

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) enclose a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. 1 DATED 6/18/2012 ADDENDUM NO. DATED
 ADDENDUM NO. DATED ADDENDUM NO. DATED

Number	Description
1	Revised Table of Contents, replace same; Revised NTB No. 3926, replaces same; Remove SP 907-310-1; Amendment EBS Download Required.

TOTAL ADDENDA: 1
 (Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

 Contractor

BY _____
 Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.

Revised 09/21/2005

STP-0472-00(011) / 106374301

Newton County(ies)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

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SECTION 905 - PROPOSAL,
PROPOSAL BID SHEETS,
COMBINATION BID PROPOSAL,
CERTIFICATION OF PERFORMANCE - PRIOR FEDERAL-AID CONTRACTS,
CERTIFICATION REGARDING NON-COLLUSION, DEBARMENT AND SUSPENSION,
SECTION 902 - CONTRACT FORM, AND SECTION 903 - CONTRACT BOND FORMS,
OCR-485.

(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 3926

CODE: (SP)

DATE: 06/18/2012

SUBJECT: Scope of Work

PROJECT: STP-0472-00(011) / 106374301 -- Newton County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings". All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following:

Mill and overlay approximately 10.25 miles of existing composite pavement on U.S. Hwy 80 in Newton County from just east of the Veteran's Memorial Cemetery (Station 214+88) to Newton/Lauderdale County Line (Station 744+76)

From the BOP at Station 214+88 to 394+85, 423+28 to 699+00, and 703+00 to the EOP at Station 744+76

Work in this area shall consist of repairing failed areas in the underlying concrete pavement, cleaning and filling joints in the underlying concrete pavement, and milling and overlaying the travel lanes with 2" and variable 12.5mm asphalt pavement, MT. The pavement shall be milled 2" and overlaid with 2-1/4" at centerline and 1-1/2" at the edge of the travel lane, in order to facilitate slope correction.

Prior to the mill and paving operations, failed areas in the existing travel lanes shall be removed and backfilled with 19.0 mm MT asphalt pavement as per the attached typical sections and details. A maximum lift of 3" is to be used for the backfilling. Any granular base material deemed unsuitable by the Engineer shall be removed as directed. Payment for the excavation of the base material will be paid under Pay Item 203-G Excess Excavation. A list of the failed areas is shown in the attached table; additional areas may be determined by the Engineer. Also, any failures in the previously widened shoulders will be removed and replaced with 19.0 mm MT asphalt pavement. Removal of the asphalt shoulder will be paid using the appropriate pay item(s).

Failed area repairs shall be completed as a continuous operation in order to minimize the traffic impact. Lane closures shall remain in place until the failed area has been completely repaired. Overnight lane closures will only be permitted in an emergency situation as determined by the Engineer. Should overnight closures be necessary, they shall be maintained by the contractor's personnel and in accordance with a lighting plan approved by the Engineer.

The concrete pavement joints in the travel lanes not repaired as failed areas are to be cleaned and filled. Joints in the underlying concrete pavement are to be cleaned full depth (5"- 6") in the concrete pavement. For joints up to 1-1/2" wide, fill with hot, applied, fiber reinforced polymer patching material (See attached Special Provision); for joints 1-1/2" or wider, fill with 12.5mm MT asphalt pavement. Cracked and broken pieces of existing asphalt pavement are to be removed

within 1' of each side of joint during joint cleaning, and replaced with 12.5mm compacted asphalt pavement after joint is filled or as directed (absorbed item). Cold mix asphalt will not be allowed for this operation. Locations are to be determined by the Engineer.

Prior to the milling and overlaying operations, the travel lanes are to be widened. The existing lanes are 10' in width and are to be widened 3' to provide an 11' lane and a 2' paved shoulder. The existing shoulder shall be excavated 7.5" below the existing pavement surface and widened 3' with 6" of 19.0 mm asphalt pavement, MT, Trench Widening prior to the mill and overlay. The trench widening operation may be completed by placing the widening lift in a maximum of 6" depth. The excavated material shall be retained and used to raise the existing shoulder to match the new pavement elevation. The cost of blading will be an absorbed item and is not to be included in the price of pay items bid. Material which cannot be placed and blended in adjacent areas and deemed to be excess excavation by the Engineer shall be removed under Pay Item No. 203-G Excess Excavation. **Some areas of the widening may contain variable depth bituminous material originally used to patch low spots in the shoulder. This material shall be excavated and removed prior to the paving operation and will not be measured for separate payment.**

Following the trench widening operation, the pavement is to be milled overlaid with 2" and variable 12.5mm asphalt pavement, MT. In order to tie the overlay to the existing pavement elevations, a 200' milling transition (2" to 0") at all bridge ends within the project limits, except the bridge at Station 309+08. This bridge is currently overlaid with asphalt pavement and it is to be milled and overlaid with 12.5mm MT asphalt pavement. Streets/roads adjacent to the mainline will be milled to the right-of-way at a depth of 2" and overlaid with 2" of 12.5mm MT asphalt pavement.

Following the mill and overlay operation, the transverse joints in the pavement shall be sawed and sealed within 7 days. The details for sawing and sealing transverse joints for this section are as shown in the attached drawings. The width of the sawing and sealing operation shall extend to the new edge of pavement, unless otherwise directed, to prevent "sympathy cracking."

From Station 394+85 to 423+28 and Station 699+00 to 703+00

Work within these station limits will consist of repairing failed areas of the underlying concrete pavement, cleaning and filling joints in underlying concrete pavement, and milling and overlaying the travel lanes with 12.5mm MT asphalt pavement.

The pavement repair and joint cleaning and filling of the underlying concrete pavement shall be performed as described in the previous section.

Due to the fact that the existing pavement within these station limits has paved shoulders, this section will not require trench widening. The existing pavement will be milled 22' wide at a depth of 2". In order to facilitate slope correction, the milled surface will be overlaid with a depth of 2-1/2" at centerline and 2" at the edge of 12.5mm MT asphalt pavement.

Following the mill and overlay operation within these sections, the transverse joints in the pavement shall be sawed and sealed within 7 days. The details for sawing and sealing transverse joints for this section are as shown in the attached drawings. The width of the sawing and sealing operation shall extend 1' outside the mill/overlay limits to prevent "sympathy cracking."

General Notes: * These general notes are applicable to all sites.

Potholes that may exist are to be patched in a timely manner and prior to beginning the asphalt overlay. Patching of potholes shall be considered an absorbed item.

Several concrete driveways and paved flumes are located within the project limits and within the area to be widened. Any driveways or flumes encountered are to be removed to the widening limits as required and replaced with 19.0mm asphalt during the widening operation. The depth of the widening shall be varied at these locations and paved in a manner to prevent a bump at driveways and to allow positive drainage at flumes. Saw cuts shall be used to create a neat edge along the existing pavement and to the widening limit. Any driveways or flumes removed shall be replaced the same day. To prevent an inconvenience to local residents or businesses, driveways should not be removed until immediately before the widening.

Guardrails are required to be replaced at various locations within the project (see attached Table for locations/quantities). All guardrail removed is to be replaced the same day and prior to reopening the adjacent lane of traffic. Voids created by the removal of posts, concrete anchors, footings, etc. shall be backfilled and tamped in accordance with Section 203 of the Standard Specifications. The guardrail pads are to be excavated and paved with 3" of 12.5mm MT asphalt pavement as per the attached drawings. The excavated material shall be retained and used to raise the existing shoulder to match the new pavement elevation. The cost of blading will be an absorbed item and is not to be included in the price of pay items bid. Material which cannot be placed and blended in adjacent areas and deemed to be excess excavation by the Engineer shall be removed under Pay Item No. 203-G Excess Excavation.

Traffic may be allowed to run on the milled surface a maximum of 2 calendar days. Traffic may be allowed to run on the milled surface of the local roads a maximum of 7 days. Temporary striping shall be placed as required prior to opening the roadway to traffic. Overnight lane closures will not be allowed for this operation. Milling shall be performed in accordance with the attached drawings at all city streets, mainline tie-ins, crossovers, etc. Temporary pavement joints (paper joints) shall be at least 3 paper-widths long, shall be used at all milled tie-ins, and shall be adequately maintained.

The Reclaimed Asphalt Pavement (RAP) material removed by the milling operation shall become the property of the contractor with the exception of 10,000 tons or 50% of the total anticipated RAP tonnage, whichever is less, to be stockpiled at the MDOT maintenance office yard at the Newton District Office. Unless the Contractor desires otherwise, the Contractor's milled material will be obtained first. The Contractor will be required to coordinate the efforts with the maintenance office to effectively stockpile the milled material as directed by the Engineer. All costs associated with the hauling, placing, and stockpiling the state retained material shall be included in the price bid for the cold milling.

