

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) enclose a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u>1</u>	DATED <u>5/14/2012</u>	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. _____	DATED _____	ADDENDUM NO. _____	DATED _____

Number	Description
1	Revised Table of Contents,replace same;Revise NTB No. 3884,replace same;Add SP No. 907-702-3;EBS Download Required.

TOTAL ADDENDA: 1
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.

Revised 09/21/2005

STP-0019-02(045) / 106314301

Lafayette County(ies)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

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SECTION 905 - PROPOSAL, PROPOSAL BID SHEETS

COMBINATION BID PROPOSAL

CERTIFICATION OF PERFORMANCE - PRIOR FEDERAL-AID CONTRACTS

CERTIFICATION REGARDING NON-COLLUSION, DEBARMENT AND SUSPENSION

SECTION 902- CONTRACT FORM, AND SECTION 903 - CONTRACT BOND FORMS
FORM -- OCR-485

(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 3884

CODE: (SP)

DATE: 4/02/2012

SUBJECT: SCOPE OF WORK

PROJECT: STP-0019-02(045) / 106314301 -- Lafayette County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings". All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

The work to be accomplished using the Pay Items and corresponding specifications set forth in this contract, which is for trench widening and overlaying on approximately eleven (11) miles of State Highway No.7, beginning at the State Highway 30 Interchange to the Tallahatchie River, in Lafayette County, this includes the interchange ramps and an approximately one (1) mile section of the State Highway 30 overpass beginning at the beginning of the four (4) lane section on the east end and ending at the end of State Maintenance on the west end. This project will also add a north bound left turn lane to the intersection of State Highway No. 7 and Woodland Hills Drive.

Bidders are advised that cross-slopes for curve superelevations are to be constructed in accordance with information provided by the Department. To assist the contractor in correctly placing the cross slope transitions, the Department will provide at the preconstruction conference the stationing and percent slope information. Sufficient preleveling and leveling quantities are provided in this contract to facilitate the work. All transition cross slopes are to be established and checked prior to placement of the final lift of pavement.

It shall be the responsibility of the contractor to protect the roadway and all existing structures, such as bridges and curb, from damage occurring as a result of the contractor's operations. Damages to existing features caused by the contractor's operations shall be repaired or replaced at no cost to the Mississippi Department of Transportation.

At bridge ends and at the end of work day, a taper of one (1) vertical inch for each three (3) horizontal foot shall be provided.

The contractor shall make a utility location request to 811 prior to any excavation, excepting trench widening or pavement removal/repair.

In order to expedite the safe movement of traffic and to protect each phase of the work as it is performed, a firm sequence of operations is essential. The work shall be begun and continually prosecuted.

The work shall consist of the following:

1. A Northbound Left Turn Lane shall be added at Woodland Hill Drive by adding eight (8) feet and variable of travel lane to each side of the existing roadway. See attached layout and typical sections. The work shall consist of the following:
 - Grade out existing shoulder material a depth of approximately fourteen and one quarter (14 ¼) inches and variable at eight (8) feet and variable width to be paid for under Pay Item 203-A003 Unclassified Excavation, FM, AH.
 - Place eight (8) inches of Size 610 Crushed Stone over Type V Geotextile fabric (non-Woven) in the previously excavated area.
 - Place one lift of 19-mm ST HMA or WMA at two and one quarter (2 ¼) inches on top of the crushed stone.
 - Place two lifts of 12.5-mm MT HMA or WMA at two (2) inches on top of the 19-mm lift.
 - The final lift of 9.5-mm MT HMA or WMA shall be placed along with the surface lift of the mainline paving.

NOTE: At the end of each work day, a safety wedge shall be placed according to the attached standard drawing to allow no more than a four (4) inch drop off. This work shall be paid for under Pay Item 907-304-F002 Size 610 Crushed Stone. If needed all efforts shall be made to utilize this material multiple times. Safety shoulders shall be constructed daily to adhere to the drop off policy at no additional cost to the state.

2. Excavate three (3) inches deep and two (2) feet wide at the pavement edges for trench widening. The remaining shoulder width will require placement of granular material.

NOTE: Prior to this excavation, sufficient granular material must be in place to provide an acceptable trench widening operation.

NOTE: Any material removed from the excavation operation of trench widening that cannot be reasonably used as part of the final shoulder shall be removed; and this material shall be moved simultaneous with the trench widening operation. Neither shall this material be allowed to remain rolled up on the existing shoulder or bladed down the existing slope. There will be no separate payment for this work which shall be included in the pay items provided.

