

**SECTION 905 -- PROPOSAL (CONTINUED)**

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) enclose a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.   1   DATED   6/18/2013   ADDENDUM NO.            DATED             
 ADDENDUM NO.            DATED            ADDENDUM NO.            DATED           

Number	Description
1	Revise NTB No. 4501, replace same; Amendment EBS Download Required.

TOTAL ADDENDA:   1    
 (Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE \_\_\_\_\_

\_\_\_\_\_  
 Contractor

BY \_\_\_\_\_  
 Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.

MP-6018-31(010) / 304810301 Jasper County(ies)

Revised 09/21/2005

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 4501

CODE: (SP)

DATE: 05/01/2013

SUBJECT: Scope of Work

PROJECT: MP-6018-31(010) / 304810301 -- Jasper County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings". All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following.

### SR 18

#### LEVEL AND OVERLAY FROM SMITH COUNTY LINE EAST 8.3 MILES

##### 10+00 – 116+08 (R1) 10+00 – 348+00 (R2)

(A) Prior to the overlay, centerline alignment shall be determined by the Contractor by measuring the existing roadway at 500-foot intervals in tangent sections, and 100-foot intervals in horizontal curves. The existing shoulders shall be clipped and surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Project Engineer, and will be an absorbed item. The brick section in at the intersection of SR 18 and 3<sup>rd</sup> Street (Station 15+85 to Station 16+57) shall be omitted.

(B) Cold Mill 1.5" & Variable on SR 18 at all the transitions including E.O.P., B.O.P., bridge approaches, and all tie-ins. Cold Mill 1.5" for the entire road width from station 110+04 (R1) thru the city of Bay Springs to station 23+00 (R2). Vehicle Assembly Loops shall be placed in the underlying layer of asphalt prior to placing the surface lift. Work performed within this section shall be performed between the hours of 7:00 PM and 6:00 AM. All milled areas shall be backfilled with asphalt within 48 hours of milling.

(C) Overlay SR 18 with 1.5" of 9.5-mm, MT, asphalt mixture from the Smith County line 8.3 miles east. Prior to the overlay, a leveling course of ¾" & variable of 9.5-mm, MT, asphalt shall be required. Leveling will not be performed thru the milling limits within the City of Bay Springs. Smoothness incentive/disincentive will be governed by special provision for MRI (mean roughness index). All incentive/disincentive will be based on theoretical tonnage placed on a 12-foot travel lane. Remove any failed areas on the main facility and repair by backfilling with 19-mm, MT, asphalt as directed by the Project Engineer. Removal areas will be marked by MDOT personnel and include, but are not limited to, areas included in Table 1. Approximately 550 tons of 19-mm, MT, asphalt will be used for backfill in all removal sections. Publicly maintained roads or streets shall be surfaced to the existing R.O.W.; privately owned entrances shall be surfaced a

distance of 10 feet & variable from edge of pavement. Any site grading at local roads or drives will not be measured for separate payment but will be considered an absorbed item. Cross slopes shall be maintained to achieve a uniform cross slope of 2% where practical with plan quantities. The Contractor will mill a 12-inch rumble strip along the inside of the shoulders and place the traffic stripe on the inside six inches (6") of the rumble strip. If water stands when project is complete, the contractor shall correct at no additional cost to the State.

(D) Raise the existing shoulders to match the new pavement elevation by placing 2.25" & variable depth crushed stone on the shoulders. Placement of the crushed stone on the finished surface course shall not be permitted. The material shall be bladed, rolled and compacted to a finished slope of 4% where practical. Shoulders with existing adequate shoulder material in place shall be bladed to a slope of 4%, the cost of which shall be included in the prices of other items bid.

(E) Temporary striping shall conform to finished stripe specifications for alignment, neatness, reflectivity, and straightness. All permanent pavement markings are to be hot thermoplastic. Edge lines will be placed so as to maintain the original lane width. Glass beads applied to thermoplastic shall conform to Subsection 720.01. On all bridges and concrete sections of highway, old traffic stripe shall be removed and replaced with High Performance Thermoplastic. Special care should be taken for the placement of Thermoplastic Detail Stripe along the edge of pavement at turn-outs on all local roads and along tapers where Detail Stripe is required as per Typical PMD-1.

(F) Raised pavement markers will be placed as per sheet PM-1 of the Standard Drawings. Any removal of existing raised pavement markers or rumble bars shall be considered an absorbed item. Only flexible adhesive shall be allowed for placement of raised pavement markers meeting the requirements of Subsection 720.03.7.7 of the 2004 Mississippi Standard Specifications for Road and Bridge.

(G) Approximately 33 SY of 10-inch island pavement, 179 SY of 4-inch island pavement and 1302 LF of Type 2 Combination Curb and Gutter shall be placed as specified in typical drawings. Final location and dimension of concrete islands shall be directed by the Project Engineer. Concrete shall be Class B and an approved mix design shall be obtained prior to beginning work on any paved islands. Islands shall be backfilled with crushed stone. Removal of existing pavement and excavation for islands will be paid for under Removal of Pavement, All Types and Depths. Concrete testing shall be performed by MDOT personnel.

(H) The installation of terminal end sections, installed as per manufactures recommendations, shall be National Cooperative Highway Research Program (NCHRP) Report 350 Test Level 3 (TL-3) approved. The flared Terminal End Section or equivalent shall be used at each of the bridge locations on this project. All terminal end sections shall be marked with a Samford Mean Streak grease pen to identify type installed. The Contractor shall furnish the Project Engineer two (2) copies of the manufacture's installation instructions prior to beginning guardrail operations. Any site grading and all fill material necessary at the guardrail locations will not be measured for separate payment but will be absorbed in other guardrail pay items. Guard rail pads shall be

overlaid prior to placing guardrail and will be paid for under 907-403, 9.5-mm, MT, asphalt mixture. Seeding, fertilizer and vegetative mulch are required on all disturbed areas at the guardrail site and will be considered an absorbed item. Application rate for Bermuda grass seed will be 25 pounds per acre. Application rate for combination fertilizer (13-13-13) will be 1,000 pounds per acre. Application rate for mulch shall be two (2) tons per acre.

Pay Item 907-824-PP008 shall be used on Bridge Locations 127.4 and 127.8 (See attached detail).

The Contractor shall erect and maintain construction signing, and provide all signs and traffic handling devices, and shall provide two portable R16-3 signs per work zone or lane closure in addition to signs required by standard drawings in accordance with Manual Uniform Traffic Control Devices (MUTCD) (the cost is to be included in the price bid for pay item No. 618-A, Maintenance of Traffic).

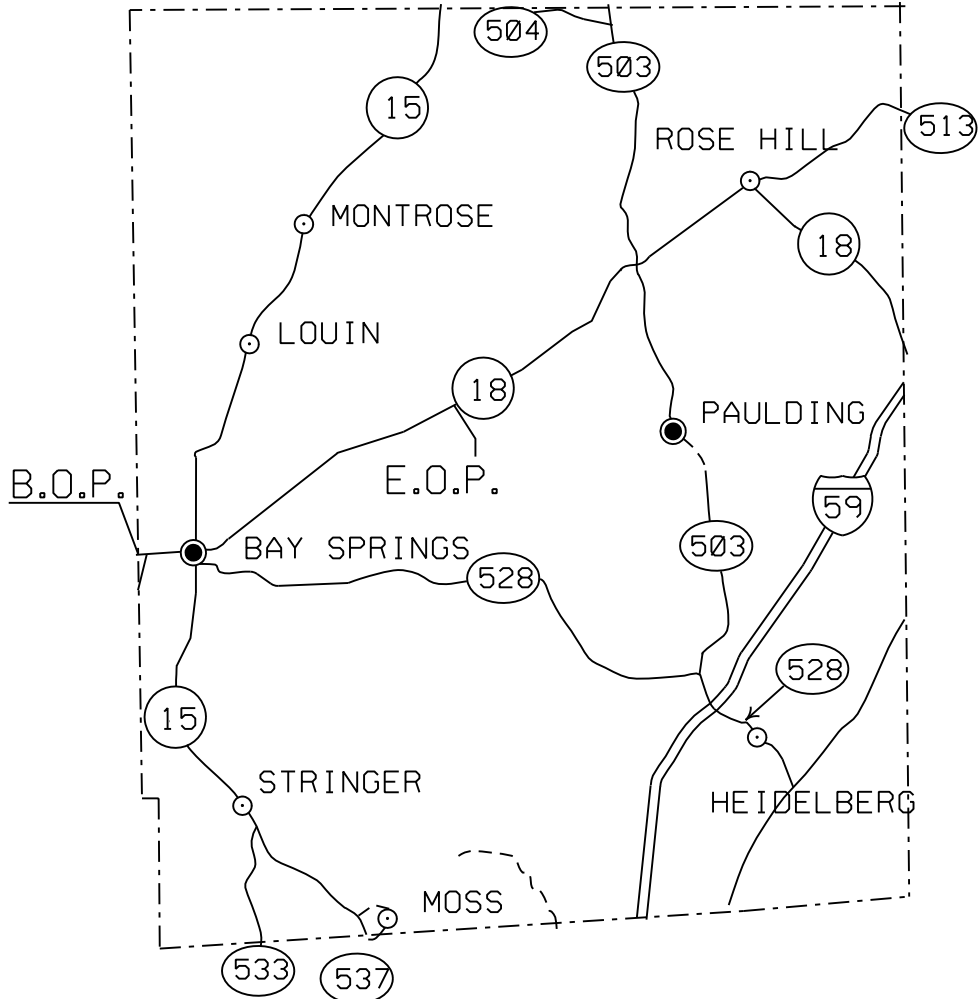
Incidental work such as removing vegetation, shaping and compaction of shoulder, removing excess asphalt material, project clean-up, and other incidental work necessary to complete the project will not be measured for separate payment, but will be included in other bid items, and must be performed during the operating hours for this project.

If pavement section marking tape is encountered on this project, it shall be located prior to overlaying and placed back in same location after paving operations have ceased. The section marking shall be eight inch high performance cold plastic detail stripe and shall be four feet in length. The marking shall be centered across the centerline stripe. The cost of this item shall be absorbed in other bid items.

