

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) enclose a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. 1 DATED 10/15/2013 ADDENDUM NO. DATED
ADDENDUM NO. DATED ADDENDUM NO. DATED

Number	Description
1	Added or Revised Plan Sheet Nos. 2, 60, 102, 103, & 110; Amendment EBS Download Required.

TOTAL ADDENDA: 1
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.

Revised 09/21/2005

SP-9392-00(008) / 100710302

Jackson County(ies)

ADDENDUM

STATE	PROJECT NO.
MISS.	SP-9392-00(008)

DESCRIPTION OF SHEETS	WORKING NO.	SHEET NO.
TITLE SHEET (1)		1
DETAILED INDEX SHEETS (3)		
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SUMMARY OF QUANTITIES (ROADWAY)	SQS-3	11
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SR 611 STA. 2+300 TO STA. 3+100	5	23
LEE HENNING RD. STA. 0+767.889 TO STA. 0+991	5A	24
SR 611 STA. 3+100 TO STA. 3+900	6	25
SR 611 STA. 3+900 TO STA. 4+700	7	26
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* ENGLISH DRAWINGS

ROADWAY DESIGN MISSISSIPPI DEPARTMENT OF TRANSPORTATION METRIC PLAN SHEET

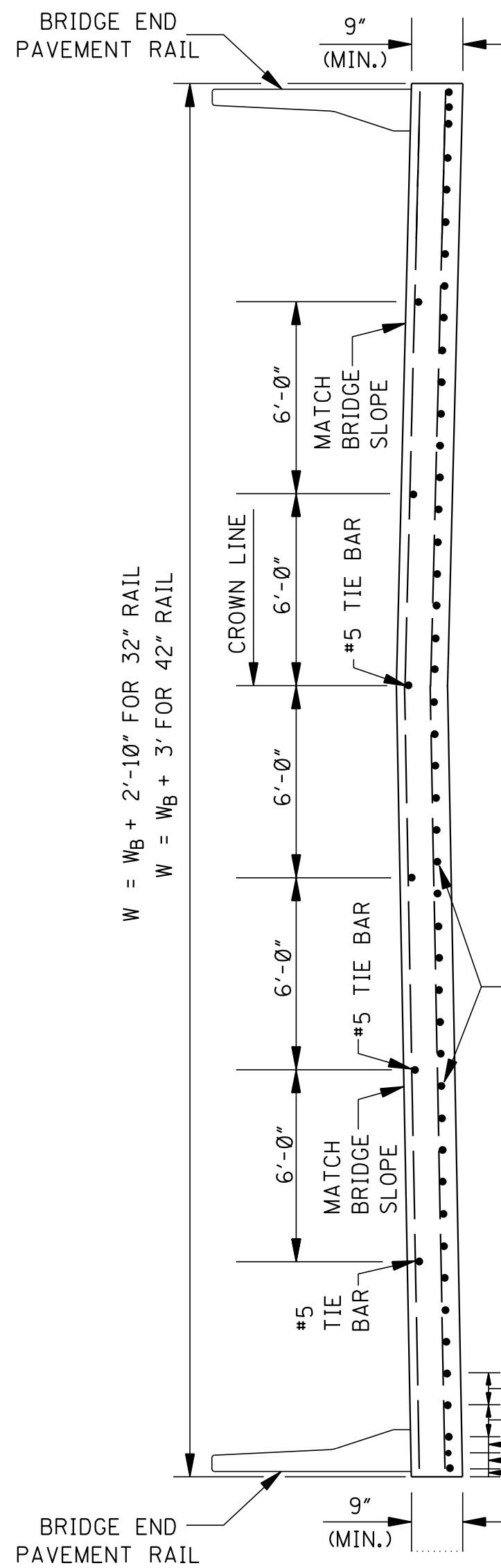
GARVER, LLC		
FINAL PLANS-DATE 04-10-2013		
FMS CON. # 100710/302000		
REVISIONS		
DATE	SHEET NO.	BY
08/19/13	4, 9, 11, 12	TWB
09/25/13	2, 60, 102, 103, 110	SLH



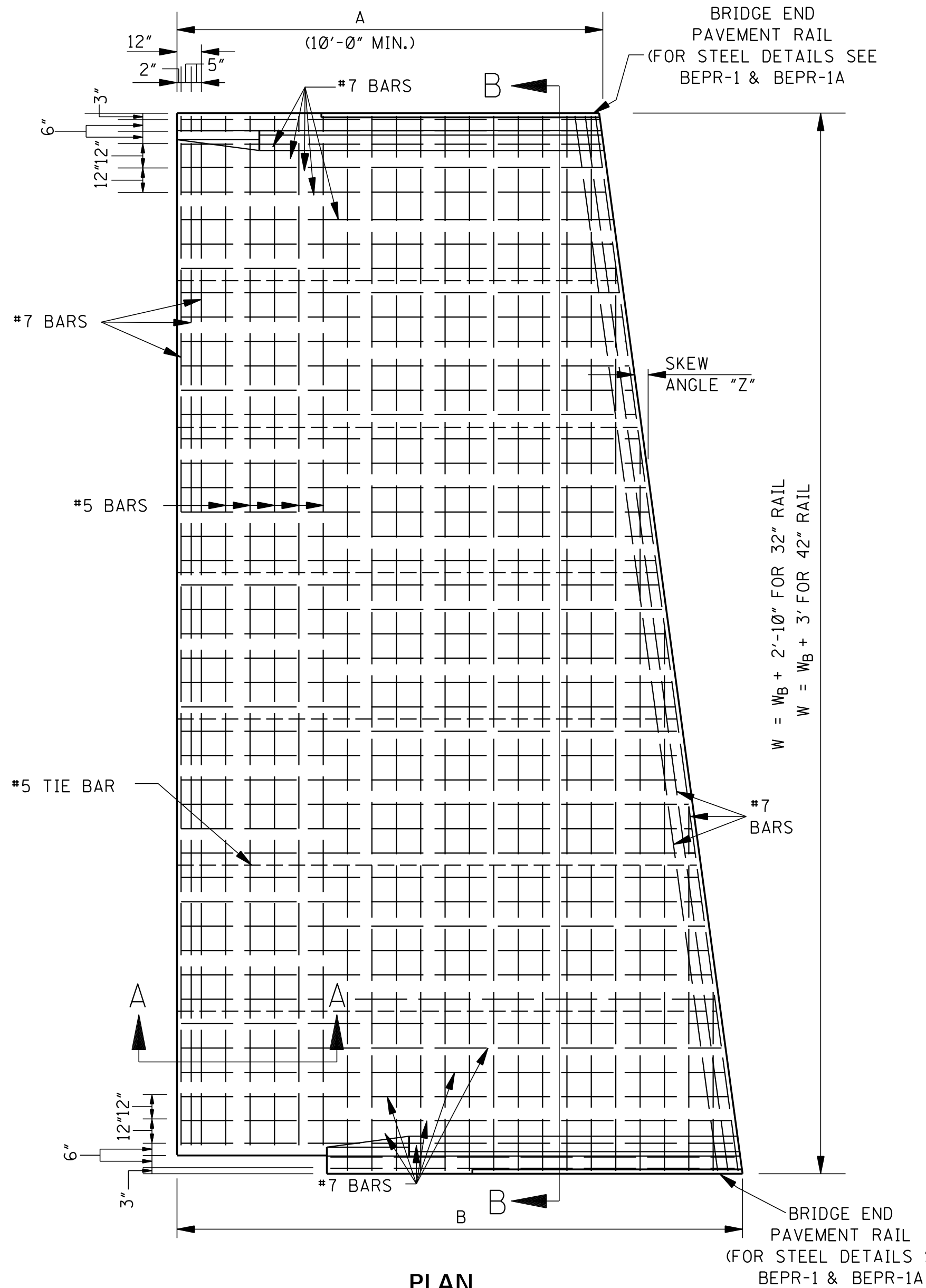
SLH BY ADD * REVISION	MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
	DETAILED INDEX SHEET	
	(ROADWAY)	
	PROJECT NO. SP-9392-00(008)	
JACKSON COUNTY		
FILENAME: 611INDX.DGN		
DESIGN TEAM GARVER CHECKED TWB DATE 04/2013		
09/2013	DATE	WORKING NUMBER D11
		SHEET NUMBER 2

ADDENDUM

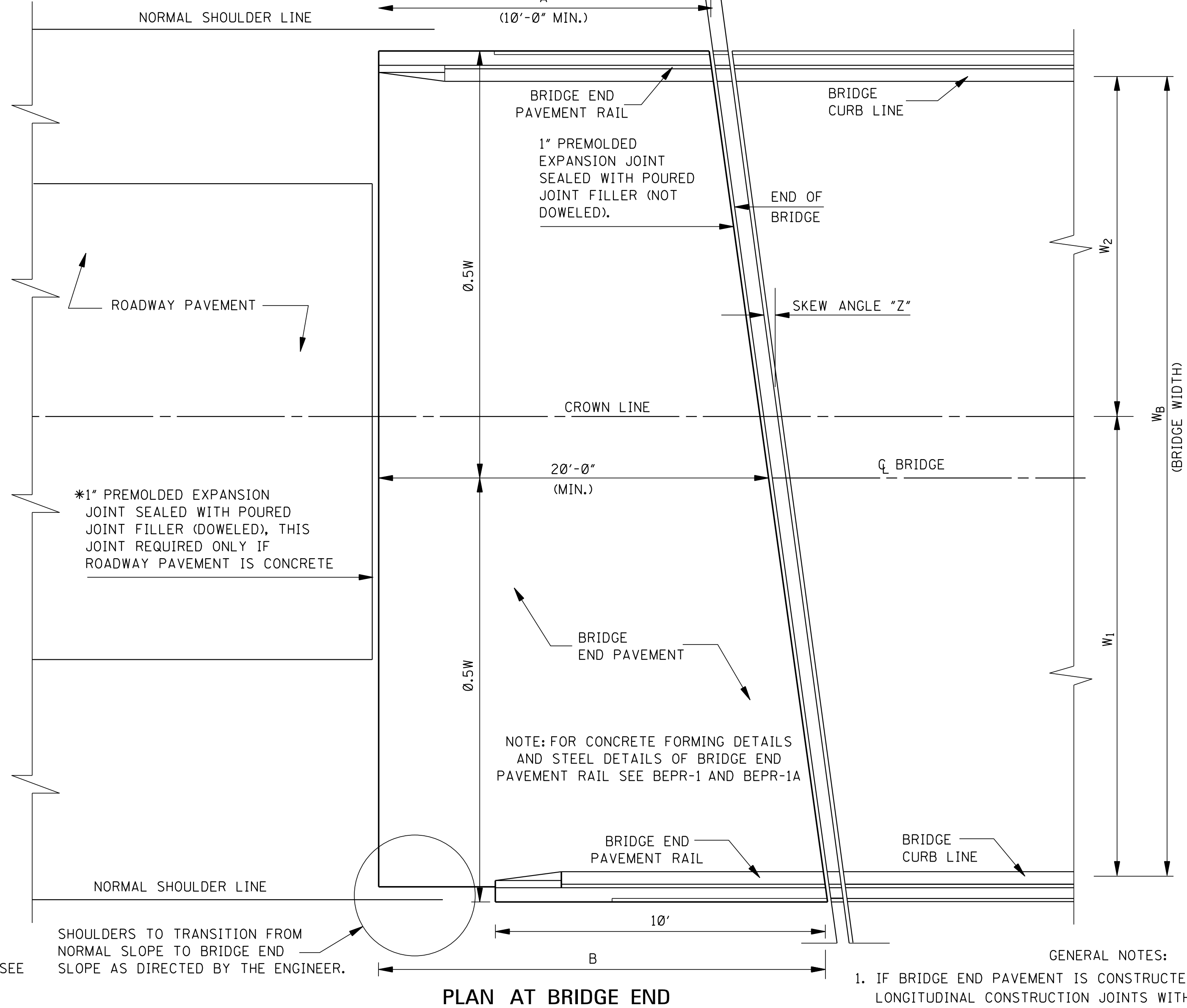
STATE	PROJECT NO.
MISS.	SP-9392-00(008)



