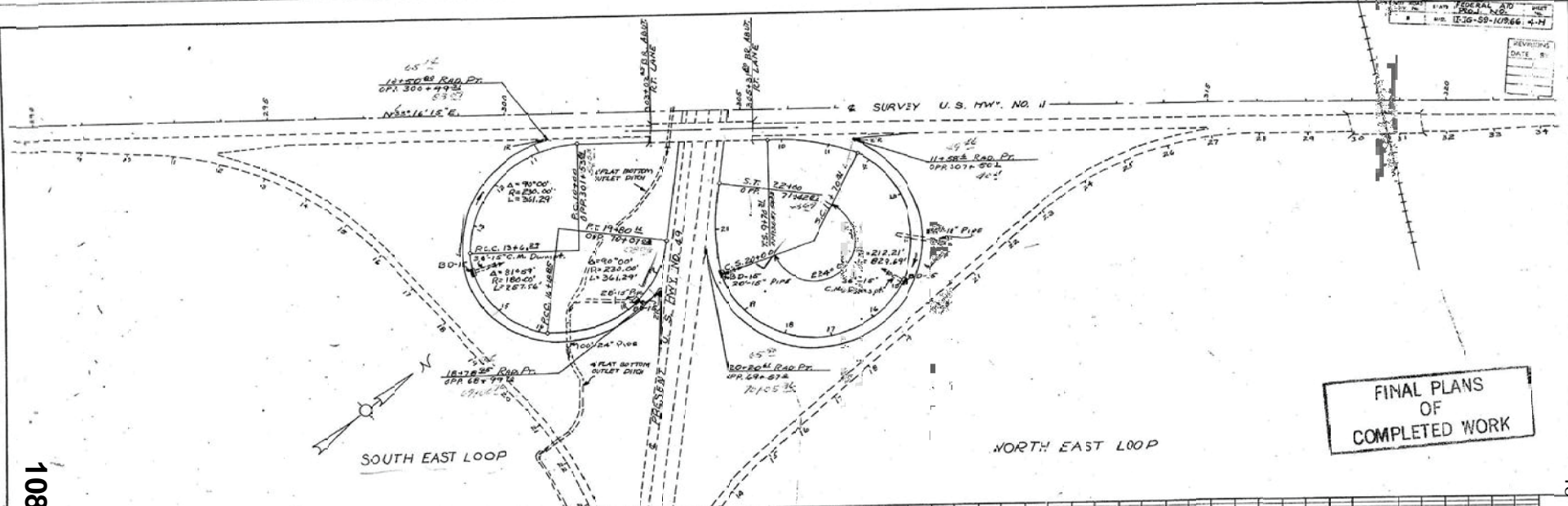
Notice To Bidders No. 3560--Cont'd

1-73



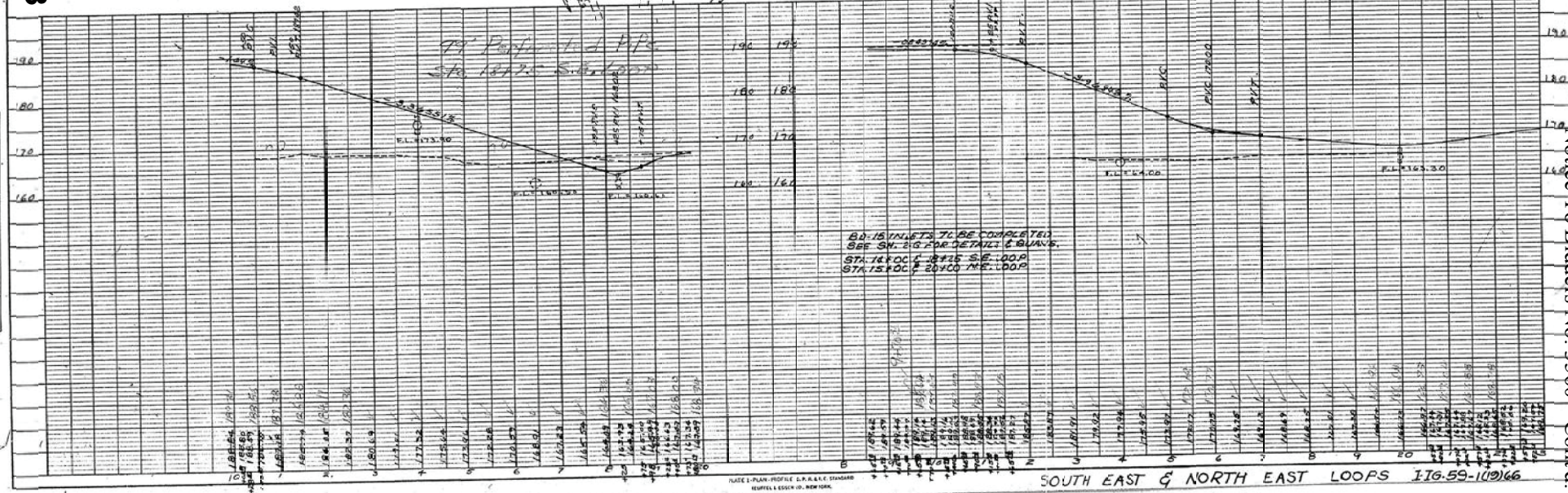
PLAN  
 PROJECT NO. 100-1-10  
 SHEET NO. 108  
 DATE: 11-16-59

PROFILE  
 PROJECT NO. 100-1-10  
 SHEET NO. 108  
 DATE: 11-16-59



FINAL PLANS  
 OF  
 COMPLETED WORK

108



SOUTH EAST & NORTH EAST LOOPS IIG-59-109/66

Notice To Bidders No. 35601-Cont'd



*J. D. Payne*

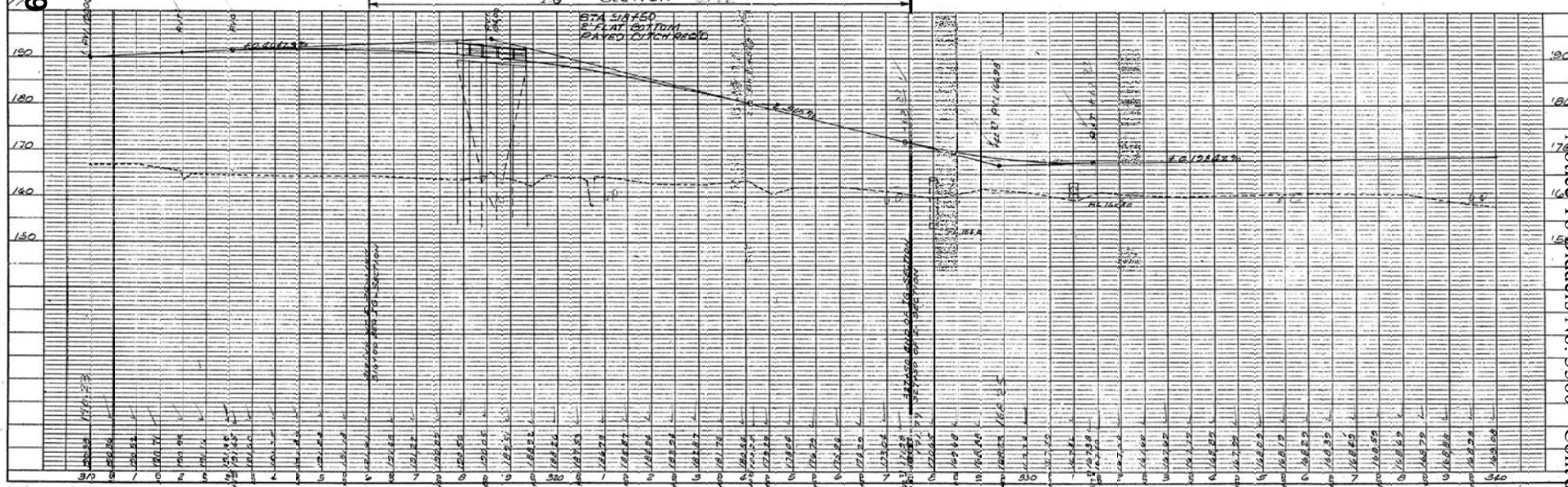
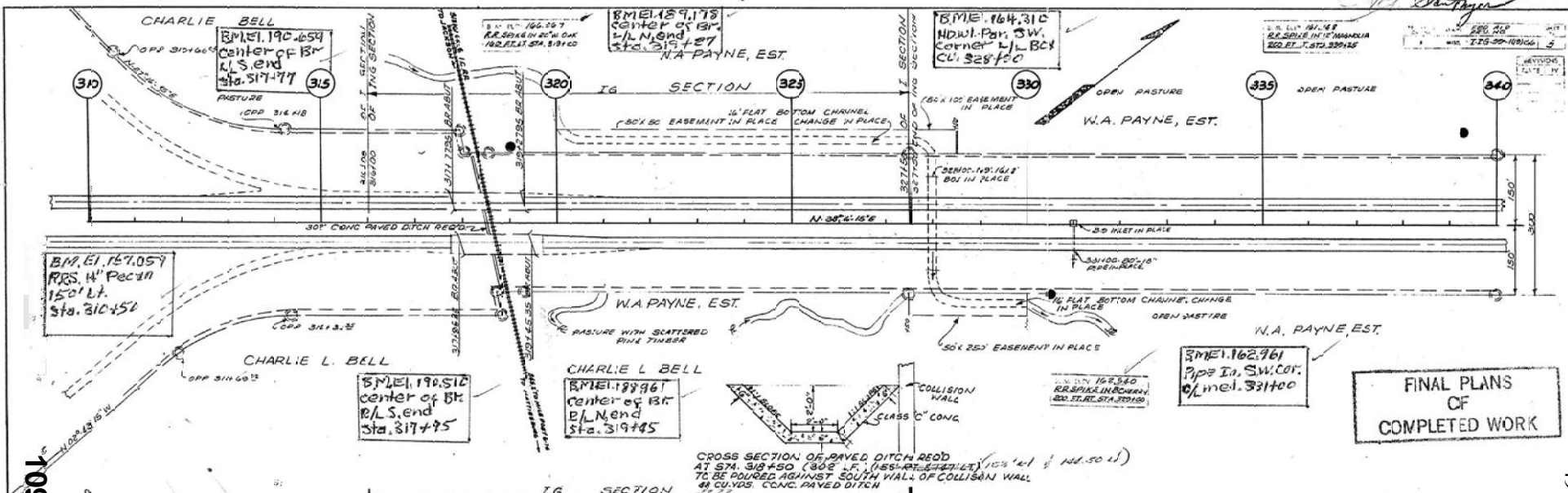


PLATE 1 - PLAN - PROFILE OF P.A. & C.R. E. 1514 0480  
REVISED EDITION, 10/10/10

T.G.-52-1(7) G.G

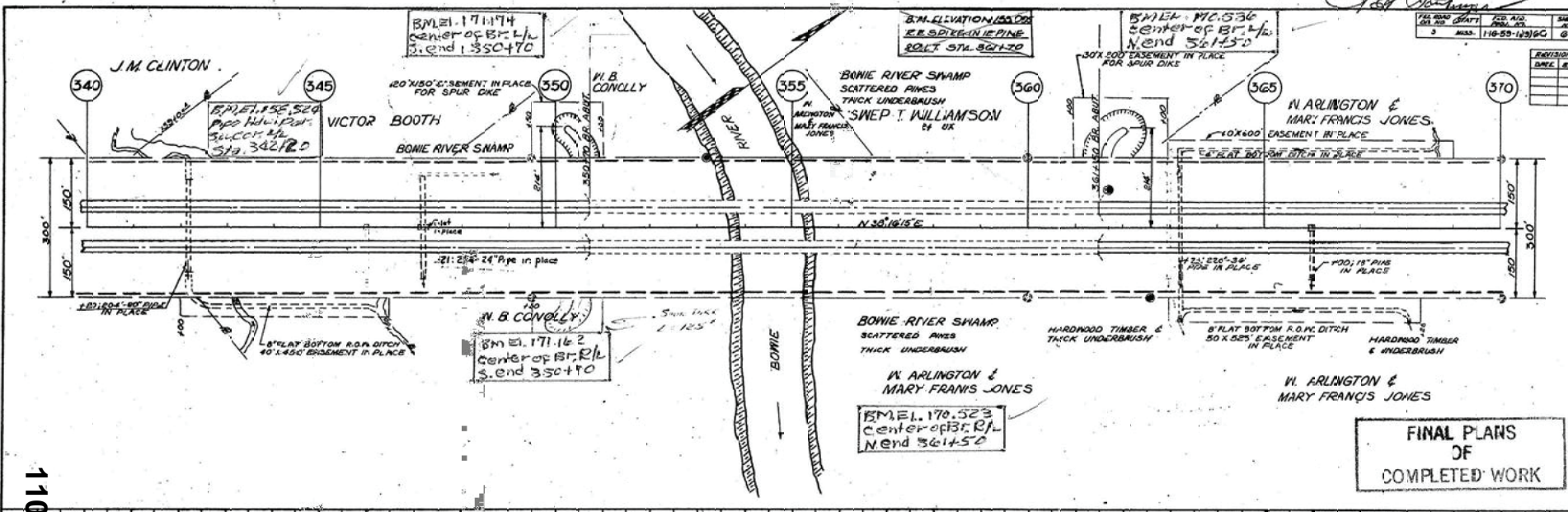
49 - Notice To Bidders No. 3560 - Cont'd



*J. J. [Signature]*  
 THE NEW BRITAIN ENGINEERING CO.  
 110-55-11916C

DATE	NO.	BY
3	110-55-11916C	G

REVISIONS	DATE	BY



**FINAL PLANS  
 OF  
 COMPLETED WORK**

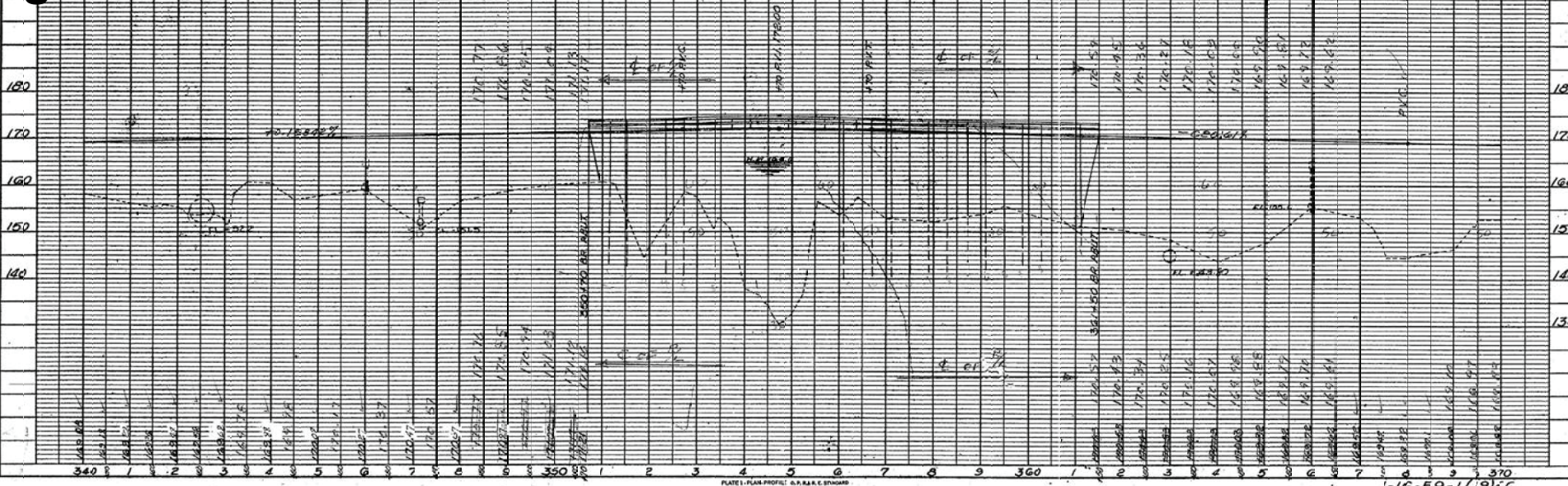


PLATE 1 PLAN PROFILE S.P. & A.E. STUBBS  
 AGENTS & ENGINEERS, NEW YORK

1-16-59-111916C

PLAN	DATE	NO.	BY

PROFILE	DATE	NO.	BY

110

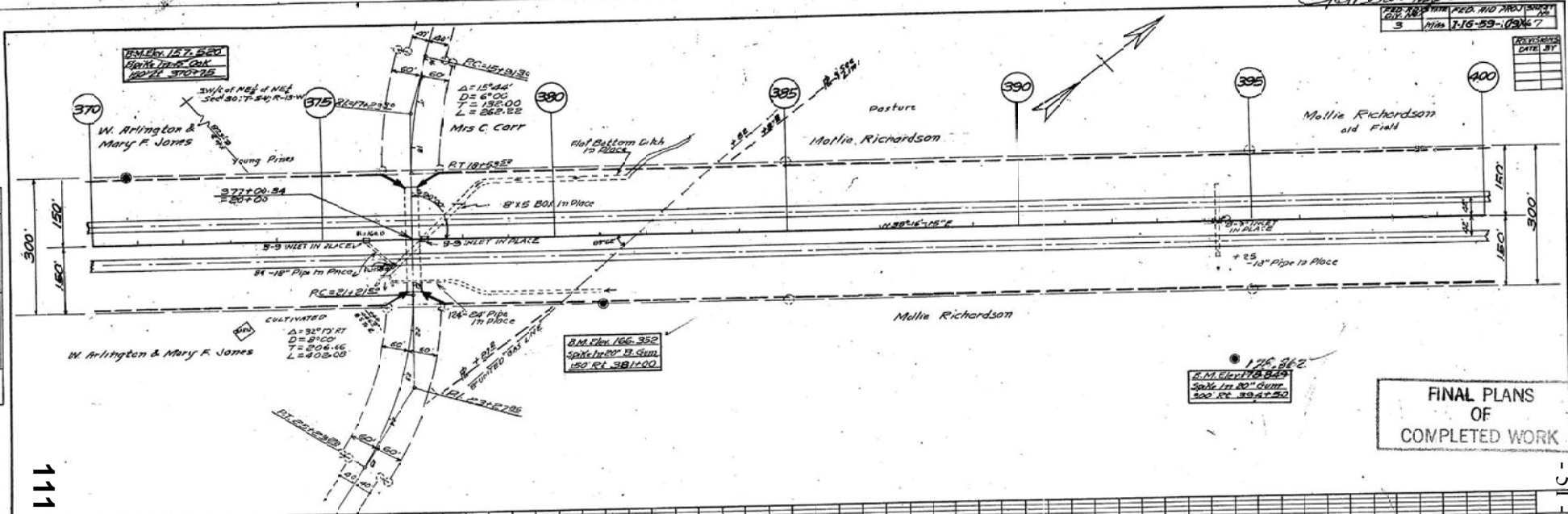
50

Notice To Bidders No. 3560-Cont'd



DATE: 7-16-59  
 3  
 7-16-59-10267

DATE	BY



**FINAL PLANS  
 OF  
 COMPLETED WORK**

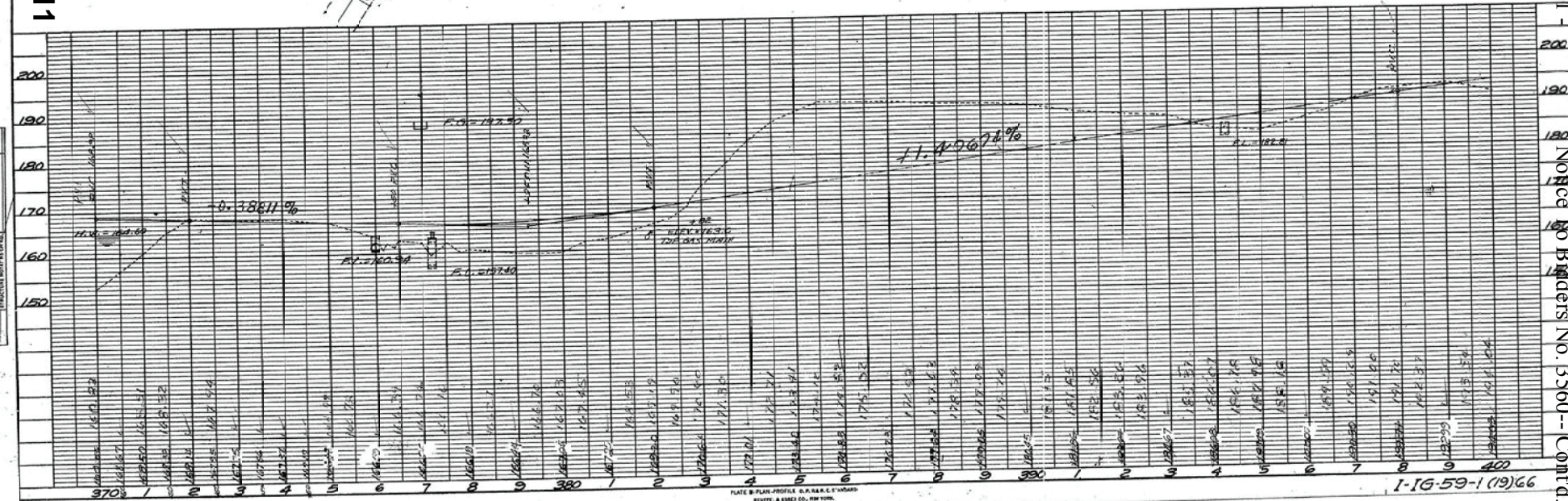


PLATE B - PLAN PROFILE U.S. & A.E.C. FORM NO. 1  
 EQUIP. & ENGR. CO., NEW YORK

1-16-59-1(19)66

Notice to Bidders No. 3560--Cont'd

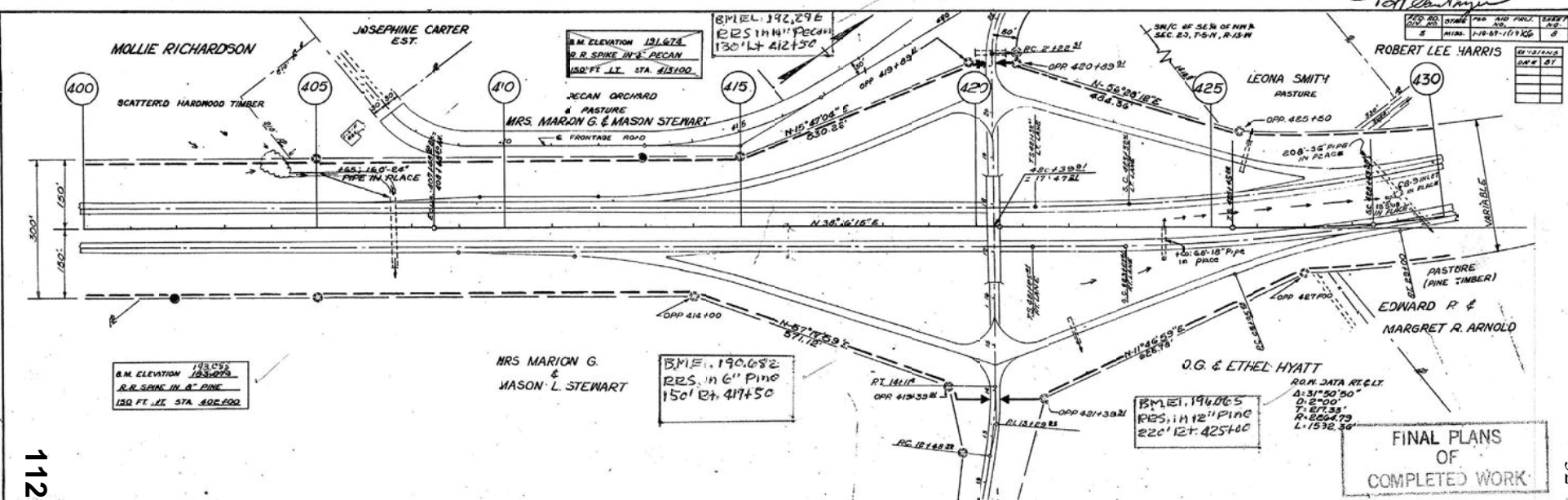
PLAN  
 DATE: 7-16-59  
 DRAWN BY: J. J. [unclear]  
 CHECKED BY: [unclear]  
 IN CHARGE: [unclear]

PROFILE  
 DATE: 7-16-59  
 DRAWN BY: J. J. [unclear]  
 CHECKED BY: [unclear]  
 IN CHARGE: [unclear]

111



*J.H. Hartman*

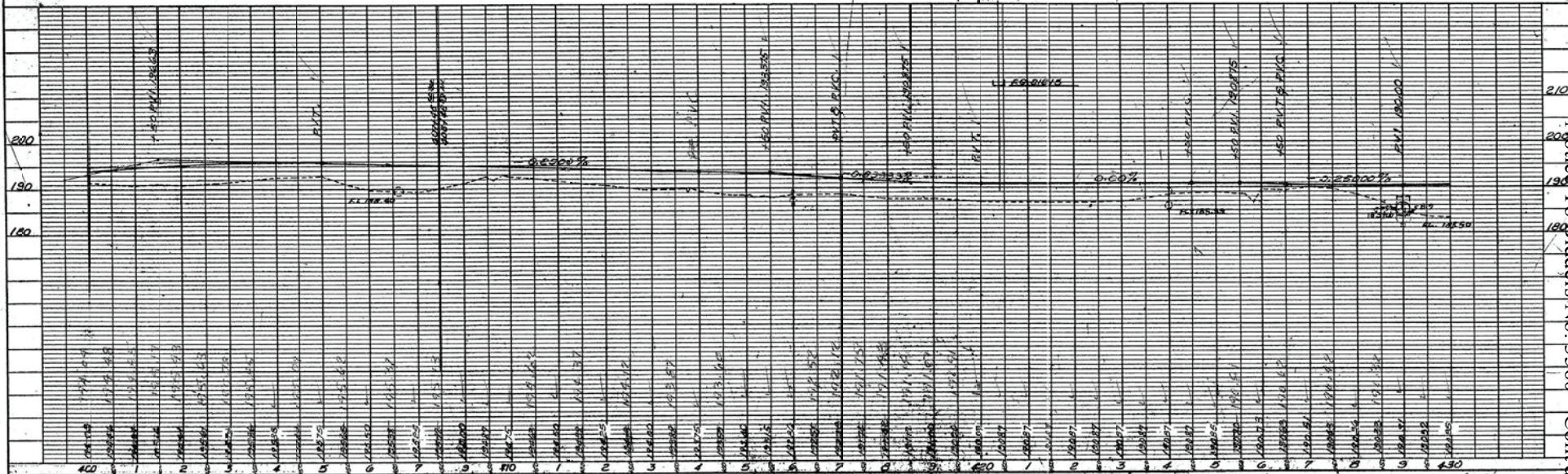


NO.	DATE	BY	REVISION
1	1-18-59	J.H. Hartman	FINAL

NO.	DATE	BY	REVISION
1	1-18-59	J.H. Hartman	FINAL

NO.	DATE	BY	REVISION
1	1-18-59	J.H. Hartman	FINAL

112



PLANS 170M-1000A, 1700A, 1700B, 1700C, 1700D, 1700E, 1700F, 1700G, 1700H, 1700I, 1700J, 1700K, 1700L, 1700M, 1700N, 1700O, 1700P, 1700Q, 1700R, 1700S, 1700T, 1700U, 1700V, 1700W, 1700X, 1700Y, 1700Z

1-13-59-1(19)66

Notice To Bidders No. 3560--Cont'd



FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	MISS.	65-10166	8-A	

DRAWN BY	DATE

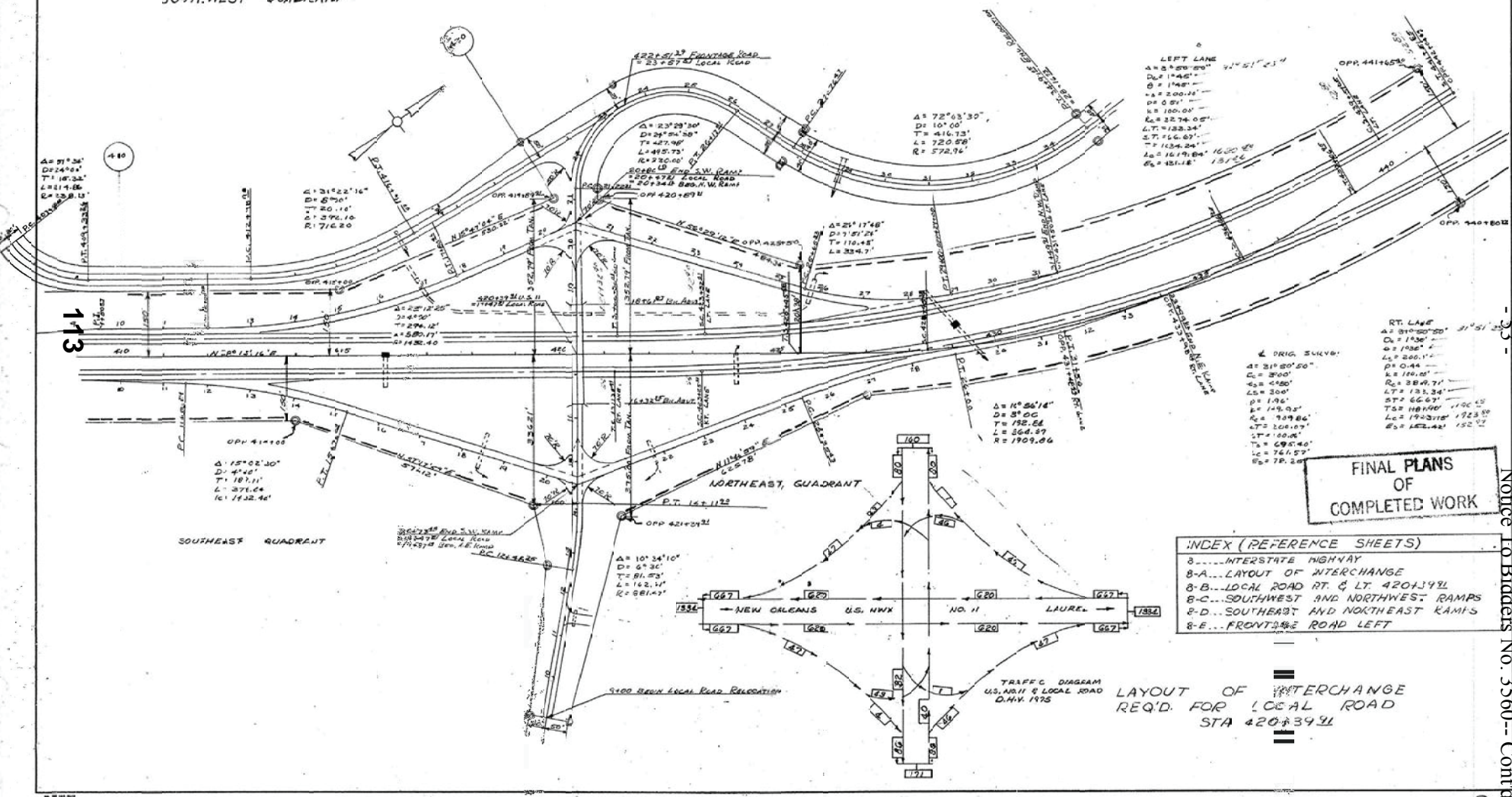
  

REVISIONS	
DATE	BY

SOUTHWEST QUADRANT

Side Interference Quadrants for Data

NORTHWEST QUADRANT



LEFT LANE  
 $\Delta = 23^{\circ}28'30''$   
 $D = 101'00''$   
 $T = 416.33'$   
 $L = 720.58'$   
 $R = 572.96'$

RT. LANE  
 $\Delta = 31^{\circ}20'00''$   
 $D = 1136'$   
 $T = 196'$   
 $L = 200.1'$   
 $R = 100.0'$   
 $\Delta = 22^{\circ}34'00''$   
 $D = 133.34'$   
 $T = 116.60'$   
 $L = 1619.04'$   
 $R = 151.18'$

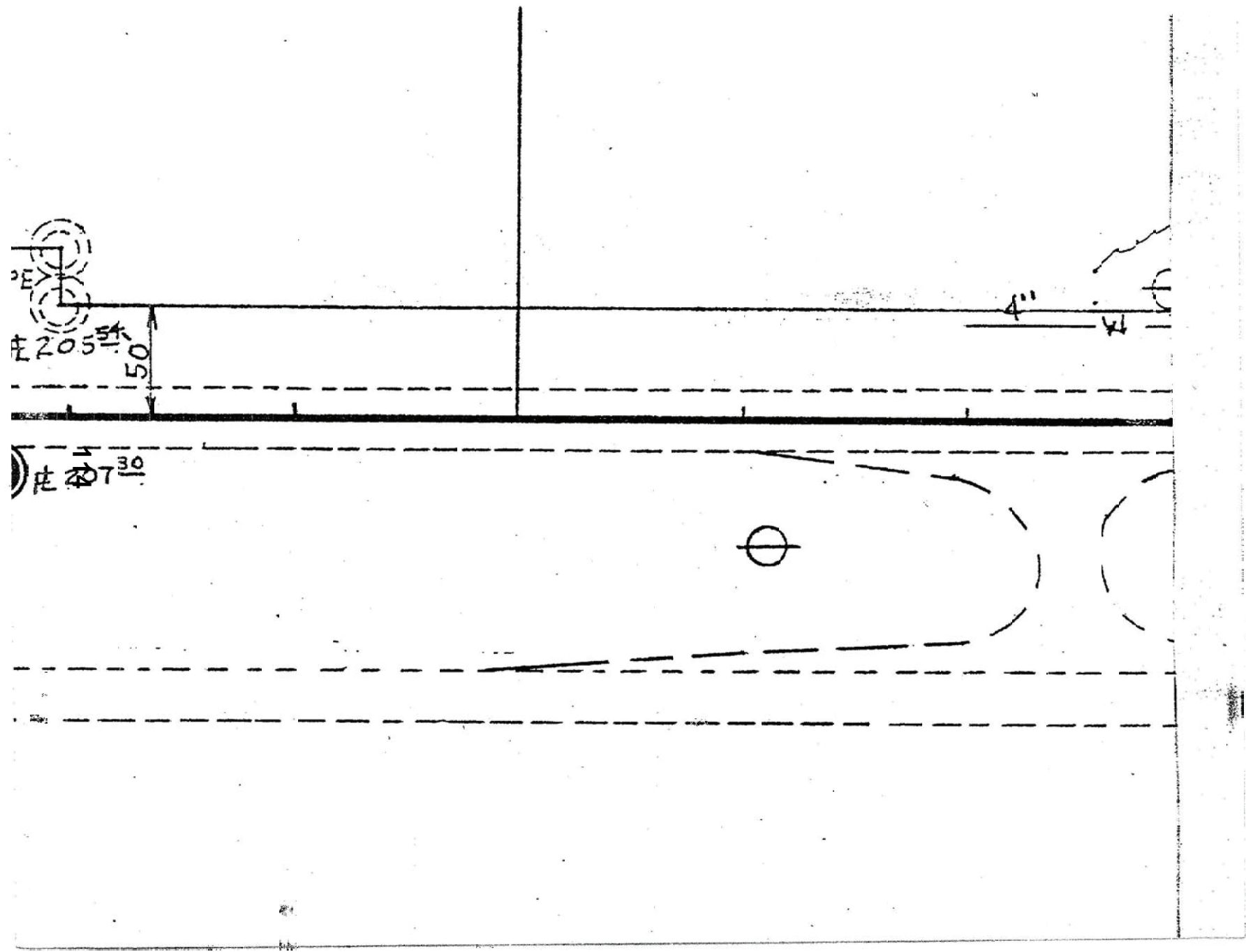
**FINAL PLANS OF COMPLETED WORK**

- INDEX (REFERENCE SHEETS)
- 8-... INTERSTATE HIGHWAY
  - 8-A-... LAYOUT OF INTERCHANGE
  - 8-B-... LOCAL ROAD RT. & LT. 420+39.2
  - 8-C-... SOUTHWEST AND NORTHWEST RAMPS
  - 8-D-... SOUTHWEST AND NORTHEAST RAMPS
  - 8-E-... FRONTAGE ROAD LEFT

