

**SECTION 905 -- PROPOSAL (CONTINUED)**

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) enclose a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.   1   DATED   4/15/2013   ADDENDUM NO.            DATED             
 ADDENDUM NO.   2   DATED   4/17/2013   ADDENDUM NO.            DATED           

Number	Description
1	Revise Table of Contents, replace same; Add Supplement to NTB No. 2596; Revise Supplement to NTB No. 4103, replace same; Revise wage rates, replace same; Add Supplement to SP No. 907-413-4; Amendment EBS Download Required.
2	Revise NTB No. 4401, replace same; Amendment EBS Download Required.

TOTAL ADDENDA:   2    
 (Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE \_\_\_\_\_

\_\_\_\_\_  
 Contractor

BY \_\_\_\_\_  
 Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.

Revised 09/21/2005

STP-0456-00(005) / 106493301

Lauderdale County(ies)

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 - NOTICE TO BIDDERS NO. 4401**

**CODE: (SP)**

**DATE: 3/05/2013**

**SUBJECT: Scope of Work**

**PROJECT: STP-0456-00(005) / 106493301 – Lauderdale County**

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings". All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following:

Mill and overlay approximately 3.5 miles of existing asphalt pavement on SR 854 from SR 39 (Station 23+47) to the Naval Air Station (Station 199+65) in Lauderdale County. The left lane has a station equation as follows: (Station 150+55 Back – Station 153+95 Ahead).

## **From the BOP at Station 23+47 to EOP at Station 199+65**

The work shall consist of cold milling the existing asphalt pavement to a depth of 1½" and overlaying with 2" and variable of 12.5-mm Mix, ST. In areas where the existing cross-slope is less than 2%, the overlay shall be placed 2¼" at the centerline and 1½" at the edge of the pavement. The intent is to correct the cross-slope to 2% or required Super Elevation. The existing slope may vary in areas and paving thicknesses should be adjusted accordingly. Existing paved cross-overs shall be milled 1½" and inlaid with 1½" of 12.5-mm Mix, ST.

The existing pavement is to be widened two (2) linear feet on each lane as directed by the Engineer. Asphalt for this widening is to be placed full depth, five (5) inches, concurrently with the placement of the surface lift mainline overlay. Prior to placement of the asphalt, the shoulders are to be bladed to provide a suitable paving surface; the cost of which is to be included in the price bid for other items. Any material excavated from the existing shoulder shall be used to raise the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, foreslopes, or other adjacent areas as directed by the Engineer and will be an absorbed item.

Publicly maintained roads and streets should be milled and inlaid to the existing right-of-way with 1½" lift 12.5-mm, ST. Privately owned entrances shall be paved to the shoulder line per the included typical drawing.

Prior to milling and inlays operations, failed areas listed in the attached schedule shall be removed and replaced with HMA/WMA 12.5-mm, ST as directed by the Engineer. Payment for the pavement removal and the asphalt shall be made under the appropriate pay item.

**Guardrail Replacement:**

This work shall consist of replacing the guardrail bridge end sections at the bridge approaches as shown in the attached table. All guardrail bridge end sections removed are to be replaced the same day and prior to reopening the adjacent lane of traffic. Voids created by the removal of posts, concrete anchors, footing, etc. shall be backfilled in accordance with Section 203 of the Standard Specifications.

**General Notes:**

Milling will not begin until an **approved** asphalt mix design has been received, nor until such time that, in the opinion of the Engineer, weather conditions have been consistently suitable enough to allow placement of the HMA/WMA after the milling operations. Milling will be limited to an area in a single lane established by the Engineer, which in his judgment, can be overlaid the same day. The Contractor will not exceed that area. **The milled lane shall not be opened to traffic until the asphalt has been replaced.** All roadway not overlaid the same day due to unforeseen circumstances shall be protected by an overnight lane closure. The cost of all items involved in this closure is to be included in the price bid for Pay Item No. 618-A, Maintenance of Traffic.

Temporary pavement transition joints (paper joints) shall be at least three (3) paper-widths long and shall be adequately maintained. Sand or dirt form the edge of roadway shall not be used as a substitute for treated paper

Potholes that may exist or occur in the existing pavement are to be patched in a timely manner and prior to beginning the asphalt overlay. Patching of potholes shall be considered an absorbed item.