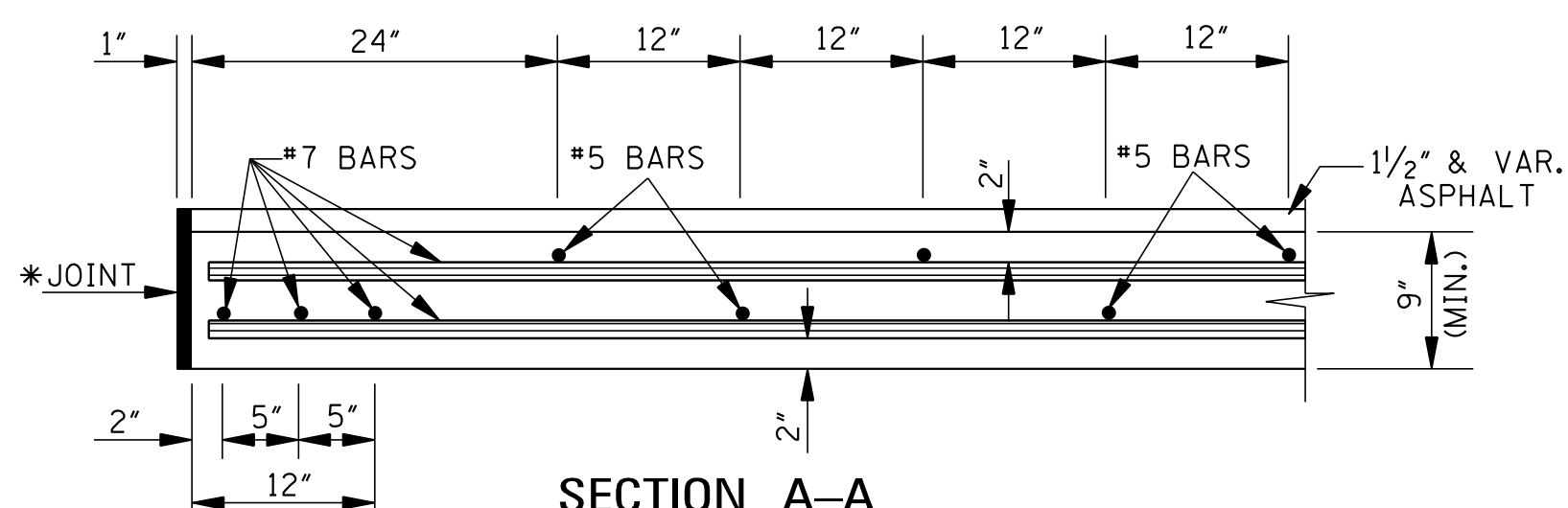
SECTION B-B



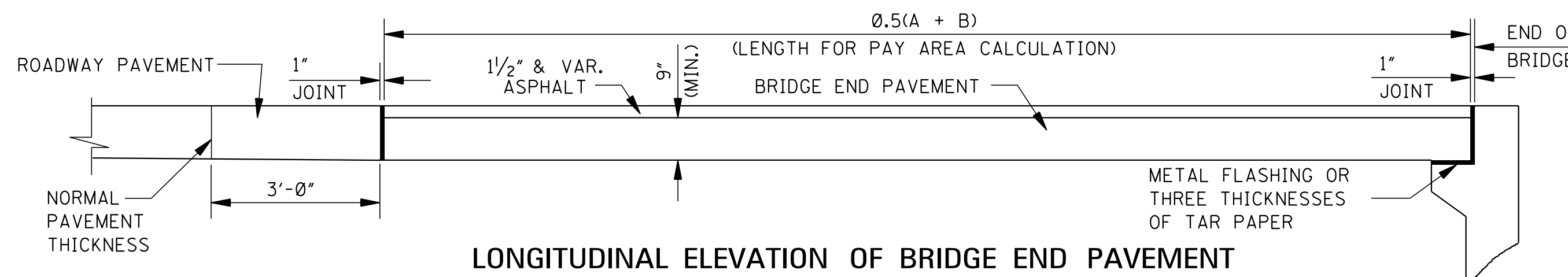
PLAN



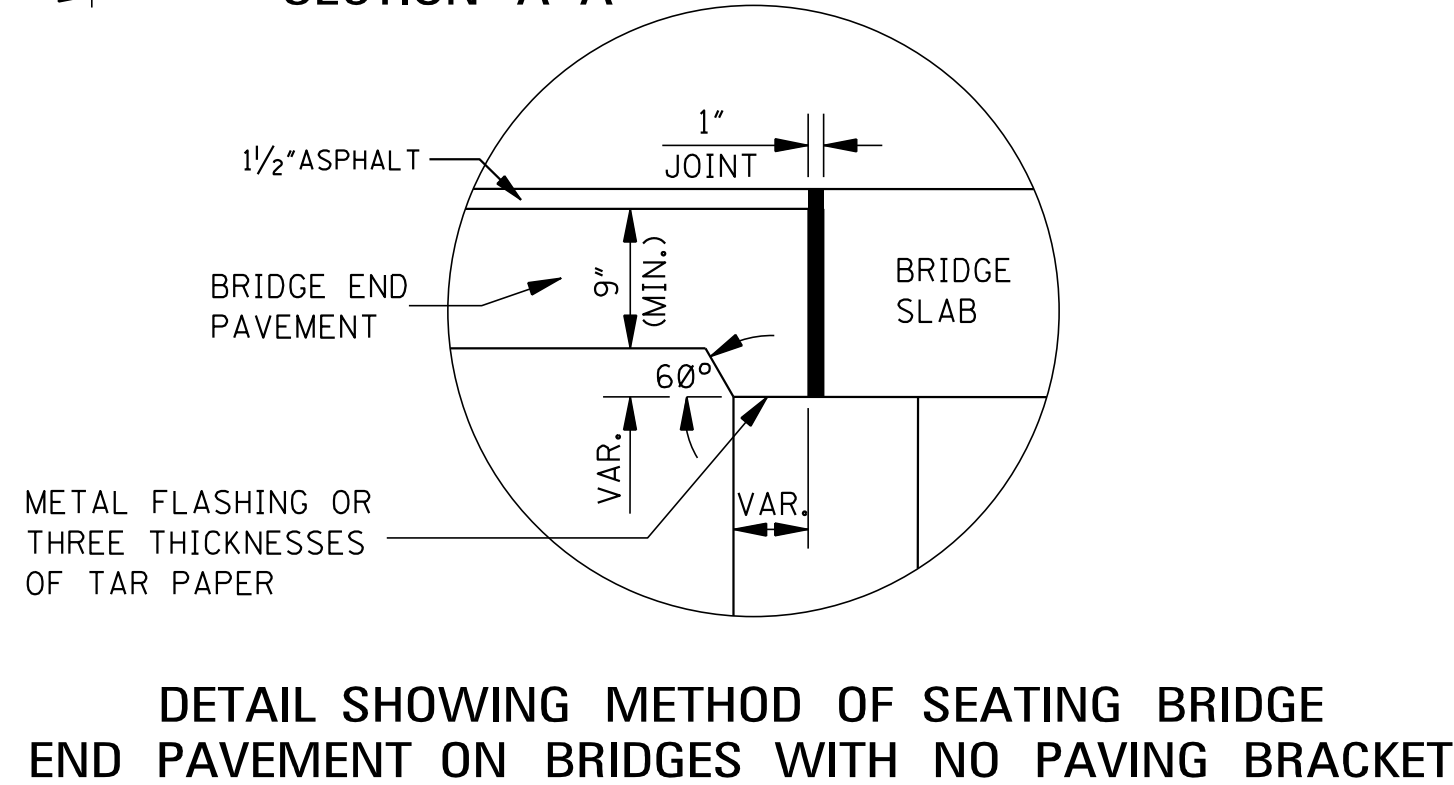
PLAN AT BRIDGE END



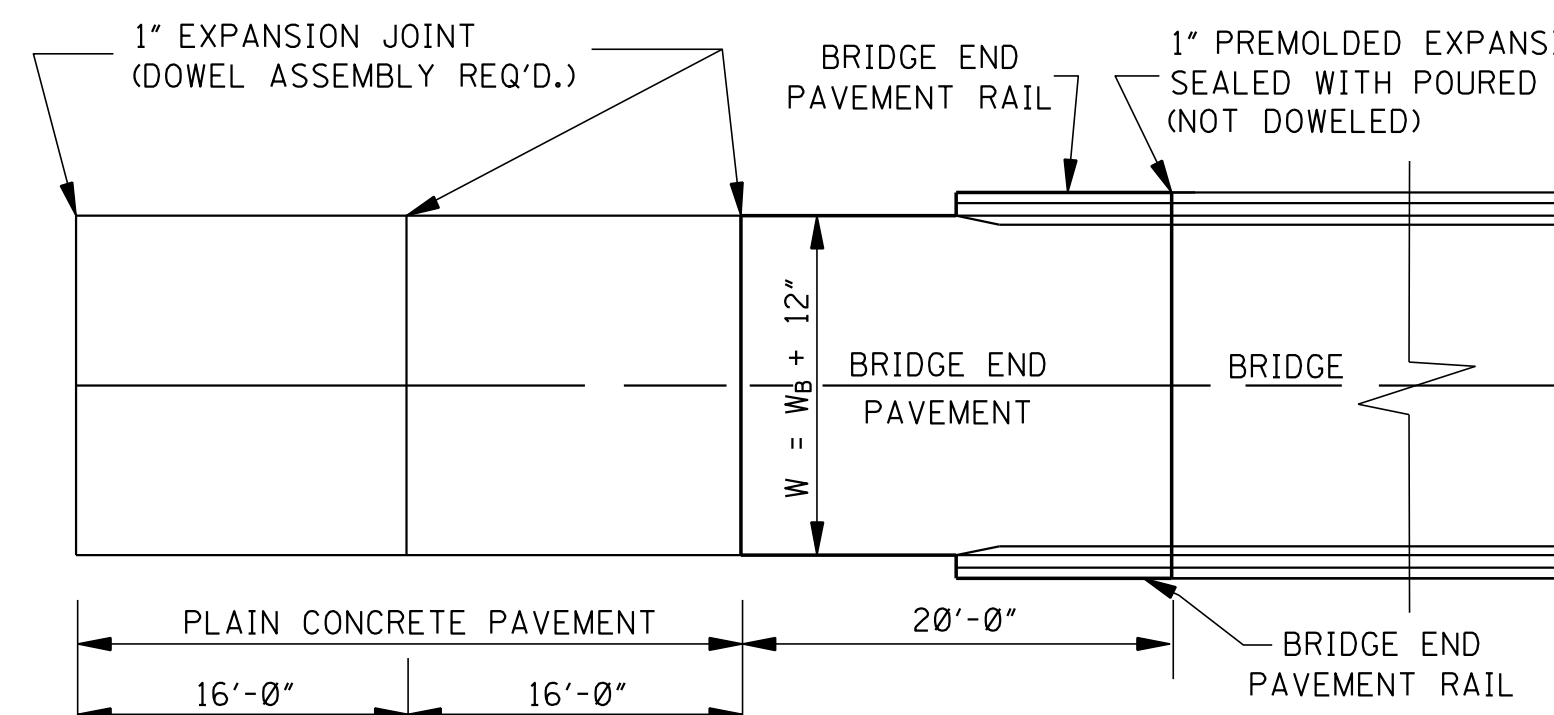
SECTION A-A



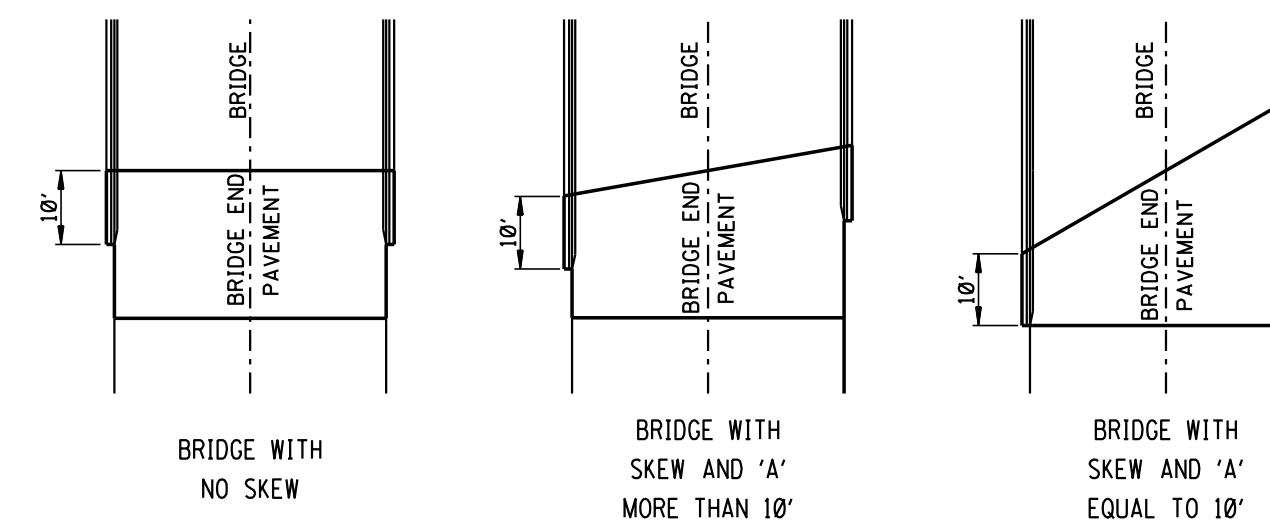
LONGITUDINAL ELEVATION OF BRIDGE END PAVEMENT



DETAIL SHOWING METHOD OF SEATING BRIDGE END PAVEMENT ON BRIDGES WITH NO PAVING BRACKET



PLAN OF EXPANSION JOINTS AND BRIDGE END PAVEMENT



GENERAL NOTES:

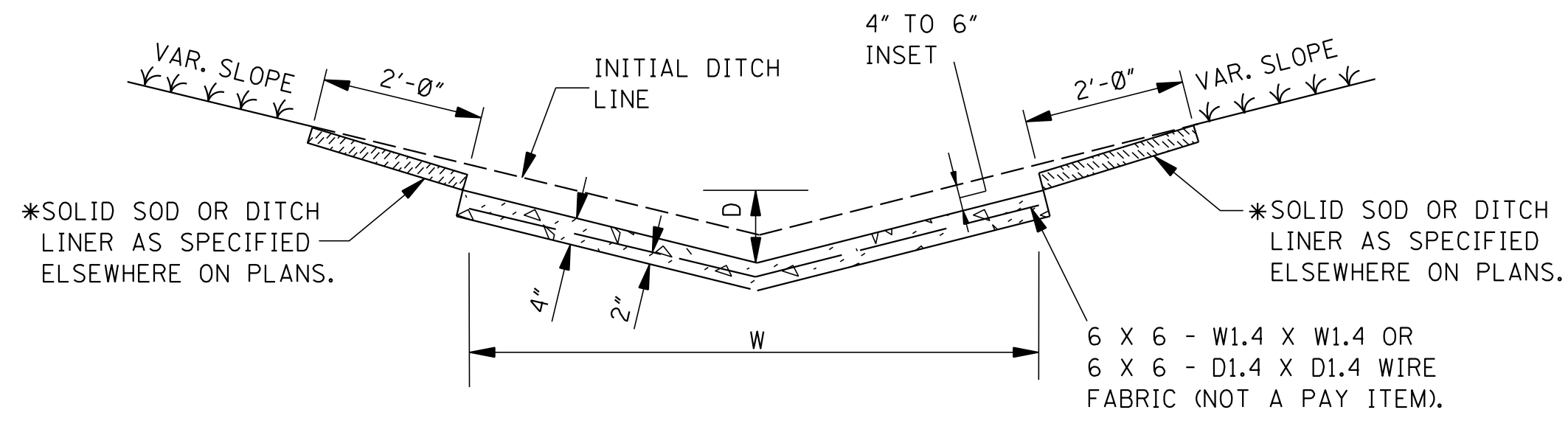
- IF BRIDGE END PAVEMENT IS CONSTRUCTED IN MORE THAN ONE SECTION, LONGITUDINAL CONSTRUCTION JOINTS WITH TIE BARS WILL BE USED. TIE BARS WILL BE #5 BARS, 30" LONG AND SPACED 30" O.C. SUCH CONSTRUCTION WILL BE USED WHERE INDICATED ON PLANS.
- DIMENSIONS "A" & "B" ARE BASED ON A MID-LENGTH OF 20'-0". EXCEPT IN NO CASE SHALL "A" BE LESS THAN 10'-0".
- SEE QUANTITY SECTION OF PLANS FOR DIMENSIONS "W", "W1", "W2", "A", "B", SKEW ANGLE "Z" AND QUANTITIES.