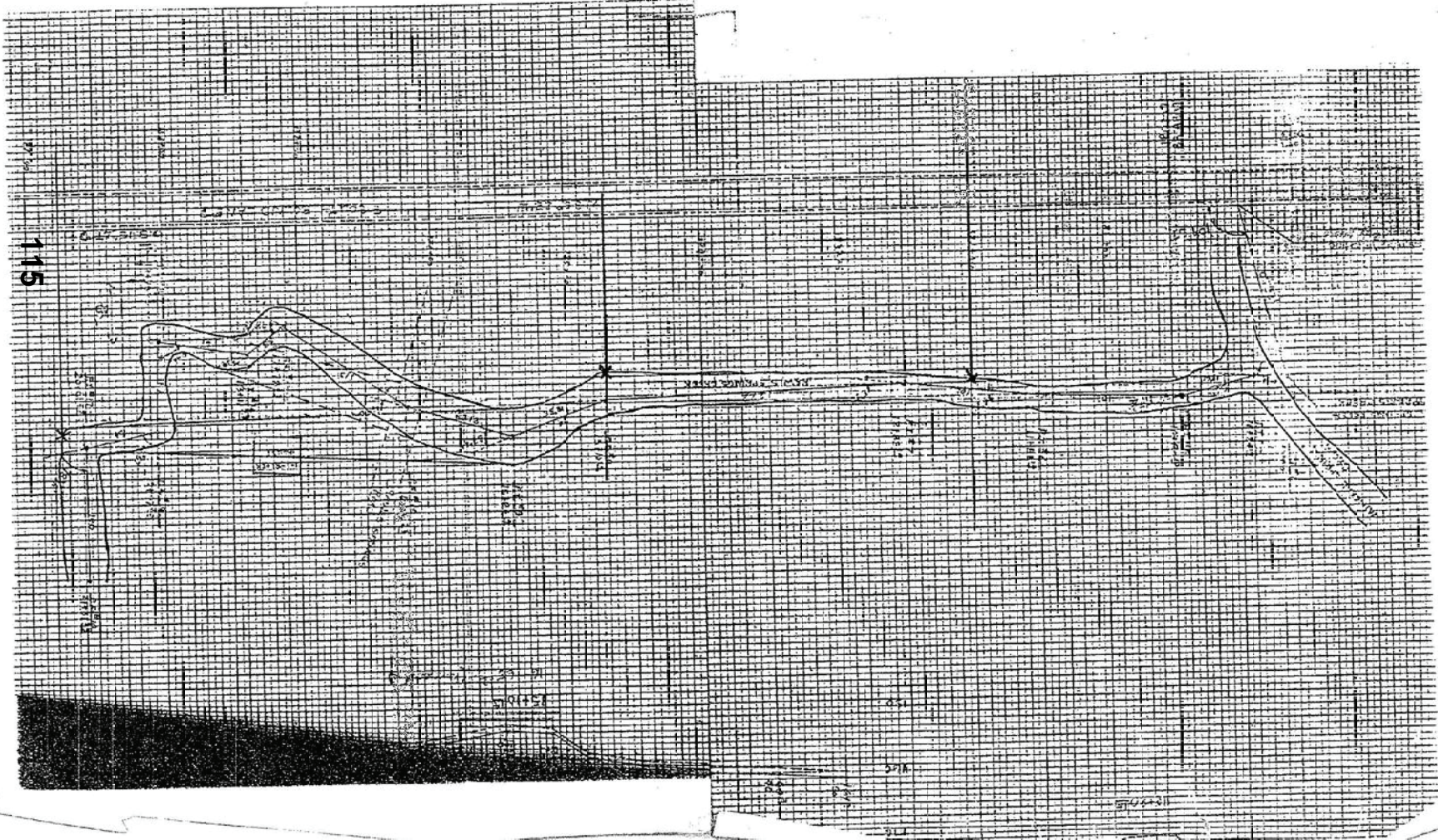
TRAFFIC DIAGRAM  
 U.S. HWY 11 & LOCAL ROAD  
 D.H.V. 1975

LAYOUT OF INTERCHANGE  
 REQ'D FOR LOCAL ROAD  
 STA 420+39.2

Notice To Bidders No. 3560--Cont'd







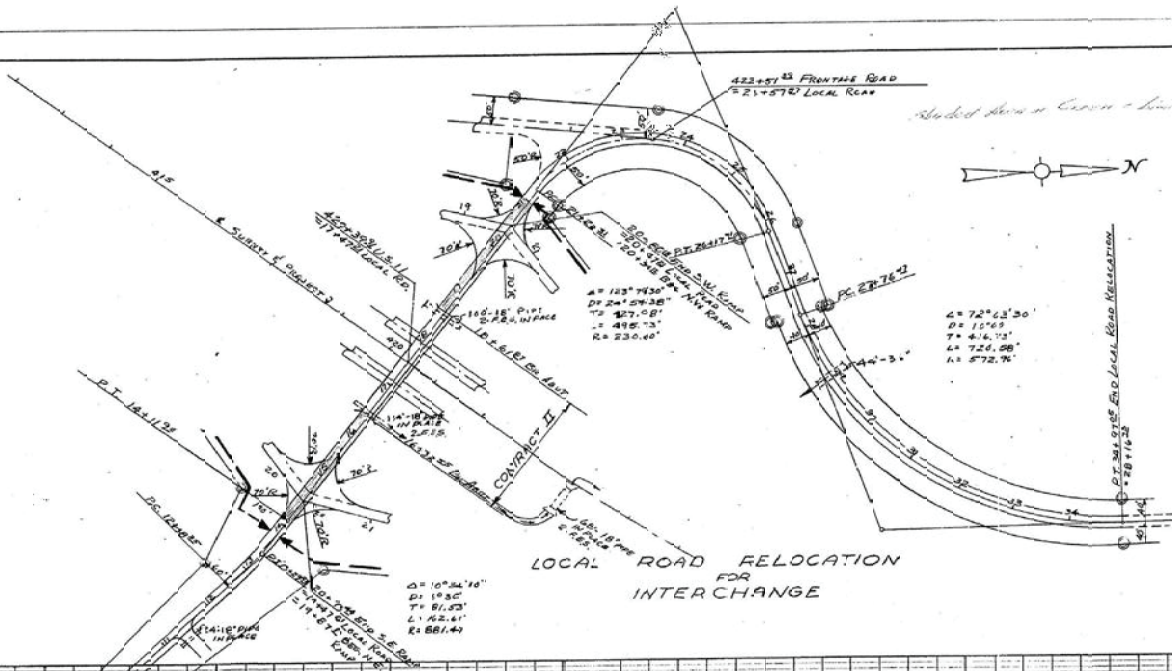
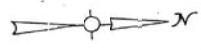


*Handwritten signature*

116 LOCAL ROAD RELOCATION PROJECT SHEET NO. 1 OF 2

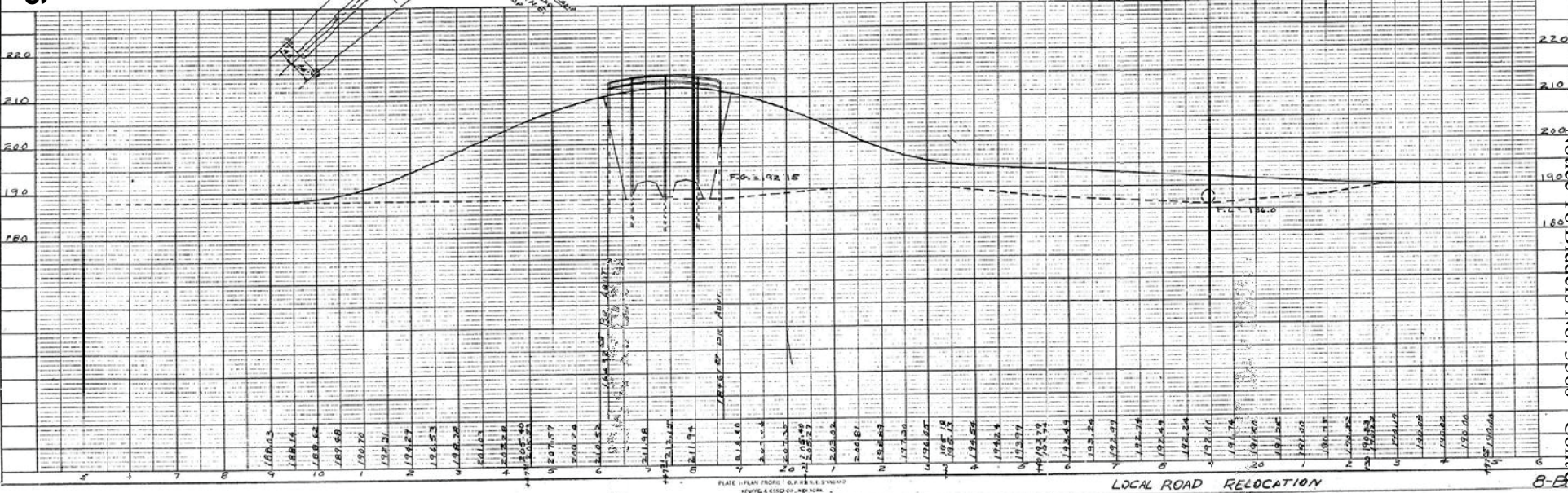
REVISION	DATE	BY

*Handwritten note:* Shaded Area is Crown - limits of the Bank on both sides



**FINAL PLANS  
OF  
COMPLETED WORK**

116



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Notice To Bidders No. 3560--Cont'd

LOCAL ROAD RELOCATION

87

PLAN  
PROJECT NO. 116  
SHEET NO. 1 OF 2

PROFILE  
PROJECT NO. 116  
SHEET NO. 1 OF 2

SCALE: 1" = 40' HORIZ. 1" = 4' VERT.

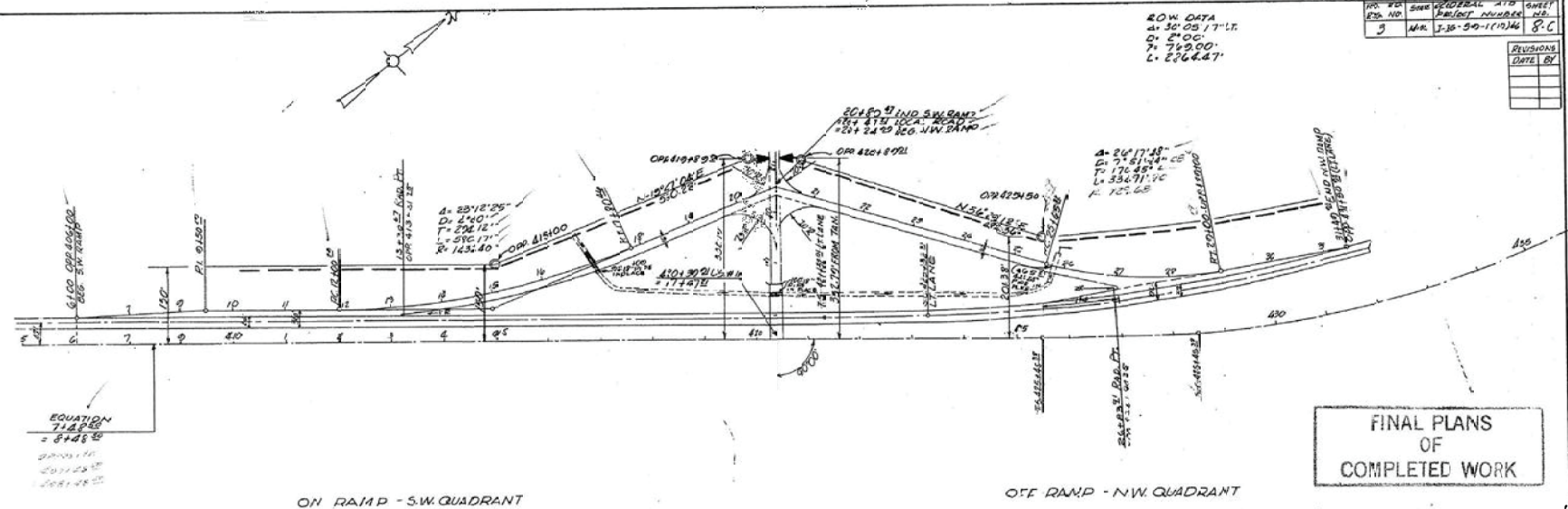


*J. J. [Signature]*

PROJECT NO.	DATE	GENERAL	BY
5	1-30-50	117	J.C.

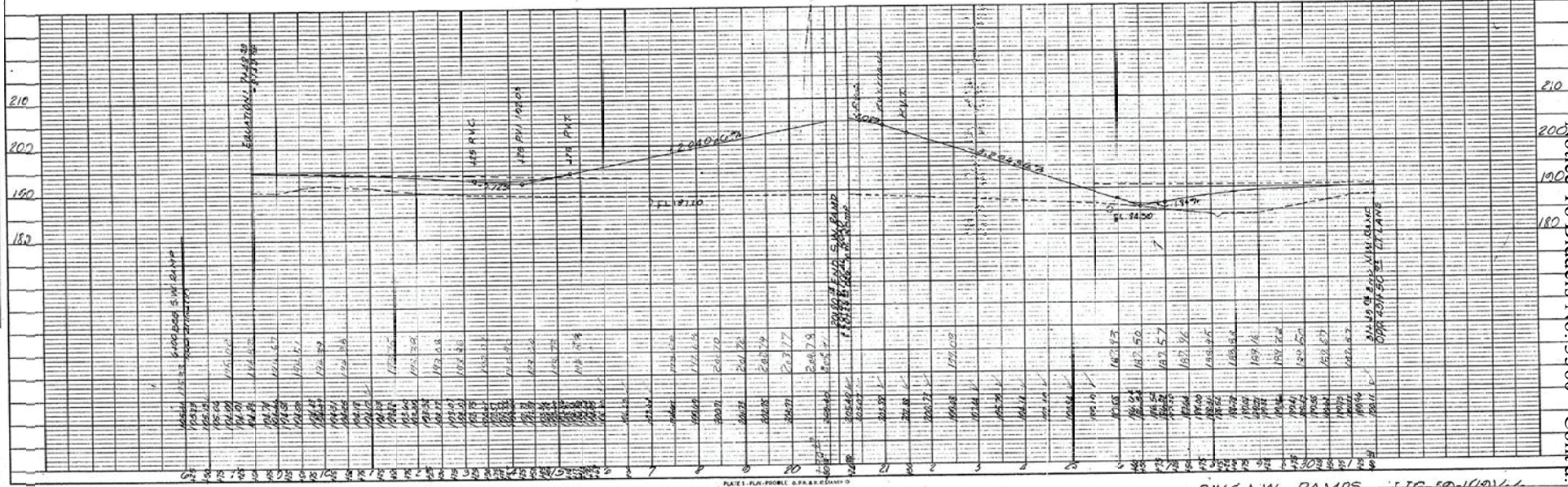
REVISIONS	DATE	BY

ROW DATA  
 A. 36' 00" 17" LF.  
 C. 24' 00"  
 F. 76' 00"  
 L. 2762.67'



FINAL PLANS  
 OF  
 COMPLETED WORK

117



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Notice To Bidders No. 3560--Cont'd

PLAN  
 DRAWING  
 PRINTED  
 BY THE  
 STATE OF  
 TEXAS

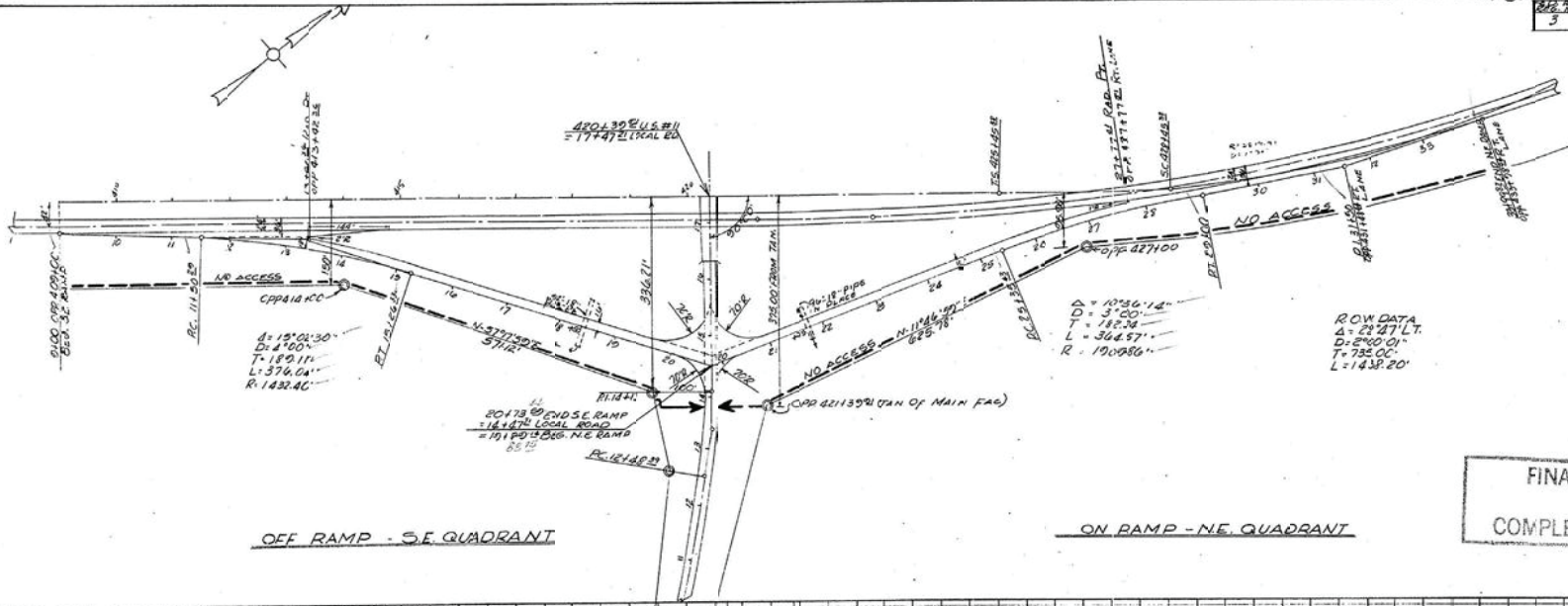
PROFILE  
 DRAWING  
 PRINTED  
 BY THE  
 STATE OF  
 TEXAS



*J.H. [Signature]*

3  
 DATE 07

REVISION	DATE



ROW DATA  
 $\Delta = 10^{\circ}50'14''$   
 $D = 5320'$   
 $T = 185.79'$   
 $L = 364.57'$   
 $R = 190086'$

FINAL PLANS  
 OF  
 COMPLETED WORK

OFF RAMP - SE QUADRANT

ON RAMP - NE QUADRANT

118

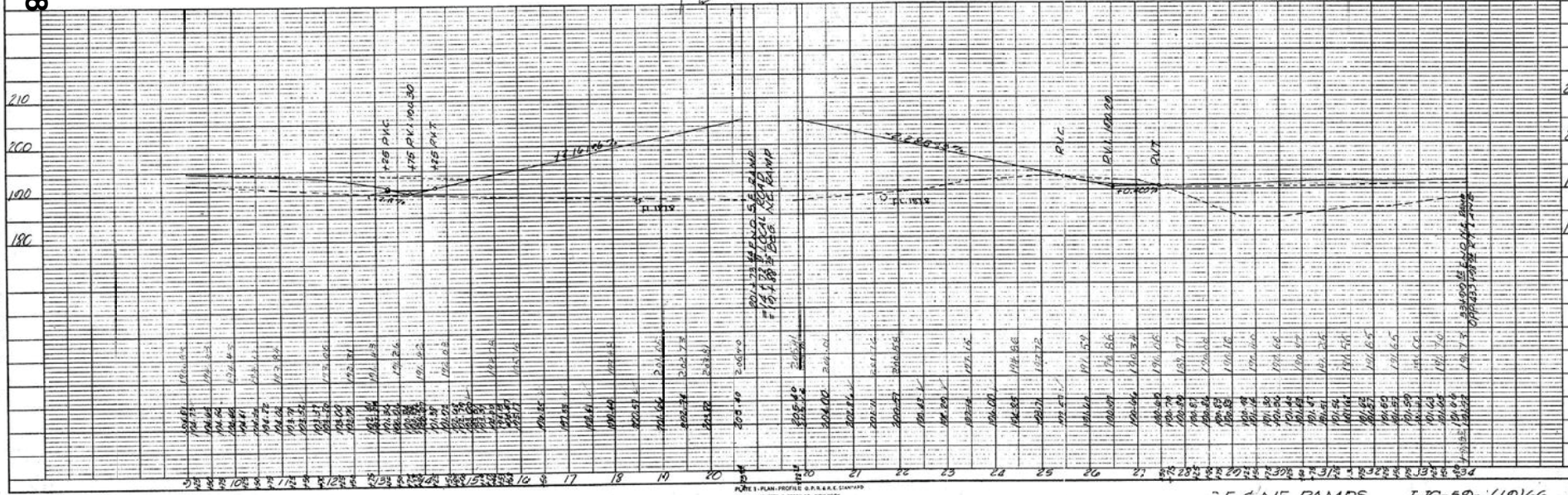


FIG. 1 - PLAN - PROFILE  
 KEUFFEL & ESSER CO., NEW YORK

S.E. & N.E. RAMPS I-10-50-(10)66 81

58

Notice To Bidders No. 3560 - Cont'd

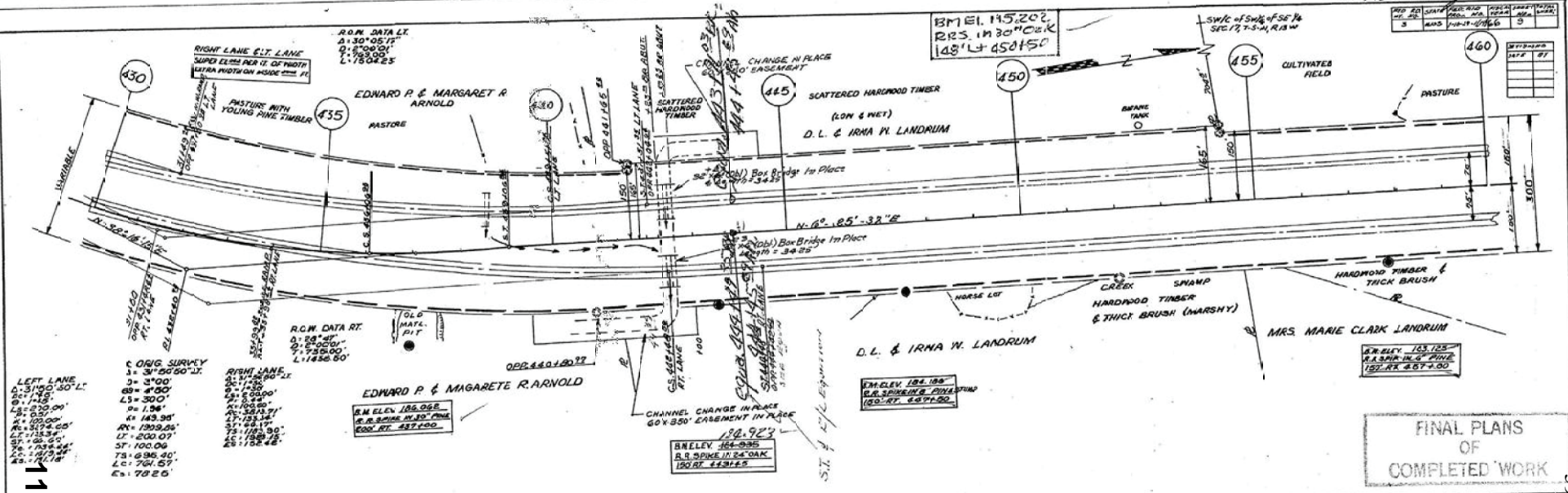
PLAN  
 SHEET NO. 118  
 DATE 07

PROFILE  
 SHEET NO. 118  
 DATE 07

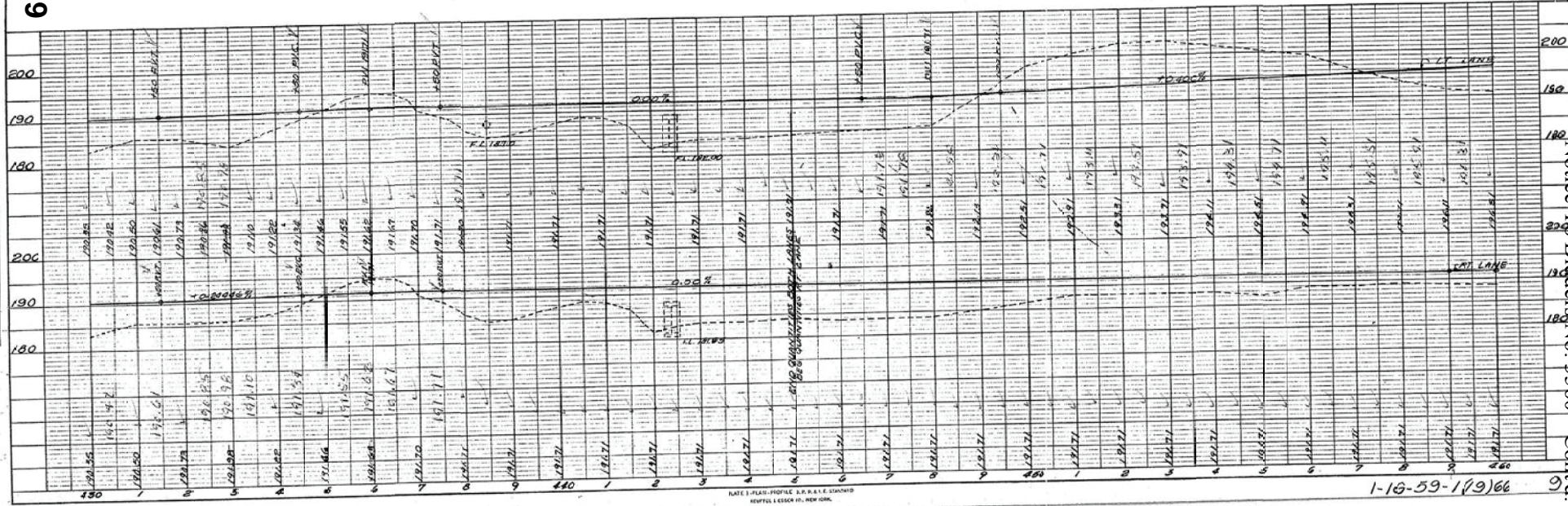


*J. G. Hartung*

NO. 20	DATE	DESCRIPTION	BY
3	11-11-66	FINAL PLANS	JGH



FINAL PLANS  
OF  
COMPLETED WORK



DATE: 1-16-59 PROFILE BY: R.A.L.E. SHAW  
SCALE: 1" = 40' HORIZ. 1" = 4' VERT.

1-16-59-1(9)66

Notice To Bidders, No. 3560--Cont'd.

PLAN  
NOTED: ALL DIMENSIONS ARE IN FEET UNLESS OTHERWISE NOTED.

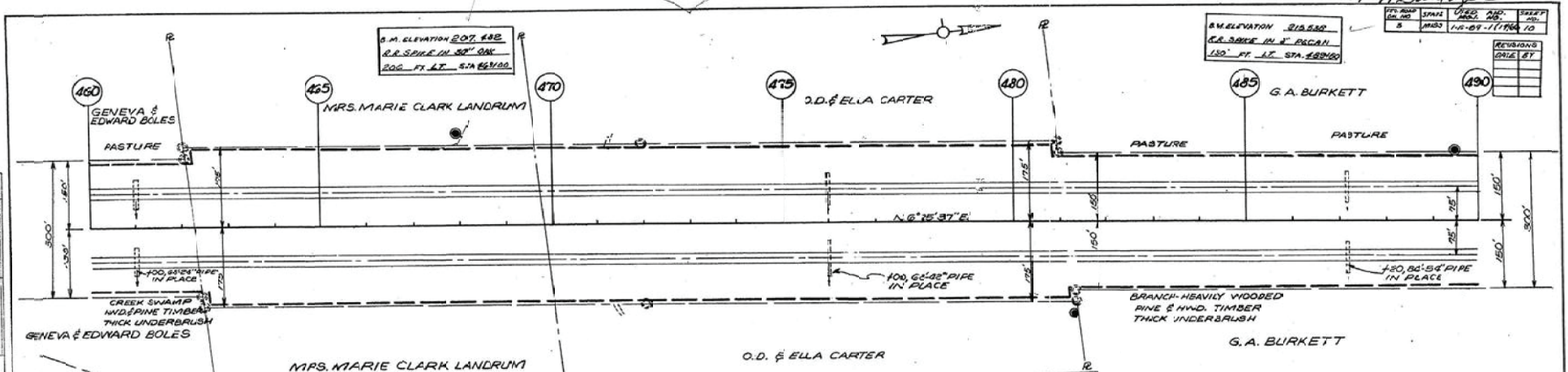
PROFILE  
NOTED: ALL DIMENSIONS ARE IN FEET UNLESS OTHERWISE NOTED.

119

9



*H. J. ...*



ST. NO.	DATE	BY	REVISIONS
1	10-07-1964	JD	1
2			
3			
4			
5			

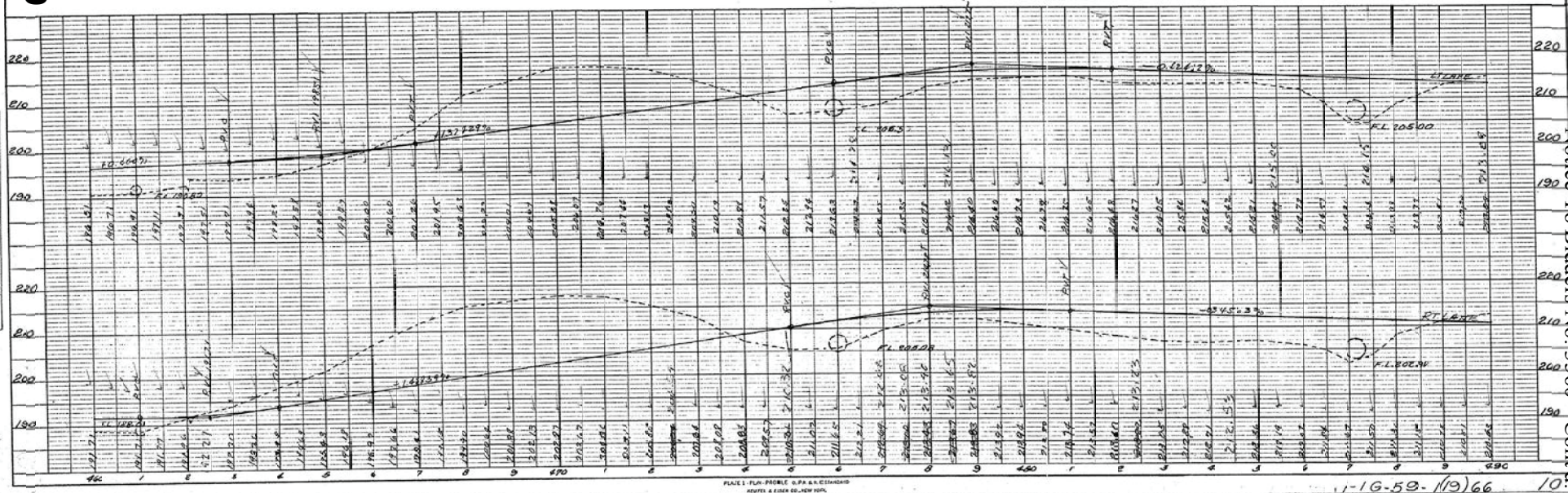
B.M. E. 187.865  
R.R. SINK IN 30" PIPE  
150' RT 462+00

B.M. E. 204.858  
R.R. SINK IN 30" PIPE  
150' RT 480+75

FINAL PLANS  
OF  
COMPLETED WORK

120

120



Notice To Bidders No. 3560--Cont'd.