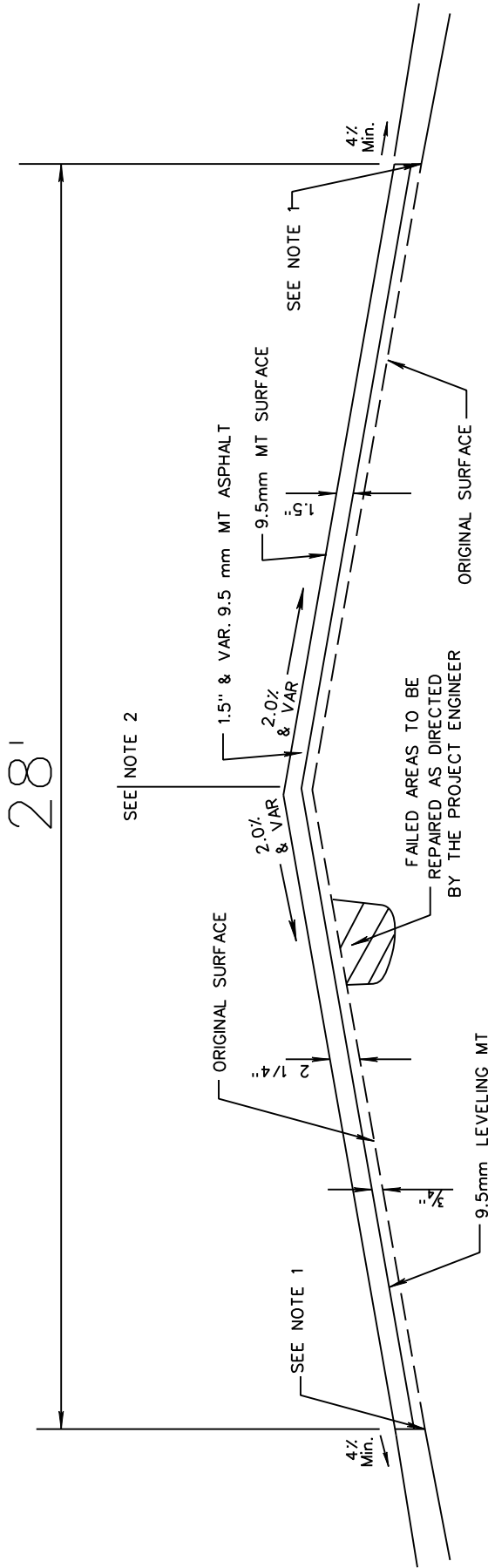
**TABLE 1**  
-4-

<b>PAY ITEM / LANE</b>	<b>BEGIN STATION</b>	<b>Length</b>	<b>Width</b>	<b>Area</b>
<b>202-A078</b>				
<b>Removal of Pavements, SY</b>	36+40	165	8	146.667
	45+00	28	7	21.778
	49+92	33	7	25.667
<b>EAST BOUND</b>	130+26	50	7	38.889
	234+57	9	7	7.000
	293+26	69	8	61.333
	295+95	84	7	65.333
	340+22	245	9	245.000
	343+95	90	8	80.000
<b>WEST BOUND</b>	44+42	22	14	34.222
	48+70	18	7	14.000
	59+95	18	7	14.000
	117+04	73	14	113.556
	130+33	40	7	31.111
	169+32	18	7	14.000
	211+00	29	7	22.556
	232+42	110	14	171.111
	251+53	47	7	36.556
	263+00	28	7	21.778
	264+03	14	7	10.889
	265+44	56	7	43.556
	268+09	84	7	65.333
	277+81	20	7	15.556
	293+62	74	7	57.556
<b>ISLANDS</b>				537.000
<b>PAGE TOTAL Removal of Pavements SY</b>				<b>1747.78</b>

MP - 6018 - 31(010) / 304810301  
JASPER COUNTY



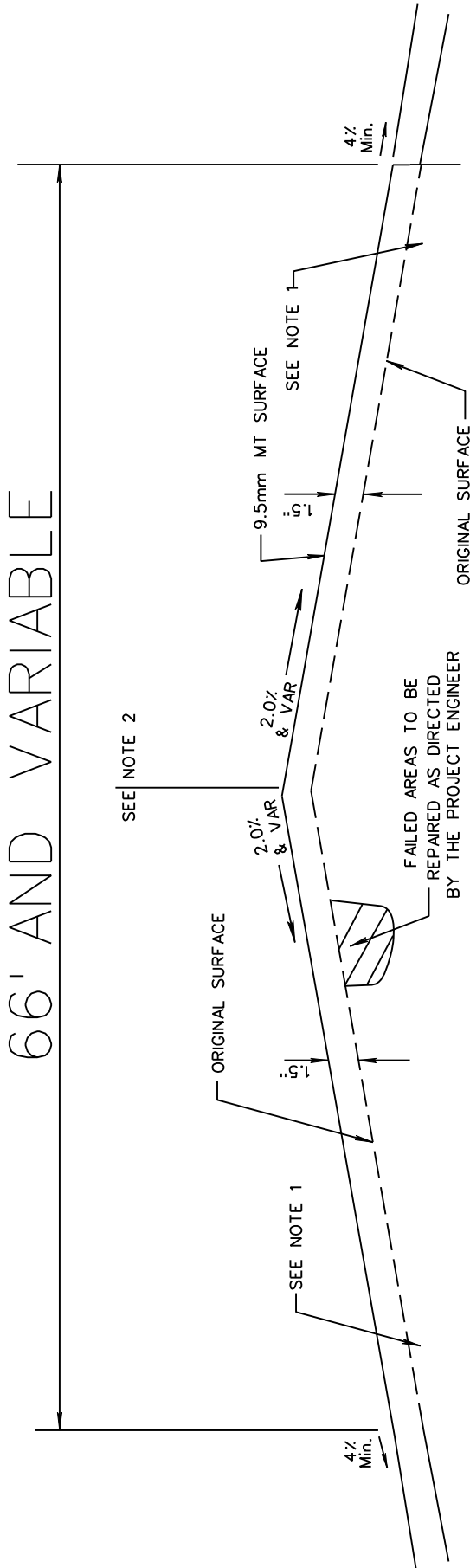
THE ABOVE REFERENCED PROJECT IS FOR LEVELING  
AND OVERLAYING HWY 18 FROM THE SMITH COUNTY  
LINE EAST 8.25 MILES



# TYPICAL FOR STATION 10+00(R1) TO 110+00 (R1)

NOTES:

- (1) THE EXISTING PAVEMENT EDGE SHALL BE CUT TO A SMOOTH AND NEAR VERTICAL FACE WITH AN APPROVED CUTTING DEVICE. ( NOT TO BE MEASURED FOR SEPARATE PAY.)
- (2) TRUE CENTERLINE PAVEMENT ALIGNMENT SHALL BE DETERMINED BY THE CONTRACTOR BY MEASURING THE EXISTING ROADWAY AT 500ft. INTERVALS IN TANGENT SECTIONS, AND 100ft. INTERVALS IN HORIZONTAL CURVES.



# TYPICAL FOR STATION 110+00(R1) TO 23+00 (R2)

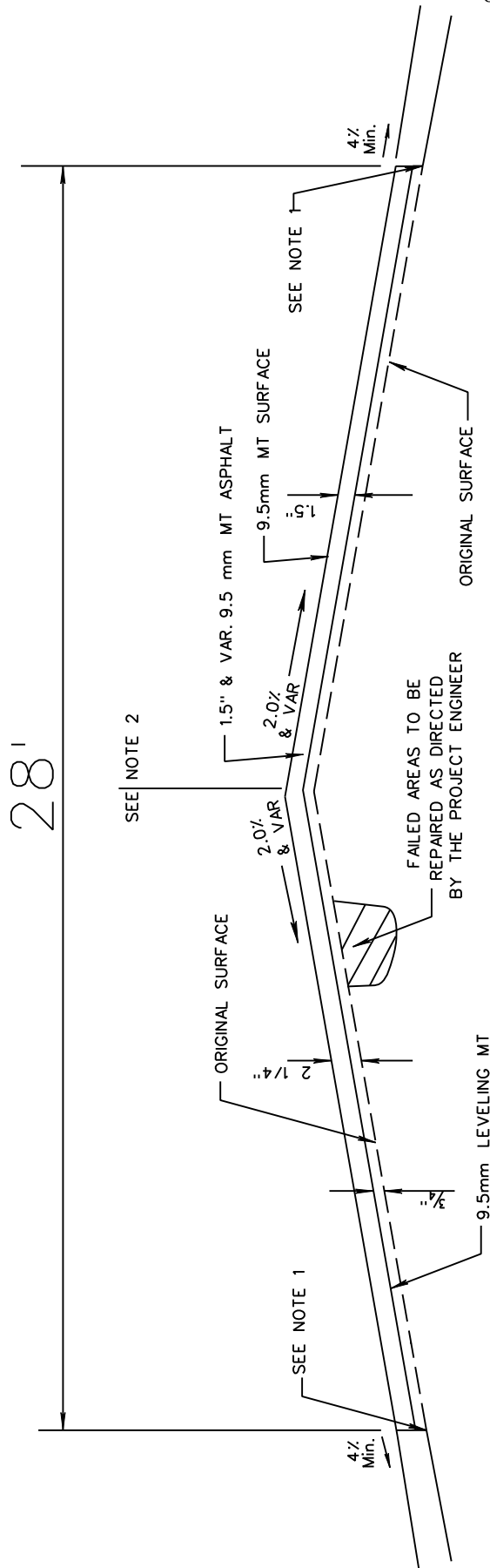
NOTES:

(1) THE EXISTING PAVEMENT EDGE SHALL BE CUT TO A SMOOTH AND NEAR VERTICAL FACE WITH AN APPROVED CUTTING DEVICE. ( NOT TO BE MEASURED FOR SEPARATE PAY.)

(2) TRUE CENTERLINE PAVEMENT ALIGNMENT SHALL BE DETERMINED BY THE CONTRACTOR BY MEASURING THE EXISTING ROADWAY AT 500ft. INTERVALS IN TANGENT SECTIONS, AND 100ft. INTERVALS IN HORIZONTAL CURVES.