- SPACING OF OUTER LONGITUDINAL EDGE BARS AND TIE BARS MAY BE ADJUSTED TO MEET VARIOUS WIDTHS ("W") OF BRIDGE END PAVEMENT. SPACINGS SHOWN ARE FOR A BRIDGE WIDTH OF 40'-0".
- REINFORCEMENT (DEFORMED) MAY BE FURNISHED FULL LENGTH OR MAY BE LAPPED. IF BARS ARE LAPPED THEY SHALL BE LAPPED NOT LESS THAN 30 DIAMETERS.
- IF TOP LIFT IS ANYTHING OTHER THAN 1.5", THE LIFT SHALL BE TRANSITIONED TO 1.5" ACROSS THE LENGTH OF THE BRIDGE END SLAB.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION		BRIDGE END PAVEMENT WITH RAIL AND OVERLAY	
PROJ NO: SP-9392-00(008)		WORKING NUMBER: BE-1C	
COUNTY: JACKSON		SHEET NUMBER: 60	
FILENAME: BE-1C.DGN		DESIGN TEAM: SPELL CHECKED REEVES DATE 03/17/10	

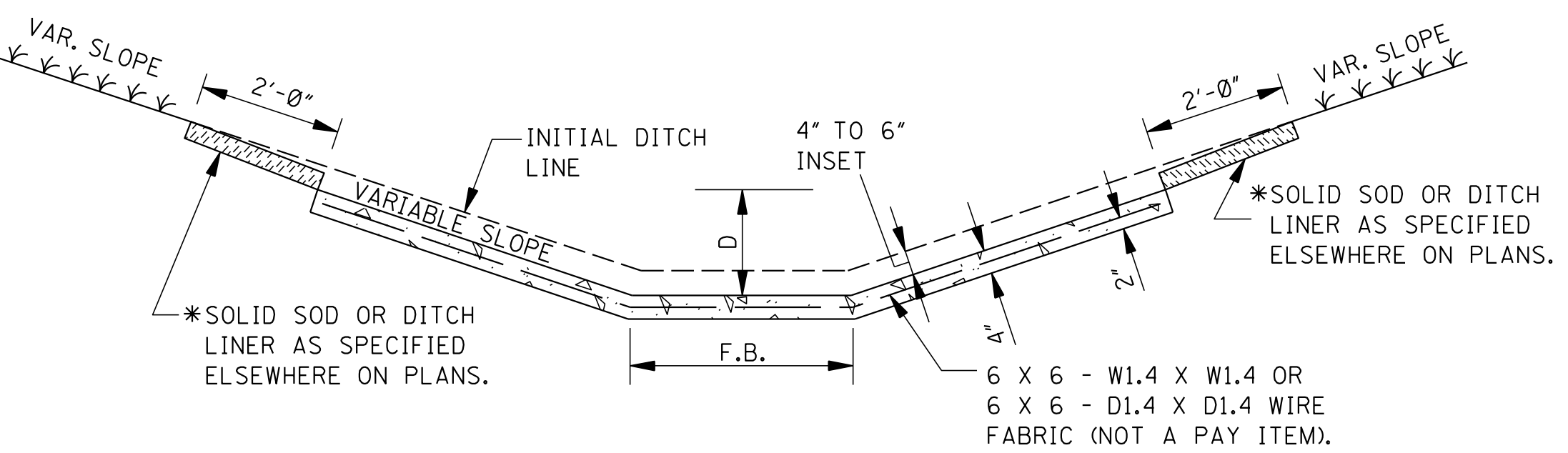
MMDDYY 00:00 ANPK DGN FILE NAME

ADDENDUM

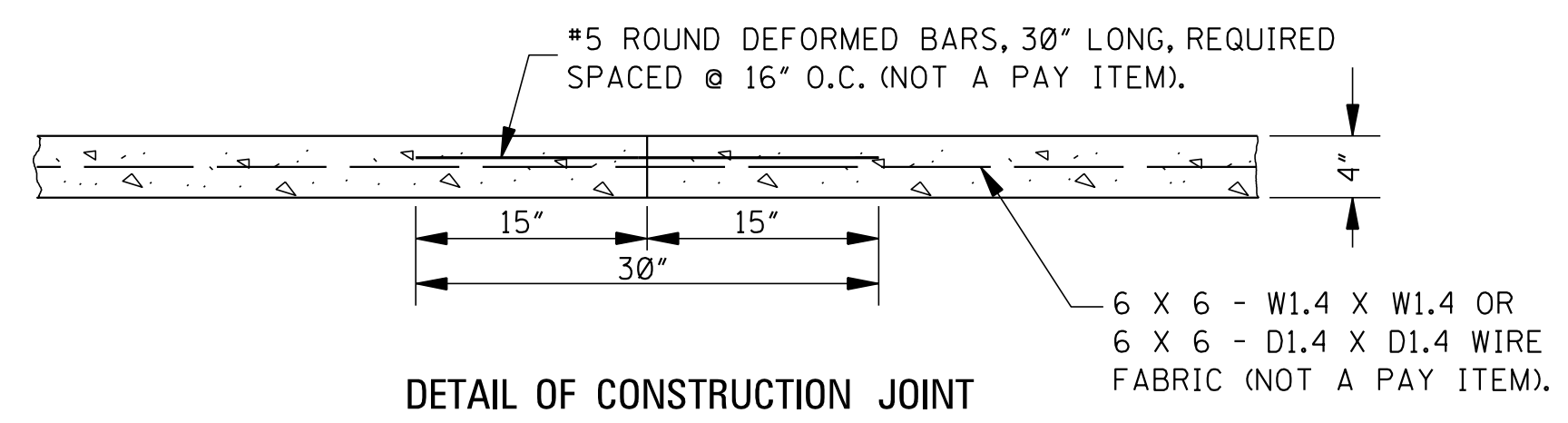
STATE	PROJECT NO.
MISS.	SP-9392-00(008)



