

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) enclose a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. 1 DATED 6/19/2014 ADDENDUM NO. DATED
 ADDENDUM NO. 2 DATED 07/16/2014 ADDENDUM NO. DATED

Number	Description
1	Revised Table of Contents; Add NTB Nos. 5117; Amendment EBS Download Required.
2	Revised Advertisement; Revised Table of Contents; Revised NTB Nos. 5035 & 5037; NTB No. 5050 replaces 4612; Add NTB No. 5053; NTB No. 5080 replaces 4100; Added NTB 5140; Revised Supplement to SP 907-107-14; Revised BidItems; Revised OCR 485; Amendment EBS Download Required.

TOTAL ADDENDA: 2
 (Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

 Contractor

BY _____
 Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.

HSIP-0020-01(214) / 106780301 HSIP-0020-01(215) / 106780302 & HSIP-0020-02(086) / 106780303
 Hinds, Rankin, & Lauderdale County(ies)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 901 - ADVERTISEMENT

Sealed bids will be received by the Mississippi Transportation Commission in the Office of the Contract Administration Engineer, Room 1013, Mississippi Department of Transportation Administration Building, 401 North West Street, Jackson, Mississippi, until 10:00 o'clock A.M., Tuesday, July 22, 2014, and shortly thereafter publicly opened on the Sixth Floor for:

Installing Median Cable Barriers, known as Federal Aid Project Nos. HSIP-0020-01(214), HSIP-0020-01(215), and HSIP-0020-02(086) / 106780301, 302, & 303 in Hinds, Rankin, and Lauderdale Counties.

The attention of bidders is directed to the Contract Provisions governing selection and employment of labor. Minimum wage rates have been predetermined by the Secretary of Labor and are subject to Public Law 87-581, Work Hours Act of 1962, as set forth in the Contract Provisions.

The Mississippi Department of Transportation hereby notifies all bidders that it will affirmatively insure that in any contract entered into pursuant to this advertisement, disadvantaged business enterprises will be afforded full opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of race, color, sex, age, disability, religion or national origin in consideration for an award.

The award of this contract will be contingent upon the Contractor satisfying the DBE requirements.

The specifications are on file in the offices of the Mississippi Department of Transportation.

Bid proposals must be purchased online at <https://shopmdot.ms.gov>. Specimen proposals may be viewed and downloaded online at no cost at <http://mdot.ms.gov> or purchased online. Proposals are available at a cost of Ten Dollars (\$10.00) per proposal plus a small convenience fee. Cash or checks will not be accepted as payment.

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

MELINDA L. MCGRATH
EXECUTIVE DIRECTOR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

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PROJECT: HSIP-0020-01(214) / 106780301 – Hinds County
HSIP-0020-01(215) / 106780302 – Rankin County
HSIP-0020-02(086) / 106780303 – Lauderdale County

901--Advertisement

904--Notice to Bidders: Governing Specs. - # 1
Fiber Reinforced Concrete - # 640
Errata & Modifications to 2004 Standard Specifications - # 1405
Federal Bridge Formula - # 1928
Status of ROW, W/Attachments - # 2382
Non-Quality Control/Quality Assurance Concrete - # 2818
Reduced Speed Limit Signs - # 2937
Wage Rates – # 3512
Storm Water Discharge Associated with Construction Activities
(≥5 Acres) - # 3581
Additional Erosion Control Requirements - # 3612
Type III Barricade Rails - # 3655
Petroleum Products Base Price - # 3893
Disadvantaged Business Enterprise, W/Supplement - # 4103
Safety Apparel - # 4214
Terminal End Sections - # 4308
DBE Forms, Participation, and Payment - # 4488
Electronic Addendum Process - # 4526
Manual on Uniform Traffic Control Devices (MUTCD) - # 4565
DUNS Requirement for Federal Funded Projects - # 4566
Payroll Requirements - # 4661
Contract Time - # 5035
Specialty Items - # 5036
Scope of Work - # 5037
Lane Closure Restrictions - # 5038
Questions Regarding Bidding - # 5044
Adjustments for Bituminous Materials - # 5050
Contractor's Correspondence - # 5053
Standard Drawings - # 5080
Contractor's Schedule of Operations - # 5117
General Notes For Cable Rail Installation - # 5140

906: Required Federal Contract Provisions -- FHWA-1273, W/ Supplements

907-101-4: Definitions
907-102-10: Bidding Requirements and Conditions
907-103-8: Award and Execution of Contract
907-104-5: Scope of Work
907-105-7: Control of Work, W/ Supplement
907-107-13: Legal Relations & Responsibility to Public

-- CONTINUED ON NEXT PAGE --

PAGE 2 **PROJECT: HSIP-0020-01(214) / 106780301 – Hinds County**
HSIP-0020-01(215) / 106780302 – Rankin County
HSIP-0020-02(086) / 106780303 – Lauderdale County

907-107-14: Contractor's Protection Plan, W/ Supplement
907-108-30: Prosecution and Progress
907-109-6: Measurement and Payment, W/ Supplement
907-110-2: Wage Rates
907-234-5: Siltation Barriers
907-237-4: Wattles
907-304-13: Granular Courses
907-601-1: Structural Concrete
907-606-8: High Tension Cable Barrier
907-703-11: Aggregates
907-711-4: Synthetic Structural Fiber Reinforcement
907-713-3: Admixtures for Concrete
907-714-8: Miscellaneous Materials
907-804-13: Concrete Bridges and Structures, W/ Supplement

SECTION 905 - PROPOSAL,
PROPOSAL BID ITEMS,
COMBINATION BID PROPOSAL,
CERTIFICATION OF PERFORMANCE - PRIOR FEDERAL-AID CONTRACTS,
CERTIFICATION REGARDING NON-COLLUSION, DEBARMENT AND SUSPENSION,
SECTION 902 - CONTRACT FORM, AND SECTION 903 - CONTRACT BOND FORMS,
OCR-485.

(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 5035

CODE: (SP)

DATE: 7/14/2014

SUBJECT: Contract Time

**PROJECT: HSIP-0020-01(214) / 106780 – Hinds County
HSIP-0020-01(215) / 106780 – Rankin County
HSIP-0020-01(086) / 106780 – Lauderdale County**

The calendar date for completion of work to be performed by the Contractor for this project shall be **August 5, 2015** which date or extended date as provided in Subsection 907-108.06 shall be the end of contract time. It is anticipated that the Notice of Award will be issued no later than **August 12, 2014** and the effective date of the Notice to Proceed / Beginning of Contract Time will be **October 13, 2014**.

Should the Contractor request a Notice to Proceed earlier than **October 13, 2014** and it is agreeable with the Department for an early Notice to Proceed, the requested date will become the new Notice to Proceed / Beginning of Contract Time date. If an erosion control plan is required, the Contractor's erosion control plan will have to be approved prior to issuing an early Notice to Proceed.

The available productive days for this project are **128**.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 5037

CODE: (SP)

DATE: 05/08/2014

SUBJECT: Scope of Work

PROJECT: HSIP-0020-01(214) / 106780301 – Hinds County
HSIP-0020-01(215) / 106780302 – Rankin County
HSIP-0020-02(086) / 106780303 – Lauderdale County

The contract documents do not include an official set of construction plans, but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings”. All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

The work to be accomplished using the pay items and corresponding specifications set forth in the contract is for placing a cable barrier system in the median of Interstate 20 and installation of guardrail in the median of I-20 at the SR 471 Bridge in Rankin County. All stationing is provided for information purposes only. All stationing is based on centerline of right lane unless stated otherwise. All stationing is approximate and locations shall be field verified by the Engineer. All locations with station overlaps are to be placed as per Typical Section TS-5.

The following are the locations where the cable barrier systems are to be placed as per the applicable typical section.

Interstate 20 in Hinds County

1. Beginning approximately at the eastern bridge end of Big Black Creek Relief Bridge (STA. 723+70) to approximately an official vehicle median crossover (STA. 747+20). There is an equation at 741+80.99BK/742+21.98AH.
2. Beginning approximately at an official vehicle median crossover (STA 747+20) to approximately the end of concrete median barrier wall west of SR 22 Interchange at Exit 19 (STA. 798+00). There is an equation at 757+06.18BK/749+89.14AH.
3. Beginning approximately at the end of concrete median barrier wall east of SR 22 Interchange at Exit 19 (STA. 840+10) to approximately an official vehicle median crossover (STA. 950+30). There is an equation at 884+30.25BK/874+64.37AH.
4. Beginning approximately at an official vehicle median crossover (STA. 950+30) to approximately the bridge column at Farr Road (STA. 636+48). There is an equation at 1025+16.50BK/595+13.24AH.
5. Beginning approximately at the bridge column at Farr Road (STA. 636+48) to approximately an official vehicle median crossover (STA. 696+00).

6. Beginning approximately at an official vehicle median crossover (STA. 696+00) to approximately the western bridge end of Fleetwood Creek Bridge (STA. 773+82). LT Lane Stationing
7. Beginning approximately at the eastern bridge end of Fleetwood Creek Bridge (STA. 774+81) to approximately the bridge column at Bolton Brownsville Road Interchange at Exit 27 (STA. 807+30).
8. Beginning approximately at the bridge column at Bolton Brownsville Road Interchange at Exit 27 (STA. 807+30) to approximately the western bridge end at Northside Drive Bridge (STA. 886+78). LT Lane Stationing.
9. Beginning approximately at the eastern bridge end at Northside Drive Bridge (STA. 888+12) to approximately an official vehicle median crossover (STA. 940+50).
10. Beginning approximately at an official vehicle median crossover (STA. 940+50) to approximately a point where median widens (STA. 956+70).
11. Beginning approximately at an official vehicle median crossover (STA. 1086+00) to approximately a change in pavement (STA. 1140+00). There is an equation at 1142+32BK/9+85AH.
12. Beginning approximately at the guard rail at the eastern bridge end of W Highland Drive Bridge (STA. 432+37) to approximately the western bridge end of Gibraltar Drive Bridge (STA. 455+50). LT Lane Stationing
13. Beginning approximately at the guard rail at the eastern bridge end of the Gibraltar Drive Bridge (STA. 457+13) to approximately the western bridge end of the Ellis Avenue Bridge (STA. 469+31). LT Lane Stationing
14. Beginning approximately at the guard rail at the eastern bridge end of the Ellis Avenue Bridge (STA. 471+63) to approximately the western bridge end of the Lynch Creek Bridge (STA. 497+02). LT Lane Stationing.

Interstate 20 in Rankin County

1. Beginning approximately at point in the median (STA. 600+00) to approximately the western bridge end of the KCS Railroad Bridge (STA. 636+21). LT Lane Stationing.
2. Beginning approximately at the guard rail at the eastern bridge end of the KCS Railroad Bridge (STA. 639+80) to approximately the western bridge end of the US Highway 80 Bridge (STA. 651+27). LT Lane Stationing.
3. Beginning approximately at the eastern bridge end of the US Highway 80 Bridge (STA. 653+61) to approximately the guard rail for the bridge column at SR 471 (STA. 670+75).

4. Beginning approximately at the guard rail for the bridge column at SR 471 (STA. 679+05) to approximately the western bridge end at North Street Bridge (STA. 695+31).
5. Beginning approximately at the eastern bridge end of the North Street Bridge (STA. 696+93) to approximately the bridge column at Trickham Bridge Road (STA. 728+95).
6. Beginning approximately at the bridge column at Trickham Bridge Road (STA. 728+95) to approximately a point where the median widens (STA. 755+00).
7. Beginning approximately at the eastern bridge end of the US Highway 80 Bridge (STA. 816+71) to approximately a point where the median widens (STA. 843+70).
8. Beginning approximately at a point where the median narrows (STA. 864+00) to approximately the guard rail in the median (STA. 902+50).
9. Beginning approximately at an official vehicle median crossover (STA. 908+50) to approximately the bridge column at Rankin Road (STA. 955+54).
10. Beginning approximately at the bridge column at Rankin Road (STA. 955+54) to approximately an official vehicle median crossover (STA. 1036+20).
11. Beginning approximately a point where paved ditch ends (STA. 1146+00) to approximately the bridge column at Gulde Road (STA. 1161+39).
12. Beginning approximately at the bridge column at Gulde Road (STA. 1161+39) to approximately the western bridge end of the SR 43 Bridge (STA. 1286+16).
13. Beginning approximately at the eastern bridge end of the SR 43 Bridge (STA. 1287+82) to approximately the guard rail at the western bridge end at the Noblin Bridge Road Bridge (STA. 1466+00).
14. Beginning approximately at the eastern bridge end of Noblin Bridge Road Bridge (STA. 1467+55) to approximately the official vehicle median crossover at the Rankin/Scott County Line (STA. 1521+88).

Interstate 20 in Lauderdale County

1. Beginning approximately at a point where concrete median barrier wall ends (STA. 893+00) to approximately the western bridge end of the SR 39/SR 19 Bridge (STA. 898+71).
2. Beginning approximately at the eastern bridge end of the SR 39/SR 19 Bridge (STA. 901+66) to approximately the guard rail protecting the bridge column at the Jimmy Rodgers Parkway Interchange Exit 156 (STA. 962+26).

3. Beginning approximately at the guard rail protecting the bridge column at the Jimmy Rodgers Parkway Exit 156 (STA. 962+26) to approximately the western bridge end of the Knight Parker Road Bridge (STA. 1001+56).
4. Beginning approximately at the eastern bridge end of the Knight Parker Road Bridge (STA. 1002+70) to approximately the guard rail protecting the bridge columns at US 45 Interchange Exit 157 (STA. 1027+85).
5. Beginning approximately at the guard rail protecting the bridge columns at US 45 Interchange Exit 157 (STA. 1027+85) to approximately the guard rail protecting the bridge column at Sweet Gum Bottom Road (STA. 1075+50).
6. Beginning approximately at the guard rail protecting the bridge column at Sweet Gum Bottom Road (STA. 1075+50) to approximately the guard rail protecting the bridge column at Murphy Road (STA. 1124+10).
7. Beginning approximately at the guard rail protecting the bridge column at Murphy Road (STA. 1124+10) to approximately the guard rail protecting the bridge column at Harper Road (STA. 1157+87).
8. Beginning approximately at the guard rail protecting the bridge column at Harper Road (STA. 1157+87) to approximately the guard rail protecting the bridge column at Russell Road Interchange Exit 160 (STA 1209+05).
9. Beginning approximately at the guard rail protecting the bridge column at Russell Road Interchange Exit 160 (STA. 1209+05) to approximately the guard rail protecting the bridge column at Dees Road (STA. 1245+89).

The following are locations where guard rail is to be placed as per standard drawing GR-4C:

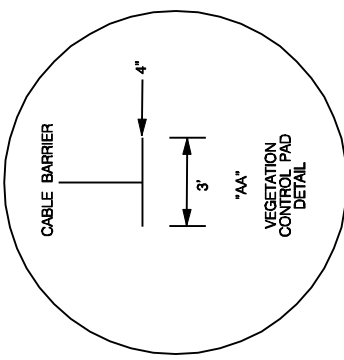
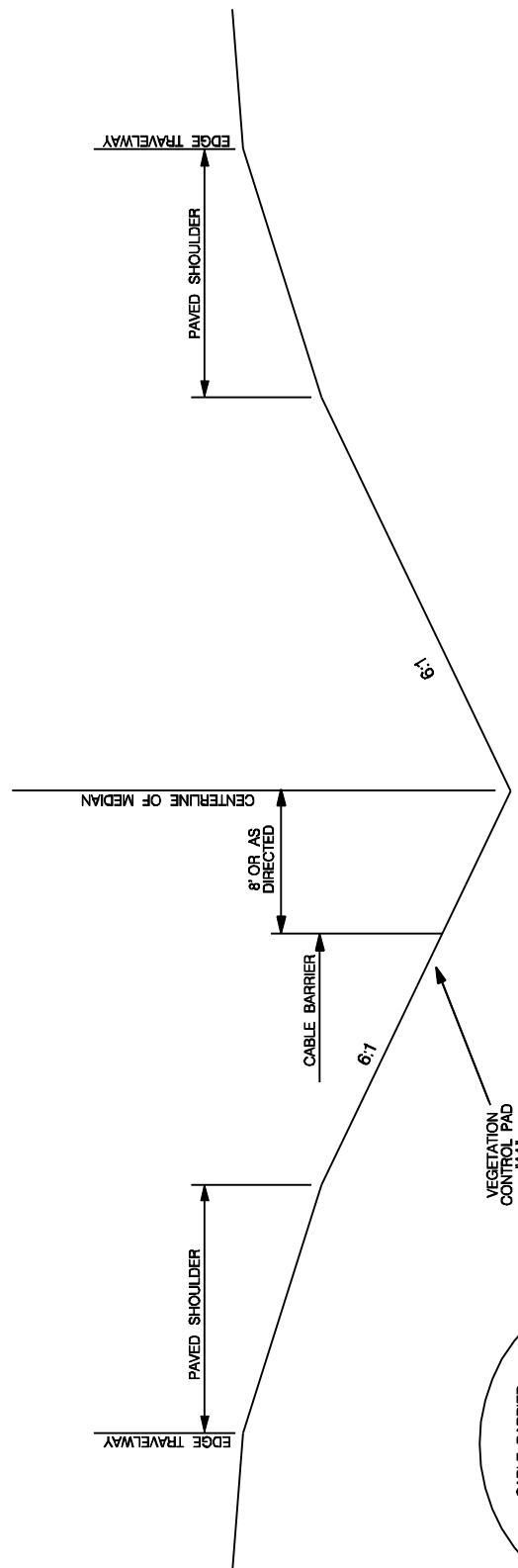
Interstate 20 in Rankin County

1. Beginning approximately at a point 415 feet west of the bridge column at STA. 674+90 to a point approximately 16 feet east of the bridge column at STA. 674+90 on the LT Shoulder of the LT Lane.
2. Beginning approximately at a point 16 feet west of the bridge column at STA. 674+90 to a point approximately 415 feet east of the bridge column at STA. 674+90 on the RT Shoulder of the RT Lane.

Interstate 20 in Hinds County and Lauderdale County

No Locations

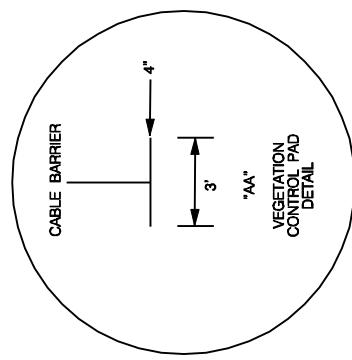
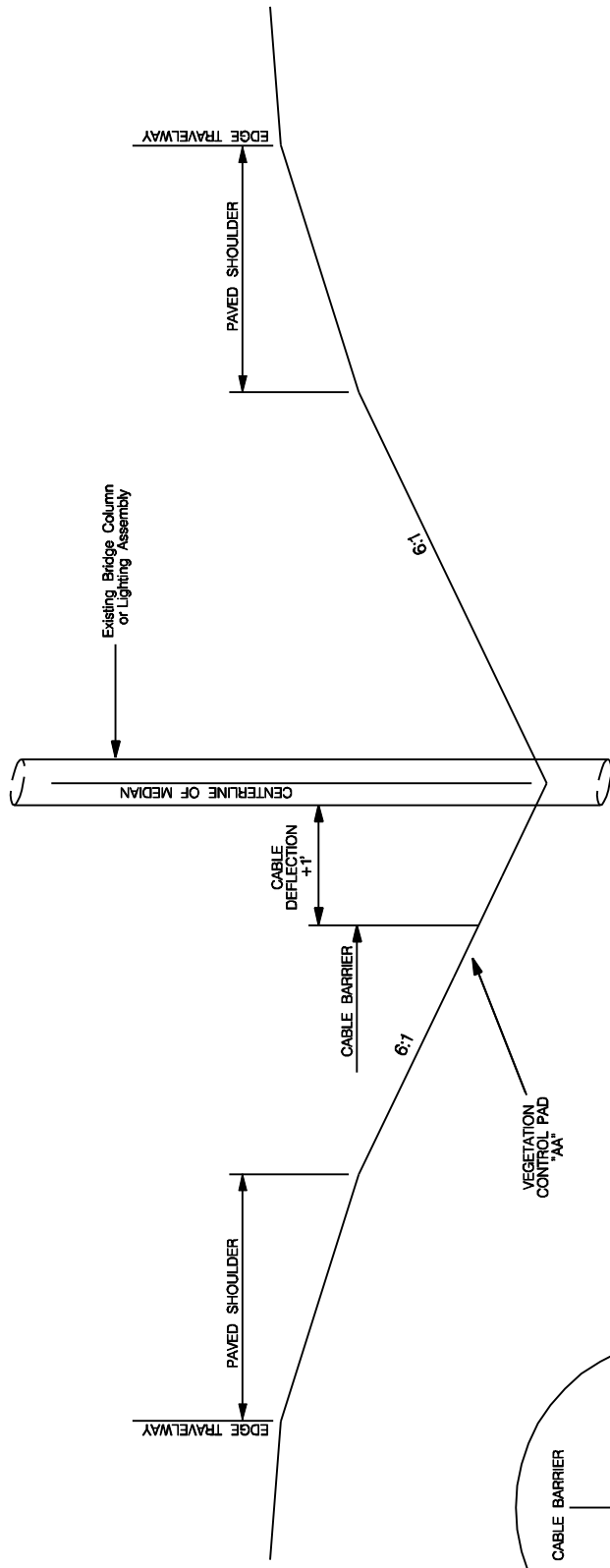
STATE	PROJECT NO.
MISS.	



NOTE: LOCATION MAY BE MOVED BY THE ENGINEER TO MEET FIELD CONDITIONS.

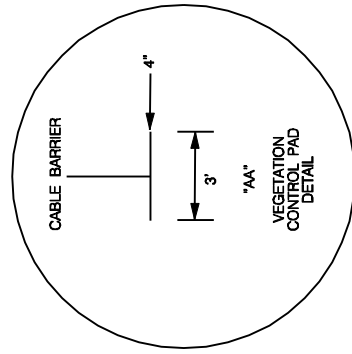
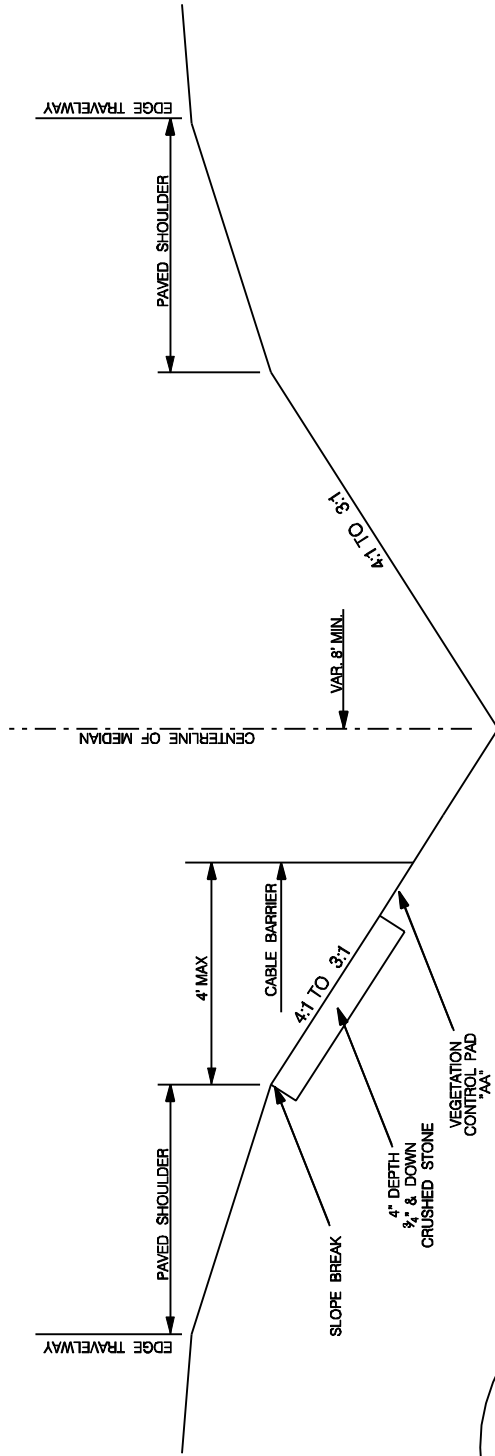
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
CABLE BARRIER	
TYPICAL SECTION	
SINGLE LINE	
6:1	
NOT TO SCALE	
DATE	WORKING NUMBER
	TS-1
	SHEET NUMBER
	DATE REVISED

STATE PROJECT NO.
MSS.



MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
CABLE BARRIER	
TYPICAL SECTION	
SINGLE LINE	
6:1 WITH MEDIAN STRUCTURE	
DATE	NOT TO SCALE
REVISION	FILE NAME: TSCB2.DGN
BY	WORKING NUMBER: TS-2
	SHEET NUMBER
	DATE REVISION

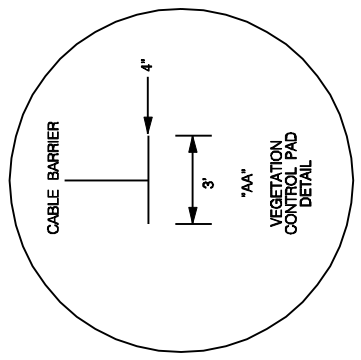
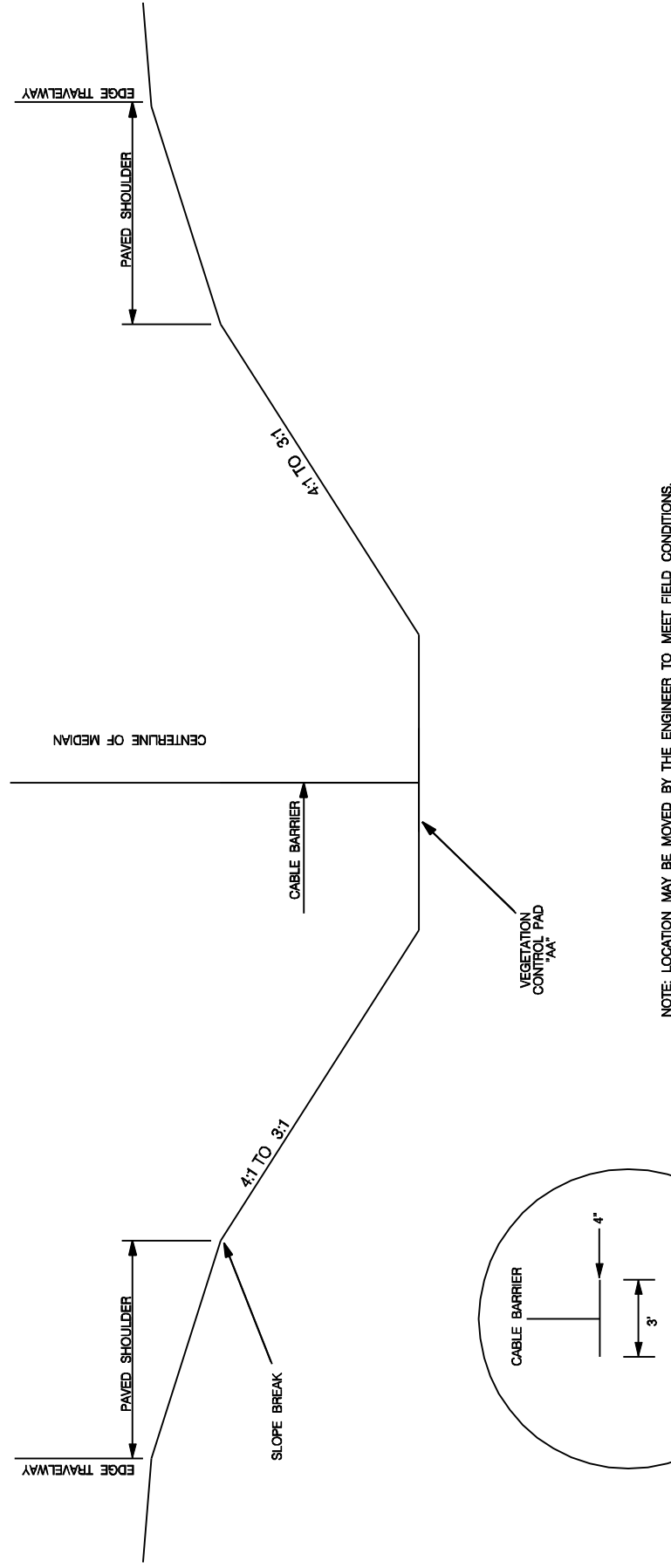
STATE	PROJECT NO.
MISS.	



NOTE: LOCATION MAY BE MOVED BY THE ENGINEER TO MEET FIELD CONDITIONS.
 NOTE: FOR SLOPES STEEPER THAN 4:1, CABLE BARRIER SHALL BE PLACED AT SHOULDER BREAK.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
CABLE BARRIER	
TYPICAL SECTION	
SINGLE LINE	
4:1 TO 3:1	
DATE	NOT TO SCALE
REVISION	TS-3
FILE NAME: TSCB3.DGN	WORKING NUMBER
DESIGN: TR-602031/BJC/BJC/KCC/00	SHEET NUMBER
	DATE: 08/03/10

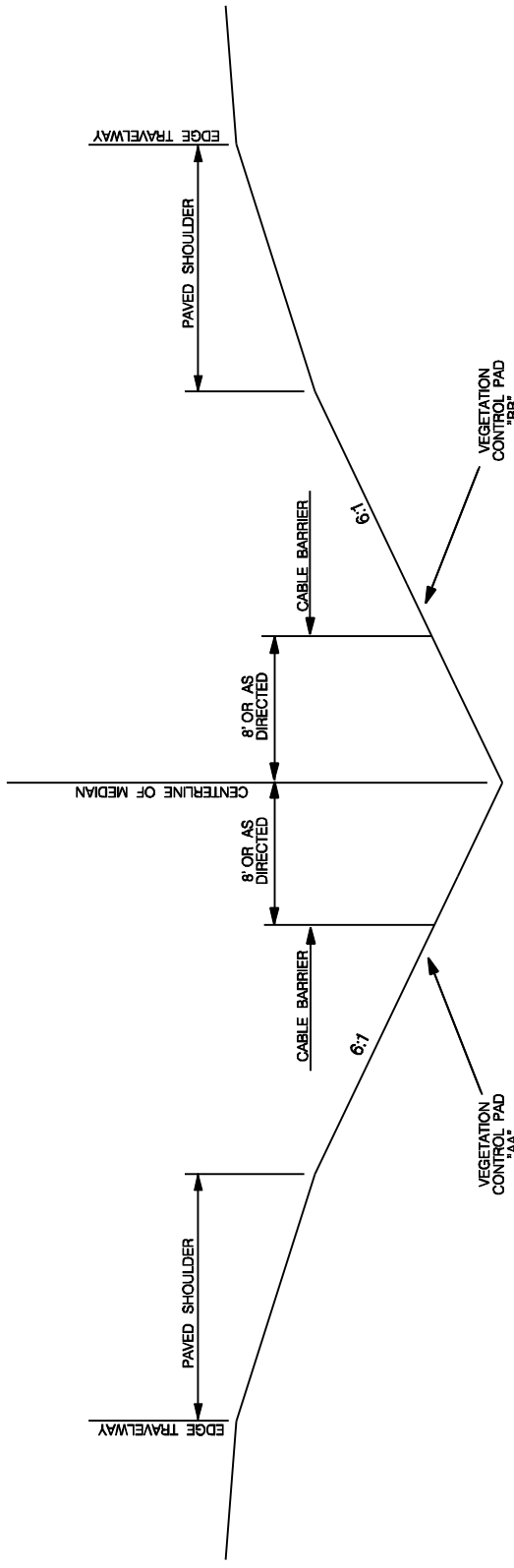
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MISS.	