MILL AND REPLACE 1.5" OF 9.5MM HMA



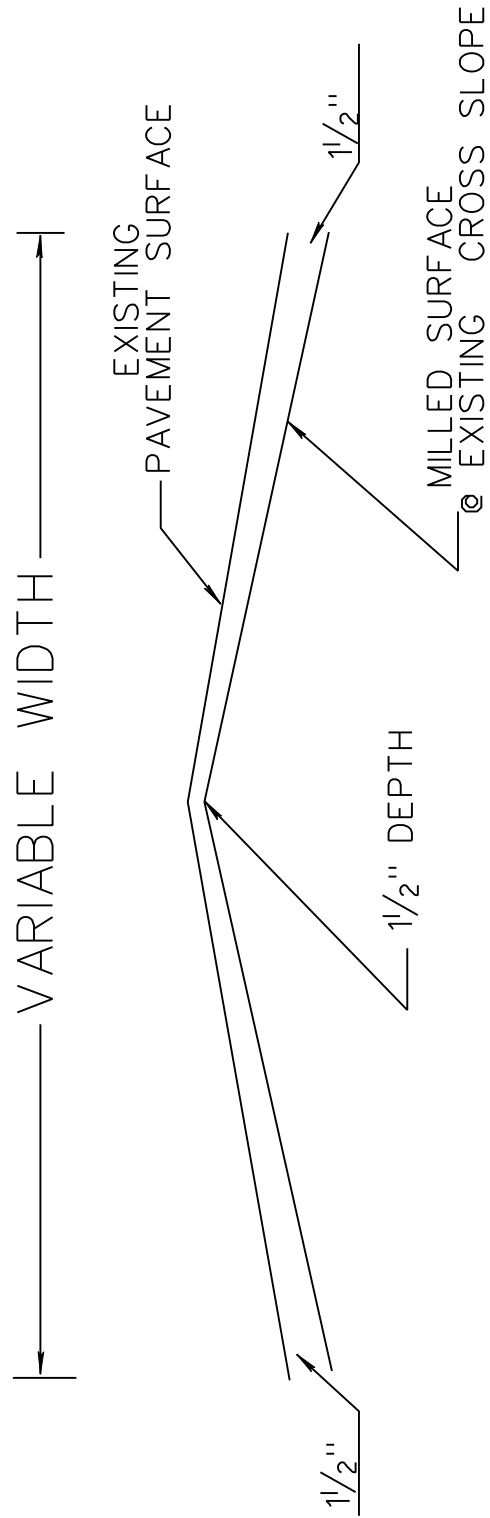


# TYPICAL FOR STATION 23+00(R2) TO 348+00(R2)

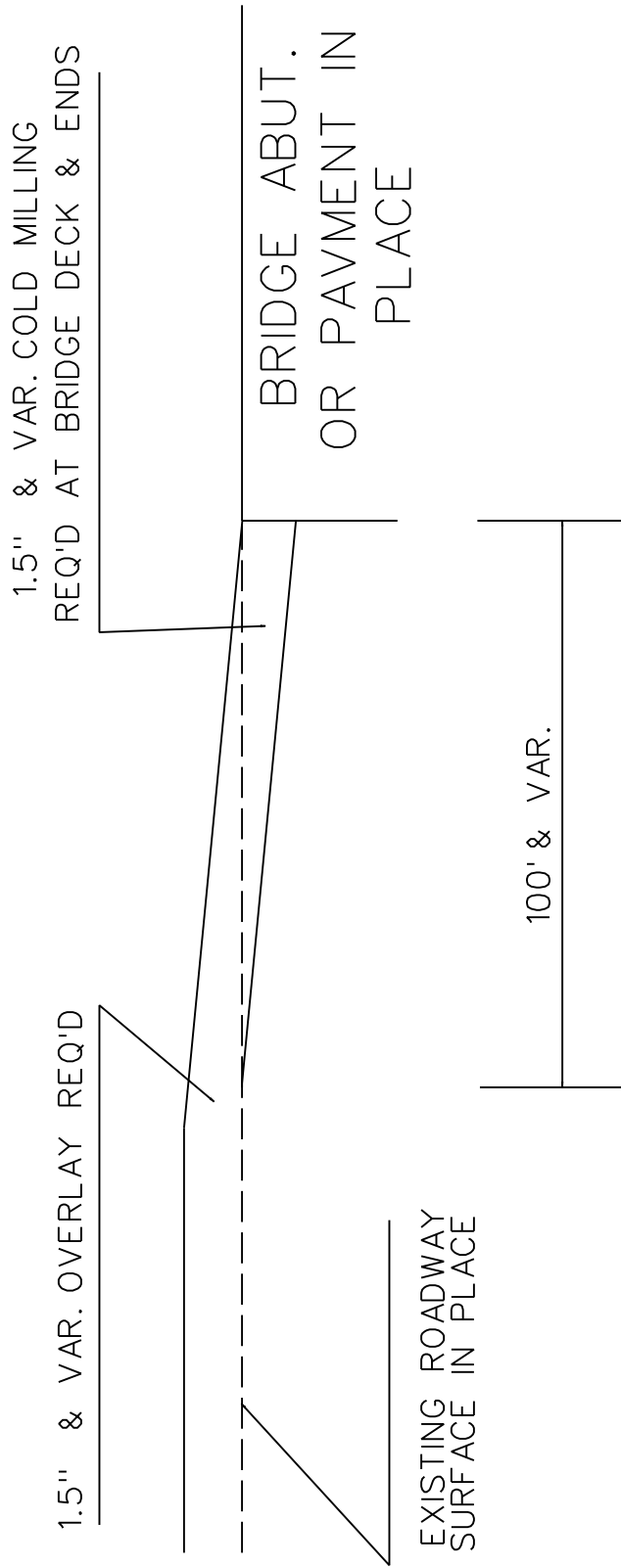
NOTES:

- (1) THE EXISTING PAVEMENT EDGE SHALL BE CUT TO A SMOOTH AND NEAR VERTICAL FACE WITH AN APPROVED CUTTING DEVICE. (NOT TO BE MEASURED FOR SEPARATE PAY.)
- (2) TRUE CENTERLINE PAVEMENT ALIGNMENT SHALL BE DETERMINED BY THE CONTRACTOR BY MEASURING THE EXISTING ROADWAY AT 500ft. INTERVALS IN TANGENT SECTIONS, AND 100ft. INTERVALS IN HORIZONTAL CURVES.