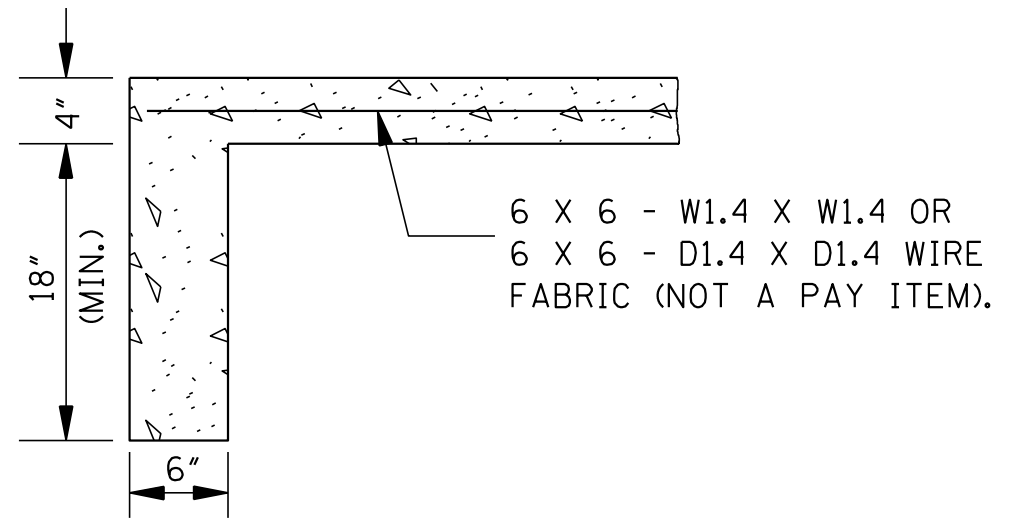
"V" TYPE SECTION



FLAT BOTTOM SECTION



DETAIL OF CONSTRUCTION JOINT

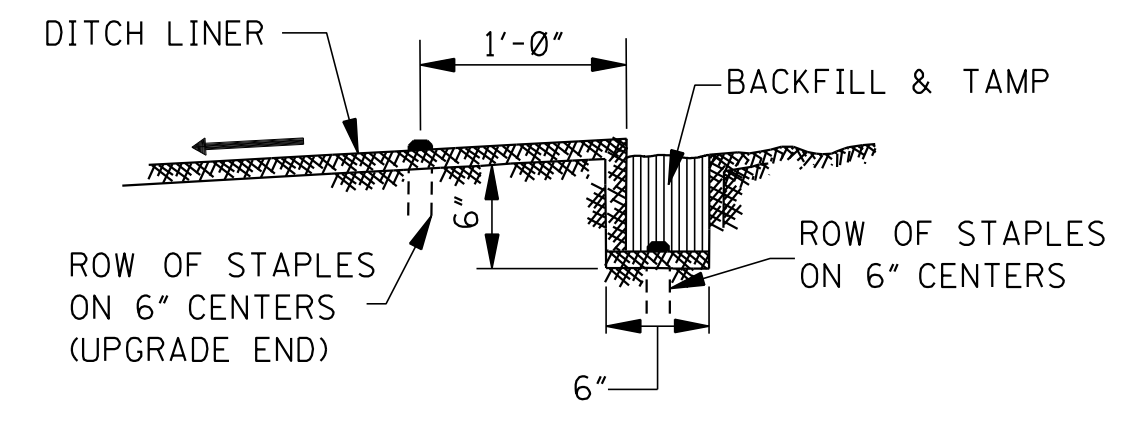


DETAIL OF TOE WALL

NOTE: TOE WALL REQUIRED UPSTREAM AND DOWNSTREAM.

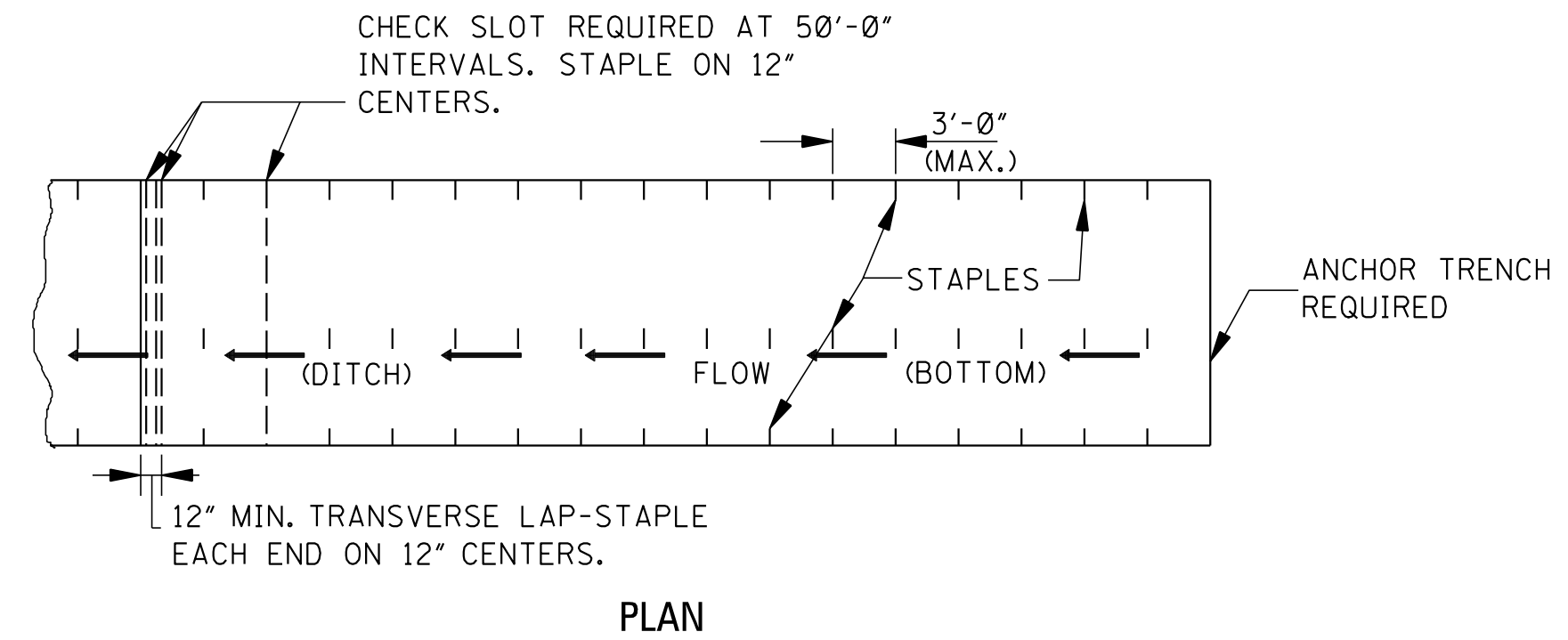
CONCRETE PAVED DITCH

- NOTES:
- CONCRETE PAVED DITCHES SHALL BE GROOVED AT 20'-0" INTERVALS. THE GROOVES SHALL BE CUT TO A DEPTH OF NOT LESS THAN 1".
 - DIMENSIONS D & W ARE AS FOLLOWS:
 D(MINIMUM) = 6"
 D(NOMINAL) = 9"
 W(MINIMUM) = 24"
 - CHAIR SUPPORTS FOR THE WIRE MESH WILL NOT BE REQUIRED. HOWEVER, THE CONTRACTOR SHALL PLACE THE WIRE MESH IN A SATISFACTORY AND WORKMANLIKE MANNER TO ENSURE THAT THE FINAL POSITION IS REASONABLY NEAR THE POSITION INDICATED.
 - CENTER ROW OF STAPLES MAY BE OMITTED ON DITCH LINER.

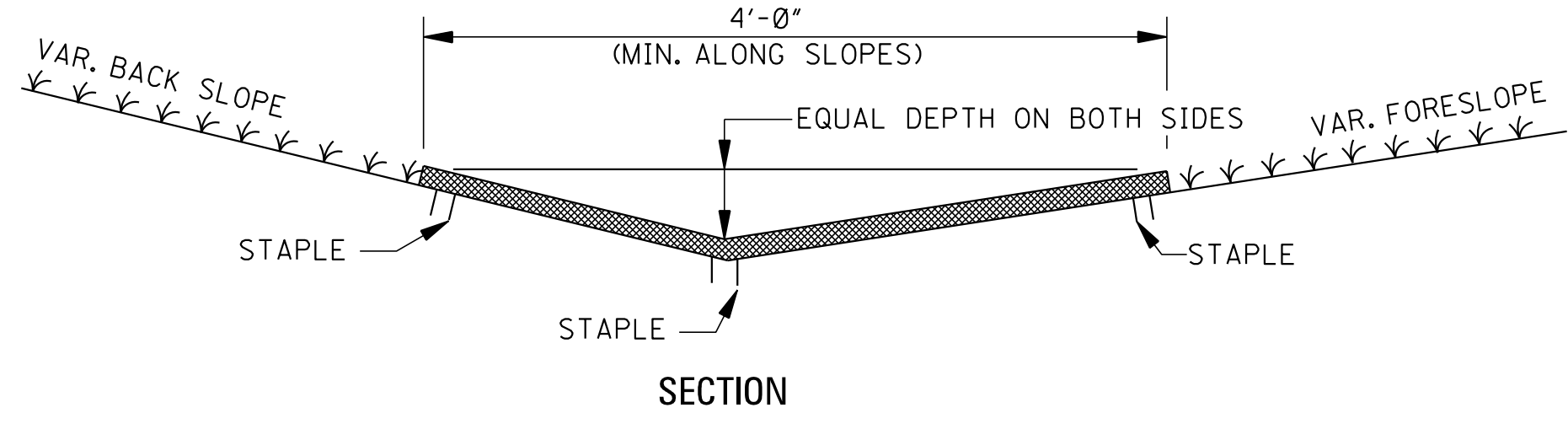


ANCHOR TRENCH DETAIL

NOTE: ANCHOR TRENCH REQUIRED AT THE BEGINNING AND ENDING OF EACH AREA TO BE COVERED, EXCEPT DOWNSTREAM END ADJOINING A STRUCTURE.