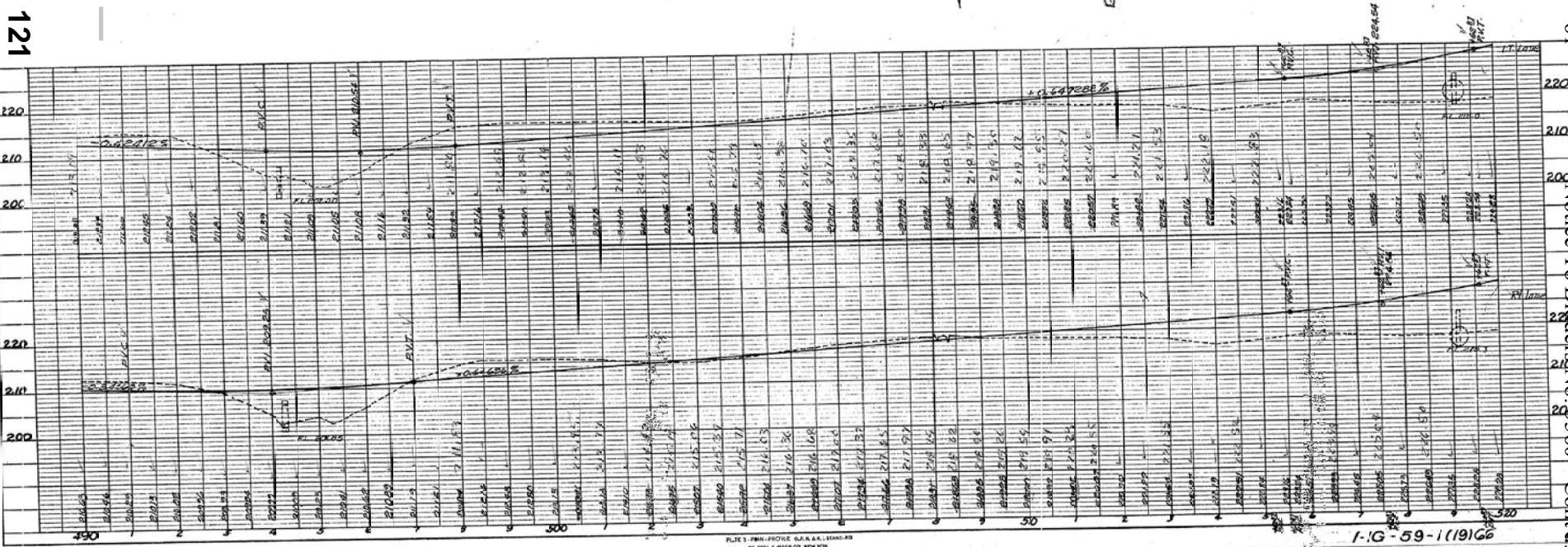
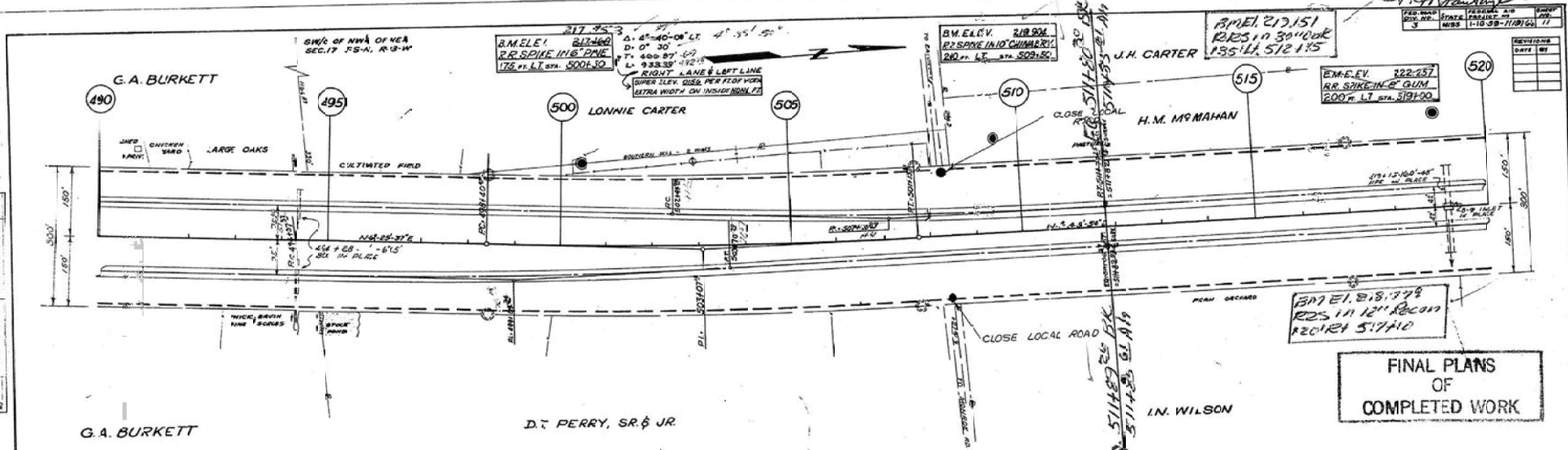
PLAN SHEET NO. 1  
 DATE: 1-10-59  
 DRAWN BY: J. H. CARTER  
 CHECKED BY:

E.M.E.V. 222-257  
 RR SPIKE-IN GUM  
 200 FT. LT. STA. 5181.90

B.M.E.L.V. 217.151  
 RES. IN 30" DIA. C  
 135' LT. STA. 5121.15

B.M.E.L.V. 212.160  
 RES. IN 18" PINE  
 175 FT. LT. STA. 5001.30

**FINAL PLANS  
 OF  
 COMPLETED WORK**



Notice To Bidders No. 3560-Com 7

1-G-59-1(19)66

PL. 1 - PLAN PROFILE G.A. & J. H. BURKETT  
 1/2" V. & 1" H. SCALE

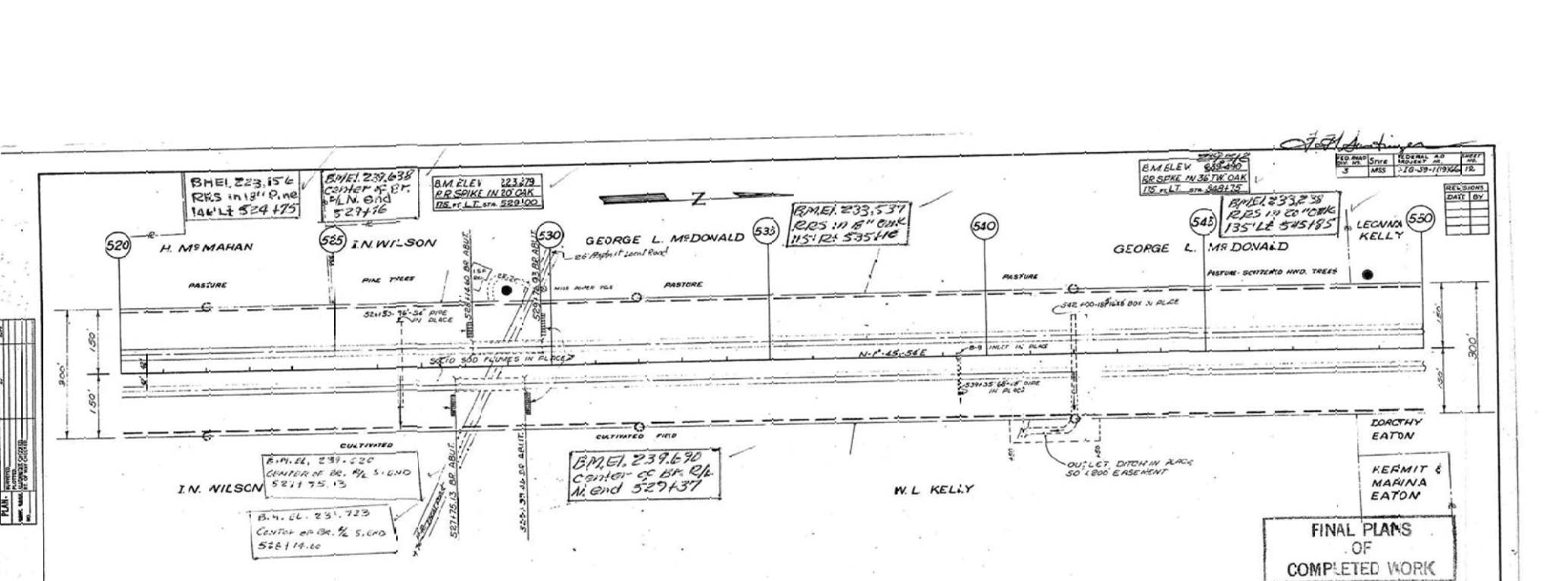
PLAN SHEET NO. 1  
 DATE: 1-10-59  
 DRAWN BY: J. H. CARTER  
 CHECKED BY:

PROFILE SHEET NO. 1  
 DATE: 1-10-59  
 DRAWN BY: J. H. CARTER  
 CHECKED BY:

121

6





DESIGN NO.	1965-59-119-122
DATE	12/15/57
SCALE	1"=40'
SHEET NO.	12
TOTAL SHEETS	13

DATE	
BY	
CHECKED	
APPROVED	
DRAWN	

DATE	
BY	
CHECKED	
APPROVED	
DRAWN	

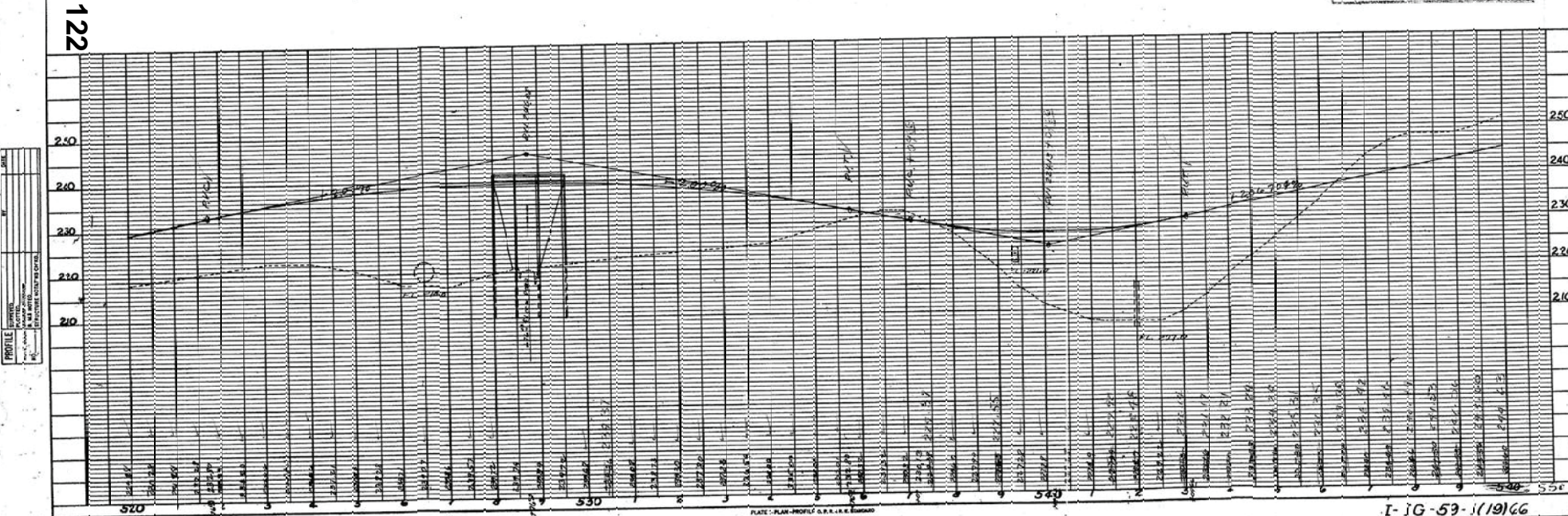


PLATE - PLAN-PROFILE G. B. & A. R. K. ENGINEERS  
 QUINCY, ILLINOIS

I-IG-59-1(19)66

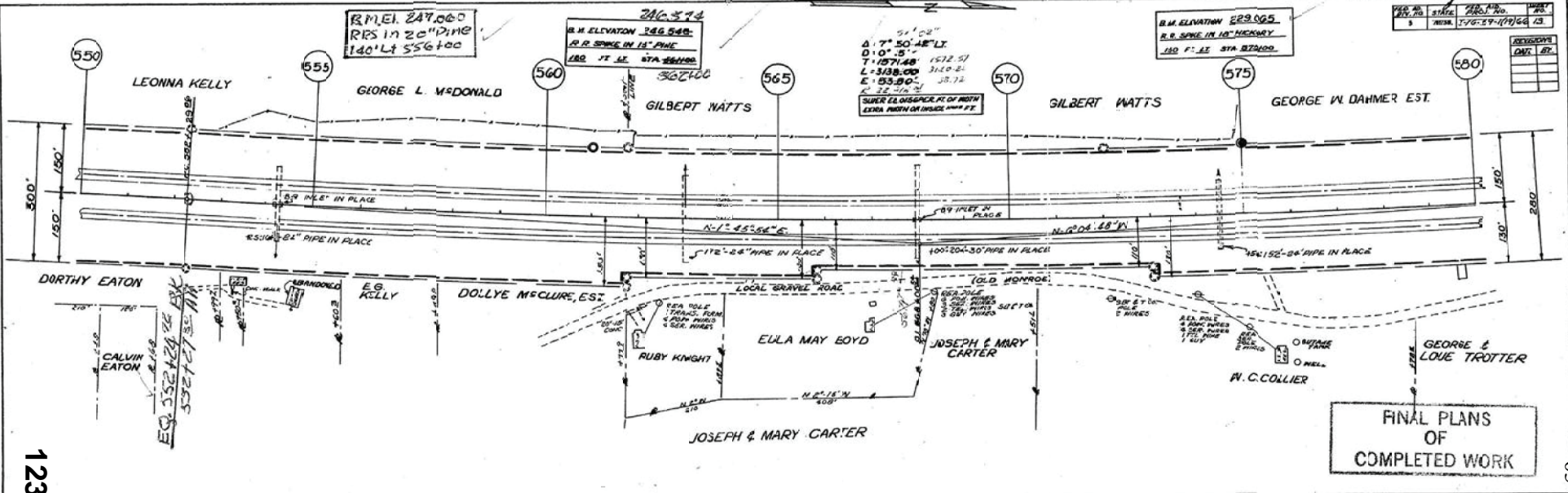
FINAL PLANS  
 OF  
 COMPLETED WORK

Notice To Bidders No. 5560 - CONT'D

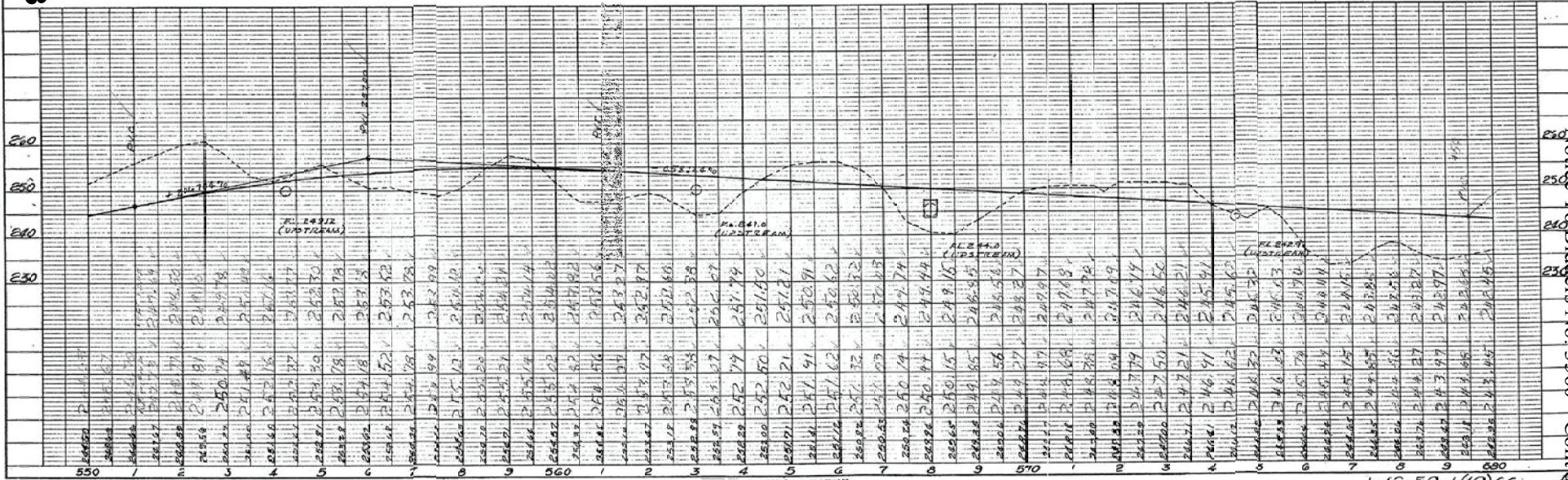


*J. H. Hentzen*

PROJ. NO.	STATE	PROJ. NO.	DATE
5	MD	176-27-11954	12
REVISIONS		DATE	BY



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Notice To Bidders No. 3560-1-Cont'd

1-16-59-1(19)66

PLANS - PLAN PROFILE 61.9 & 61.1 STANDARD  
HEWLETT & PACKARD CO. NEW YORK



REVISION	DATE	BY

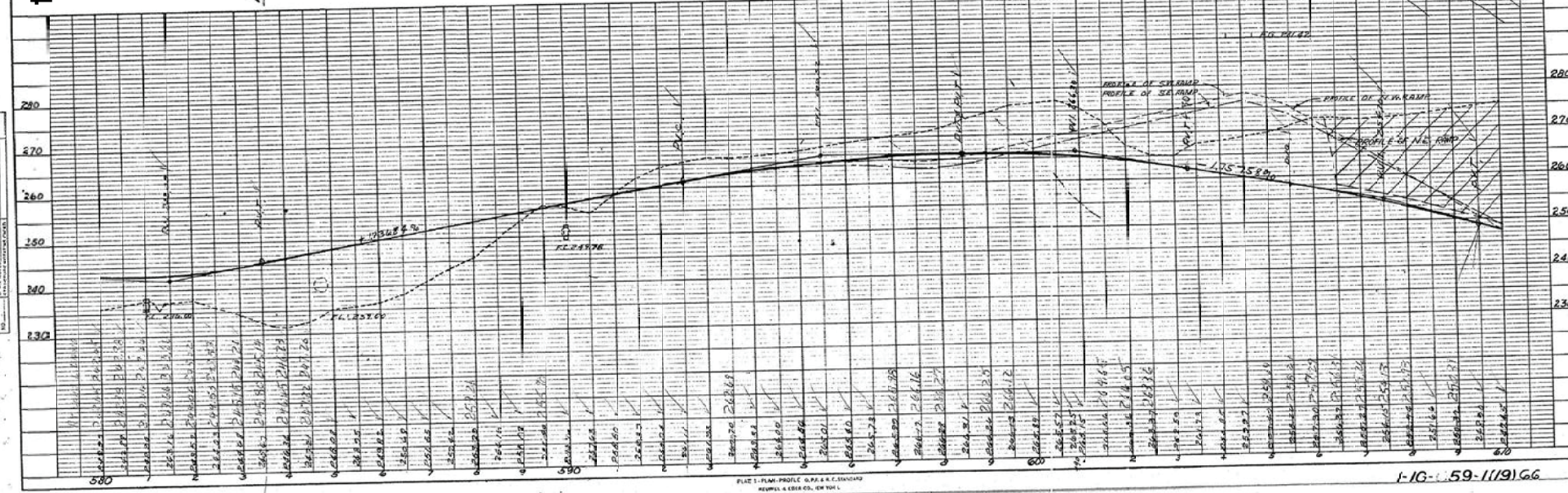
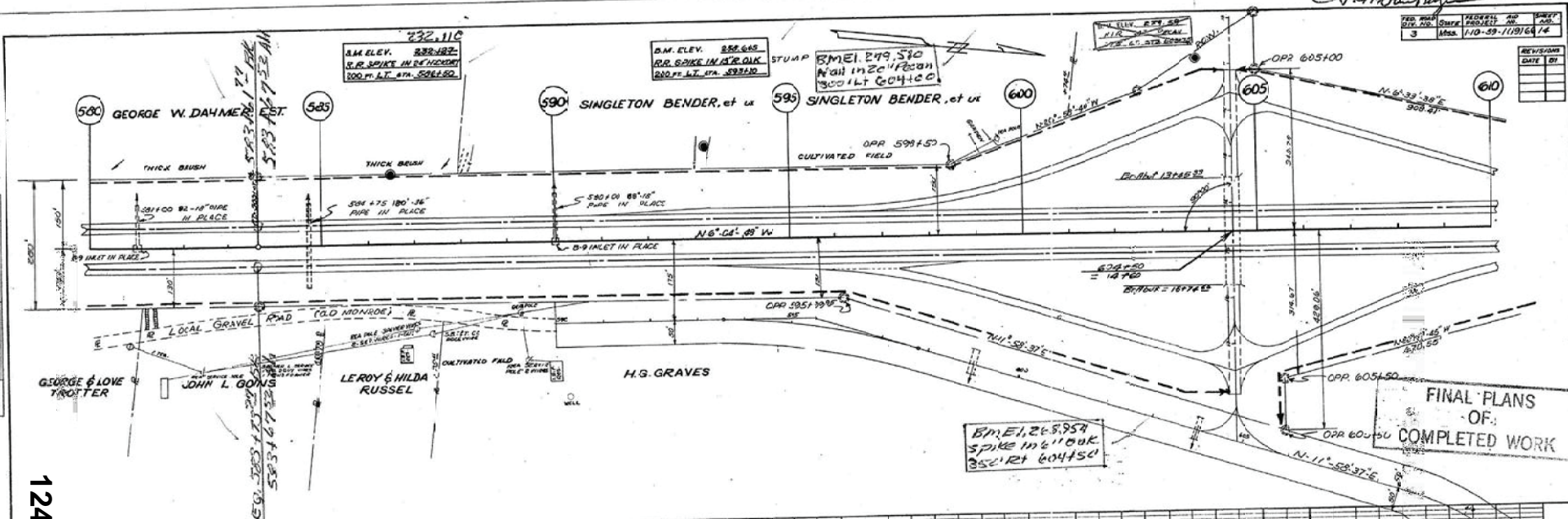


PLATE 1 - PLAN, PROFILE, O.P.R. & E.C. SHEET  
 NEWELL & CO. INC., CHICAGO

1-10-59-1119166

Notice To Bidders, No. 3560--Cont'd

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PLAN  
 DRAWN BY  
 CHECKED BY  
 DATE

PROFILE  
 DRAWN BY  
 CHECKED BY  
 DATE



*Handwritten signature*

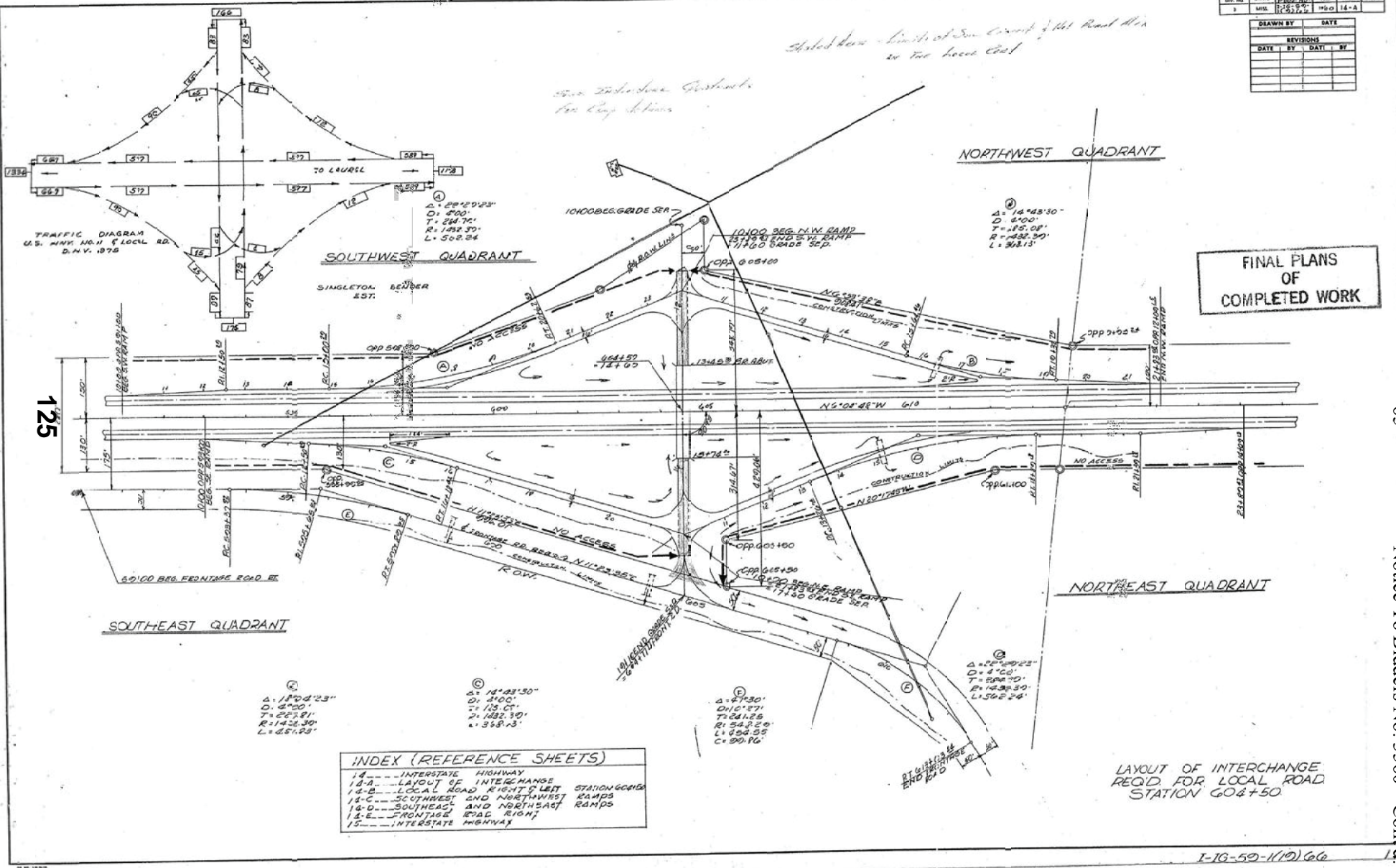
REV. NO.	STATE	PROJ. NO.	TITLE	SHEET NO.
3	NY	125	14-A	14-A

DRAWN BY		DATE	
REVISIONS			
DATE	BY	DATE	BY

*aligned here - limits of Sun County of Hill Road etc. in the lower part*

*see subdivision sketches for Reg. Station*



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TRAFFIC DIAGRAM  
U.S. HWY. NO. 11 E LOCAL RD.  
D.N.V. 1978

SOUTHWEST QUADRANT

NORTHWEST QUADRANT

FINAL PLANS  
OF  
COMPLETED WORK

NORTHEAST QUADRANT

SOUTHEAST QUADRANT

INDEX (REFERENCE SHEETS)

14	INTERSTATE HIGHWAY
14-A	LAYOUT OF INTERCHANGE
14-B	LOCAL ROAD RIGHT & LEFT STATIONING
14-C	SOUTHWEST AND NORTHWEST RAMP
14-D	SOUTHEAST AND NORTHEAST RAMP
14-E	FRONTAGE ROAD RIGHT
15	INTERSTATE HIGHWAY

LAYOUT OF INTERCHANGE  
REQ'D FOR LOCAL ROAD  
STATION 604+50

1-10-59-1(10)66

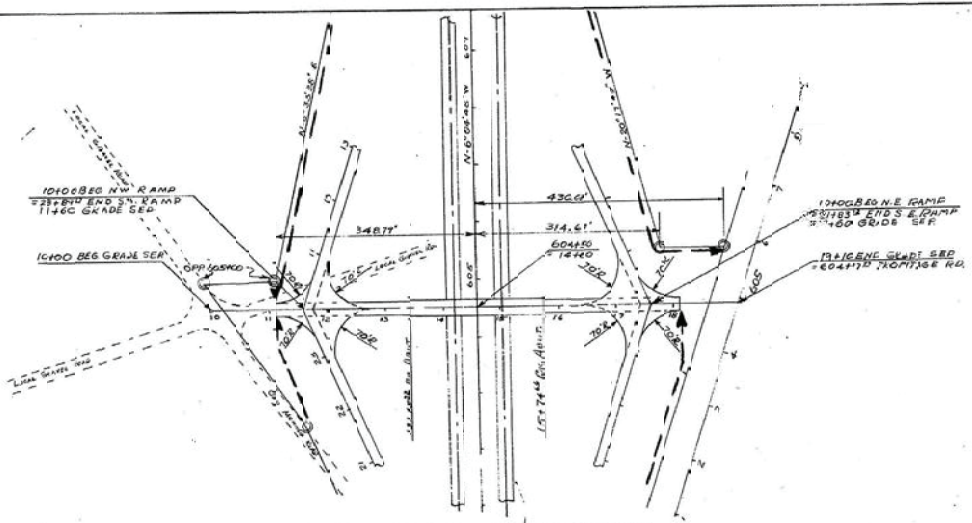
- 65 - Notice to Bidders No. 3560--Cont'd



*F. J. Santora*

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS  
 PROJECT NO. 11-10-59-1119-11-11

DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_



LOCAL ROAD RELOCATION  
 FOR  
 INTERCHANGE

FINAL PLANS  
 OF  
 COMPLETED WORK

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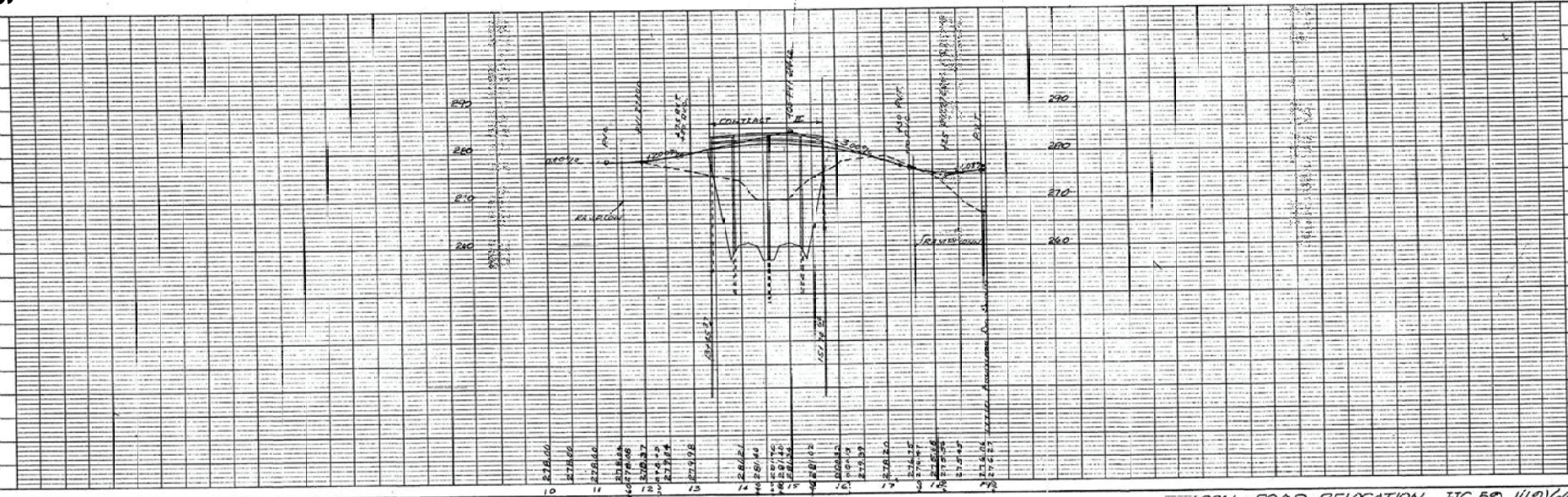


PLATE 3 - L&N PROFILE A.P. & I.E. STANDARD  
 HUFFEL & EGGER CO., NEW ORL.

LOCAL ROAD RELOCATION 11G-59-1119-11-11

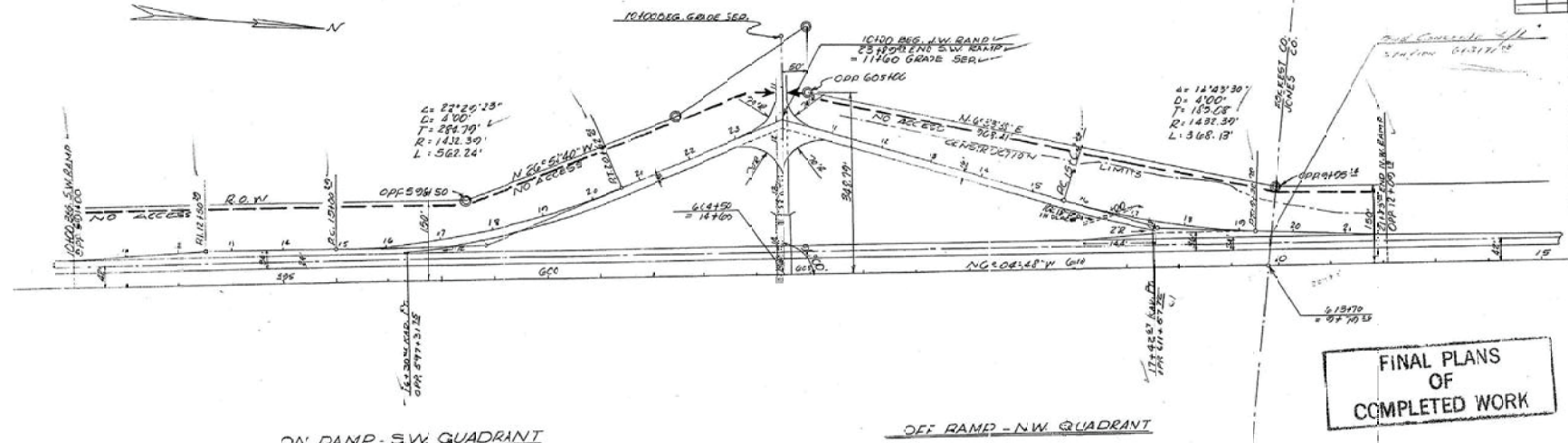
Notice To Bidders No. 3560-1 Cont'd.

-66-

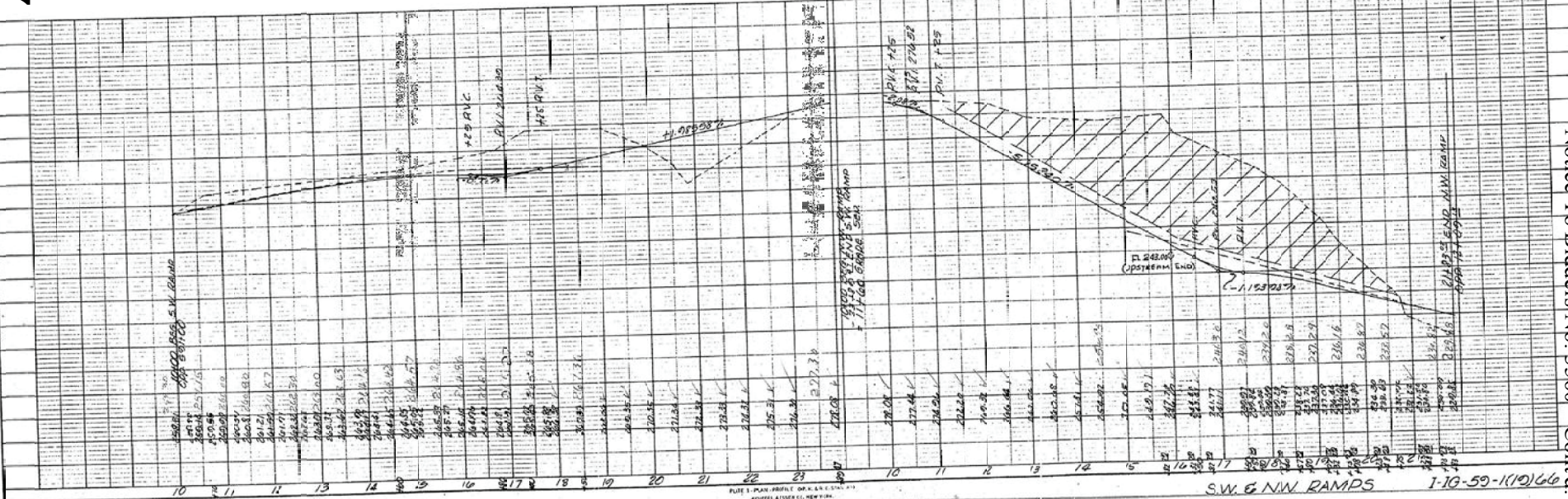


*Handwritten signature*

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127



Notice To Bidders No. 3560--Cont'd.