NOTE: The trench widening excavation may include the removal of some asphalt along the shoulder due to previous maintenance repairs. There will be no separate payment for this work which shall be included in the pay items provided.

NOTE: Due care shall be taken during this operation to blade material to the roadway and away from the ditch line. Material inadvertently bladed to the roadway vegetation shall be removed at no cost to the Mississippi Department of Transportation.

3. Placement of three (3) inches of trench widening (ST, 19 mm) in the previous excavation.
4. Repair failed areas using the following:
 - 202-B, Removal of Pavement, All Types and Thicknesses—for pavement structure
 - 203-EX, Excess Excavation –for material below the pavement structure
 - 907-403-C, Hot Mix Asphalt, ST, 19 mm, Trench Widening to replace failed areas

NOTE: Replacement shall be made same day as excavation.

5. Cold milling of the existing asphalt pavement at the tie-ins and at bridge ends to a depth of one and one-half (1 1/2) inches and variable in order to provide a smooth-transition will be required. The entire section shall not be milled. The cold milling material obtained shall become the property of the contractor.

Payment for Cold Milling of Pavement will be made under Pay Item no. 406-A, per square yard, and shall include all cost associated with the milling operation.

NOTE: During this operation and prior to placement of the asphalt, due care shall be required to keep surface water of ponding on the roadway surface; continuous monitoring of the project may be required.

NOTE: Work requiring a lane closure will not be allowed on Ole Miss Football Home Game Days.

6. 50 Tons/Mile preliminary leveling has been set up to correct any irregular areas prior to the leveling lift.
7. Placement of a 3/4" and variable leveling asphalt mix to establish a uniform cross section and an acceptable centerline profile prior to placement of the surface course to achieve a 2% cross slope. Mix to be used shall be (MT, 9.5-mm leveling).
8. Placement of one and one-half (1 1/2) in of surface course (MT, 9.5-mm) on the twenty-eight (28) feet of pavement previously leveled.
9. Placement of granular material on the shoulders as directed to raise the existing shoulders to the new surface course grade.

NOTE: Shoulders shall be bladed, shaped and compacted throughout the length of the project regardless of whether granular material is required.

NOTE: Granular material not required for the final shape of the shoulders may require removal under the pay item for excess excavation and may include small amounts of asphalt.

NOTE: Due care shall be taken during this operation to blade material to the roadway and away from the ditch line. Material inadvertently bladed to the roadway vegetation shall be removed at no cost to the Mississippi Department of Transportation.

10. Placement of Temporary Traffic Stripe daily as per Special Provisions Nos. 907-618-4.
11. Install rumble strips for rumble stripe.
12. Place permanent pavement markings as required (Thermoplastic Striping, Red-Clear, and Two-way yellow Reflective High Performance Raised Markers).

All ramps are to be paved a maximum distance of 10' or to the existing ROW and shall be tapered to fit existing conditions to allow smooth entry and exit. County roads and/or city streets are to be paved and/or milled to the right-of-way line. County roads and/or city streets are to be restriped.

The contractor shall provide all signs and traffic handling devices necessary to safely maintain traffic around or through the work areas.


Incidental work such as removing vegetation, shaping and compaction of shoulder, necessary and incidental grading of roadway ditches and other incidental work that is necessary to complete the work will not be measured for separate payment and the cost will be included in the bid items provided.

The Engineer may direct the use of additional cones at County roads or intersections within lane closures and will be absorbed in Maintenance of Traffic.