PLAN

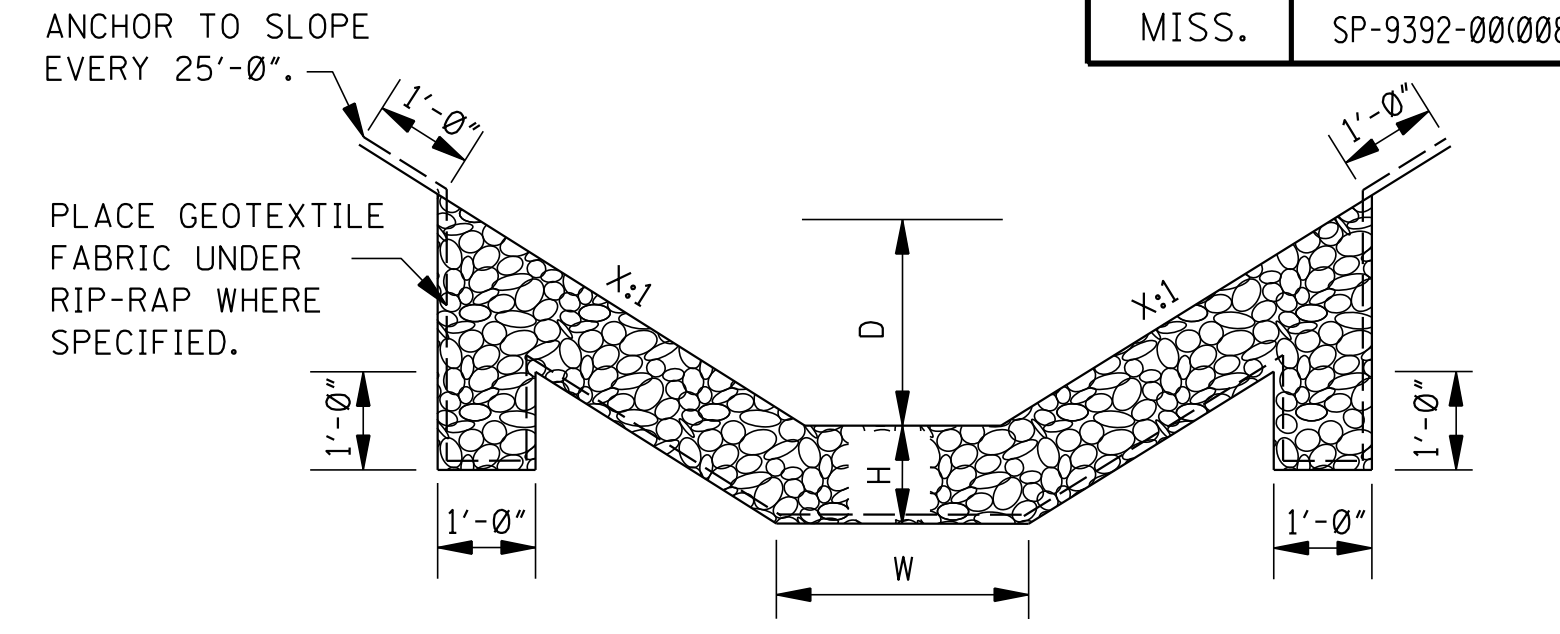


SECTION

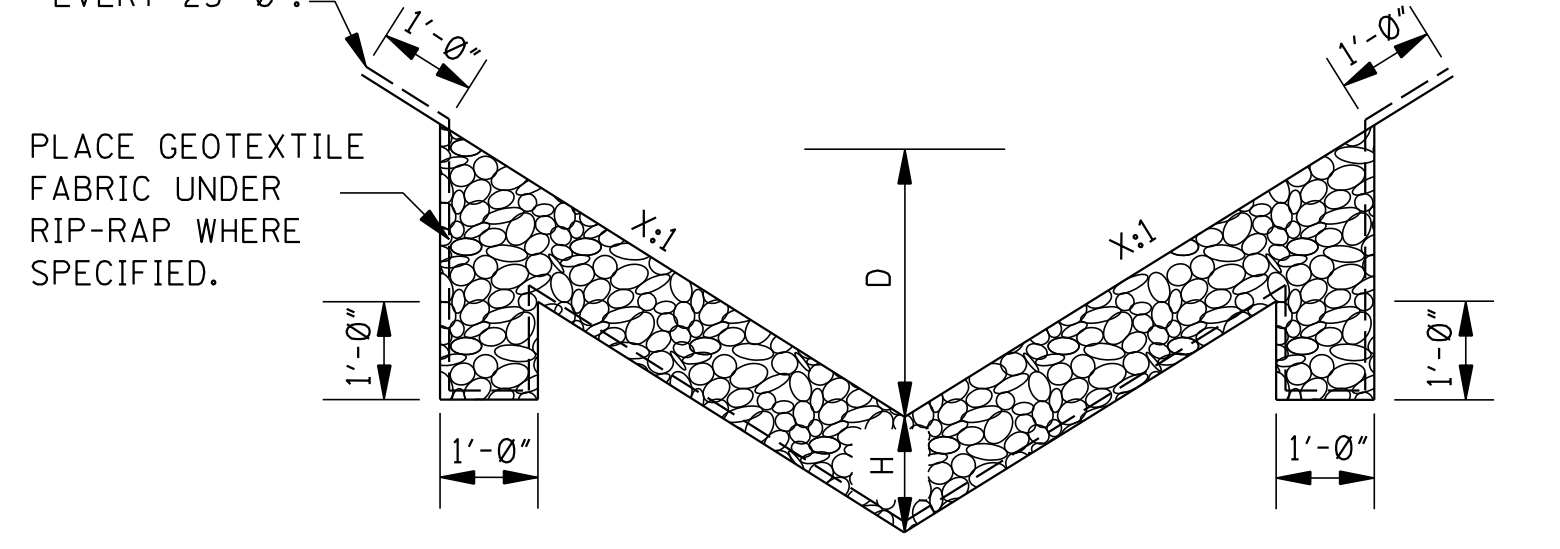
DITCH LINER TREATMENT

(EXCELSIOR BLANKET, JUTE MESH OR EROSION CONTROL FABRIC)

NOTE: DITCHES TREATED WITH DITCH LINER WILL BE VEGETATED PRIOR TO TREATMENT, UNLESS OTHERWISE INDICATED.



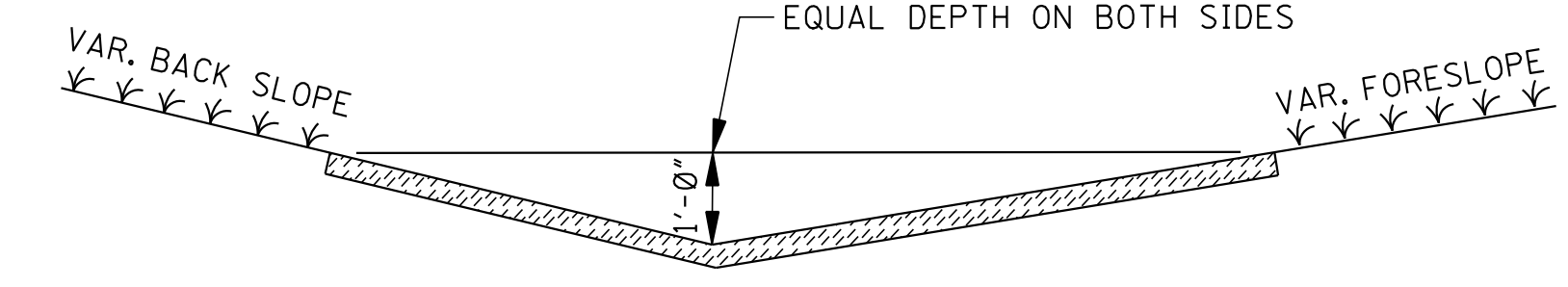
FLAT BOTTOM SECTION



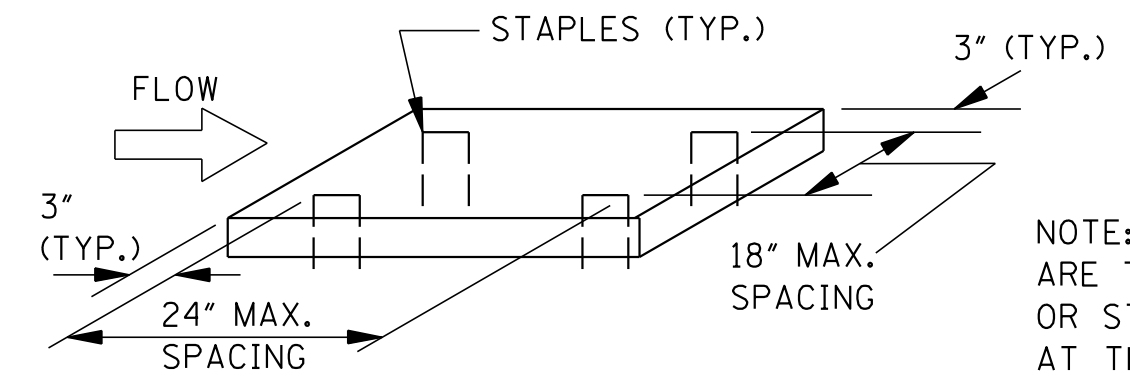
"V" TYPE SECTION
RIP-RAP TREATMENT

- NOTES:
- DIMENSIONS D, W AND X ARE VARIABLE AND ARE SHOWN ELSEWHERE ON THE PLANS.
 - THE RIP-RAP SIZE AND MINIMUM DEPTH "H" FOR RIP-RAP TREATMENT ARE AS FOLLOWS.

RIP-RAP SIZE & MINIMUM DEPTH "H"	
H (in)	RIP-RAP SIZE (lbs)
12"	100
18"	300



SOLID SOD TREATMENT



NOTE: SOLID SOD (STRIPS OR BLOCKS) ARE TO BE STAPLED, PINNED, PEGGED OR STAKED AT THE FOUR CORNERS OR AT THE MAXIMUM SPECIFIED SPACING.