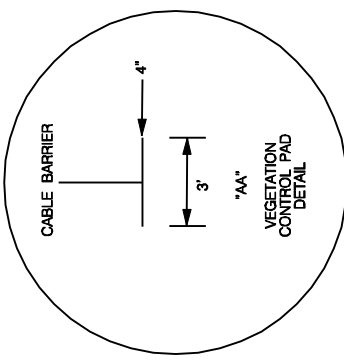
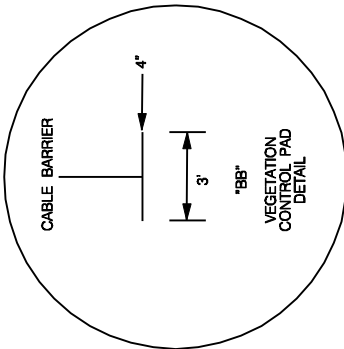
NOTE: LOCATION MAY BE MOVED BY THE ENGINEER TO MEET FIELD CONDITIONS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION CABLE BARRIER TYPICAL SECTION SINGLE LINE 3:1 & VARIABLE	WORKING NUMBER TS-4
FILE NAME: TSCBA.DGN	SHEET NUMBER
DATE: 05/08/10	DATE REVISED
DATE	REVISION

STATE	PROJECT NO.
MISS.	

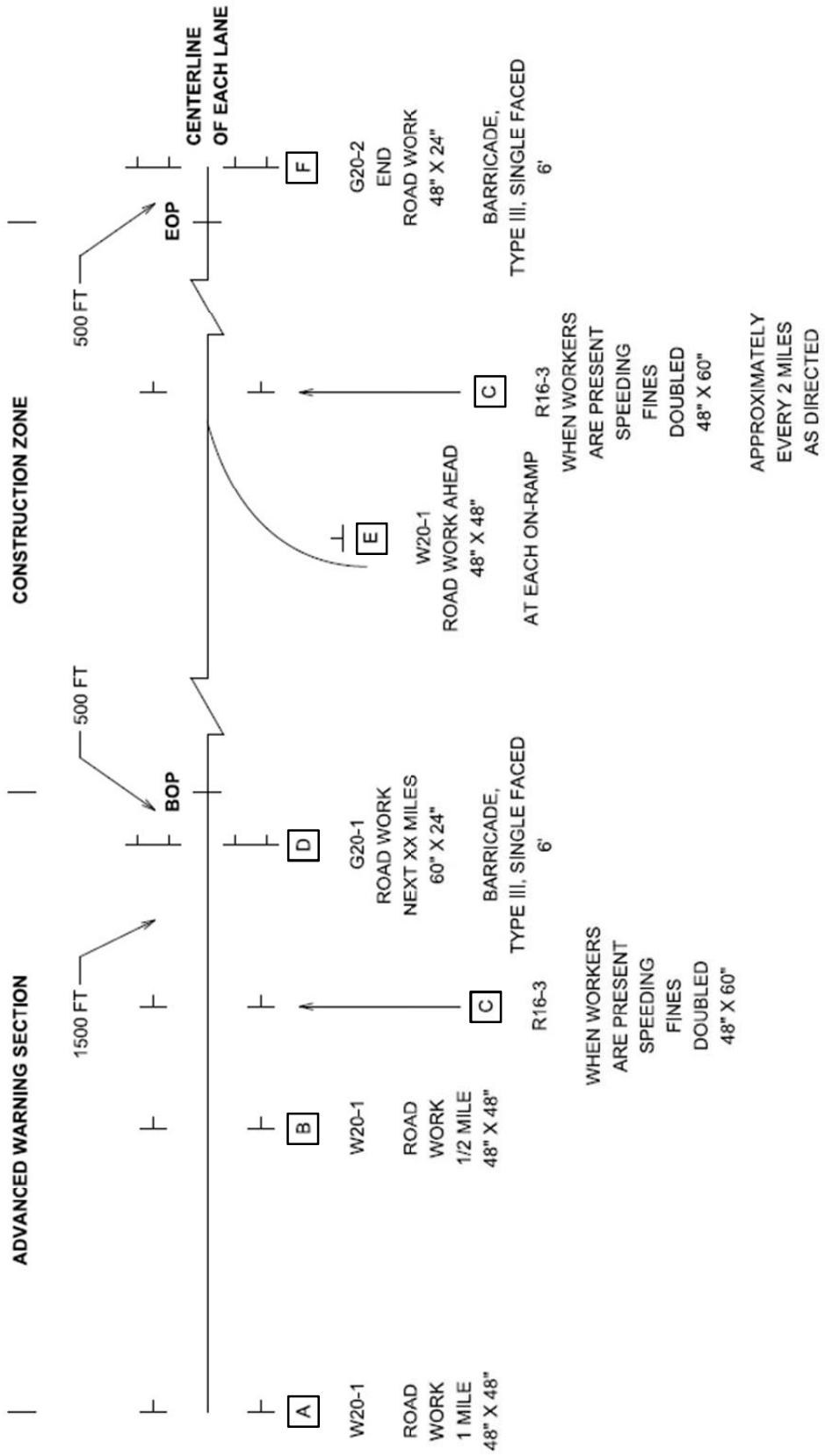


NOTE: LOCATION MAY BE MOVED BY THE ENGINEER TO MEET FIELD CONDITIONS.



MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
CABLE BARRIER	
TYPICAL SECTION	
DOUBLE LINE	
6:1	
NOT TO SCALE	
DATE	WORKING NUMBER
REVISION	TS-5
FILE NAME: TSCB5.DGN	SHEET NUMBER
DESIGN: TRACOR/STANLEY/CHICKO	DRAWN: BR/BJL/BJL

Construction Signing Detail
I-20 from Big Black to Valley Street
Hinds County



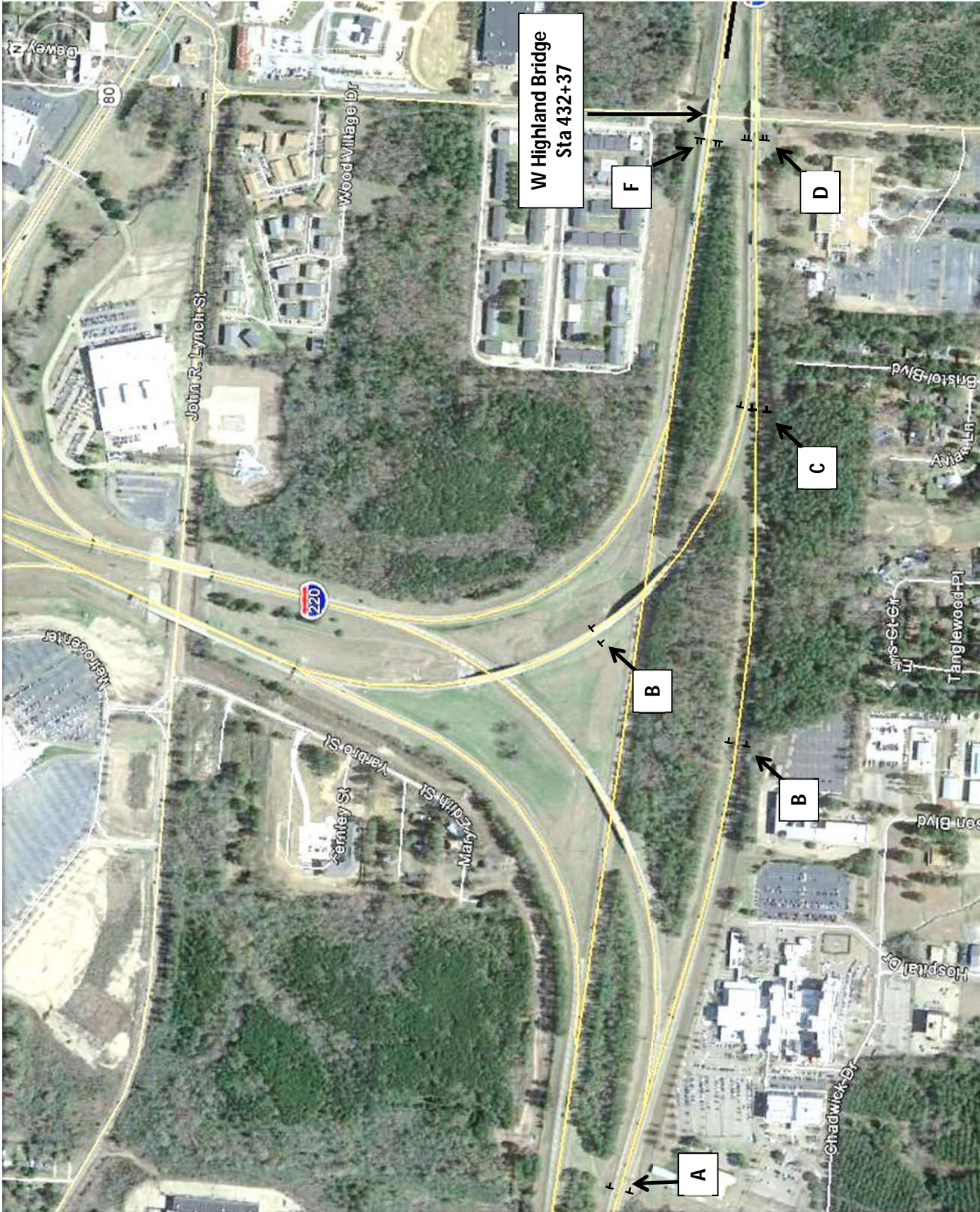
Construction Signing Detail
I-20 from Big Black to Valley Street
Hinds County



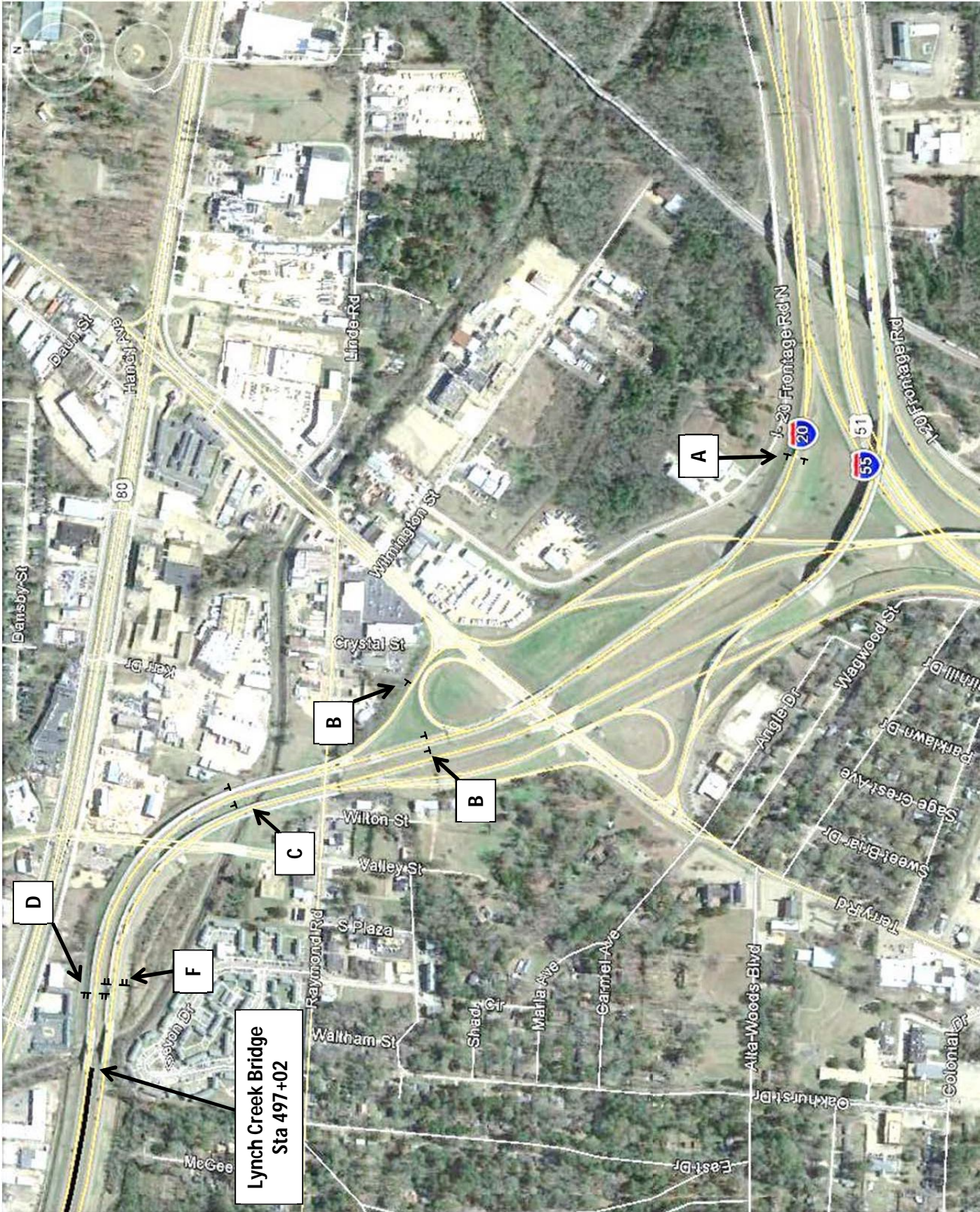
Construction Signing Detail
I-20 from Big Black to Valley Street
Hinds County



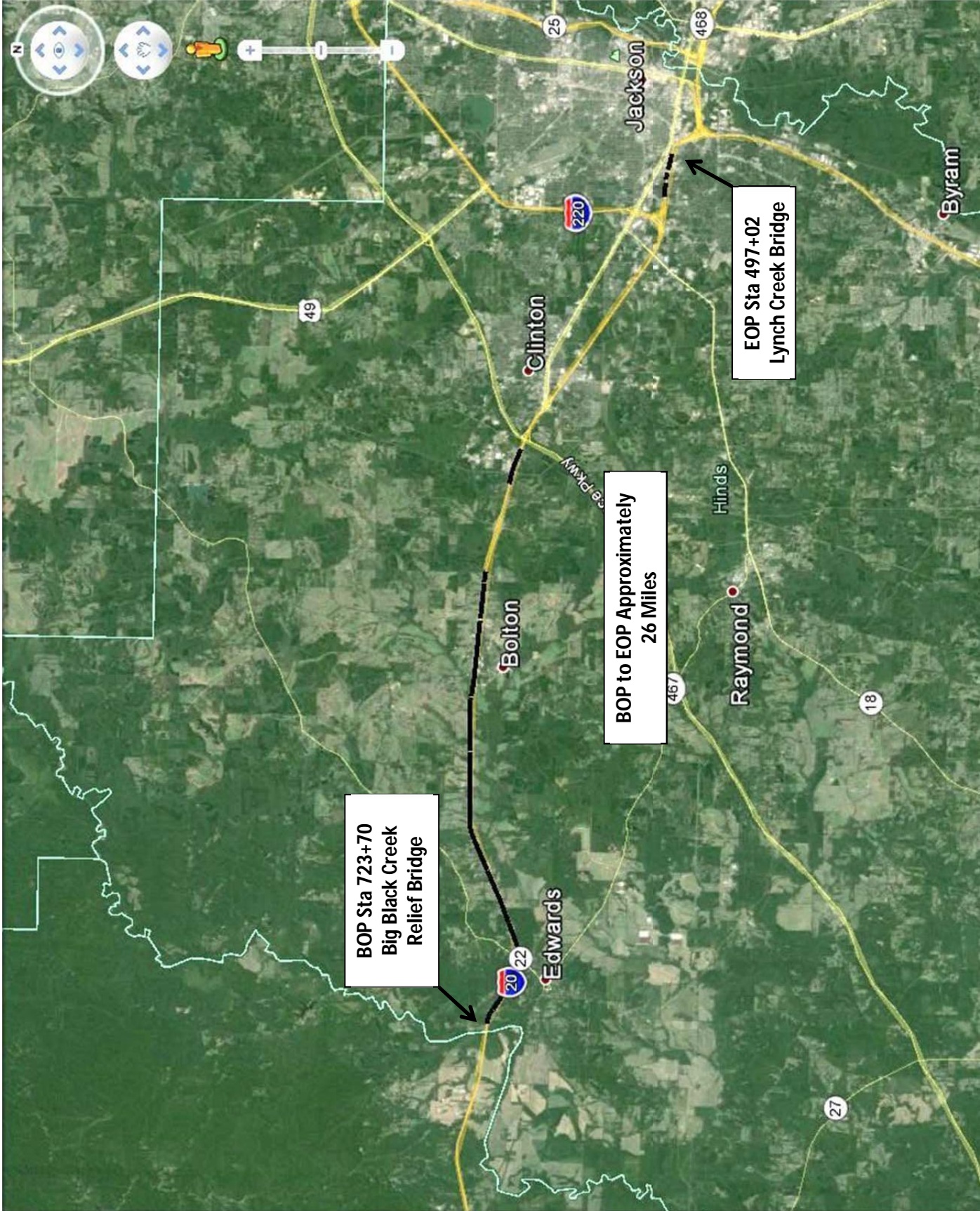
Construction Signing Detail
I-20 from Big Black to Valley Street
Hinds County



Construction Signing Detail
I-20 from Big Black to Valley Street
Hinds County



Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County

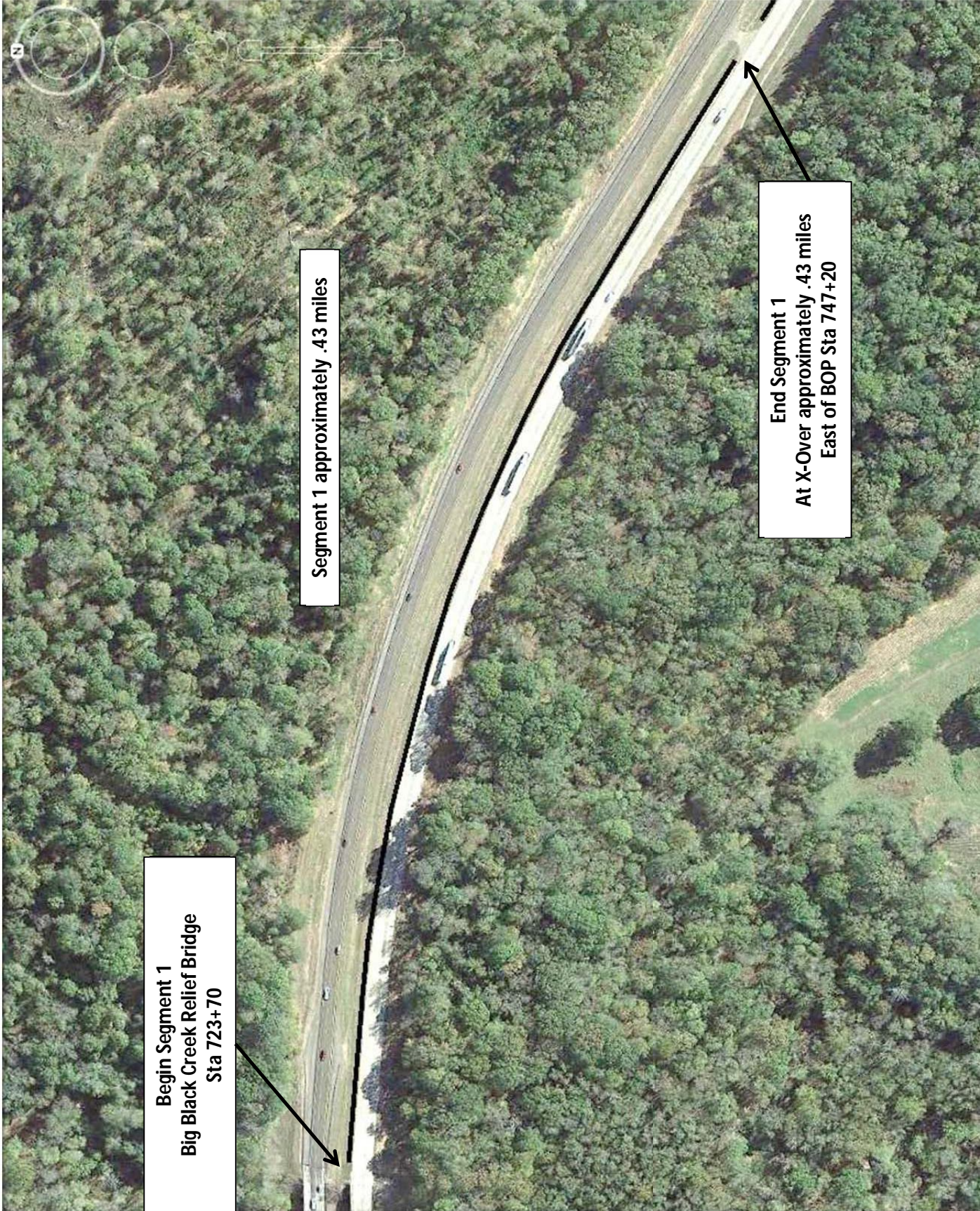


Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



Big Black Creek Relief Bridge
BOP Sta 723+70

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



Begin Segment 1
Big Black Creek Relief Bridge
Sta 723+70

Segment 1 approximately .43 miles

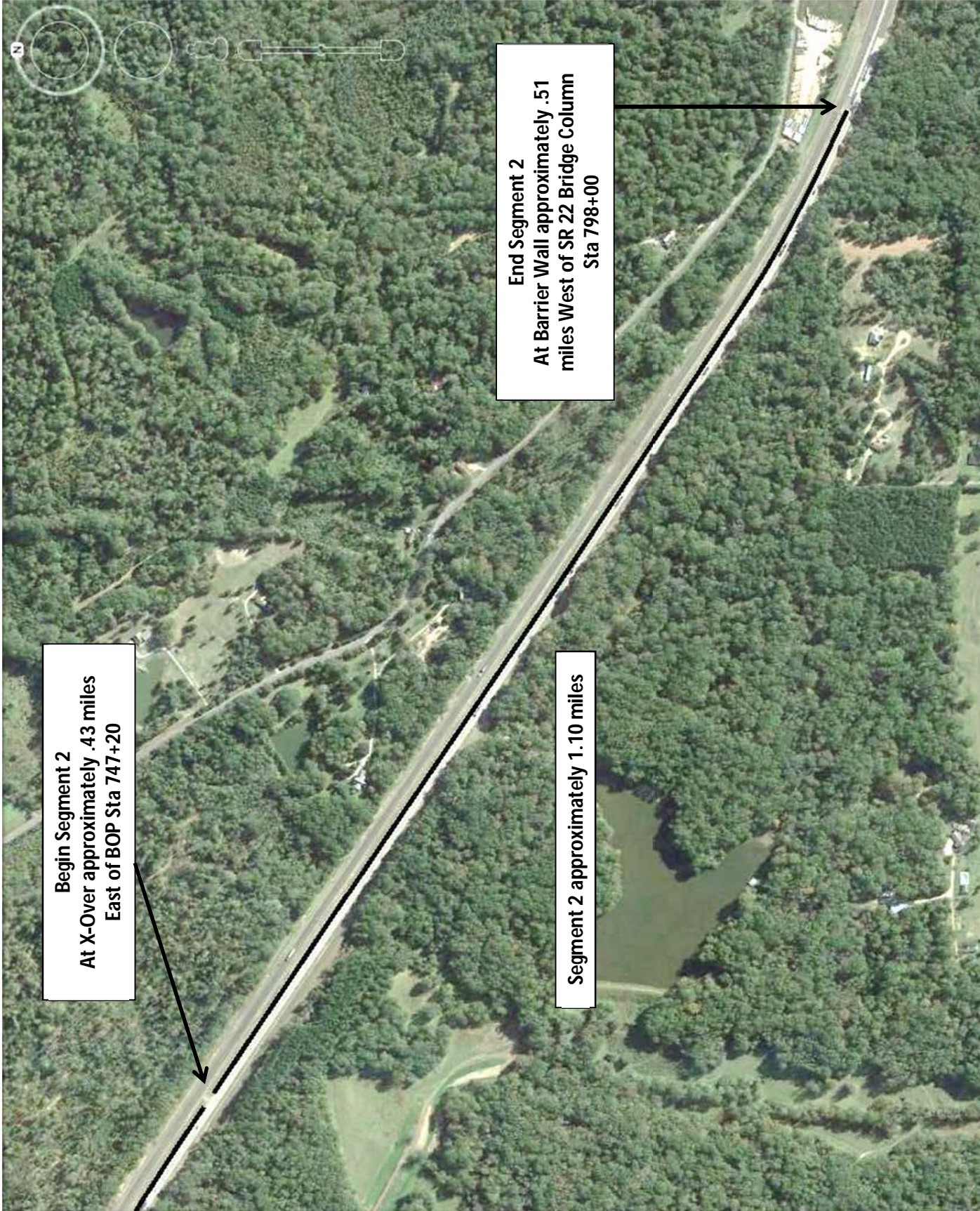
End Segment 1
At X-Over approximately .43 miles
East of BOP Sta 747+20

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



X-Over approximately .43 miles
East of BOP Sta 747+20

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



Begin Segment 2
At X-Over approximately .43 miles
East of BOP Sta 747+20

Segment 2 approximately 1.10 miles

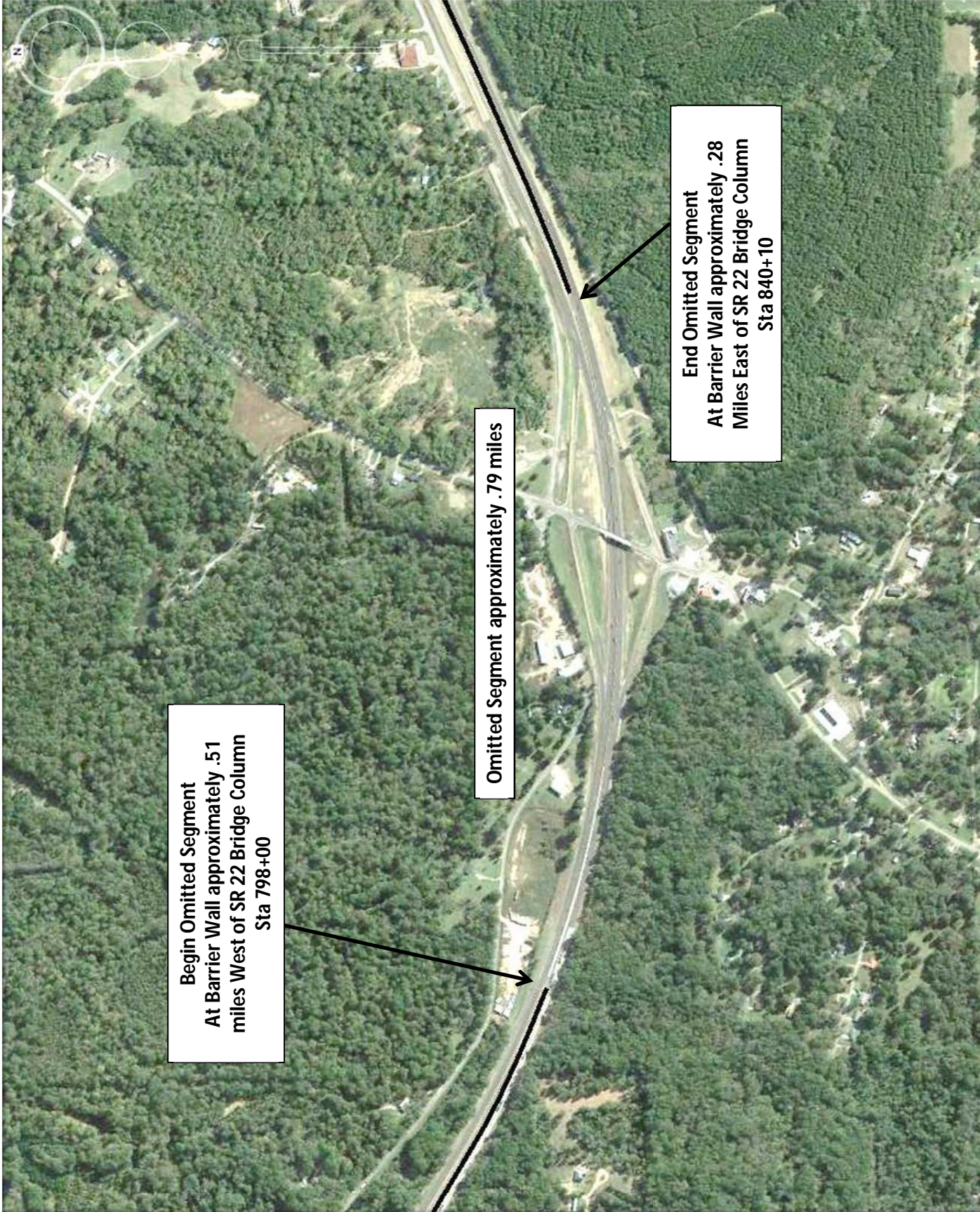
End Segment 2
At Barrier Wall approximately .51
miles West of SR 22 Bridge Column
Sta 798+00

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



Barrier Wall approximately .51 miles
West of SR 22 Bridge Column
Sta 798+00

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



**Begin Omitted Segment
At Barrier Wall approximately .51
miles West of SR 22 Bridge Column
Sta 798+00**

Omitted Segment approximately .79 miles

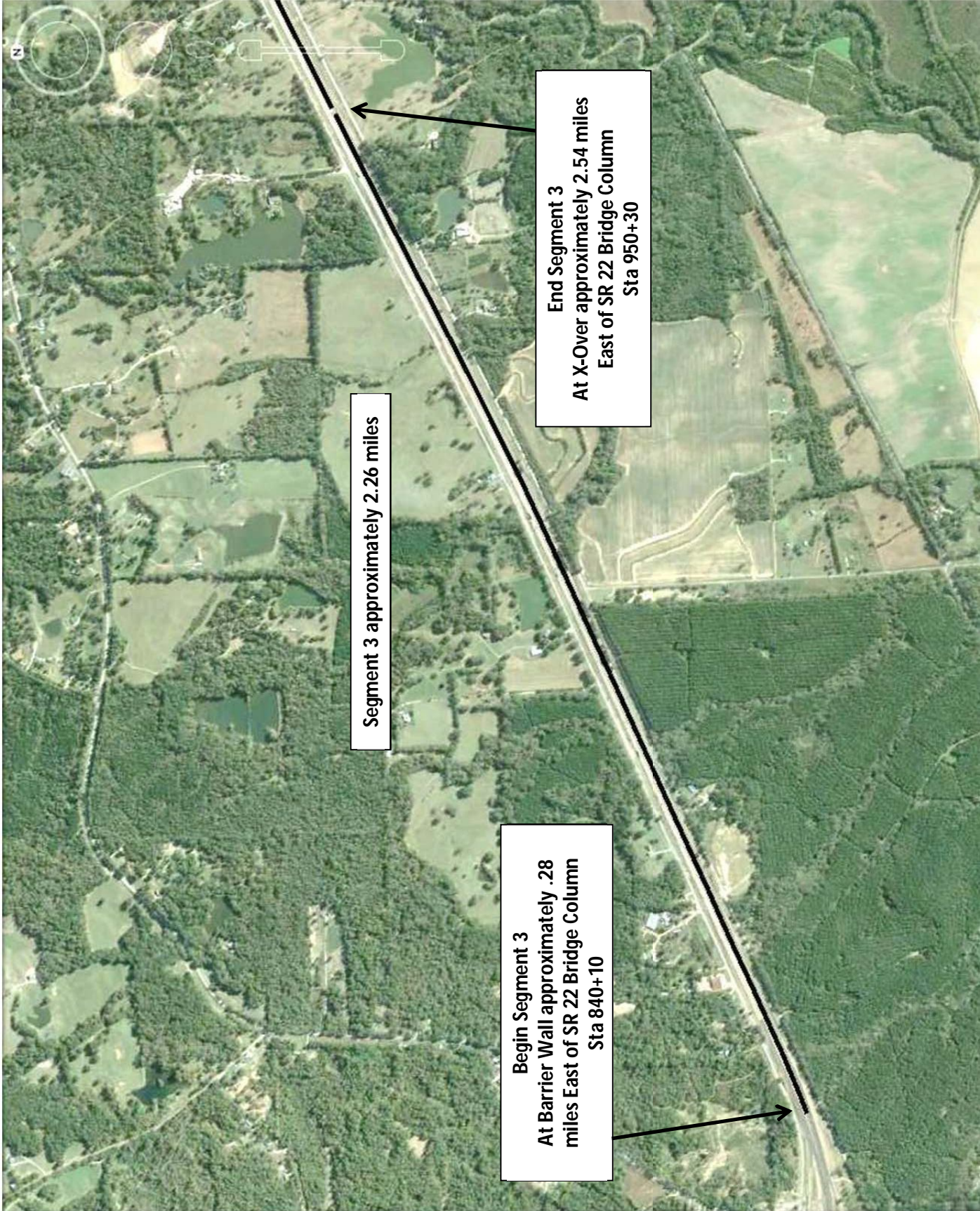
**End Omitted Segment
At Barrier Wall approximately .28
Miles East of SR 22 Bridge Column
Sta 840+10**