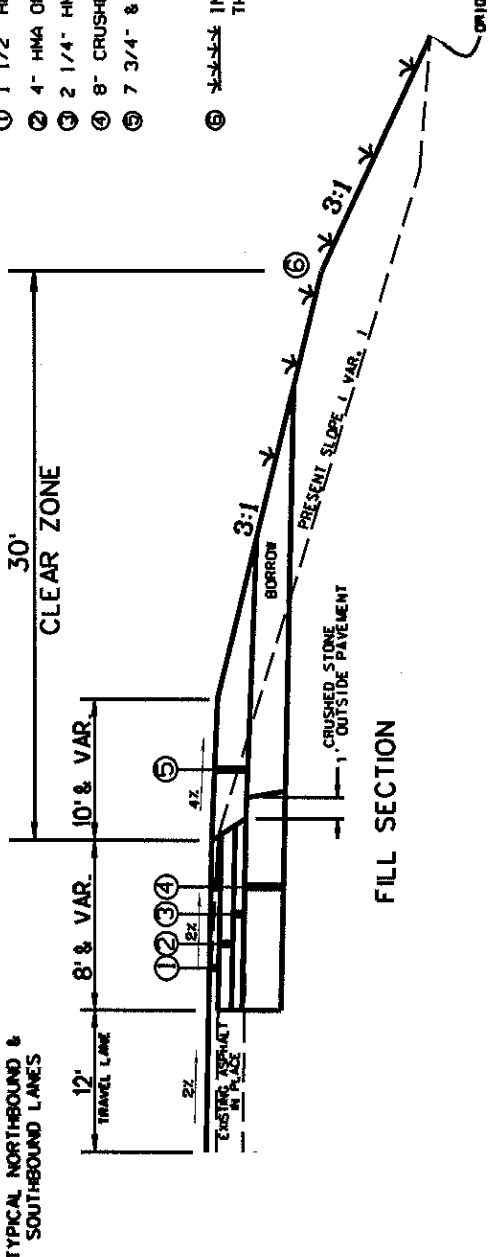
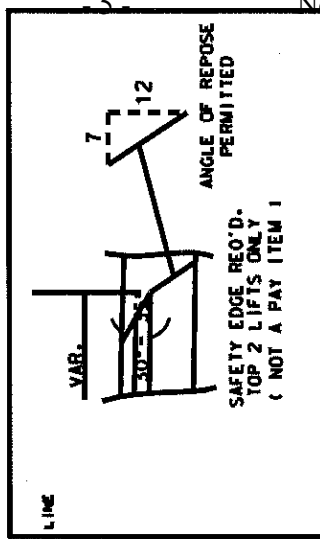
Prior to grinding rumble strips, the Contractor shall mark the beginning and ending of these rumble strips. Temporary paint shall not be used solely as a final location of begin and end of rumble strip.

STATE PROJECT NO.
MS. 319-0019-0204

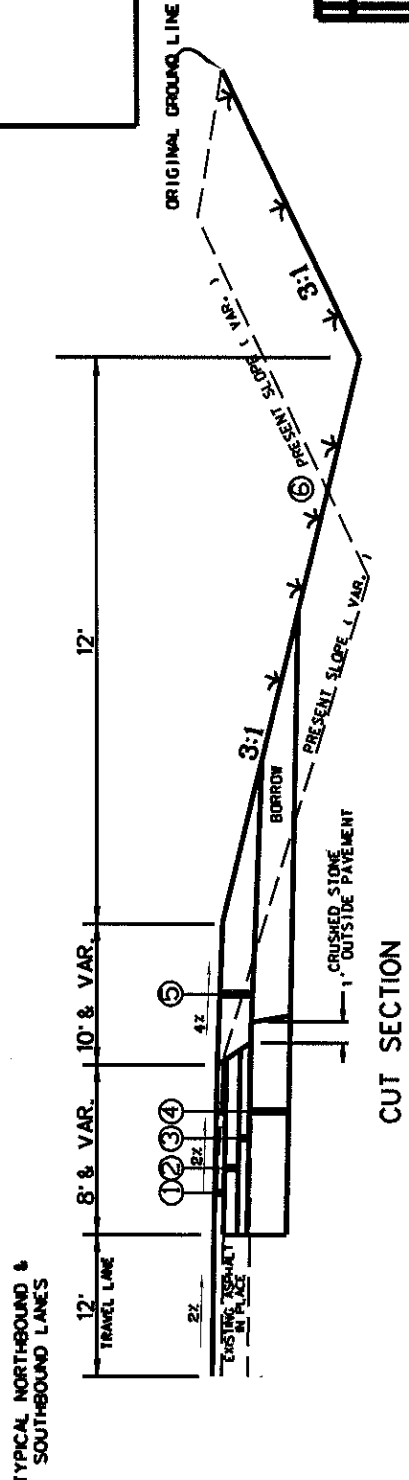
LEGEND

- ① 1 1/2" HMA OR WMA, MT (9.5 MM MIXTURE) REQUIRED
- ② 4" HMA OR WMA, MT (12.5 MM MIXTURE) (2 @ 2" LIFTS) REQUIRED
- ③ 2 1/4" HMA OR WMA, MT (19 MM MIXTURE) REQUIRED
- ④ 8" CRUSHED STONE W/GEOTEXTILE FABRIC TYPE V (NON-WOVEN) REQUIRED
- ⑤ 7 3/4" & VARIABLE GRANULAR MATERIAL, CLASS 3, GROUP D REQUIRED
- ⑥  INDICATES AREA TO BE TREATED IN ACCORDANCE WITH THE VEGETATION SCHEDULE. SEE WK. SH. NO. VS-1

NOTE: NO JOINT PERMITTED IN TRAVEL LANE.