The HMA on the Bridge at station 182+12 left lane shall be milled and overlaid. Following the overlaying operation, transverse joints in the HMA/WMA shall be sawed and sealed. The sawcut joints shall be directly over the existing Bridge joints. The sawing and sealing shall be done as soon as possible to avoid premature cracking above the bridge joints. The work shall conform to the specifications shown in Subsection 413.03.3 of the Standard Specifications and the typical and details for sawing and sealing in asphalt pavement included in this contract.

A minimal amount of Class 5, Group E, Granular Material has been included to provide a means of correcting any deficiencies on the existing shoulders. Placement of the granular material on the finished asphalt course shall not be permitted. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%). Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum amount of material placed. Removal of the existing shoulder material shall be coincident with the milling/overlaying operation to prevent the possible ponding of water. No payment will be made for blading or removal of the existing shoulder material. Any material excavated from the existing shoulder shall be used to raise the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, foreslopes, or other adjacent areas as directed by the Engineer and will be an absorbed item.

Size I Stabilizer Aggregate is to be placed on existing driveways behind asphalt ramps only as needed and as directed by the Engineer.

Existing raised pavement markers are to be removed prior to beginning the overlay operation. No measurement will be made for separate payment; the cost is to be included in the prices for other items bid. Permanent pavement markers are to be placed in accordance with the attached drawings and Standard Drawings. Two-way yellow markers are to be placed on two-way roads and county roads. Two-way clear markers are to be placed on county roads.

**Prior to opening area to traffic, temporary stripe, either paint or tape, will be required immediately after overlaying operations.** Separate payment will be made for temporary traffic stripe under the appropriate bid schedule pay items. Temporary stripe shall be placed in the same location and layout as permanent stripe. Temporary edge stripe may be delayed for a period not to exceed three (3) days.

All permanent striping will be thermoplastic. The width of the permanent stripe will be six inches (6"). High performance raised pavement markers will be placed at 80-foot intervals in tangents and 40-foot intervals in curves and as required at entrance / exit ramps.

Ground-In-Rumble Stripe will be required on the left and right pavement edges per the attached special drawing.