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



Barrier Wall approximately .28 miles
East of SR 22 Bridge Column
Sta 840+10

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



Segment 3 approximately 2.26 miles

Begin Segment 3
At Barrier Wall approximately .28
miles East of SR 22 Bridge Column
Sta 840+10

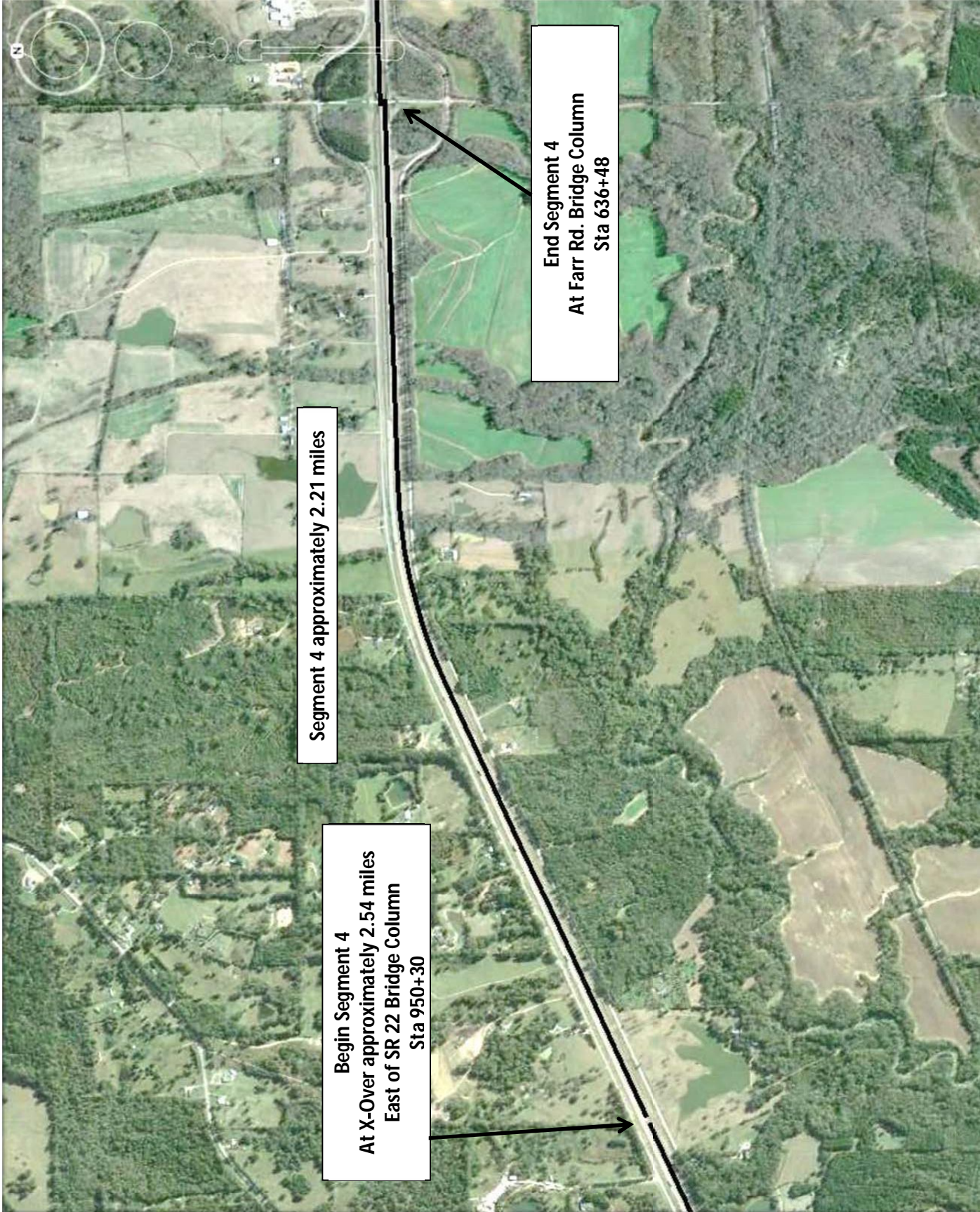
End Segment 3
At X-Over approximately 2.54 miles
East of SR 22 Bridge Column
Sta 950+30

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



X-Over approximately 2.54 miles East
of SR 22 Bridge Column
Sta 950+30

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County

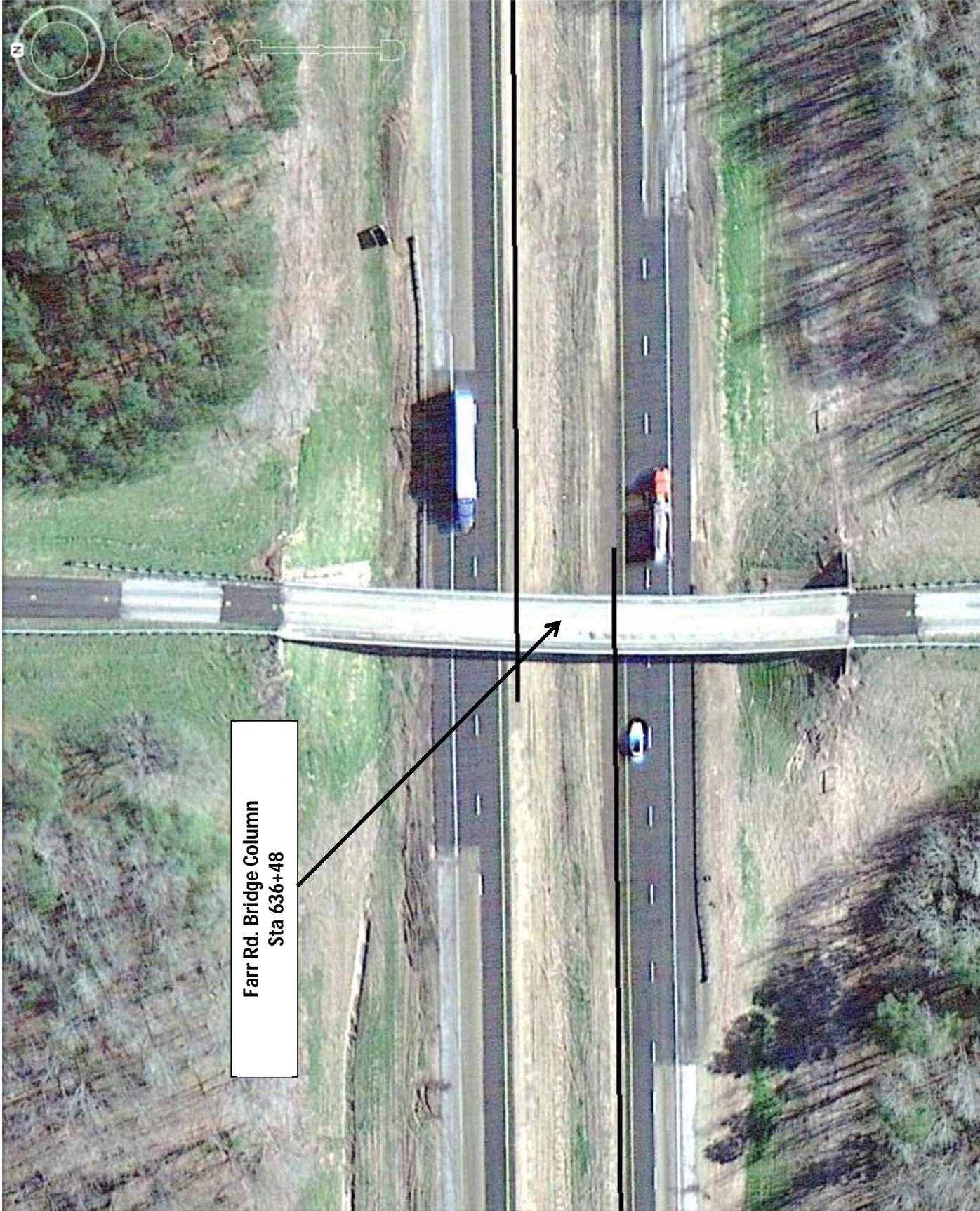


Segment 4 approximately 2.21 miles

Begin Segment 4
At X-Over approximately 2.54 miles
East of SR 22 Bridge Column
Sta 950+30

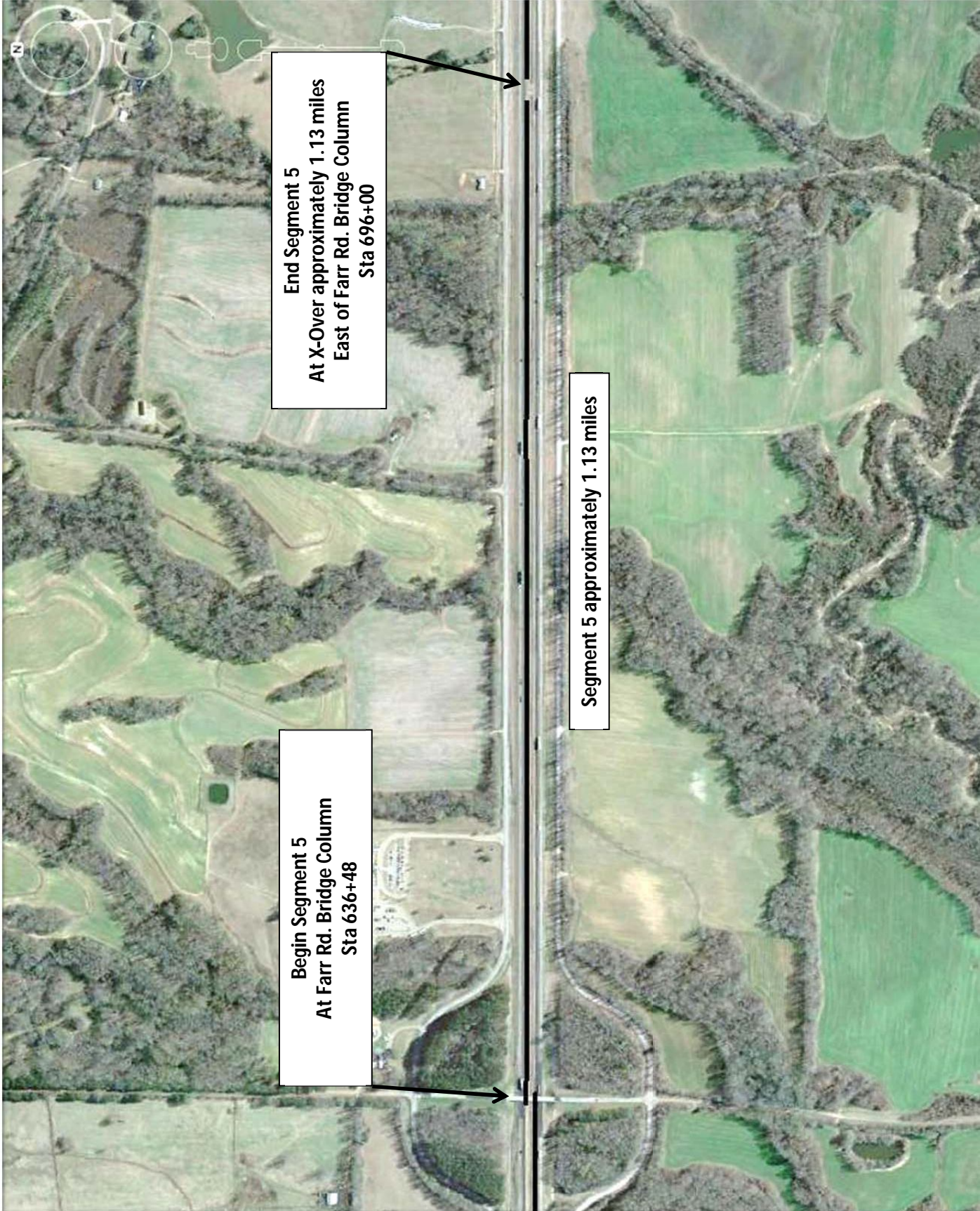
End Segment 4
At Farr Rd. Bridge Column
Sta 636+48

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



Farr Rd. Bridge Column
Sta 636+48

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



End Segment 5
At X-Over approximately 1.13 miles
East of Farr Rd. Bridge Column
Sta 696+00

Begin Segment 5
At Farr Rd. Bridge Column
Sta 636+48

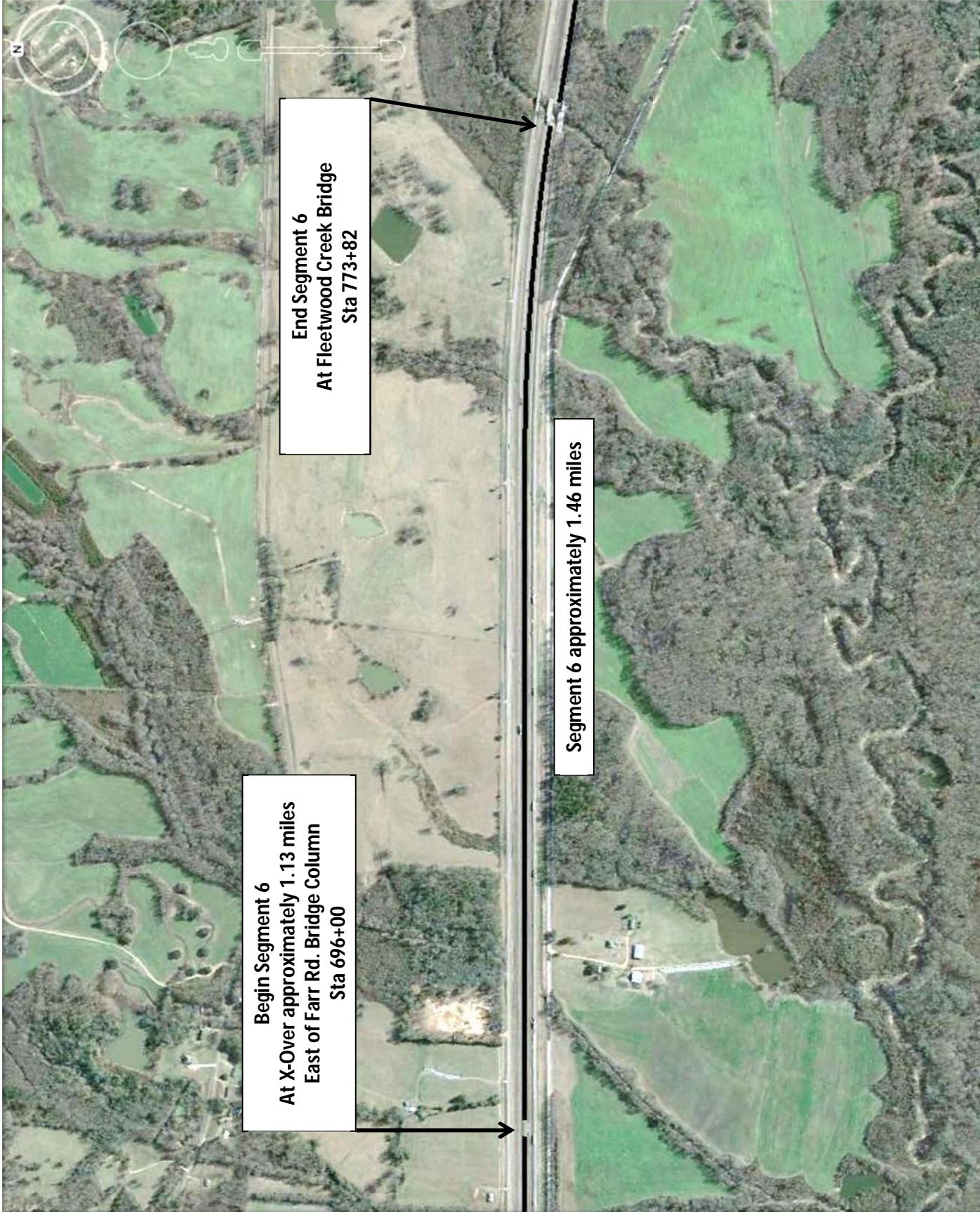
Segment 5 approximately 1.13 miles

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



X-Over approximately 1.13 miles East
of Farr Rd. Bridge Column
Sta 696+00

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County

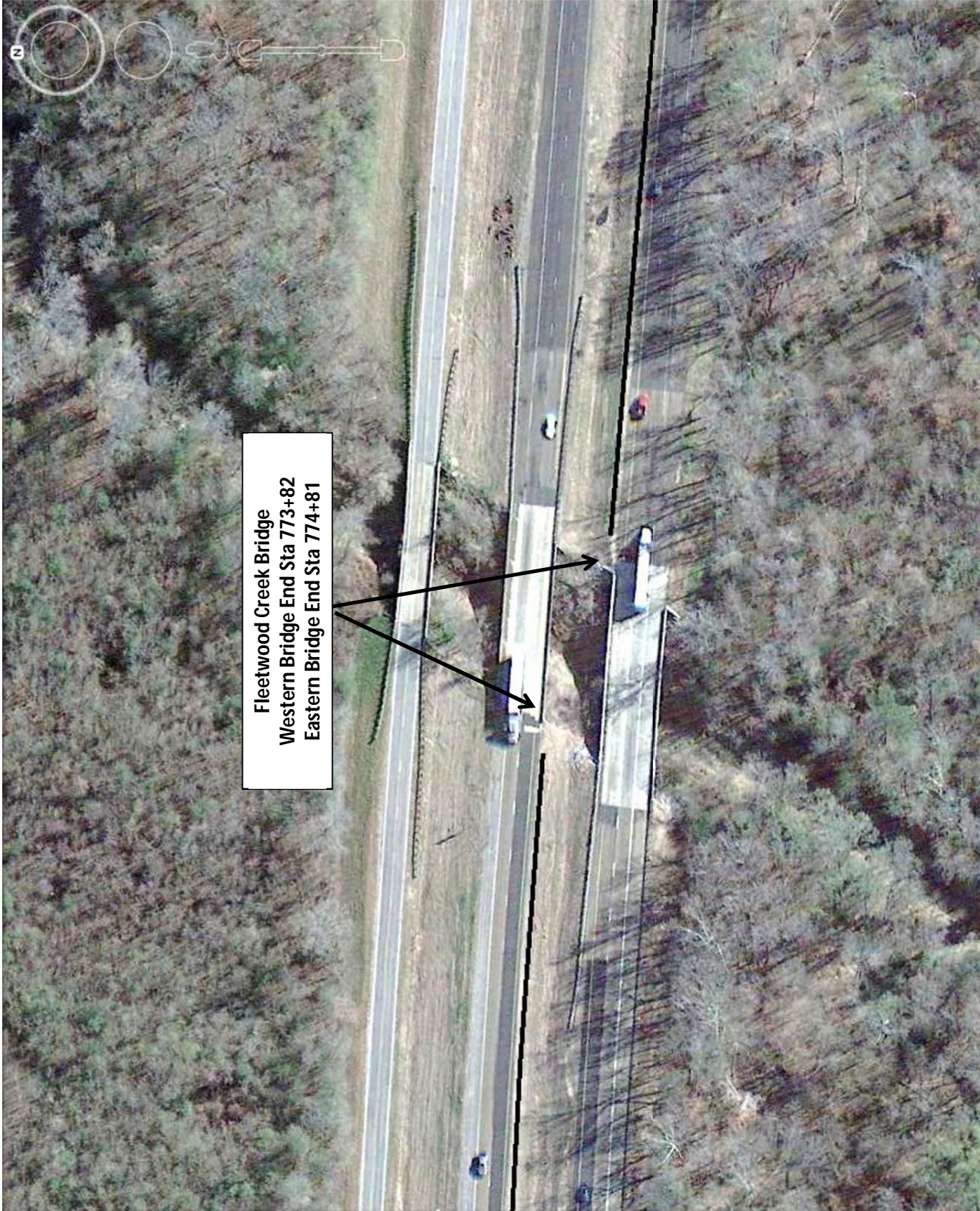


Begin Segment 6
At X-Over approximately 1.13 miles
East of Farr Rd. Bridge Column
Sta 696+00

End Segment 6
At Fleetwood Creek Bridge
Sta 773+82

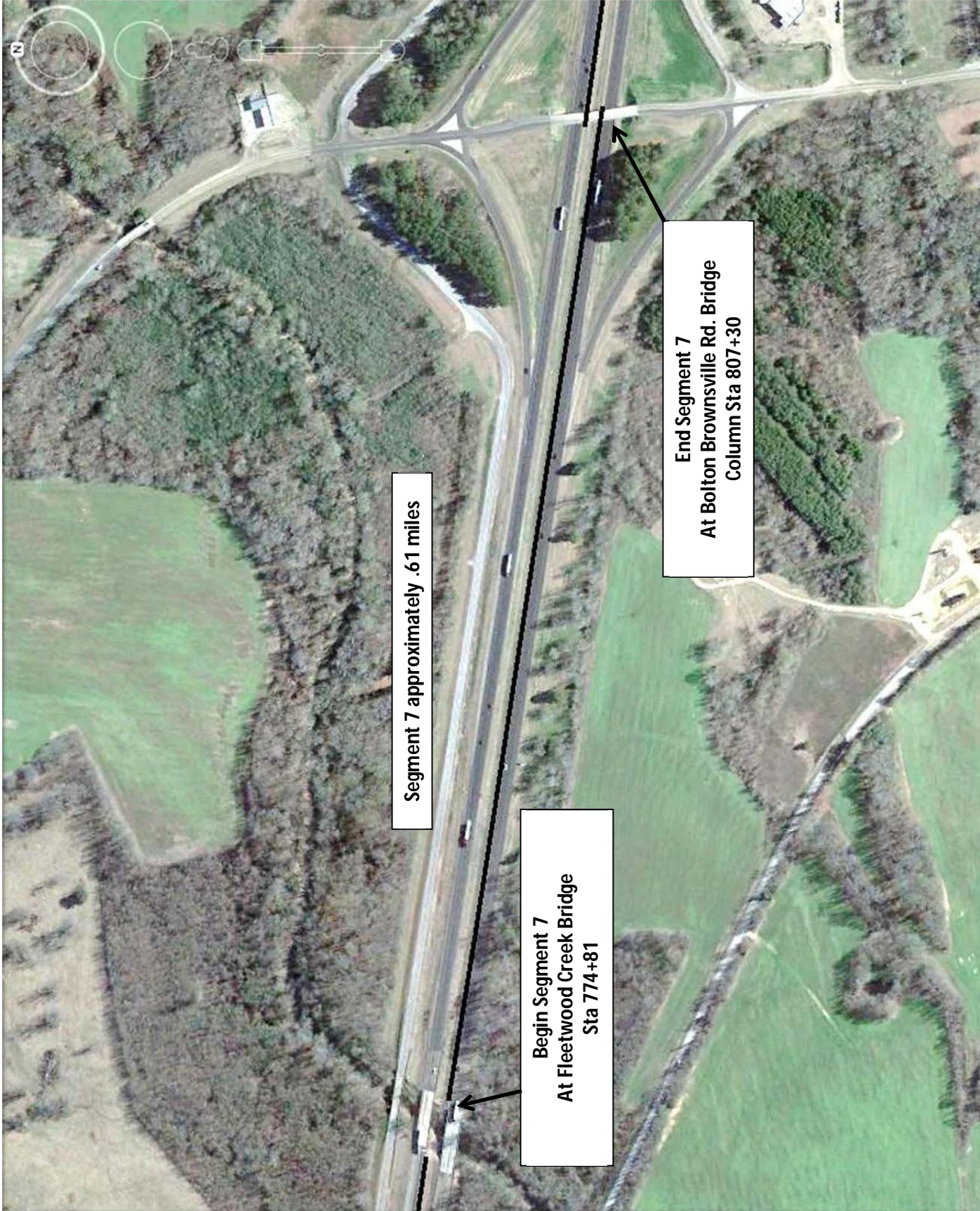
Segment 6 approximately 1.46 miles

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



Fleetwood Creek Bridge
Western Bridge End Sta 773+82
Eastern Bridge End Sta 774+81

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County

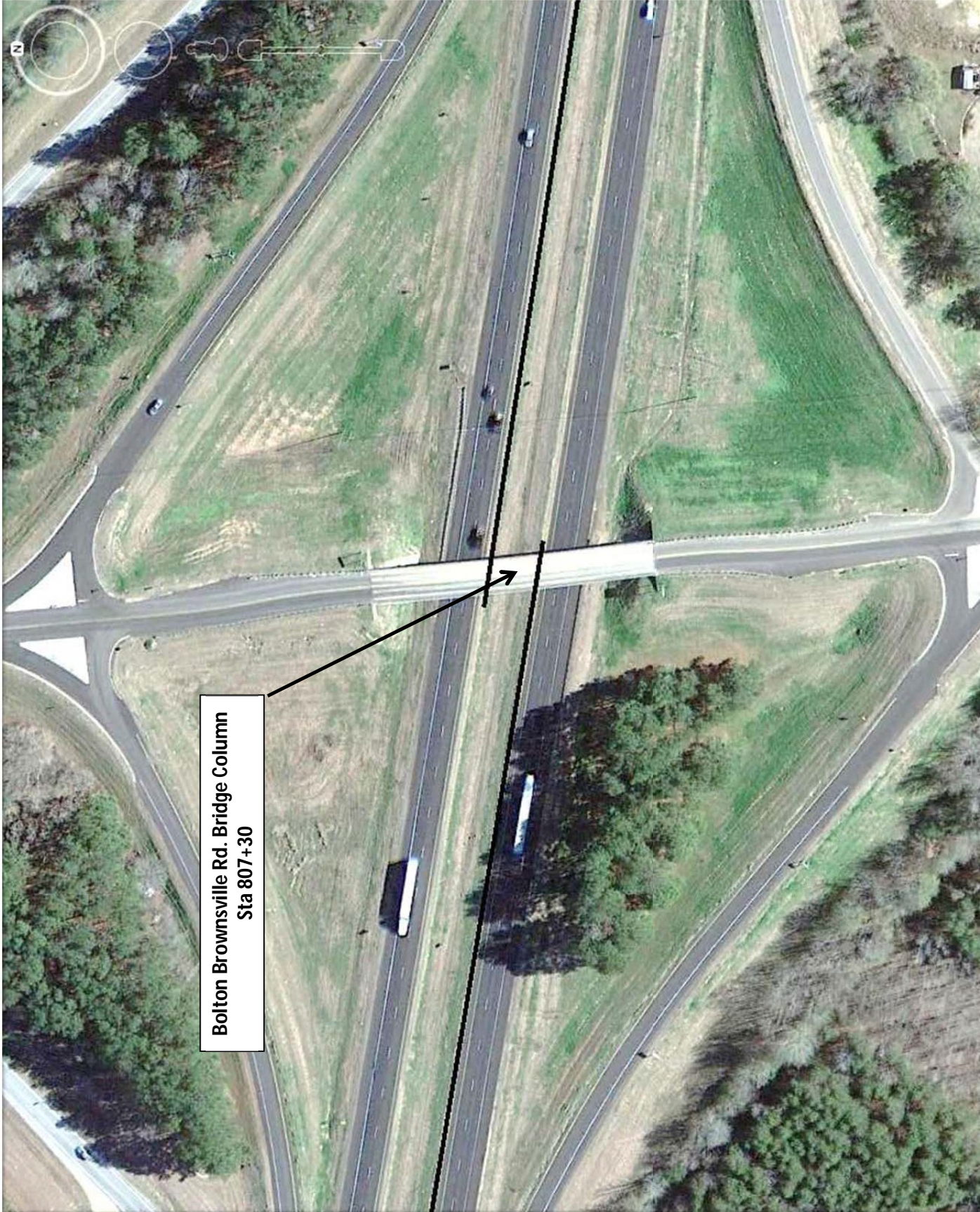


Segment 7 approximately .61 miles

Begin Segment 7
At Fleetwood Creek Bridge
Sta 774+81

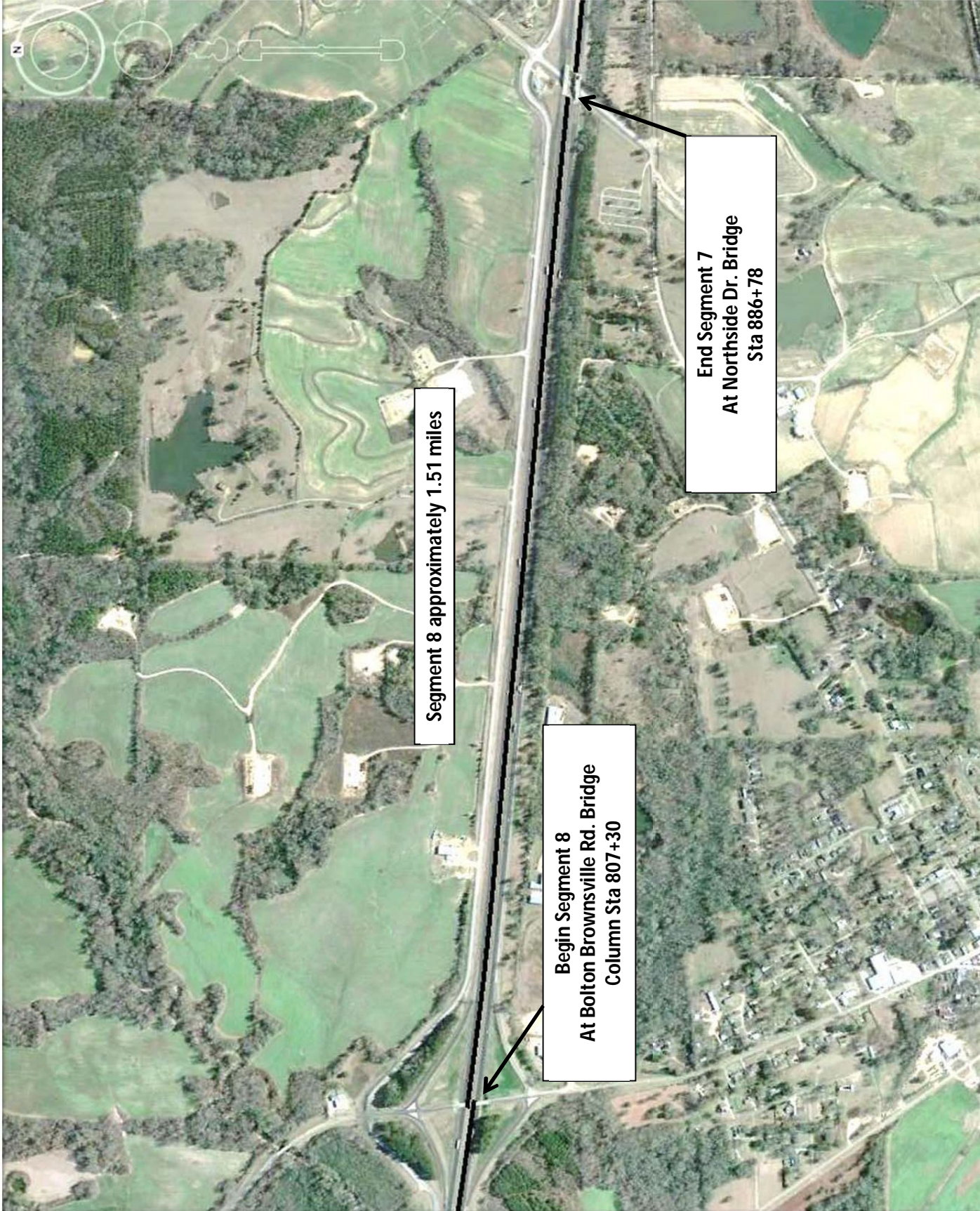
End Segment 7
At Bolton Brownsville Rd. Bridge
Column Sta 807+30