TYPICAL SECTION OF TURN LANE ADDITION @ S.R. 7
STATION 1652+50 TO STATION 1655+50



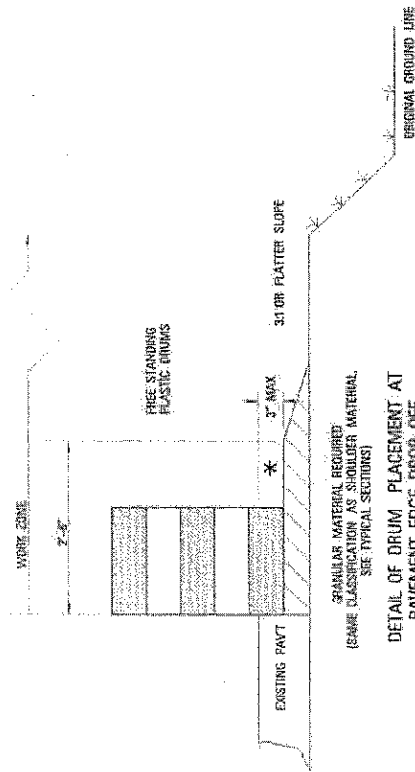
CUT SECTION

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
TYPICAL SECTION
RAMP EXTENSION
S.R. 7 TURN LANE ADDITION
PROJECT NO. 319-0019-0204
COUNTY: LAFAYETTE
DATE: 01/20/04

PROJ. NO. 319-0019-0204

COUNTY: LAFAYETTE

STATE	PROJECT NO.
WIS.	



GRAVELLY MATERIAL REQUIRED
(SEE CLASSIFICATION AS SHOULDER MATERIAL
SEE TYPICAL SECTIONS)

DETAIL OF DRUM PLACEMENT AT PAVEMENT EDGE DROP-OFF

NOTES

* A. PAVEMENT EDGE DROP-OFF

- IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED PLACE A SHOULDER WORK SIGN (W21-4) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W21-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750' ± O.C.)
- TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. DRUMS MAY BE USED IN PLACE OF DRUMS, VERTICAL PANELS AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVED SECTIONS DRUMS SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE MULTIPLE $1/3 L$, WHERE L IS THE TAPER LENGTH IN FEET.
- GREATER THAN THREE (3) INCHES-POSTIVE SEPARATION ON HEDGES WITH 2:1 OR FLATTER SLOPE NEEDED IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF. THEN DRUMS, VERTICAL PANELS OR BARRICADES MAY BE USED. IF CONCRETE BARRIERS ARE USED, SPECIAL REFLECTIVE DEVICES OR STEADY BURN LIGHTS SHOULD BE USED FOR OVERNIGHT INSTALLATIONS.
- FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
- LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

B. DRUM SPACING

- TANGENTS = $7 \times S$
- TAPERS = $1/3 L$
- WHERE $L = S \times W$
- L = TAPER LENGTH IN FEET
- S = SPEED IN MPH (POSTED OR 85 PERCENTILE)
- W = WIDTH OF OFFSET IN FEET

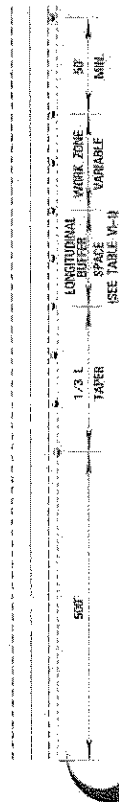
C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

TABLE 1-1. DRUMS ARE LENGTH OF
UNOCCUPIED BEHIND SPACE

SPACING (FEET)	DRUMS PER SPACE
25	1
30	1
35	1
40	1
45	1
50	1
55	1
60	1
65	1
70	1
75	1
80	1
85	1
90	1
95	1
100	1
105	1
110	1
115	1
120	1
125	1
130	1
135	1
140	1
145	1
150	1
155	1
160	1
165	1
170	1
175	1
180	1
185	1
190	1
195	1
200	1
205	1
210	1
215	1
220	1
225	1
230	1
235	1
240	1
245	1
250	1
255	1
260	1
265	1
270	1
275	1
280	1
285	1
290	1
295	1
300	1
305	1
310	1
315	1
320	1
325	1
330	1
335	1
340	1
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350	1
355	1
360	1
365	1
370	1
375	1
380	1
385	1
390	1
395	1
400	1
405	1
410	1
415	1
420	1
425	1
430	1
435	1
440	1
445	1
450	1
455	1
460	1
465	1
470	1
475	1
480	1
485	1
490	1
495	1
500	1