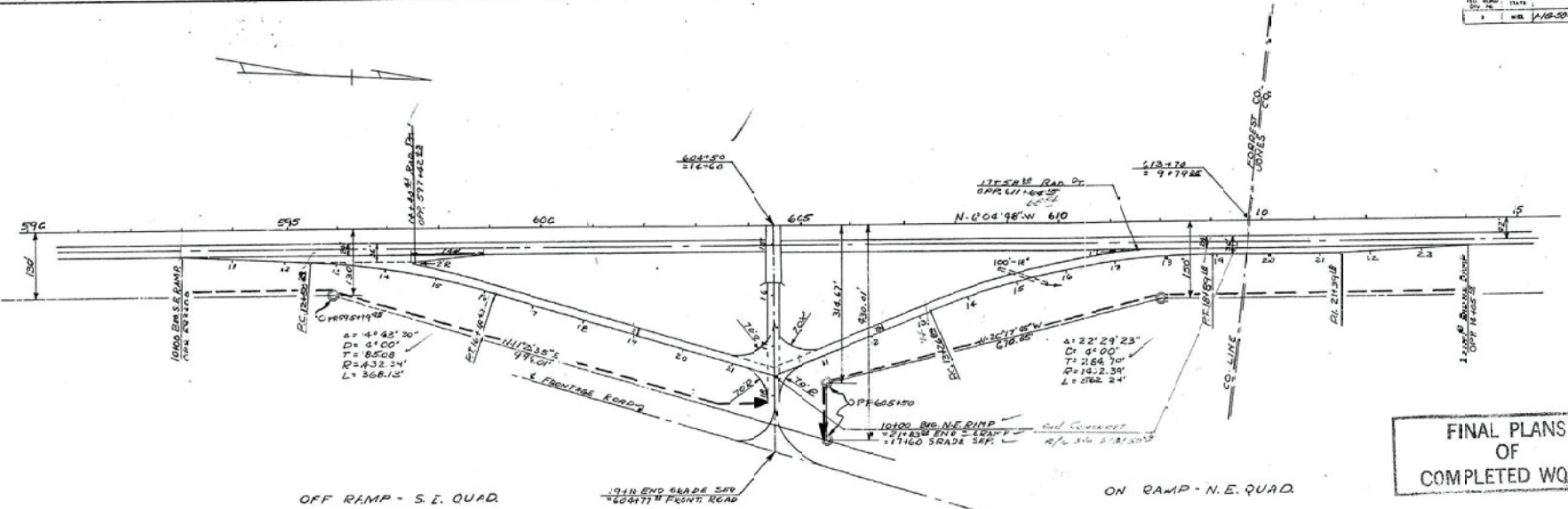
- 67 -



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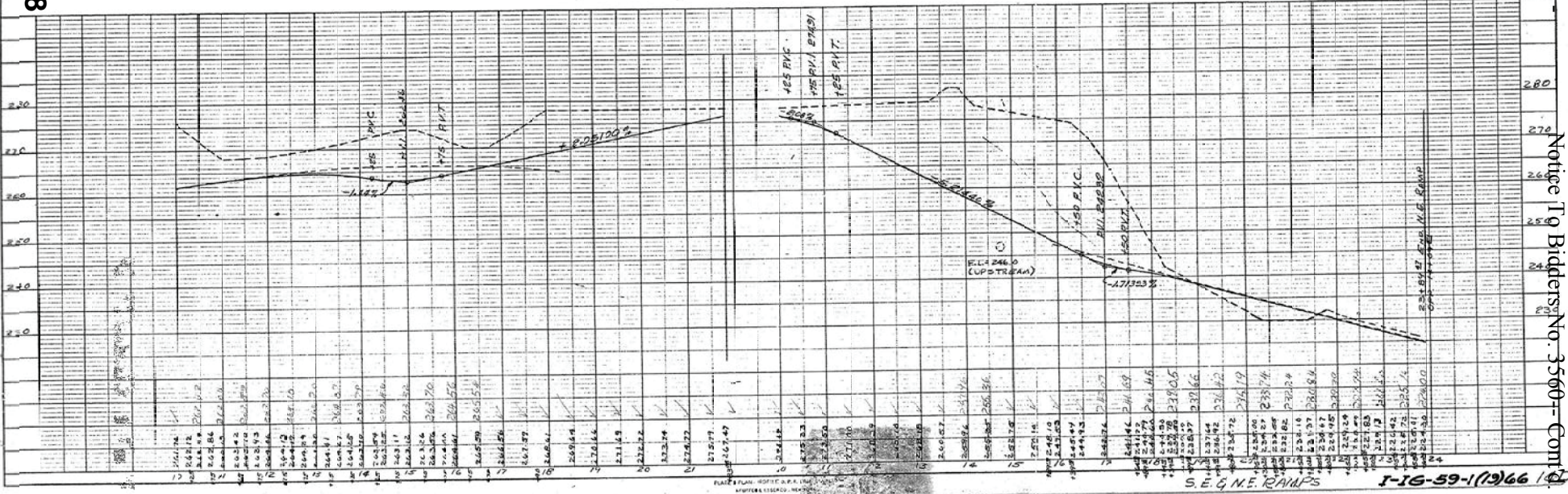
NO. ROAD STATE SHEET  
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DATE: 11-27-66



FINAL PLANS  
 OF  
 COMPLETED WORK

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Notice To Bidders No. 3560-Com

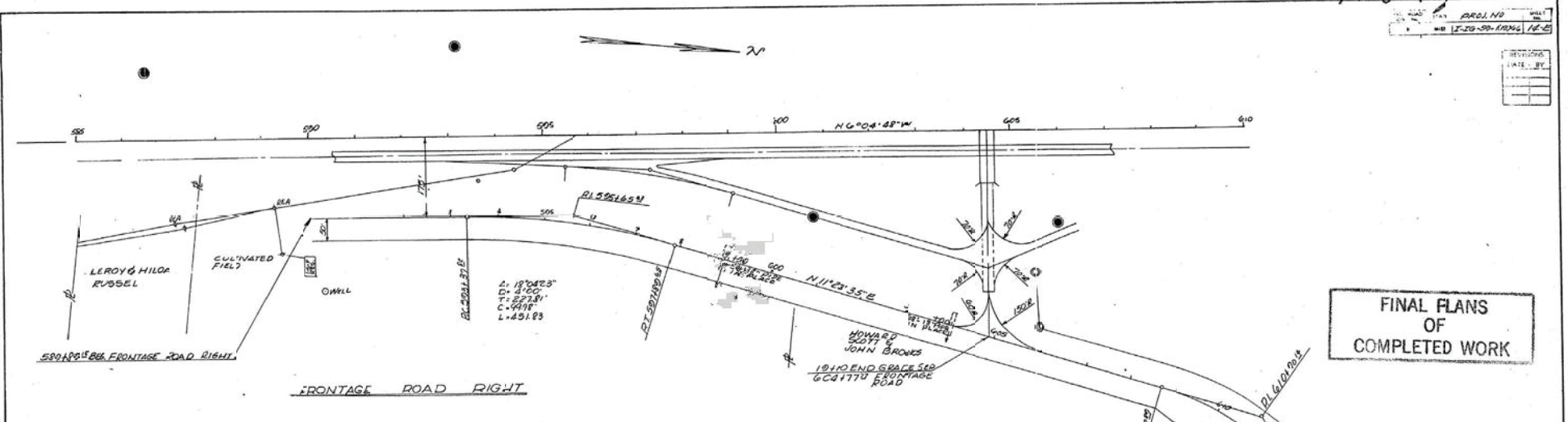
S.E. & N.E. RAMPS 1-16-59-1(12)66 12



*J. J. [Signature]*

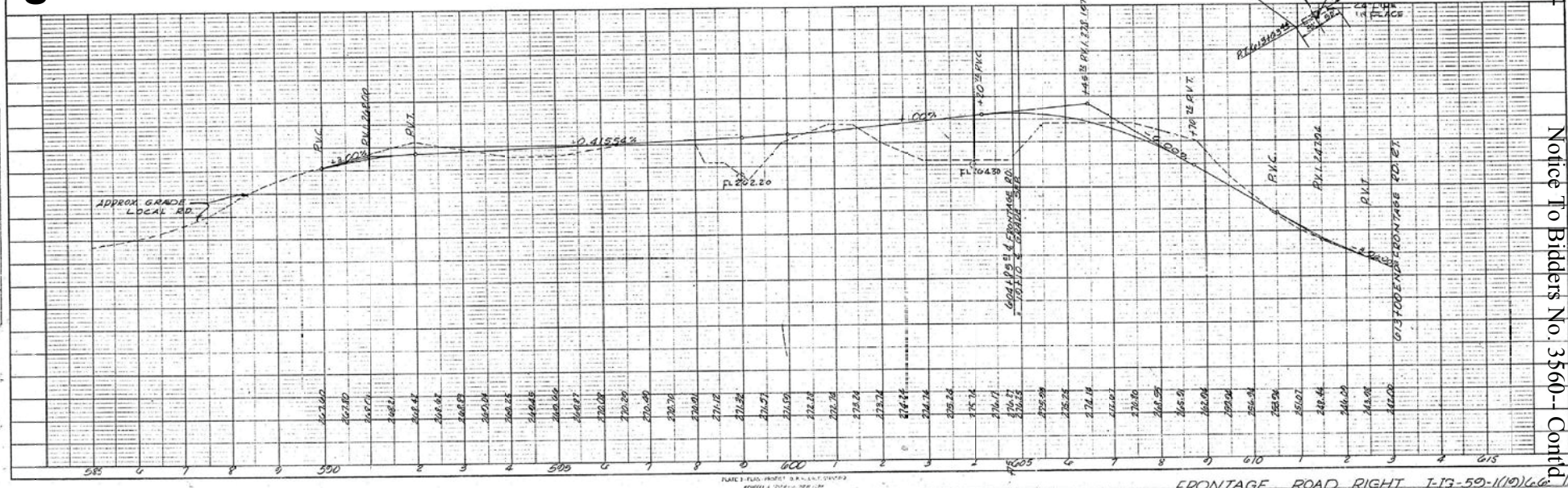
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**FINAL PLANS  
OF  
COMPLETED WORK**

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Notice To Bidders No. 3560-1 Cont'd

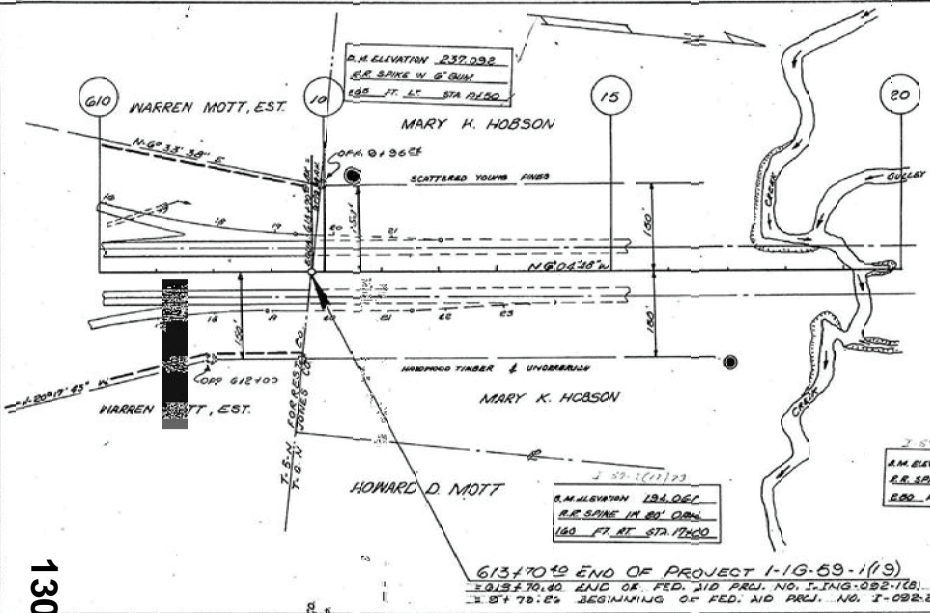
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*J. H. Dunbar*

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FINAL PLANS  
 OF  
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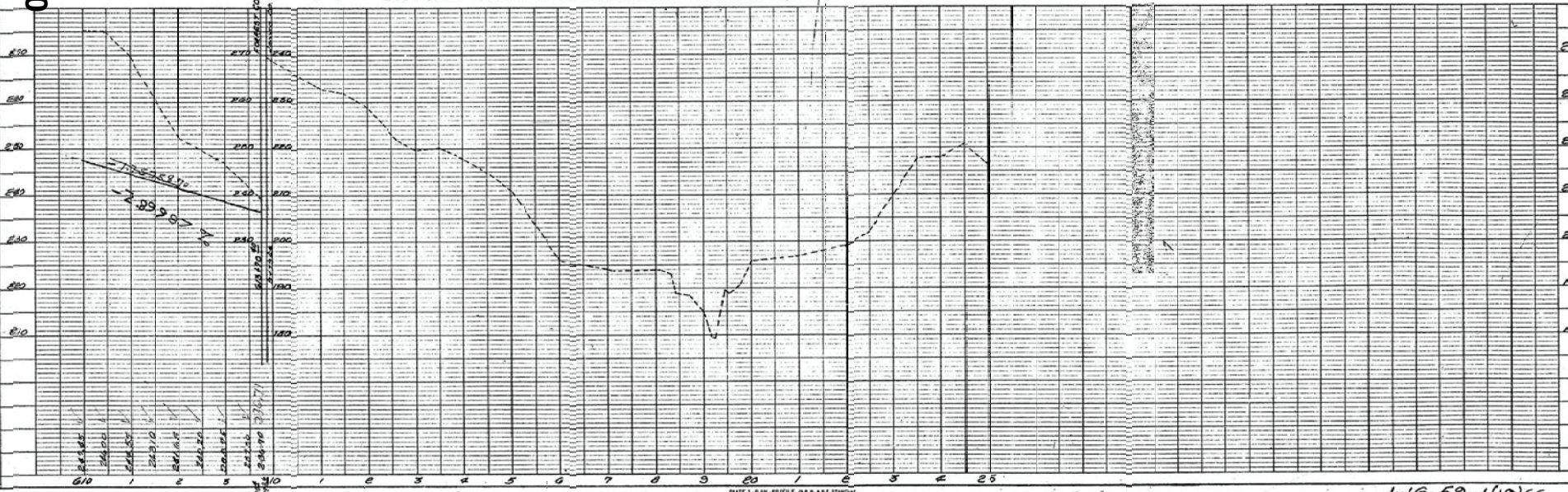


PLATE 1: PLAN PROFILE O.P.A. & R.E. STANDARD  
 HARTFORD & CO. INC. NEW YORK

1-10-69-1(19)66

Notice to Bidders No. 3560-1-Cont. 15

General Decision Number: MS100212 12/03/2010 MS212

Superseded General Decision Number: MS20080212

State: Mississippi

Construction Type: Highway

County: Forrest County in Mississippi.

HIGHWAY CONSTRUCTION PROJECTS (excluding tunnels, building structures in rest area projects & railroad construction; bascule, suspension & spandrel arch bridges designed for commercial navigation, bridges involving marine construction; and other major bridges).

Modification Number	Publication Date
0	03/12/2010
1	06/04/2010
2	12/03/2010

\* ELEC0903-003 12/01/2010

	Rates	Fringes
ELECTRICIAN.....	\$ 24.00	6.88

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SUMS2008-130 09/04/2008

	Rates	Fringes
CARPENTER.....	\$ 13.00	0.00
CEMENT MASON/CONCRETE FINISHER...	\$ 11.54	1.63
LABORER: Common or General.....	\$ 8.34	0.00
LABORER: Pipelayer.....	\$ 10.17	0.00
OPERATOR: Backhoe.....	\$ 12.57	0.00
OPERATOR: Broom.....	\$ 8.00	0.00
OPERATOR: Bulldozer.....	\$ 11.63	0.00
OPERATOR: Grader/Blade.....	\$ 11.10	0.00
OPERATOR: Mechanic.....	\$ 13.00	0.00
OPERATOR: Piledriver.....	\$ 12.50	1.23
OPERATOR: Roller.....	\$ 9.31	0.00
OPERATOR: Scraper.....	\$ 10.00	0.00
TRUCK DRIVER.....	\$ 10.34	0.00

-----  
WELDERS - Receive rate prescribed for craft performing



operation to which welding is incidental.

=====  
Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29 CFR 5.5(a)(1)(ii)).

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--  
In the listing above, the "SU" designation means that rates listed under the identifier do not reflect collectively bargained wage and fringe benefit rates. Other designations indicate unions whose rates have been determined to be prevailing.

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--  
WAGE DETERMINATION APPEALS PROCESS

1.) Has there been an initial decision in he matter? This can be:

- \* an existing published wage determination
- \* a survey underlying a wage determination
- \* a Wage and Hour Division letter setting forth a position on a wage determination matter
- \* a conformance (additional classification and rate) ruling

On survey related matters, initial contact, including requests for summaries of surveys, should be with the Wage and Hour Regional Office for the area in which the survey was conducted because those Regional Offices have responsibility for the Davis-Bacon survey program. If the response from this initial contact is not satisfactory, then the process described in 2.) and 3.) should be followed.

With regard to any other matter not yet ripe for the formal process described here, initial contact should be with the Branch of Construction Wage Determinations. Write to:

Branch of Construction Wage Determinations  
Wage and Hour Division  
U.S. Department of Labor  
200 Constitution Avenue, N.W.  
Washington, DC 20210

2.) If the answer to the question in 1.) is yes, then an interested party (those affected by the action) can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7).

Write to:

Wage and Hour Administrator  
U.S. Department of Labor  
200 Constitution Avenue, N.W.  
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and by any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3.) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board  
U.S. Department of Labor  
200 Constitution Avenue, N.W.  
Washington, DC 20210

4.) All decisions by the Administrative Review Board are final.

END OF GENERAL DECISION

**SUPPLEMENT TO FORM FHWA-1273**

**DATE:** 6/15/94

**SUBJECT: Final Certificate and Contract Provisions for Subcontracts**

All subcontracts shall be in writing and contain all pertinent provisions and requirements of the prime contract.

Each "Request for Permission to Subcontract" (Mississippi Department of Transportation Form CAD-720) shall include a copy of subcontract for review by the Mississippi Department of Transportation. The federal contract provisions may be omitted from the subcontract copy submitted for review provided the Contractor certifies that the provisions will be physically incorporated into the agreement furnished to the Subcontractor.

In lieu of submitting a copy of the subcontract for review, the Contractor may certify that the subcontract agreement is in writing and that it contains all the requirements and pertinent provisions of the prime contract.

Each Subcontractor will be required to provide a copy of the subcontract agreement for contract compliance reviews, along with physical evidence (copy of FHWA-1273) that requirements and pertinent provisions have been provided for review and adherence.



**REQUIRED CONTRACT PROVISIONS  
FEDERAL-AID CONSTRUCTION CONTRACTS**

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ATTACHMENTS

- A. Employment Preference for Appalachian Contracts  
(included in Appalachian contracts only)

**I. GENERAL**

1. These contract provisions shall apply to all work performed on the contract by the contractor's own organization and with the assistance of workers under the contractor's immediate superintendence and to all work performed on the contract by piecework, station work, or by subcontract.

2. Except as otherwise provided for in each section, the contractor shall insert in each subcontract all of the stipulations contained in these Required Contract Provisions, and further require their inclusion in any lower tier subcontract or purchase order that may in turn be made. The Required Contract Provisions shall not be incorporated by reference in any case. The prime contractor shall be responsible for compliance by any subcontractor or lower tier subcontractor with these Required Contract Provisions.

3. A breach of any of the stipulations contained in these Required Contract Provisions shall be sufficient grounds for termination of the contract.

4. A breach of the following clauses of the Required Contract Provisions may also be grounds for debarment as provided in 29 CFR 5.12:

- Section I, paragraph 2;
- Section IV, paragraphs 1, 2, 3, 4, and 7;
- Section V, paragraphs 1 and 2a through 2g.

5. Disputes arising out of the labor standards provisions of Section IV (except paragraph 5) and Section V of these Required Contract Provisions shall not be subject to the general disputes clause of this contract. Such disputes shall be resolved in accordance with the procedures of the U.S. Department of Labor (DOL) as set forth in 29 CFR 5, 6, and 7. Disputes within the meaning of this clause include disputes between the contractor (or any of its subcontractors) and the contracting agency, the DOL, or the contractor's employees or their representatives.

6. **Selection of Labor:** During the performance of this contract, the contractor shall not:

a. discriminate against labor from any other State, possession, or territory of the United States (except for employment preference for Appalachian contracts, when applicable, as specified in Attachment A), or

b. employ convict labor for any purpose within the limits of the project unless it is labor performed by convicts who are on parole, supervised release, or probation.

**II. NONDISCRIMINATION**

(Applicable to all Federal-aid construction contracts and to all related subcontracts of \$10,000 or more.)

1. **Equal Employment Opportunity:** Equal employment opportunity (EEO) requirements not to discriminate and to take affirmative action to assure equal opportunity as set forth under laws, executive orders, rules, regulations (28 CFR 35, 29 CFR 1630 and 41 CFR 60) and orders of the Secretary of Labor as modified by the provisions prescribed herein, and imposed pursuant to 23 U.S.C. 140 shall constitute the EEO and specific affirmative action standards for the contractor's project activities under this contract. The Equal Opportunity Construction Contract Specifications set forth under 41 CFR 60-4.3 and the provisions of the American Disabilities Act of 1990 (42 U.S.C. 12101 *et seq.*) set forth under 28 CFR 35 and 29 CFR 1630 are incorporated by reference in this contract. In the execution of this contract, the contractor agrees to comply with the following minimum specific requirement activities of EEO:

a. The contractor will work with the State highway agency (SHA) and the Federal Government in carrying out EEO obligations and in their review of his/her activities under the contract.

b. The contractor will accept as his operating policy the following statement:

"It is the policy of this Company to assure that applicants are employed, and that employees are treated during employment, without regard to their race, religion, sex, color, national origin, age or disability. Such action shall include: employment, upgrading, demotion, or transfer; recruitment or recruitment advertising; layoff or termination; rates of pay or other forms of compensation; and selection for training, including apprenticeship, preapprenticeship, and/or on-the-job training."

2. **EEO Officer:** The contractor will designate and make known to the SHA contracting officers an EEO Officer who will have the responsibility for and must be capable of effectively administering and promoting an active contractor program of EEO and who must be assigned adequate authority and responsibility to do so.

3. **Dissemination of Policy:** All members of the contractor's staff who are authorized to hire, supervise, promote, and discharge employees, or who recommend such action, or who are substantially involved in such action, will be made fully cognizant

of, and will implement, the contractor's EEO policy and contractual responsibilities to provide EEO in each grade and classification of employment. To ensure that the above agreement will be met, the following actions will be taken as a minimum:

a. Periodic meetings of supervisory and personnel office employees will be conducted before the start of work and then not less often than once every six months, at which time the contractor's EEO policy and its implementation will be reviewed and explained. The meetings will be conducted by the EEO Officer.

b. All new supervisory or personnel office employees will be given a thorough indoctrination by the EEO Officer, covering all major aspects of the contractor's EEO obligations within thirty days following their reporting for duty with the contractor.

c. All personnel who are engaged in direct recruitment for the project will be instructed by the EEO Officer in the contractor's procedures for locating and hiring minority group employees.

d. Notices and posters setting forth the contractor's EEO policy will be placed in areas readily accessible to employees, applicants for employment and potential employees.

e. The contractor's EEO policy and the procedures to implement such policy will be brought to the attention of employees by means of meetings, employee handbooks, or other appropriate means.

**4. Recruitment:** When advertising for employees, the contractor will include in all advertisements for employees the notation: "An Equal Opportunity Employer." All such advertisements will be placed in publications having a large circulation among minority groups in the area from which the project work force would normally be derived.

a. The contractor will, unless precluded by a valid bargaining agreement, conduct systematic and direct recruitment through public and private employee referral sources likely to yield qualified minority group applicants. To meet this requirement, the contractor will identify sources of potential minority group employees, and establish with such identified sources procedures whereby minority group applicants may be referred to the contractor for employment consideration.

b. In the event the contractor has a valid bargaining agreement providing for exclusive hiring hall referrals, he is expected to observe the provisions of that agreement to the extent that the system permits the contractor's compliance with EEO contract provisions. (The DOL has held that where implementation of such agreements have the effect of discriminating against minorities or women, or obligates the contractor to do the same, such implementation violates Executive Order 11246, as amended.)

c. The contractor will encourage his present employees to refer minority group applicants for employment. Information and procedures with regard to referring minority group applicants will be discussed with employees.

**5. Personnel Actions:** Wages, working conditions, and employee benefits shall be established and administered, and personnel actions of every type, including hiring, upgrading, promotion, transfer, demotion, layoff, and termination, shall be

taken without regard to race, color, religion, sex, national origin, age or disability. The following procedures shall be followed:

a. The contractor will conduct periodic inspections of project sites to insure that working conditions and employee facilities do not indicate discriminatory treatment of project site personnel.

b. The contractor will periodically evaluate the spread of wages paid within each classification to determine any evidence of discriminatory wage practices.

c. The contractor will periodically review selected personnel actions in depth to determine whether there is evidence of discrimination. Where evidence is found, the contractor will promptly take corrective action. If the review indicates that the discrimination may extend beyond the actions reviewed, such corrective action shall include all affected persons.

d. The contractor will promptly investigate all complaints of alleged discrimination made to the contractor in connection with his obligations under this contract, will attempt to resolve such complaints, and will take appropriate corrective action within a reasonable time. If the investigation indicates that the discrimination may affect persons other than the complainant, such corrective action shall include such other persons. Upon completion of each investigation, the contractor will inform every complainant of all of his avenues of appeal.

#### **6. Training and Promotion:**

a. The contractor will assist in locating, qualifying, and increasing the skills of minority group and women employees, and applicants for employment.

b. Consistent with the contractor's work force requirements and as permissible under Federal and State regulations, the contractor shall make full use of training programs, i.e., apprenticeship, and on-the-job training programs for the geographical area of contract performance. Where feasible, 25 percent of apprentices or trainees in each occupation shall be in their first year of apprenticeship or training. In the event a special provision for training is provided under this contract, this subparagraph will be superseded as indicated in the special provision.

c. The contractor will advise employees and applicants for employment of available training programs and entrance requirements for each.

d. The contractor will periodically review the training and promotion potential of minority group and women employees and will encourage eligible employees to apply for such training and promotion.

**7. Unions:** If the contractor relies in whole or in part upon unions as a source of employees, the contractor will use his/her best efforts to obtain the cooperation of such unions to increase opportunities for minority groups and women within the unions, and to effect referrals by such unions of minority and female employees. Actions by the contractor either directly or through a contractor's association acting as agent will include the procedures set forth below:

a. The contractor will use best efforts to develop, in cooperation with the unions, joint training programs aimed toward

qualifying more minority group members and women for membership in the unions and increasing the skills of minority group employees and women so that they may qualify for higher paying employment.

b. The contractor will use best efforts to incorporate an EEO clause into each union agreement to the end that such union will be contractually bound to refer applicants without regard to their race, color, religion, sex, national origin, age or disability.

c. The contractor is to obtain information as to the referral practices and policies of the labor union except that to the extent such information is within the exclusive possession of the labor union and such labor union refuses to furnish such information to the contractor, the contractor shall so certify to the SHA and shall set forth what efforts have been made to obtain such information.

d. In the event the union is unable to provide the contractor with a reasonable flow of minority and women referrals within the time limit set forth in the collective bargaining agreement, the contractor will, through independent recruitment efforts, fill the employment vacancies without regard to race, color, religion, sex, national origin, age or disability; making full efforts to obtain qualified and/or qualifiable minority group persons and women. (The DOL has held that it shall be no excuse that the union with which the contractor has a collective bargaining agreement providing for exclusive referral failed to refer minority employees.) In the event the union referral practice prevents the contractor from meeting the obligations pursuant to Executive Order 11246, as amended, and these special provisions, such contractor shall immediately notify the SHA.

**8. Selection of Subcontractors, Procurement of Materials and Leasing of Equipment:** The contractor shall not discriminate on the grounds of race, color, religion, sex, national origin, age or disability in the selection and retention of subcontractors, including procurement of materials and leases of equipment.

a. The contractor shall notify all potential subcontractors and suppliers of his/her EEO obligations under this contract.

b. Disadvantaged business enterprises (DBE), as defined in 49 CFR 23, shall have equal opportunity to compete for and perform subcontracts which the contractor enters into pursuant to this contract. The contractor will use his best efforts to solicit bids from and to utilize DBE subcontractors or subcontractors with meaningful minority group and female representation among their employees. Contractors shall obtain lists of DBE construction firms from SHA personnel.

c. The contractor will use his best efforts to ensure subcontractor compliance with their EEO obligations.

**9. Records and Reports:** The contractor shall keep such records as necessary to document compliance with the EEO requirements. Such records shall be retained for a period of three years following completion of the contract work and shall be available at reasonable times and places for inspection by authorized representatives of the SHA and the FHWA.

a. The records kept by the contractor shall document the following:

(1) The number of minority and non-minority group members and women employed in each work classification on the project;

(2) The progress and efforts being made in cooperation with unions, when applicable, to increase employment opportunities for minorities and women;

(3) The progress and efforts being made in locating, hiring, training, qualifying, and upgrading minority and female employees; and

(4) The progress and efforts being made in securing the services of DBE subcontractors or subcontractors with meaningful minority and female representation among their employees.

b. The contractors will submit an annual report to the SHA each July for the duration of the project, indicating the number of minority, women, and non-minority group employees currently engaged in each work classification required by the contract work. This information is to be reported on Form FHWA-1391. If on-the-job training is being required by special provision, the contractor will be required to collect and report training data.

### III. NONSEGREGATED FACILITIES

(Applicable to all Federal-aid construction contracts and to all related subcontracts of \$10,000 or more.)

a. By submission of this bid, the execution of this contract or subcontract, or the consummation of this material supply agreement or purchase order, as appropriate, the bidder, Federal-aid construction contractor, subcontractor, material supplier, or vendor, as appropriate, certifies that the firm does not maintain or provide for its employees any segregated facilities at any of its establishments, and that the firm does not permit its employees to perform their services at any location, under its control, where segregated facilities are maintained. The firm agrees that a breach of this certification is a violation of the EEO provisions of this contract. The firm further certifies that no employee will be denied access to adequate facilities on the basis of sex or disability.

b. As used in this certification, the term "segregated facilities" means any waiting rooms, work areas, restrooms and washrooms, restaurants and other eating areas, timeclocks, locker rooms, and other storage or dressing areas, parking lots, drinking fountains, recreation or entertainment areas, transportation, and housing facilities provided for employees which are segregated by explicit directive, or are, in fact, segregated on the basis of race, color, religion, national origin, age or disability, because of habit, local custom, or otherwise. The only exception will be for the disabled when the demands for accessibility override (e.g. disabled parking).

c. The contractor agrees that it has obtained or will obtain identical certification from proposed subcontractors or material suppliers prior to award of subcontracts or consummation of material supply agreements of \$10,000 or more and that it will retain such certifications in its files.

### IV. PAYMENT OF PREDETERMINED MINIMUM WAGE



(Applicable to all Federal-aid construction contracts exceeding \$2,000 and to all related subcontracts, except for projects located on roadways classified as local roads or rural minor collectors, which are exempt.)

**1. General:**

a. All mechanics and laborers employed or working upon the site of the work will be paid unconditionally and not less often than once a week and without subsequent deduction or rebate on any account [except such payroll deductions as are permitted by regulations (29 CFR 3) issued by the Secretary of Labor under the Copeland Act (40 U.S.C. 276c)] the full amounts of wages and bona fide fringe benefits (or cash equivalents thereof) due at time of payment. The payment shall be computed at wage rates not less than those contained in the wage determination of the Secretary of Labor (hereinafter "the wage determination") which is attached hereto and made a part hereof, regardless of any contractual relationship which may be alleged to exist between the contractor or its subcontractors and such laborers and mechanics. The wage determination (including any additional classifications and wage rates conformed under paragraph 2 of this Section IV and the DOL poster (WH-1321) or Form FHWA-1495) shall be posted at all times by the contractor and its subcontractors at the site of the work in a prominent and accessible place where it can be easily seen by the workers. For the purpose of this Section, contributions made or costs reasonably anticipated for bona fide fringe benefits under Section 1(b)(2) of the Davis-Bacon Act (40 U.S.C. 276a) on behalf of laborers or mechanics are considered wages paid to such laborers or mechanics, subject to the provisions of Section IV, paragraph 3b, hereof. Also, for the purpose of this Section, regular contributions made or costs incurred for more than a weekly period (but not less often than quarterly) under plans, funds, or programs, which cover the particular weekly period, are deemed to be constructively made or incurred during such weekly period. Such laborers and mechanics shall be paid the appropriate wage rate and fringe benefits on the wage determination for the classification of work actually performed, without regard to skill, except as provided in paragraphs 4 and 5 of this Section IV.

b. Laborers or mechanics performing work in more than one classification may be compensated at the rate specified for each classification for the time actually worked therein, provided, that the employer's payroll records accurately set forth the time spent in each classification in which work is performed.

c. All rulings and interpretations of the Davis-Bacon Act and related acts contained in 29 CFR 1, 3, and 5 are herein incorporated by reference in this contract.