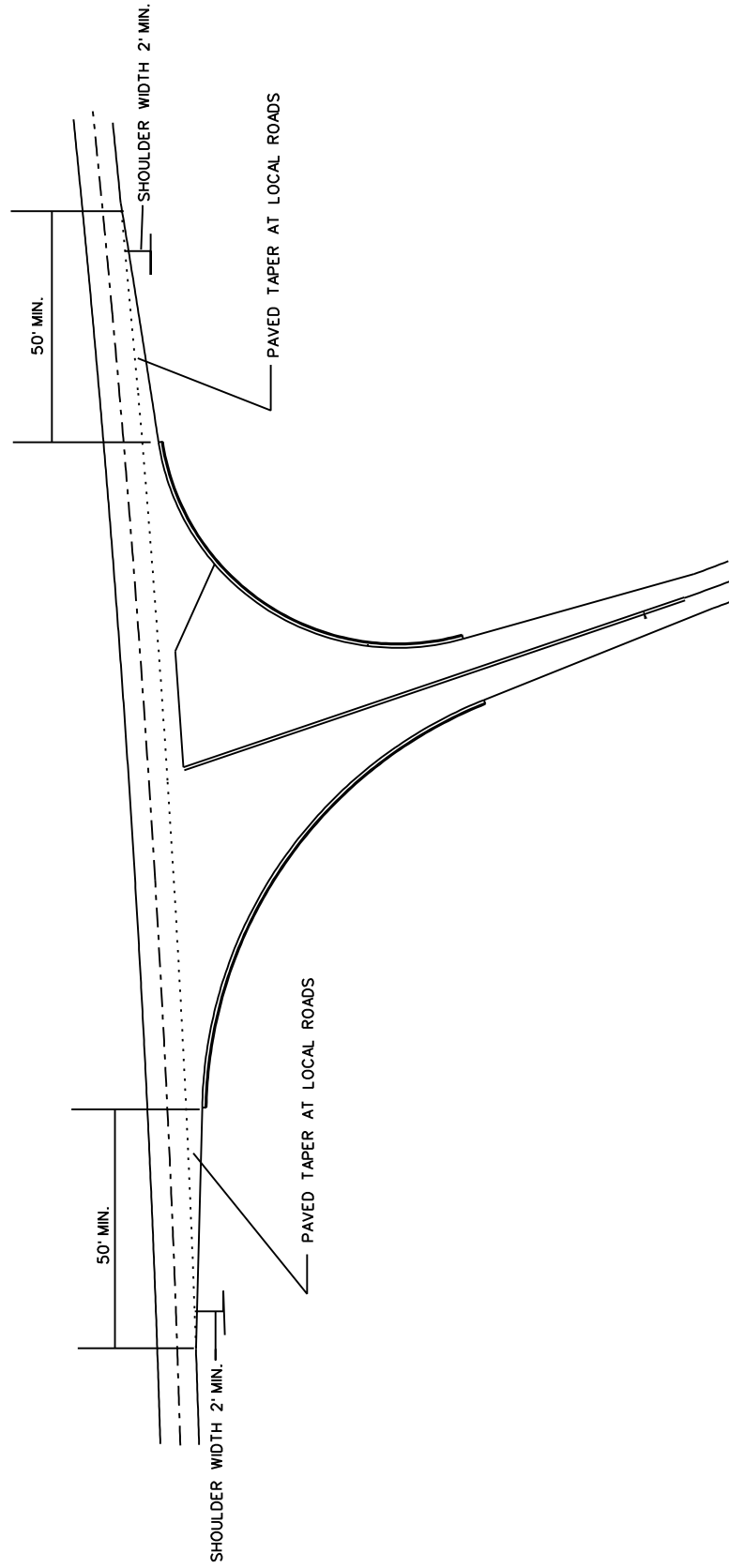
# TYPICAL MILLING DIAGRAM



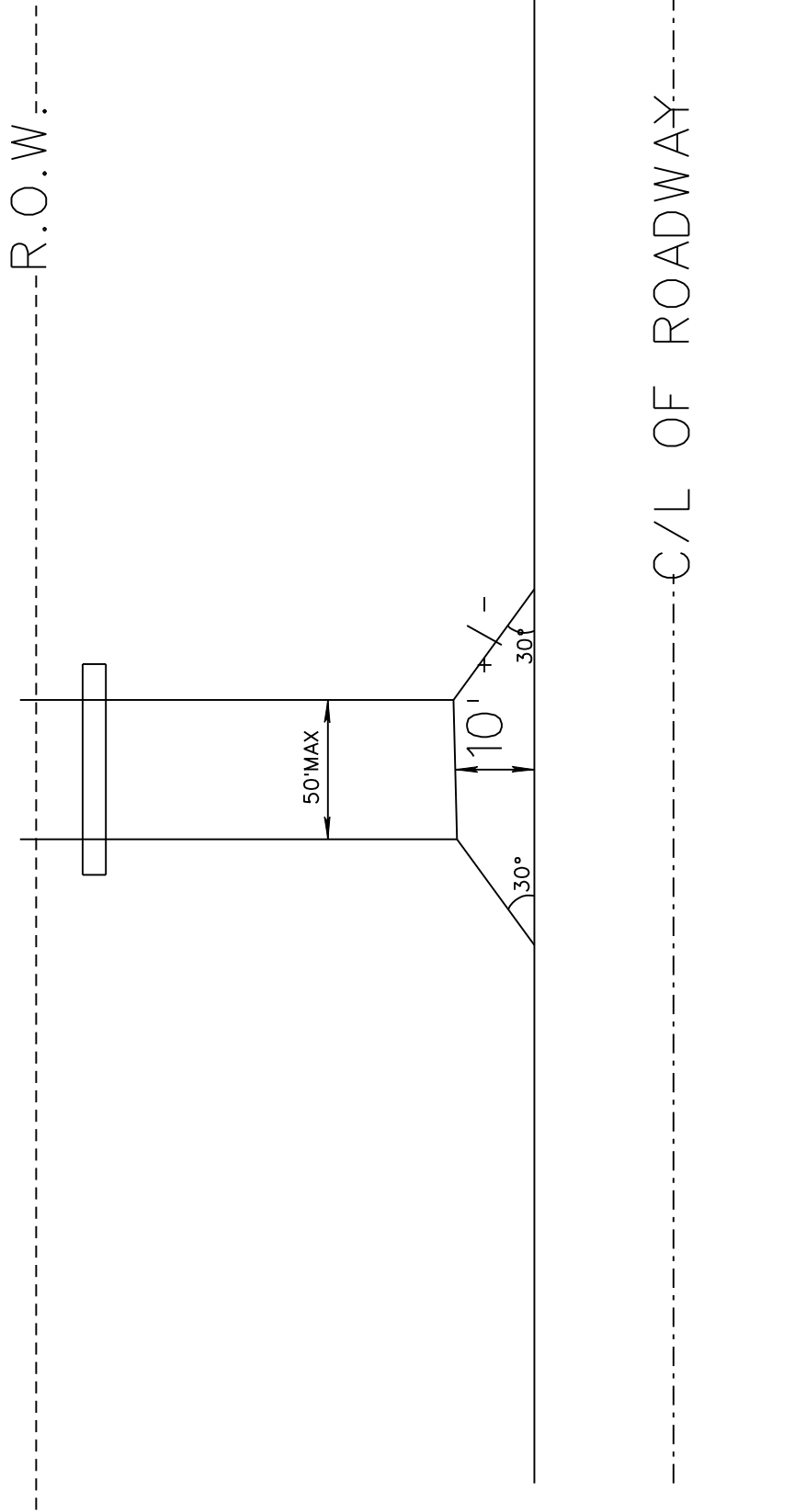
# TYPICAL MILLED TRANSITION AT BRIDGE ABUT. OR PAVEMENT IN PLACE



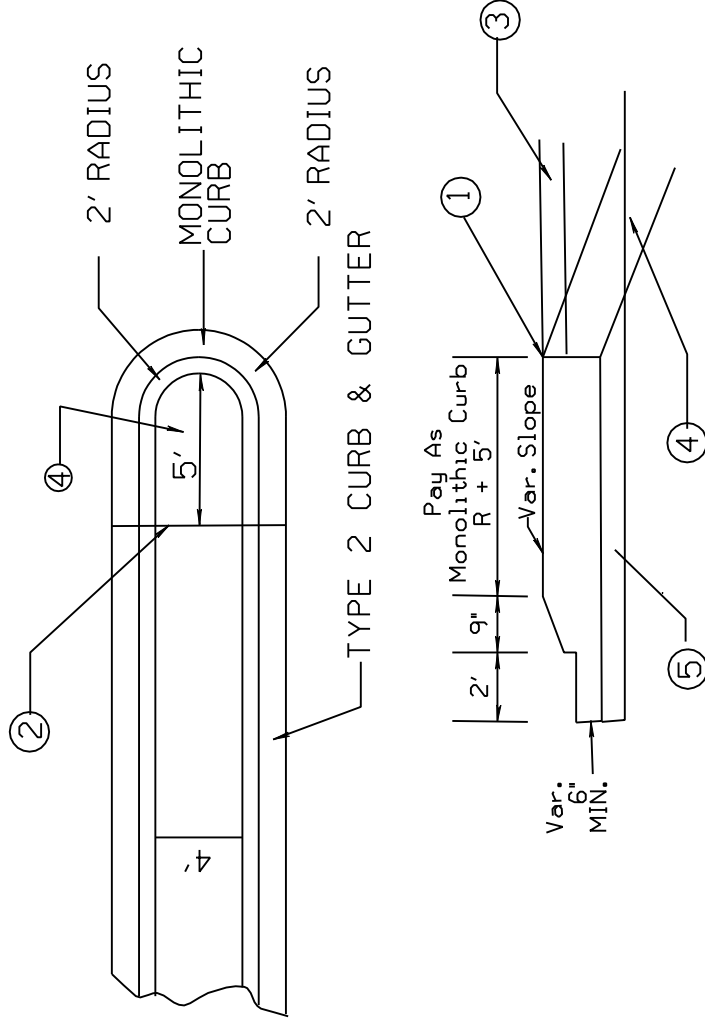
# TYPICAL FOR PAVED TAPER AT LOCAL ROADS



PRIVATE DRIVEWAY DETAIL

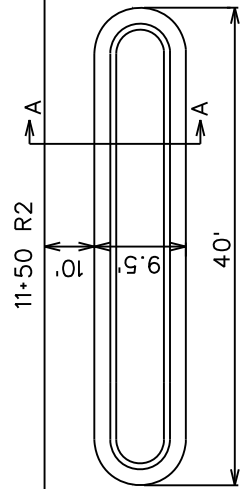


TYPICAL DETAIL OF  
MONOLITHIC CURB &  
GUTTER AT ISLAND

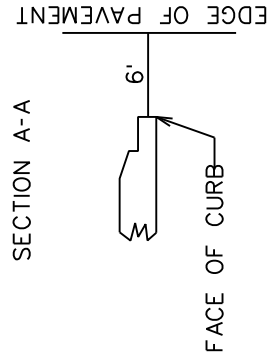


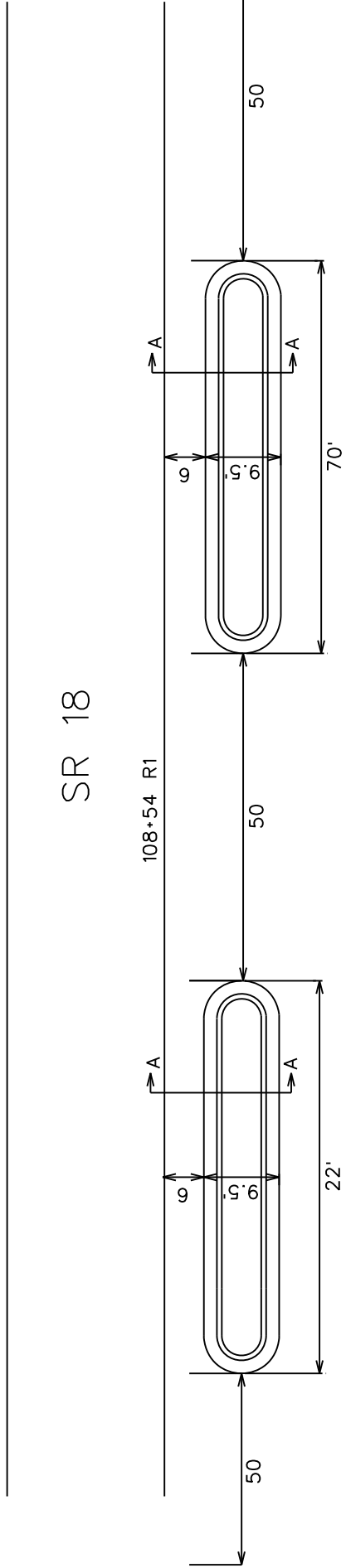
- ① - 1/2" PREMOULDED EXPANSION JOINT ( NOT A PAY ITEM )
- ② - 1/2" TOOLED CONTRACTION JOINT ( NOT A PAY ITEM )
- ③ - 4" CLASS "C" STRUCTURAL CONCRETE ( FOR MEDIAN & ISLAND PAV'T. ) PAY ITEM NO. 616-A
- ④ - 10" & VAR. DEPTH CLASS "C" STRUCTURAL CONCRETE (FOR MEDIAN & ISLAND PAV'T.)
- ⑤ - 2" MIN. ASPHALT BASE SHALL BE REQUIRED BEFORE PLACING ISLANDS