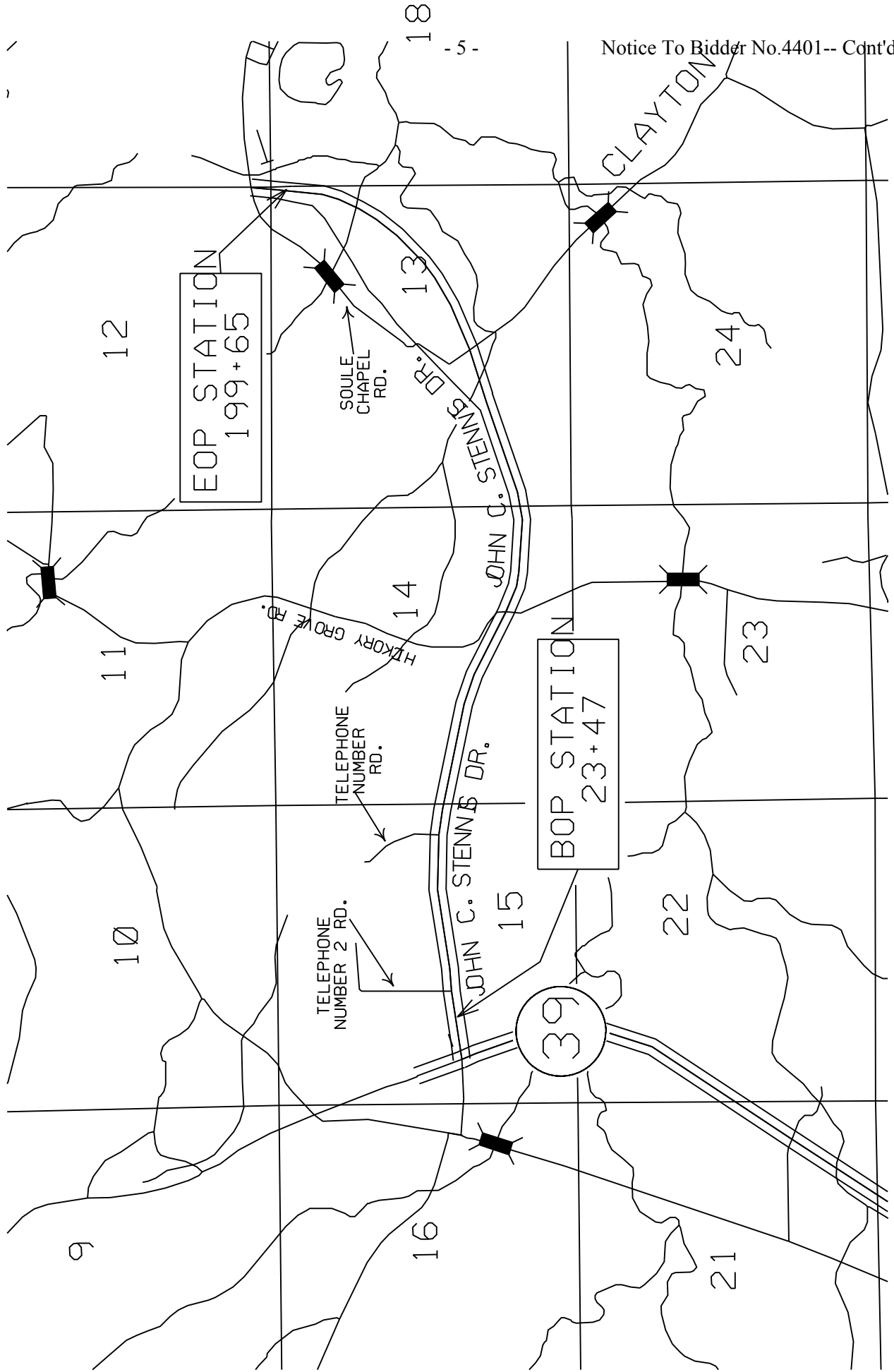
The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost is to be included in the price bid for Pay Item No. 618-A, Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background. Standard roadside construction signs and barricades will be paid for using the appropriate pay items.