**2. Classification:**

a. The SHA contracting officer shall require that any class of laborers or mechanics employed under the contract, which is not listed in the wage determination, shall be classified in conformance with the wage determination.

b. The contracting officer shall approve an additional classification, wage rate and fringe benefits only when the following criteria have been met:

(1) the work to be performed by the additional classification requested is not performed by a classification in the wage determination;

(2) the additional classification is utilized in the area by the construction industry;

(3) the proposed wage rate, including any bona fide fringe benefits, bears a reasonable relationship to the wage rates contained in the wage determination; and

(4) with respect to helpers, when such a classification prevails in the area in which the work is performed.

c. If the contractor or subcontractors, as appropriate, the laborers and mechanics (if known) to be employed in the additional classification or their representatives, and the contracting officer agree on the classification and wage rate (including the amount designated for fringe benefits where appropriate), a report of the action taken shall be sent by the contracting officer to the DOL, Administrator of the Wage and Hour Division, Employment Standards Administration, Washington, D.C. 20210. The Wage and Hour Administrator, or an authorized representative, will approve, modify, or disapprove every additional classification action within 30 days of receipt and so advise the contracting officer or will notify the contracting officer within the 30-day period that additional time is necessary.

d. In the event the contractor or subcontractors, as appropriate, the laborers or mechanics to be employed in the additional classification or their representatives, and the contracting officer do not agree on the proposed classification and wage rate (including the amount designated for fringe benefits, where appropriate), the contracting officer shall refer the questions, including the views of all interested parties and the recommendation of the contracting officer, to the Wage and Hour Administrator for determination. Said Administrator, or an authorized representative, will issue a determination within 30 days of receipt and so advise the contracting officer or will notify the contracting officer within the 30-day period that additional time is necessary.

e. The wage rate (including fringe benefits where appropriate) determined pursuant to paragraph 2c or 2d of this Section IV shall be paid to all workers performing work in the additional classification from the first day on which work is performed in the classification.

**3. Payment of Fringe Benefits:**

a. Whenever the minimum wage rate prescribed in the contract for a class of laborers or mechanics includes a fringe benefit which is not expressed as an hourly rate, the contractor or subcontractors, as appropriate, shall either pay the benefit as stated in the wage determination or shall pay another bona fide fringe benefit or an hourly case equivalent thereof.

b. If the contractor or subcontractor, as appropriate, does not make payments to a trustee or other third person, he/she may consider as a part of the wages of any laborer or mechanic the amount of any costs reasonably anticipated in providing bona fide fringe benefits under a plan or program, provided, that the Secretary of Labor has found, upon the written request of the contractor, that the applicable standards of the Davis-Bacon Act have been met. The Secretary of Labor may require the contractor to set aside in a separate account assets for the meeting of obligations under the plan or program.

**4. Apprentices and Trainees (Programs of the U.S. DOL) and Helpers:**

**a. Apprentices:**

(1) Apprentices will be permitted to work at less than the predetermined rate for the work they performed when they are employed pursuant to and individually registered in a bona fide apprenticeship program registered with the DOL, Employment and Training Administration, Bureau of Apprenticeship and Training, or with a State apprenticeship agency recognized by the Bureau, or if a person is employed in his/her first 90 days of probationary employment as an apprentice in such an apprenticeship program, who is not individually registered in the program, but who has been certified by the Bureau of Apprenticeship and Training or a State apprenticeship agency (where appropriate) to be eligible for probationary employment as an apprentice.

(2) The allowable ratio of apprentices to journeyman-level employees on the job site in any craft classification shall not be greater than the ratio permitted to the contractor as to the entire work force under the registered program. Any employee listed on a payroll at an apprentice wage rate, who is not registered or otherwise employed as stated above, shall be paid not less than the applicable wage rate listed in the wage determination for the classification of work actually performed. In addition, any apprentice performing work on the job site in excess of the ratio permitted under the registered program shall be paid not less than the applicable wage rate on the wage determination for the work actually performed. Where a contractor or subcontractor is performing construction on a project in a locality other than that in which its program is registered, the ratios and wage rates (expressed in percentages of the journeyman-level hourly rate) specified in the contractor's or subcontractor's registered program shall be observed.

(3) Every apprentice must be paid at not less than the rate specified in the registered program for the apprentice's level of progress, expressed as a percentage of the journeyman-level hourly rate specified in the applicable wage determination. Apprentices shall be paid fringe benefits in accordance with the provisions of the apprenticeship program. If the apprenticeship program does not specify fringe benefits, apprentices must be paid the full amount of fringe benefits listed on the wage determination for the applicable classification. If the Administrator for the Wage and Hour Division determines that a different practice prevails for the applicable apprentice classification, fringes shall be paid in accordance with that determination.

(4) In the event the Bureau of Apprenticeship and Training, or a State apprenticeship agency recognized by the Bureau, withdraws approval of an apprenticeship program, the contractor or subcontractor will no longer be permitted to utilize apprentices at less than the applicable predetermined rate for the comparable work performed by regular employees until an acceptable program is approved.

**b. Trainees:**

(1) Except as provided in 29 CFR 5.16, trainees will not be permitted to work at less than the predetermined rate for the work performed unless they are employed pursuant to and individually registered in a program which has received prior approval, evidenced by formal certification by the DOL, Employment and Training Administration.

(2) The ratio of trainees to journeyman-level employees on the job site shall not be greater than permitted under the plan approved by the Employment and Training Administration. Any employee listed on the payroll at a trainee rate who is not registered and participating in a training plan approved by the Employment and Training Administration shall be paid not less than the applicable wage rate on the wage determination for the classification of work actually performed. In addition, any trainee performing work on the job site in excess of the ratio permitted under the registered program shall be paid not less than the applicable wage rate on the wage determination for the work actually performed.

(3) Every trainee must be paid at not less than the rate specified in the approved program for his/her level of progress, expressed as a percentage of the journeyman-level hourly rate specified in the applicable wage determination. Trainees shall be paid fringe benefits in accordance with the provisions of the trainee program. If the trainee program does not mention fringe benefits, trainees shall be paid the full amount of fringe benefits listed on the wage determination unless the Administrator of the Wage and Hour Division determines that there is an apprenticeship program associated with the corresponding journeyman-level wage rate on the wage determination which provides for less than full fringe benefits for apprentices, in which case such trainees shall receive the same fringe benefits as apprentices.

(4) In the event the Employment and Training Administration withdraws approval of a training program, the contractor or subcontractor will no longer be permitted to utilize trainees at less than the applicable predetermined rate for the work performed until an acceptable program is approved.

**c. Helpers:**

Helpers will be permitted to work on a project if the helper classification is specified and defined on the applicable wage determination or is approved pursuant to the conformance procedure set forth in Section IV.2. Any worker listed on a payroll at a helper wage rate, who is not a helper under an approved definition, shall be paid not less than the applicable wage rate on the wage determination for the classification of work actually performed.

**5. Apprentices and Trainees (Programs of the U.S. DOT):**

Apprentices and trainees working under apprenticeship and skill training programs which have been certified by the Secretary of Transportation as promoting EEO in connection with Federal-aid highway construction programs are not subject to the requirements of paragraph 4 of this Section IV. The straight time hourly wage rates for apprentices and trainees under such programs will be established by the particular programs. The ratio of apprentices and trainees to journeymen shall not be greater than permitted by the terms of the particular program.

**6. Withholding:**

The SHA shall upon its own action or upon written request of an authorized representative of the DOL withhold, or cause to be withheld, from the contractor or subcontractor under this contract or any other Federal contract with the same prime contractor, or any other Federally-assisted contract subject to Davis-Bacon prevailing wage requirements which is held by the

same prime contractor, as much of the accrued payments or advances as may be considered necessary to pay laborers and mechanics, including apprentices, trainees, and helpers, employed by the contractor or any subcontractor the full amount of wages required by the contract. In the event of failure to pay any laborer or mechanic, including any apprentice, trainee, or helper, employed or working on the site of the work, all or part of the wages required by the contract, the SHA contracting officer may, after written notice to the contractor, take such action as may be necessary to cause the suspension of any further payment, advance, or guarantee of funds until such violations have ceased.

#### **7. Overtime Requirements:**

No contractor or subcontractor contracting for any part of the contract work which may require or involve the employment of laborers, mechanics, watchmen, or guards (including apprentices, trainees, and helpers described in paragraphs 4 and 5 above) shall require or permit any laborer, mechanic, watchman, or guard in any workweek in which he/she is employed on such work, to work in excess of 40 hours in such workweek unless such laborer, mechanic, watchman, or guard receives compensation at a rate not less than one-and-one-half times his/her basic rate of pay for all hours worked in excess of 40 hours in such workweek.

#### **8. Violation:**

**Liability for Unpaid Wages; Liquidated Damages:** In the event of any violation of the clause set forth in paragraph 7 above, the contractor and any subcontractor responsible thereof shall be liable to the affected employee for his/her unpaid wages. In addition, such contractor and subcontractor shall be liable to the United States (in the case of work done under contract for the District of Columbia or a territory, to such District or to such territory) for liquidated damages. Such liquidated damages shall be computed with respect to each individual laborer, mechanic, watchman, or guard employed in violation of the clause set forth in paragraph 7, in the sum of \$10 for each calendar day on which such employee was required or permitted to work in excess of the standard work week of 40 hours without payment of the overtime wages required by the clause set forth in paragraph 7.

#### **9. Withholding for Unpaid Wages and Liquidated Damages:**

The SHA shall upon its own action or upon written request of any authorized representative of the DOL withhold, or cause to be withheld, from any monies payable on account of work performed by the contractor or subcontractor under any such contract or any other Federal contract with the same prime contractor, or any other Federally-assisted contract subject to the Contract Work Hours and Safety Standards Act, which is held by the same prime contractor, such sums as may be determined to be necessary to satisfy any liabilities of such contractor or subcontractor for unpaid wages and liquidated damages as provided in the clause set forth in paragraph 8 above.

### **V. STATEMENTS AND PAYROLLS**

(Applicable to all Federal-aid construction contracts exceeding \$2,000 and to all related subcontracts, except for projects located on roadways classified as local roads or rural collectors, which are exempt.)

#### **1. Compliance with Copeland Regulations (29 CFR 3):**

The contractor shall comply with the Copeland Regulations of the Secretary of Labor which are herein incorporated by reference.

#### **2. Payrolls and Payroll Records:**

a. Payrolls and basic records relating thereto shall be maintained by the contractor and each subcontractor during the course of the work and preserved for a period of 3 years from the date of completion of the contract for all laborers, mechanics, apprentices, trainees, watchmen, helpers, and guards working at the site of the work.

b. The payroll records shall contain the name, social security number, and address of each such employee; his or her correct classification; hourly rates of wages paid (including rates of contributions or costs anticipated for bona fide fringe benefits or cash equivalent thereof the types described in Section 1(b)(2)(B) of the Davis Bacon Act); daily and weekly number of hours worked; deductions made; and actual wages paid. In addition, for Appalachian contracts, the payroll records shall contain a notation indicating whether the employee does, or does not, normally reside in the labor area as defined in Attachment A, paragraph 1. Whenever the Secretary of Labor, pursuant to Section IV, paragraph 3b, has found that the wages of any laborer or mechanic include the amount of any costs reasonably anticipated in providing benefits under a plan or program described in Section 1(b)(2)(B) of the Davis Bacon Act, the contractor and each subcontractor shall maintain records which show that the commitment to provide such benefits is enforceable, that the plan or program is financially responsible, that the plan or program has been communicated in writing to the laborers or mechanics affected, and show the cost anticipated or the actual cost incurred in providing benefits. Contractors or subcontractors employing apprentices or trainees under approved programs shall maintain written evidence of the registration of apprentices and trainees, and ratios and wage rates prescribed in the applicable programs.

c. Each contractor and subcontractor shall furnish, each week in which any contract work is performed, to the SHA resident engineer a payroll of wages paid each of its employees (including apprentices, trainees, and helpers, described in Section IV, paragraphs 4 and 5, and watchmen and guards engaged on work during the preceding weekly payroll period). The payroll submitted required to be maintained under paragraph 2b of this Section V. This information may be submitted in any form desired. Optional Form WH-347 is available for this purpose and may be purchased from the Superintendent of Documents (Federal stock number 029-005-0014-1), U.S. Government Printing Office, Washington, D.C. 20402. The prime contractor is responsible for the submission of copies of payrolls by all subcontractors.

d. Each payroll submitted shall be accompanied by a "Statement of Compliance," signed by the contractor or subcontractor or his/her agent who pays or supervises the payment of the persons employed under the contract and shall certify the following:

(1) that the payroll for the payroll period contains the information required to be maintained under paragraph 2b of this Section V and that such information is correct and complete;

(2) that such laborer or mechanic (including each helper, apprentice, and trainee) employed on the contract during the payroll period has been paid the full weekly wages earned,



without rebate, either directly or indirectly, and that no deductions have been made either directly or indirectly from the full wages earned, other than permissible deductions as set forth in the Regulations, 29 CFR 3;

(3) that each laborer or mechanic has been paid not less than the applicable wage rate and fringe benefits or cash equivalent for the classification of work performed, as specified in the applicable wage determination incorporated into the contract.

e. The weekly submission of a properly executed certification set forth on the reverse side of Optional Form WH-347 shall satisfy the requirement for submission of the "Statement of Compliance" required by paragraph 2d of this Section V.

f. The falsification of any of the above certifications may subject the contractor to civil or criminal prosecution under 18 U.S.C. 1001 and 31 U.S.C. 231.

g. The contractor or subcontractor shall make the records required under paragraph 2b of this Section V available for inspection, copying, or transcription by authorized representatives of the SHA, the FHWA, or the DOL, and shall permit such representatives to interview employees during working hours on the job. If the contractor or subcontractor fails to submit the required records or to make them available, the SHA, the FHWA, the DOL, or all may, after written notice to the contractor, sponsor, applicant, or owner, take such actions as may be necessary to cause the suspension of any further payment, advance, or guarantee of funds. Furthermore, failure to submit the required records upon request or to make such records available may be grounds for debarment action pursuant to 29 CFR 5.12.

## VI. RECORD OF MATERIALS, SUPPLIES, AND LABOR

1. On all Federal-aid contracts on the National Highway System, except those which provide solely for the installation of protective devices at railroad grade crossings, those which are constructed on a force account or direct labor basis, highway beautification contracts, and contracts for which the total final construction cost for roadway and bridge is less than \$1,000,000 (23 CFR 635) the contractor shall:

a. Become familiar with the list of specific materials and supplies contained in Form FHWA-47, "Statement of Materials and Labor Used by Contractor of Highway Construction Involving Federal Funds," prior to the commencement of work under this contract.

b. Maintain a record of the total cost of all materials and supplies purchased for and incorporated in the work, and also of the quantities of those specific materials and supplies listed on Form FHWA-47, and in the units shown on Form FHWA-47.

c. Furnish, upon the completion of the contract, to the SHA resident engineer on Form FHWA-47 together with the data required in paragraph 1b relative to materials and supplies, a final labor summary of all contract work indicating the total hours worked and the total amount earned.

2. At the prime contractor's option, either a single report covering all contract work or separate reports for the contractor and for each subcontract shall be submitted.

## VII. SUBLETTING OR ASSIGNING THE CONTRACT

1. The contractor shall perform with its own organization contract work amounting to not less than 30 percent (or a greater percentage if specified elsewhere in the contract) of the total original contract price, excluding any specialty items designated by the State. Specialty items may be performed by subcontract and the amount of any such specialty items performed may be deducted from the total original contract price before computing the amount of work required to be performed by the contractor's own organization (23 CFR 635).

a. "Its own organization" shall be construed to include only workers employed and paid directly by the prime contractor and equipment owned or rented by the prime contractor, with or without operators. Such term does not include employees or equipment of a subcontractor, assignee, or agent of the prime contractor.

b. "Specialty Items" shall be construed to be limited to work that requires highly specialized knowledge, abilities, or equipment not ordinarily available in the type of contracting organizations qualified and expected to bid on the contract as a whole and in general are to be limited to minor components of the overall contract.

2. The contract amount upon which the requirements set forth in paragraph 1 of Section VII is computed includes the cost of material and manufactured products which are to be purchased or produced by the contractor under the contract provisions.

3. The contractor shall furnish (a) a competent superintendent or supervisor who is employed by the firm, has full authority to direct performance of the work in accordance with the contract requirements, and is in charge of all construction operations (regardless of who performs the work) and (b) such other of its own organizational resources (supervision, management, and engineering services) as the SHA contracting officer determines is necessary to assure the performance of the contract.

4. No portion of the contract shall be sublet, assigned or otherwise disposed of except with the written consent of the SHA contracting officer, or authorized representative, and such consent when given shall not be construed to relieve the contractor of any responsibility for the fulfillment of the contract. Written consent will be given only after the SHA has assured that each subcontract is evidenced in writing and that it contains all pertinent provisions and requirements of the prime contract.

## VIII. SAFETY: ACCIDENT PREVENTION

1. In the performance of this contract the contractor shall comply with all applicable Federal, State, and local laws governing safety, health, and sanitation (23 CFR 635). The contractor shall provide all safeguards, safety devices and protective equipment and take any other needed actions as it determines, or as the SHA contracting officer may determine, to be reasonably necessary to protect the life and health of employees on the job and the safety of the public and to protect property in connection with the performance of the work covered by the contract.

2. It is a condition of this contract, and shall be made a condition of each subcontract, which the contractor enters into pursuant to this contract, that the contractor and any subcontractor shall not permit any employee, in performance of the contract, to work in surroundings or under conditions which are unsanitary,

hazardous or dangerous to his/her health or safety, as determined under construction safety and health standards (29 CFR 1926) promulgated by the Secretary of Labor, in accordance with Section 107 of the Contract Work Hours and Safety Standards Act (40 U.S.C. 333).

3. Pursuant to 29 CFR 1926.3, it is a condition of this contract that the Secretary of Labor or authorized representative thereof, shall have right of entry to any site of contract performance to inspect or investigate the matter of compliance with the construction safety and health standards and to carry out the duties of the Secretary under Section 107 of the Contract Work Hours and Safety Standards Act (40 U.S.C. 333).

#### **IX. FALSE STATEMENTS CONCERNING HIGHWAY PROJECTS**

In order to assure high quality and durable construction in conformity with approved plans and specifications and a high degree of reliability on statements and representations made by engineers, contractors, suppliers, and workers on Federal-aid highway projects, it is essential that all persons concerned with the project perform their functions as carefully, thoroughly, and honestly as possible. Willful falsification, distortion, or misrepresentation with respect to any facts related to the project is a violation of Federal law. To prevent any misunderstanding regarding the seriousness of these and similar acts, the following notice shall be posted on each Federal-aid highway project (23 CFR 635) in one or more places where it is readily available to all persons concerned with the project:

##### **NOTICE TO ALL PERSONNEL ENGAGED ON FEDERAL-AID HIGHWAY PROJECTS**

18 U.S.C. 1020 reads as follows:

*"Whoever, being an officer, agent, or employee of the United States, or of any State or Territory, or whoever, whether a person, association, firm, or corporation, knowingly makes any false statement, false representation, or false report as to the character, quality, quantity, or cost of the material used or to be used, or the quantity or quality of the work performed or to be performed, or the cost thereof in connection with the submission of plans, maps, specifications, contracts, or costs of construction on any highway or related project submitted for approval to the Secretary of Transportation; or*

*Whoever knowingly makes any false statement, false representation, false report or false claim with respect to the character, quality, quantity, or cost of any work performed or to be performed, or materials furnished or to be furnished, in connection with the construction of any highway or related project approved by the Secretary of Transportation; or*

*Whoever knowingly makes any false statement or false representation as to material fact in any statement, certificate, or report submitted pursuant to provisions of the Federal-aid Roads Act approved July 1, 1916, (39 Stat. 355), as amended and supplemented;*

*Shall be fined not more than \$10,000 or imprisoned not more than 5 years or both."*

#### **X. IMPLEMENTATION OF CLEAN AIR ACT AND FEDERAL WATER POLLUTION CONTROL ACT**

(Applicable to all Federal-aid construction contracts and to all related subcontracts of \$100,000 or more.)

By submission of this bid or the execution of this contract, or subcontract, as appropriate, the bidder, Federal-aid construction contractor, or subcontractor, as appropriate, will be deemed to have stipulated as follows:

1. That any facility that is or will be utilized in the performance of this contract, unless such contract is exempt under the Clean Air Act, as amended (42 U.S.C. 1857 et seq., as amended by Pub.L. 91-604), and under the Federal Water Pollution Control Act, as amended (33 U.S.C. 1251 et seq., as amended by Pub.L. 92-500), Executive Order 11738, and regulations in implementation thereof (40 CFR 15) is not listed, on the date of contract award, on the U.S. Environmental Protection Agency (EPA) List of Violating Facilities pursuant to 40 CFR 15.20.

2. That the firm agrees to comply and remain in compliance with all the requirements of Section 114 of the Clean Air Act and Section 308 of the Federal Water Pollution Control Act and all regulations and guidelines listed thereunder.

3. That the firm shall promptly notify the SHA of the receipt of any communication from the Director, Office of Federal Activities, EPA, indicating that a facility that is or will be utilized for the contract is under consideration to be listed on the EPA List of Violating Facilities.

4. That the firm agrees to include or cause to be included the requirements of paragraph 1 through 4 of this Section X in every nonexempt subcontract, and further agrees to take such action as the government may direct as a means of enforcing such requirements.

#### **XI. CERTIFICATION REGARDING DEBARMENT, SUSPENSION, INELIGIBILITY AND VOLUNTARY EXCLUSION**

##### **1. Instructions for Certification - Primary Covered Transactions:**

(Applicable to all Federal-aid contracts - 49 CFR 29)

a. By signing and submitting this proposal, the prospective primary participant is providing the certification set out below.

b. The inability of a person to provide the certification set out below will not necessarily result in denial of participation in this covered transaction. The prospective participant shall submit an explanation of why it cannot provide the certification set out below. The certification or explanation will be considered in connection with the department or agency's determination whether to enter into this transaction. However, failure of the prospective primary participant to furnish a certification or an explanation shall disqualify such a person from participation in this transaction.

c. The certification in this clause is a material representation of fact upon which reliance was placed when the department or agency determined to enter into this transaction. If it is later determined that the prospective primary participant knowingly rendered an erroneous certification, in addition to other remedies available to the Federal Government, the department or agency may terminate this transaction for cause of default.

d. The prospective primary participant shall provide immediate written notice to the department or agency to whom this proposal is submitted if any time the prospective primary participant learns that its certification was erroneous when submitted or has become erroneous by reason of changed circumstances.

e. The terms "covered transaction," "debarred," "suspended," "ineligible," "lower tier covered transaction," "participant," "person," "primary covered transaction," "principal," "proposal," and "voluntarily excluded," as used in this clause, have the meanings set out in the Definitions and Coverage sections of rules implementing Executive Order 12549. You may contact the department or agency to which this proposal is submitted for assistance in obtaining a copy of those regulations.

f. The prospective primary participant agrees by submitting this proposal that, should the proposed covered transaction be entered into, it shall not knowingly enter into any lower tier covered transaction with a person who is debarred, suspended, declared ineligible, or voluntarily excluded from participation in this covered transaction, unless authorized by the department or agency entering into this transaction.

g. The prospective primary participant further agrees by submitting this proposal that it will include the clause titled "Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion-Lower Tier Covered Transaction," provided by the department or agency entering into this covered transaction, without modification, in all lower tier covered transactions and in all solicitations for lower tier covered transactions.

h. A participant in a covered transaction may rely upon a certification of a prospective participant in a lower tier covered transaction that is not debarred, suspended, ineligible, or voluntarily excluded from the covered transaction, unless it knows that the certification is erroneous. A participant may decide the method and frequency by which it determines the eligibility of its principals. Each participant may, but is not required to, check the nonprocurement portion of the "Lists of Parties Excluded From Federal Procurement or Nonprocurement Programs" (Nonprocurement List) which is compiled by the General Services Administration.

i. Nothing contained in the foregoing shall be construed to require establishment of a system of records in order to render in good faith the certification required by this clause. The knowledge and information of participant is not required to exceed that which is normally possessed by a prudent person in the ordinary course of business dealings.

j. Except for transactions authorized under paragraph f of these instructions, if a participant in a covered transaction knowingly enters into a lower tier covered transaction with a person who is suspended, debarred, ineligible, or voluntarily excluded from participation in this transaction, in addition to other remedies available to the Federal Government, the department or agency may terminate this transaction for cause or default.

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#### **Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion--Primary Covered Transactions**

1. The prospective primary participant certifies to the best of its knowledge and belief, that it and its principals:

a. Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any Federal department or agency;

b. Have not within a 3-year period preceding this proposal been convicted of or had a civil judgement rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, or receiving stolen property;

c. Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated in paragraph 1b of this certification; and

d. Have not within a 3-year period preceding this application/proposal had one or more public transactions (Federal, State or local) terminated for cause or default.

2. Where the prospective primary participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

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#### **2. Instructions for Certification - Lower Tier Covered Transactions:**

(Applicable to all subcontracts, purchase orders and other lower tier transactions of \$25,000 or more - 49 CFR 29)

a. By signing and submitting this proposal, the prospective lower tier is providing the certification set out below.

b. The certification in this clause is a material representation of fact upon which reliance was placed when this transaction was entered into. If it is later determined that the prospective lower tier participant knowingly rendered an erroneous certification, in addition to other remedies available to the Federal Government, the department, or agency with which this transaction originated may pursue available remedies, including suspension and/or debarment.

c. The prospective lower tier participant shall provide immediate written notice to the person to which this proposal is submitted if at any time the prospective lower tier participant learns that its certification was erroneous by reason of changed circumstances.

d. The terms "covered transaction," "debarred," "suspended," "ineligible," "primary covered transaction," "participant," "person," "principal," "proposal," and "voluntarily excluded," as used in this clause, have the meanings set out in the Definitions and Coverage sections of rules implementing Executive

Order 12549. You may contact the person to which this proposal is submitted for assistance in obtaining a copy of those regulations.

e. The prospective lower tier participant agrees by submitting this proposal that, should the proposed covered transaction be entered into, it shall not knowingly enter into any lower tier covered transaction with a person who is debarred, suspended, declared ineligible, or voluntarily excluded from participation in this covered transaction, unless authorized by the department or agency with which this transaction originated.

f. The prospective lower tier participant further agrees by submitting this proposal that it will include this clause titled "Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion-Lower Tier Covered Transaction," without modification, in all lower tier covered transactions and in all solicitations for lower tier covered transactions.

g. A participant in a covered transaction may rely upon a certification of a prospective participant in a lower tier covered transaction that is not debarred, suspended, ineligible, or voluntarily excluded from the covered transaction, unless it knows that the certification is erroneous. A participant may decide the method and frequency by which it determines the eligibility of its principals. Each participant may, but is not required to, check the Nonprocurement List.

h. Nothing contained in the foregoing shall be construed to require establishment of a system of records in order to render in good faith the certification required by this clause. The knowledge and information of participant is not required to exceed that which is normally possessed by a prudent person in the ordinary course of business dealings.

i. Except for transactions authorized under paragraph e of these instructions, if a participant in a covered transaction knowingly enters into a lower tier covered transaction with a person who is suspended, debarred, ineligible, or voluntarily excluded from participation in this transaction, in addition to other remedies available to the Federal Government, the department or agency with which this transaction originated may pursue available remedies, including suspension and/or debarment.

\* \* \* \* \*

**Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion—Lower Tier Covered Transactions:**

1. The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared

ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.

2. Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

\* \* \* \* \*

**XII. CERTIFICATION REGARDING USE OF CONTRACT FUNDS FOR LOBBYING**

(Applicable to all Federal-aid construction contracts and to all related subcontracts which exceed \$100,000 - 49 CFR 20)

1. The prospective participant certifies, by signing and submitting this bid or proposal, to the best of his or her knowledge and belief, that:

a. No Federal appropriated funds have been paid or will be paid, by or on behalf of the undersigned, to any person for influencing or attempting to influence an officer or employee of any Federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any Federal contract, the making of any Federal grant, the making of any Federal loan, the entering into of any cooperative agreement, and the extension, continuation, renewal, amendment, or modification of any Federal contract, grant, loan, or cooperative agreement.

b. If any funds other than Federal appropriated funds have been paid or will be paid to any person for influencing or attempting to influence an officer or employee of any Federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with this Federal contract, grant, loan, or cooperative agreement, the undersigned shall complete and submit Standard Form-LLL, "Disclosure Form to Report Lobbying," in accordance with its instructions.

2. This certification is a material representation of fact upon which reliance was placed when this transaction was made or entered into. Submission of this certification is a prerequisite for making or entering into this transaction imposed by 31 U.S.C. 1352. Any person who fails to file the required certification shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.

3. The prospective participant also agrees by submitting his or her bid or proposal that he or she shall require that the language of this certification be included in all lower tier subcontracts, which exceed \$100,000 and that all such recipients shall certify and disclose accordingly.



**NOTICE OF REQUIREMENTS FOR AFFIRMATIVE  
ACTION TO ENSURE EQUAL EMPLOYMENT  
OPPORTUNITY (EXECUTIVE ORDER 11246)**

1. The Offeror's or Bidder's attention is called to the "Equal Opportunity Clause" and the "Standard Federal Equal Employment Opportunity Construction Contract Specifications" set forth herein.
2. The goals and timetables for minority and female participation, expressed in percentage terms for the Contractor's aggregate workforce in each trade on all construction work in the covered area, are as follows:

Timetables	Goals for female participation in each trade (percent)
From April 1, 1978 until March 31, 1979	3.1
From April 1, 1979 until March 31, 1980	5.1
From April 1, 1980 until March 31, 1981	6.9

Until further notice	Goals for minority participation for each trade (percent)
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SHSA Cities:

Pascagoula - Moss Point -----	16.9
Biloxi - Gulfport -----	19.2
Jackson -----	30.3

SMSA Counties:

Desoto -----	32.3
Hancock, Harrison, Stone -----	19.2
Hinds, Rankin -----	30.3
Jackson -----	16.9

Non-SMSA Counties:

George, Greene -----	26.4
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Alcorn, Benton, Bolivar, Calhoun, Carroll, Chickasaw, Clay, Coahoma, Grenada, Itawamba, Lafayette, Lee, Leflore, Marshall, Monroe, Montgomery, Panola, Pontotoc, Prentiss, Quitman, Sunflower, Tallahatchie, Tate, Tippah, Tishomingo, Tunica, Union, Washington, Webster, Yalobusha ----- 26.5

Attala, Choctaw, Claiborne, Clarke, Copiah, Covington, Franklin, Holmes, Humphreys, Issaquena, Jasper, Jefferson, Jefferson Davis, Jones Kemper, Lauderdale, Lawrence, Leake, Lincoln, Lowndes, Madison, Neshoba, Newton, Noxubee, Oktibbeha, Scott, Sharkey, Simpson, Smith, Warren, Wayne, Winston, Yazoo ----- 32.0

Forrest, Lamar, Marion, Pearl River, Perry, Pike, Walthall ----- 27.7

Adams, Amite, Wilkinson ----- 30.4

These goals are applicable to all the Contractor's construction work (whether or not it is Federal or federally assisted) performed in the covered area. If the contractor performs construction work in a geographical area located outside of the covered area, it shall apply the goals established for such geographical area where the work is actually performed. With regard to this second area, the contractor also is subject to the goals for both its federally involved and nonfederally involved construction.

The Contractor's compliance with the Executive Order and the regulations in CFR Part 60-4 shall be based on its implementation of the Equal Opportunity clause, specific affirmative action obligations required by the specifications set forth in 41 CFR 60-4.3(a), and its efforts to meet the goals. The hours of minority and female employment and training must be substantially uniform throughout the length of the contract, and in each trade, and the Contractor shall make a good faith effort to employ minorities and women evenly on each of its projects. The transfer of minority or female employees or trainees from Contractor to Contractor or from project to project for the sole purpose of meeting the Contractor's goals shall be a violation of the contract, the Executive Order and the regulations in 41 CFR Part 60-4. Compliance with the goals will be measured against the total work hours performed.

3. The Contractor shall provide written notification to the Director of the Office of Federal Contract Compliance Programs within 10 working days of award of any construction subcontract in excess of \$10,000 at any tier for construction work under the contract resulting from this solicitation. The notification shall list the name, address and telephone number of the subcontractor, employer identification number of the subcontractor, estimated dollar amount of the subcontract; estimated starting and completion dates of the subcontract; and the geographical area in which the subcontract is to be performed.

4. As used in this Notice, and in the contract resulting from this solicitation, the "covered area" is to the county and city (if any), stated in the advertisement.

5. The notification required in Paragraph 3 shall be addressed to the following:

Contract Compliance Officer  
Mississippi Department of Transportation  
P.O. Box 1850  
Jackson, Mississippi 39215-1850

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-101-4

CODE: (IS)

DATE: 11/05/2008

SUBJECT: Definitions

Section 101, Definitions and Terms, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

**907-101.02--Definitions.** Replace the following definitions in Subsection 101.02 on pages 3 through 13.

**Contract** - The written agreement between the Mississippi Transportation Commission and the Contractor setting forth the obligations of the parties thereunder, including but not limited to, the performance of the work, the furnishing of labor and materials, and the basis of payment.

The contract includes the invitation for bids, proposal, contract form and contract bonds, specifications, supplemental specifications, interim specifications, general and detailed plans, special provisions, notices to bidders, notice to proceed, and also any agreements that are required to complete the construction of the work in an acceptable manner, including authorized extensions thereof, all of which constitute one instrument.

**Contract Bonds** - The approved form of security, executed by the Contractor and the Contractor's Surety(ies), guaranteeing complete execution of the contract and all supplemental agreements pertaining thereto and the payment of all legal debts pertaining to the construction of the project. This term includes Performance and Payment Bond(s).

**Surety** - A corporate body, qualified under the laws of Mississippi, which is bound with and for the successful bidder by "contract bond(s)" to guarantee acceptable performance of the contract and payment of all legal taxes and debts pertaining to the construction of the project, including payment of State Sales Tax as prescribed by law, and any overpayment made to the Contractor.

Add the following to the list of definitions in Subsection 101.02 on pages 3 through 13.

**Performance Bond** - The approved form of security, executed by the Contractor and issued by the Contractor's Surety(ies), guaranteeing satisfactory completion of the contract and all supplemental agreements pertaining thereto.

**Payment Bond** - The approved form of security, executed by the Contractor and issued by the Contractor's Surety(ies), guaranteeing the payment of all legal debts pertaining to the construction of the project including, but not limited to, the labor and materials of subcontractors and suppliers to the prime contractor.

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

| SPECIAL PROVISION NO. 907-102-8

CODE: (IS)

| DATE: 01/20/2011

**SUBJECT: Bidding Requirements and Conditions**

**907-102.06--Preparation of Proposal.** Delete the fifth, sixth, and seventh paragraphs of Subsection 102.06 on page 18 and substitute the following:

Bid sheets generated by the Department's Electronic Bid System (Trns•port Expedite Bid) along with a completed proposal package will constitute the official bid and shall be signed on the last sheet of the Expedite Bid generated bid sheets and delivered to the Department in accordance with the provisions of Subsection 102.09.

Bidders are cautioned that using other versions of the Expedite Bid may result in improperly printed bid sheets. The correct version of Expedite Bid can be obtained at no cost from the MDOT Contract Administration Division or at the MDOT website, [www.gomdot.com](http://www.gomdot.com).