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



**Bolton Brownsville Rd. Bridge Column
Sta 807+30**

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



Segment 8 approximately 1.51 miles

Begin Segment 8
At Bolton Brownsville Rd. Bridge
Column Sta 807+30

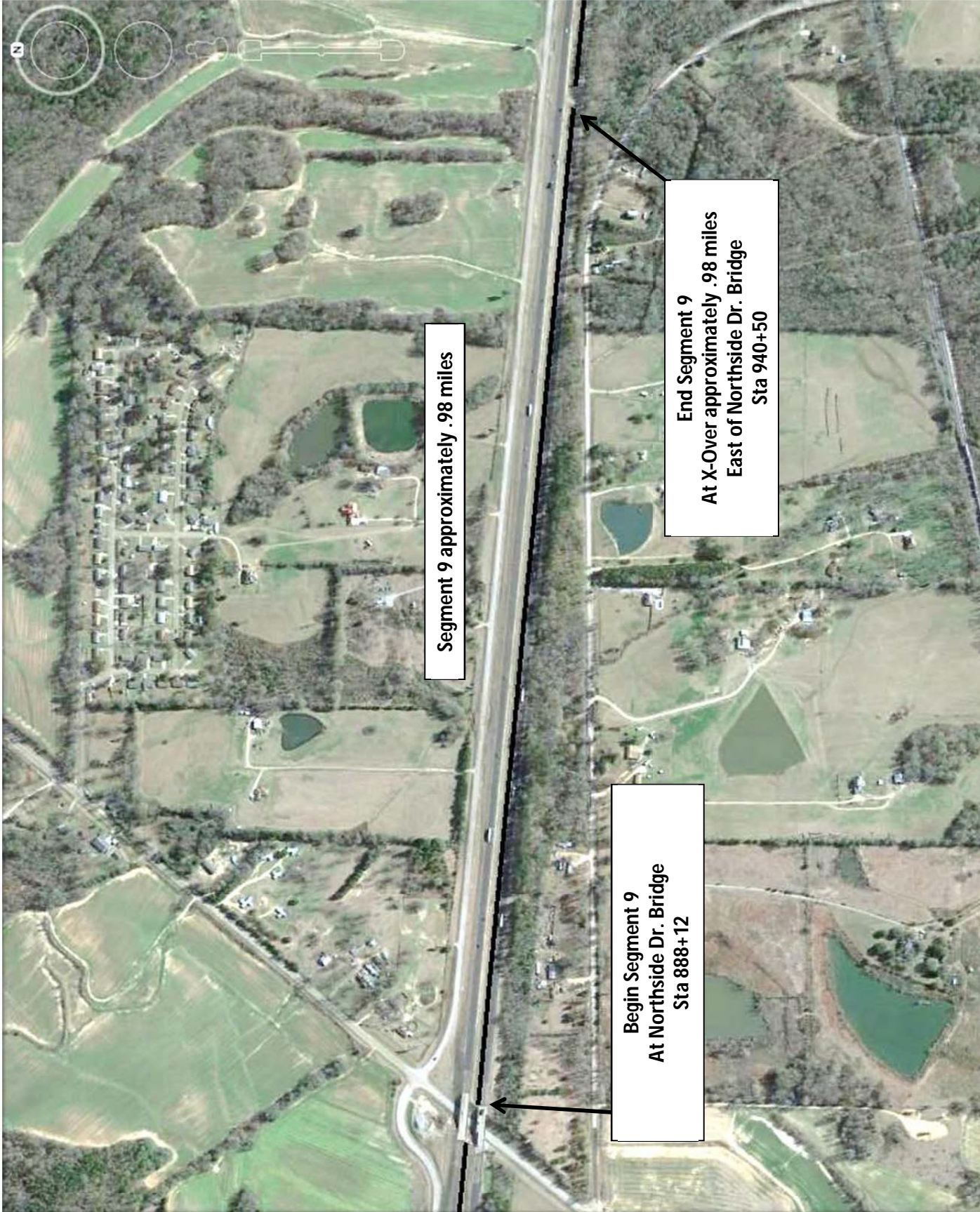
End Segment 7
At Northside Dr. Bridge
Sta 886+78

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County

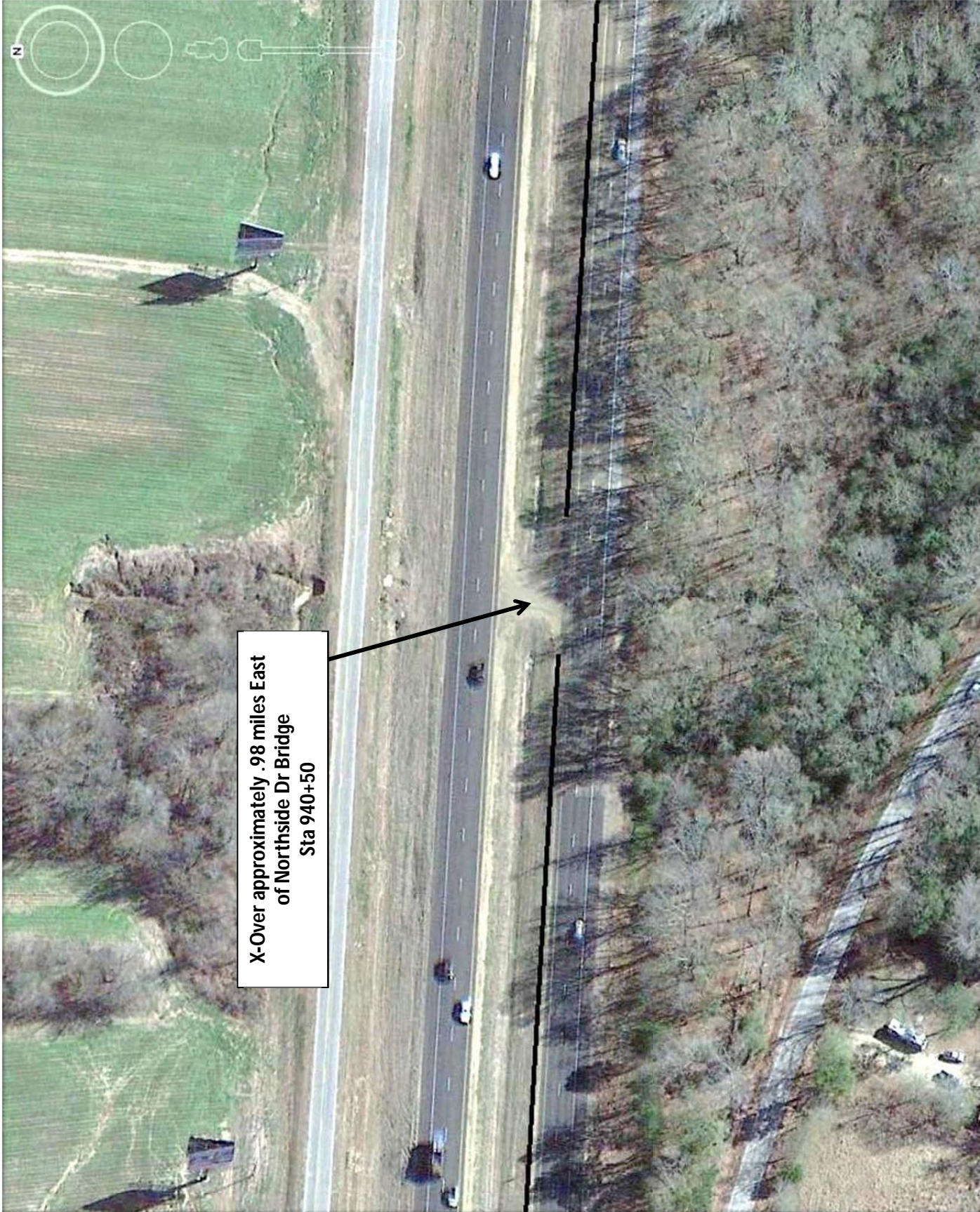


Northside Dr. Bridge
Western Bridge End Sta 886+78
Eastern Bridge End Sta 888+12

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County

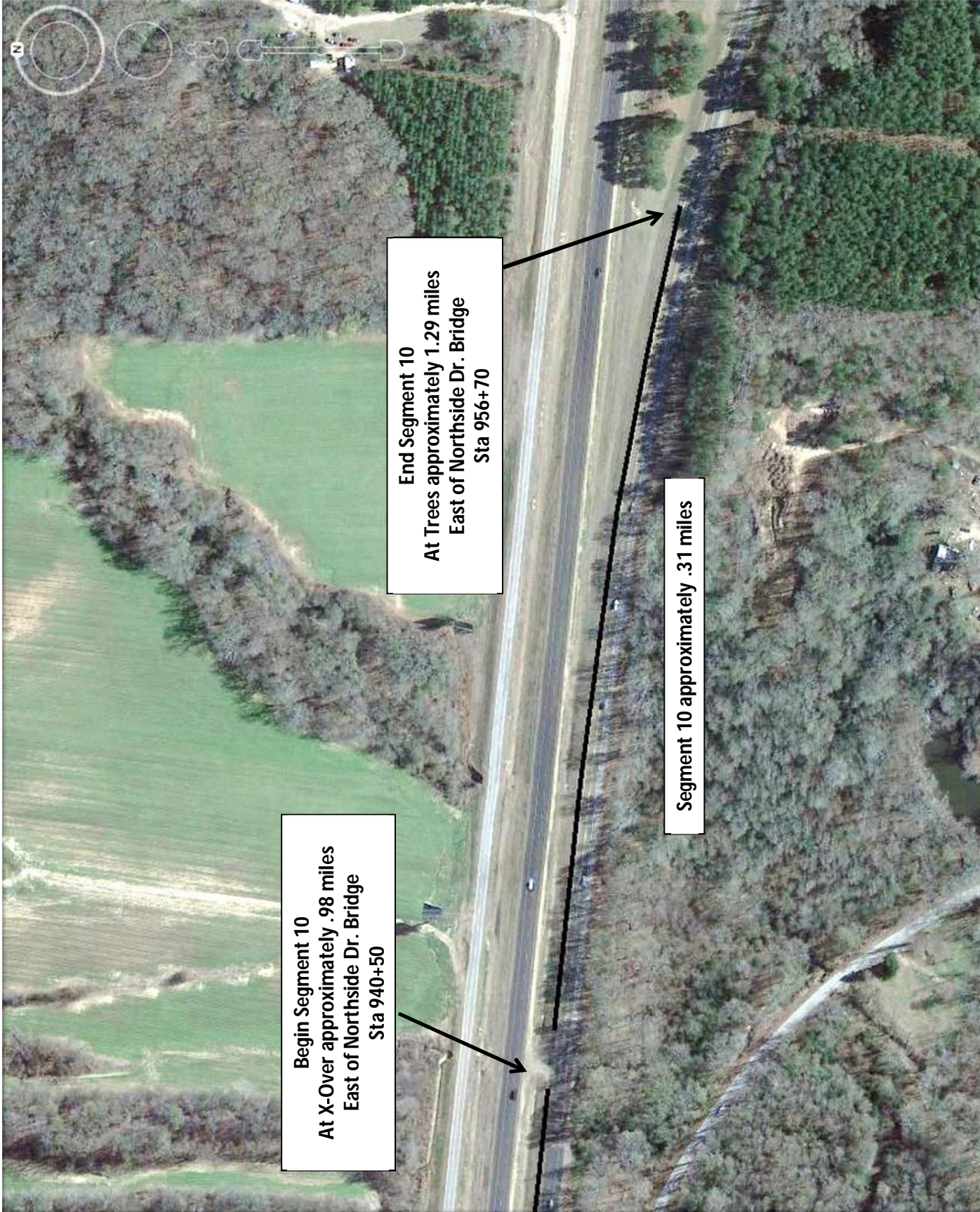


Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



X-Over approximately .98 miles East
of Northside Dr Bridge
Sta 940+50

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



Begin Segment 10
At X-Over approximately .98 miles
East of Northside Dr. Bridge
Sta 940+50

End Segment 10
At Trees approximately 1.29 miles
East of Northside Dr. Bridge
Sta 956+70

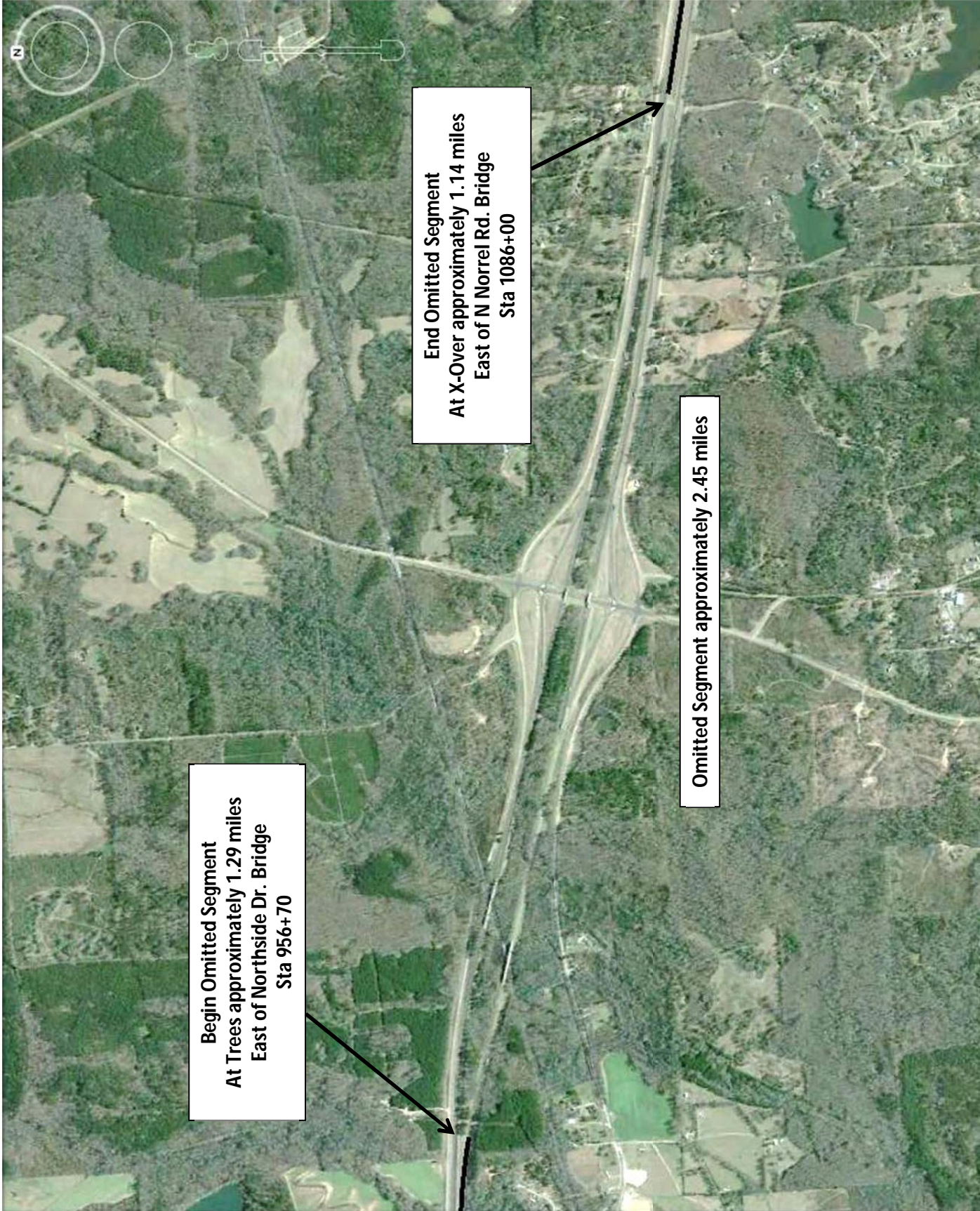
Segment 10 approximately .31 miles

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



Trees approximately 1.29 miles East
of Northside Dr. Bridge
Sta 956+70

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



Begin Omitted Segment
At Trees approximately 1.29 miles
East of Northside Dr. Bridge
Sta 956+70

End Omitted Segment
At X-Over approximately 1.14 miles
East of N Norrel Rd. Bridge
Sta 1086+00

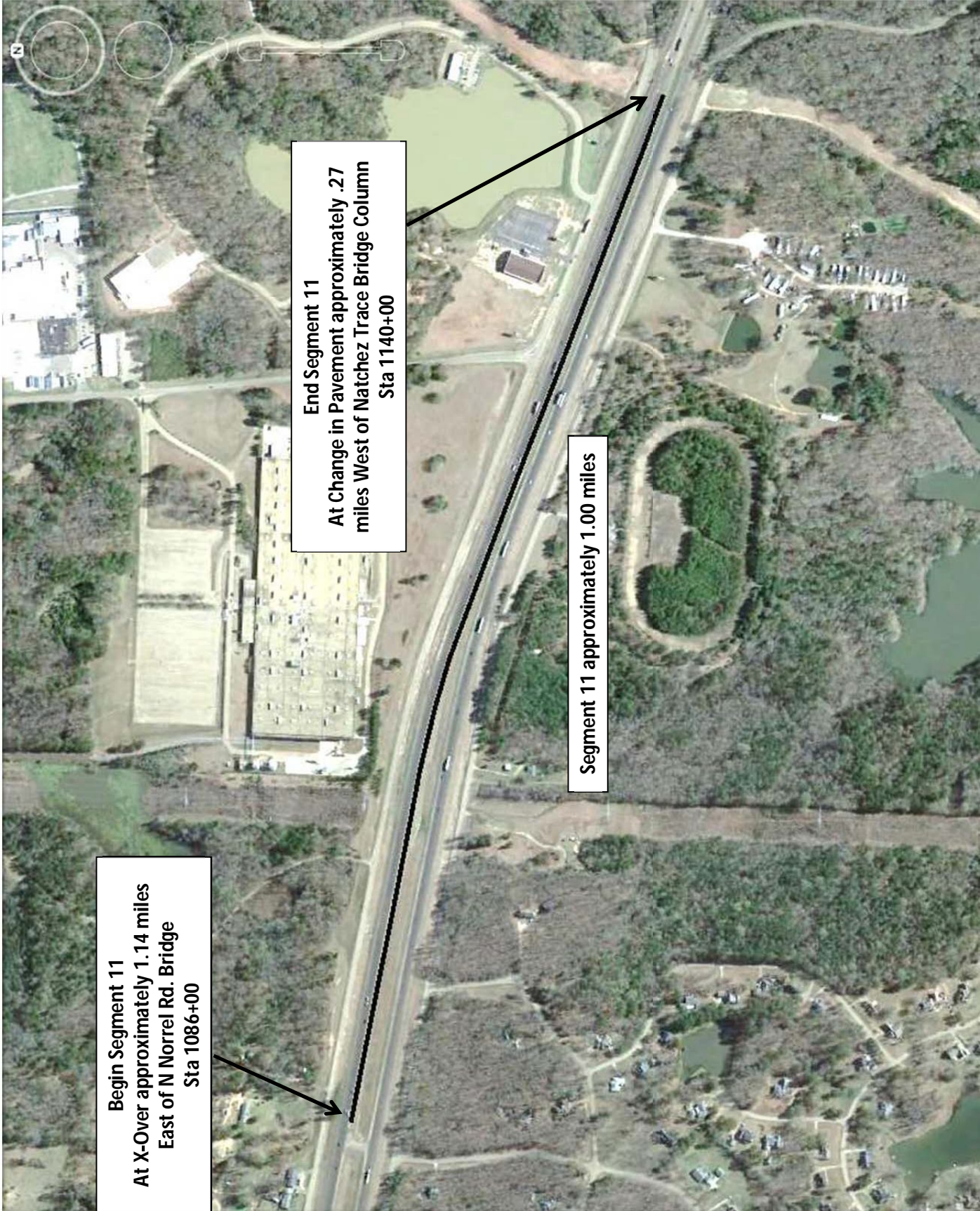
Omitted Segment approximately 2.45 miles

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



X-Over approximately 1.14 miles East
of N Norrel Rd. Bridge
Sta 1086+00

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County

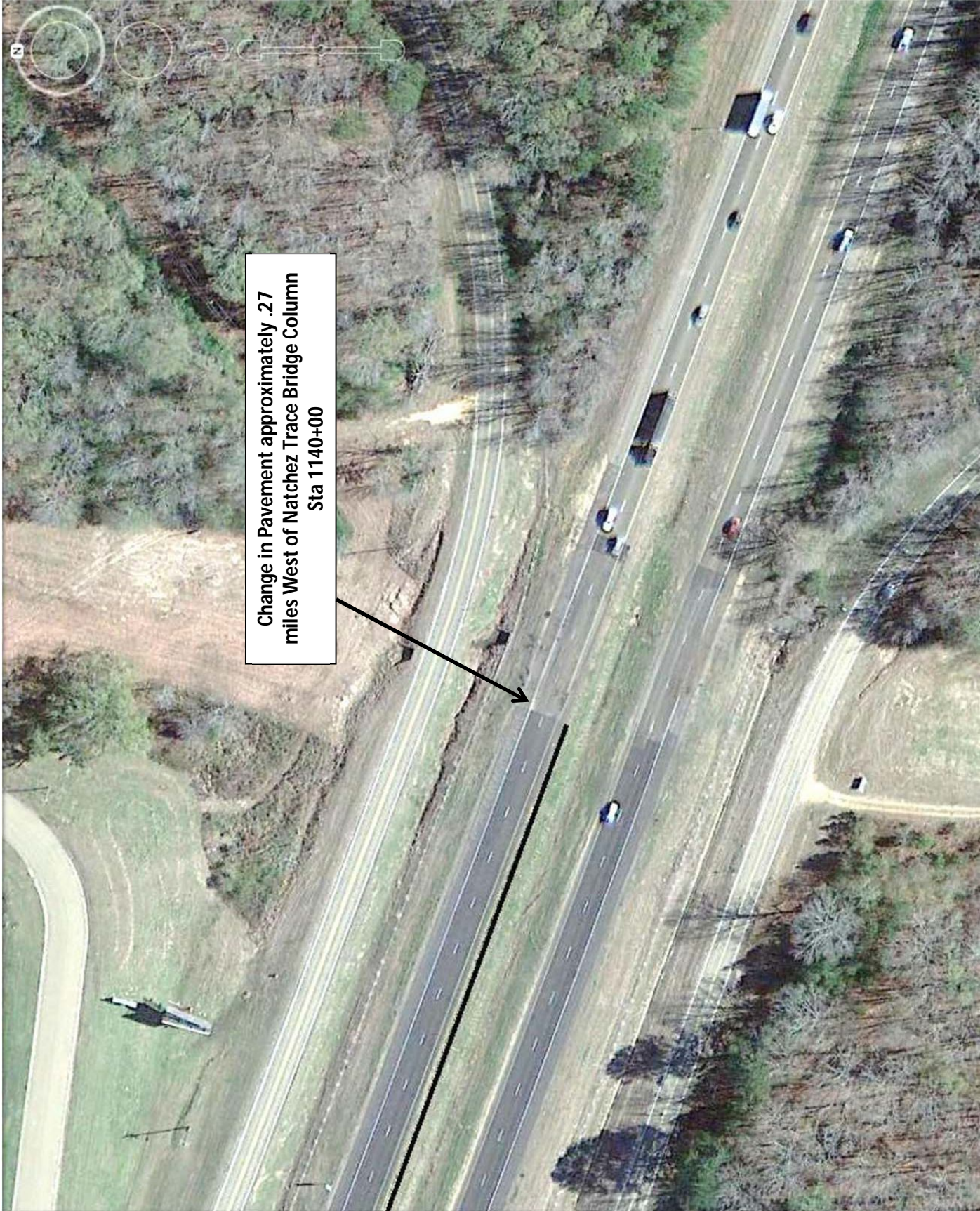


Begin Segment 11
At X-Over approximately 1.14 miles
East of N Norrel Rd. Bridge
Sta 1086+00

End Segment 11
At Change in Pavement approximately .27
miles West of Natchez Trace Bridge Column
Sta 1140+00

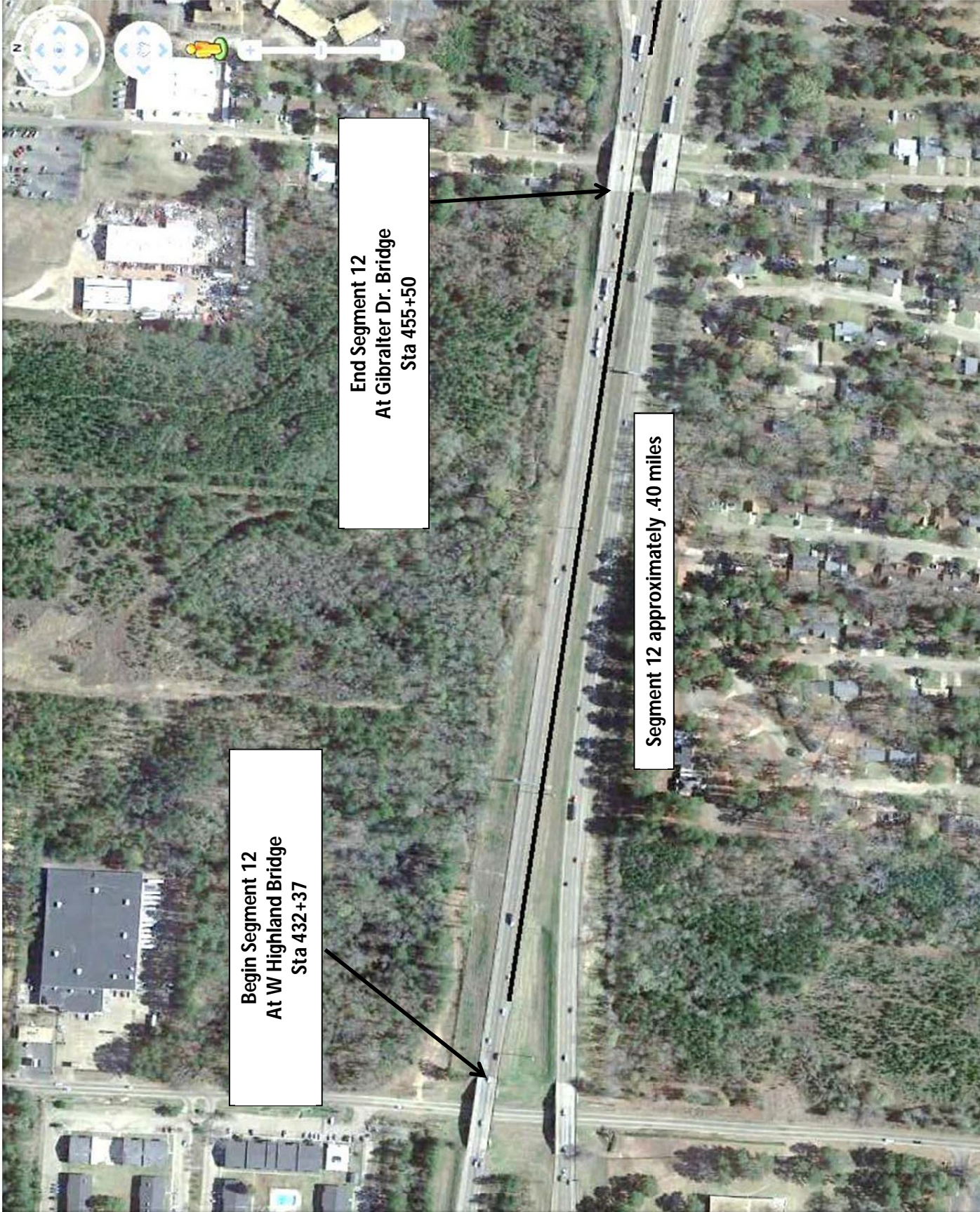
Segment 11 approximately 1.00 miles

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



Change in Pavement approximately .27
miles West of Natchez Trace Bridge Column
Sta 1140+00