- GENERAL NOTE:
- FOR LOCATION OF APPROPRIATE DITCH TREATMENTS, SEE PLAN SHEETS AS DENOTED BY THE FOLLOWING LEGEND OR AS DIRECTED BY THE ENGINEER:

- DITCH LINER
- SOLID SOD
- CONCRETE PAVED DITCH
- RIP-RAP

SLH	BY
09/13	DATE
CHANGE	REVISION

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

DETAILS OF TYPICAL DITCH TREATMENTS

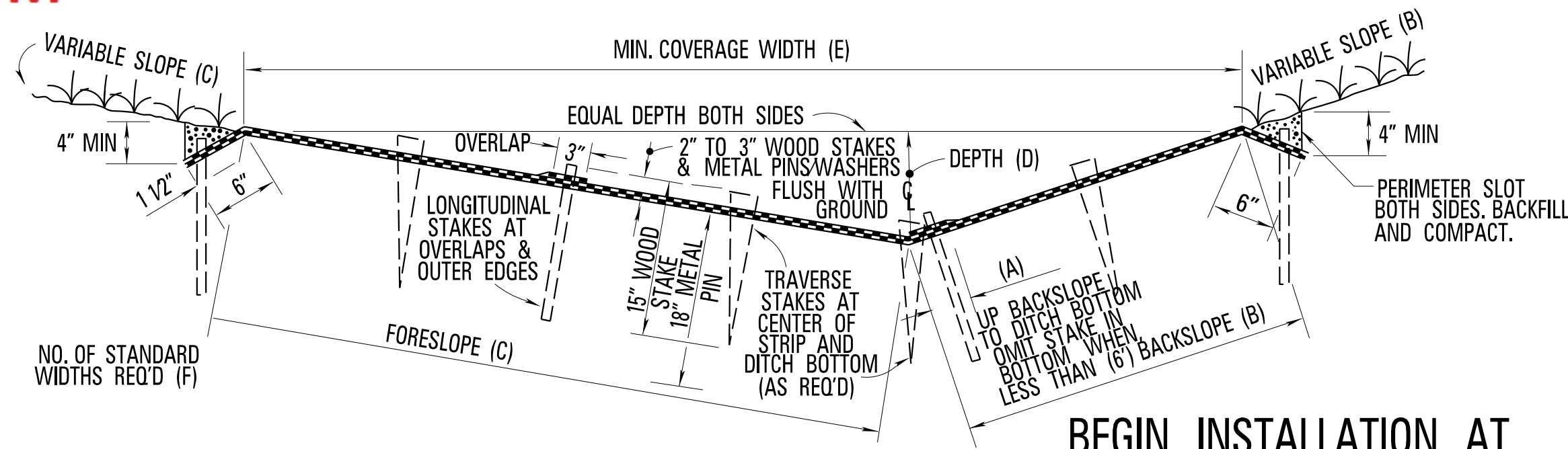
PROJECT NO. SP-9392-00(008)
JACKSON COUNTY



WORKING NUMBER	DT-1
SHEET NUMBER	102

FILENAME:	DT-1.DGN
DESIGN TEAM	GARVER
CHECKED	SLH
DATE	10-10

MMDDYY 00:00 ANPM DGNFILENAME MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY PLAN DIVISION



TYPICAL DITCH SECTION

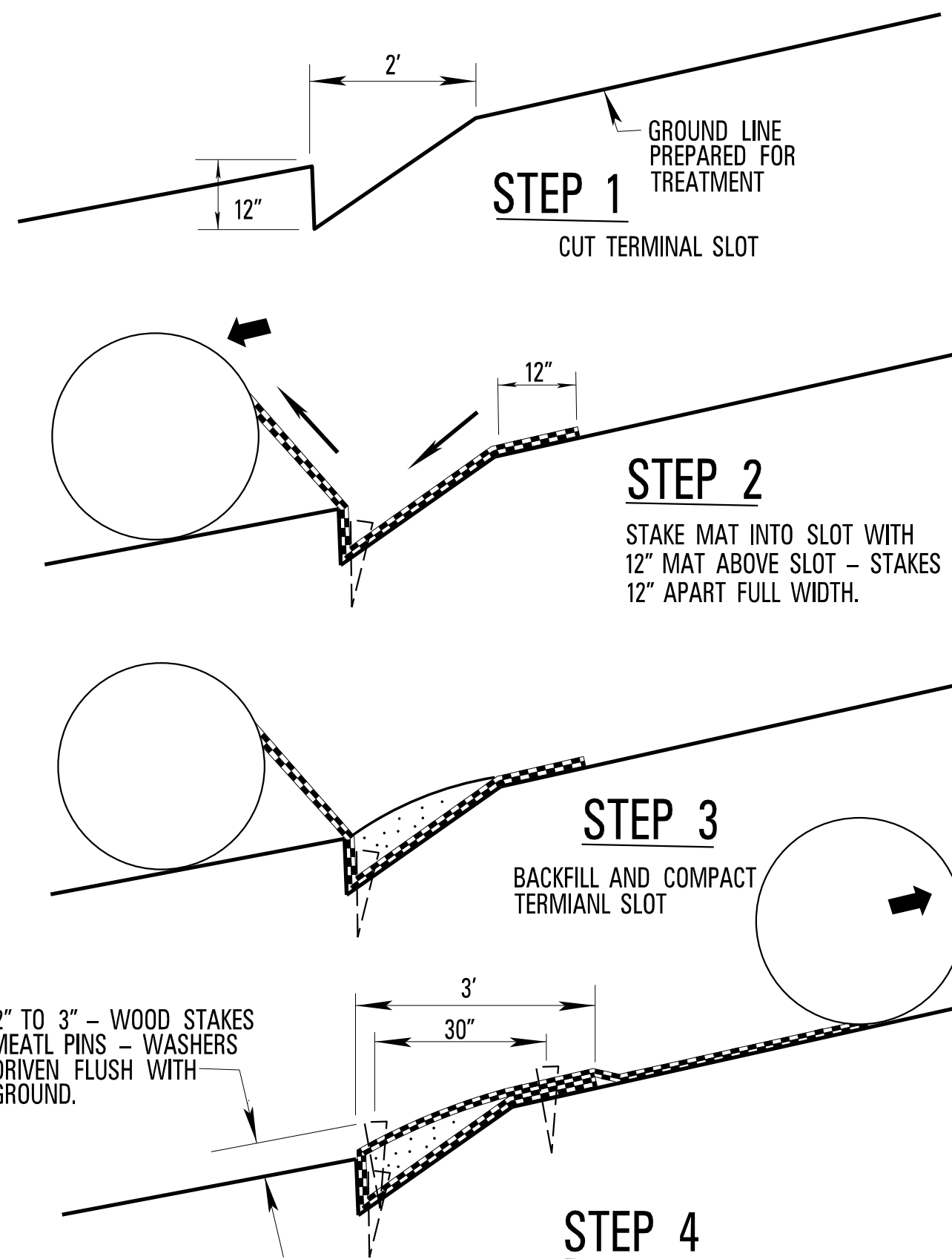
MAT PLACEMENT TABLE

DIMENSIONS OF MAT PLACEMENT IN DITCH (INDIVIDUAL 38" WIDTH ROLLS)					
ELEMENTS OF MAT PLACEMENT	SIDE SLOPE COMBINATIONS FORESLOPE - BACKSLOPE				
	3:1 & 3:1	4:1 & 3:1	6:1 & 3:1	6:1 & 4:1	6:1 & 6:1
(A) UP BACKSLOPE TO DITCH BOTTOM	1' - 7"	1' - 1"	0' - 4"	0' - 10"	0' - 1 1/2"
(B) BACKSLOPE	4' - 0"	3' - 6"	2' - 9"	3' - 3"	3' - 5 1/2"
(C) FORESLOPE	4' - 0"	4' - 6"	5' - 3"	4' - 9"	5' - 5 1/2"
(D) DEPTH OF COVERAGE	1' - 3"	1' - 1"	0' - 10"	0' - 9"	0' - 11"
(E) WIDTH OF COVERAGE	7' - 7"	7' - 8"	7' - 9"	7' - 10"	10' - 9"
(F) MINIMUM NUMBER OF STAND WIDTH STRIPES	3	3	3	3	4
(B) + (C) TOTAL COVERAGE ON SLOPES	8' - 0"	8' - 0"	8' - 0"	8' - 0"	10' - 11"
SQ. YDS. /LIN. FT.	0.89	0.89	0.89	0.89	1.22
MULTI-WIDTH WELDED SEAM MAT (WELDED 38" WIDTH STRIPS)					
(B) + (C) TOTAL COVERAGE MULTI-WIDTH ROLLS	8' - 3"	8' - 3"	8' - 3"	8' - 3"	11' - 3 1/2"
SQ. YDS. /LIN. FT.	0.92	0.92	0.92	0.92	1.25