If bidders submit Expedite Bid generated bid sheets, then the bid sheets included in the proposal should not be completed. The Expedite Bid generated bid sheets should be stapled together, signed and included in the bid proposal package in the sealed envelope. If both the forms in the proposal and the Expedite Bid generated bid sheets are completed and submitted, only the Expedite Bid generated sheets will be recognized and used for the official bid. The USB Flash Drive containing the information printed on the Expedite Bid generated bid sheets should be placed in the padded envelope included with the bid proposal package and enclosed in the sealed envelope. Bid sheets printed from Expedite Bid should be a representation of the data returned on the flash drive. To have a true representation of the bid sheets, the Bidder must copy the EBS and EBS amendment files used to prepare the bid sheets to the flash drive. Otherwise, the unit prices bid will not be recorded to the flash drive. Bidders are cautioned that failure to follow proper flash drive handling procedures could result in the Department being unable to process the flash drive. Any modification or manipulation of the data contained on the flash drive, other than entering unit bid prices and completing all required Expedite Bid sections, will not be allowed and will cause the Contractor's bid to be considered irregular.

**907-102.08--Proposal Guaranty.** Delete the first and second paragraphs in Subsection 102.08 on page 20 and substitute the following:

No proposal will be considered unless accompanied by certified check, cashier's check or bid bond, made payable to the State of Mississippi, in an amount of not less than five percent (5%) of the total amount of the proposal offered. The guaranty shall be evidence of good faith that, if awarded the contract, the bidder will execute the contract and give performance and payment contract bond(s) as stipulated in Subsection 907-103.05.1, 907-103.05.2, and as required by law.

If a bid bond is offered as guaranty, the bond must be on a form approved by the Executive Director, made by a Surety acceptable to the Executive Director and signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent and the Bidder. Such bid bond shall also conform to the requirements and conditions stipulated in Subsection 907-103.05.2 as applicable.



## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-103-8

CODE: (SP)

DATE: 12/15/2009

SUBJECT: Award and Execution of Contract

Section 103, Award and Execution of Contract, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

**907-103.04--Return of Proposal Guaranty.** Delete the second paragraph of Subsection 103.04 on page 23 and substitute the following:

Certified checks or cashier's checks submitted as proposal guaranties, except those of the two lowest bidders, will be returned within 10 days of contract award. The retained proposal guaranty of the unsuccessful of the two lowest bidders will be returned within ten days following the execution of a contract with the successful low bidder. The retained proposal guaranty of the successful bidder will be returned after satisfactory performance and payment bonds have been furnished and the contract has been executed.

In the event all bids are rejected by the Commission, certified checks or cashier's checks submitted as proposal guaranty by all bidders will be returned within 10 days of rejection.

Delete Subsection 103.05 on page 23 and substitute the following:

**907-103.05--Contract Bonds.**

**907-103.05.1--Requirement of Contract Bonds.** Prior to the execution of the contract, the successful bidder shall execute and deliver to the Executive Director a performance and payment bond(s), in a sum equal to the full amount of the contract as a guaranty for complete and full performance of the contract and the protection of the claimants and the Department for materials and equipment and full payment of wages in accordance with Section 65-1-85 Miss. Code Ann. (1972 as amended). In the event of award of a joint bid, each individual, partnership, firm or corporation shall assume jointly the full obligations under the contract and the contract bond(s).

**907-103.05.2--Form of Bonds.** The form of bond(s) shall be that provided by or acceptable to the Department. These bonds shall be executed by a Mississippi agent or qualified nonresident agent and shall be accompanied by a certification as to authorization of the attorney-in-fact to commit the Surety company. A power of attorney exhibiting the Surety's original seal supporting the Mississippi agent or the qualified nonresident agent's signature shall be furnished with each bond. The Surety company shall be currently authorized and licensed in good standing to conduct business in the State of Mississippi with a minimum rating by A.M. Best of (A-) in the latest printing "Best's Key Rating Guide" to write individual bonds up to ten percent of the policy holders' surplus or listed on the current list of "Companies Holding Certificates of Authority as Acceptable Sureties on Federal Bonds and as Acceptable Reinsuring Companies" as

published by the United States Department of the Treasury, Financial Management Service, Circular 570 (latest revision as published and supplemented on the Financial Management Service Web site and in the Federal Register) within the underwriting limits listed for that Surety. All required signatures on the bond(s) and certifications shall be original signatures, in ink, and not mechanical reproductions or facsimiles. The [Mississippi agent](#) or [qualified nonresident agent](#) shall be in good standing and currently licensed by the Insurance Commissioner of the State of Mississippi to represent the Surety company(ies) executing the bonds.

Surety bonds shall continue to be acceptable to the Commission throughout the life of the Contract and shall not be canceled by the Surety without the consent of the Department. In the event the Surety fails or becomes financially insolvent, the Contractor shall file a new Bond in the amount designated by the Executive Director within thirty (30) days of such failure, insolvency, or bankruptcy. Subsequent to award of Contract, the Commission or the Department may [require additional security for any supplemental agreements executed under the contract or replacement security in the event of the surety\(ies\) loss of the ratings required above](#). Suits concerning bonds shall be filed in the State of Mississippi and adjudicated under its laws without reference to conflict of laws principles.

**907-103.08--Failure to Execute Contract.** In the first sentence of Subsection 103.08 on page 24, change “bond” to “performance and payment bonds”.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SPECIAL PROVISION NO. 907-104-4**

**CODE: (SP)**

**DATE: 03/01/2011**

**SUBJECT: Disposal of Materials**

Section 104, Scope of Work, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

**907-104.05--Removal and Disposal of All Materials From the Project.** Delete the second sentence of the first full paragraph of Subsection 104.05 on page 30 and substitute the following:

The Contractor shall also furnish the Engineer a certified letter stating that the area of disposal is not in a wetland or in Waters of the U.S.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SUPPLEMENT TO SPECIAL PROVISION NO. 907-105-6**

**DATE: 03/01/2011**

**SUBJECT: Maintenance During Construction**

After Subsection 907-105.05 on page 1, add the following.

**907-105.14--Maintenance During Construction.** Before the first sentence Subsection 105.14 on page 39, add the following:

The Contractor will be responsible for the maintenance of existing roadways within the limits of this project starting on the date of the Notice To Proceed / Beginning of Contract Time. Anytime work is performed in a travel lane, the Contractor shall install portable lane closure signs meeting the requirement of the MDOT Standard Drawing or MUTCD.



## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

| SPECIAL PROVISION NO. 907-105-6

CODE: (IS)

| DATE: 01/20/2011

| SUBJECT: Control of Work

Section 105, Control of Work, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is modified as follows:

**907-105.05--Cooperation by Contractor.** In the third sentence of the second paragraph of Subsection 105.05 on page 35, change “Notice to Proceed” to “Notice of Award”.

Delete the fourth paragraph of Subsection 105.05 on page 35, and substitute the following.

| On projects that include erosion control pay items, the Contractor shall also designate a responsible person whose primary duty shall be to monitor and maintain the effectiveness of the erosion control plan, including NPDES permit requirements. This responsible person must be a Certified Erosion Control Person certified by an organization approved by the Department. Prior to or at the pre-construction conference, the Contractor shall designate in writing the Certified Erosion Control Person to the Project Engineer. The designated Certified Erosion Control Person shall be assigned to only one (1) project. When special conditions exist, such as two (2) adjoining projects or two (2) projects in close proximity, the Contractor may request in writing that the State Construction Engineer approve the use of one (1) Certified Erosion Control Person for both projects. The Contractor may request in writing that the Engineer authorize a substitute Certified Erosion Control Person to act in the absence of the Certified Erosion Control Person. The substitute Certified Erosion Control Person must also be certified by an organization approved by the Department. A copy of the Certified Erosion Control Person's certification must be included in the Contractor's Protection Plan as outlined in Subsection 907-107.22.1. This in no way modifies the requirements regarding the assignment and availability of the superintendent.

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

| SPECIAL PROVISION NO. 907-107-9

CODE: (IS)

| DATE: 01/20/2011

SUBJECT: Legal Relations and Responsibility to Public

Section 107, Legal Relations and Responsibility to Public, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

**907-107.02--Permits, Licenses and Taxes.** Delete in toto Subsection 107.02 on page 49 and substitute the following:

The Contractor or any Subcontractor shall have the duty to determine any and all permits and licenses required and to procure all permits and licenses, pay all charges, fees and taxes and issue all notices necessary and incidental to the due and lawful prosecution of the work. At any time during the life of this contract, the Department may audit the Contractor's or Subcontractor's compliance with the requirements of this section.

The Contractor or any Subcontractor is advised that the "Mississippi Special Fuel Tax Law", Section 27-55-501, et seq. and the Mississippi Use Tax Law, Section 27-67-1, et seq., and their requirements and penalties, apply to any contract or subcontract for construction, reconstruction, maintenance or repairs, for contracts or subcontracts entered into with the State of Mississippi, any political subdivision of the State of Mississippi, or any Department, Agency, Institute of the State of Mississippi or any political subdivision thereof.

The Contractor or any Subcontractor will be subject to one or more audits by the Department during the life of this contract to make certain that all applicable fuel taxes, as outlined in Section 27-55-501, et seq., and any sales and/or use taxes, as outlined in Section 27-67-1, et seq. are being paid in compliance with the law. The Department will notify the Mississippi State Tax Commission of the names and addresses of any Contractors or Subcontractors.

| **907-107.14--Damage Claims and Insurance.**

**907-107.14.2--Liability Insurance.** Delete Subsection 107.14.2 beginning on page 60 and substitute:

**907-107.14.2.1--General.** The Contractor shall carry Contractor's liability, including subcontractors and contractual, with limits not less than: \$500,000 each occurrence; \$1,000,000 aggregate; automobile liability - \$500,000 combined single limit - each accident; Workers' Compensation and Employers' Liability - Statutory & \$100,000 each accident; \$100,000 each employee; \$500,000 policy limit. **Each policy shall be signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent of the Insurance Company.**

The Contractor shall have certificates furnished to the Department from the insurance companies providing the required coverage. The certificates shall be on the form furnished by the Department and will show the types and limits of coverage.

**907-107.14.2.2--Railroad Protective.** The following provisions are applicable to all work performed under a contract on, over or under the rights-of-way of each railroad shown on the plans.

The Contractor shall assume all liability for any and all damages to work, employees, servants, equipment and materials caused by railroad traffic.

Prior to starting any work on railroad property, the Contractor shall furnish satisfactory evidence to the Department that insurance of the forms and amounts set out herein in paragraphs (a) and (b) has been obtained. Also, the Contractor shall furnish similar evidence to the Railroad Company that insurance has been obtained in accordance with the Standard Provisions for General Liability Policies and the Railroad Protective Liability Form as published in the Code of Federal Regulations, 23 CFR 646, Subpart A. Evidence to the Railroad Company shall be in the form of a Certificate of Insurance for coverages required in paragraph (b), and the original policy of the Railroad Protective Liability Insurance for coverage required in paragraph (a).

All insurance herein specified shall be carried until the contract is satisfactorily complete as evidenced by a release of maintenance from the Department.

The Railroad Company shall be given at least 30 days notice prior to cancellation of the Railroad Protective Liability Insurance policy.

For work within the limits set out in Subsection 107.18 and this subsection, the Contractor shall provide insurance for bodily injury liability, property damage liability and physical damage to property with coverages and limits no less than shown in paragraphs (a) and (b). Bodily injury shall mean bodily injury, sickness, or disease, including death at anytime resulting therefrom. Property damage shall mean damages because of physical injury to or destruction of property, including loss of use of any property due to such injury or destruction. Physical damage shall mean direct and accidental loss of or damage to rolling stock and their contents, mechanical construction equipment or motive power equipment.

(a) **Railroad Protective Liability Insurance** shall be purchased on behalf of the Railroad Company with limits of \$2,000,000 each occurrence; \$6,000,000 aggregate applying separately to each annual period for lines without passenger trains. If the line carries passenger train(s), railroad protective liability insurance shall be purchased on behalf of the Railroad Company with limits of \$5,000,000 each occurrence; \$10,000,000 aggregate applying separately to each annual period.

Coverage shall be limited to damage suffered by the railroad on account of occurrences arising out of the work of the Contractor on or about the railroad right-of-way, independent of the railroad's general supervision or control, except as noted in paragraph 4 below.

Coverage shall include:

- (1) death of or bodily injury to passengers of the railroad and employees of the railroad not covered by State workmen's compensation laws,
- (2) personal property owned by or in the care, custody or control of the railroads,
- (3) the Contractor, or any of the Contractor's agents or employees who suffer bodily injury or death as a result of acts of the railroad or its agents, regardless of the negligence of the railroads, and
- (4) negligence of only the following classes of railroad employees:
  - (i) any supervisory employee of the railroad at the job site
  - (ii) any employee of the railroad while operating, attached to, or engaged on, work trains or other railroad equipment at the job site which are assigned exclusively to the Contractor, or
  - (iii) any employee of the railroad not within (i) or (ii) above who is specifically loaned or assigned to the work of the Contractor for prevention of accidents or protection of property, the cost of whose services is borne specifically by the Contractor or Governmental authority.

(b) **Regular Contractor's Liability**, including subcontractors, XCU and railroad contractual with limits of \$1,000,000 each occurrence; \$2,000,000 aggregate. **Automobile** with limits of \$1,000,000 combined single limit any one accident; **Workers' Compensation and Employer's Liability** - statutory and \$100,000 each accident; \$100,000 each employee; \$500,000 policy limit. **Excess/Umbrella Liability** \$5,000,000 each occurrence; \$5,000,000 aggregate. All coverage to be issued in the name of the Contractor shall be so written as to furnish protection to the Contractor respecting the Contractor's operations in performing work covered by the contract. Coverage shall include protection from damages arising out of bodily injury or death and damage or destruction of property which may be suffered by persons other than the Contractor's own employees.

In addition, the Contractor shall provide for and on behalf of each subcontractor by means of a separate and individual liability and property damage policy to cover like liability imposed upon the subcontractor as a result of the subcontractor's operations in the same amounts as contained above; or, in the alternative each subcontractor shall provide same.

**907-107.15--Third Party Beneficiary Clause.** In the first sentence of the first paragraph of Subsection 107.15 on page 61, change "create the public" to "create in the public".

**907-107.17--Contractor's Responsibility for Work.** Delete the fifth sentence of the fifth paragraph of Subsection 107.17 on page 63 and substitute the following:



The eligible permanent items shall be limited to traffic signal systems, changeable message signs, roadway signs and sign supports, lighting items, guard rail items, delineators, impact attenuators, median barriers, bridge railing or pavement markings. The eligible temporary items shall be limited to changeable message signs, guard rail items, or median barriers.

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

| SPECIAL PROVISION NO. 907-108-24

CODE: (SP)

| DATE: 03/15/2011

**SUBJECT: Prosecution and Progress**

Section 108, Prosecution and Progress, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

**907-108.01--Subletting of Contract.**

**907-108.01.1--General.** At the end of the last paragraph of Subsection 108.01.1 on page 73, add the following:

The Engineer will have the authority to suspend the work wholly or in part and to withhold payments because of the Contractor's failure to make prompt payment within 15 calendar days as required above, or failure to submit the required OCR-484 Form, Certification of Payments to Subcontractors, which is also designed to comply with prompt payment requirements.

**907-108.02--Notice To Proceed.** Delete the second paragraph of Subsection 108.02 on page 75 and substitute the following:

The anticipated date of the Notice to Proceed (NTP) / Beginning of Contract Time (BCT) will be specified in the proposal.

Delete the fourth paragraph of Subsection 108.02 on page 75 and substitute the following:

Upon written request from the Contractor and if circumstances permit, the Notice to Proceed may be issued at an earlier date subject to the conditions stated therein. The Contractor shall not be entitled to any monetary damages or extension of contract time for any delay claim or claim of inefficiency occurring between the early issuance Notice To Proceed date and the Notice to Proceed date stated in the contract.

**907-108.03--Prosecution and Progress.** Delete Subsection 108.03.1 on pages 75 & 76, and substitute the following:

**907-108.03.1--Progress Schedule.** Prior to or at the Pre-Construction Conference, the Contractor shall furnish a progress schedule and be prepared to discuss both its proposed methodologies for fulfilling the scheduling requirements and its sequence of operations. The Engineer will review the schedule and approve the schedule as it relates to compliance with the specifications and logic. The progress schedule must be approved by the Engineer prior to commencing work. The schedule shall be a bar-chart type schedule submitted on 11"x17" paper meeting the below minimum requirements. These activities shall be significantly detailed enough to communicate the Contractor's understanding of the construction sequencing and phasing of the project.

When preparing the progress schedule, the Contractor shall include the following:

- Show a time scale to graphically show the completion of the work within contract time.
- Define and relate activities to the contract pay items.
- Show all activities in the order the work is to be performed including submittals, submittal reviews, fabrication and delivery.
- Show all activities that are controlling factors in the completion of the work.
- Show the time needed to perform each activity and its relationship in time to other activities.

Should the schedule not include the above requirements or becomes unrealistic during construction, the Contractor should immediately submit a revised, more realistic schedule for approval.

**907-108.03.2--Preconstruction Conference.** Delete the first paragraph of Subsection 108.03.2 on page 76 and substitute the following:

Prior to commencement of the work, a preconstruction conference shall be held for the purpose of discussing with the Contractor essential matters pertaining to the prosecution and satisfactory completion of the work. The Contractor will be responsible for scheduling the preconstruction conference. The Contractor will advise the Project Engineer in writing 14 days prior to the requested date that a conference is requested. When the contract requires the Contractor to have a certified erosion control person, the Contractor's certified erosion control person shall be at the preconstruction conference. The Department will arrange for utility representatives and other affected parties to be present.

Delete the third paragraph of Subsection 108.03.2 on page 76.

**907-108.06--Determination and Extension of Contract Time.** Delete Subsections 108.06.1 and 108.06.2 on pages 79 thru 85 and substitute the following:

**907-108.06.1--Blank.**

**907-108.06.2--Based on Calendar Date Completion.**

**907-108.06.2.1--General.** Contract Time will be established on the basis of a Completion Date, as indicated in the contract. The span of time allowed for the completion of the work included in the contract will be indicated in the contract documents and will be known as "Contract Time".

The span of time allowed in the contract as awarded is based on the quantities used for comparison of bids. If satisfactory fulfillment of the contract requires performance of work in greater quantities than those set forth in the proposal, the time allowed for completion shall be increased in Calendar Days in the same ratio that the cost of such added work, exclusive of the cost of work altered by Supplemental Agreement for which a time adjustment is made for such altered work in the Supplemental Agreement, bears to the total value of the original contract unless it can be established that the extra work was of such character that it required more time

than is indicated by the money value.

The Contractor shall provide sufficient materials, equipment and labor to guarantee the completion of the work in the contract in accordance with the plans and specifications within the Contract Time.

**907-108.06.2.2--Contract Time.** The following TABLE OF ANTICIPATED PRODUCTIVE DAYS indicates an average/anticipated number of productive days per month.

**TABLE OF ANTICIPATED PRODUCTIVE DAYS**

Month	Available Productive Days
January	6
February	7
March	11
April	15
May	19
June	20
July	21
August	21
September	20
October	16
November	11
December	5
Calendar Year	172

Allocation of anticipated productive days for a fractional part of the month will be computed as a proportion of the listed anticipated productive days for the applicable month.

An available productive day will be assessed (a) any day of the week, Monday through Friday, exclusive of legal holidays recognized by the Department in Subsection 108.04.1, in which the Contractor works or could have worked for more than six (6) consecutive hours on the controlling items of work, as determined by the Engineer, or (b) any Saturday, exclusive of legal holidays recognized by the Department in Subsection 108.04.1, in which the Contractor works for more than six (6) consecutive hours on the controlling items of work, as determined by the Engineer. When the Contractor works less than four consecutive hours during the day, no time will be charged for that day. When the Contractor works more than four but less than six consecutive hours, one-half (0.5) of an available work day will be charged for that day. When he Contractor works six or more consecutive hours during the day, one (1.0) available work day will be charged for that day.



Should the weather or other conditions be such that four (4) consecutive satisfactory hours are not available prior to noon (for daytime operations) or midnight (for nighttime operations), no time will be assessed for that day regardless of the above conditions. However, if the Contractor elects to work, time will be assessed in accordance with the previous paragraph.

Weather delays will not be considered for Saturdays, Sundays or legal holidays recognized by the Department in Subsection 108.04.1.

Available productive days will be based on soil and weather conditions and other specific conditions cited in the contract. The Engineer will determine on each applicable day the extent to which work in progress could have been productive, regardless of whether the Contractor actually worked.

Each month the Engineer will complete, and furnish to the Contractor, an "Assessment Report for Available Productive Days" (CSD-765). This report shows the number of available productive days during the estimate period and the cumulative available productive days to date. The Contractor should review the Engineer's report as to the accuracy of the assessment and confer with the Resident or Project Engineer to rectify any differences. Each should make a record of the differences, if any, and conclusions reached. In the event mutual agreement cannot be reached, the Contractor will be allowed a maximum of 15 calendar days following the ending date of the monthly report in question to file a protest Notice of Claim in accordance with the provisions of Subsection 105.17. Otherwise, the Engineer's assessment shall be final unless mathematical errors of assessment are subsequently found to exist, and any claim of the Contractor as to such matter shall be waived.

At any given date, the ratio of the accumulated monetary value of that part of the work actually accomplished to the total contract bid amount adjusted to reflect approved increases or decreases shall determine the "percent complete" of the work.

The "percentage of elapsed time" shall be calculated as a direct ratio of the expired calendar days to the total calendar days between the Beginning of Contract Time and the Specified Completion Date in the contract.

When the "percent complete" lags more than 20 percent behind the "percentage of elapsed time", the Contractor shall immediately submit a written statement and revised progress schedule indicating any additional equipment, labor, materials, etc. to be assigned to the work to ensure completion within the specified contract time. When the "percent complete" lags more than 40 percent behind the "percentage of elapsed time", the contract may be terminated.

**907-108.06.2.3--Extension of Time.** The Contractor may, prior to the expiration of the Contract Time, make a written request to the Engineer for an extension of time with a valid justification for the request. The Contractor's plea that insufficient time was specified is not a valid reason for extension of time.

No extension of the specified completion date will be granted except as provided herein. An extension of contract time may be granted for unusually severe weather, abnormal delays caused

solely by the State or other governmental authorities, or unforeseeable disastrous phenomena of nature of the magnitude of earthquakes, hurricanes, tornadoes, or flooded essential work areas which are deemed to unavoidably prevent prosecuting the work.

Unusually severe weather is defined as when the actual available productive days for the contract time are less than the number of available productive days shown in the Table of Anticipated Productive Days.

Any extension of contract time will be based on a calendar days basis, excluding Saturdays, Sundays or legal holidays recognized by the Department in Subsection 108.04.1. No proration of contract time will be made. Any extension of contract time will be made on or after the specified completion date. No extension of contract time will be made on a monthly basis.

Any revision of the specified completion date provided in the contract will be made automatically on the specified completion date as established in the contract, and at a later date if additional conditions so warrant.

If the completion of the project is extended into a season of the year in which completion of certain items of work would be prohibited or delayed because of seasonal or temperature limitations, the Engineer may waive the limitations provided the completion of the work will not result in a reduction in quality. When determined that the completion of the out-of-season items will cause a reduction in the quality of the work, the completion of the project will be further extended so the items may be completed under favorable weather conditions. In either case, the Engineer will notify the Contractor in writing.

Liquidated damages as set forth in Subsection 108.07 under the heading "Daily Charge Per Calendar Day" in the Table titled "Schedule of Deductions for Each Day of Overrun in Contract Time", shall be applicable to each calendar day after the specified completion date, or authorized extension thereof, and until all work under the contract is completed.

**907-108.06.2.4--Cessation of Contract Time.** When the Engineer by written notice schedules a final inspection, time will be suspended until the final inspection is conducted and for an additional 14 calendar days thereafter. If after the end of the 14-day suspension all necessary items of work have not been completed, time charges will resume. If the specified completion date had not been reached at the time the Contractor called for a final inspection, the calendar day difference between the specified completion date and the date the Contractor called for a final inspection will be added after the 14-day period before starting liquidation damages. If a project is on liquidated damages at the time a final inspection is scheduled, liquidated damages will be suspended until the final inspection is conducted and for seven (7) calendar days thereafter. If after the end of the 7-day suspension all necessary items of work have not been completed, liquidated damages will resume. When final inspection has been made by the Engineer as prescribed in Subsection 105.16 and all items of work have been completed, the daily time charge will cease.

**907-108.10--Termination of Contractor's Responsibility.** In the last sentence of Subsection 108.10 on page 88, change "bond" to "performance and payment bond(s)".

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SPECIAL PROVISION NO. 907-109-5**

**CODE: (IS)**

**DATE: 1/20/2011**

**SUBJECT: Measurement and Payment**

Section 109, Measurement and Payment, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

**907-109.01--Measurement of Quantities.** Delete the third full paragraph of Subsection 109.01 on page 90 and substitute the following.

When requested by the Contractor, material specified to be measured by the cubic yard or ton may be converted to the other measure as appropriate. Factors for this conversion will be determined by the District Materials Engineer and agreed to by the Contractor. The conversion of the materials along with the conversion factor will be incorporated into the contract by supplemental agreement. The supplemental agreement must be executed before such method of measurement is used.

**907-109.04--Extra and Force Account Work.** In the last sentence of subparagraph (b) in Subsection 109.04 on page 91, change “bond” to “bond(s)”.

Delete the first sentence of the second paragraph of subparagraph (d) in Subsection 109.04 on page 92 and substitute the following:

In the event an agreement cannot be reached for a particular piece of equipment, the book entitled "Rental Rate Blue Book For Construction Equipment" as published by EquipmentWatch® and is current at the time the force account work is authorized will be used to determine equipment ownership and operating expense rates.

**907-109.06--Partial Payment.**

**907-109.06.1--General.** Delete the fourth and fifth sentences of the third paragraph of Subsection 109.06.1 on page 94, and substitute the following:

In the event mutual agreement cannot be reached, the Contractor will be allowed a maximum of 25 calendar days following the Contractor's receipt of the monthly estimate in question to file in writing, a protest Notice of Claim in accordance with the provisions Subsection 105.17. Otherwise, the Engineer's estimated quantities shall be considered acceptable pending any changes made during the checking of final quantities.

**907-109.07--Changes in Material Costs.** Delete the third full paragraph of Subsection 109.07 on page 96 and substitute the following:

A link to the established base prices for bituminous products and fuels will be included in the contract documents under a Notice to Bidders entitled "Petroleum Products Base Prices."



**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

| **SPECIAL PROVISION NO. 907-110-2**

**CODE: (SP)**

| **DATE: 04/02/2010**

**SUBJECT: Wage Rates**

Section 110, Required Contract Provisions, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

**907-110.02--Application.** Delete Subsection 110.02.2 on page 100 and substitute the following.

**907-110.02.2--Wage Rates.** All persons employed or working upon the site of the work will be paid at wage rates not less than those contained in the wage determination decision of the Secretary of Labor in effect 10 days prior to taking bids.

| **Bidders are advised that regardless of the wage rates listed in the Supplement to FHWA 1273 in the contract, minimum federal wage rates must be paid.**

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SPECIAL PROVISION NO. 907-304-12**

**CODE: (IS)**

**DATE: 06/01/2009**

**SUBJECT: Granular Courses**

Section 907-304, Granular Courses, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

**907-304.02--Materials.** After the first paragraph of Subsection 304.02.1 on page 183, add the following:

When the contract includes pay item 907-304-E, Granular Material, LVM, RAP, it shall be milled recycled asphalt pavement and shall be visually inspected by the Engineer to insure it is free from chunks and deleterious materials.

Crushed concrete meeting the requirements of Subsection 907-703.04.4 may be used in lieu of other crushed courses specified in the contract.

**907-304.03--Construction Requirements.**

**907-304.03.5--Shaping, Compacting and Finishing.** Delete the sixth paragraph of Subsection 304.03.5 on page 185.

Delete the first table in Subsection 304.03.5 on page 186 and substitute the following:

Granular Material <u>Class</u>	Lot <u>Average</u>	Individual <u>Test</u>
7,8,9 or 10	97.0	93.0
5 or 6	99.0	95.0
3 or 4	100.0	96.0
1 or 2	102.0	98.0
Crushed Courses*	99.0	95.0

\* When placed on filter fabric on untreated subgrade, the individual tests and the average of the five (5) tests shall equal or exceed the following values:

<u>Lot Average</u>	<u>Individual Test</u>
96.0	92.0

Before the last paragraph of Subsection 304.03.5 on page 186, add the following:

Unless otherwise specified, density for granular material, RAP, shall be achieved by two passes of an approved roller and density tests will not be required.

**907-304.05--Basis of Payment.** Add the “907” prefix to the pay items listed on page 187.

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

## SUPPLEMENT TO SPECIAL PROVISION NO. 907-401-2

**DATE:** 01/13/2011

**SUBJECT:** Hot Mix Asphalt (HMA)

Add the following before 907-401.02.6.2 on page 1.

**907-401.02.4--Substitution of Mixture.** Delete the table in Subsection 401.02.4 on page 242, and substitute the following:

Mixture	Single Lift Laying Thickness Inches	
	Minimum	Maximum
25 mm	3	4
19 mm	2 ¼	3 ½
12.5 mm	1 ½	2 ½
9.5 mm	1	1 ½
4.75 mm	½	¾

After Subsection 907-401-02.6.2 on page 2, add the following:

**907-401.02.6.4.1--Roadway Density.** Delete subparagraphs 1., 2., & 3. on page 251 and substitute the following:

1. For all leveling lifts, when full lane width and with a thickness as specified in the table in Subsection 401.02.4, the required lot density shall be 92.0 percent of maximum density.
2. For all single lift overlays, with or without leveling and/or milling, the required lot density shall be 92.0 percent of maximum density.
3. For all multiple lift overlays of two (2) or more lifts excluding leveling lifts, the required lot density of the bottom lift shall be 92.0 percent of maximum density. The required lot density for all subsequent lifts shall be 93.0 percent of maximum density.
4. For all pavements on new construction, the required lot density for all lifts shall be 93.0 percent of maximum density.

**907-401.03.1.2--Tack Coat.** Delete the three sentences of Subsection 401.03.1.2 on page 259, and substitute the following:

Tack coat shall be applied to previously placed HMA and between lifts, unless otherwise directed by the Engineer. Tack coat shall be applied with a distributor spray bar. A hand wand

will only be allowed for applying tack coat on ramp pads, irregular shoulder areas, median crossovers, turnouts, or other irregular areas. Bituminous materials and application rates for tack coat shall be as specified in Table 410-A on page 293. Construction requirements shall be in accordance with Subsection 407.03 of the Standard Specifications.

**907-401.03.1.4--Density.** Delete the first sentence of the first paragraph of Subsection 401.03.1.4 on page 259 and substitute the following:

The lot density for all dense graded pavement lifts, except as provided below for preleveling, wedging [less than fifty percent (50%) of width greater than minimum lift thickness], ramp pads, irregular shoulder areas, median crossovers, turnouts, or other areas where the established rolling pattern cannot be performed, shall not be less than the specified percent (92.0% or 93.0%) of the maximum density based on AASHTO Designation: T 209 for the day's production. For all leveling lifts, when full lane width and with a thickness as specified in the table in Subsection 401.02.4, the required lot density shall be 92.0 percent of maximum density.

**907-401.03.9--Material Transfer Equipment.** Delete the paragraph in Subsection 401.03.9 on page 264 and substitute the following:

Excluding the areas mentioned below, the material transferred from the hauling unit when placing the top lift, or the top two (2) lifts of a multi-lift HMA pavement with density requirements, shall be remixed prior to being placed in the paver hopper or insert by using an approved Materials Transfer Device. Information on approved devices can be obtained from the State Construction Engineer. Areas excluded from this requirement include: leveling courses, temporary work of short duration, detours, bridge replacement projects having less than 1,000 feet of pavement on each side of the structure, acceleration and deceleration lanes less than 1,000 feet in length, tapered sections, transition sections for width, shoulders less than 10 feet in width, crossovers, ramps, side street returns and other areas designated by the Engineer.

After Subsection 401.03.13 on page 266, add the following:

**907-401.03.14--Shoulder Wedge.** The Contractor shall attach a device to the screed of the paver that confines the material at the end gate and extrudes the asphalt material in such a way that results in a compacted wedge shape pavement edge of approximately 30 degrees, but not steeper than 35 degrees. The device shall maintain contact between itself and the road shoulder surface and allow for automatic transition to cross roads, driveways, and obstructions. The device shall be used to constrain the asphalt head reducing the area by 10% to 15% increasing the density of the extruded profile. Conventional single plate strike off shall not be used.

The device shall be TransTech Shoulder Wedge Maker, the Advant-Edge, or a similar approved equal device that produces the same wedge consolidation results. Contact information for these wedge shape compaction devices is the following:

1. TransTech Systems, Inc.  
1594 State Street  
Schenectady, NY 12304  
800-724-6306  
[www.transtechsys.com](http://www.transtechsys.com)



2. Advant-Edge Paving Equipment, LLC  
P.O. Box 9163  
Niskayuna, NY 12309-0163  
518-280-6090  
Contact; Gary D. Antonelli  
Cell: 518-368-5699  
email: [garya@nycap.rr.com](mailto:garya@nycap.rr.com)  
Website: [www.advantedgepaving.com](http://www.advantedgepaving.com)

Before using a similar device, the Contractor shall provide proof that the device has been used on previous projects with acceptable results, or construct a test section prior to the beginning of work and demonstrate wedge compaction to the satisfaction of the Engineer. Short sections of handwork will be allowed when necessary for transitions and turnouts, or otherwise authorized by the Engineer.

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SPECIAL PROVISION NO. 907-401-2**

**CODE: (IS)**

**DATE: 11/04/2005**

**SUBJECT: Hot Mix Asphalt (HMA)**

Section 401, Hot Mix Asphalt (HMA) - General, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

Delete in toto Subsection 401.02.6.2 on pages 248 and 249, and substitute:

**907-401.02.6.2--Assurance Program for Mixture Quality.** The Engineer will conduct a quality assurance program. The quality assurance program will be accomplished as follows:

- 1) Conducting verification tests.
- 2) Validate Contractor test results.
- 3) Periodically observing Contractor quality control sampling and testing.
- 4) Monitoring required quality control charts and test results.
- 5) Sampling and testing materials at any time and at any point in the production or laydown process.

The rounding of all test results will be in accordance with Subsection 700.04.

The Engineer will conduct verification tests on samples taken by the Contractor under the direct supervision of the Engineer at a time specified by the Engineer. The frequency will be equal to or greater than ten percent (10%) of the tests required for Contractor quality control and the data will be provided to the Contractor within two asphalt mixture production days after the sample has been obtained by the Engineer. At least one sample shall be tested from the first two days of production. All testing and data analysis shall be performed by a Certified Asphalt Technician-I (CAT-I) or by an assistant under the direct supervision of the CAT-I. Certification shall be in accordance with the *MDOT HMA Technician Certification Program* chapter in the Materials Division Inspection, Testing, and Certification Manual. The Department shall post a chart giving the names and telephone numbers for the personnel responsible for the assurance program.