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County

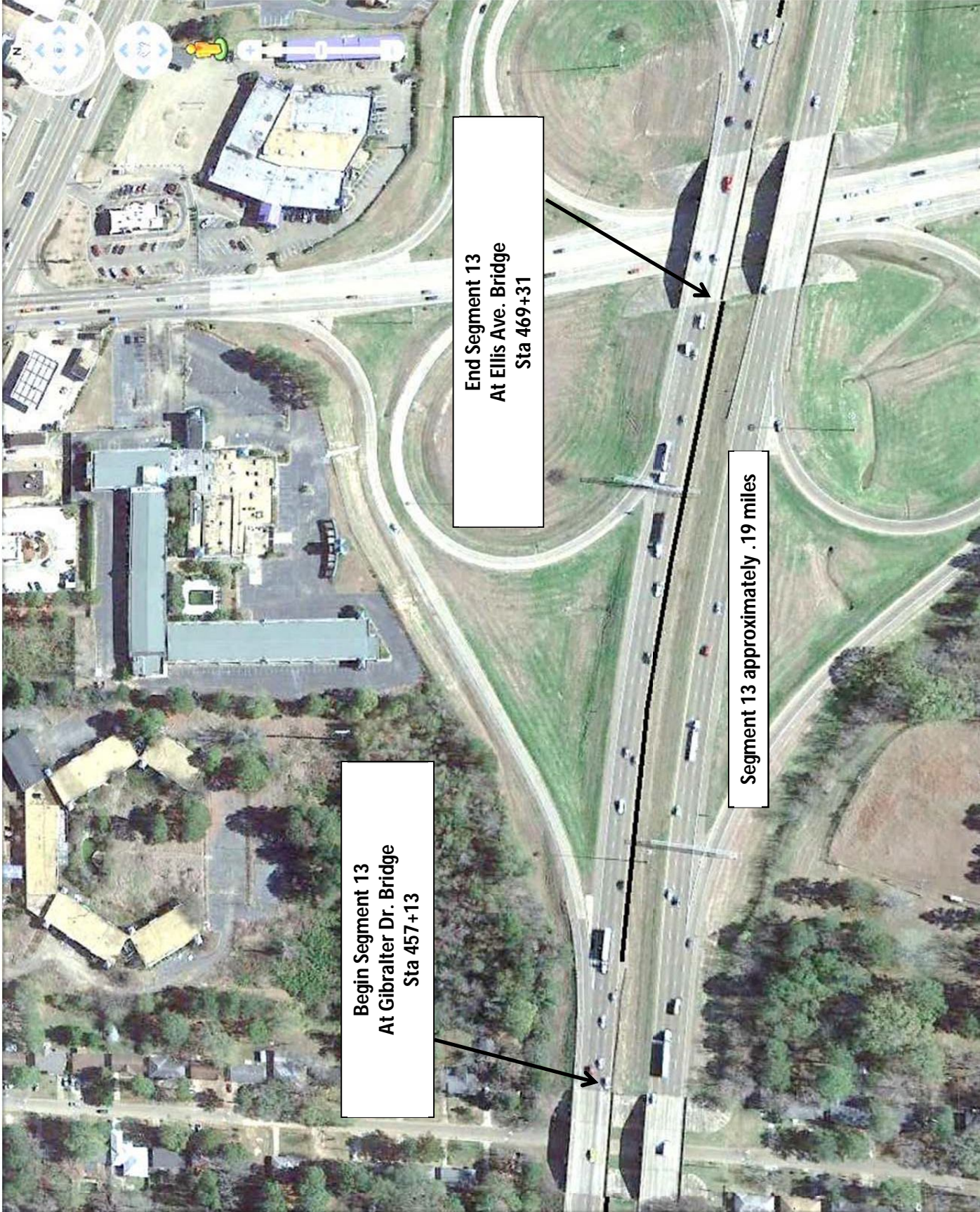


**Begin Segment 12
At W Highland Bridge
Sta 432+37**

**End Segment 12
At Gibraltar Dr. Bridge
Sta 455+50**

Segment 12 approximately .40 miles

Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



Begin Segment 13
At Gibraltar Dr. Bridge
Sta 457+13

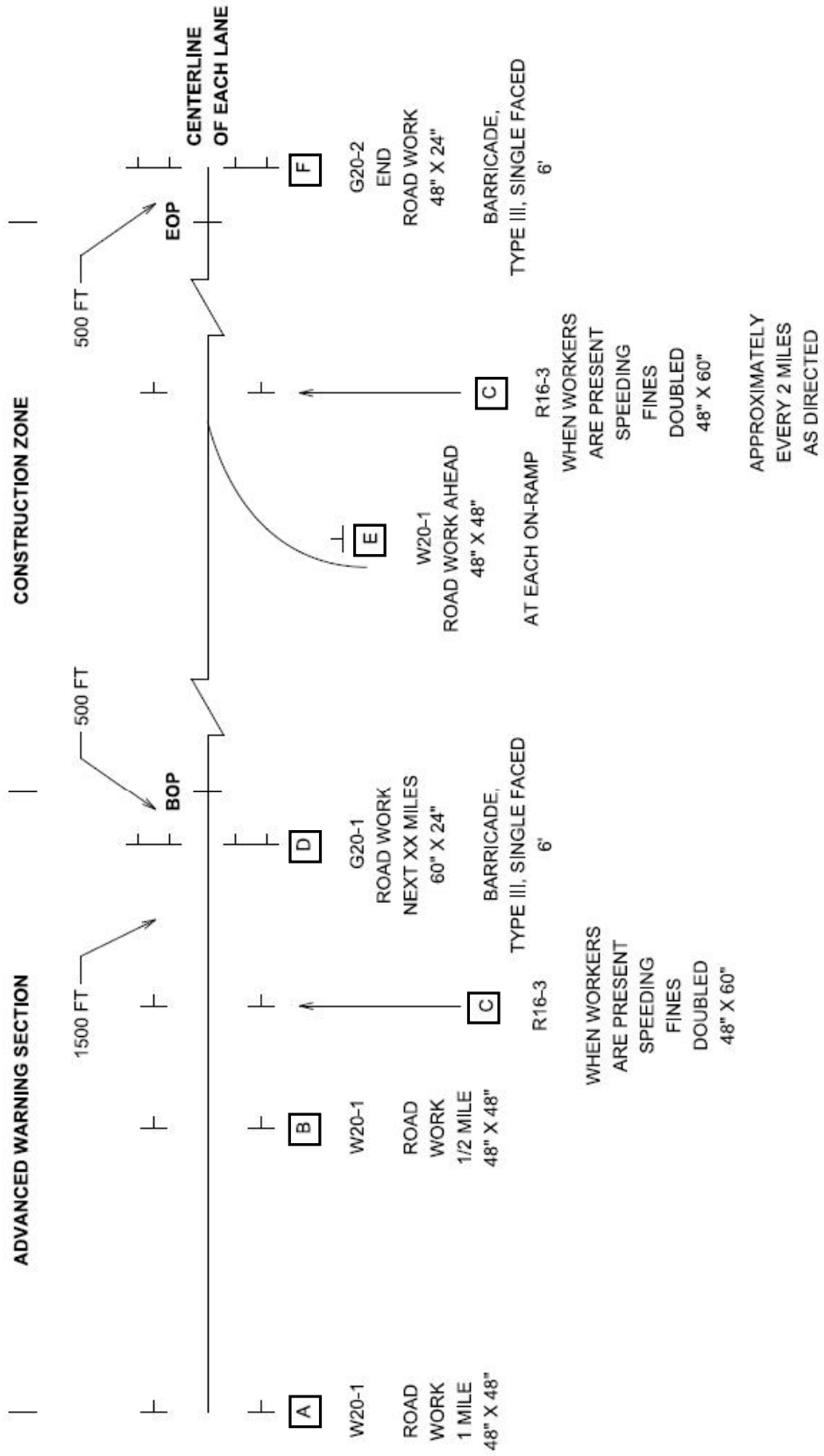
End Segment 13
At Ellis Ave. Bridge
Sta 469+31

Segment 13 approximately .19 miles

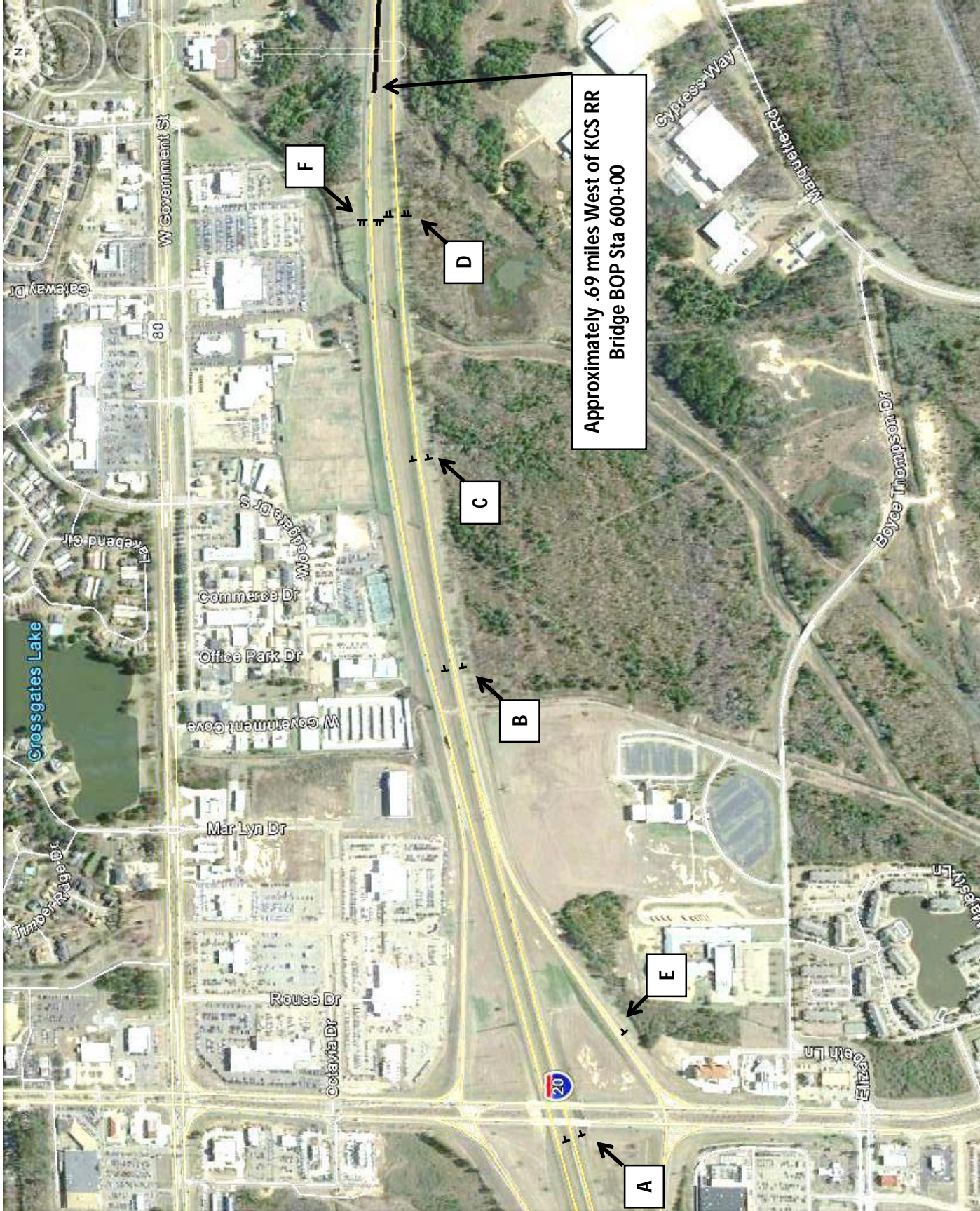
Cable Rail Location Detail
I-20 from Big Black to Valley Street
Hinds County



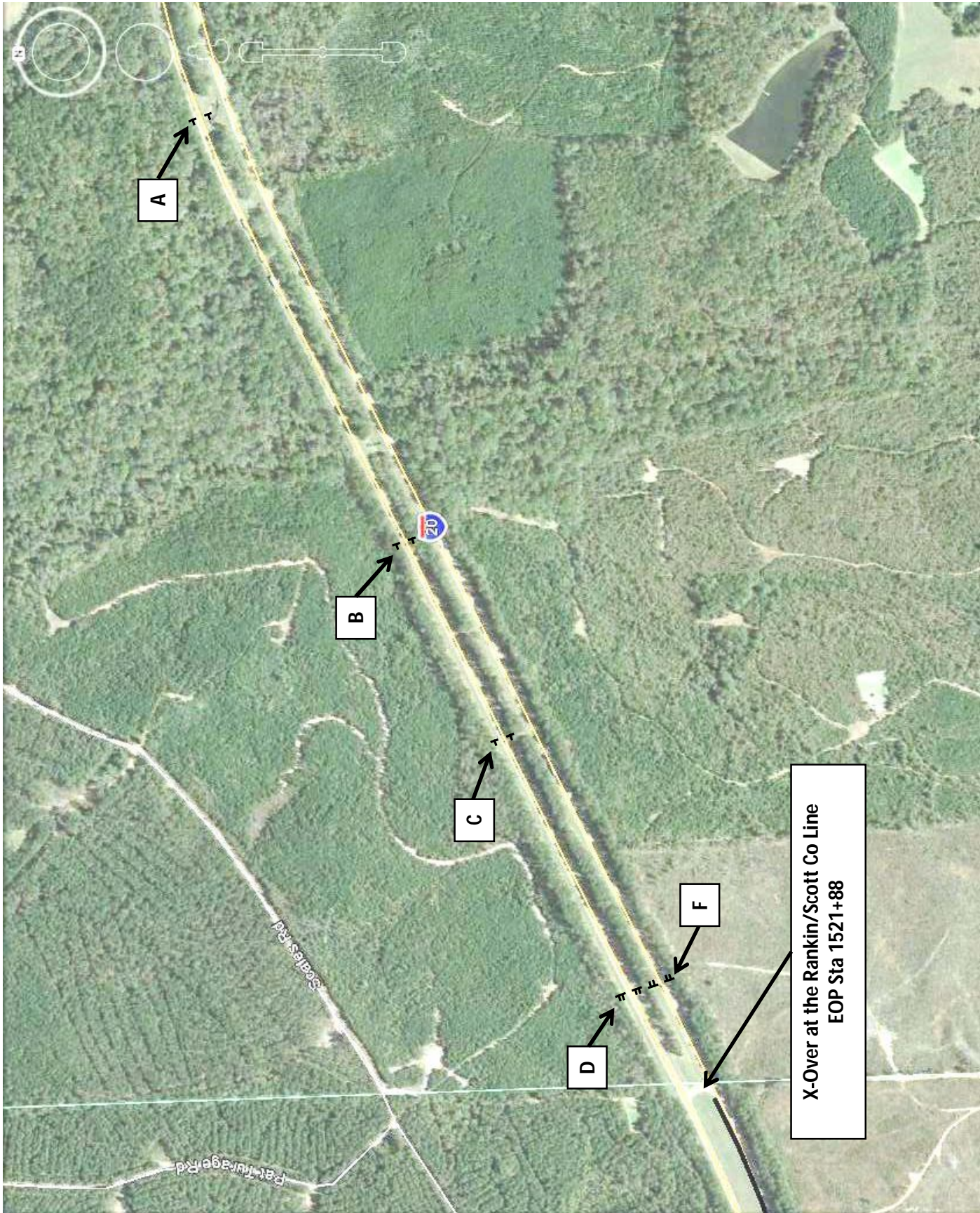
Construction Signing Detail
 I-20 from Crossgates Blvd. to 1 Mile West of Gulde-Shilo Rd.
 Rankin County



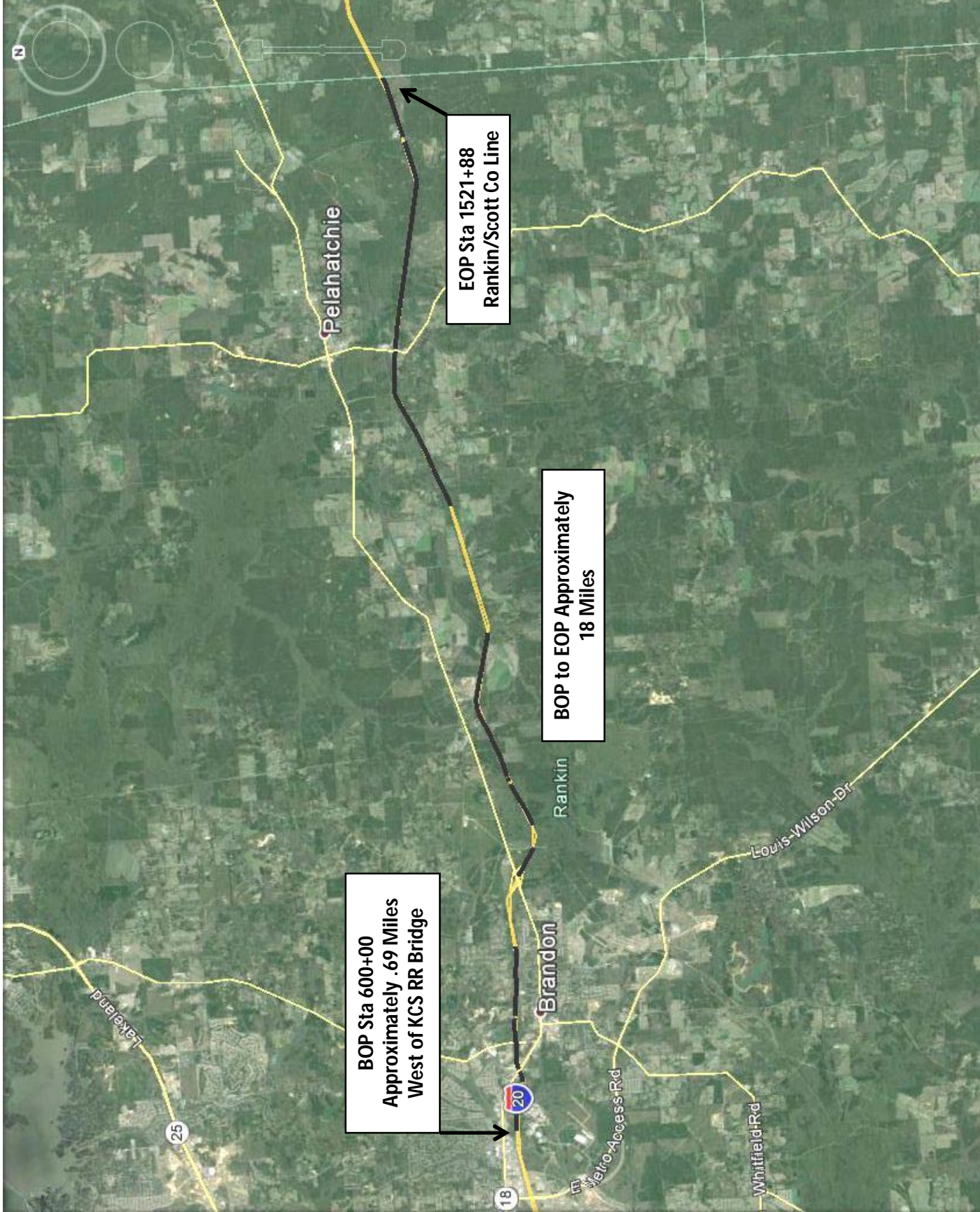
Construction Signing Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



Construction Signing Detail
I-20 from Crossgates Blvd. to 1 Mile West of Gulde-Shilo Rd.
Rankin County



Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County

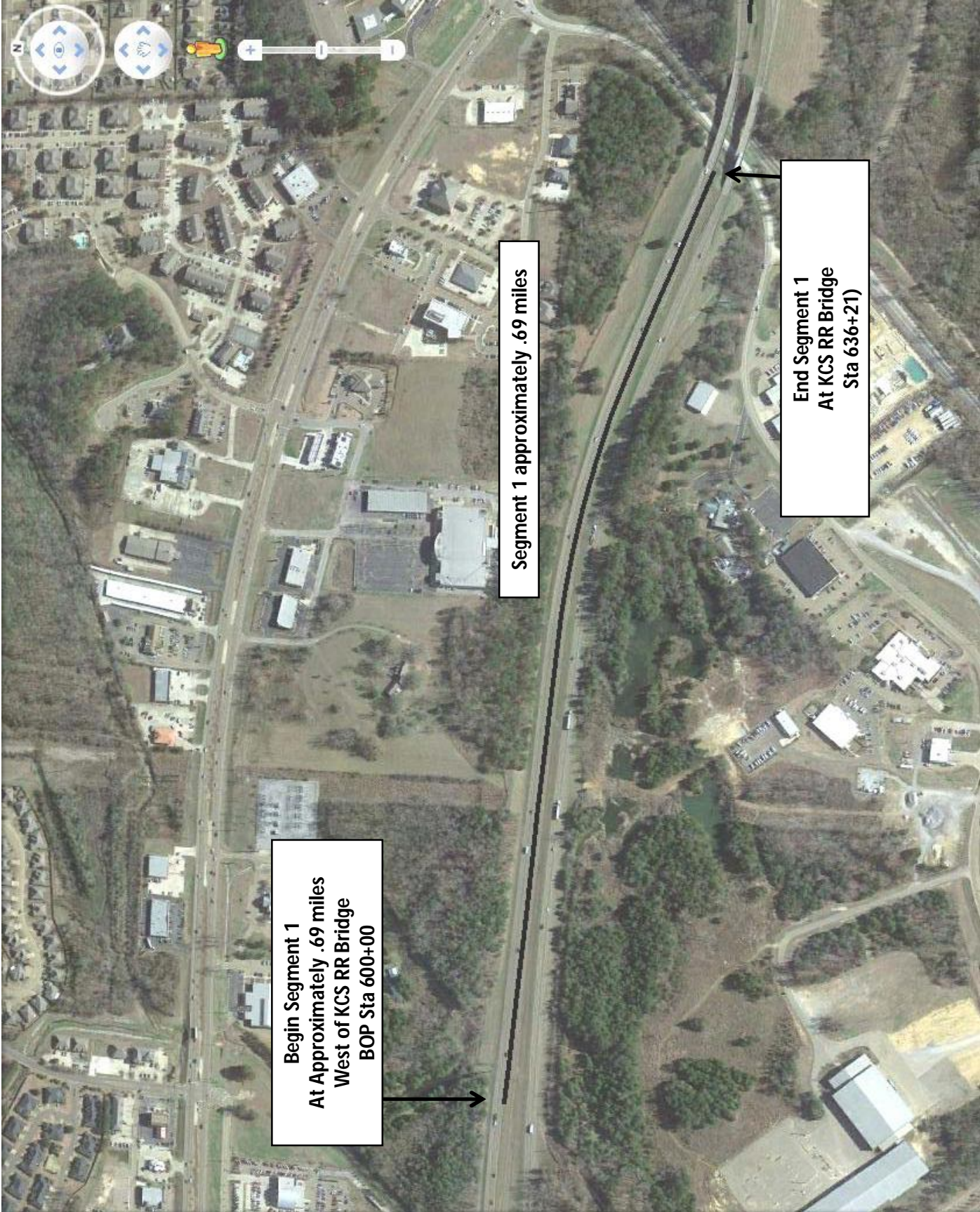


Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



Approximately .69 miles West of KCS
RR Bridge BOP Sta 600+00

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



**Begin Segment 1
At Approximately .69 miles
West of KCS RR Bridge
BOP Sta 600+00**

Segment 1 approximately .69 miles

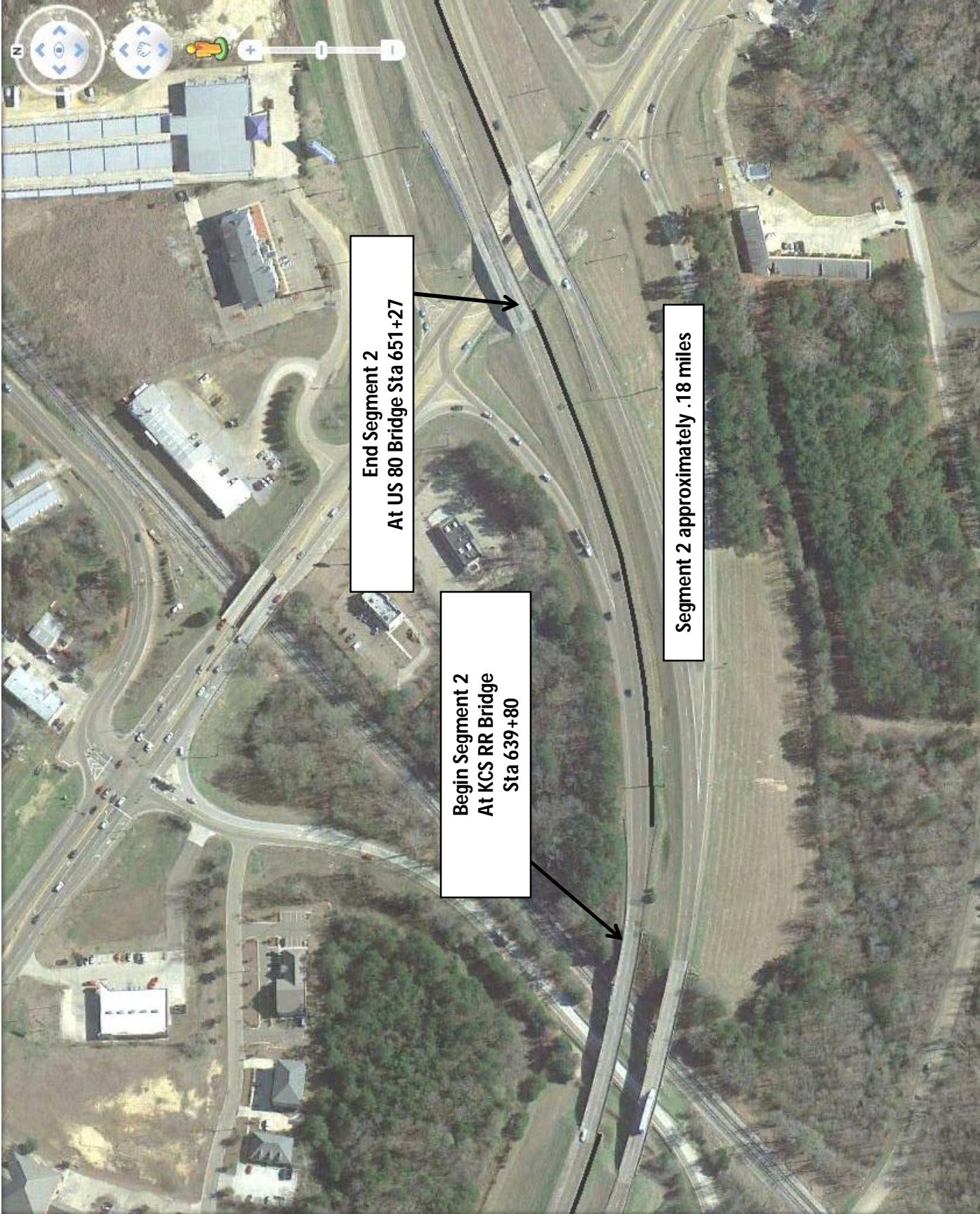
**End Segment 1
At KCS RR Bridge
Sta 636+21**

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Gulde-Shilo Rd.
Rankin County



KCS RR Bridge
Sta 636+21

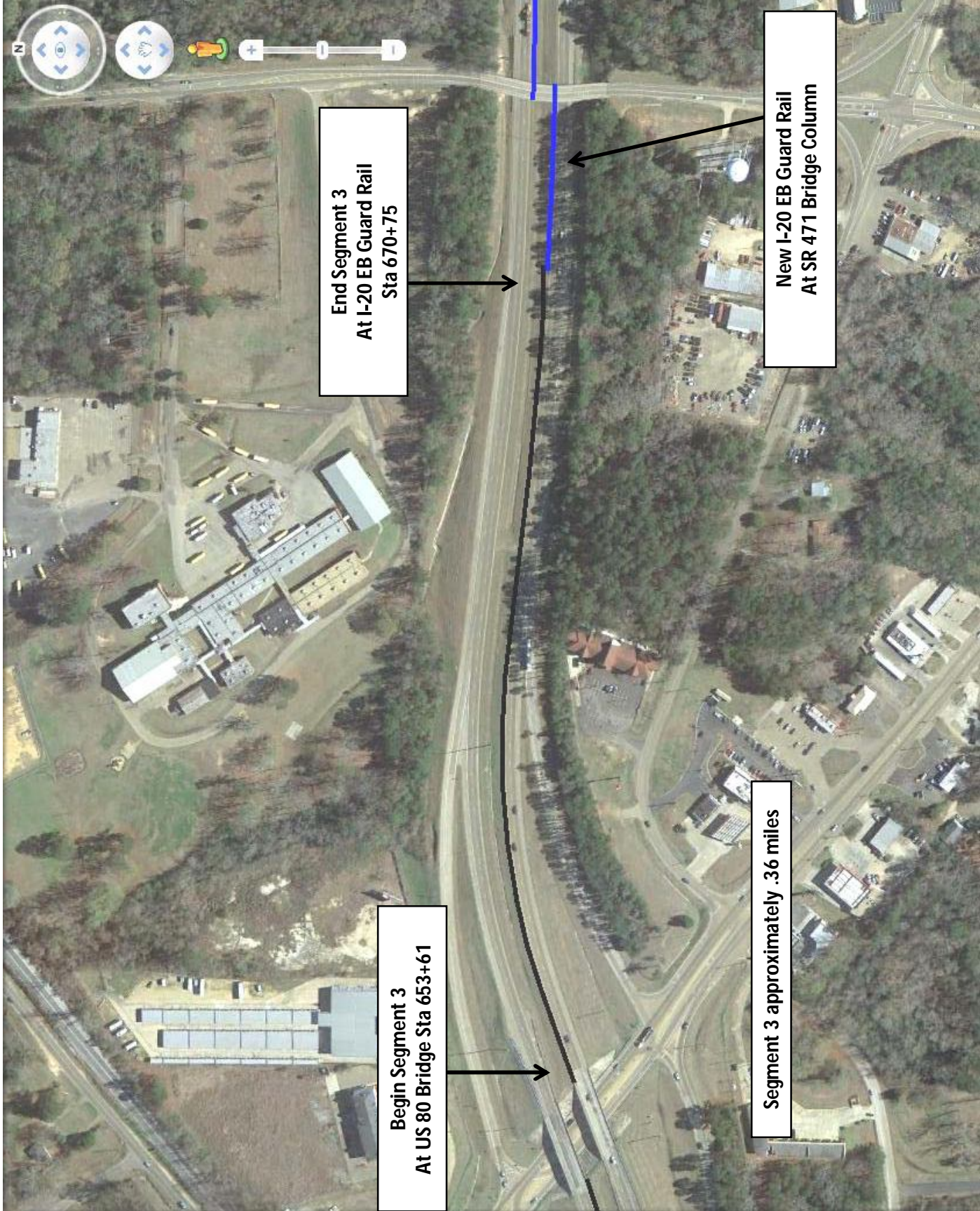
Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



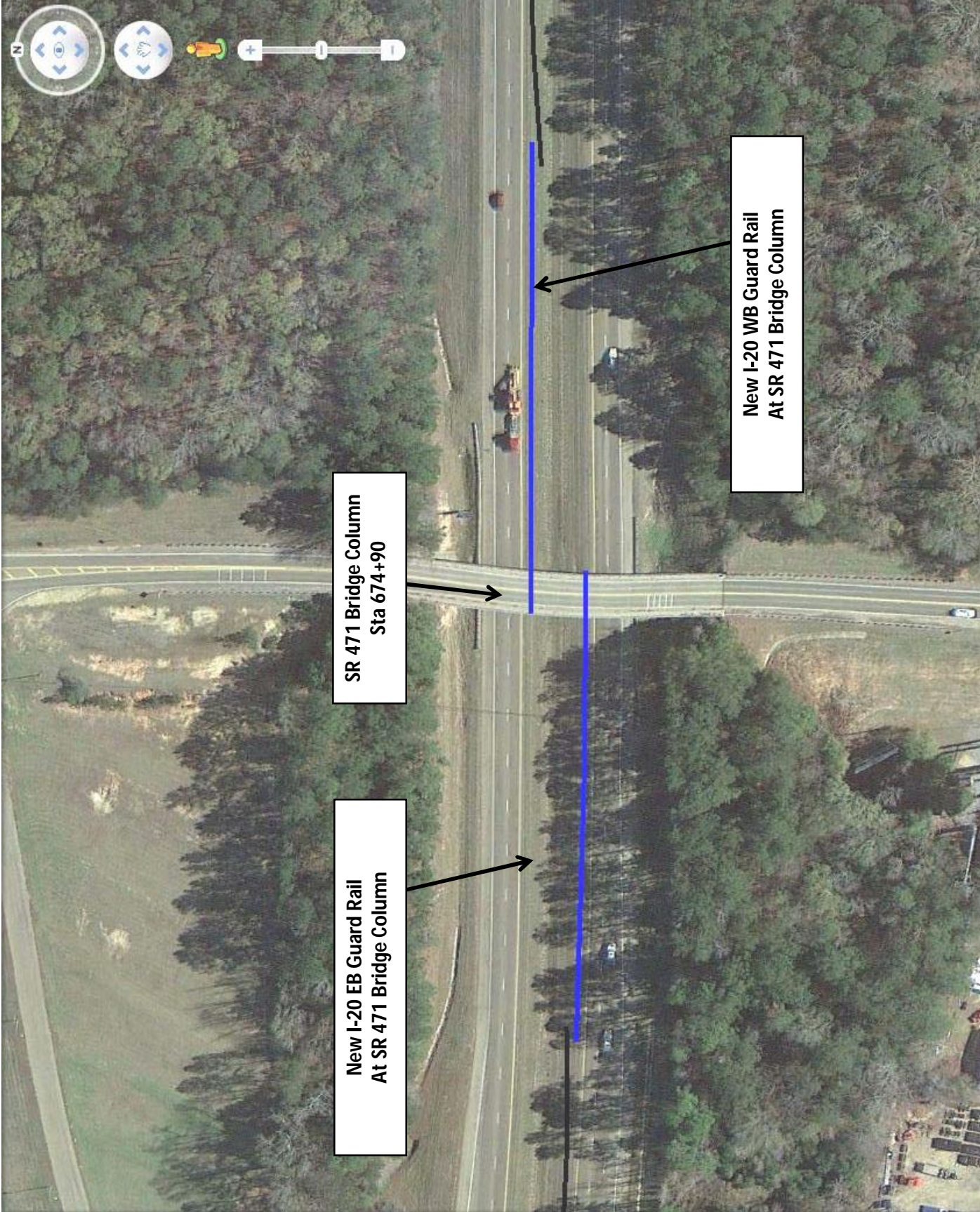
**End Segment 3
At I-20 EB Guard Rail
Sta 670+75**