Publicly maintained roads and streets should be paved to the existing right-of-way. Traffic may be allowed to run on the milled surface of the local roads a maximum of 7 days. Privately owned entrances shall be paved to the shoulder line per the included typical drawing. The asphalt thickness shall be consistent with that of the surface lift. All existing driveway pads shall be milled and replaced. Additional pads shall be placed at locations as directed by the Engineer. Pads shall be shaped horizontally and vertically to prevent excessive drop-offs. Grading for the placement of new pads shall be done as required. Grading for new pads will not be paid separately. The excavated material shall be retained and used to raise the existing shoulder to match the new pavement elevation. Granular material (Class 5, Group 'E') shall be provided

around the pads to prevent shoulder drop-offs as directed and shall be placed in a timely manner. Drop-offs exceeding 2.5" shall be corrected within 2 calendar days of the placement of the pad. Stabilizer aggregate shall be used as directed by the Engineer.

It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the engineer, any structures damaged during the life of the contract. No payment will be made for replacement or repair of damaged items.

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost is to be included in the price bid for Pay Item No. 618-A, Maintenance of Traffic unless shown on the Construction Signing Schedule. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background. Standard roadside construction signs and barricades will be paid for using the appropriate pay items.

Roadside construction signs, barrels, etc. shall be placed in accordance with the attached drawings or as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed. Additionally, traffic will be allowed to run on uneven lanes; however, the Contractor will have to place additional "Uneven Pavement" signs and the costs associated with this additional signage shall be included in the price bid for Pay Item No. 618-A, Maintenance of Traffic.

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal; the cost is to be included in the prices of items bid. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor. As described in the applicable Notice-To-Bidders, final project cleanup is required and will be completed prior to the scheduling of the final inspection.

Where applicable the existing shoulders are to be raised to match the new pavement elevation by placing variable depth Granular Material (Class 5, Group E) on the existing shoulders. Placement of the granular material on the finished asphalt course shall not be permitted. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%). Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%). The cost of blading will be an absorbed item and is not to be included in the price of pay items bid.

Existing raised pavement markers are to be removed prior to beginning the overlay operation. No measurement will be made for separate payment; the cost is to be included in the prices for other

items bid. Permanent pavement markers are to be placed in accordance with the attached drawings and Standard Drawings. Two-way yellow markers are to be placed on two-way roads and county roads. Red-clear markers are to be placed on divided roadways. Two-way clear markers are to be placed on county roads as per the attached detail.

Temporary traffic stripe will be required immediately after the required overlay and prior to opening area to traffic. Temporary stripe is to be placed in the same location and configuration as the permanent stripe.

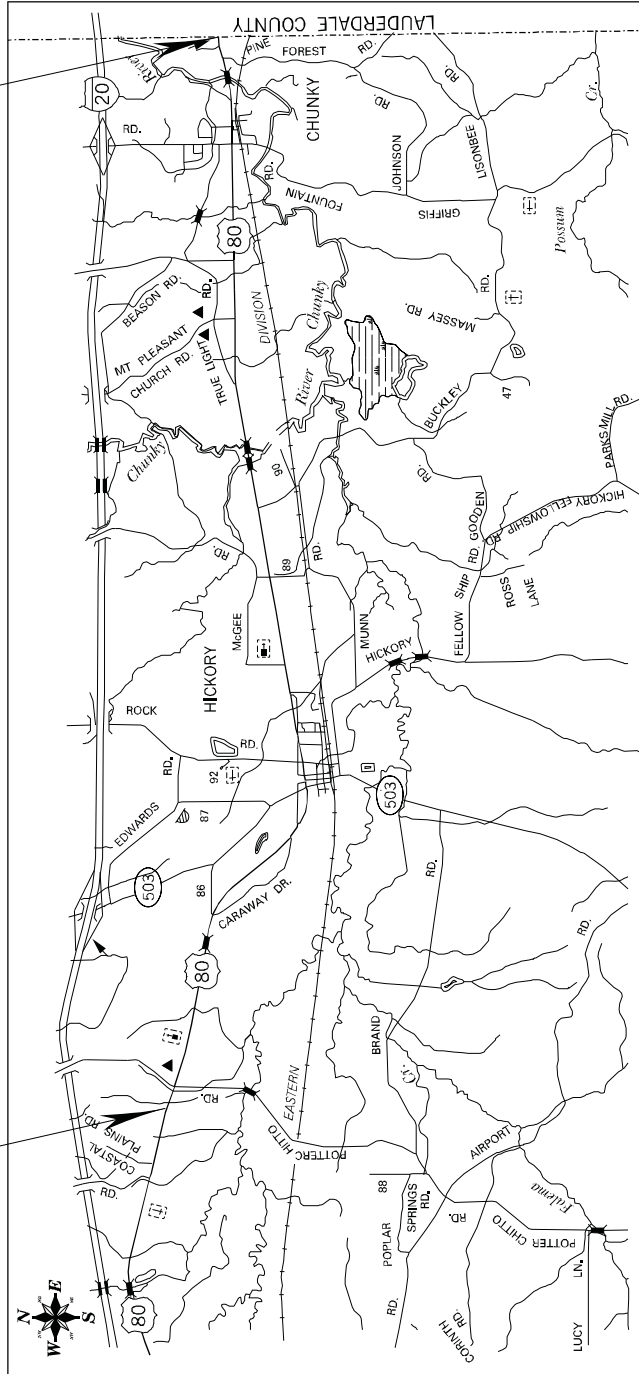
Existing traffic stripe on bridges and bridge approach slabs is to be removed and replaced. Payment for the removal of stripe will be made under Pay Item No. 202-B Removal of Traffic Stripe.

All permanent striping will be thermoplastic. Edge lines shall be placed to accommodate the lane widths shown on the applicable typical sections unless prevented by field conditions. Rumble stripe will be placed throughout the project limits in accordance with the attached detail.

**NEWTON COUNTY
STP-0472-00(011)/ 106374-301000
MILL AND OVERLAY FROM PAVEMENT CHANGE
EAST OF VETERAN'S MEMORIAL CEMETARY
TO LAUDERDALE COUNTY LINE**