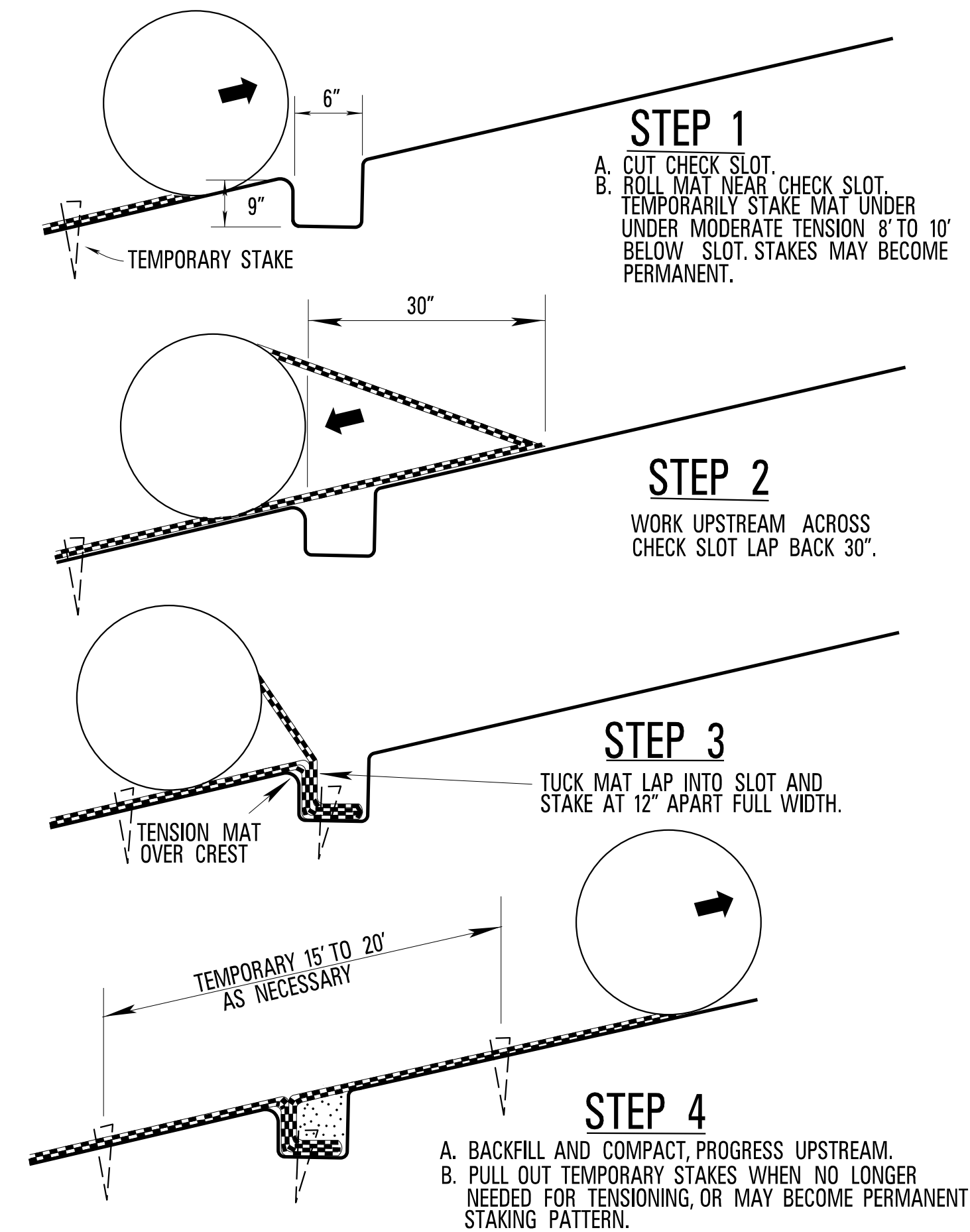
GENERAL INSTRUCTIONS

- BEGIN INSTALLATION AT DOWNSTREAM TERMINAL AND PROGRESS UPSTREAM.
- FIRST ROLL IS ALIGNED FROM DITCH BOTTOM UP BACKSLOPE (SEE MAT PLACEMENT TABLE) AND UNDER MODERATE TENSION TEMPORARILY STAKED TO MAINTAIN PROPER DESIGN COVERAGE ALIGNMENT.
- WORKING OUTWARD FROM DITCH BOTTOM TO EDGES, SUBSEQUENT ADJACENT ROLLS FOLLOW IN STAGGERED SEQUENCE UNDER MODERATE TENSION.
- OVERLAP MAT SEAMS 3 INCHES AND STAKE AT 3 FOOT INTERVALS WITH STAKES ALIGNED LONGITUDINALLY TO DITCH AND DIAGONAL EDGE OF STAKE TO THE UPSTREAM. OUTER EDGES (PERIMETER) OF MAT ARE STAKED SIMILARLY.
- STAKE THE CENTER OF EACH MAT STRIP AND WHEN REQUIRED ALONG THE DITCH BOTTOM AT 6 - FOOT INTERVALS STAGGERED BETWEEN THE 3 - FOOT SPACING OF OVERLAP AND OUTER EDGE STAKES WITH THE BROADSIDE TO THE FLOW DIRECTION AND DIAGONAL EDGE TOWARD THE UPSLOPE.
- USE 3 - FOOT OVERLAP AT END OF MAT ROLL SPLICES WITH UPGRADE STRIP ON TOP, STAKED IN TWO ROWS 30 INCHES APART, AND STAKES 18 INCHES APART ACROSS FULL WIDTH.
- TRANSVERSE CHECK SLOTS 6 - INCH WIDTH BY 9 - INCH DEPTH ARE EXCAVATED AT 25 - FOOT INTERVALS WITH STAKES 12 INCHES APART FULL WIDTH OF TREATMENT, WELDED SEAM MULTI-WIDTH MAT WILL HAVE SIMILAR TRANSVERSE CHECKS OMITTING EXCAVATED SLOT ONLY.
- END INSTALLATION AT UPSTREAM TERMINAL. TEMPORARILY STAKING MAY BE PLACED TO BECOME PART OF PERMANENT STAKING PATTERN.

BEGIN INSTALLATION AT DOWNSTREAM TERMINAL

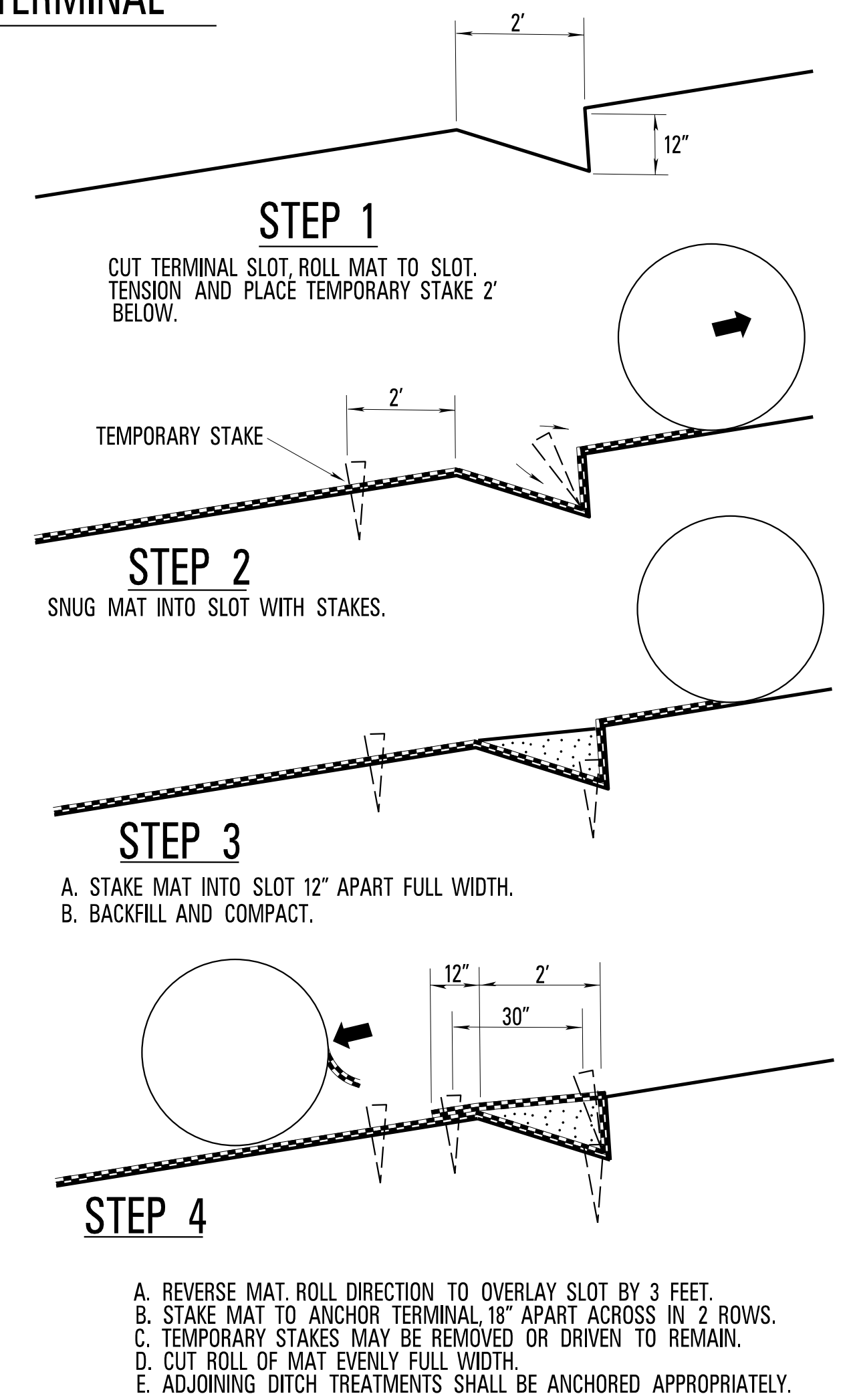


25 - FOOT INTERVAL TRANSVERSE CHECK SLOT (FOR INDIVIDUAL ROLLS*)



* WHEN MULTI-WIDTH (WELDED SEAM) ROLLS ARE USED, OMIT EXCAVATED CHECK SLOT AND REPLACE WITH A ROW OF STAKES ONE (1) FOOT APART AT 25 - FOOT INTERVALS (SEE DETAILS). TEMPORARY STAKES NOT REQUIRED.