SR 18

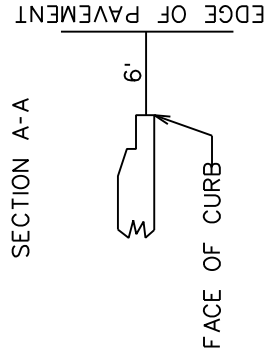


BIG O'S  
SEAFOOD

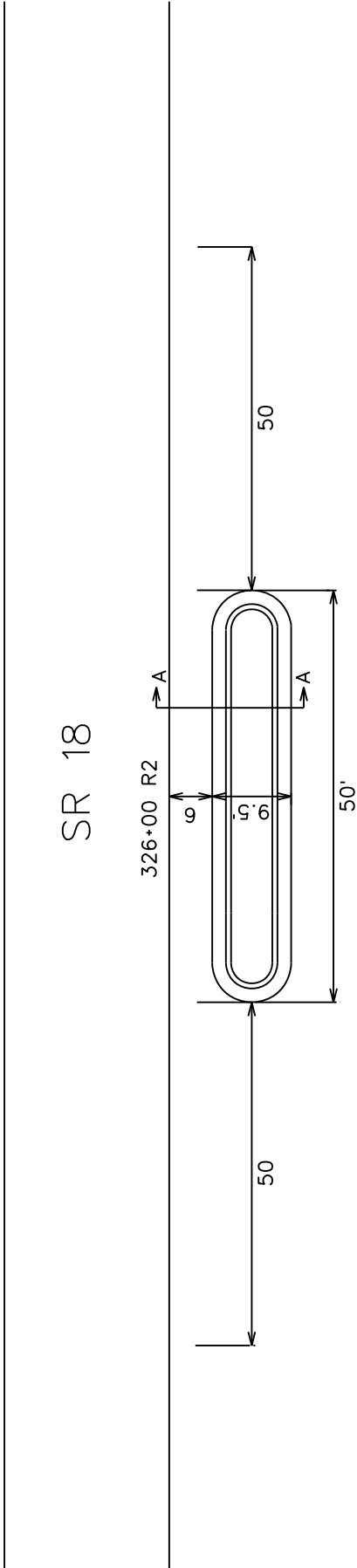




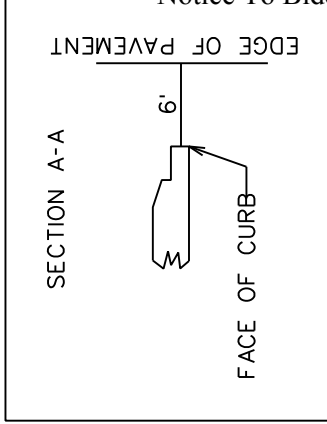
FIRST BAPTIST  
CHRUCH



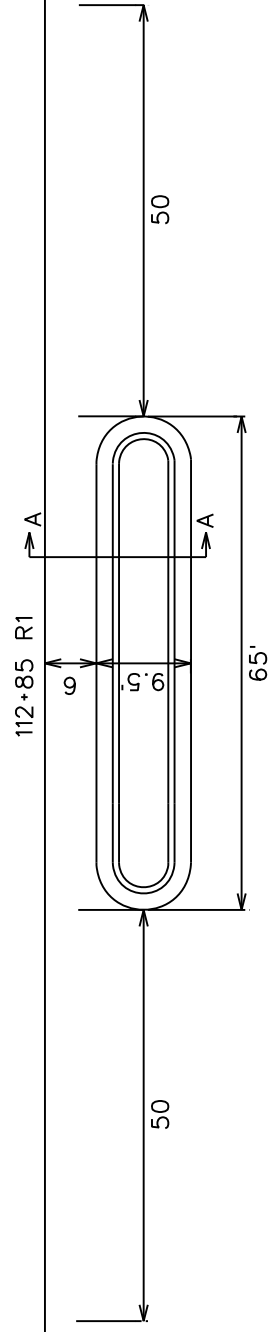




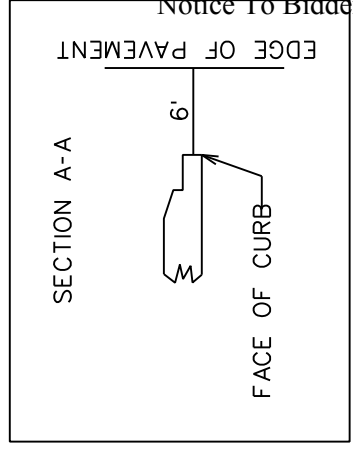
M & G  
GROCERY



SR 18

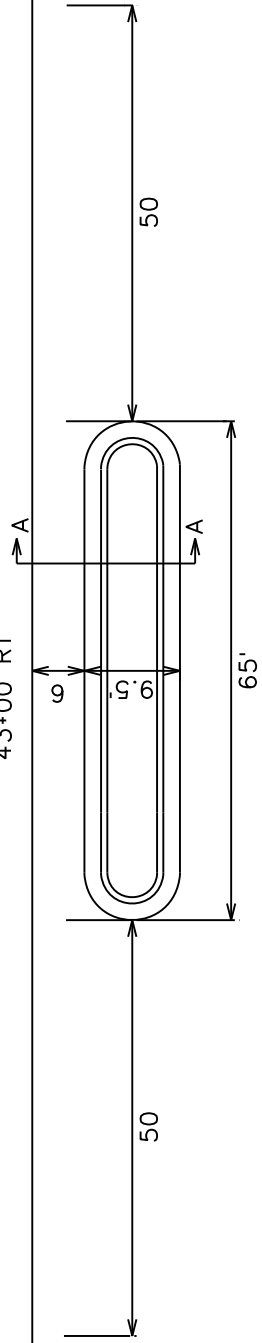


METHODIST  
CHRUCH

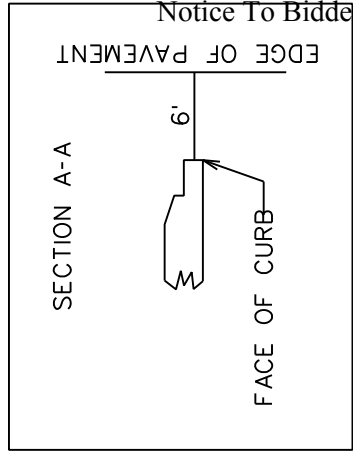


SR 18

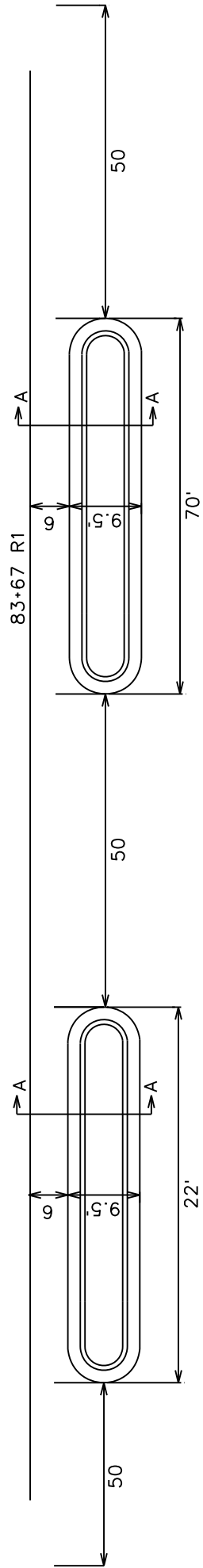
43+00 R1



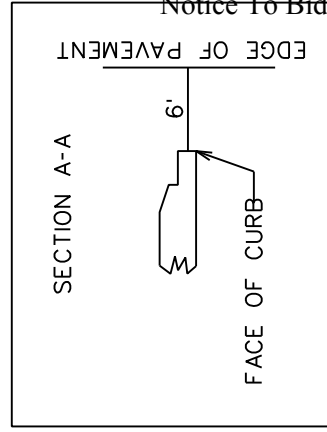
NEW ZION



SR 18

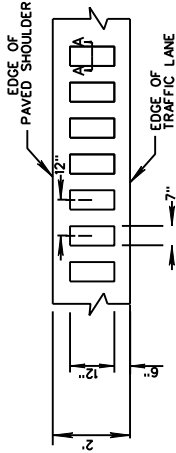


WAYNE TIRE



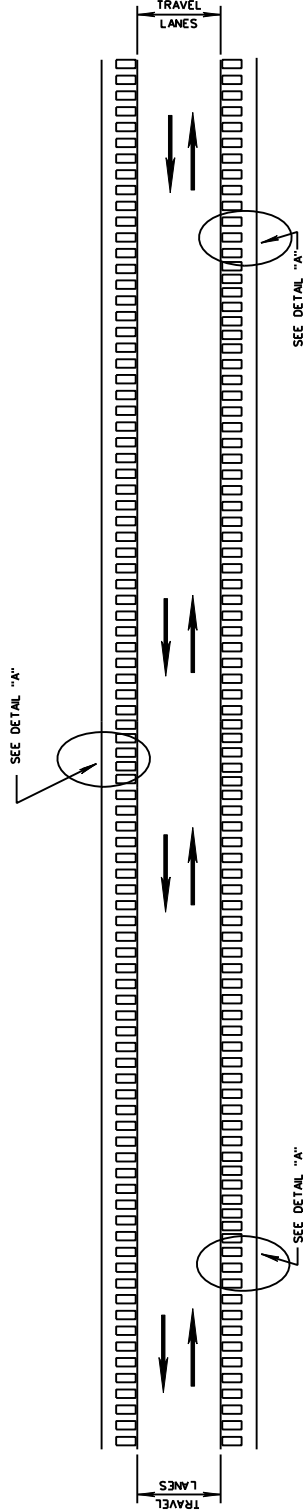
GENERAL NOTES

1. GROUND-IN RUMBLE STRIPS SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
2. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED TO MAINLINE ONLY.



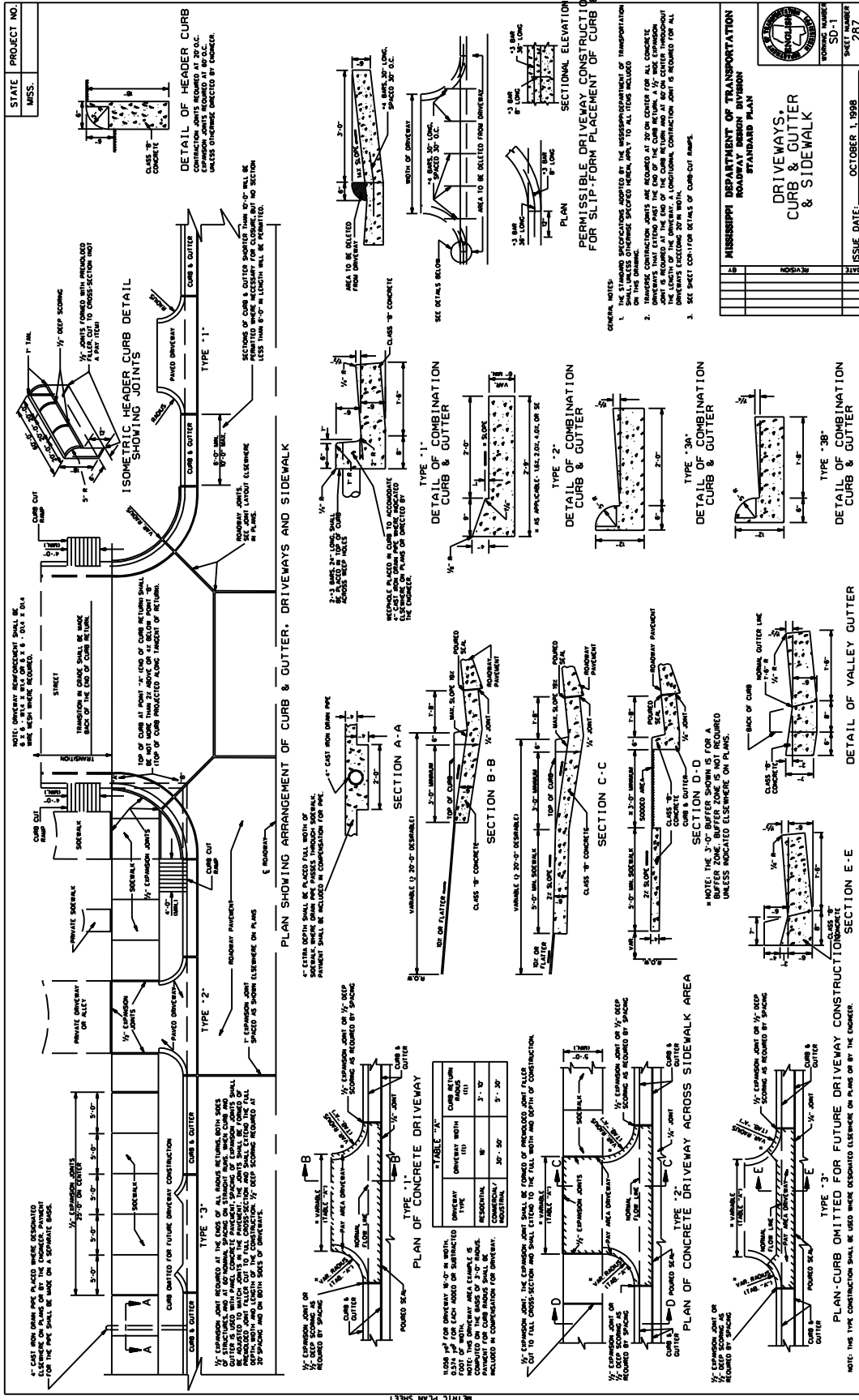
SECTION "A-A"