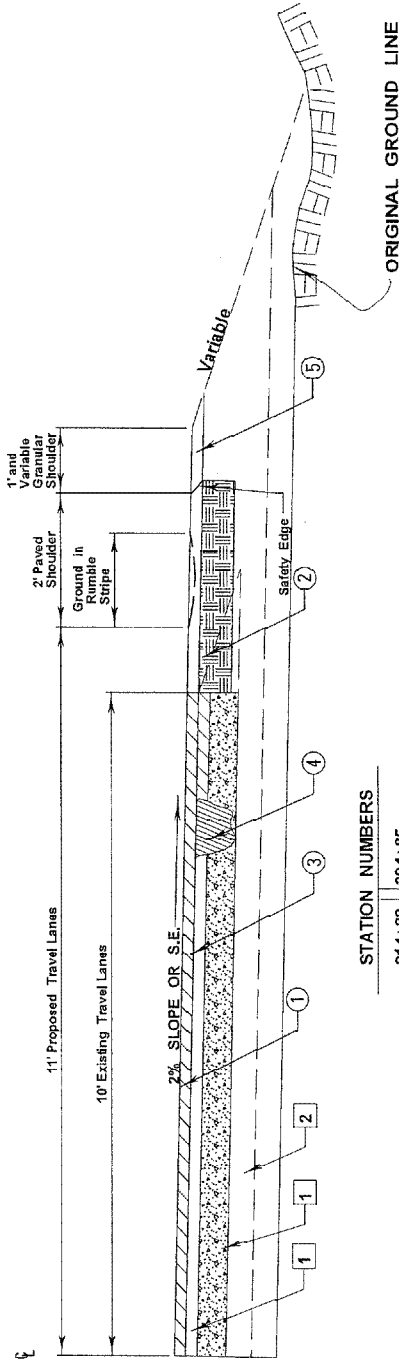
BOP STA. 214+88

EOP STA. 744+76



NEWTON COUNTY STP-0472-00(011)/ 106374-301000

TYPICAL SECTION 1 - WIDENING & OVERLAY



STATION NUMBERS

214+88	- 394+85	
423+28	- 699+00	
703+00	- 744+75	

EXISTING

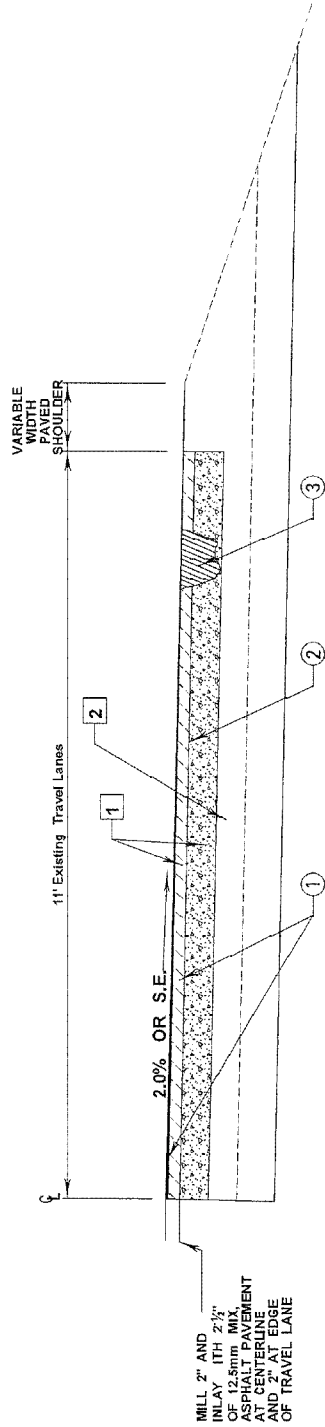
- 1 3" AND VARIABLE ASPHALT OVER 5"-6" OF JRPC
- 2 10" GRANULAR MATERIAL

PROPOSED

- 1 OVERLAY WITH 2" AND VARIABLE ASPHALT PAVEMENT, 12.5mm MIX, MT.
- 2 PRIOR TO OVERLAY, WIDEN EXISTING 10' LANES TO 13' BY TRENCH WIDENING. EXCAVATE 6" DEEP X 3' WIDE AND PLACE 1 LIFT OF 6" OF HOT MIX ASPHALT (OR WARM MIX ASPHALT) PAVEMENT, 19.0mm MIX, MT.
- 3 MILL EXISTING ASPHALT PAVEMENT 2".
- 4 REPAIR FAILED AREAS FULL DEPTH. REPAIR WITH ASPHALT PAVEMENT 19.0mm MIX, MT.
- 5 MATERIAL EXCAVATED DURING TRENCH WIDENING TO BE USED TO BRING SHOULDERS TO GRADE. PROVIDE ADDITIONAL QUANTITY (IF REQUIRED) OF CLASS 5, GROUP "E" GRANULAR MATERIAL TO BRING SHOULDERS TO GRADE AFTER OVERLAY

**NEWTON COUNTY
STP-0472-00(011)/ 106374-301000**

TYPICAL SECTION 2-MILL & INLAY



MILL 2" AND INLAY WITH 2 1/2" OF 12.5mm MIX ASPHALT PAVEMENT OVER CENTERLINE AND 2" AT EDGE OF TRAVEL LANE

LOCATIONS	STATION NUMBERS
HICKORY	394+85 - 423+28
CHUNKY	699+00 - 703+00

EXISTING

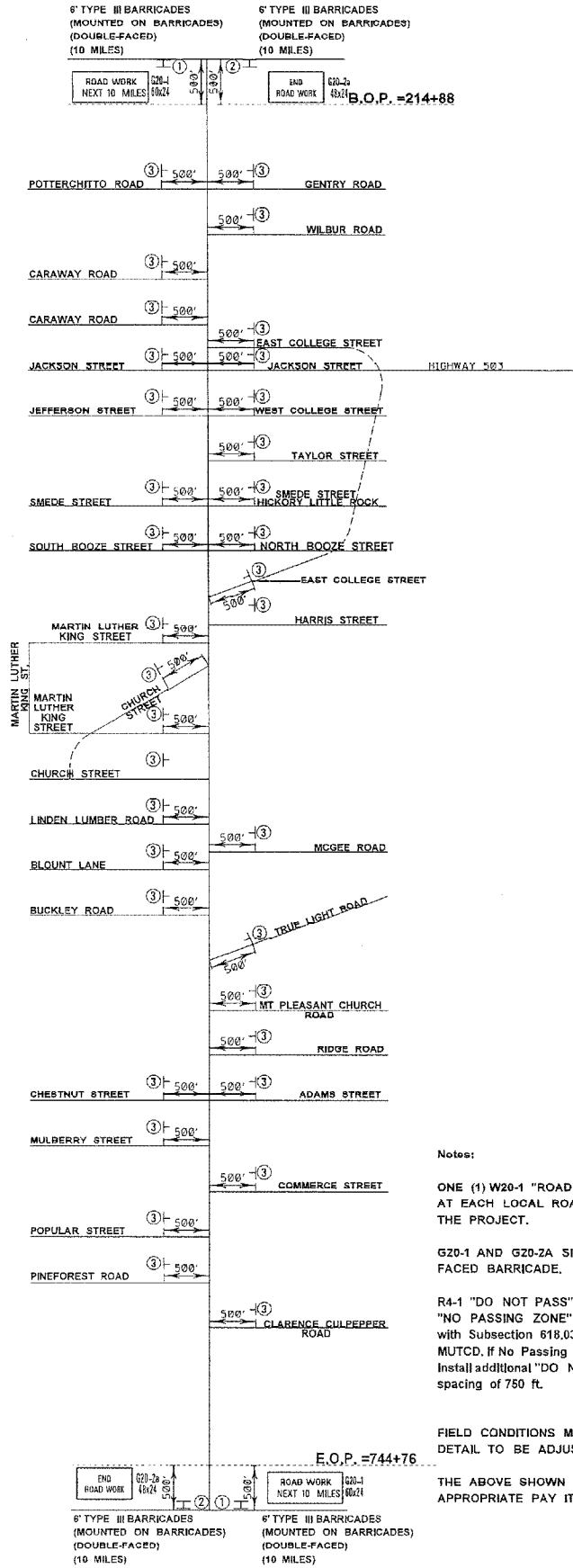
- 1 3" AND VARIABLE ASPHALT OVER 6" OF JRCP PAVEMENT
- 2 10" GRANULAR MATERIAL

PROPOSED

- 1 INLAY WITH 2" 12.5mm MIX ASPHALT PAVEMENT, MT AND VARIABLE (2 1/2" ON CENTERLINE AND 2" AT EDGE OF PAYMENT OF 12' TRAVEL LANE)
- 2 MILL EXISTING ASPHALT PAVEMENT 2".
- 3 REPAIR FAILED AREAS FULL DEPTH. REPAIR WITH 19.0mm MIX ASPHALT PAVEMENT, MT.

MILLED AREA

NEWTON COUNTY
STP-0472-00(011)/ 106374-301000
CONSTRUCTION SIGNING DETAIL



6' TYPE III BARRICADES (MOUNTED ON BARRICADES) (DOUBLE-FACED) (10 MILES)
6' TYPE III BARRICADES (MOUNTED ON BARRICADES) (DOUBLE-FACED) (10 MILES)

B.O.P. = 214+88

E.O.P. = 744+76

SIGN LEGEND	
NUMBER	DESCRIPTION
①	ROAD WORK NEXT 10 MILES G20-1 620-1 620-2A
②	END ROAD WORK G20-2A 620-2A 620-2A
③	ROAD WORK AHEAD W20-1 420-1 420-1

- TRAFFIC CONTROL SIGNS REQUIRED
 2 - G20-1 "ROAD WORK NEXT 10 MILES"
 2 - G20-2A "END ROAD WORK"
 36 - W20-1 "ROAD WORK AHEAD"
 73 - R4-1 "DO NOT PASS"
 18 - R4-2 "PASS WITH CARE"
 71 - W14-3 "NO PASSING ZONE"

Notes:
 ONE (1) W20-1 "ROAD WORK AHEAD" SIGN IS REQUIRED AT EACH LOCAL ROAD, STREET, OR HIGHWAY ENTERING THE PROJECT.
 G20-1 AND G20-2A SIGNS MOUNTED ON TYPE III DOUBLE FACED BARRICADE.
 R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE", AND W14-3 "NO PASSING ZONE" signs are required in accordance with Subsection 618.03.3 and as specified in the MUTCD. If No Passing zones are 1000 ft or more, install additional "DO NOT PASS" signs on maximum spacing of 750 ft.
 FIELD CONDITIONS MAY REQUIRE SOME SIGNS ON THIS DETAIL TO BE ADJUSTED.
 THE ABOVE SHOWN ITEMS WILL BE PAID UNDER THE APPROPRIATE PAY ITEMS.