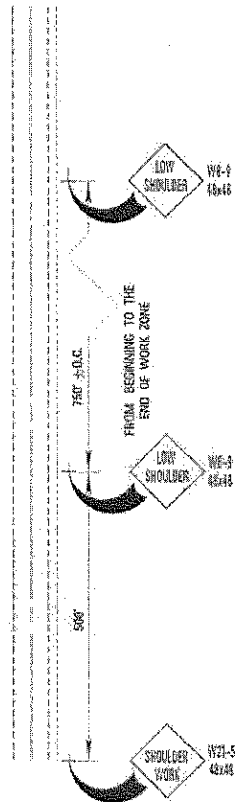
* PLACED WITHIN THE 100 FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA. OR SEE NOTE A-3 THIS SHEET.

PLASTIC DRUMS
(SEE NOTE FOR SPACING)



TYPICAL SHOULDER CLOSURE

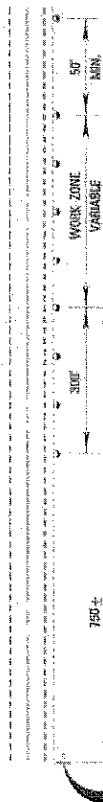
- TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
- TO BE USED WHEN CONSTRUCTION VEHICLES (EQUIPMENT) ENCROACHES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.



TYPICAL SHOULDER WORK #1

(SEE NOTE A-1 THIS SHEET)

PLASTIC DRUMS
(SEE NOTE FOR SPACING)

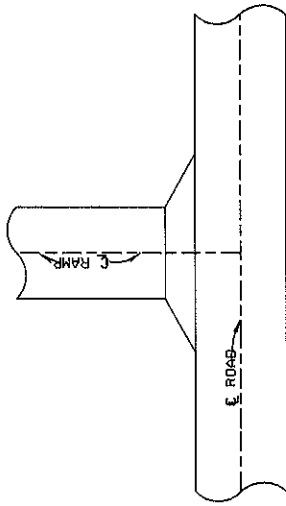


TYPICAL SHOULDER WORK #2

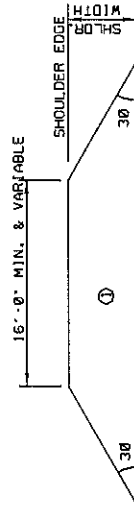
NOTE: WORK OUTSIDE THE (2) FOOT LIMIT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA. OR SEE NOTE A-3 THIS SHEET.

RETURN TO:	
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TRAFFIC CONTROL DETAIL	
DRUM PLACEMENT	
AND	
SHOULDER CLOSURE	
PROJECT NO.:	
COUNTY:	

STATE	PROJECT NO.
MSS.	



PLAN VIEW OF RAMP



DETAIL OF PAVED APRON

① 1 1/2" & VARIABLE DEPTH HOT MIX ASPHALT, MT. (9.5 MM MIX) REQUIRED

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
PAVED APRON [TAIL	
PROJECT NUMBER	884--
FILE NAME	MDOT 1
DATE	0000



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-702-3

CODE: (SP)

DATE: 05/08/2012

SUBJECT: Polyphosphoric Acid (PPA) Modification of Petroleum Asphalt Cement

Section 702.05, Petroleum Asphalt Cement, of the 2004 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows:

907-702.05--Petroleum Asphalt Cement. Delete the third paragraph of Subsection 702.05 on page 598, and substitute the following.

The bituminous material used in all types of asphalt mixtures shall conform to AASHTO Designation: M 320, Performance Grade PG 67-22, as modified in the table below, except that Polyphosphoric Acid (PPA) may be used at low dosage rates as a modifier to enhance the physical properties of a base binder to meet the requirements for Performance Grade PG 67-22. In addition, PPA may be used as a catalyst or mixing agent at low dosage rates in the production of Polymer Modified, Performance Grade PG 76-22.

When PPA is used as a modifier, in no case shall the PPA modifier be used to adjust the physical properties of the binder a full binder grade. For example: the base binder (unmodified) is graded as a PG 64-22 and should only be modified by the addition of PPA to a modified binder grade of PG 67-22.

When petroleum asphalt cement is modified by PPA, the following dosage limits shall be applied.

Grade	Dosage Limit
PG 67-22	0.75% by weight of binder
PG 76-22	0.50% by weight of binder