**New I-20 EB Guard Rail
At SR 471 Bridge Column**

**Begin Segment 3
At US 80 Bridge Sta 653+61**

Segment 3 approximately .36 miles

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County

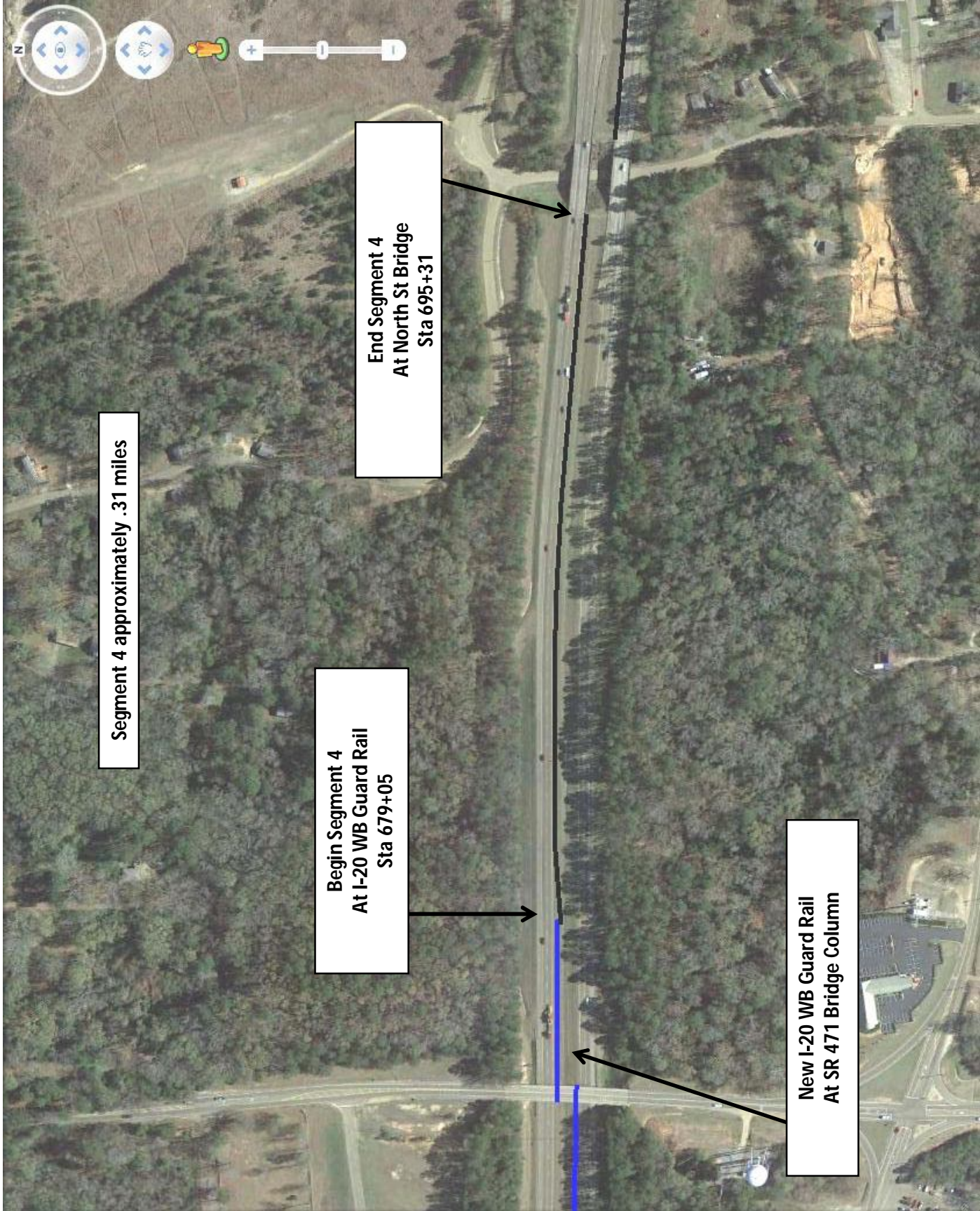


SR 471 Bridge Column
Sta 674+90

New I-20 EB Guard Rail
At SR 471 Bridge Column

New I-20 WB Guard Rail
At SR 471 Bridge Column

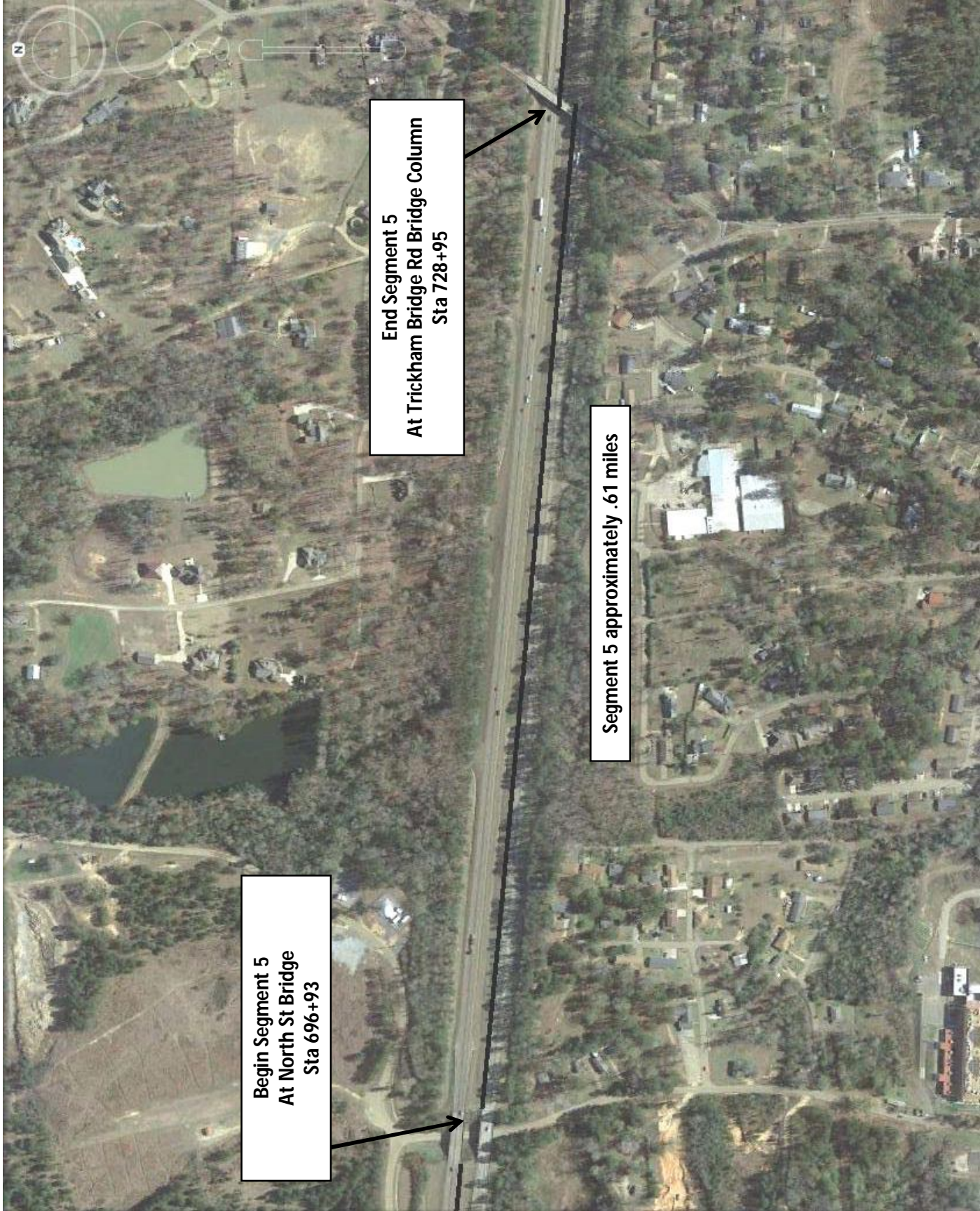
Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



**Begin Segment 5
At North St Bridge
Sta 696+93**

**End Segment 5
At Trickham Bridge Rd Bridge Column
Sta 728+95**

Segment 5 approximately .61 miles

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



Trickham Bridge Rd
Bridge Column
Sta 728+95

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County

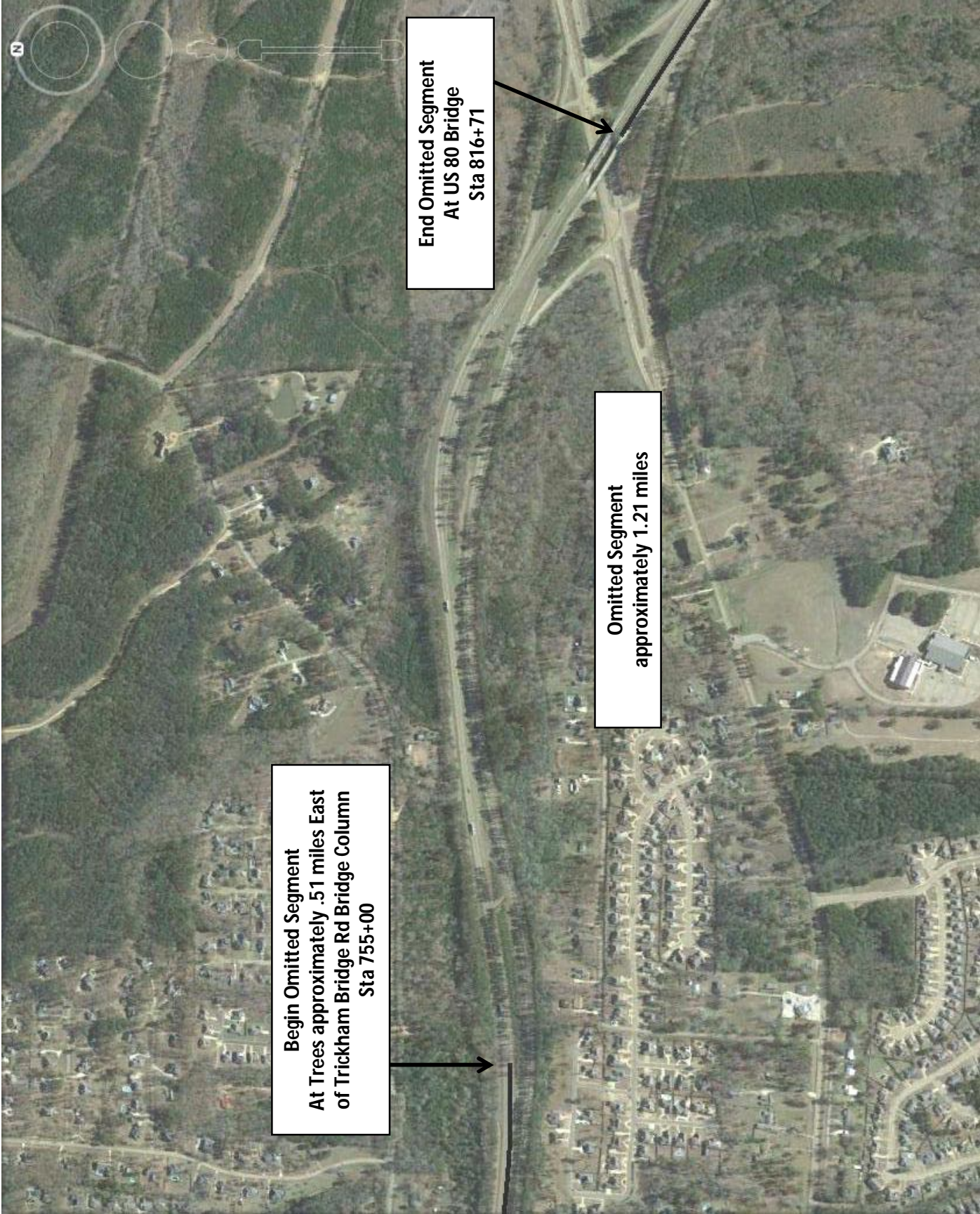


**Begin Segment 6
At Trickham Bridge Rd
Bridge Column
Sta 728+95**

**End Segment 6
At Trees approximately .51 miles East
of Trickham Bridge Rd Bridge Column
Sta 755+00**

Segment 6 approximately .51 miles

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County

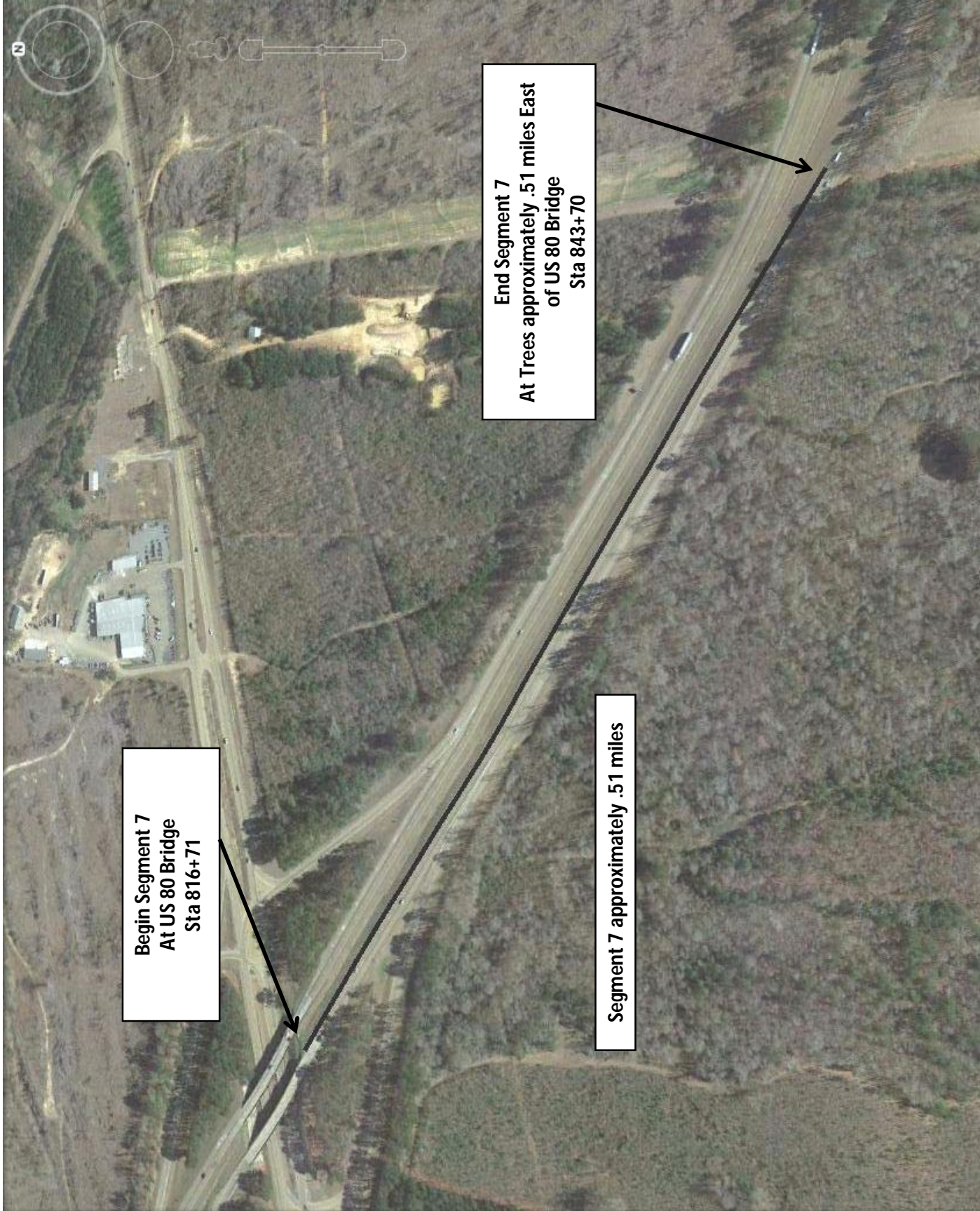


Begin Omitted Segment
At Trees approximately .51 miles East
of Trickham Bridge Rd Bridge Column
Sta 755+00

Omitted Segment
approximately 1.21 miles

End Omitted Segment
At US 80 Bridge
Sta 816+71

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County

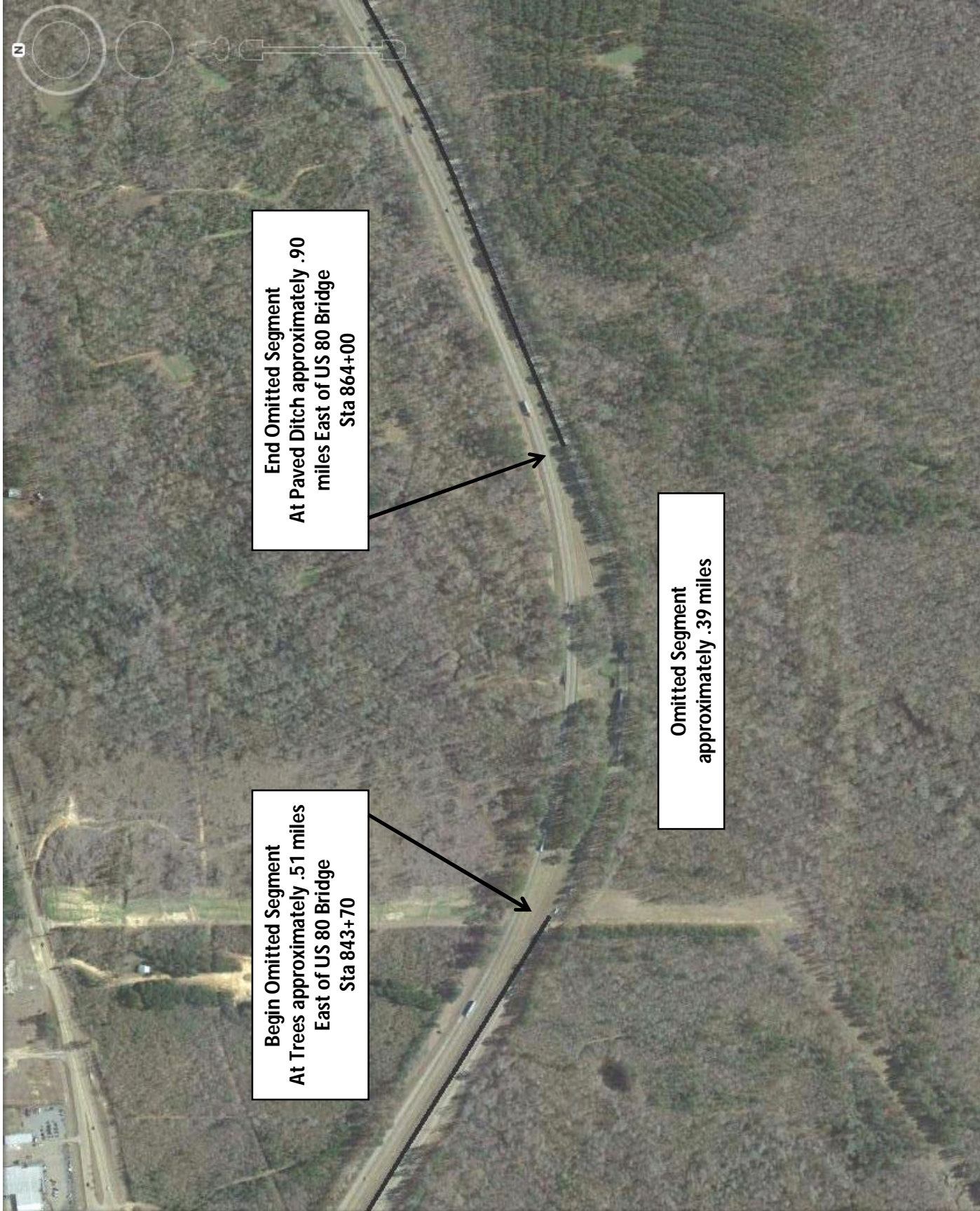


**Begin Segment 7
At US 80 Bridge
Sta 816+71**

**End Segment 7
At Trees approximately .51 miles East
of US 80 Bridge
Sta 843+70**

Segment 7 approximately .51 miles

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



Begin Omitted Segment
At Trees approximately .51 miles
East of US 80 Bridge
Sta 843+70

End Omitted Segment
At Paved Ditch approximately .90
miles East of US 80 Bridge
Sta 864+00

Omitted Segment
approximately .39 miles

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



End Segment 8
At Guard Rail approximately 1.63
miles East of US 80 Bridge
Sta 902+50

Segment 8 approximately .74 miles

Begin Segment 8
At Paved Ditch approximately .90
miles East of US 80 Bridge
Sta 864+00

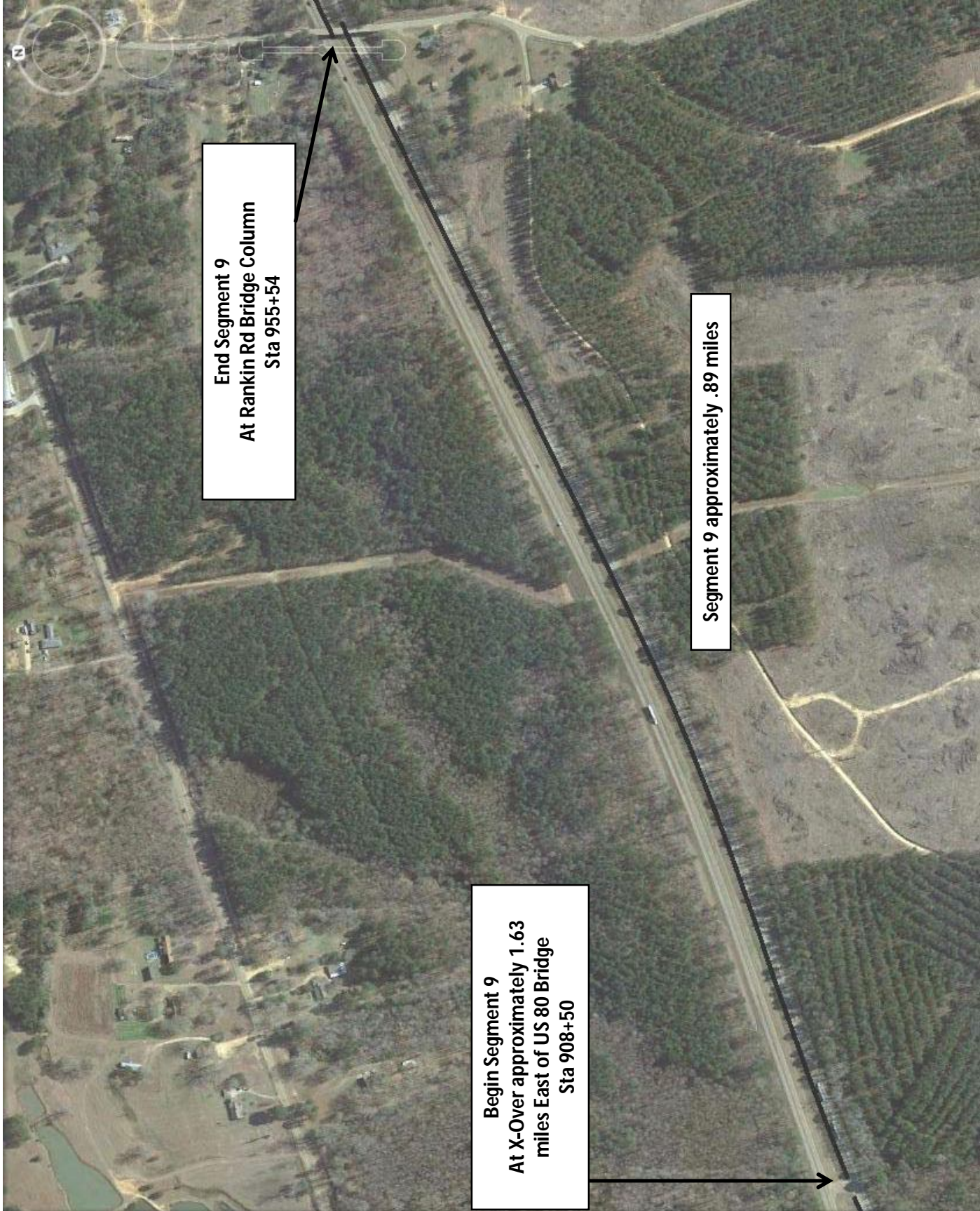
Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



**X-Over approximately 1.75 miles
East of US 80 Bridge
Sta 908+50**

**Box Culvert approximately 1.63 miles
East of US 80 Bridge
Sta 902+50**

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



**End Segment 9
At Rankin Rd Bridge Column
Sta 955+54**

Segment 9 approximately .89 miles

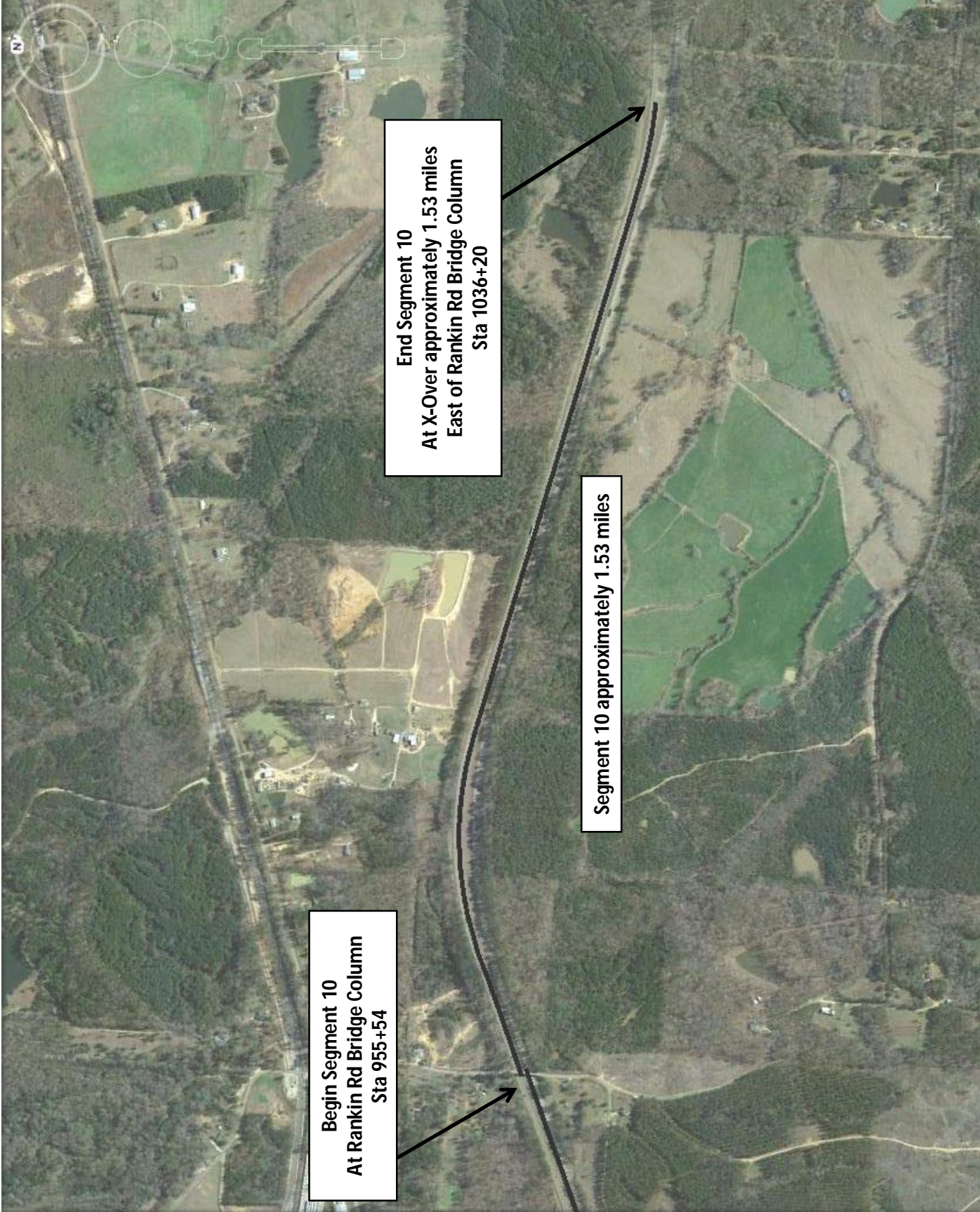
**Begin Segment 9
At X-Over approximately 1.63
miles East of US 80 Bridge
Sta 908+50**