STP-0472-00(011) / 106374-301000 202-B025 Removal of Concrete Paved Ditch						
Station	Location	Width (FT)	Length (FT)	Total (SF)	Total (SY)	Remarks
218+58	RT.LN	3	9	27,000	3,000	Paved Flume
232+00	RT.LN	3	9	27,000	3,000	Paved Flume
238+85	RT.LN	3	9	27,000	3,000	Paved Flume
258+95	RT.LN	3	9	27,000	3,000	Paved Flume
276+00	RT.LN	3	9	27,000	3,000	Paved Flume
279+25	RT.LN	3	9	27,000	3,000	Paved Flume
306+00	RT.LN	3	9	27,000	3,000	Paved Flume
321+30	RT.LN	3	9	27,000	3,000	Paved Flume
323+30	RT.LN	3	9	27,000	3,000	Paved Flume
495+00	RT.LN	3	9	27,000	3,000	Paved Flume
510+75	RT.LN	3	9	27,000	3,000	Paved Flume
517+50	RT.LN	3	9	27,000	3,000	Paved Flume
596+60	RT.LN	3	9	27,000	3,000	Paved Flume
611+00	RT.LN	3	9	27,000	3,000	Paved Flume
619+50	RT.LN	3	9	27,000	3,000	Paved Flume
621+00	RT.LN	3	9	27,000	3,000	Paved Flume
630+35	RT.LN	3	9	27,000	3,000	Paved Flume
680+50	RT.LN	3	9	27,000	3,000	Paved Flume
683+00	RT.LN	3	9	27,000	3,000	Paved Flume
710+50	RT.LN	3	9	27,000	3,000	Paved Flume
215+85	LT.LN	3	9	27,000	3,000	Paved Flume
218+50	LT.LN	3	9	27,000	3,000	Paved Flume
232+00	LT.LN	3	9	27,000	3,000	Paved Flume
238+90	LT.LN	3	9	27,000	3,000	Paved Flume
258+95	LT.LN	3	9	27,000	3,000	Paved Flume
260+00	LT.LN	3	9	27,000	3,000	Paved Flume
276+42	LT.LN	3	9	27,000	3,000	Paved Flume
306+00	LT.LN	3	9	27,000	3,000	Paved Flume

STP-0472-00(01) / 106374-301000 Newton County
Failed Areas

Station	to	Station	Location	Width (FT)	Length (FT)	Area (SF)*	Removal of Concrete Overfield w/Asphalt (SY)	Asphalt, 19mm Mix (Tons)	Longitudinal	Saw Cuts	Full Depth
216+57	-	216+66	RT LANE	10	9	90.0	10.0	5.06	9		20
216+60	-	216+66	LT LANE	10	6	60.0	6.7	3.38			20
216+87	-	218+99	RT LANE	10	12	120.0	13.3	6.76	12		20
218+90	-	218+99	LT LANE	10	9	90.0	10.0	5.06			20
229+69	-	229+69	RT LANE	10	10	100.0	11.1	5.63	10		20
229+69	-	229+69	LT LANE	10	10	100.0	11.1	5.63			20
229+76	-	229+81	RT LANE	10	6	60.0	6.7	3.38	6		20
229+76	-	229+81	LT LANE	10	6	60.0	6.7	3.38			20
287+24	-	287+43	RT LANE	10	19	190.0	21.1	10.69	19		20
287+27	-	287+33	RT LANE	10	6	60.0	6.7	3.38			20
302+85	-	302+97	RT LANE	10	12	120.0	13.3	6.76	12		20
313+87	-	314+09	RT LANE	10	22	220.0	24.4	12.38	22		20
313+86	-	314+06	RT LANE	10	10	100.0	11.1	5.63			20
314+17	-	314+32	RT LANE	10	15	150.0	16.7	8.44	15		20
329+89	-	330+14	LT LANE	10	15	150.0	16.7	8.44	15		20
339+06	-	339+14	LT LANE	10	9	90.0	10.0	5.06	9		20
340+38	-	340+49	LT LANE	10	11	110.0	12.2	6.19	11		20
370+10	-	370+19	LT LANE	10	9	90.0	10.0	5.06	9		20
370+10	-	370+19	RT LANE	10	9	90.0	10.0	5.06	9		20
402+69	-	402+76	RT LANE	10	7	70.0	7.8	3.94	7		20
412+13	-	412+26	RT LANE	10	12	120.0	13.3	6.76	12		20
412+12	-	412+20	LT LANE	10	8	80.0	8.9	4.60	8		20
415+31	-	415+41	RT LANE	10	10	100.0	11.1	5.63	10		20
415+31	-	415+39	LT LANE	10	8	80.0	8.9	4.60	8		20
422+77	-	422+83	RT LANE	10	6	60.0	6.7	3.38	6		20
422+77	-	422+83	LT LANE	10	6	60.0	6.7	3.38	6		20
435+09	-	435+18	RT LANE	10	9	90.0	10.0	5.06	9		20
438+82	-	438+88	LT LANE	10	6	60.0	6.7	3.38	6		20
443+64	-	443+60	LT LANE	10	6	60.0	6.7	3.38	6		20
443+64	-	443+60	RT LANE	10	6	60.0	6.7	3.38	6		20
460+62	-	460+62	LT LANE	10	10	100.0	11.1	5.63	10		20
460+62	-	460+62	RT LANE	10	10	100.0	11.1	5.63	10		20
484+94	-	485+04	RT LANE	10	10	100.0	11.1	5.63	10		20
484+94	-	485+04	LT LANE	10	10	100.0	11.1	5.63	10		20
604+78	-	604+89	RT LANE	10	11	110.0	12.2	6.19	11		20
604+78	-	604+89	LT LANE	10	11	110.0	12.2	6.19	11		20
510+40	-	510+68	RT LANE	10	28	280.0	31.1	15.76	28		20
510+40	-	510+68	LT LANE	10	21	210.0	23.3	11.81	21		20
514+78	-	514+94	RT LANE	10	16	160.0	17.8	9.00	16		20
528+00	-	528+10	RT LANE	10	10	100.0	11.1	5.63	10		20
536+88	-	536+95	RT LANE	10	7	70.0	7.8	3.94	7		20
566+83	-	566+89	RT LANE	10	16	160.0	17.8	9.00	16		20
566+42	-	566+49	RT LANE	10	11	110.0	12.2	6.19	11		20
566+42	-	566+49	LT LANE	10	7	70.0	7.8	3.94	7		20
569+60	-	569+69	LT LANE	10	9	90.0	10.0	5.06	9		20
572+37	-	572+60	RT LANE	10	13	130.0	14.4	7.31	13		20
574+87	-	574+94	LT LANE	10	7	70.0	7.8	3.94	7		20
576+64	-	576+77	LT LANE	10	13	130.0	14.4	7.31	13		20
582+79	-	582+89	RT LANE	10	10	100.0	11.1	5.63	10		20
586+21	-	586+27	LT LANE	10	6	60.0	6.7	3.38	6		20
586+78	-	586+84	LT LANE	10	6	60.0	6.7	3.38	6		20
586+78	-	586+91	RT LANE	10	13	130.0	14.4	7.31	13		20
589+09	-	589+16	LT LANE	10	6	60.0	6.7	3.38	6		20
607+42	-	607+61	RT LANE	10	9	90.0	10.0	5.06	9		20
611+35	-	611+46	LT LANE	10	11	110.0	12.2	6.19	11		20
611+35	-	611+63	RT LANE	10	18	180.0	20.0	10.13	18		20

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Failed Areas

615+64	-	615+64	RT LANE	10	10	100.0	11.1	5.63	10	20
615+64	-	615+64	LT LANE	10	10	100.0	11.1	5.63	10	20
615+83	-	615+90	RT LANE	10	7	70.0	7.8	3.94	7	20
620+70	-	620+80	RT LANE	10	10	100.0	11.1	5.63	10	20
623+65	-	623+72	RT LANE	10	7	70.0	7.8	3.94	7	20
629+85	-	630+05	LT LANE	10	10	100.0	11.1	5.63	10	20
629+85	-	630+05	RT LANE	10	10	100.0	11.1	5.63	10	20
634+95	-	635+05	RT LANE	10	10	100.0	11.1	5.63	10	20
641+79	-	641+87	RT LANE	10	8	80.0	8.9	4.50	8	20
641+79	-	641+85	LT LANE	10	6	60.0	6.7	3.38	6	20
664+13	-	664+22	RT LANE	10	9	90.0	10.0	5.06	9	20
390+75	-	390+90	RT LANE	10	15	150.0	16.7	8.44	15	20
390+39	-	390+49	LT LANE	10	10	100.0	11.1	5.63	10	20
390+75	-	390+96	LT LANE	10	21	210.0	23.3	11.81	21	20
713+73	-	713+79	RT LANE	10	6	60.0	6.7	3.38	6	20
720+55	-	721+13	RT LANE	10	58	580.0	64.4	32.63	58	20
Total										
								865,556	583,000	1400,00
								SY	LF	LF
								438.19	583.00	1400.00
								Ton	LF	LF

STP-0472-00[011] / 106374-301000
619-D Standard Roadside Construction Signs (Less than 10 Sq. Ft.)