The Engineer shall be allowed to inspect Contractor testing equipment and equipment calibration records to confirm both calibration and condition. The Contractor shall calibrate and correlate all testing equipment in accordance with the latest versions of the Department's Test Methods and AASHTO Designation: R 18.

Random differences between the Engineer's verification tests and the current running average of four quality control tests at the time of obtaining the verification sample will be considered acceptable if within the following limits:

Item	Allowable Differences
Sieve - % Passing	
3/8-inch and above	6.0
No. 4	5.0
No. 8	4.0
No. 16, for 4.75 mm mixtures ONLY	3.5
No. 30	3.5
No. 200	2.0
AC Content	0.4
Specimen Bulk SG, Gmb @ N <sub>Design</sub>	0.030
Maximum SG, Gmm	0.020

If four quality control tests have not been tested prior to the time of the first verification test, the verification test results will be compared to the average of the preceding quality control tests. If the verification test is the first material tested on the project or if a significant process adjustment was made just prior to the verification test, the verification test results will be compared to the average of four subsequent quality control test results. For all other cases after a significant process adjustment, the verification test results will be compared to the average of the preceding quality control tests (taken after the adjustment) as in the case of a new project start-up when four quality control tests are not available.

In the event that; 1) the comparison of the Contractor’s running average quality control data and Engineer’s quality assurance verification test results are outside the allowable differences in the above table, or 2) if a bias exists between the results, such that one of the results is predominately higher or lower than the other, and the Engineer’s results fail to meet the JMF control limits, the Engineer will investigate the reason immediately. As soon as the need for an investigation becomes known, the Engineer will increase the quality assurance sampling rate to the same frequency required for Contractor testing. The additional samples obtained by the Engineer may be used as part of the investigation process or for routine quality assurance verification tests. The Engineer's investigation may include testing of the remaining quality control split samples, review and observation of the Contractor's testing procedures and equipment, and a comparison of split sample test results by the Contractor quality control laboratory, Department quality assurance laboratory and the Materials Division laboratory. The procedures outlined in the latest edition of MDOT’s Field Manual for HMA may be used as a guide for the investigation. In the event that the Contractor’s results are determined to be incorrect, the Engineer's results will be used for the quality control data and the appropriate payment for the mixture will be based on the procedures specified in Subsection 401.02.5.8(j).

The Engineer will periodically witness the sampling and testing being performed by the Contractor. The Engineer, both verbally and in writing, will promptly notify the Contractor of any observed deficiencies. When differences exist between the Contractor and the Engineer which cannot be resolved, a decision will be made by the State Materials Engineer, acting as the referee. The Contractor will be promptly notified in writing of the decision. If the deficiencies are not corrected, the Engineer will stop production until corrective action is taken.

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

## SUPPLEMENT TO SPECIAL PROVISION NO. 907-401-4

**DATE:** 10/05/2010

**SUBJECT:** Warm Mix Asphalt

Delete Subsection 907-401.03.8 on page 2 and substitute the following:

**907-401.03.8--Preparation of Mixture.** After the sentence in Subsection 401.03.8 on page 264, add the following:

Warm mix asphalt is defined as a plant produced asphalt mixture that can be produced and constructed at lower temperatures than typical hot mix asphalt. Typical temperature ranges of non-polymer modified, WMA produced by foaming the asphalt binder at the plant are typically 270°F to 295°F at the point of discharge of the plant. Typical temperature ranges of polymer modified, WMA produced by foaming the asphalt binder at the plant are typically 280°F to 305°F at the point of discharge of the plant. WMA produced by addition of a terminal blended additive may allow the producer to reduce the temperatures below 270°F as long as all mixture quality and field density requirements are met. Production temperatures at the plant may need to be increased or decreased due to factors such as material characteristics, environmental conditions, and haul time to achieve mixture temperatures at the time of compaction in which uniform mat density can be achieved.



# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SPECIAL PROVISION NO. 907-401-4**

**CODE: (SP)**

**DATE: 03/22/2010**

**SUBJECT: Warm Mix Asphalt (WMA)**

Section 401, Hot Mix Asphalt (HMA) - General, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction as amended by this special provision is applicable to Warm Mix Asphalt Only.

**907-401.01--Description.** Delete the first and second paragraphs of Subsection 401.01 on page 236, and substitute the following:

These specifications include general requirements for all types of WMA.

This work consists of the construction of one or more lifts of WMA in accordance with these specifications and the specific requirements for the mixture to be produced and in reasonably close conformity with the lines, grades, thicknesses and typical sections shown on the plans or established by the Engineer.

**907-401.02--Materials.** Delete Subsection 401.02.2 on page 239, and substitute the following:

**907-401.02.2--WMA Products and Processes.** The Department will maintain a list of qualified WMA products and processes. No product or process shall be used unless it appears on this list.

The Contractor may propose other products or processes for approval by the Product Evaluation Committee. Documentation shall be provided to demonstrate laboratory performance, field performance, and construction experience.

**907-401.03--Construction Requirements.**

**907-401.03.1.1--Weather Limitations.** Delete the second sentence of the first paragraph and the Temperature Limitation Table in Subsection 401.03.1.1 on page 258, and substitute the following:

The air and pavement temperature at the time of placement shall equal or exceed 40°F, regardless of compacted lift thickness.

**907-401.03.1.2--Tack Coat.** Delete the first sentence of the first paragraph of Subsection 401.03.1.2 on page 259 and substitute the following:

Tack coat shall be applied to previously placed WMA and between lifts, unless otherwise directed by the Engineer.

**907-401.03.8--Preparation of Mixture.** Delete the sentence in Subsection 401.03.8 on page 264, and substitute the following:

The temperature of the WMA mixture, when discharged from the mixer, shall not exceed 280° F.

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

## SUPPLEMENT TO SPECIAL PROVISION NO. 907-403-4

**DATE:** 08/03/2010

**SUBJECT:** Hot Mix Asphalt (HMA)

Before Subsection 907-403.05.2 on page 1, add the following:

Delete the last paragraph of Subsection 403.03.2 at the bottom of page 268, and the table at the top of page 269 and substitute the following:

Regardless of the Surface Profile Index requirement, when the Profile Index for the final surface lift is less than or equal to twenty-two inches per mile (22.0 inches / mile) per segment, a unit price increase will be added. The following schedule lists the Profile Index range and the corresponding contract price adjustment:

Profile Index inches / mile / segment	Contract Price Adjustment percent of unit bid price
less than 10.0	108
10.0 to 14.0	106
14.1 to 18.0	104
18.1 to 22.0	102
22.1 to Required P.I.	100
over Required P.I.	100 (with correction to Required P.I.)

Delete the first full paragraph of Subsection 403.03.2 on page 269 and substitute the following:

Contract price adjustments for rideability shall only be applicable to the surface lift and furthermore to only the segment(s) or portions of the segments(s) of the surface lift that require smoothness be determined by using a profilograph.

Delete Subsection 403.03.5.5 on page 273 and substitute the following:

**907-403.03.5.5--Preliminary Leveling.** All irregularities of the existing pavement, such as ruts, cross-slope deficiencies, etc., shall be corrected by spot leveling, skin patching, feather edging or a wedge lift in advance of placing the first overall lift.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SPECIAL PROVISION NO. 907-403-4**

**CODE: (IS)**

**DATE: 11/04/2005**

**SUBJECT: Hot Mix Asphalt (HMA)**

Section 403, Hot Bituminous Pavement, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

**907-403.05.2--Pay Items.** Add the "907" prefix to the pay items listed on page 275 & 276.



**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SUPPLEMENT TO SPECIAL PROVISION NO. 907-403-9**

**DATE: 08/03/2010**

**SUBJECT: Warm Mix Asphalt (WMA)**

After Subsection 907-403.01 on page 1, add the following:

**907-403.03.2--Smoothness Tolerances.** Delete the last paragraph of Subsection 403.03.2 at the bottom of page 268, and the table at the top of page 269 and substitute the following:

Regardless of the Surface Profile Index requirement, when the Profile Index for the final surface lift is less than or equal to twenty-two inches per mile (22.0 inches / mile) per segment, a unit price increase will be added. The following schedule lists the Profile Index range and the corresponding contract price adjustment:

Profile Index inches / mile / segment	Contract Price Adjustment percent of unit bid price
less than 10.0	108
10.0 to 14.0	106
14.1 to 18.0	104
18.1 to 22.0	102
22.1 to Required P.I.	100
over Required P.I.	100 (with correction to Required P.I.)

Delete the first full paragraph of Subsection 403.03.2 on page 269 and substitute the following:

Contract price adjustments for rideability shall only be applicable to the surface lift and furthermore to only the segment(s) or portions of the segments(s) of the surface lift that require smoothness be determined by using a profilograph.

Delete Subsection 403.03.5.5 on page 273 and substitute the following:

**907-403.03.5.5--Preliminary Leveling.** All irregularities of the existing pavement, such as ruts, cross-slope deficiencies, etc., shall be corrected by spot leveling, skin patching, feather edging or a wedge lift in advance of placing the first overall lift.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SPECIAL PROVISION NO. 907-403-9**

**CODE: (SP)**

**DATE: 03/15/2010**

**SUBJECT: Warm Mix Asphalt (WMA)**

Section 403, Hot Bituminous Pavement, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction as amended by this special provision is applicable to Warm Mix Asphalt Only.

**907-403.01--Description.** Delete the first sentence of Subsection 403.01 on page 266, and substitute the following:

This work consists of constructing one or more lifts of WMA pavement meeting the requirements of Section 401 on a prepared surface in accordance with the requirements of this section and in reasonably close conformity with the lines, grade, thicknesses, and typical cross sections shown on the plans or established by the Engineer.

**907-403.05--Basis of Payment.**

**907-403.05.2--Pay Items.** After the last pay item listed on page 276, add the following:

907-403-M: Warm Mix Asphalt,  $\frac{(1)}{\text{Type}}, \frac{(2)}{\text{Mixture}}$  - per ton

907-403-N: Warm Mix Asphalt,  $\frac{(1)}{\text{Type}}, \frac{(3)}{\text{Mixture}}$ , Leveling - per ton

907-403-O: Warm Mix Asphalt,  $\frac{(1)}{\text{Type}}, \frac{(4)}{\text{Mixture}}$ , Trench Widening - per ton

907-403-P: Warm Mix Asphalt, HT,  $\frac{(3)}{\text{Mixture}}$ , Polymer Modified - per ton

907-403-Q: Warm Mix Asphalt, HT,  $\frac{(3)}{\text{Mixture}}$ , Polymer Modified, Leveling - per ton

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SPECIAL PROVISION NO. 907-407-1**

**CODE: (SP)**

**DATE: 02/26/2008**

**SUBJECT: Tack Coat**

Section 407, Tack Coat, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

**907-407.02.1--Bituminous Material.** Delete the second sentence of the first paragraph of Subsection 407.02.1 on page 281, and substitute the following:

When not specified, the materials shall be as specified in Table 410-A on page 293.

**907-407.03.3--Application of Bituminous Material.** Delete the first paragraph of Subsection 407.03.3 on page 281, and substitute the following.

Tack coat shall be applied with a distributor spray bar. A hand wand will only be allowed for applying tack coat on ramp pads, irregular shoulder areas, median crossovers, turnouts, or other irregular areas. Bituminous materials and application rates for tack coat shall be as specified in Table 410-A on page 293. Tack coat shall not be applied during wet or cold weather, after sunset, or to a wet surface. Emulsions shall be allowed to "break" prior to superimposed construction.

**907-407.05--Basis of Payment.** Delete the pay item at the end of Subsection 407.05 on page 282, and substitute the following:

907-407-A: Asphalt for Tack Coat \* - per gallon

\* Grade may be specified

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-413-2

CODE: (SP)

DATE: 05/09/2008

**SUBJECT: Sawing and Sealing Transverse Joints in Asphalt Pavement**

Section 413, Cleaning and Sealing Joints and Cracks, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

**907-413.03--Construction Requirements.**

**907-413.03.3-- Sawing and Sealing Transverse Joints in Asphalt Pavement.**

**907-413.03.3.1--General.** Delete the first paragraph of Subsection 413.03.3.1 on page 296 and substitute the following:

The Contractor's operation shall be conducted so that sawcutting of transverse joints, cleaning, and sealing is a continuous operation. The entire sawing and sealing operation shall be completed within seven (7) days after the placement of the final wearing course, unless the approved traffic control plan or sequence of operations provide otherwise. Traffic shall not be allowed on sawed unsealed joints in the final wearing course.

When intermediate lifts must be exposed to traffic for over seven (7) days, the Contractor shall be required to make an interim 1/8-inch wide saw cut which is one third (1/3) as deep as the hot mix asphalt layer. This interim saw cut does not require sealing. Costs of this interim cut(s) is to be absorbed the pay item for sawing and sealing transverse joints in asphalt pavement.

**907-413.05--Basis of Payment.** Add the "907" prefix to pay item no. 413-E on page 298.



# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

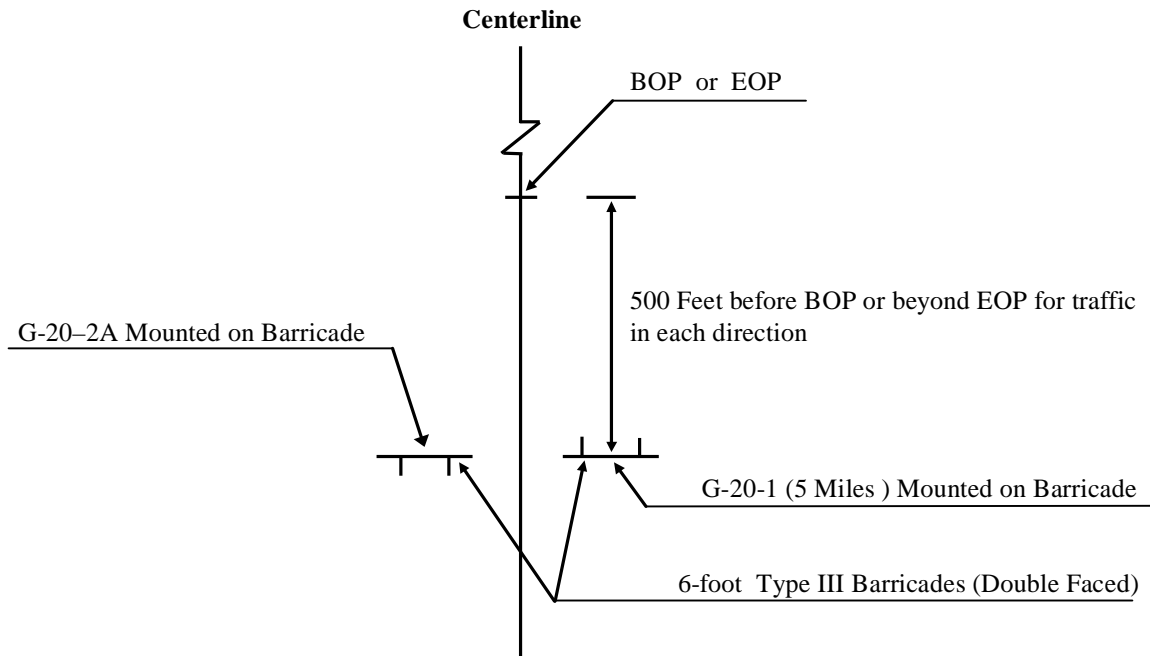
## SUPPLEMENT TO SPECIAL PROVISION NO. 907-618-1

**DATE:** 6/20/2011

**PROJECT:** STP-2170-00(023) / 106222301 – Forrest County

After the first paragraph of Subsection 907-618.01.2 on page 1, add the following:

Additional signs will be required as follows:



### ADDITIONAL TRAFFIC CONTROL SIGNS REQUIRED:

- 25 - W20-1 (AHEAD) signs required. One (1) W20-1 (AHEAD) sign is required at each local road or street entering the project.
- 42 - R4 -1 "DO NOT PASS" signs required.
- 26 - R4 -2 "PASS WITH CARE" signs required.
- 25 - W14 -3 "NO PASSING ZONE" signs required.
- 4 - R16-3 (SPEEDING FINES DOUBLED) signs required.

R4 -1 " DO NOT PASS ", R4 -2 " PASS WITH CARE", and W14 -3 "NO PASSING ZONE" signs are required in accordance with Subsection 618.03.3, attached drawing, and as specified in the Manual on Uniform Traffic Control Devices. If no passing zones are 1,000 feet or more, install additional "DO NOT PASS" signs per attached drawing.

All Construction signing is included in the bid for Pay Item 618-A, Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except for R4-1 and R4-2 signs which shall be black legend and border on white background.

Two portable R16-3 signs are required per work zone or lane closure in addition to signs required by MUTCD.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SPECIAL PROVISION NO. 907-618-1**

**CODE: (SP)**

**DATE: 04/29/2004**

**SUBJECT: Additional Signing Requirements**

Section 618, Maintenance of Traffic and Traffic Control Plan, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

**907-618.01.2--Traffic Control Plan.** At the end of Subsection 618.01.2 on page 413, add the following:

For compliance with the traffic control plan, the Contractor will be required to install and maintain construction signs at various location throughout the project. Payment for these signs will be included in the price bid for pay item no. 618-A, Maintenance of Traffic per lump sum.

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SPECIAL PROVISION NO. 907-618-4**

**CODE: (SP)**

**DATE: 12/12/2006**

**SUBJECT: Placement of Temporary Traffic Stripe**

Section 618, Maintenance of Traffic and Traffic Control Plan, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

**907-618.03.3--Safe Movement of Traffic.** Delete subparagraphs (2) and (3) of Subsection 618.03.3 on pages 415 & 416, and substitute the following:

- (2) Temporary edge lines on projects requiring shoulders constructed of granular material may be delayed for a period not to exceed three (3) days.

Temporary edge lines placed on the final pavement course of projects requiring paved shoulders with surface treatment may be placed on the adjacent shoulder in as near the permanent location as possible until the surface treatment is placed. When the edge lines are obliterated by the placement of the surface treatment, the edge lines shall be placed in the permanent stripe location. The replacement of edge lines may be delayed for a period not to exceed three (3) days for a two or three-lane roads.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SPECIAL PROVISION NO. 907-626-15**

**CODE: (IS)**

**DATE: 03/17/2008**

**SUBJECT: Thermoplastic Traffic Markings**

Section 626, Thermoplastic Traffic Markings, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

**907-626.05--Basis of Payment.** Add the “907” prefix to the pay items listed on page 446.



**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SPECIAL PROVISION NO. 907-626-22**

**CODE: (SP)**

**DATE: 04/06/2010**

**SUBJECT: Double Drop Thermoplastic Markings**

Section 626, Thermoplastic Traffic Markings, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

**907-626.03.1.1--Equipment.** After the second paragraph of Subsection 626.03.1.1 on page 444, add the following:

When edge lines are placed over rumble strips, the equipment must be able to apply the markings using the atomization method instead of extrusion / ribbon method.

**907-626.03.1.2--Construction Details.** After the second sentence of the first full paragraph of Subsection 626.03.1.2 on page 445 add the following:

When edge lines are placed on rumble strips, the thickness of the edge line shall be 90 mils.

After the last sentence of the third full paragraph of Subsection 626.03.1.2 on page 445, add the following:

When double drop thermoplastic stripe is called for in the contract, additional beads by the drop-on method shall be applied as follows:

Class A glass beads at a rate of not less than three pounds of beads per 100 feet of six-inch stripe.  
Class B glass beads at a rate of not less than three pounds of beads per 100 feet of six-inch stripe.

The Class B glass beads shall be applied to the newly placed stripe first, followed by the application of the Class A glass beads.

**907-626.05--Basis of Payment.** Add the following to the list of pay items on page 446.

- 907-626-A: 6" Thermoplastic Double Drop Traffic Stripe,  
Skip White \* - per linear foot or mile
- 907-626-B: 6" Thermoplastic Double Drop Traffic Stripe,  
Continuous White \* - per linear foot or mile
- 907-626-C: 6" Thermoplastic Double Drop Edge Stripe,  
Continuous White \* - per linear foot or mile

907-626-D: 6" Thermoplastic Double Drop Traffic Stripe, Skip Yellow	- per linear foot or mile
907-626-E: 6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow	- per linear foot or mile
907-626-F: 6" Thermoplastic Double Drop Edge Stripe, Continuous Yellow, *	- per linear foot or mile
907-626-G: Thermoplastic Double Drop Detail Stripe, <u>Color</u>	- per linear foot
907-626-H: Thermoplastic Double Drop Legend, White	- per linear foot or square foot

\* Thickness may be specified

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SUPPLEMENT TO SPECIAL PROVISION NO. 907-703-9**

**DATE: 03/01/2011**

**SUBJECT: Stabilizer Aggregate**

After the last paragraph on page 3, add the following:

**907-703.20.3--Gradation.** Delete the table and notes in Subsection 703.20.3 at the top of page 626, and substitute the following

**PERCENT PASSING BY WEIGHT**

Square Mesh Sieves	Shell	Coarse			Medium	Fine
		Size I	Size II Note (1)	Size III Note (3)		
3 inch	90-100			100		
2 1/2 inch				90-100		
2 inch		100				
1 1/2 inch		90-100	100	25-60		
1 inch		80-100	97-100			
3/4 inch		55-100	55-100	0-10		
1/2 inch		35-85	35-85	0-5	100	
3/8 inch		12-65	12-65		97-100	
No. 4, Note (2)		0-30	0-30		92-100	
No. 10		0-8	0-8		80-100	100
No. 40					10-40	80-100
No. 60					0-20	30-100
No. 100						15-80
No. 200		0-5	0-4	0-4	0-5	0-30
PI Material Passing No. 40				6 or less	0	

Note (1): Size II is intended for use in bases in which portland cement is used.

Note (2): Ground shell shall contain at least 97% passing the No. 4 sieve.

Note (3): Size III is intended for use in stabilized construction entrances.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-703-9

CODE: (IS)

DATE: 11/09/2010

SUBJECT: Aggregates

Section 703, Aggregates, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

**907-703.03.2.4--Gradation.** Delete the last sentence of the last paragraph of Subsection 703.03.2.4 on page 611.

**907-703.04--Aggregate for Crushed Stone Courses.**

**907-703.04.1--Coarse Aggregate.** Delete the first paragraph of Subsection 703.04.1 on page 611, and substitute the following:

Coarse aggregate, defined as material retained on No. 8 sieve, shall be either crushed stone, slag, granite, shell, concrete, or combination thereof.

**907-703.04.2--Fine Aggregate.** Delete the first sentence of the first paragraph of Subsection 703..04.2 on page 612, and substitute the following:

Fine aggregate, defined as material passing no. 8 sieve, shall be material resulting from the crushing of stone, slag, concrete, or combination thereof.

**907-703.04.3--Gradation.** Add the following to the "TABLE OF SIZES AND GRADATION OF CRUSHED STONE AGGREGATE" in Subsection 703.04.3 on page 613.



Sieve Size	Percent Passing By Weight	
	Size No. 825	Crushed Stone
2 inch	100	
1 1/2 inch	90 - 100	100
1 inch	75 - 98	90 - 100
3/4 inch		
1/2 inch	60 - 85	62 - 90
3/8 inch		
No. 4	40 - 65	30 - 65
No. 8	28 - 54	
No. 10		15 - 40
No. 16	19 - 42	
No. 40		
No. 50	9 - 27	
No. 200	4 - 18	3 - 16

After the "TABLE OF SIZES AND GRADATION OF CRUSHED STONE AGGREGATE" in Subsection 703.04.3 on page 613, add the following:

**907-703.04.4--Crushed Concrete.** Crushed reclaimed concrete shall also be allowed as a crushed aggregate course provided it meets the requirements of Subsection 703.04 and the following.

**Crushed Concrete**

Sieve Size	Percent Passing By Weight
2 inch	
1 1/2 inch	100
1 inch	90 - 100
3/4 inch	
1/2 inch	60 - 85
3/8 inch	
No. 4	40 - 65
No. 8	28 - 54
No. 10	
No. 16	19 - 42
No. 40	
No. 50	9 - 27
No. 200	2 - 18

**907-703.06--Aggregates for Hot Mix Asphalt.**

**907-703.06.1.2--Fine Aggregates.** Delete the last sentence of Subsection 703.06.1.2 on page 614.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SPECIAL PROVISION NO. 907-707-2**

**CODE: (IS)**

**DATE: 02/15/2006**

**SUBJECT: Joint Materials**

Section 707, Joint Materials, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

**907-707.02.1.3--Concrete Joint Sealer Compound - Hot-Poured Elastic Type.** In the first paragraph of Subsection 707.02.1.3 on page 633, delete "AASHTO Designation: M 173" and replace with "AASHTO Designation: M 324 for Type I Joint and Crack Sealant".

Delete in toto Subsection 707.02.1.5 on pages 634 and 635 and substitute:

**907-707.02.1.5--Backer Rod for Use with Hot and Cold Poured Joint Sealer.** The backer rod shall be a closed-cell foam rod made from polyethylene, polyolefin or similar type material, and shall conform to ASTM Designation: D 5249. The backer rod shall either be a Type 1, for use with either hot or cold poured joint sealers, or a Type 3, for use with cold poured joint sealers only.

The Contractor shall furnish a three linear foot sample of each shipment, and three copies of the manufacturer's certification that the backer rod meets the requirements of this specifications.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SPECIAL PROVISION NO. 907-720-1**

**CODE: (IS)**

**DATE: 3/17/2008**

**SUBJECT: Pavement Markings Materials**

Section 720, Pavement Marking Materials, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

**907-720.02--Thermoplastic Pavement Markings.** Delete the first paragraph of Subsection 720.02 on page 730 and substitute the following:

The thermoplastic material shall be lead free and conform to AASHTO Designation: M 249 except the glass beads shall be moisture resistant coated.

After the first sentence of the second paragraph of Subsection 720.02 on page 730, add the following:

In addition, the certification for the thermoplastic material shall state that the material is lead free.



### **SPECIAL PROVISION NO. 906-3**

#### **Training Special Provisions**

This Training Special Provision supersedes subparagraph 7b of the Special Provision entitled "Specific Equal Employment Opportunity Responsibilities," (Attachment 1), and is in implementation of 23 U.S.C. 140(a).

As part of the Contractor's equal employment opportunity affirmative action program training shall be provided as follows:

The Contractor shall provide on-the-job training aimed at developing full journeymen in the type of trade or job classification involved.

The number of trainees to be trained under this special provision will be as indicated in the bid schedule of the contract.

In the event that a Contractor subcontracts a portion of the contract work, he shall determine how many, if any, of the trainees are to be trained by the subcontractor, provided, however, that the Contractor shall retain the primary responsibility for meeting the training requirements imposed by this special provision. The Contractor shall also insure that this training special provision is made applicable to such subcontract. Where feasible, 25 percent of apprentices or trainees in each occupation shall be in their first year of apprenticeship or training.

The number of trainees shall be distributed among the work classifications on the basis of the Contractor's needs and the availability of journeymen in the various classifications within a reasonable area of recruitment. Prior to commencing construction, the Contractor shall submit to the State highway agency for approval the number of trainees to be trained in each selected classification and training program to be used. Furthermore, the Contractor shall specify the starting time for training in each of the classifications. The Contractor will be credited for each trainee employed by him on the contract work who is currently enrolled or becomes enrolled in an approved program and will be reimbursed for such trainees as provided hereinafter.

Training and upgrading of minorities and women toward journeymen status is a primary objective of this Training Special Provision. Accordingly, the Contractor shall make every effort to enroll minority trainees and women (e.g., by conducting systematic and direct recruitment through public and private sources likely to yield minority and women trainees) to the extent that such persons are available within a reasonable area of recruitment. The Contractor will be responsible for demonstrating the steps that he has taken in pursuance thereof, prior to a determination as to whether the Contractor is in compliance with this Training Special Provision. This training commitment is not intended, and shall not be used, to discriminate against any applicant for training, whether a member of a minority group or not.

No employee shall be employed as a trainee in any classification in which he has successfully completed a training course leading to journeyman status or in which he has been employed as a journeyman. The Contractor should satisfy this requirement by including appropriate questions in the employee application or by other suitable means. Regardless of the method used the Contractor's records should document the findings in each case.

The minimum length and type of training for each classification will be as established in the training program selected by the Contractor and approved by the State highway agency and the Federal Highway Administration. The State highway agency and the Federal Highway Administration shall approve a program if it is reasonably calculated to meet the equal employment opportunity obligations of the Contractor and to qualify the average trainee for journeyman status in the classification concerned by the end of the training period. Furthermore,

apprenticeship programs registered with the U.S. Department of Labor, Bureau of Apprenticeship and Training, or with a State apprenticeship agency recognized by the Bureau and training programs approved but not necessarily sponsored by the U.S. Department of Labor, Manpower Administration, Bureau of Apprenticeship and Training shall also be considered acceptable provided it is being administered in a manner consistent with the equal employment obligations of Federal-aid highway construction contracts. Approval or acceptance of a training program shall be obtained from the State prior to commencing work on the classification covered by the program. It is the intention of these provisions that training is to be provided in the construction crafts rather than clerk-typists or secretarial-type positions. Training is permissible in lower level management positions such as office engineers, estimators, timekeepers, etc., where the training is oriented toward construction applications. Training in the laborer classification may be permitted provided that significant and meaningful training is provided and approved by the division office. Some offsite training is permissible as long as the training is an integral part of an approved training program and does not comprise a significant part of the overall training.

Except as otherwise noted below, the Contractor will be reimbursed 80 cents per hour of training given an employee on this contract in accordance with an approved training program. As approved by the engineer, reimbursement will be made for training persons in excess of the number specified herein. This reimbursement will be made even though the Contractor receives additional training program funds from other sources, provided such other does not specifically prohibit the Contractor from receiving other reimbursement. Reimbursement for offsite training indicated above may only be made to the Contractor where he does one or more of the following and the trainees are concurrently employed on a Federal-aid project; contributes to the cost of the training, provides the instruction to the trainee or pays the trainee's wages during the offsite training period.

No payment shall be made to the Contractor if either the failure to provide the required training, or the failure to hire the trainee as a journeyman, is caused by the Contractor and evidences a lack of good faith on the part of the Contractor in meeting the requirements of this Training Special Provision. It is normally expected that a trainee will begin his training on the project as soon as feasible after start of work utilizing the skill involved and remain on the project as long as training opportunities exist in his work classification or until he has completed his training program. It is not required that all trainees be on board for the entire length of the contract. A Contractor will have fulfilled his responsibilities under this Training Special Provision if he has provided acceptable training to the number of trainees specified. The number trained shall be determined on the basis of the total number enrolled on the contract for a significant period.

Trainees will be paid at least 60 percent of the appropriate minimum journeyman's rate specified in the contract for the first half of the training period, 75 percent for the third quarter of the training period, and 90 percent for the last quarter of the training period, unless apprentices or trainees in an approved existing program are enrolled as trainees on this project. In that case, the appropriate rates approved by the Departments of Labor or Transportation in connection with the existing program shall apply to all trainees being trained for the same classification who are covered by this Training Special Provision.

The Contractor shall furnish the trainee a copy of the program he will follow in providing the training. The Contractor shall provide each trainee with a certification showing the type and length of training satisfactorily completed.

The Contractor will provide for the maintenance of records and furnish periodic reports documenting his performance under this Training Special Provision.

## **SPECIAL PROVISION NO. 906-6**

### **MISSISSIPPI DEPARTMENT OF TRANSPORTATION ON-THE-JOB TRAINING PROGRAM**

#### **ALTERNATE TRAINING SPECIAL PROVISION**

##### **PURPOSE**

The purpose of the On-The-Job Training (OJT) Program is to provide training for minority, female and economically disadvantaged individuals in order that they may develop marketable skills and gain journey status in the skilled craft classifications in which they are being trained.

##### **INTRODUCTION**

This voluntary OJT Program has been developed through the partnering efforts of the Road Builders of Mississippi, the Federal Highway Administration (FHWA) and the Mississippi Department of Transportation (MDOT).

The OJT Program has been designed for use by participating contractors and subcontractors in meeting their training needs. The objective of the OJT Program is to develop skilled workers in the skilled craft trade areas of highway construction who are sufficiently trained to be productive employees in the highway construction industry work force.

The success of the OJT Program will require that contractors and subcontractors take part in the program and follow uniform procedures in training and in tracking trainee's progress.

##### **FUNDING**

MDOT will establish an annual OJT Fund from which, contractors and subcontractors may bill the Department directly for hours worked by trainees. The funding source of this money will be state and federal funds for MDOT's OJT Program.

##### **DISBURSEMENT OF FUNDS**

MDOT will pay \$3.00 per hour toward the trainee's salary for each hour of training performed by each trainee in an approved training program. Program reimbursements will be made directly to the prime or sub contractor. Requests for payment will be submitted to the Office of Civil Rights for approval.

Contractors must provide a signed invoice providing the following information to be reimbursed.

- Contractor's Name
- Mailing Address
- Trainee Name
- Social Security Number

- Race
- Sex
- Project Number
- Job Classification
- Total Number of Hours Completed

### **TRAINING PROGRAM APPROVAL**

A. To use the OJT Program on highway construction projects, the contractor will notify the Department Office of Civil Rights using the On-the-Job Trainee Schedule Form. The notification must include the following information:

- Trainee Starting Date
- Project number (s) trainee starting on
- Training program (classification) to be used; and
- Number of Training Hours Required

B. If a contractor chooses to use a training program different from those listed in the OJT Program Manual, or desires to train in a different classification, the training program must be submitted in its entirety for approval by the Department and FHWA. The training proposal must include the following:

1. The primary objective of the program: To provide training for minority, female and economically disadvantaged individuals for development to full journey status in the work classifications in which they are being trained.
2. The minimum number of hours and type of training the trainee will receive as it relates to each specific task required to achieve journey status.
3. No less than minimum wage.
4. Trainee certification of completion.
5. Records and reports submitted to the Office of Civil Rights on a monthly basis.

### **DEPARTMENT RESPONSIBILITY**

1. Department project staff will monitor trainees on the project. They will monitor payrolls for payment of correct wage rates and fringe benefits. The Office of Civil Rights will maintain a master list by contractor name, project number, trainee name and trainee social security number to aid project staff in monitoring trainees who work on multiple projects.
2. The Office of Civil Rights may elect to interview trainees periodically during the training period to assess their performance and training program.



## **CONTRACTOR RESPONSIBILITY**

1. Trainees must be identified on payrolls (i.e. dragline trainee).
2. When any trainee completes a program, or is terminated for a reason or reasons other than successful completion, the contractor must include the date of completion or an explanation for the termination and date of termination on the OJT Termination Report.
3. The contractor will assign each trainee to a particular person--either a supervisor or a journeyman/woman who is proficient in the craft the trainee is being trained in, to ensure that timely instructional experience is received by the trainee. This person, cooperating with the appropriate company personnel, will see that proper records and the total intended training hours are completed during the allocated number of hours set up in the classification criteria.
4. The contractor has the prerogative of terminating the training period of the trainee and advancing the trainee to journey status. Approval requests must be submitted to the Office of Civil Rights with an explanation (*refer to 2 above*).
5. Upon notification from the contractor, the Department will issue a skill verification card and certificate of training to the trainee.
6. Trainees may be transferred to state-aid highway construction projects in order to complete the training program. If transfers are made the Office of Civil Rights must be notified on the Monthly Trainee Form. All of the training hours completed by trainees will count toward overall program completion.
7. Program reimbursements will be made directly to the prime or sub contractor.