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County

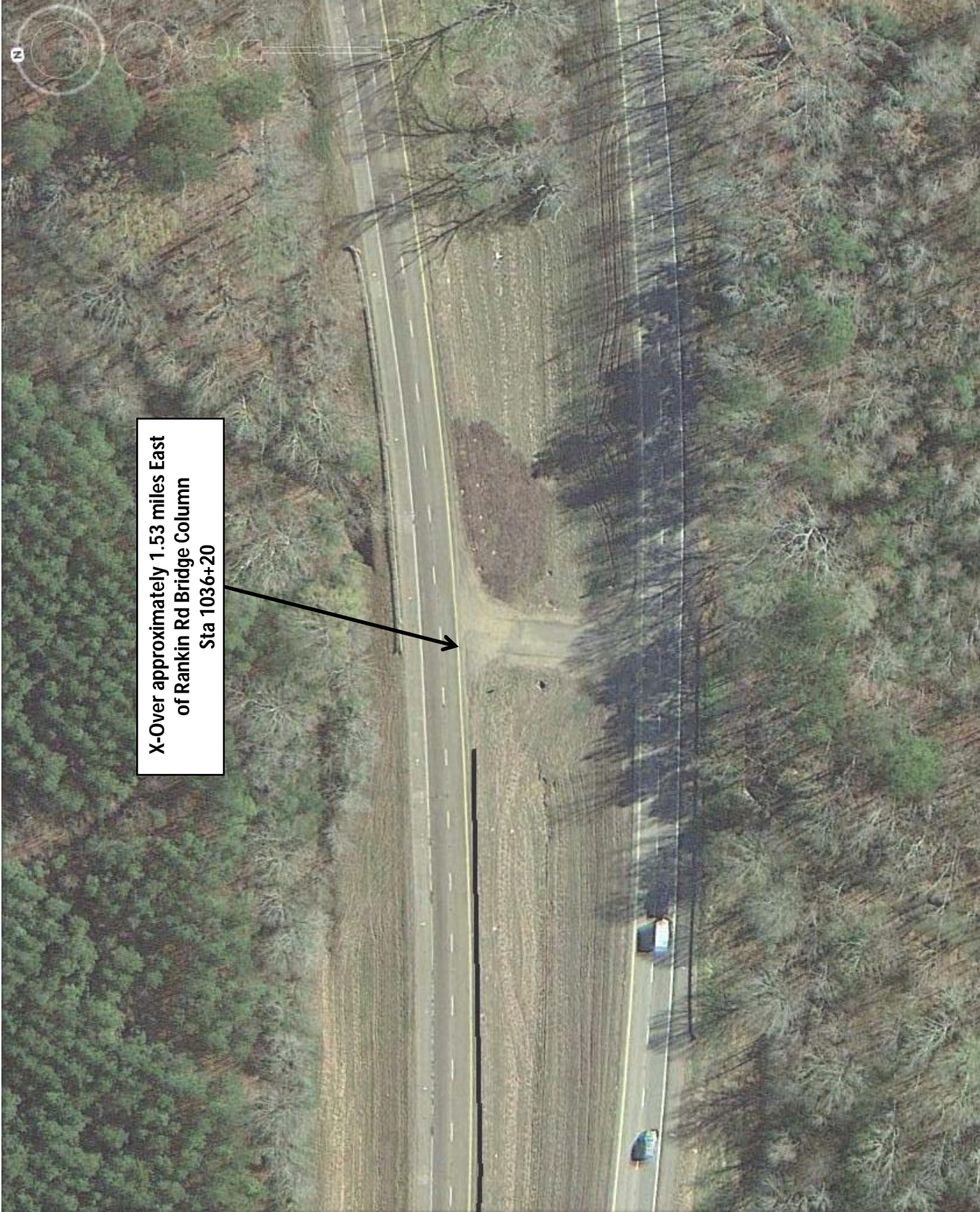


Rankin Rd Bridge Column
Sta 955+54

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County

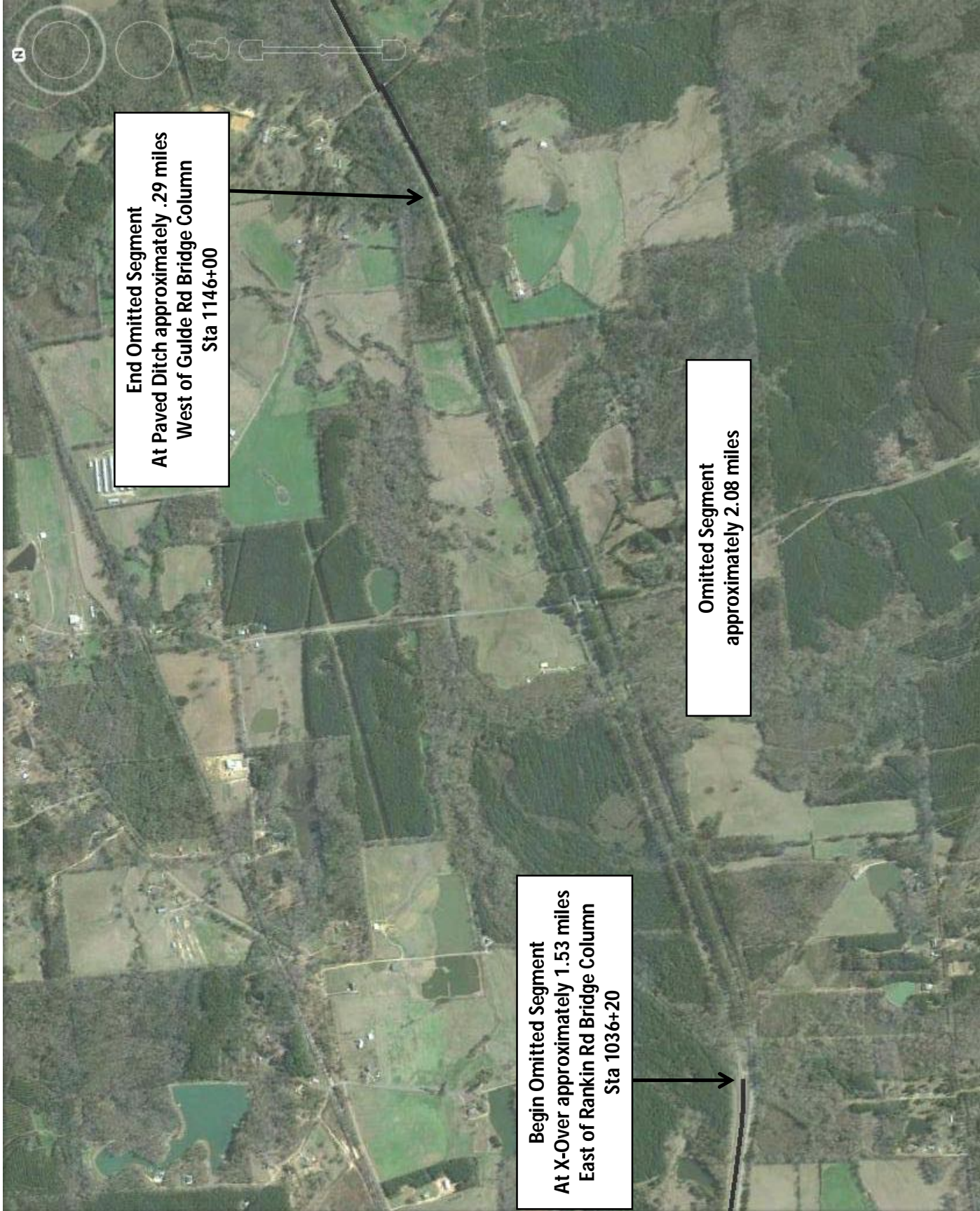


Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



X-Over approximately 1.53 miles East
of Rankin Rd Bridge Column
Sta 1036+20

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Gulde-Shilo Rd.
Rankin County

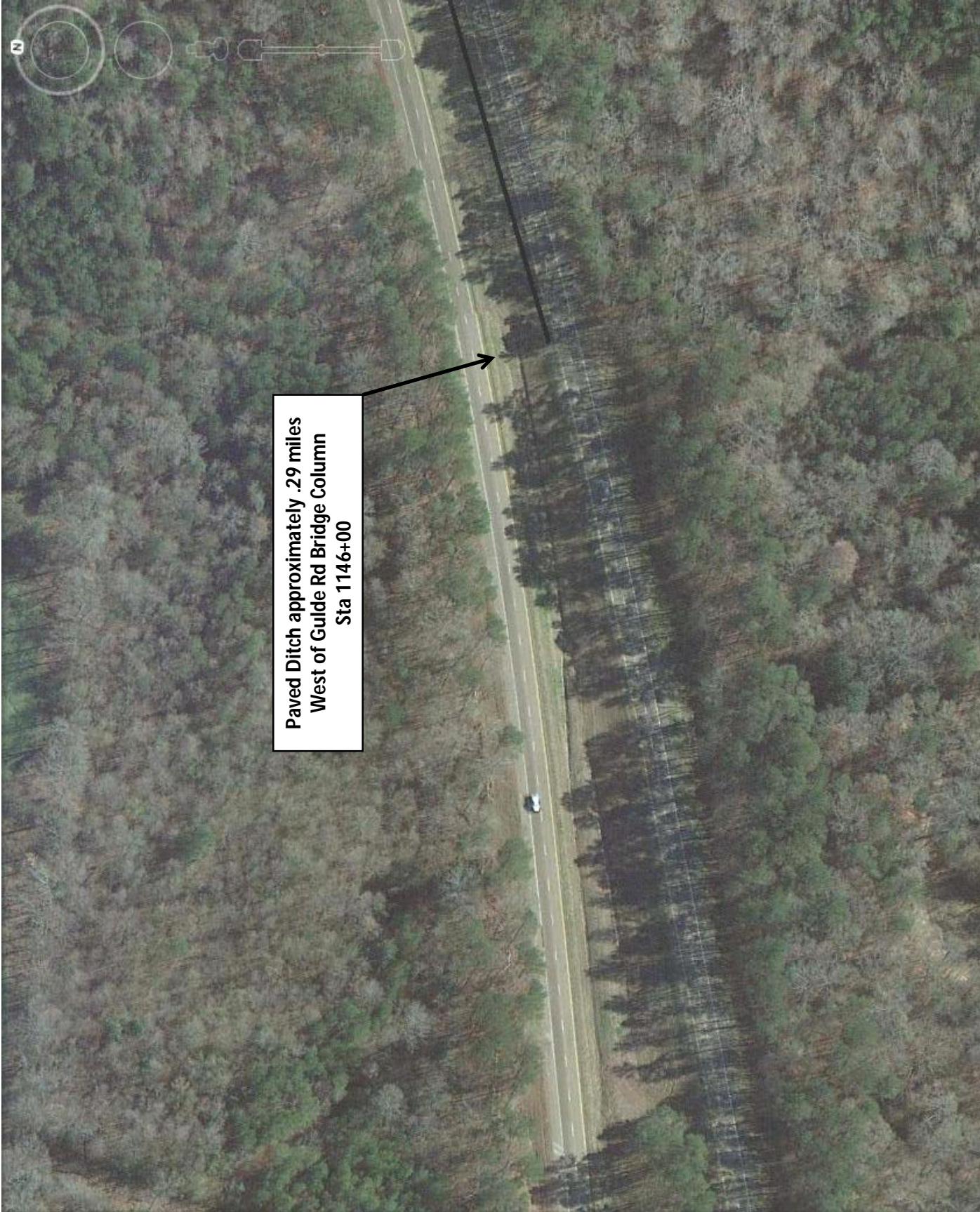


End Omitted Segment
At Paved Ditch approximately .29 miles
West of Gulde Rd Bridge Column
Sta 1146+00

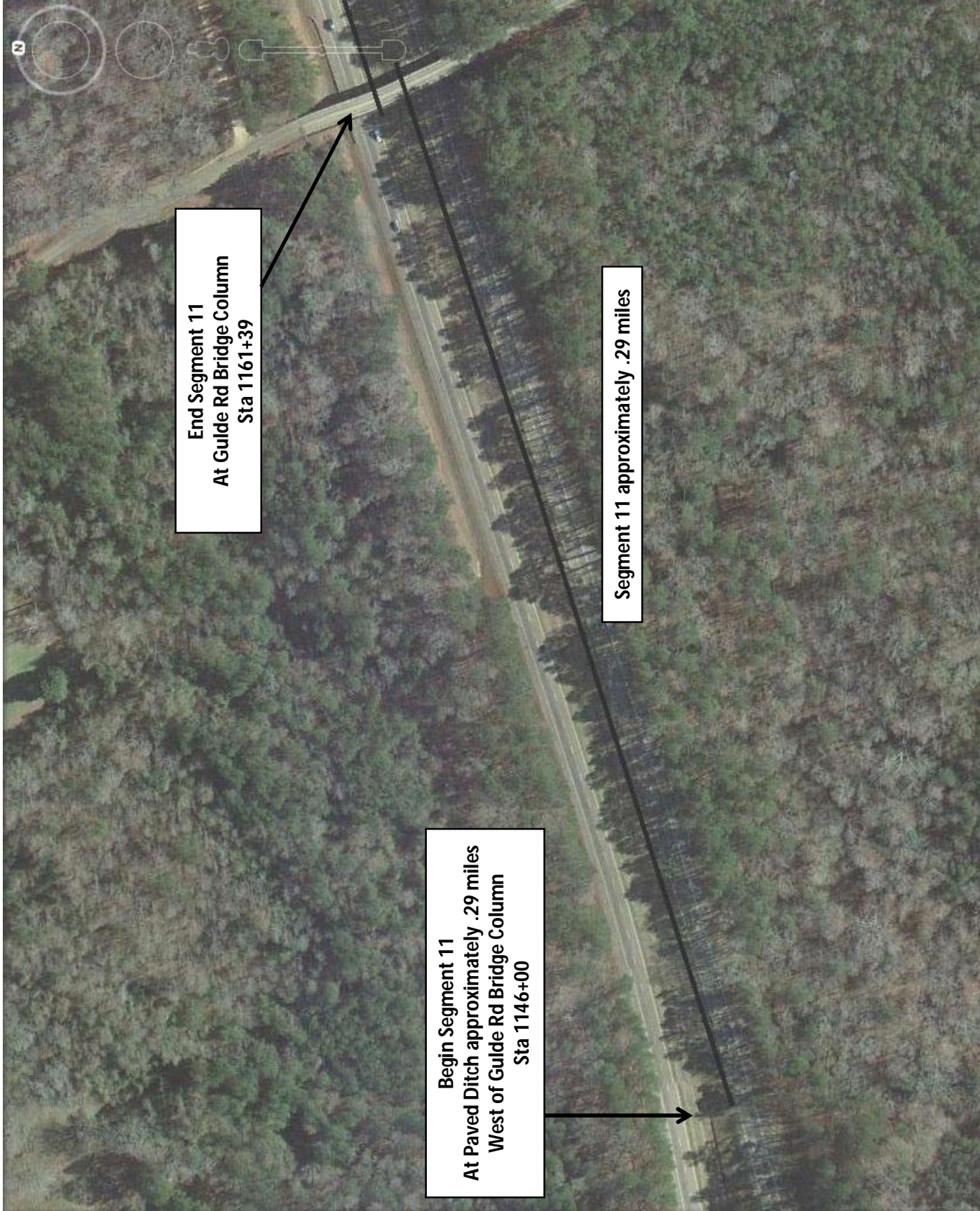
Omitted Segment
approximately 2.08 miles

Begin Omitted Segment
At X-Over approximately 1.53 miles
East of Rankin Rd Bridge Column
Sta 1036+20

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Gulde-Shilo Rd.
Rankin County



Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Gulde-Shilo Rd.
Rankin County



**End Segment 11
At Gulde Rd Bridge Column
Sta 1161+39**

Segment 11 approximately .29 miles

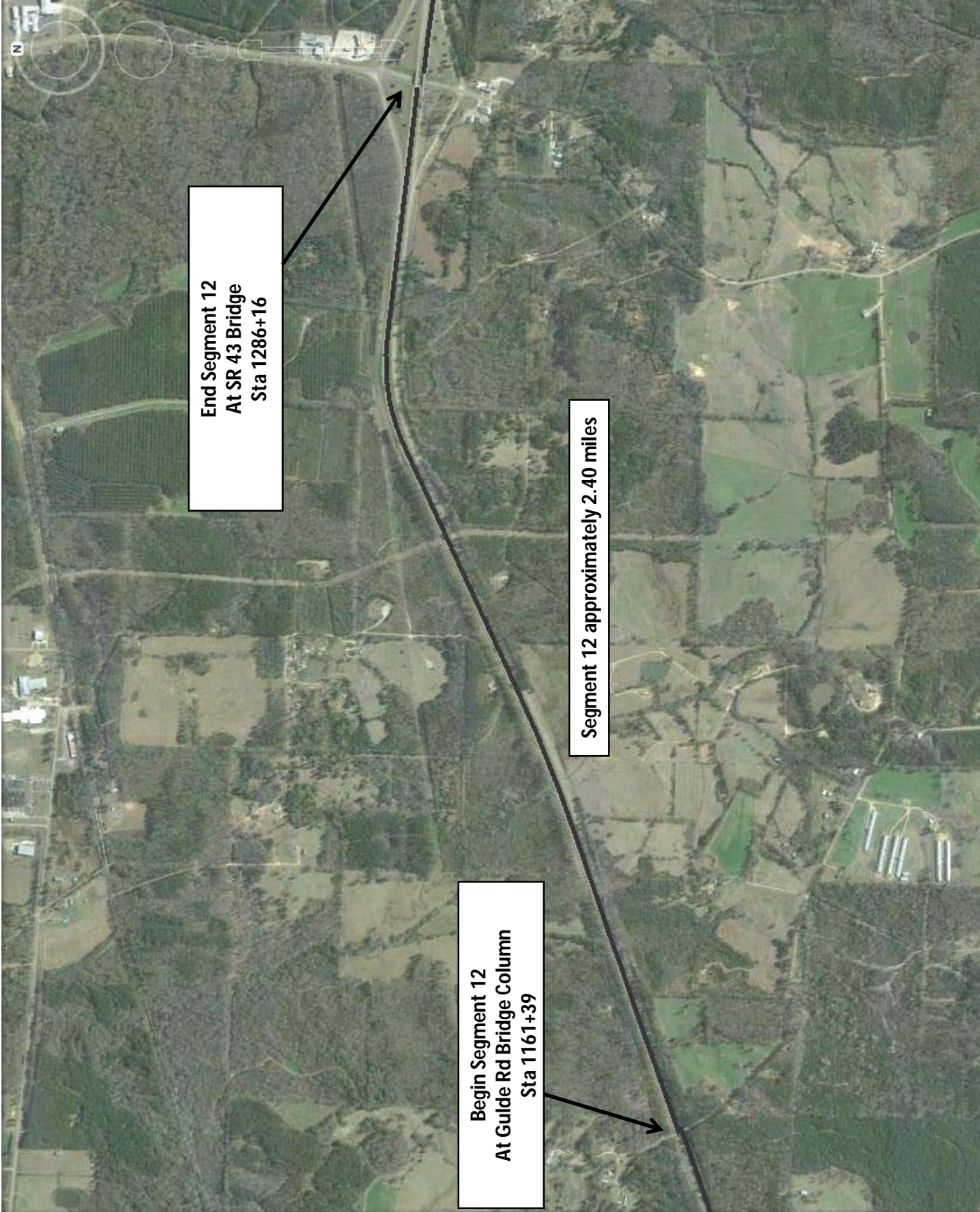
**Begin Segment 11
At Paved Ditch approximately .29 miles
West of Gulde Rd Bridge Column
Sta 1146+00**

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Gulde-Shilo Rd.
Rankin County



Gulde Rd Bridge Column
Sta 1161+39

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



End Segment 12
At SR 43 Bridge
Sta 1286+16

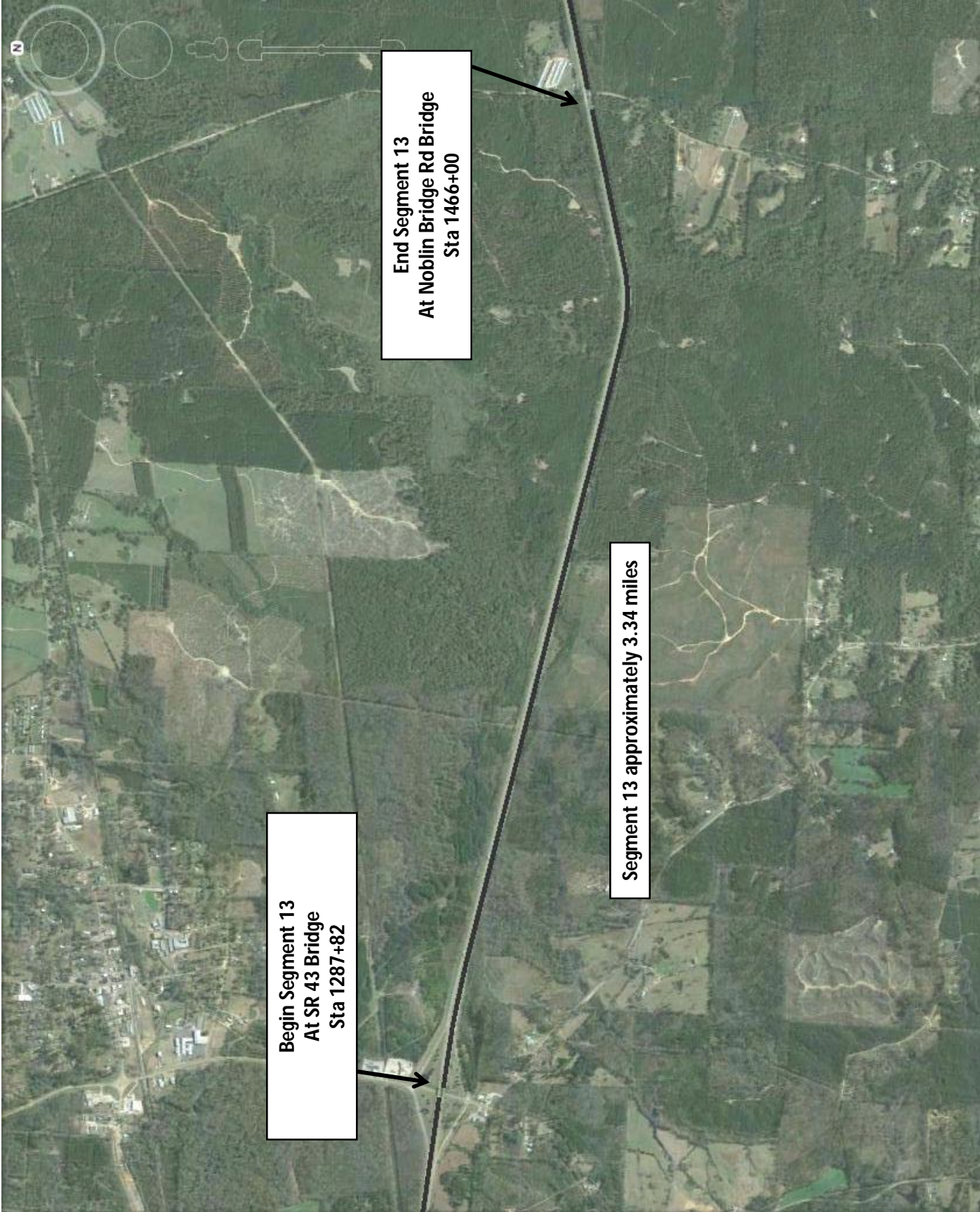
Segment 12 approximately 2.40 miles

Begin Segment 12
At Guide Rd Bridge Column
Sta 1161+39

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



**Begin Segment 13
At SR 43 Bridge
Sta 1287+82**

**End Segment 13
At Noblin Bridge Rd Bridge
Sta 1466+00**

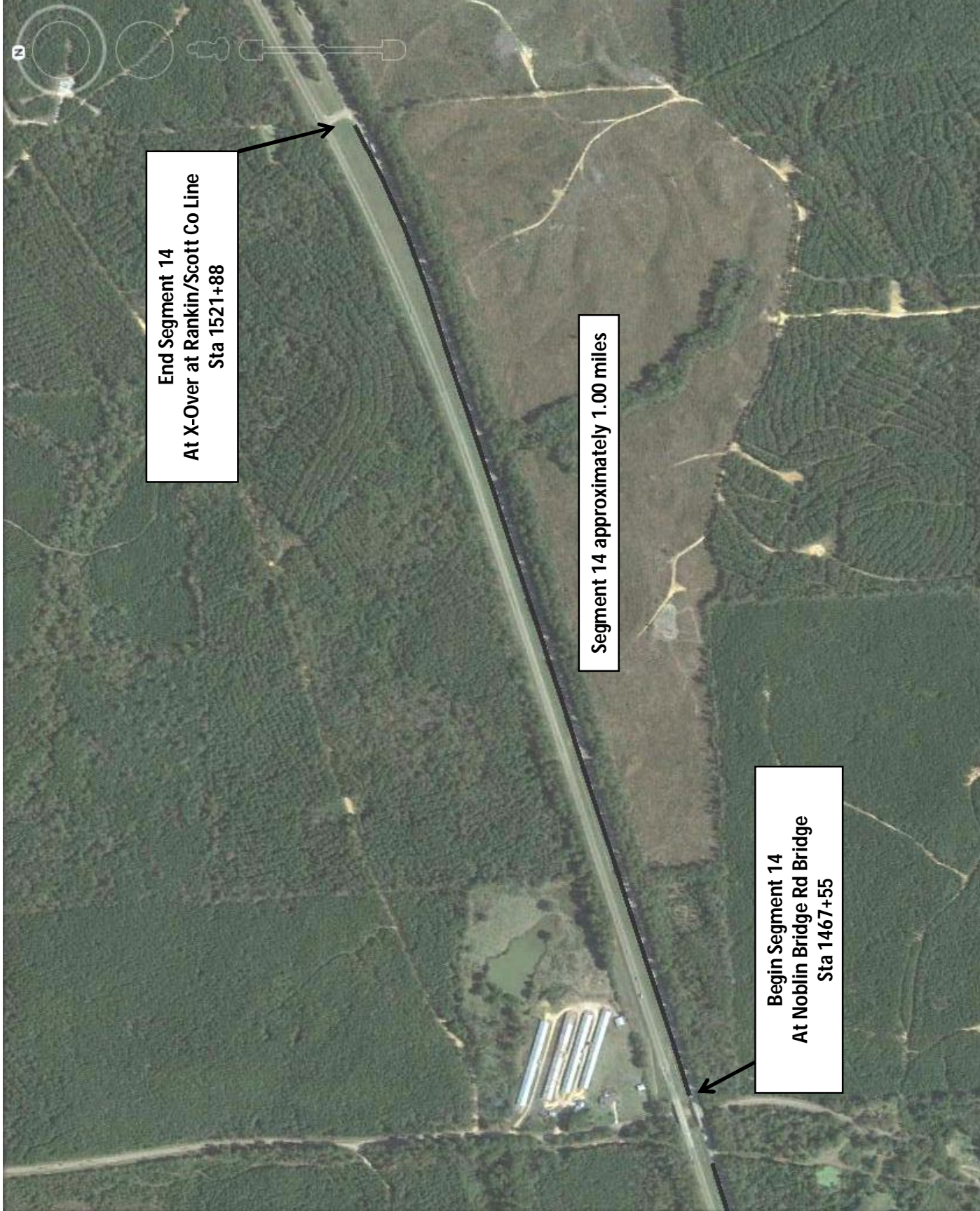
Segment 13 approximately 3.34 miles

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County



Noblin Bridge Rd Bridge
Western Bridge End Sta 1466+00
Eastern Bridge End Sta 1467+55

Cable Rail Location Detail
I-20 from Crossgates Blvd. to 1 Mile West of Guide-Shilo Rd.
Rankin County