Station	Location	Description	Quantity	Unit	Remarks
214+88	LT, LN	G20-2A	8	SF	500' West of BOP
	RT, LN	R4-1	5	SF	
214+88	LT, LN	W14-3	5.56	SF	
	LT, LN	R4-1	5	SF	
215+92	RT, LN	W14-3	5.56	SF	
222+38	LT, LN	W14-3	5.56	SF	
	RT, LN	R4-1	5	SF	
226+01	LT, LN	R4-2	5	SF	
226+01	RT, LN	R4-2	5	SF	
227+80	LT, LN	R4-1	5	SF	
	RT, LN	W14-3	5.56	SF	
234+70	LT, LN	W14-3	5.56	SF	
	RT, LN	R4-1	5	SF	
235+30	LT, LN	R4-1	5	SF	
235+30	RT, LN	W14-3	5.56	SF	
242+20	LT, LN	W14-3	5.56	SF	
	RT, LN	R4-1	5	SF	
245+88	LT, LN	R4-2	5	SF	
249+70	LT, LN	W14-3	5.56	SF	
	RT, LN	R4-2	5	SF	
254+44	LT, LN	R4-1	5	SF	
254+44	RT, LN	W14-3	5.56	SF	
257+20	LT, LN	W14-3	5.56	SF	
257+20	RT, LN	R4-1	5	SF	
261+94	LT, LN	W14-3	5.56	SF	
261+94	RT, LN	R4-1	5	SF	
264+70	LT, LN	W14-3	5.56	SF	
264+70	RT, LN	R4-1	5	SF	
265+65	LT, LN	R4-2	5	SF	
265+65	RT, LN	R4-1	5	SF	
269+44	LT, LN	W14-3	5.56	SF	
276+40	LT, LN	W14-3	5.56	SF	
276+40	RT, LN	R4-1	5	SF	
276+69	LT, LN	R4-1	5	SF	
276+69	RT, LN	W14-3	5.56	SF	
283+90	LT, LN	W14-3	5.56	SF	
283+90	RT, LN	R4-1	5	SF	
287+42	LT, LN	R4-2	5	SF	
291+40	LT, LN	W14-3	5.56	SF	
291+40	RT, LN	R4-1	5	SF	
295+81	LT, LN	R4-2	5	SF	
299+04	LT, LN	R4-1	5	SF	
299+04	RT, LN	W14-3	5.56	SF	
305+65	LT, LN	W14-3	5.56	SF	
305+65	RT, LN	R4-1	5	SF	
306+54	LT, LN	R4-1	5	SF	
306+54	RT, LN	W14-3	5.56	SF	
313+15	LT, LN	W14-3	5.56	SF	
313+15	RT, LN	R4-1	5	SF	
317+27	LT, LN	R4-2	5	SF	
320+65	LT, LN	W14-3	5.56	SF	
320+65	RT, LN	R4-1	5	SF	
321+42	LT, LN	R4-1	5	SF	
321+42	RT, LN	W14-3	5.56	SF	
328+15	LT, LN	W14-3	5.56	SF	
328+15	RT, LN	R4-1	5	SF	
329+42	LT, LN	R4-1	5	SF	

STP-0472-00(011) / 106374-301000
619-D Standard Roadside Construction Signs (less than 10 Sq. Ft.)

Station	Location	Description	Quantity	Unit	Remarks
329+42	RT.LN	W14-3	5.56	SF	
333+51	RT.LN	R4-2	5	SF	
338+92	LT.LN	R4-1	5	SF	
336+92	RT.LN	W14-3	5.56	SF	
344+42	LT.LN	R4-1	5	SF	
344+42	RT.LN	W14-3	5.56	SF	
348+61	LT.LN	W14-3	5.56	SF	
348+61	RT.LN	R4-1	5	SF	
356+11	LT.LN	W14-3	5.56	SF	
356+11	RT.LN	R4-1	5	SF	
358+73	LT.LN	R4-2	5	SF	
360+23	LT.LN	R4-1	5	SF	
360+23	RT.LN	W14-3	5.56	SF	
363+61	LT.LN	W14-3	5.56	SF	
363+61	RT.LN	R4-1	5	SF	
367+73	LT.LN	R4-1	5	SF	
367+73	RT.LN	W14-3	5.56	SF	
371+11	LT.LN	W14-3	5.56	SF	
371+11	RT.LN	R4-1	5	SF	
375+23	LT.LN	R4-1	5	SF	
375+23	RT.LN	W14-3	5.56	SF	
378+61	RT.LN	R4-1	5	SF	
382+73	LT.LN	R4-1	5	SF	
382+73	RT.LN	W14-3	5.56	SF	
386+11	LT.LN	W14-3	5.56	SF	
386+11	RT.LN	R4-1	5	SF	
390+23	RT.LN	W14-3	5.56	SF	
390+23	LT.LN	R4-1	5	SF	
393+61	LT.LN	R4-1	5	SF	
393+61	RT.LN	W14-3	5.56	SF	
397+73	LT.LN	R4-1	5	SF	
397+73	RT.LN	W14-3	5.56	SF	
399+52	RT.LN	R4-2	5	SF	
405+23	LT.LN	R4-1	5	SF	
405+23	RT.LN	W14-3	5.56	SF	
490+65	LT.LN	W14-3	5.56	SF	
490+65	RT.LN	R4-1	5	SF	
498+15	LT.LN	W14-3	5.56	SF	
498+15	RT.LN	R4-1	5	SF	
499+13	RT.LN	R4-2	5	SF	
501+17	LT.LN	R4-2	5	SF	
502+72	LT.LN	R4-1	5	SF	
502+72	RT.LN	W14-3	5.56	SF	
510+55	LT.LN	R4-1	5	SF	
510+55	RT.LN	W14-3	5.56	SF	
510+55	LT.LN	W14-3	5.56	SF	
580+93	LT.LN	W14-3	5.56	SF	
580+93	RT.LN	R4-1	5	SF	
588+43	LT.LN	W14-3	5.56	SF	
588+43	RT.LN	R4-1	5	SF	
588+85	LT.LN	R4-2	5	SF	
588+85	RT.LN	W14-3	5.56	SF	
595+93	LT.LN	W14-3	5.56	SF	
595+93	RT.LN	R4-1	5	SF	
596+22	LT.LN	R4-1	5	SF	
596+22	RT.LN	W14-3	5.56	SF	
603+43	LT.LN	W14-3	5.56	SF	
603+43	RT.LN	R4-1	5	SF	
603+72	LT.LN	R4-1	5	SF	

STP-0472-00(011) / 106374-301000
619-D Standard Roadside Construction Signs (Less than 10 Sq. Ft.)

Station	Location	Description	Quantity	Unit	Remarks
603+72	RT/LN	W14-3	5.56	SF	
610+93	LT/LN	W14-3	5.56	SF	
610+93	RT/LN	R4-1	5	SF	
611+22	LT/LN	R4-1	5	SF	
611+22	RT/LN	W14-3	5.56	SF	
618+43	LT/LN	W14-3	5.56	SF	
618+43	RT/LN	R4-1	5	SF	
618+72	LT/LN	R4-1	5	SF	
618+72	RT/LN	W14-3	5.56	SF	
625+93	LT/LN	W14-3	5.56	SF	
625+93	RT/LN	R4-1	5	SF	
626+22	LT/LN	R4-1	5	SF	
626+22	RT/LN	W14-3	5.56	SF	
633+43	LT/LN	W14-3	5.56	SF	
633+43	RT/LN	R4-1	5	SF	
633+72	LT/LN	R4-1	5	SF	
633+72	RT/LN	W14-3	5.56	SF	
641+80	LT/LN	R4-1	5	SF	
641+80	RT/LN	W14-3	5.56	SF	
648+72	LT/LN	R4-1	5	SF	
648+72	RT/LN	W14-3	5.56	SF	
675+79	LT/LN	W14-3	5.56	SF	
675+79	RT/LN	R4-1	5	SF	
683+29	LT/LN	W14-3	5.56	SF	
683+29	RT/LN	R4-1	5	SF	
685+85	LT/LN	R4-2	5	SF	
685+85	RT/LN	R4-2	5	SF	
687+17	LT/LN	R4-2	5	SF	
687+17	RT/LN	R4-1	5	SF	
687+47	LT/LN	R4-1	5	SF	
687+47	RT/LN	W14-3	5.56	SF	
694+97	LT/LN	R4-1	5	SF	
694+97	RT/LN	W14-3	5.56	SF	
701+56	LT/LN	R4-1	5	SF	
701+56	RT/LN	W14-3	5.56	SF	
709+06	LT/LN	R4-1	5	SF	
709+06	RT/LN	R4-2	5	SF	
709+13	LT/LN	R4-2	5	SF	
714+76	LT/LN	R4-1	5	SF	
714+76	RT/LN	W14-3	5.56	SF	
716+56	LT/LN	W14-3	5.56	SF	
716+56	RT/LN	R4-1	5	SF	
729+76	LT/LN	R4-1	5	SF	
729+76	RT/LN	W14-3	5.56	SF	
731+56	LT/LN	W14-3	5.56	SF	
731+56	RT/LN	R4-1	5	SF	
737+26	LT/LN	R4-1	5	SF	
737+26	RT/LN	W14-3	5.56	SF	
739+06	LT/LN	R4-1	5	SF	
739+06	RT/LN	R4-1	5	SF	
744+76	LT/LN	W14-3	5.56	SF	
744+76	RT/LN	G70-2A	8	SF	500' East of EOP
		Total	860.76	SF	