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal; the cost is to be included in the prices of items bid. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor.

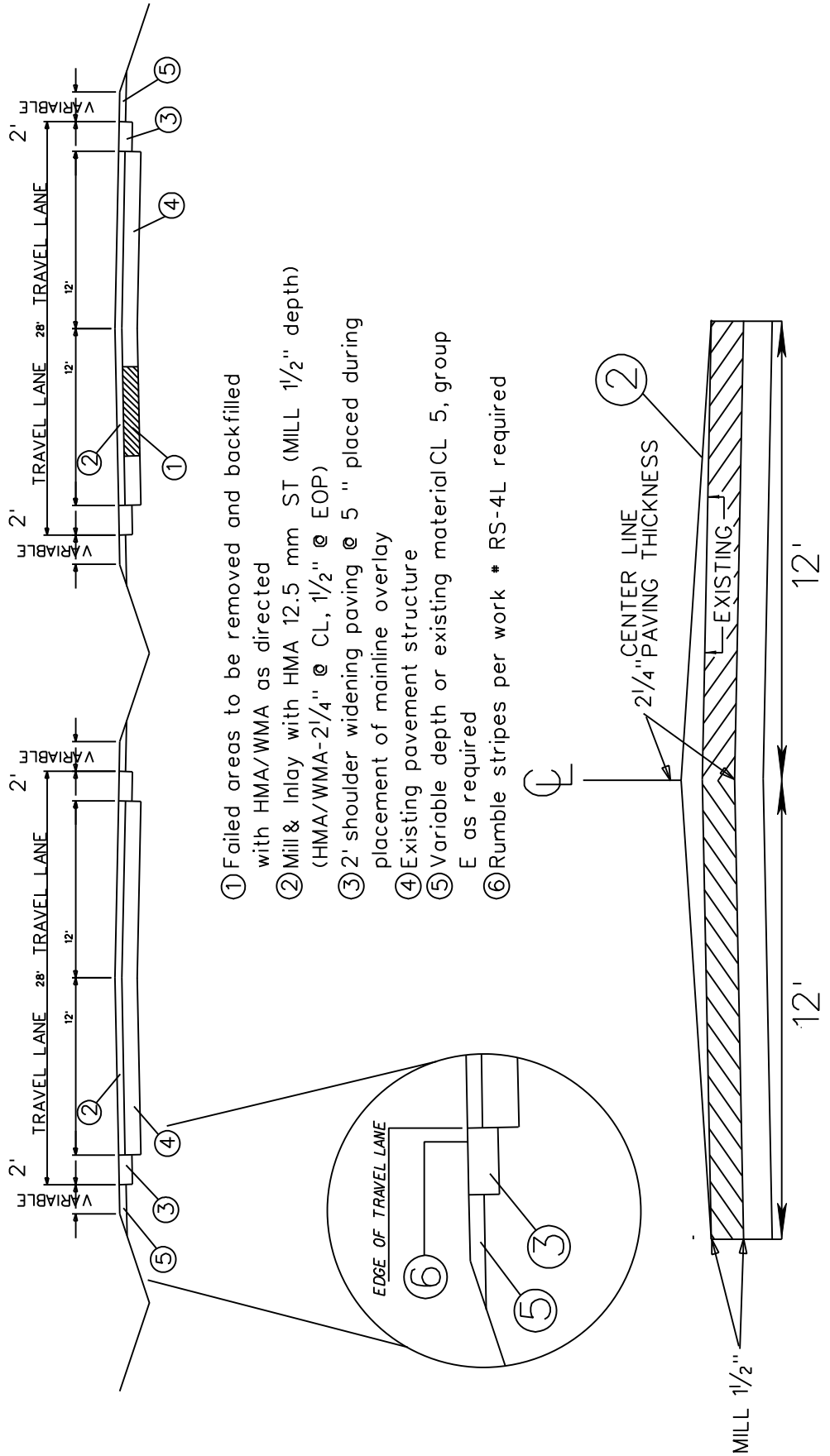
Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid. Any signs or mailboxes that are affected shall be replaced the same day that they are removed and at the earliest practical time.