DETAIL "A"



PLAN  
NOT TO SCALE  
DETAIL OF  
RUMBLE STRIPS  
(GROUND-IN)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTION	
RUMBLE STRIPS (GROUND-IN)	
DATE	FILE NAME:
REVISED	DESIGN TITLE
DESIGNED BY	CHECKED
DRAWN BY	DATE
PROJECT NO.	SHEET NO.
PROJECT TITLE	DATE

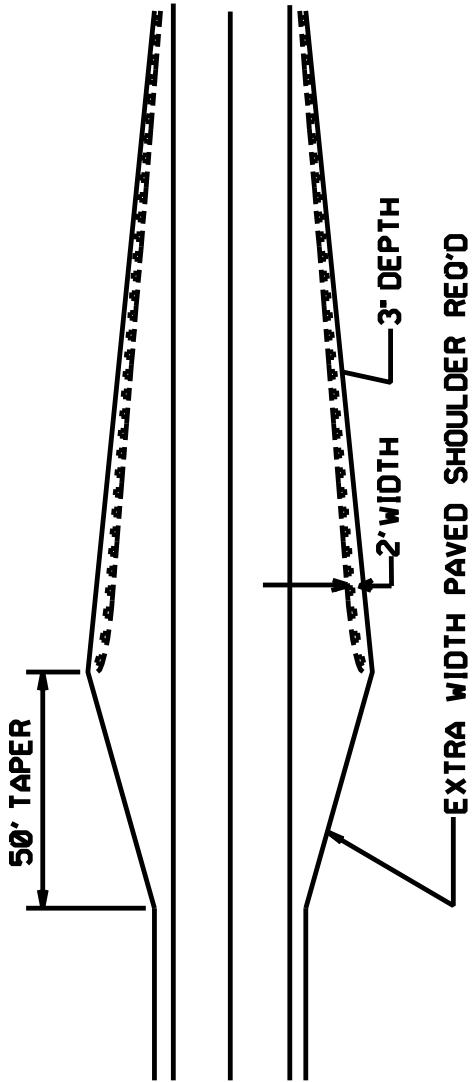


MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

**DRIVEWAYS,  
CURB & GUTTER  
& SIDEWALK**

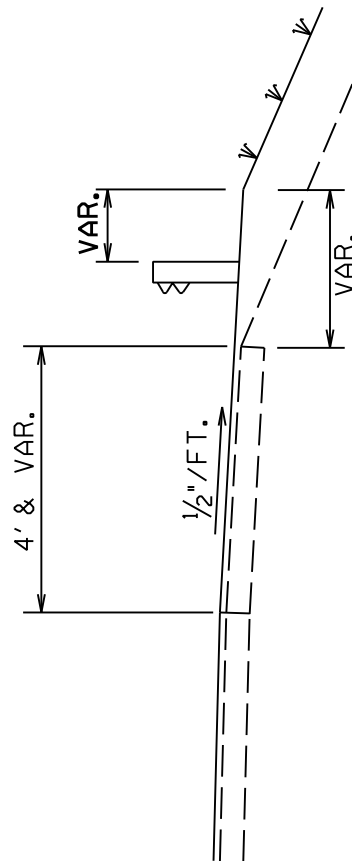
SD-1  
SHEET NUMBER  
287

DATE	ISSUE DATE	OCTOBER 1, 1998
NO.		
BY		
CHKD		
APP'D		



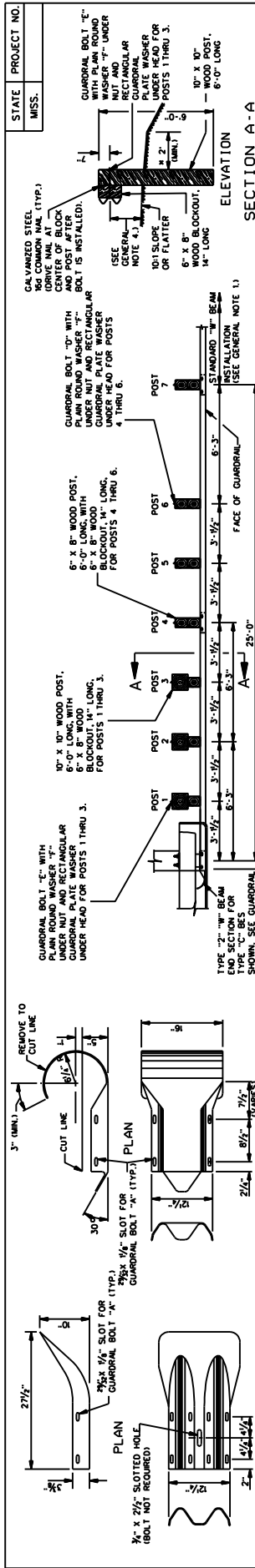
DETAIL OF INSTALLATION OF EXTRA WIDTH PAVED SHOULDERS AT BRIDGES

1. 3" AND VAR. DEPTH 9.5MM HOT BITUMINOUS PAVEMENT REQ'D



TYPICAL SECTION  
DETAILS OF PAVED SHOULDERS AT  
BRIDGE GUARD RAIL INSTALLATIONS

BOTH SIDES

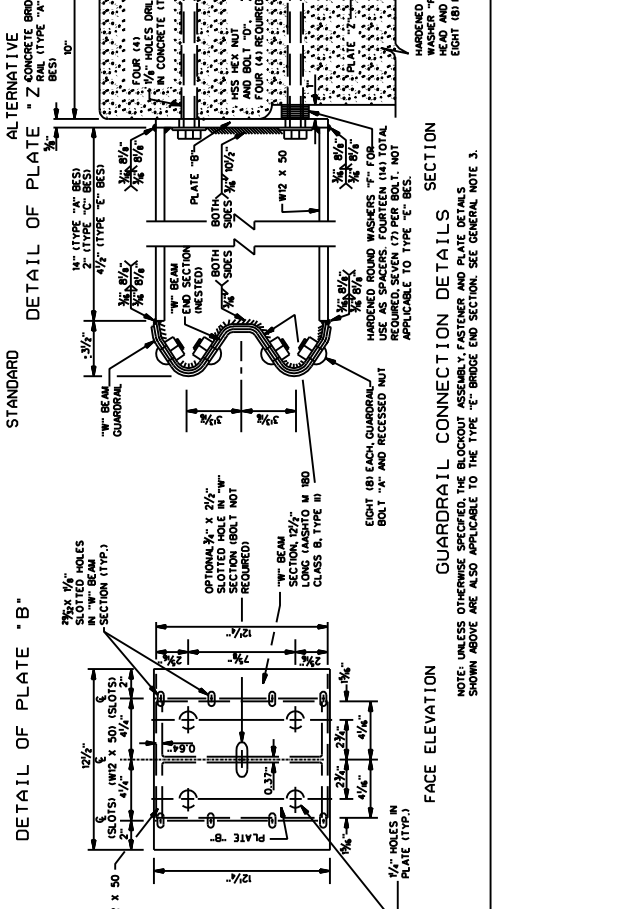
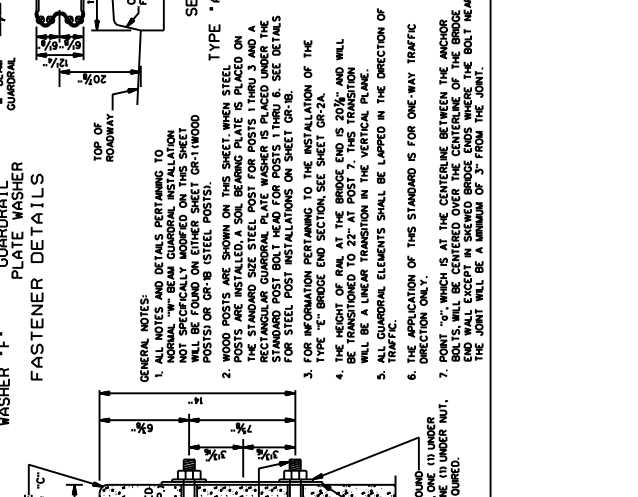
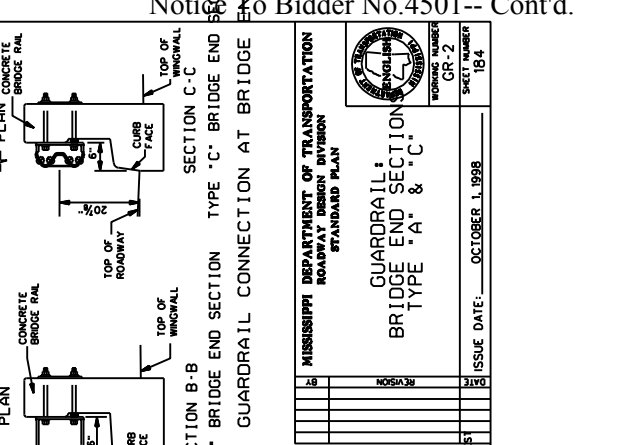
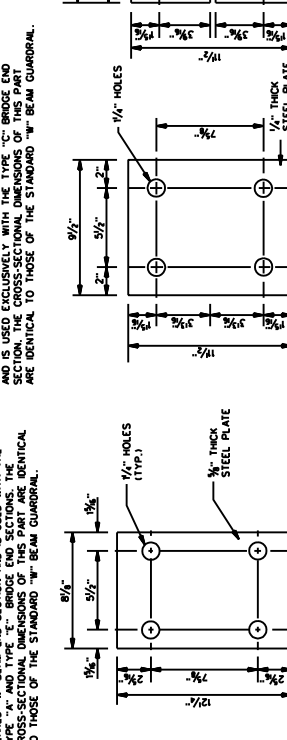
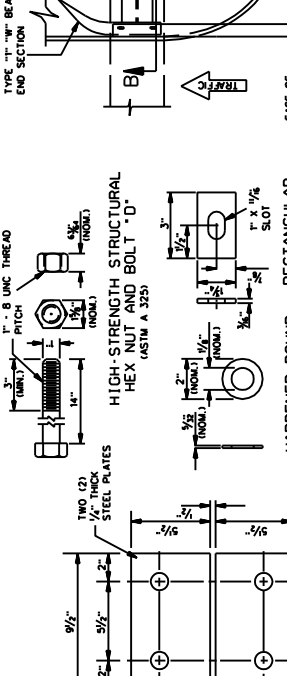