STP-0472-00(011) / 106374-301000		Roadside Construction Signs (10 Sq. Ft. or More)			619-D Standard	
Station	Location	Description	Quantity	Unit	Remarks	
	RT.LN	G20-1	10	SF	500' West of BOP	
240+00	RT.LN	W20-1	16	SF	Potterchitto RD	
240+00	LT.LN	W20-1	16	SF	Gentry RD	
319+00	LT.LN	W20-1	16	SF	Wilbur RD	
338+60	RT.LN	W20-1	16	SF	Caraway RD	
373+00	RT.LN	W20-1	16	SF	Caraway RD	
379+00	LT.LN	W20-1	16	SF	West College ST	
396+75	LT.LN	W20-1	16	SF	HWY 503 North	
396+75	RT.LN	W20-1	16	SF	Jackson ST	
399+25	LT.LN	W20-1	16	SF	North Hickory ST	
399+25	RT.LN	W20-1	16	SF	HWY 503 South	
401+15	LT.LN	W20-1	16	SF	North College ST	
402+15	RT.LN	W20-1	16	SF	South Taylor ST	
408+00	RT.LN	W20-1	16	SF	Smede ST	
408+00	LT.LN	W20-1	16	SF	Hickory Little Rock RD	
416+55	RT.LN	W20-1	16	SF	South Boose ST	
416+70	LT.LN	W20-1	16	SF	North Boose ST	
418+80	LT.LN	W20-1	16	SF	East College ST	
421+10	LT.LN	W20-1	16	SF	Harris ST	
422+25	RT.LN	W20-1	16	SF	M.L.K Loop	
427+00	RT.LN	W20-1	16	SF	M.L.K Loop	
440+00	RT.LN	W20-1	16	SF	M.L.K Loop	
440+25	LT.LN	W20-1	16	SF	No Name RD	
440+80	RT.LN	W20-1	16	SF	Linden Lumber Co. RD	
453+60	LT.LN	W20-1	16	SF	McGee RD	
494+20	LT.LN	W20-1	16	SF	No Name RD	
494+20	RT.LN	W20-1	16	SF	Blount LN	
525+85	RT.LN	W20-1	16	SF	Buckley RD	
582+00	LT.LN	W20-1	16	SF	Truelight RD	
614+25	LT.LN	W20-1	16	SF	MT. Pleasant Church RD	
640+70	LT.LN	W20-1	16	SF	Ridge RD	
693+50	LT.LN	W20-1	16	SF	Adams ST	
693+75	RT.LN	W20-1	16	SF	Chestnut ST	
701+85	LT.LN	W20-1	16	SF	Commerce ST	
704+25	RT.LN	W20-1	16	SF	Poplar ST	
733+30	RT.LN	W20-1	16	SF	Pine Forest RD	
739+20	LT.LN	W20-1	16	SF	Clarence Culpepper RD	
	LT.LN	G20-1	10	SF	500' East of EOP	
		Total	362	SF		

STP-0472-00(011) / 106374-301000

Saw Cuts

Station	Location	Length	Longitudinal Saw Cuts (LF)	Remarks
218+58	RT.LN	9X18	27	Paved Flume
232+00	RT.LN	9X18	27	Paved Flume
238+85	RT.LN	9X18	27	Paved Flume
258+95	RT.LN	9X18	27	Paved Flume
276+00	RT.LN	9X18	27	Paved Flume
279+25	RT.LN	9X18	27	Paved Flume
306+00	RT.LN	9X18	27	Paved Flume
321+30	RT.LN	9X18	27	Paved Flume
323+30	RT.LN	9X18	27	Paved Flume
495+00	RT.LN	9X18	27	Paved Flume
510+75	RT.LN	9X18	27	Paved Flume
517+50	RT.LN	9X18	27	Paved Flume
596+60	RT.LN	9X18	27	Paved Flume
611+00	RT.LN	9X18	27	Paved Flume
619+50	RT.LN	9X18	27	Paved Flume
621+00	RT.LN	9X18	27	Paved Flume
630+35	RT.LN	9X18	27	Paved Flume
680+50	RT.LN	9X18	27	Paved Flume
683+00	RT.LN	9X18	27	Paved Flume
710+50	RT.LN	9X18	27	Paved Flume
215+85	LT.LN	9X18	27	Paved Flume
218+50	LT.LN	9X18	27	Paved Flume
232+00	LT.LN	9X18	27	Paved Flume
238+90	LT.LN	9X18	27	Paved Flume
258+95	LT.LN	9X18	27	Paved Flume
260+00	LT.LN	9X18	27	Paved Flume
276+42	LT.LN	9X18	27	Paved Flume
306+00	LT.LN	9X18	27	Paved Flume
362+30	LT.LN	9X18	27	Paved Flume
374+33	LT.LN	9X18	27	Paved Flume
379+10	LT.LN	9X18	27	Paved Flume
495+00	LT.LN	9X18	27	Paved Flume
505+00	LT.LN	9X18	27	Paved Flume
508+40	LT.LN	9X18	27	Paved Flume
517+50	LT.LN	9X18	27	Paved Flume
596+65	LT.LN	9X18	27	Paved Flume
599+85	LT.LN	9X18	27	Paved Flume

STP-0472-00(011) / 106374-301000

Saw Cuts

Station	Location	Length	Longitudinal Saw Cuts (LF)	Remarks
611+00	LT.LN	9X18	27	Paved Flume
621+00	LT.LN	9X18	27	Paved Flume
653+30	LT.LN	9X18	27	Paved Flume
683+00	LT.LN	9X18	27	Paved Flume
225+10	RT.LN	44X51	95	Concrete Driveway
348+50	RT.LN	25X29	54	Concrete Driveway
405+90	RT.LN	25X30	55	Concrete Driveway
406+00	RT.LN	26X28	54	Concrete Driveway
601+30	RT.LN	24X29	53	Concrete Driveway
64300	RT.LN	28X35	63	Concrete Driveway
687+90	RT.LN	18X25	43	Concrete Driveway
689+50	RT.LN	23X33	56	Concrete Driveway
691+40	RT.LN	17X23	40	Concrete Driveway
735+25	RT.LN	23X28	51	Concrete Driveway
224+74	LT.LN	25X35	60	Concrete Driveway
351+75	LT.LN	15X20	35	Concrete Driveway
468+50	LT.LN	31X43	74	Concrete Driveway
500+00	LT.LN	23X33	56	Concrete Driveway
680+00	LT.LN	24X30	54	Concrete Driveway
689+50	LT.LN	27X35	62	Concrete Driveway
697+20	LT.LN	20X25	45	Concrete Driveway

2057

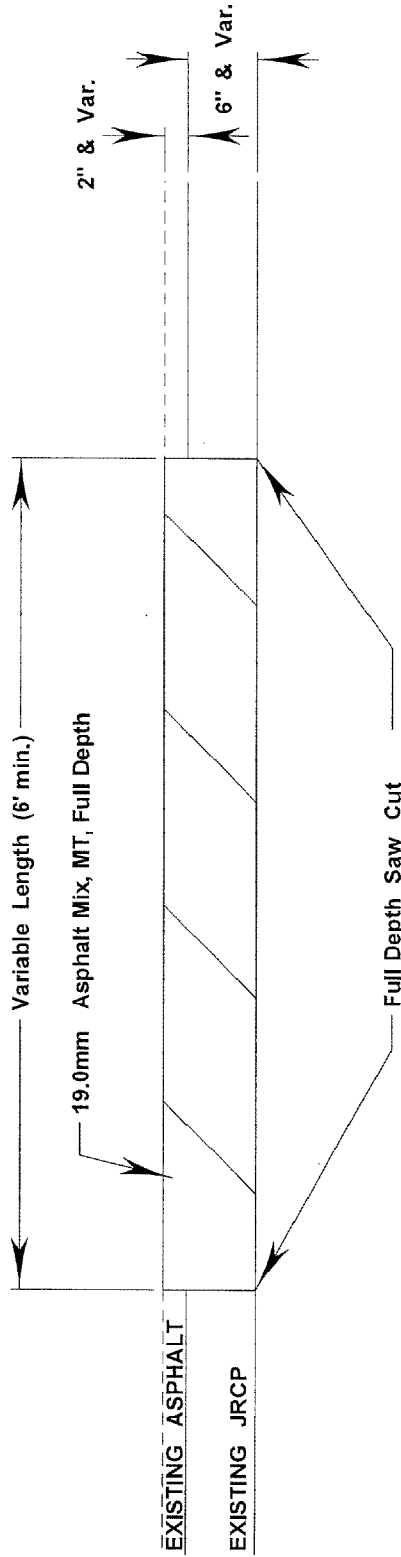
Total

STP-0472-00(011) / 106374-301000		Barricades Double-Faced			619-G Type III	
Location	Station	Quantity	Unit	Description		
LT.LN	500' West of BOP	6	LF			
RT.LN	500' West of BOP	6	LF			
LT.LN	500' East of EOP	6	LF			
RT.LN	500' East of EOP	6	LF			
	Total	24	LF			