It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, paved ditches, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the engineer, any structures damaged during the life of the contract. No payment will be made for replacement or repair of damaged items.

Roadside construction signs, barrels, etc. shall be placed in accordance with the attached drawings or as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed.

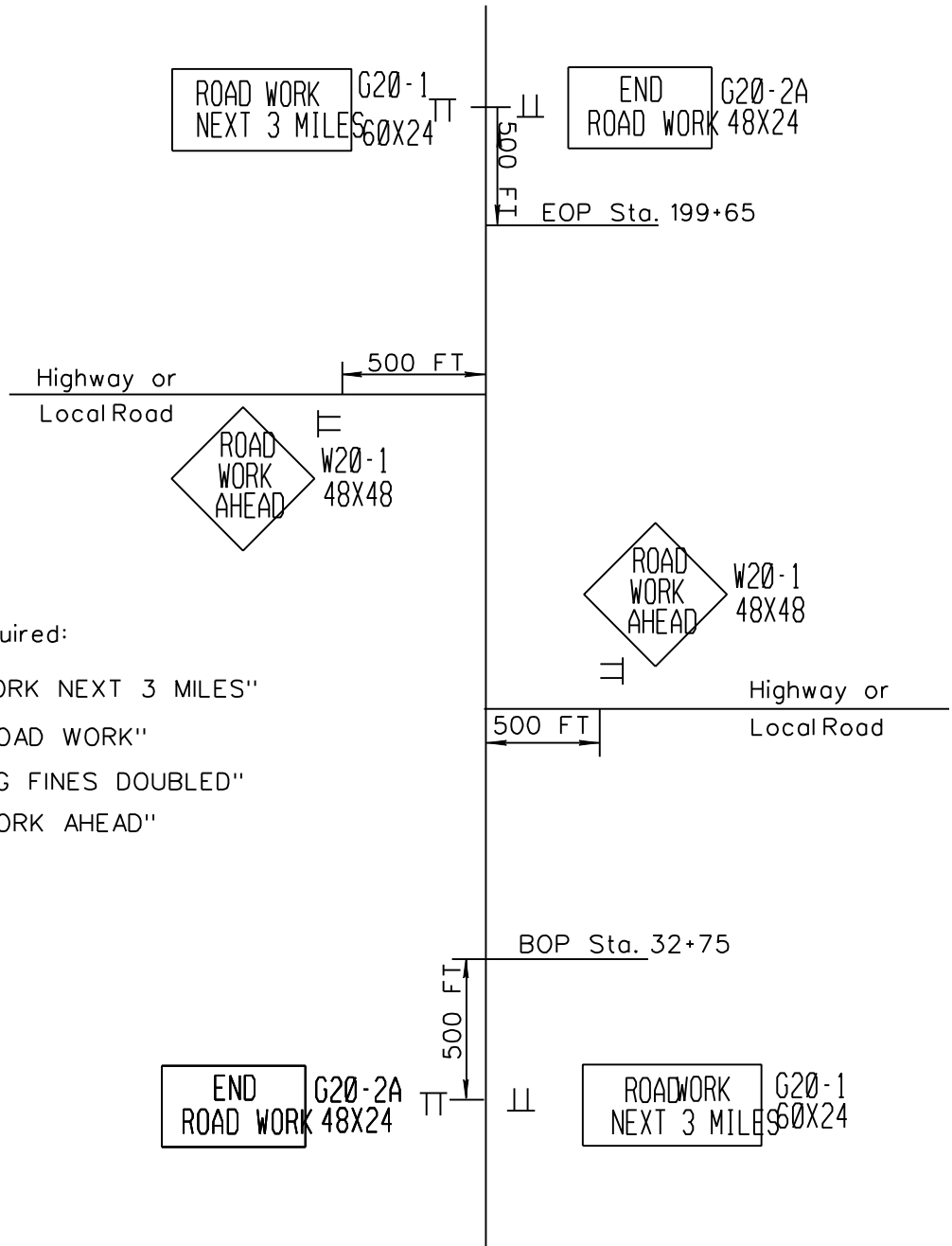


TYPICAL SECTION  
 Sta 23+47 to 199+65  
 HWY 854 (STENNIS DRIVE)



- ① Failed areas to be removed and backfilled with HMA/WMA as directed
- ② Mill & Inlay with HMA 12.5 mm ST (MILL 1 1/2" depth) (HMA/WMA-2 1/4" @ CL, 1 1/2" @ EOP)
- ③ 2' shoulder widening paving @ 5" placed during placement of mainline overlay
- ④ Existing pavement structure
- ⑤ Variable depth or existing material CL 5, group E as required
- ⑥ Rumble stripes per work # RS-4L required

# CONSTRUCTION SIGNING DETAIL



### Traffic Control Signs Required:

- 4 - G20-1 "ROAD WORK NEXT 3 MILES"
- 2 - G20-2a "END ROAD WORK"
- 14 - R16-3 "SPEEDING FINES DOUBLED"
- 13 - W20-1 "ROAD WORK AHEAD"

NOTES: One (1) W20-1 "ROAD WORK AHEAD" Sign is Required at each Local Road, street or highway Entering the Project.  
 Location of additional W20-1 (ROAD WORK AHEAD) signs are shown on the attached table.  
 G20-1 and G20-2a signs mounted on Type III Single Faced Barricade.  
 Placed a minimum of 500' before the BOP and Eop.  
 Additional G20-1 required in median at the EOP.