PLAN OF BRIDGE END SECTION

TYPE "1" "W" BEAM END SECTION

NOTE: THE TYPE "2" END SECTION IS A MODIFICATION OF THE STANDARD ROUNDED "W" BEAM END SECTION AND IS USED EXCLUSIVELY WITH THE TYPE "C" BRIDGE END SECTION. THE CROSS-SECTIONAL DIMENSIONS OF THIS PART ARE IDENTICAL TO THOSE OF THE STANDARD "W" BEAM GUARDRAIL.

\* NOTE: UNLESS OTHERWISE SPECIFIED ON THE PLANS.



- GENERAL NOTES: AND DETAILS PERTAINING TO
1. ALL NOTES, AND DETAILS PERTAINING TO NORMAL "W" BEAM GUARDRAIL INSTALLATION NOT SPECIFICALLY MODIFIED ON THIS SHEET WILL BE FOUND ON OTHER SHEET GR-1 (WOOD POSTS) OR GR-8 (STEEL POSTS).
  2. POSTS ARE INSTALLED FROM WITHIN THE RAIL. THE STANDARD SIZE STEEL POST FOR POSTS 1 THRU 3 AND A RECTANGULAR GUARDRAIL PLATE WASHER IS PLACED UNDER THE STANDARD POST HEAD FOR POSTS 1 THRU 3. SEE DETAILS FOR STEEL POST INSTALLATIONS ON SHEET GR-8.
  3. FOR INFORMATION PERTAINING TO THE INSTALLATION OF THE TYPE "E" BRIDGE END SECTION, SEE SHEET GR-2A.
  4. THE HEIGHT OF RAIL AT THE BRIDGE END IS 20% AND WILL BE A LINEAR TRANSITION IN THE VERTICAL PLANE.
  5. ALL GUARDRAIL ELEMENTS SHALL BE LAPPELLED IN THE DIRECTION OF TRAFFIC.
  6. THE APPLICATION OF THIS STANDARD IS FOR ONE-WAY TRAFFIC.
  7. POINT "C", WHICH IS AT THE CENTERLINE BETWEEN THE ANCHOR END WALL EXCEPT IN SHEWED BRIDGE ENDS WHERE THE BOLT HEADS AND THE JOINT WILL BE A MINIMUM OF 3' FROM THE JOINT.

- GENERAL NOTES: AND DETAILS PERTAINING TO
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DATE	REVISION	BY	APP'D.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION

GUARDRAIL TYPE "A" & "C"  
BRIDGE END SECTION

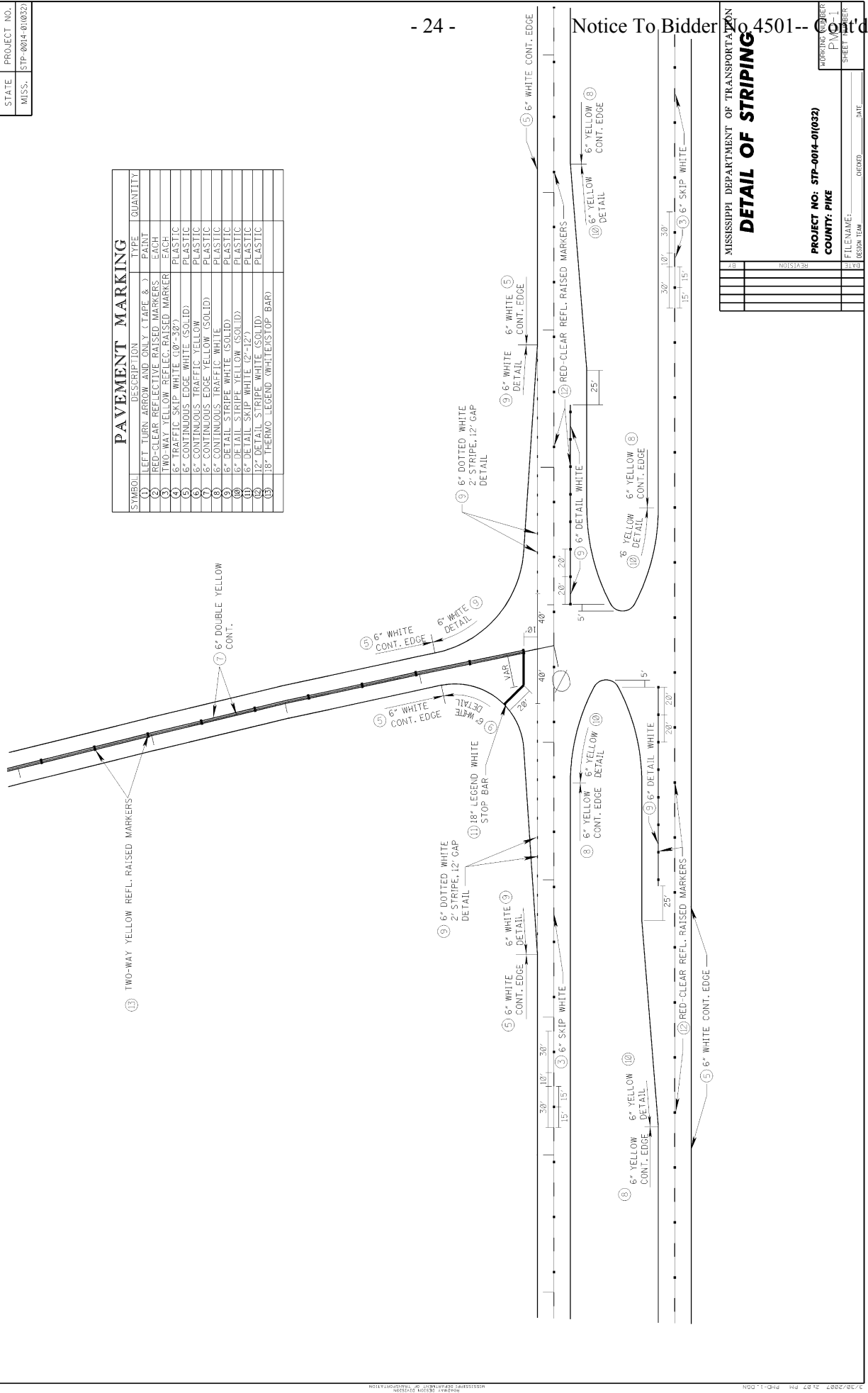
WORKING NUMBER GR-2  
SHEET NUMBER 184

ISSUE DATE: OCTOBER 1, 1988



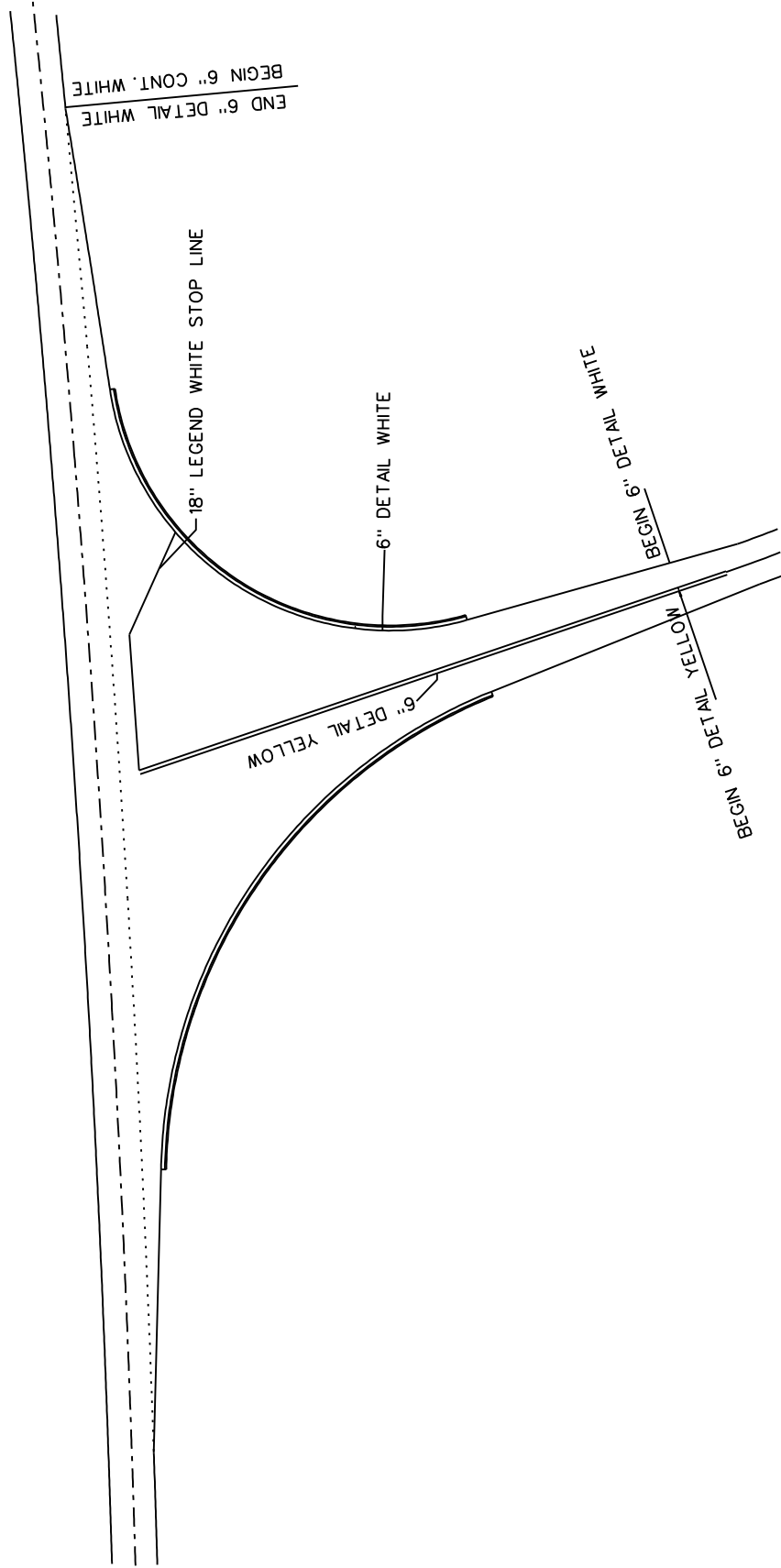
STATE PROJECT NO.  
MISS. STP-0014-01(032)