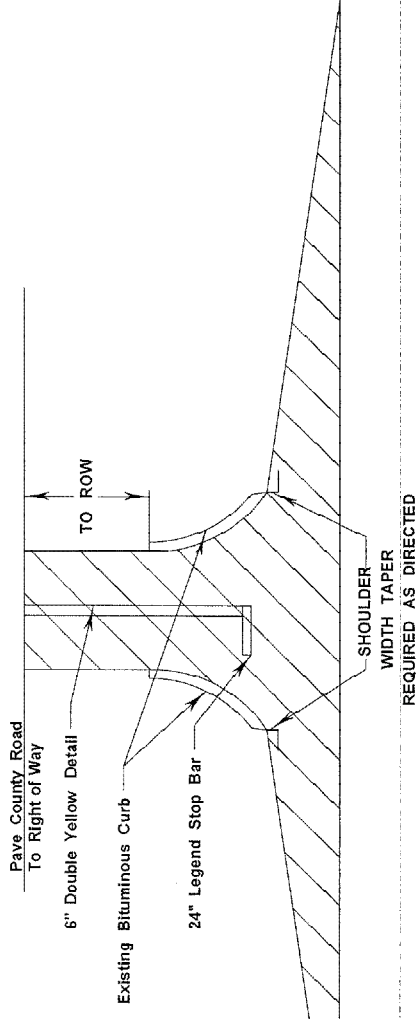
STP-0472-00(011)/106374-301000 GUARD RAIL REQUIRED												
WK. NO.	STATION	Location	GUARDRAIL		TERMINAL END SECTION	BRIDGE END SECTION TYPE E	SINGLE DELINEATORS		Removal of Guardrail	REMARKS		
			W-BEAM				WHITE	YELLOW				
	307+70	RT.LN			1		5		37.5			
	308+31	LT.LN	25.0		1		4		50.0			
	309+73	RT.LN	25.0		1		4		50.0			
	309+48	LT.LN			1		5		37.5			
	545+51	RT.LN			1		5		25.0			
	546+11	LT.LN	25.0		1		4		50.0			
	548+19	RT.LN	25.0		1		4		50.0			
	547+94	LT.LN			1		5		37.5			
	551+58	RT.LN			1		5		37.5			
	552+20	LT.LN	25.0		1		4		37.5			
	554+63	RT.LN	25.0		1		4		50.0			
	554+37	LT.LN			1		5		37.5			
	558+33	RT.LN			1		5		37.5			
	558+94	LT.LN	25.0		1		4		50.0			
	561+00	RT.LN	25.0		1		4		50.0			
	560+75	LT.LN			1		5		37.5			
	663+17	RT.LN			1	1	5		62.5	Remove BES in addition to TES		
	662+18	LT.LN			1		8		37.5			
	665+53	RT.LN			1		5		37.5			
	665+28	LT.LN	75.0		1	1	5		137.5	Remove 75' of GR, BES, in addition to TES		
	720+40	RT.LN			1		5		37.5			
	721+03	LT.LN	25.0		1		4		50.0			
	727+08	RT.LN	25.0		1		4		50.0			
	726+94	LT.LN			1		5		37.5			
UNITS			L. F.		EACH	EACH	EACH	EACH	EACH	EACH		
TOTALS			325.0		24	2	113	0	1125			

Newton County
STP-0472-00(011)/ 106374-301000

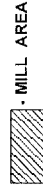
Concrete Pavement Repair Detail



NEWTON COUNTY
 STP-0472-00(011)/106374-301000
 TYPICAL SECTION
 OF MILLING COUNTY ROADS WITH
 CURB AND GUTTER



- MILL FULL DEPTH 2"
 - PLACE 2" ASPHALT PAVEMENT TO
 TIE TO MAINLINE OVERLAY.

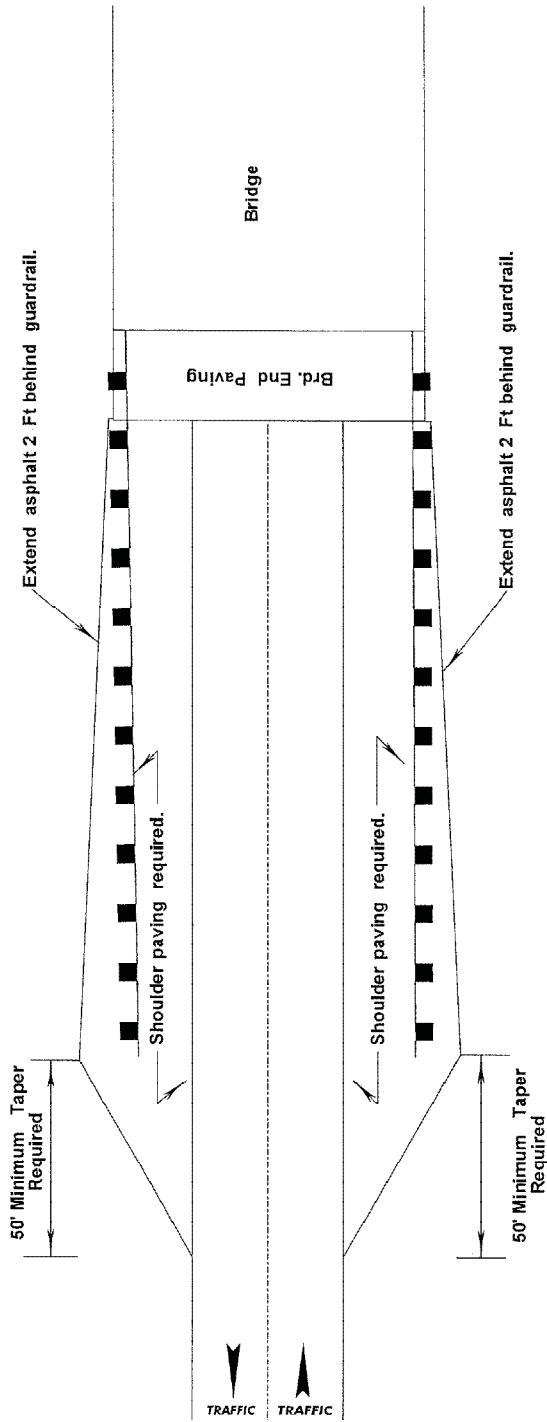


Location

- | | |
|-----------------------------------|--------------------------------|
| Potterchitto Road | East Church Street |
| Gentry Road | West Martin Luther King Street |
| Wilbur Road | West Church Street |
| East Caraway Road | Linden Lumber Road |
| East College Street | McGee Road |
| West Caraway Road | Blount Lane |
| South Jackson Street | Buckley Road |
| Highway 503(North Jackson Street) | True Light Road |
| West College Street | Mount Pleasant Church Road |
| Jefferson Street | Ridge Road |
| Taylor Street | Chestnut Street |
| South Smede Street | Adams Street |
| North Smede Street | Mulberry Street |
| (Hickory Little Rock Road) | Commerce Street |
| South Booze Street | Popular Street |
| North Booze Street | Pineforest Road |
| Harris Street | Clarence Culpepper Road |
| East Martin Luther King Street | |

NEWTON COUNTY
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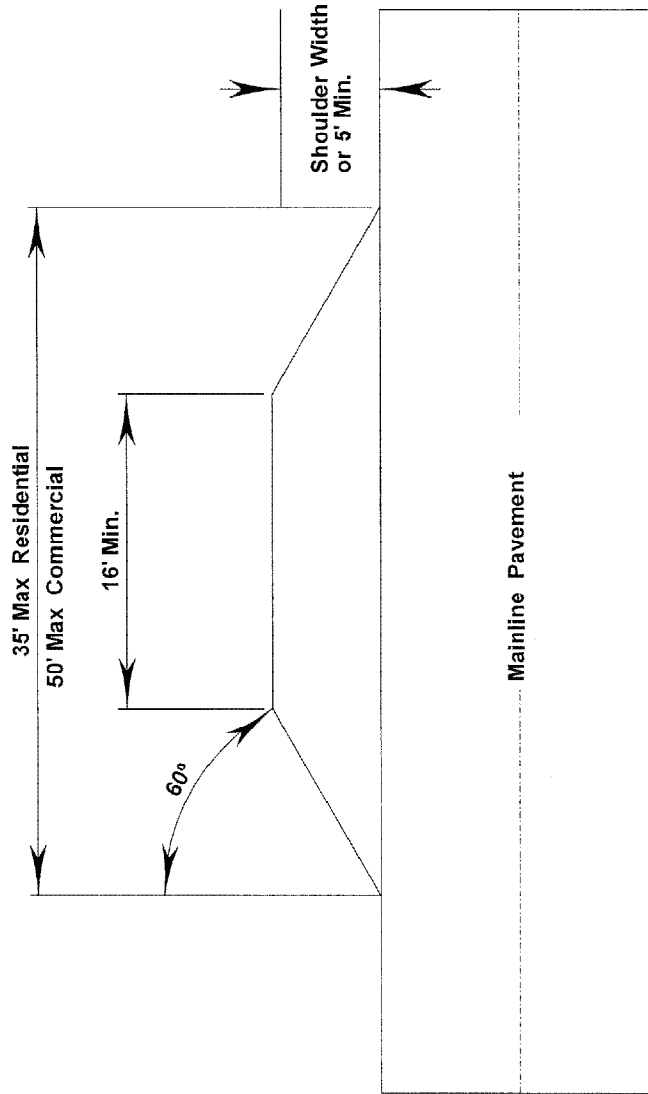
TYPICAL DETAIL OF ADDITIONAL SHOULDER PAVING
REQUIRED AT GUARDRAIL LOCATIONS