W20-1 W20-1 "ROAD WORK AHEAD"  
48X48

Additional W20-1 (ROAD WORK AHEAD) Signs

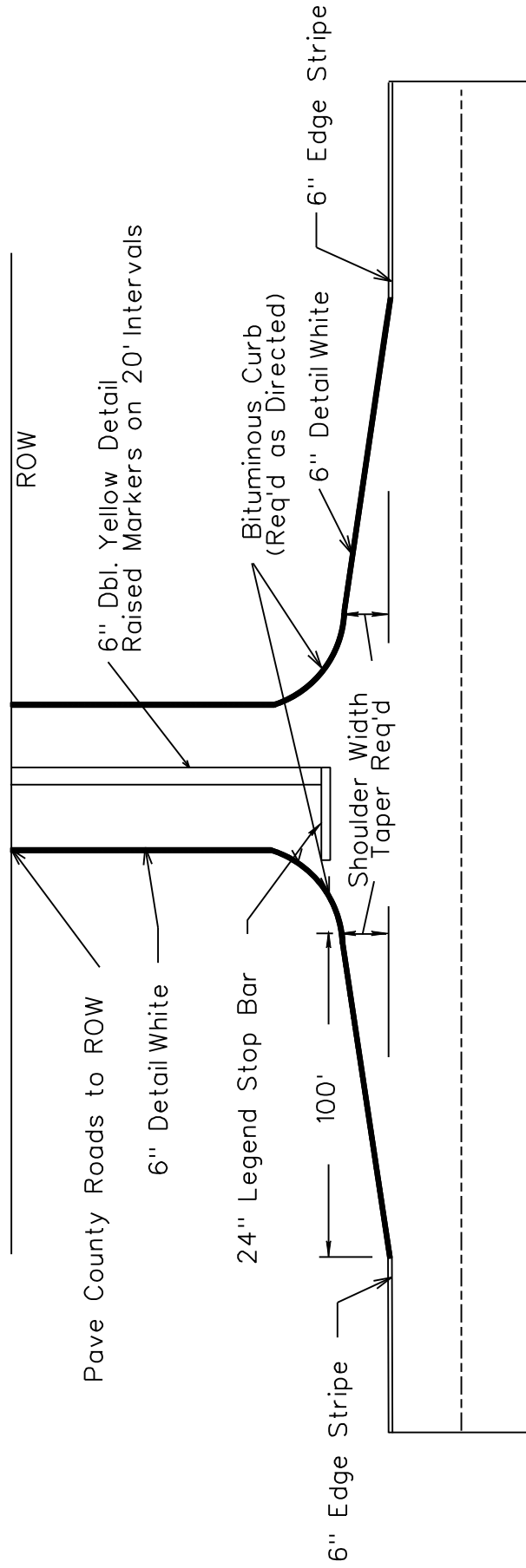
STENNIS DRIVE ( H 854)

EAST BOUND	WEST BOUND
HWY 39 RAMP	SOULE CHAPEL RD.
HWY 39 TURN LN.	SOULE CHAPEL RD.
LIZELIA ROAD	HICKORY GROVE RD.
CHIP PICKERING RD.	HICKORY GROVE RD.
RABBIT RD.	EAST TELEPHONE RD.
	WEST TELEPHONE RD.

<b>Failed Areas</b>				
Station	to	Station		Square Yards (SY)
137+75		138+50	Right of Right Lane	100.00
147+50		148+00	Right of Right Lane	66.67
150+00		151+00	Right of Right Lane	133.33
Total =				300.00

<b>Guardrail Quantities</b>			
Station	Location	Number	Type
182+00	Rt of Rt. Lane	1	Type "D" Modified
182+00	Lt of Lt. Lane	1	Type "D" Modified