## **WAGE RATE**

The wage rate for all trainees is [the current Minimum Federal Wage Rate](#), during their OJT training program. Trainees shall be paid full fringe benefit amounts, where applicable. At the completion of the training program, the trainee shall receive the wages of a skilled journey.

## **RECRUITMENT AND SELECTION PROCEDURES**

### A. Prerequisites for Trainees

To be qualified for enrollment in the OJT Program, trainees must possess basic physical fitness for the work to be performed, dependability, willingness to learn and ability to follow instructions.

### B. Licenses

Truck driver trainees must possess appropriate driver permits or licenses for the operation of Class A, B and C trucks. However, when an instructional permit is used in lieu of a license, the trainee must be accompanied by an operator who:

1. Holds a license corresponding to the vehicle being operated;
2. Has had at least one year of driving experience; and
3. Is occupying the seat next to the driver.

#### C. Recruitment

1. Notices and posters setting forth the contractor's Equal Employment Opportunity Policy and availability of training programs will be placed in areas readily accessible to employees, applicants for employment and potential employees.
2. The contractor must target minority, female or economically disadvantaged trainees.
3. The contractor will conduct systematic and direct recruitment through public and private employee referral sources. Contractors must submit the trainee's name and completed application form to the Office of Civil Rights for review and approval. Approval must be obtained before the trainee can begin work under the training program.
4. Present employees will be screened for upgrading.

#### D. Selection

1. The selection and employment of a person by participating contractor shall qualify the person for the OJT Program.
  2. Selection will be made without regard to race, color, religion, sex, age or national origin and shall be completely nondiscriminatory.
  3. Employment of trainees will be in accordance with the work force requirements of the contractor. Each contractor will hire and train the trainees for uses in their own organization.
  4. Written certification of individuals under the category of economically disadvantaged can be provided to the contractor at the time of the interview. This certification must then be provided to the Office of Civil Rights with the other required information as part of the approval process for trainees.
- **NOTE:** The OJT Program is to provide training for minority, female and economically disadvantaged individuals in order that they may develop marketable skills and gain journey status in the skilled craft classifications in which they are being trained. However, this program does not exclude trainees that are not members of the above groups.

# SECTION 905 - PROPOSAL

Date \_\_\_\_\_

Mississippi Transportation Commission  
Jackson, Mississippi

Sirs: The following proposal is made on behalf of \_\_\_\_\_  
\_\_\_\_\_ of \_\_\_\_\_  
\_\_\_\_\_

for constructing the following designated project(s) within the time(s) hereinafter specified.

The plans are composed of drawings and blue prints on file in the offices of the Mississippi Department of Transportation, Jackson, Mississippi.

The Specifications are the current Standard Specifications of the Mississippi Department of Transportation approved by the Federal Highway Administration, except where superseded or amended by the plans, Special Provisions and Notice(s) to Bidders attached hereto and made a part thereof.

I (We) certify that I (we) possess a copy of said Standard and Supplemental Specifications.

Evidence of my (our) authority to submit the Proposal is hereby furnished. The proposal is made without collusion on the part of any person, firm or corporation. I (We) certify that I (we) have carefully examined the Plans, the Specifications, including the Special Provisions and Notice(s) to Bidders, herein, and have personally examined the site of the work. On the basis of the Specifications, Special Provisions, Notice(s) to Bidders, and Plans, I (we) propose to furnish all necessary machinery, tools, apparatus and other means of construction and do all the work and furnish all the materials in the manner specified. I (We) understand that the quantities mentioned herein are approximate only and are subject to either increase or decrease, and hereby propose to perform any increased or decreased quantities of work at the unit prices bid, in accordance with the above.

Attached hereto is a certified check, cashier's check or Proposal Guaranty Bond in the amount as required in the Advertisement (or, by law).

INSTRUCTION TO BIDDERS: Alternate and Optional Items on Bid Schedule.

1. Two or more items entered opposite a single unit quantity WITHOUT DEFINITE DESIGNATION AS "ALTERNATE ITEMS" are considered as "OPTIONAL ITEMS". Bidders may or may not indicate on bids the Optional Item proposed to be furnished or performed WITHOUT PREJUDICE IN REGARD TO IRREGULARITY OF BIDS.
2. Items classified on the bid schedule as "ALTERNATE ITEMS" and/or "ALTERNATE TYPES OF CONSTRUCTION" must be preselected and indicated on bids. However, "Alternate Types of Construction" may include Optional Items to be treated as set out in Paragraph 1, above.
3. Optional items not preselected and indicated on the bid schedule MUST be designated in accordance with Subsection 102.06 prior to or at the time of execution of the contract.
4. Optional and Alternate items designated must be used throughout the project.

I (We) further propose to perform all "force account or extra work" that may be required of me (us) on the basis provided in the Specifications and to give such work my (our) personal attention in order to see that it is economically performed.

**SECTION 905 -- PROPOSAL (CONTINUED)**

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) enclose a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

Respectfully Submitted,

DATE \_\_\_\_\_

\_\_\_\_\_  
Contractor

BY \_\_\_\_\_  
Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

\_\_\_\_\_  
President Address

\_\_\_\_\_  
Secretary Address

\_\_\_\_\_  
Treasurer Address

The following is my (our) itemized proposal.

Mill and Overlay approximately 5 miles of US 11 from the Lamar County Line to Lincoln Road, known as Federal Aid Project No. STP-2170-00(023) / 106222301, in Forrest County.

I (We) agree to complete the entire project within the specified contract time.

**\*\*\* SPECIAL NOTICE TO BIDDERS \*\*\***

**BIDS WILL NOT BE CONSIDERED UNLESS BOTH UNIT PRICES AND ITEM TOTALS ARE ENTERED.  
 BIDS WILL NOT BE CONSIDERED UNLESS THE BID CERTIFICATION LOCATED AT THE END OF THE BID SHEETS IS SIGNED**

**\*\*\*BID SCHEDULE\*\*\***

Line No.	Item Code	Adj Code	Quantity	Units	Description	Unit Price		Item Amount	
						Dollar	Ct	Dollar	Ct
<b>Roadway Items</b>									
0010	202-B030		556	Square Yard	Removal of Concrete Pavement, All Depths				
0020	202-B076		13,000	Linear Feet	Removal of Traffic Stripe				
0030	202-B078		50	Square Yard	Removal of Pavement, All Types and Depths				
0040	406-A001		37,500	Square Yard	Cold Milling of Bituminous Pavement, All Depths				
0050	413-B001		3,760	Linear Feet	Cleaning and Sealing Joints				
0060	423-A001		12	Mile	Rumble Strips, Ground In				
0070	501-D001		175	Linear Feet	Expansion Joints, With Dowels				
0080	501-I001		75	Linear Feet	Joint Filler, Wooden Board				



Line No.	Item Code	Adj Code	Quantity	Units	Description	Unit Price		Bid Amount	
0090	503-A009	(C)	556	Square Yard	9" and Variable Reinforced Concrete Pavement, Broom Finish				
0100	503-B001		373	Linear Feet	Saw Cut, Longitudinal Joints				
0110	503-C006		155	Linear Feet	Saw Cut, 9-inch				
0120	503-E001		88	Each	Tie Bars, No. 4 Deformed Drilled and Epoxied or Grouted				
0130	503-E002		183	Each	Tie Bars, No. 5 Deformed Drilled and Epoxied or Grouted				
0140	503-F001		55	Each	1" Smooth Dowel Bars, Drilled & Epoxied or Grouted				
0150	606-E003		4	Each	Guard Rail, Terminal End Section, Non-Flared				
0160	609-D003	(S)	110	Linear Feet	Combination Concrete Curb and Gutter Type 3				
0170	618-A001		1	Lump Sum	Maintenance of Traffic	XXXXXXXX	XXX		
0180	618-B001		1	Square Feet	Additional Construction Signs	10.	00	10.	00
0190	619-A1002		12	Mile	Temporary Traffic Stripe, Continuous White				
0200	619-A2002		12	Mile	Temporary Traffic Stripe, Continuous Yellow				

Line No.	Item Code	Adj Code	Quantity	Units	Description	Unit Price		Bid Amount	
0210	619-A3006		15	Mile	Temporary Traffic Stripe, Skip White				
0220	619-A4006		4	Mile	Temporary Traffic Stripe, Skip Yellow				
0230	619-A6001		2,200	Linear Feet	Temporary Traffic Stripe, Legend				
0240	627-J001		350	Each	Two-Way Clear Reflective High Performance Raised Markers				
0250	627-K001		2,165	Each	Red-Clear Reflective High Performance Raised Markers				
0260	627-L001		950	Each	Two-Way Yellow Reflective High Performance Raised Markers				
0270	630-F001		6	Each	Delineators, Guard Rail, White				
0280	630-F002		6	Each	Delineators, Guard Rail, Yellow				
0290	630-F003		10	Each	Delineators, Flexible Post Mounted, Crossover, Type I, Green				
0300	630-F004		10	Each	Delineators, Flexible Post Mounted, Crossover, Type I, Yellow				
0310	635-A001		2,136	Linear Feet	Vehicle Loop Assemblies				
0320	907-304-H001	(GY)	1,600	Cubic Yard	Size 825 Crushed Stone Base, LVM				

Line No.	Item Code	Adj Code	Quantity	Units	Description	Unit Price		Bid Amount	
0330	907-407-A001	(A2 )	10,800	Gallon	Asphalt for Tack Coat				
0340	907-413-E001		5,910	Linear Feet	Sawing and Sealing Transverse Joints in Asphalt Pavement				
0350	907-626-A003		8	Mile	6" Thermoplastic Traffic Stripe, Skip White				
0360	907-626-C003		12	Mile	6" Thermoplastic Double Drop Edge Stripe, Continuous White				
0370	907-626-D003		4	Mile	6" Thermoplastic Traffic Stripe, Skip Yellow				
0380	907-626-E004		7	Mile	6" Thermoplastic Traffic Stripe, Continuous Yellow				
0390	907-626-F003		6	Mile	6" Thermoplastic Double Drop Edge Stripe, Continuous Yellow				
0400	907-626-G004		25,946	Linear Feet	Thermoplastic Detail Stripe, White				
0410	907-626-G005		25,825	Linear Feet	Thermoplastic Detail Stripe, Yellow				
0420	907-626-H004		3,810	Linear Feet	Thermoplastic Legend, White				
0430	907-626-H005		1,280	Square Feet	Thermoplastic Legend, White				
<b>ALTERNATE GROUP AA NUMBER 1</b>									

Line No.	Item Code	Adj Code	Quantity	Units	Description	Unit Price		Bid Amount	
0440	907-403-A007	(BA1)	120	Ton	Hot Mix Asphalt, MT, 19-mm mixture				
0450	907-403-A010	(BA1)	14,040	Ton	Hot Mix Asphalt, MT, 9.5-mm mixture				
0460	907-403-B006	(BA1)	7,400	Ton	Hot Mix Asphalt, MT, 9.5-mm mixture, Leveling				
<b>ALTERNATE GROUP AA NUMBER 2</b>									
0470	907-403-M006	(BA1)	14,040	Ton	Warm Mix Asphalt, MT, 9.5-mm mixture				
0480	907-403-M007	(BA1)	120	Ton	Warm Mix Asphalt, MT, 19-mm mixture				
0490	907-403-N005	(BA1)	7,400	Ton	Warm Mix Asphalt, MT, 9.5-mm mixture, Leveling				

\*\*\* BID CERTIFICATION \*\*\*

TOTAL BID.....\$\_\_\_\_\_

\*\*\* DBE/WBE SECTION \*\*\*

Complete item nos. 1, 2, and/or 3 as appropriate. See Notice to Bidders addressing Disadvantaged Business Enterprises in Highway Construction.

1. I/We agree that no less than \_\_\_\_\_ percent shall be expended with small business concerns owned and controlled by socially and economically disadvantaged individuals (DBE and WBE).
2. Classification of Bidder: Small Business (DBE)\_\_\_\_\_ Small Business (WBE)\_\_\_\_\_
3. A joint venture with a Small Business (DBE/WBE): \_\_\_\_\_

\*\*\* SIGNATURE STATEMENT \*\*\*

BIDDER ACKNOWLEDGES THAT HE/SHE HAS CHECKED ALL ITEMS IN THIS PROPOSAL FOR ACCURACY AND CERTIFIED THAT THE FIGURES SHOWN THEREIN CONSTITUTE THEIR OFFICIAL BID.

\_\_\_\_\_  
BIDDER'S SIGNATURE

\_\_\_\_\_  
BIDDER'S COMPANY

\_\_\_\_\_  
BIDDER'S FEDERAL TAX ID NUMBER



**CONDITIONS FOR COMBINATION BID**

If a bidder elects to submit a combined bid for two or more of the contracts listed for this month's letting, the bidder must complete and execute these sheets of the proposal in each of the individual proposals to constitute a combination bid. In addition to this requirement, each individual contract shall be completed, executed and submitted in the usual specified manner.

Failure to execute this Combination Bid Proposal in each of the contracts combined will be just cause for each proposal to be received and evaluated as a separate bid.

\*\*\*\*\*

**COMBINATION BID PROPOSAL**

I. This proposal is tendered as one part of a Combination Bid Proposal utilizing option \_\_\_\* of Subsection 102.11 on the following contracts:

\* Option to be shown as either (a), (b), or (c).

<u>Project No.</u>	<u>County</u>	<u>Project No.</u>	<u>County</u>
1. _____	_____	6. _____	_____
2. _____	_____	7. _____	_____
3. _____	_____	8. _____	_____
4. _____	_____	9. _____	_____
5. _____	_____	10. _____	_____

A. If option (a) has been selected, then go to II, and sign Combination Bid Proposal.

B. If option (b) has been selected, then complete the following, go to II, and sign Combination Bid Proposal.

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
1. _____	_____ _____	_____ _____	_____ _____	_____ _____	
2. _____	_____ _____	_____ _____	_____ _____	_____ _____	
3. _____	_____ _____	_____ _____	_____ _____	_____ _____	
4. _____	_____ _____	_____ _____	_____ _____	_____ _____	
5. _____	_____ _____	_____ _____	_____ _____	_____ _____	
6. _____	_____ _____	_____ _____	_____ _____	_____ _____	
7. _____	_____ _____	_____ _____	_____ _____	_____ _____	
8. _____	_____ _____	_____ _____	_____ _____	_____ _____	

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
9. _____	_____ _____	_____ _____	_____ _____	_____ _____	
10. _____	_____ _____	_____ _____	_____ _____	_____ _____	

C. If option (c) has been selected, then initial and complete one of the following, go to II. and sign Combination Bid Proposal.

\_\_\_\_\_ I (We) desire to be awarded work not to exceed a total monetary value of \$ \_\_\_\_\_.

\_\_\_\_\_ I (We) desire to be awarded work not to exceed \_\_\_\_\_ number of contracts.

II. It is understood that the Mississippi Transportation Commission not only reserves the right to reject any and all proposals, but also the right to award contracts upon the basis of lowest separate bids or combination bids most advantageous to the State.

It is further understood and agreed that the Combination Bid Proposal is for comparison of bids only and that each contract shall operate in every respect as a separate contract in accordance with its proposal and contract documents.

I (We), the undersigned, agree to complete each contract on or before its specified completion date.

SIGNED \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**Certification with regard to the Performance of Previous  
Contracts or Subcontracts subject to the Equal Opportunity  
Clause and the filing of Required Reports**

The Bidder \_\_\_\_\_, proposed Subcontractor \_\_\_\_\_, hereby certifies that he has \_\_\_\_\_, has not \_\_\_\_\_, participated in a previous contract or subcontract subject to the Equal Opportunity Clause, as required by Executive Orders 10925, 11114, or 11246, and that he has \_\_\_\_\_, has not \_\_\_\_\_, filed with the Joint Reporting Committee, the Director of the Office of Federal Contract Compliance, a Federal Government contracting or administering agency, or the former President's Committee on Equal Employment Opportunity, all reports due under the applicable filing requirements.

\_\_\_\_\_  
(COMPANY)

BY \_\_\_\_\_

\_\_\_\_\_  
(TITLE)

DATE: \_\_\_\_\_

NOTE: The above certification is required by the Equal Employment Opportunity Regulations of the Secretary of Labor (41 CFR 60-1.7 (b) (1)), and must be submitted by bidders and proposed subcontractors only in connection with contracts and subcontracts which are subject to the Equal Opportunity Clause. Contracts and Subcontracts which are exempt from the Equal Opportunity Clause are set forth in 41 CFR 60-1.5. (Generally only contracts or subcontracts of \$10,000 or under are exempt.)

Currently, Standard Form 100 (EEO-1) is the only report required by the Executive Orders or their implementing regulations.

Proposed prime Contractors and Subcontractors who have participated in a previous contract or subcontract subject to the Executive orders and have not filed the required reports should note that 41 CFR 60-1.7 (b) (1) prevents the award of contracts and subcontracts unless such Contractors submit a report covering the delinquent period or such other period specified by the Federal Highway Administration or by the Director, Office of Federal Contract Compliance, U. S. Department of Labor.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

CERTIFICATION  
(Execute in duplicate)

I, \_\_\_\_\_,  
(Name of person signing certification)

individually, and in my capacity as \_\_\_\_\_ of  
(Title)

\_\_\_\_\_ do hereby certify under  
(Name of Firm, Partnership, or Corporation)

penalty of perjury under the laws of the United States and the State of Mississippi that \_\_\_\_\_

\_\_\_\_\_, Bidder  
(Name of Firm, Partnership, or Corporation)

on Project No. **STP-2170-00(023) / 106222301**

in **Forrest** County(ies), Mississippi, has not either

directly or indirectly entered into any agreement, participated in any collusion; or otherwise taken any action in restraint of free competitive bidding in connection with this contract; nor have any of its corporate officers or principal owners.

Except as noted hereafter, it is further certified that said legal entity and its corporate officers, principal owners, managers, auditors and others in a position of administering federal funds:

- a) Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any Federal department or agency;
- b) Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, or receiving stolen property;
- c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated in (b) above; and
- d) Have not within a three-year period preceding this application/ proposal had one or more public transactions (Federal, State or local) terminated for cause or default.

Initial here " \_\_\_\_\_ " if exceptions are attached and made a part thereof. Any exceptions shall address to whom it applies, initiating agency and dates of such action.



Note: Exceptions will not necessarily result in denial of award but will be considered in determining bidder responsibility. Providing false information may result in criminal prosecution or administrative sanctions.

The bidder further certifies that the certification requirements contained in Section XI of Form FHWA 1273, will be or have been included in all subcontracts, material supply agreements, purchase orders, etc. except those procurement contracts for goods or services that are expected to be less than the Federal procurement small purchase threshold fixed at 10 U.S.C. 2304(g) and 41 U.S.C. 253(g) (currently \$25,000) which are excluded from the certification requirements.

The bidder further certifies, to the best of his or her knowledge and belief, that:

- 1) No Federal appropriated funds have been paid or will be paid to any person for influencing or attempting to influence an officer or employee of any Federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any Federal contract, the making of any Federal grant, the making of any Federal loan, the entering into of any cooperative agreement, and the extension, continuation, renewal, amendment, or modification of any Federal contract, grant, loan, or cooperative agreement.
- 2) If any funds other than Federal appropriated funds have been paid or will be paid to any person for influencing or attempting to influence an officer or employee of any Federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with this contract, Standard Form-LLL, "Disclosure Form to Report Lobbying," in accordance with its instructions will be completed and submitted.

The certification contained in (1) and (2) above is a material representation of fact upon which reliance is placed and a prerequisite imposed by Section 1352, Title 31, U.S. Code prior to entering into this contract. Failure to comply shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000. The bidder shall include the language of the certification in all subcontracts exceeding \$100,000 and all subcontractors shall certify and disclose accordingly.

All of the foregoing and attachments (when indicated) is true and correct.

Executed on \_\_\_\_\_

\_\_\_\_\_  
Signature

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

CERTIFICATION  
(Execute in duplicate)

I, \_\_\_\_\_,  
(Name of person signing certification)

individually, and in my capacity as \_\_\_\_\_ of  
(Title)

\_\_\_\_\_ do hereby certify under  
(Name of Firm, Partnership, or Corporation)

penalty of perjury under the laws of the United States and the State of Mississippi that \_\_\_\_\_

\_\_\_\_\_, Bidder  
(Name of Firm, Partnership, or Corporation)

on Project No. STP-2170-00(023) / 106222301

in Forrest County(ies), Mississippi, has not either

directly or indirectly entered into any agreement, participated in any collusion; or otherwise taken any action in restraint of free competitive bidding in connection with this contract; nor have any of its corporate officers or principal owners.

Except as noted hereafter, it is further certified that said legal entity and its corporate officers, principal owners, managers, auditors and others in a position of administering federal funds:

- a) Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any Federal department or agency;
- b) Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, or receiving stolen property;
- c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated in (b) above; and
- d) Have not within a three-year period preceding this application/ proposal had one or more public transactions (Federal, State or local) terminated for cause or default.

Initial here " \_\_\_\_\_ " if exceptions are attached and made a part thereof. Any exceptions shall address to whom it applies, initiating agency and dates of such action.

Note: Exceptions will not necessarily result in denial of award but will be considered in determining bidder responsibility. Providing false information may result in criminal prosecution or administrative sanctions.

The bidder further certifies that the certification requirements contained in Section XI of Form FHWA 1273, will be or have been included in all subcontracts, material supply agreements, purchase orders, etc. except those procurement contracts for goods or services that are expected to be less than the Federal procurement small purchase threshold fixed at 10 U.S.C. 2304(g) and 41 U.S.C. 253(g) (currently \$25,000) which are excluded from the certification requirements.

The bidder further certifies, to the best of his or her knowledge and belief, that:

- 1) No Federal appropriated funds have been paid or will be paid to any person for influencing or attempting to influence an officer or employee of any Federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any Federal contract, the making of any Federal grant, the making of any Federal loan, the entering into of any cooperative agreement, and the extension, continuation, renewal, amendment, or modification of any Federal contract, grant, loan, or cooperative agreement.
- 2) If any funds other than Federal appropriated funds have been paid or will be paid to any person for influencing or attempting to influence an officer or employee of any Federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with this contract, Standard Form-LLL, "Disclosure Form to Report Lobbying," in accordance with its instructions will be completed and submitted.

The certification contained in (1) and (2) above is a material representation of fact upon which reliance is placed and a prerequisite imposed by Section 1352, Title 31, U.S. Code prior to entering into this contract. Failure to comply shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000. The bidder shall include the language of the certification in all subcontracts exceeding \$100,000 and all subcontractors shall certify and disclose accordingly.

All of the foregoing and attachments (when indicated) is true and correct.

Executed on \_\_\_\_\_

\_\_\_\_\_  
Signature

**S E C T I O N   9 0 2**

CONTRACT FOR STP-2170-00(023) / 106222301

LOCATED IN THE COUNTY(IES) OF Forrest

STATE OF MISSISSIPPI,  
COUNTY OF HINDS

This contract entered into by and between the Mississippi Transportation Commission on one hand, and the undersigned contractor, on the other witnesseth;

That, in consideration of the payment by the Mississippi Transportation Commission of the prices set out in the proposal hereto attached, to the undersigned contractor, such payment to be made in the manner and at the time of times specified in the specifications and the special provisions, if any, the undersigned contractor hereby agrees to accept the prices stated in the proposal in full compensation for the furnishing of all materials and equipment and the executing of all the work contemplated in this contract.

It is understood and agreed that the advertising according to law, the Advertisement, the instructions to bidders, the proposal for the contract, the specifications, the revisions of the specifications, the special provisions, and also the plans for the work herein contemplated, said plans showing more particularly the details of the work to be done, shall be held to be, and are hereby made a part of this contract by specific reference thereto and with like effect as if each and all of said instruments had been set out fully herein in words and figures.

It is further agreed that for the same consideration the undersigned contractor shall be responsible for all loss or damage arising out of the nature of the work aforesaid; or from the action of the elements and unforeseen obstructions or difficulties which may be encountered in the prosecution of the same and for all risks of every description connected with the work, exceptions being those specifically set out in the contract; and for faithfully completing the whole work in good and workmanlike manner according to the approved Plans, Specifications, Special Provisions, Notice(s) to Bidders and requirements of the Mississippi Department of Transportation.

It is further agreed that the work shall be done under the direct supervision and to the complete satisfaction of the Executive Director of the Mississippi Department of Transportation, or his authorized representatives, and when Federal Funds are involved subject to inspection at all times and approval by the Federal Highway Administration, or its agents as the case may be, or the agents of any other Agency whose funds are involved in accordance with those Acts of the Legislature of the State of Mississippi approved by the Governor and such rules and regulations issued pursuant thereto by the Mississippi Transportation Commission and the authorized Federal Agencies.

The Contractor agrees that all labor as outlined in the Special Provisions may be secured from list furnished by

It is agreed and understood that each and every provision of law and clause required by law to be inserted in this contract shall be deemed to be inserted herein and this contract shall be read and enforced as though it were included herein, and, if through mere mistake or otherwise any such provision is not inserted, then upon the application of either party hereto, the contract shall forthwith be physically amended to make such insertion.

The Contractor agrees that he has read each and every clause of this Contract, and fully understands the meaning of same and that he will comply with all the terms, covenants and agreements therein set forth.

Witness our signatures this the \_\_\_\_\_ day of \_\_\_\_\_, \_\_\_\_\_.

\_\_\_\_\_  
Contractor (s)

By \_\_\_\_\_

MISSISSIPPI TRANSPORTATION COMMISSION

Title \_\_\_\_\_

By \_\_\_\_\_

Signed and sealed in the presence of:  
(names and addresses of witnesses)

Executive Director

\_\_\_\_\_

\_\_\_\_\_  
Secretary to the Commission

Award authorized by the Mississippi Transportation Commission in session on the \_\_\_\_ day of \_\_\_\_\_, \_\_\_\_\_, Minute Book No. \_\_\_\_\_, Page No. \_\_\_\_\_.

**S E C T I O N   9 0 3**  
**PERFORMANCE AND PAYMENT BOND**

CONTRACT BOND FOR: STP-2170-00(023) / 106222301

LOCATED IN THE COUNTY(IES) OF: Forrest

STATE OF MISSISSIPPI,

COUNTY OF HINDS

Know all men by these presents: that we, \_\_\_\_\_  
( Contractor )

\_\_\_\_\_ Principal, a \_\_\_\_\_

residing at \_\_\_\_\_ in the State of \_\_\_\_\_

and \_\_\_\_\_  
( Surety )

residing at \_\_\_\_\_ in the State of \_\_\_\_\_,

authorized to do business in the State of Mississippi, under the laws thereof, as surety, are held and firmly bound unto the State of Mississippi in the sum of \_\_\_\_\_

\_\_\_\_\_ Dollars, lawful money of the United States of America, to be paid to it for which payment well and truly to be made, we bind ourselves, our heirs, administrators, successors, or assigns jointly and severally by these presents.

Signed and sealed this the \_\_\_\_\_ day of \_\_\_\_\_ A.D. \_\_\_\_\_.

The conditions of this bond are such, that whereas the said \_\_\_\_\_

principal, has (have) entered into a contract with the Mississippi Transportation Commission, bearing the date of \_\_\_\_\_ day of \_\_\_\_\_ A.D. \_\_\_\_\_ hereto annexed, for the construction of certain projects(s) in the State of Mississippi as mentioned in said contract in accordance with the Contract Documents therefor, on file in the offices of the Mississippi Department of Transportation, Jackson, Mississippi.

Now therefore, if the above bounden \_\_\_\_\_

\_\_\_\_\_ in all things shall stand to and abide by and well and truly observe, do keep and perform all and singular the terms, covenants, conditions, guarantees and agreements in said contract, contained on his (their) part to be observed, done, kept and performed and each of them, at the time and in the manner and form and furnish all of the material and equipment specified in said contract in strict accordance with the terms of said contract which said plans, specifications and special provisions are included in and form a part of said contract and shall maintain the said work contemplated until its final completion and acceptance as specified in Subsection 109.11 of the approved specifications, and save harmless said Mississippi Transportation Commission from any loss or damage arising out of or occasioned by the negligence, wrongful or criminal act, overcharge, fraud, or any other loss or damage whatsoever, on the part of said principal (s), his (their) agents, servants, or employees in



**SECTION 903 - CONTINUED**

the performance of said work or in any manner connected therewith, and shall be liable and responsible in a civil action instituted by the State at the instance of the Mississippi Transportation Commission or any officer of the State authorized in such cases, for double any amount in money or property, the State may lose or be overcharged or otherwise defrauded of, by reason of wrongful or criminal act, if any, of the Contractor(s), his (their) agents or employees, and shall promptly pay the said agents, servants and employees and all persons furnishing labor, material, equipment or supplies therefor, including premiums incurred, for Surety Bonds, Liability Insurance, and Workmen's Compensation Insurance; with the additional obligation that such Contractor shall promptly make payment of all taxes, licenses, assessments, contributions, damages, any liquidated damages which may arise prior to any termination of said principal's contract, any liquidated damages which may arise after termination of the said principal's contract due to default on the part of said principal, penalties and interest thereon, when and as the same may be due this state, or any county, municipality, board, department, commission or political subdivision: in the course of the performance of said work and in accordance with Sections 31-5-51 et seq. Mississippi Code of 1972, and other State statutes applicable thereto, and shall carry out to the letter and to the satisfaction of the Executive Director of the Mississippi Department of Transportation, all, each and every one of the stipulations, obligations, conditions, covenants and agreements and terms of said contract in accordance with the terms thereof and all of the expense and cost and attorney's fee that may be incurred in the enforcement of the performance of said contract, or in the enforcement of the conditions and obligations of this bond, then this obligation shall be null and void, otherwise to be and remain in full force and virtue.

Witness our signatures and seals this the \_\_\_\_\_ day of \_\_\_\_\_ A.D. \_\_\_\_\_.

_____	_____
(Contractors) Principal	Surety
By _____	By _____
	(Signature) Attorney in Fact
	Address _____
	_____
Title _____	_____
(Contractor's Seal)	(Printed) MS Agent
	_____
	(Signature) MS Agent
	Address _____
	_____
	_____
	(Surety Seal)
	_____
	Mississippi Insurance ID Number



# BID BOND

KNOW ALL MEN BY THESE PRESENTS, that we \_\_\_\_\_  
Contractor

\_\_\_\_\_  
Address

\_\_\_\_\_  
City, State ZIP

as Principal, hereinafter called the Principal, and \_\_\_\_\_  
Surety

a corporation duly organized under the laws of the state of \_\_\_\_\_

as Surety, hereinafter called the Surety, are held and firmly bound unto State of Mississippi, Jackson, Mississippi

As Obligee, hereinafter called Obligee, in the sum of **Five Per Cent (5%) of Amount Bid**  
Dollars (\$ \_\_\_\_\_)

for the payment of which sum will and truly to be made, the said Principal and said Surety, bind ourselves, our heirs, executors, administrators, successors and assigns, jointly and severally, firmly by these presents.

WHEREAS, the Principal has submitted a bid for **Mill & Overlay approximately 5 miles of US 11 from the Lamar County Line to Lincoln Road, known as Federal Aid Project No. STP-2170-00(023) / 106222301, in Forrest County.**

NOW THEREFORE, the condition of this obligation is such that if the aforesaid Principal shall be awarded the contract, the said Principal will, within the time required, enter into a formal contract and give a good and sufficient bond to secure the performance of the terms and conditions of the contract, then this obligation to be void; otherwise the Principal and Surety will pay unto the Obligee the difference in money between the amount of the bid of the said Principal and the amount for which the Obligee legally contracts with another party to perform the work if the latter amount be in excess of the former, but in no event shall liability hereunder exceed the penal sum hereof.

Signed and sealed this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_

\_\_\_\_\_  
(Principal) (Seal)

\_\_\_\_\_  
(Witness)

By: \_\_\_\_\_  
(Name) (Title)

\_\_\_\_\_  
(Surety) (Seal)

\_\_\_\_\_  
(Witness)

By: \_\_\_\_\_  
(Attorney-in-Fact)

\_\_\_\_\_  
MS Agent

\_\_\_\_\_  
Mississippi Insurance ID Number

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
OFFICE OF CIVIL RIGHTS  
JACKSON, MISSISSIPPI  
**LIST OF FIRMS SUBMITTING QUOTES**

I/we received quotes from the following firms on Project No: **STP-2170-00(023) / 106222301**  
County: **Forrest**

Disadvantaged Business Enterprise (DBE) Regulations as stated in 49 CFR 26.11 require the Mississippi Department of Transportation (MDOT) to create and maintain a comprehensive list of all firms quoting/bidding subcontracts on prime contracts and quoting/bidding subcontracts on federally-funded transportation projects. For every firm, we require the following information:

Firm Name: \_\_\_\_\_  
Contact Name/Title: \_\_\_\_\_  
Firm Mailing Address \_\_\_\_\_  
Phone Number: \_\_\_\_\_  
\_\_\_\_\_ DBE Firm \_\_\_\_\_ Non-DBE Firm

Firm Name: \_\_\_\_\_  
Contact Name/Title: \_\_\_\_\_  
Firm Mailing Address \_\_\_\_\_  
Phone Number: \_\_\_\_\_  
\_\_\_\_\_ DBE Firm \_\_\_\_\_ Non-DBE Firm

Firm Name: \_\_\_\_\_  
Contact Name/Title: \_\_\_\_\_  
Firm Mailing Address \_\_\_\_\_  
Phone Number: \_\_\_\_\_  
\_\_\_\_\_ DBE Firm \_\_\_\_\_ Non-DBE Firm

Firm Name: \_\_\_\_\_  
Contact Name/Title: \_\_\_\_\_  
Firm Mailing Address \_\_\_\_\_  
Phone Number: \_\_\_\_\_  
\_\_\_\_\_ DBE Firm \_\_\_\_\_ Non-DBE Firm

Firm Name: \_\_\_\_\_  
Contact Name/Title: \_\_\_\_\_  
Firm Mailing Address \_\_\_\_\_  
Phone Number: \_\_\_\_\_  
\_\_\_\_\_ DBE Firm \_\_\_\_\_ Non-DBE Firm

\_\_\_\_\_  
**SUBMITTED BY (Signature)**

\_\_\_\_\_  
**FIRM NAME**

Submit this form to **Contract Administration as a part of your bid package**. If this form is not **signed** and included as part of the bid packet, your bid will be deemed irregular. For further information about this form, call Mississippi DOT's Office of Civil Rights at (601) 359-7466; FAX (601) 576-4504.  
**Please make copies of this form when needed and also add those copies to the bid package.**