*Asphalt Thickness = 3"
See Scope of Work for additional Details

**NEWTON COUNTY
STP-0472-00(011)/ 106374-301000**

DRIVEWAY PAD DETAIL

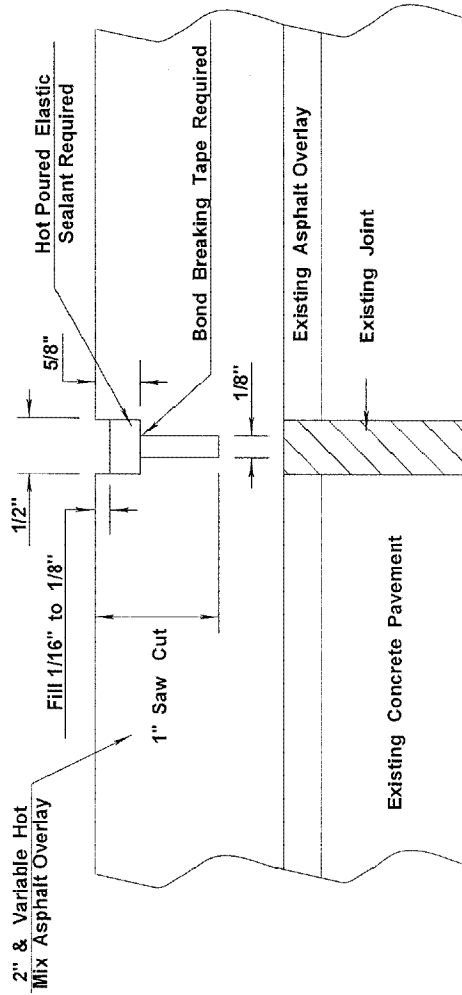


Note:

The HMA on the existing driveway/ramp pads are to remain in their current, size, and location. If, in the opinion of the engineer, a pad should be modified or replaced, payment will be made for the work using the appropriate pay items. Granular Material and/or stabilizer aggregate should be placed around the pads as required.

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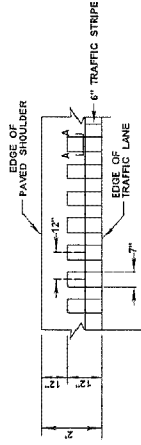
DETAIL OF SAWING AND SEALING TRANSVERSE/LONGITUDINAL JOINTS



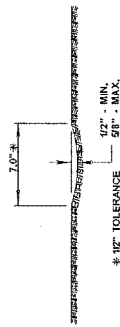
SEE INCLUDED SPECIAL PROVISIONS FOR DETAILS OF CLEANING AND FILLING JOINTS AND SAWING AND SEALING JOINTS.

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SPECIAL DESIGN:
RUMBLE STRIPES (GROUND-IN)
2 LANE

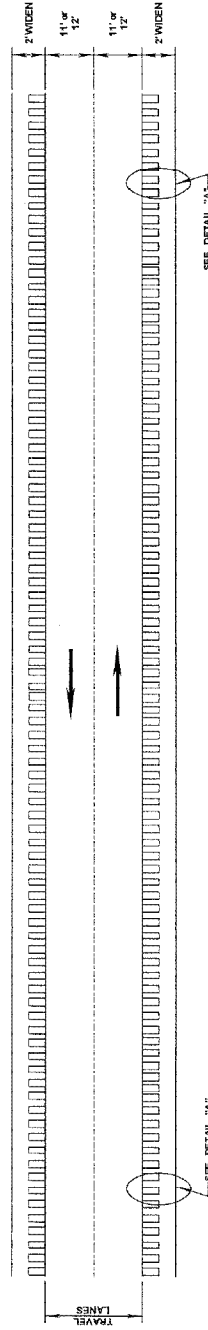


DETAIL "A"



SECTION "A-A"

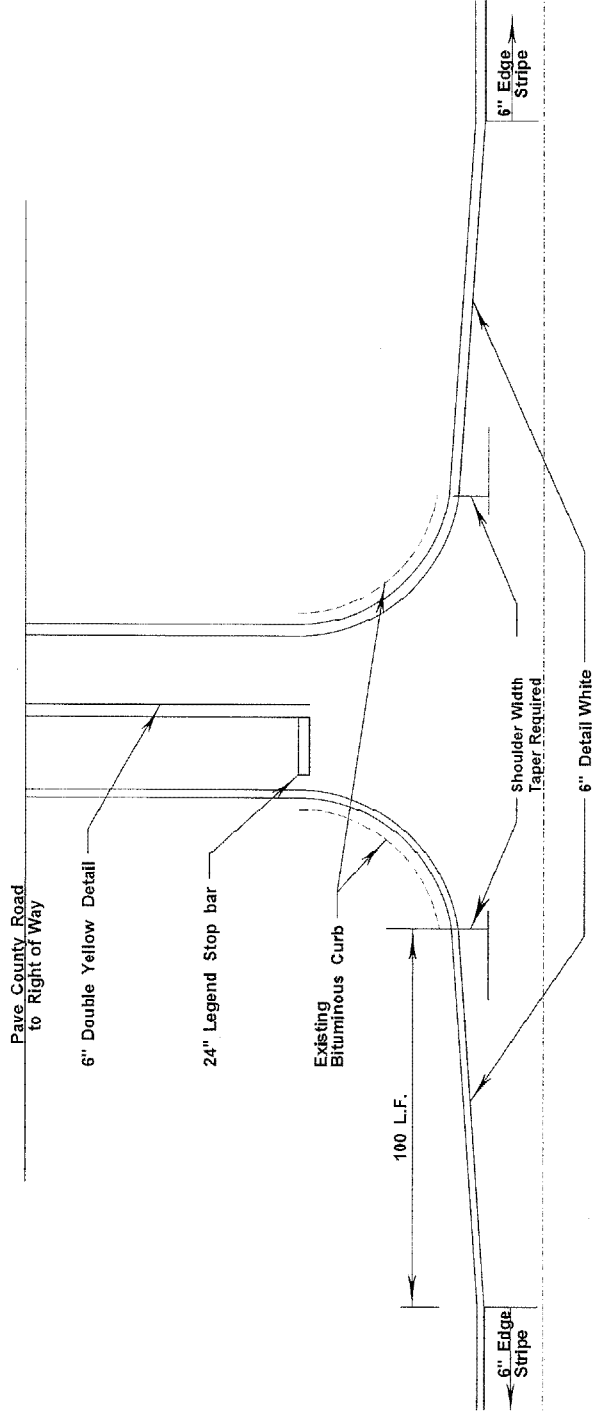
- GENERAL NOTES**
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
 - GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 - COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.
 - DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.



PLAN
 NOT TO SCALE

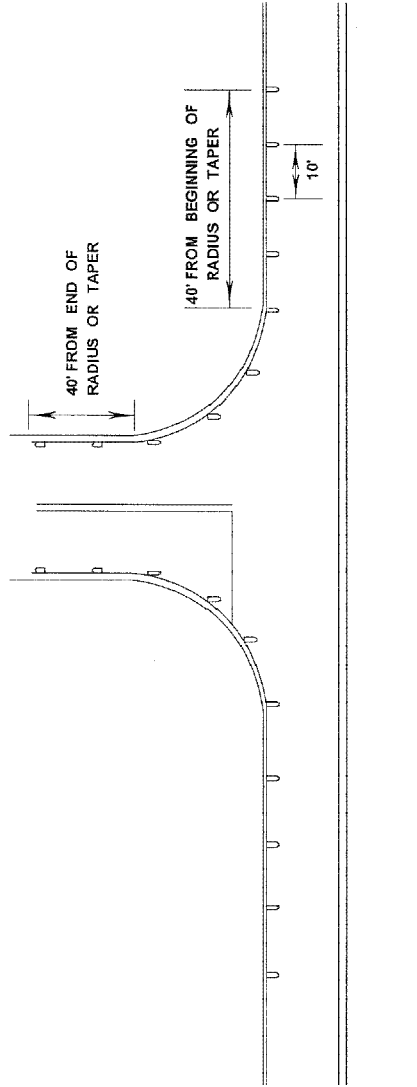
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COUNTY ROAD PAVING/STRIPING DETAIL



*SEE ATTACHED SHEET FOR TWO-WAY CLEAR RPM DETAIL

NEWTON COUNTY
STP-0472-00(011)/ 106374-301000
TYPICAL FOR RAISED PAVEMENT MARKERS
PLACED ON SIDE ROAD RADIUS



- NOTE 1. MARKERS SHALL BE PLACED EVERY 10 FEET.
- NOTE 2. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
- NOTE 3. MARKERS SHALL BE HIGH PERFORMANCE TWO WAY CLEAR.
- NOTE 4. FIVE (5) MARKERS SHALL BE PLACED ALONG MAINLINE EDGE STRIPE.
- NOTE 5. MARKERS FOR COUNTY ROADS SHALL CONTINUE DOWN THE EDGE STRIPE A DISTANCE OF 40 FEET.