### Typical Section - County Roads

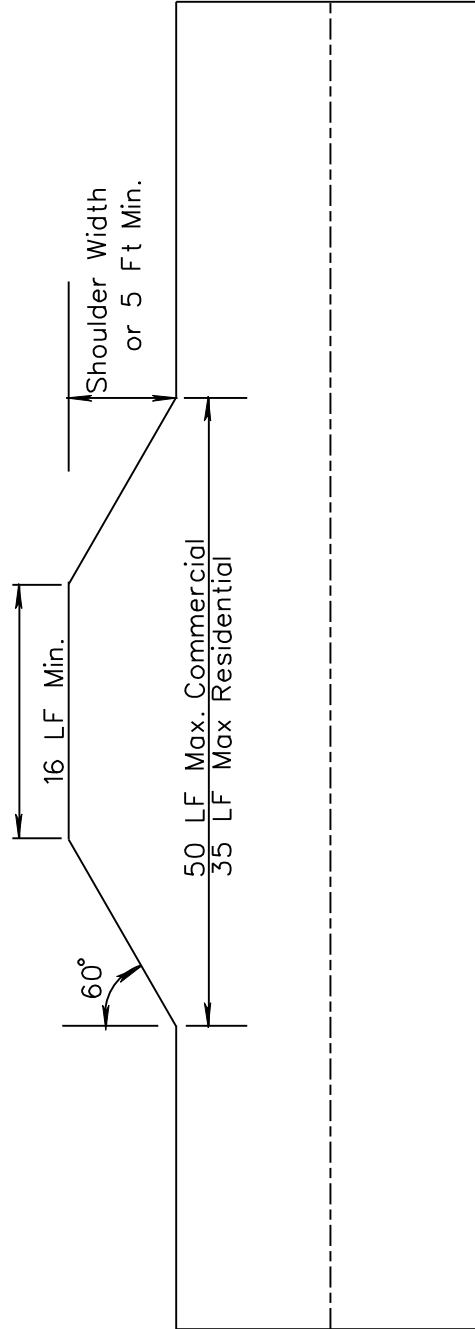


\*SEE ATTACHED SHEET FOR TWO-WAY CLEAR RPM DETAIL

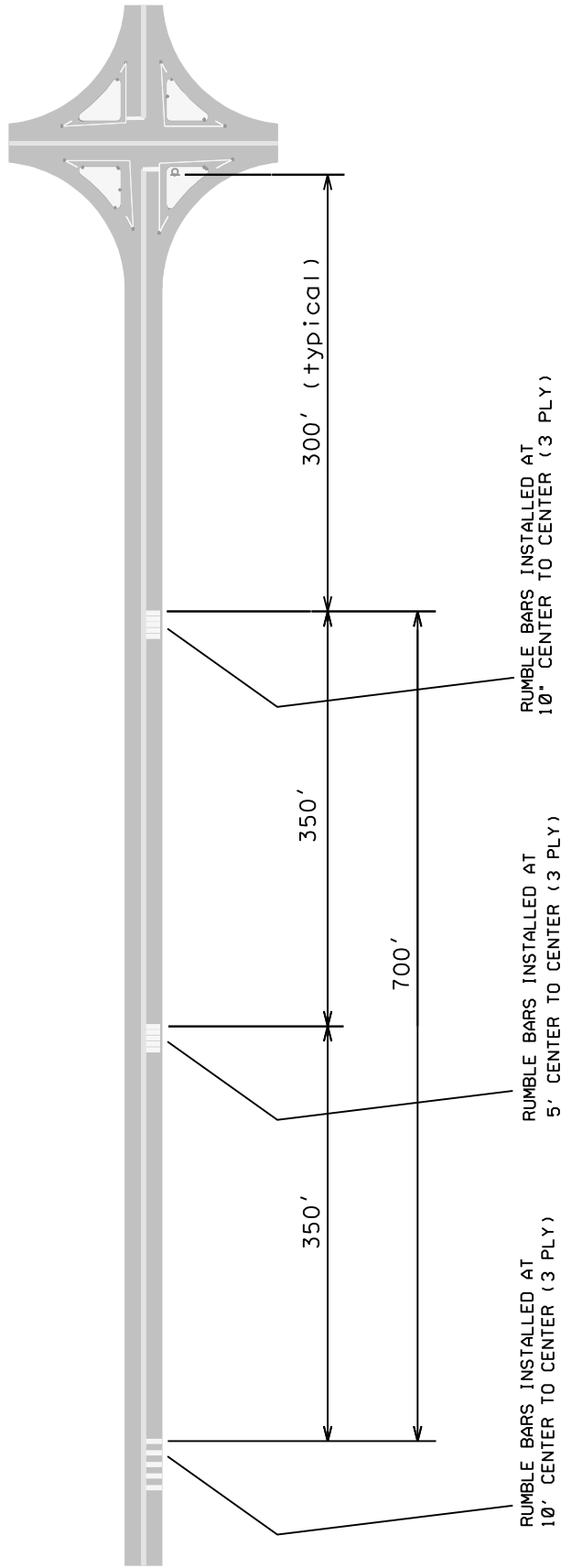




TYPICAL RAMP/PAD DETAIL







THIS DRAWING IS NOT TO SCALE

- NOTES:  
Install rumble bars as shown :
1. 1 set of rumble bars approx. 250' from STOP AHEAD
  2. 1 set of rumble bars approx. 300' from STOP
  3. 1 set of rumble bars approx. halfway between first and last set
  4. Rumble bars to be 6" thermoplastic legend ( 120 mil/each ply )
  5. 5 rumble bars per set minimum
  6. Installation may vary due to terrain