PAVEMENT MARKING			
SYMBOL	DESCRIPTION	TYPE	QUANTITY
(1)	LEFT TURN AREA REFLECTIVE RAISED MARKERS	PLASTIC	
(2)	RED-CLEAR REFLECTIVE RAISED MARKERS	PLASTIC	
(3)	TWO-WAY YELLOW REFLECTIVE RAISED MARKERS	PLASTIC	
(4)	6" TRAFFIC SKIP WHITE (10'-30')	PLASTIC	
(5)	6" CONTINUOUS EDGE WHITE (SOLID)	PLASTIC	
(6)	6" CONTINUOUS TRAFFIC YELLOW (SOLID)	PLASTIC	
(7)	6" CONTINUOUS EDGE YELLOW (SOLID)	PLASTIC	
(8)	6" DETAIL STRIPE WHITE (SOLID)	PLASTIC	
(9)	6" DETAIL STRIPE YELLOW (SOLID)	PLASTIC	
(10)	6" DETAIL STRIPE WHITE (SOLID)	PLASTIC	
(11)	18" THERMO STOP WHITE (SOLID)	PLASTIC	
(12)	18" THERMO LEGEND (WHITE/STOP BAR)	PLASTIC	



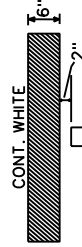
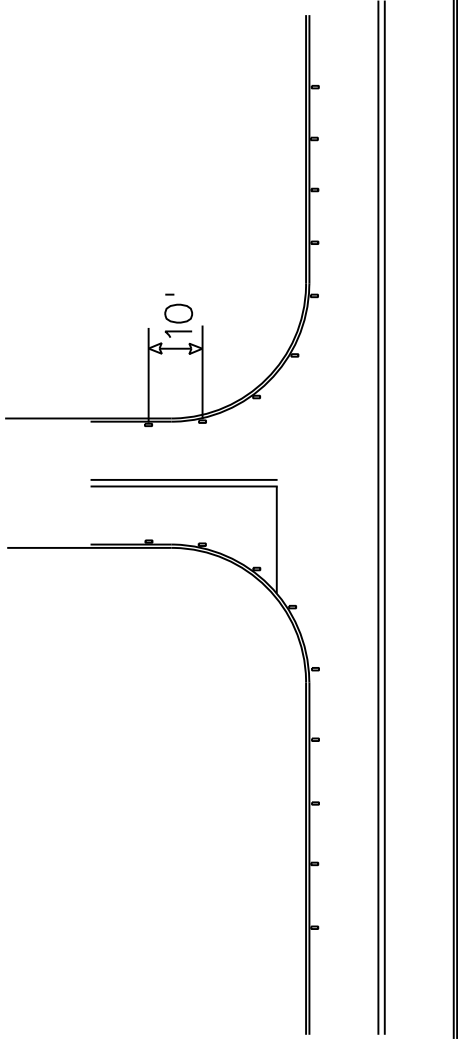
MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
**DETAIL OF STRIPING**  
 PROJECT NO: STP-0014-01(032)  
 COUNTY: PIKE  
 WORKSHEET NO: 4501  
 SHEET NO: 24  
 DATE: 8/20/14

# TYPICAL STRIPING FOR SIMPLE INTERSECTION AT LOCAL ROADS



STATE	PROJECT NO.
MISS.	

TYPICAL FOR RAISED PAVEMENT MARKERS  
PLACED ON SIDE ROAD RADIUS



DETAIL A

- NOTE 1. MARKERS SHALL BE PLACED EVERY 10 FEET.
- NOTE 2. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
- NOTE 3. MARKERS SHALL BE HIGH PERFORMANCE TWO WAY CLEAR.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
2-LANE	
RAISED PAVEMENT MARKERS	
PLACED ON SIDE ROADS	
WORKING NUMBER	
SHEET NUMBER	
FILE NAME	
ISSUE 12/11	CHECKED DATE
BY	REVISION

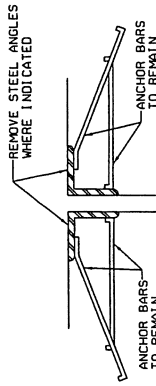
STATE	PROJECT NO.
MISS.	

**JOINT REPAIR MORTAR NOTES:**

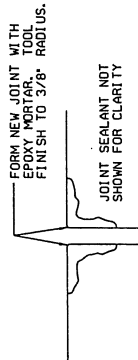
1. JOINT REPAIR MORTAR SHALL BE ONE OF THE FOLLOWING PRODUCTS:
  - A. \*HP-BLINDER, SHALL BE AS MANUFACTURED BY HUNT PROCESS CORPORATION, P. O. BOX 688, RIDGELAND, MS 39158, TELEPHONE NO. (601)856-8811, AND INSTALLED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.
  - B. \*POLYMER CONCRETE, SHALL BE AS MANUFACTURED BY POLYMER CONCRETE, INC. P. O. BOX 610, CAMDEN, AL 36726, (205)682-4296, AND INSTALLED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.
  - C. \*CERATECH PAVEMENT VR, SHALL BE AS MANUFACTURED BY CERATECH, INC., 3037 TUGGLE LIVES DR., BUFORD, GA 30519, TELEPHONE NO.(770)831-0794, AND INSTALLED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.

**GENERAL NOTES:**

1. SPECIFICATIONS: MISSISSIPPI STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, 2004.
2. NO CHANGE OF PLANS WILL BE PERMITTED EXCEPT BY WRITTEN ORDER OF THE ENGINEER. ANY CHANGES OR CORRECTIONS OF DESIGN OR CONSTRUCTION PROCEDURE MAY BE AUTHORIZED BY THE BRIDGE ENGINEER PROVIDED SUCH CHANGES WILL NOT BE CAUSE FOR CONTRACT PRICE ADJUSTMENT.
3. WORK FOR WHICH NO PAY ITEM IS PROVIDED IN THE PROPOSAL WILL NOT BE PAID FOR DIRECTLY AND PAYMENTS FOR BID ITEMS, INCLUDED IN THE PRICES AND PAYMENTS FOR BID ITEMS.



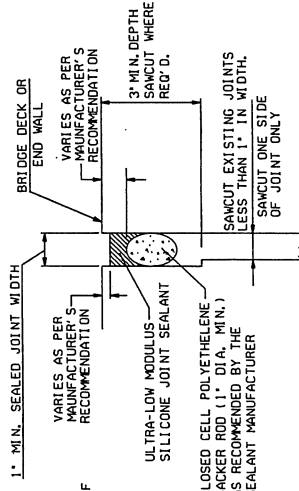
**TYPICAL SECTION AT JOINT REPAIR**  
SHOWING EXISTING JOINT ARMOR TO BE REMOVED



**TYPICAL SECTION AT JOINT REPAIR**  
SHOWING JOINT AFTER EPOXY MORTAR REPAIR

**JOINT REPAIR NOTES:**

1. THE INTENT OF THE CONTRACT IS TO REMOVE ALL JOINT ARMOR EXISTING AT THE TIME THE CONTRACT IS LET THAT IS DAMAGED, LOOSE, AND/OR HAS BEEN PREVIOUSLY REMOVED IN PART. ANY JOINT ARMOR THAT EXTENDS THE FULL WIDTH BEYOND THE BRIDGE AND IS INTACT AND IN GOOD CONDITION WILL REMAIN. JOINTS WITH EPOXY MORTAR AND SEAL JOINTS WITH A SILICONE JOINT SEALANT. IT IS POSSIBLE THAT BETWEEN PLAN PREPARATION AND LETTING TIME, JOINT ARMOR SHOWN TO BE REMOVED MAY HAVE BEEN REMOVED EARLIER DUE TO CHANGES. QUANTITIES SHALL BE ADJUSTED ACCORDINGLY. PAYMENT FOR JOINT PREPARATION, JOINT REPAIR AND REMOVAL OF JOINT ARMOR SHALL BE PAID FOR AS THE LENGTH ALONG THE BRIDGE DECK ON EACH SIDE OF THE SAWCUTTING THE JOINT IS REQUIRED AT EXISTING JOINTS THAT HAVE LESS THAN 1 INCH OF OPENING.
2. SILICONE SEALED JOINTS ARE REQUIRED AT ALL JOINTS. PAYMENT FOR SILICONE SEALED JOINTS AND SAWCUTTING SHALL BE PAID FOR AS THE LENGTH ALONG EACH SIDE OF THE JOINT.
3. REMOVAL OF EXISTING JOINT SEALANTS WILL NOT BE PAID FOR DIRECTLY AND THEREFOR SHALL BE CONSIDERED AS AN ABSORBED ITEM OF WORK.



**TYPICAL SECTION AT SAWCUT & SEALED JOINT**  
SHOWING SILICONE SEALED JOINT AFTER SAWCUT AND REPAIR WITH EPOXY MORTAR

**NOTE:**  
SEALANT SHALL BE ON THE MOST LIST OF APPROVED SILICONE JOINT SEALING COMPOUNDS FOR BRIDGE JOINTS APPLIED ACCORDING TO THE MANUFACTURER'S DIRECTIONS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
<b>DETAILS OF BRIDGE DECK JOINT REPAIR</b>	
WORKSHEET NUMBER	BR-1
SHEET NUMBER	
PROJECT NUMBER	
DATE	