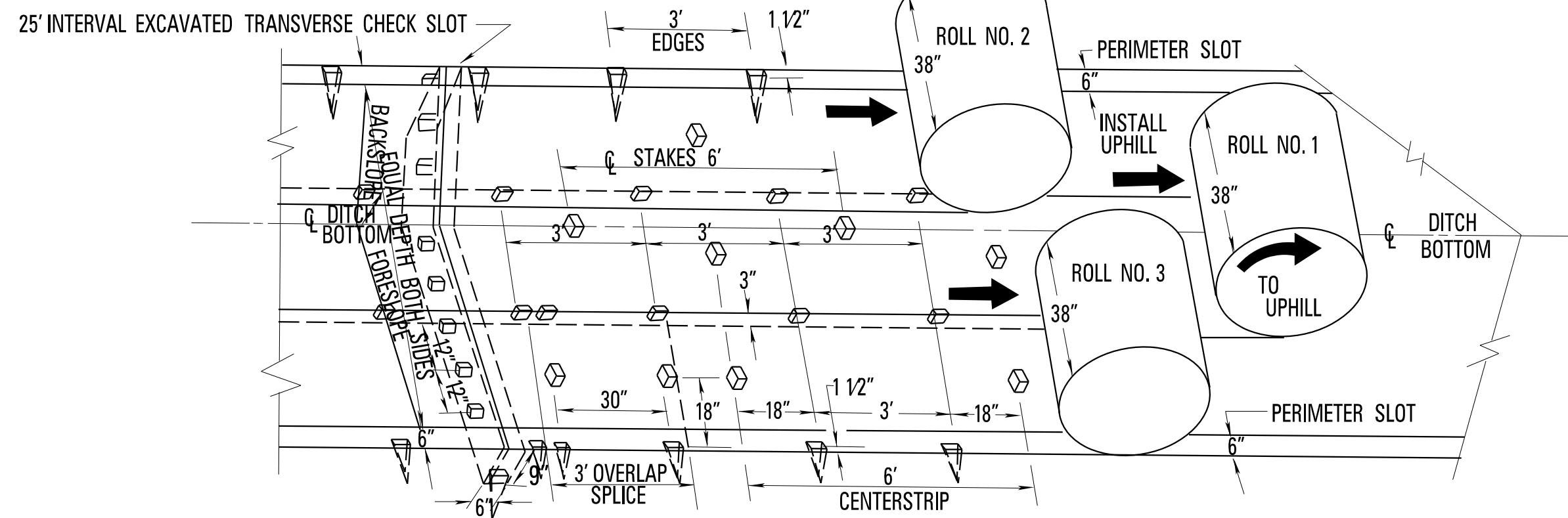
END INSTALLATION AT UPSTREAM TERMINAL



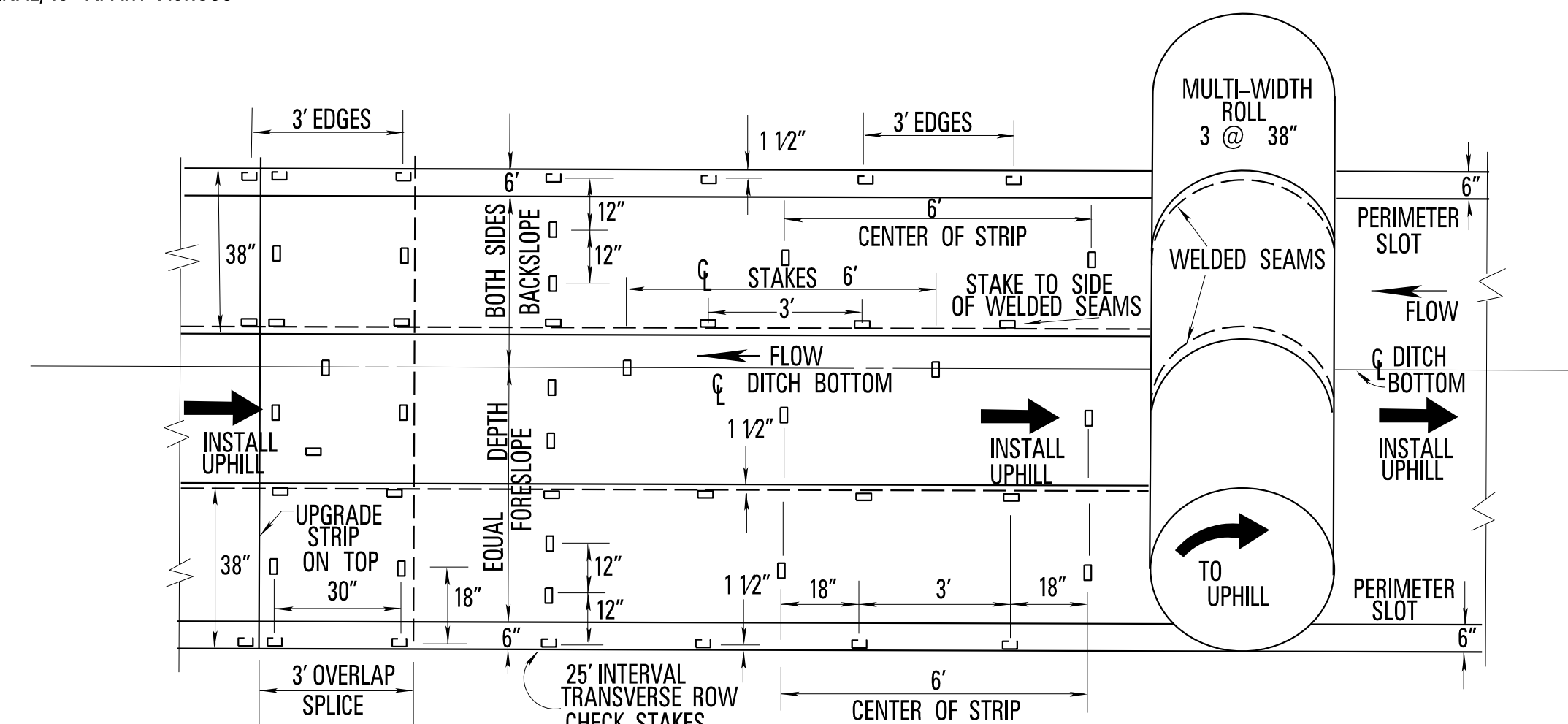
GENERAL NOTES

- WHEN METAL PINS WITH WASHERS ARE PERMITTED IN PLACE OF WOOD STAKES, THE METAL PINS ARE DRIVEN TO ASSURE THAT THE WASHERS WITH MAT UNDERNEATH ARE FLUSH WITH THE GROUND LEAVING NO PROJECTION OF THE PINS ABOVE THE GROUND LINE.
- SOIL REINFORCING MAT SHALL BE USED WHERE SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER. THE FOLLOWING DESIGNATION SHALL BE USED.

SOIL REINFORCING MAT



SEQUENTIAL ROLL RUN OUT IN DITCH WITH STAKING DETAIL

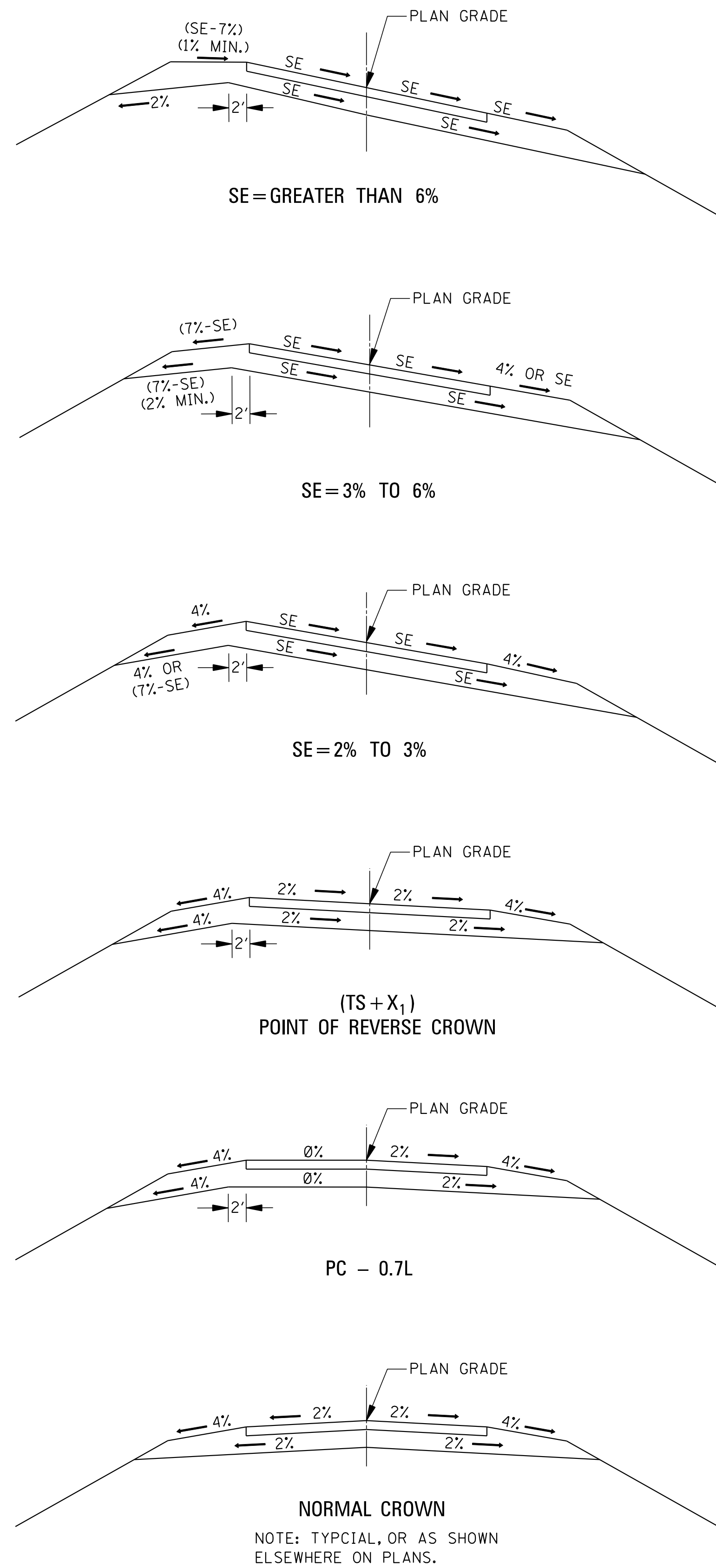


MULTI-WIDTH WELDED SEAM MAT RUN OUT IN DITCH WITH STAKING DETAIL

MISSISSIPPI DEPARTMENT OF TRANSPORTATION			
DITCH TREATMENT INSTALLATION DETAIL FOR SOIL REINFORCING MAT			
PROJECT NO. SP-9392-00(008)			
JACKSON COUNTY			
09/13	CHANGE	STAMP TO ENGLISH	SLH
DATE	REVISION	BY	
FILENAME: dt-1a.dgn			WORKING NUMBER: DT-1A
DESIGN TEAM: GARVER			CHECKED: SLH
DATE: 10-10			SHEET NUMBER: 103

MMDDYY 00:00 AM PM DGN FILE NAME MISSISSIPPI DEPARTMENT OF TRANSPORTATION

MINIMUM RADII FOR DESIGN SUPERELEVATION RATES, DESIGN SPEEDS, AND $e_{max} = 0.100$



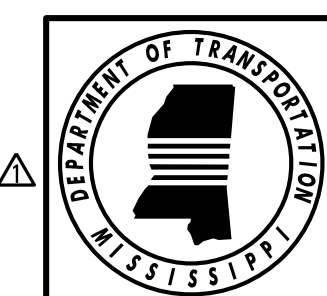
DETAILS OF SHOULDER & SUBGRADE TREATMENT

e	V = 30 mph	V = 35 mph	V = 40 mph	V = 45 mph	V = 50 mph	V = 55 mph	V = 60 mph	V = 65 mph	V = 70 mph
	R (ft)	R (ft)	R (ft)	R (ft)	R (ft)	R (ft)	R (ft)	R (ft)	R (ft)
NC	3320	4350	5520	6830	8280	9890	11700	13100	14700
0.020	2440	3210	4080	5050	6130	7330	8630	9720	10900
0.022	2200	2900	3680	4570	5540	6630	7810	8800	9860
0.024	2000	2640	3350	4160	5050	6050	7130	8040	9010
0.026	1840	2420	3080	3820	4640	5550	6550	7390	8290
0.028	1690	2230	2840	3520	4280	5130	6050	6840	7680
0.032	1450	1920	2450	3040	3700	4440	5250	5930	6680
0.034	1360	1790	2290	2850	3470	4160	4910	5560	6260
0.036	1270	1680	2150	2670	3250	3900	4620	5230	5900
0.038	1190	1580	2020	2510	3060	3680	4350	4940	5570
0.040	1120	1490	1900	2370	2890	3470	4110	4670	5270
0.042	1060	1400	1800	2240	2740	3290	3900	4430	5010
0.044	994	1330	1700	2120	2590	3120	3700	4210	4760
0.046	940	1260	1610	2020	2460	2970	3520	4010	4540
0.048	890	1190	1530	1920	2340	2830	3360	3830	4340
0.050	844	1130	1460	1830	2240	2700	3200	3660	4150
0.052	802	1080	1390	1740	2130	2580	3060	3500	3980
0.054	762	1030	1330	1660	2040	2460	2930	3360	3820
0.056	724	974	1270	1590	1950	2360	2810	3220	3670
0.058	689	929	1210	1520	1870	2260	2700	3090	3530
0.060	656	886	1160	1460	1790	2170	2590	2980	3400
0.062	624	846	1110	1400	1720	2090	2490	2870	3280
0.064	594	808	1060	1340	1650	2010	2400	2760	3160
0.066	564	772	1020	1290	1590	1930	2310	2670	3060
0.068	536	737	971	1230	1530	1860	2230	2570	2960
0.070	509	704	931	1190	1470	1790	2150	2490	2860
0.072	483	671	892	1140	1410	1730	2070	2410	2770
0.074	460	641	855	1100	1360	1670	2000	2330	2680
0.076	437	612	820	1050	1310	1610	1940	2250	2600
0.078	416	585	786	1010	1260	1550	1870	2180	2530
0.080	396	558	754	968	1220	1500	1810	2120	2450
0.082	377	533	722	930	1170	1440	1750	2050	2380
0.084	359	509	692	893	1130	1390	1690	1990	2320
0.086	341	486	662	856	1080	1340	1630	1930	2250
0.088	324	463	633	820	1040	1290	1570	1870	2190
0.090	307	440	604	784	992	1240	1520	1810	2130
0.092	291	418	574	748	948	1190	1460	1740	2060
0.094	274	395	545	710	903	1130	1390	1670	1990
0.096	256	370	513	671	854	1080	1320	1600	1910
0.098	236	343	477	625	798	1010	1250	1510	1820
$e_{max} = 0.100$	$R_{min} = 200$	$R_{min} = 292$	$R_{min} = 410$	$R_{min} = 540$	$R_{min} = 694$	$R_{min} = 877$	$R_{min} = 1090$	$R_{min} = 1340$	$R_{min} = 1630$

KEY:
V = DESIGN SPEED (mph)
R = RADIUS (ft)
e = FULL SUPERELEVATION RATE (ft/ft)
NC = NORMAL CROWN

GENERAL NOTES:

- SE RATE IS DETERMINED FROM A RADIUS EQUAL TO, OR SLIGHTLY SMALLER THAN, THE RADIUS OF THE CURVE.
- SEE SHEET SDR0-1 FOR SE RUNOFF VALUES.
- STATE AID DIVISION: USE STANDARD SA-SE-1.

SLH	BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION SUPERELEVATION CASE I ROTATION ABOUT CENTERLINE (2% NORMAL SUBGRADE)	 WORKING NUMBER SDSE-2A SHEET NUMBER 110
CHANGE	REVISION		
DATE	ISSUE DATE: _____		