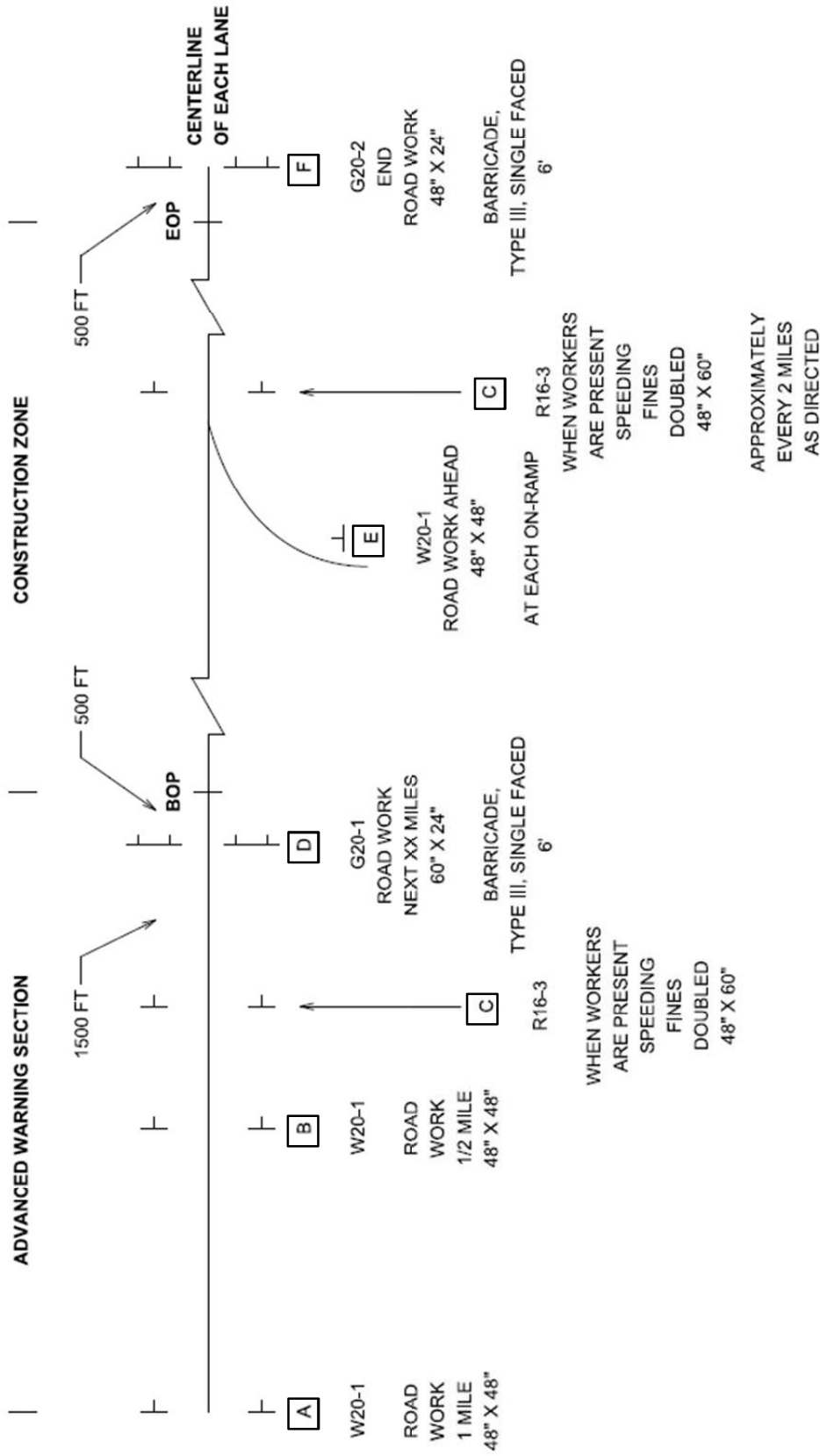


**End Segment 14
At X-Over at Rankin/Scott Co Line
Sta 1521+88**

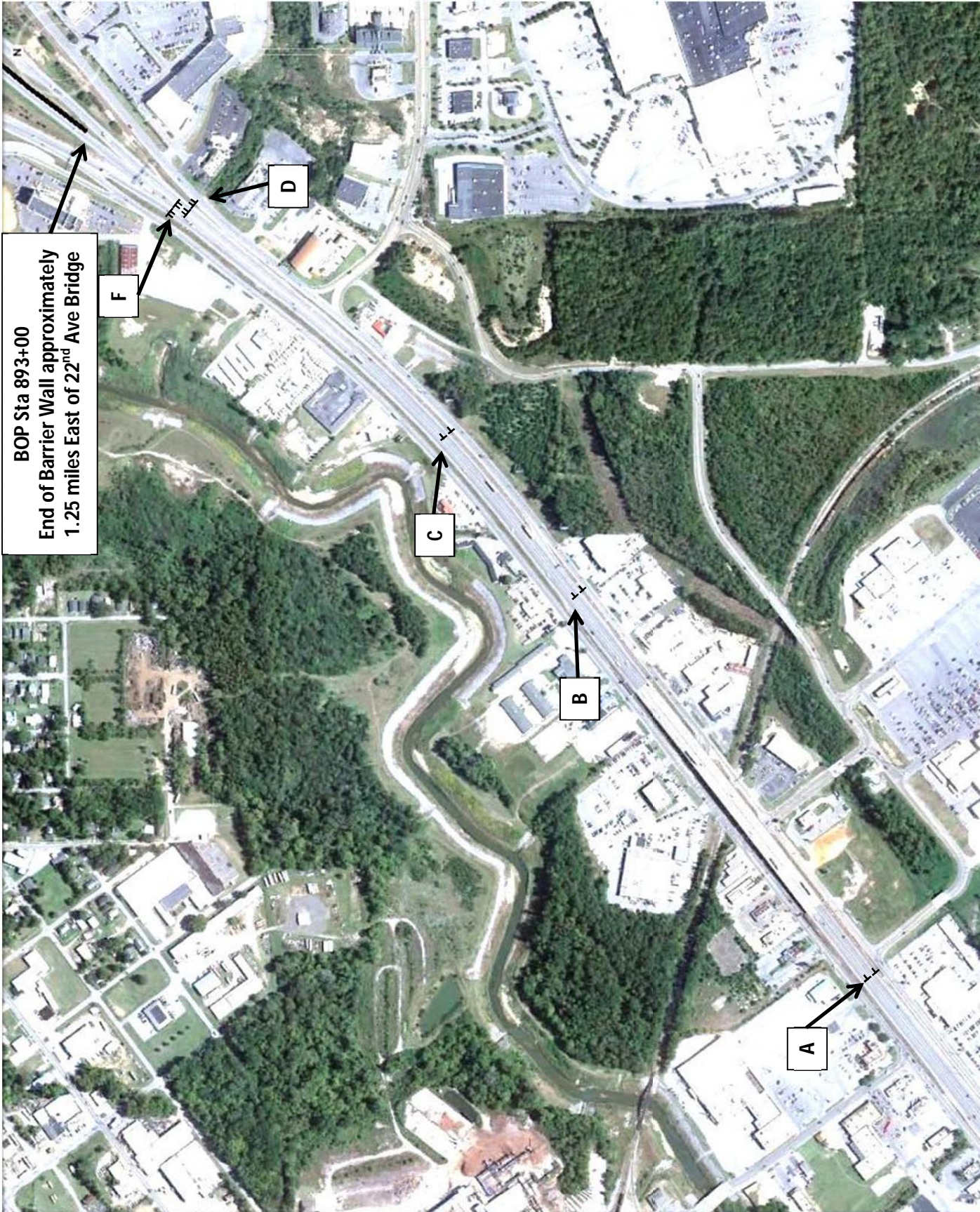
Segment 14 approximately 1.00 miles

**Begin Segment 14
At Noblin Bridge Rd Bridge
Sta 1467+55**

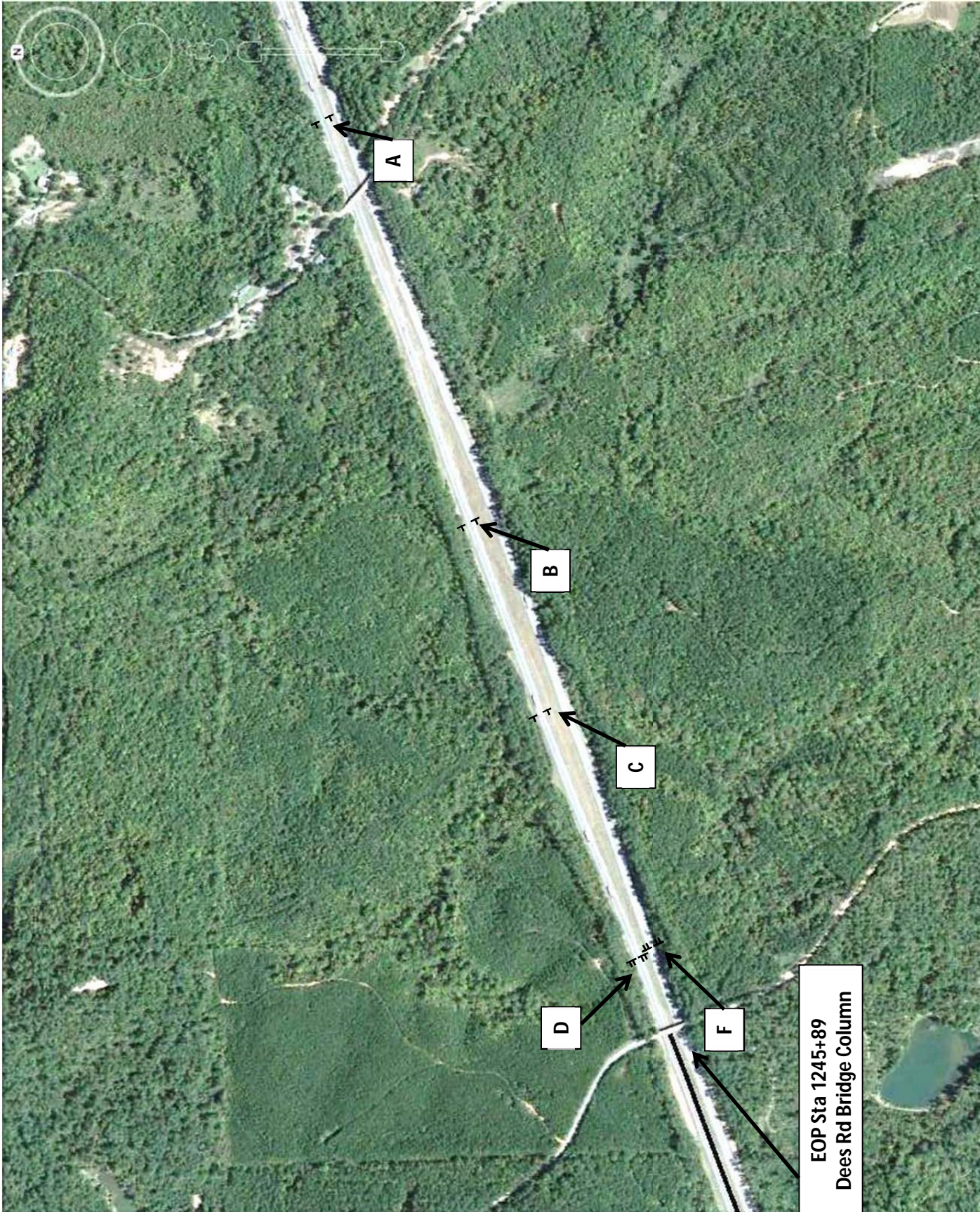
Construction Signing Detail
 I-20/I-59 from SR 19/SR 39 Interchange to Dees Rd.
 Lauderdale County



Construction Signing Detail
I-20/I-59 from SR 19/SR 39 Interchange to Dees Rd.
Lauderdale County

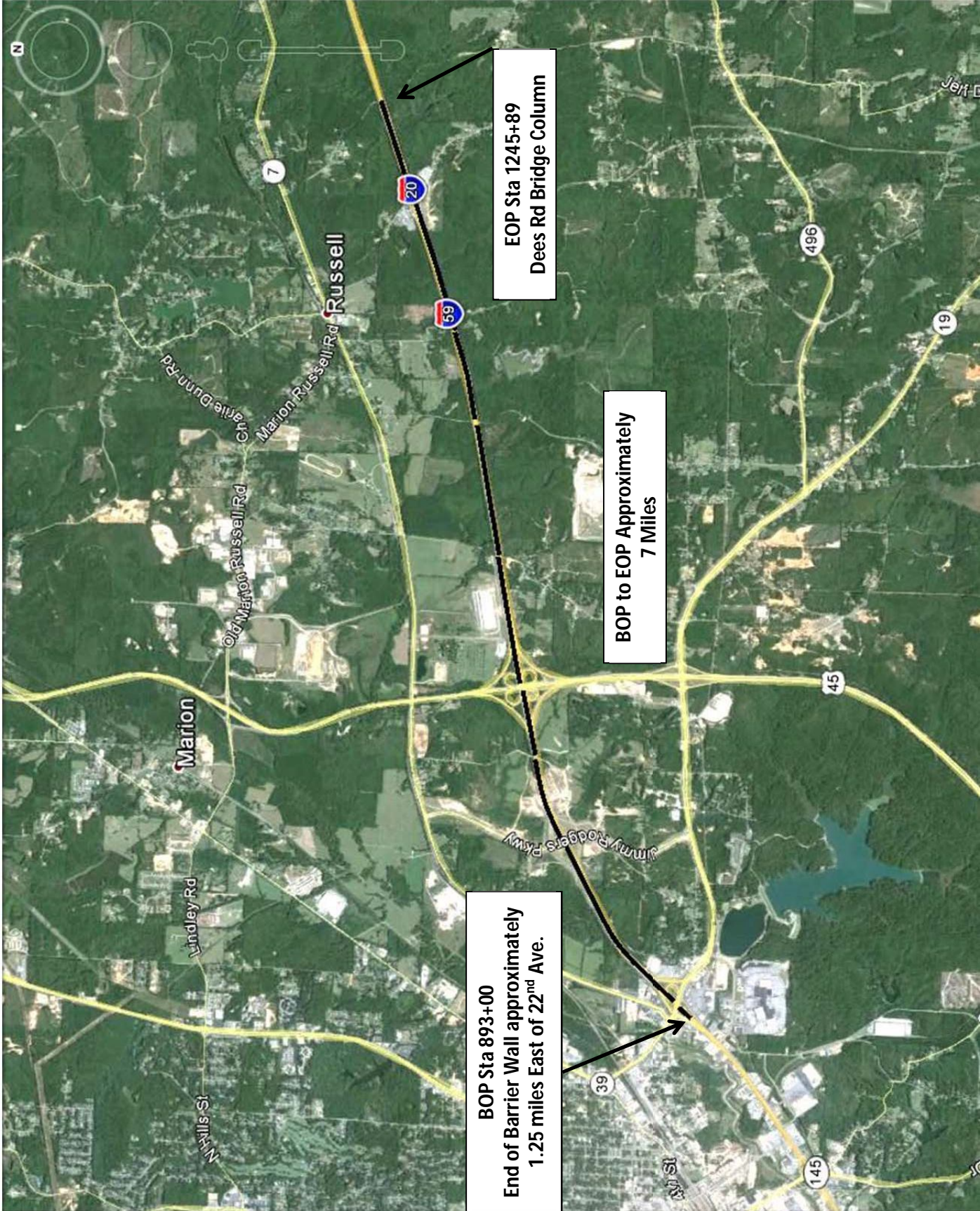


Construction Signing Detail
I-20/I-59 from SR 19/SR 39 Interchange to Dees Rd.
Lauderdale County



EOP Sta 1245+89
Dees Rd Bridge Column

Cable Rail Location Detail
I-20/I-59 from SR 19/SR 39 Interchange to Dees Rd.
Lauderdale County



BOP Sta 893+00
End of Barrier Wall approximately
1.25 miles East of 22nd Ave.

BOP to EOP Approximately
7 Miles

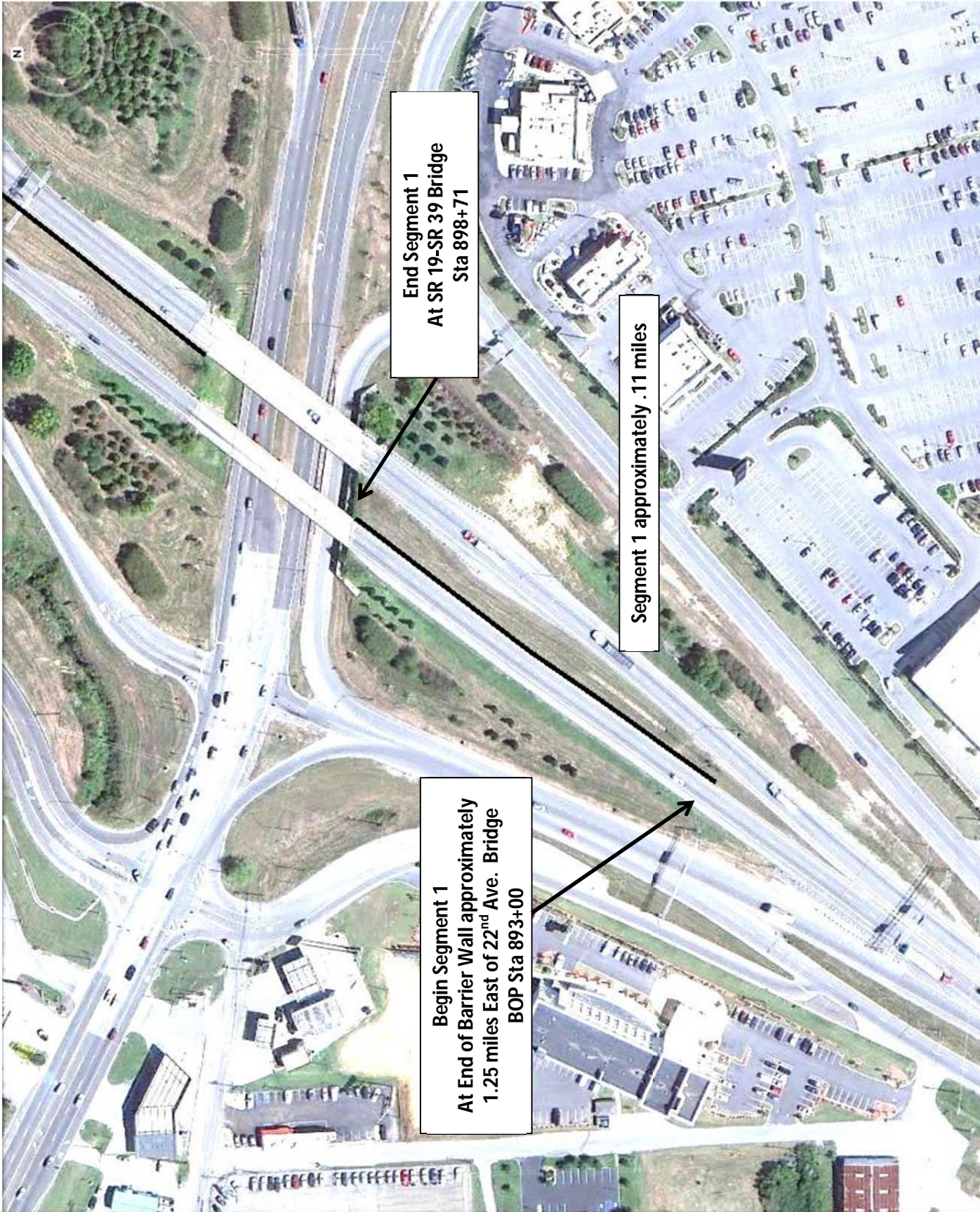
EOP Sta 1245+89
Dees Rd Bridge Column

Cable Rail Location Detail
I-20/I-59 from SR 19/SR 39 Interchange to Dees Rd.
Lauderdale County



End of Barrier Wall approximately 1.25
miles East of 22nd Ave. Bridge
BOP Sta 893+00

Cable Rail Location Detail
I-20/I-59 from SR 19/SR 39 Interchange to Dees Rd.
Lauderdale County

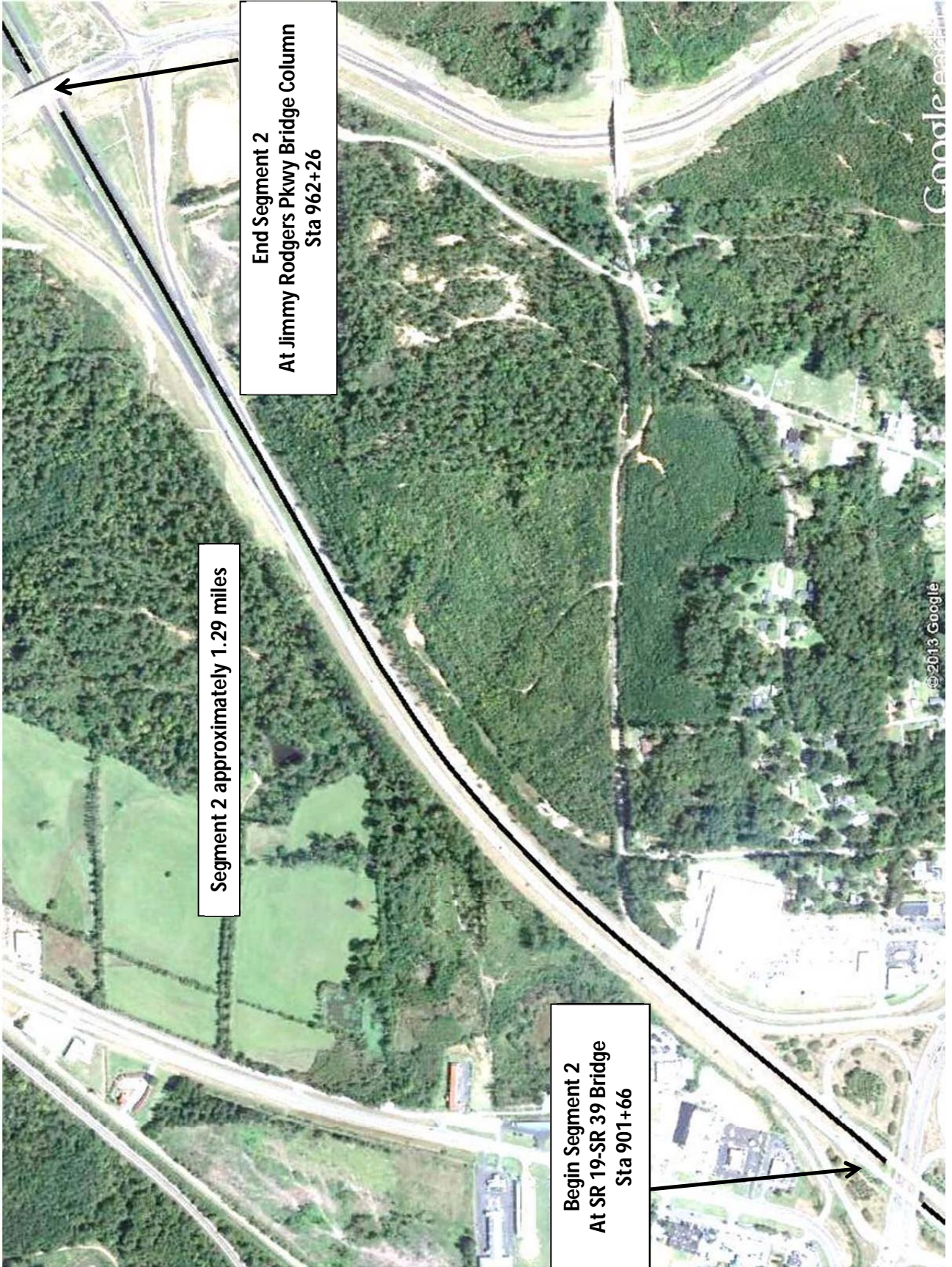


Begin Segment 1
At End of Barrier Wall approximately
1.25 miles East of 22nd Ave. Bridge
BOP Sta 893+00

End Segment 1
At SR 19-SR 39 Bridge
Sta 898+71

Segment 1 approximately .11 miles

Cable Rail Location Detail
I-20/I-59 from SR 19/SR 39 Interchange to Dees Rd.
Lauderdale County

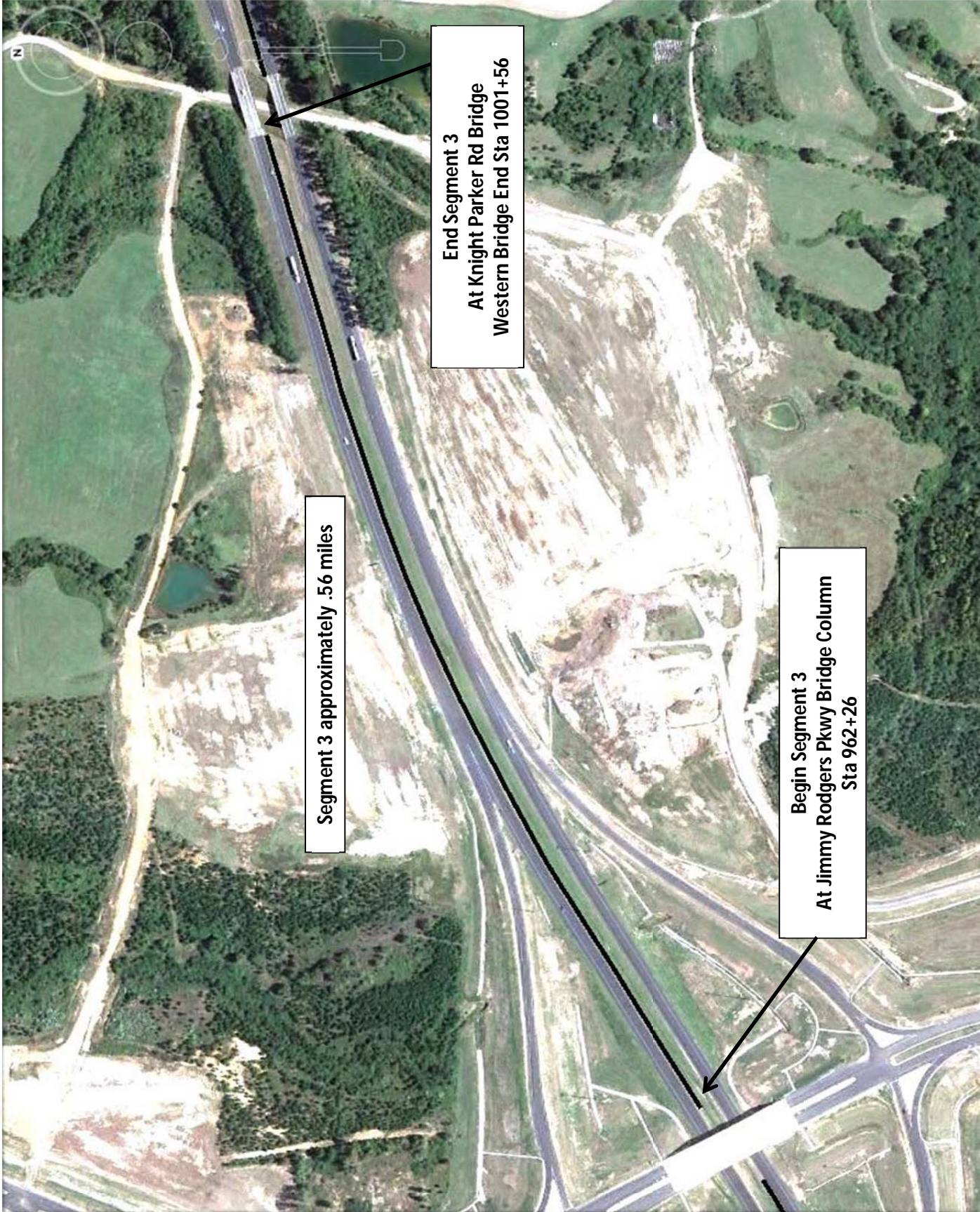


Segment 2 approximately 1.29 miles

End Segment 2
At Jimmy Rodgers Pkwy Bridge Column
Sta 962+26

Begin Segment 2
At SR 19-SR 39 Bridge
Sta 901+66

Cable Rail Location Detail
I-20/I-59 from SR 19/SR 39 Interchange to Dees Rd.
Lauderdale County

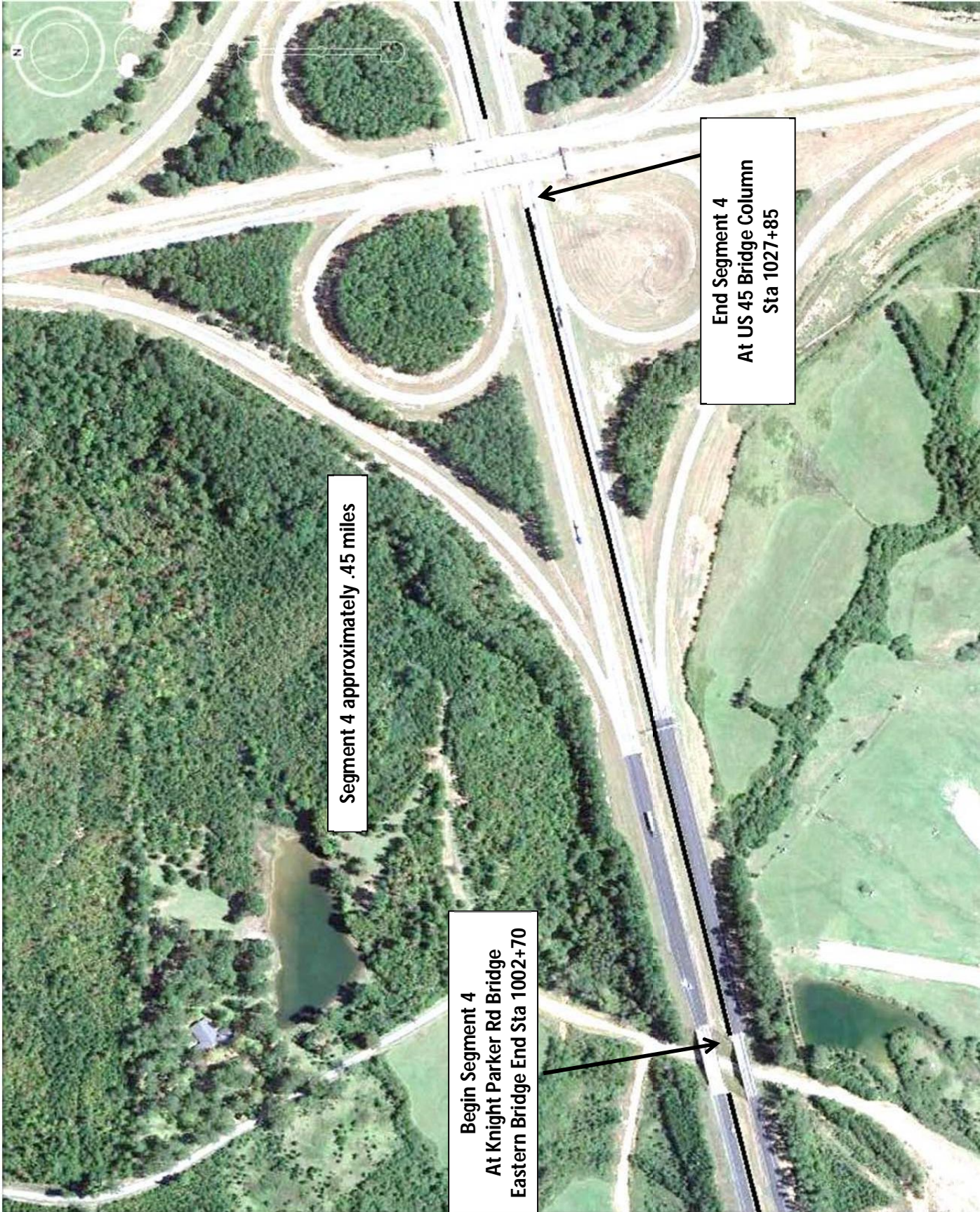


Segment 3 approximately .56 miles

End Segment 3
At Knight Parker Rd Bridge
Western Bridge End Sta 1001+56

Begin Segment 3
At Jimmy Rodgers Pkwy Bridge Column
Sta 962+26

Cable Rail Location Detail
I-20/I-59 from SR 19/SR 39 Interchange to Dees Rd.
Lauderdale County

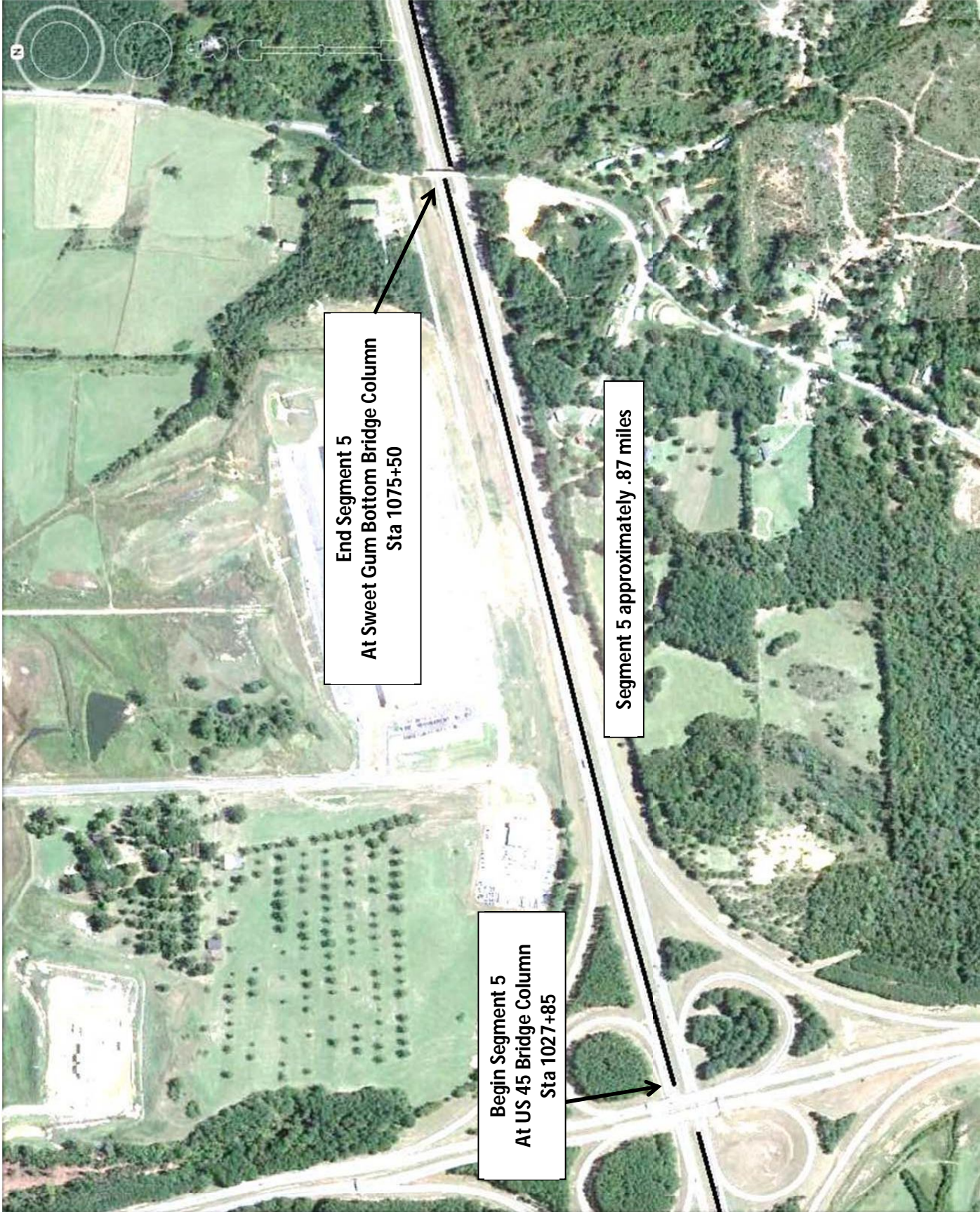


Segment 4 approximately .45 miles

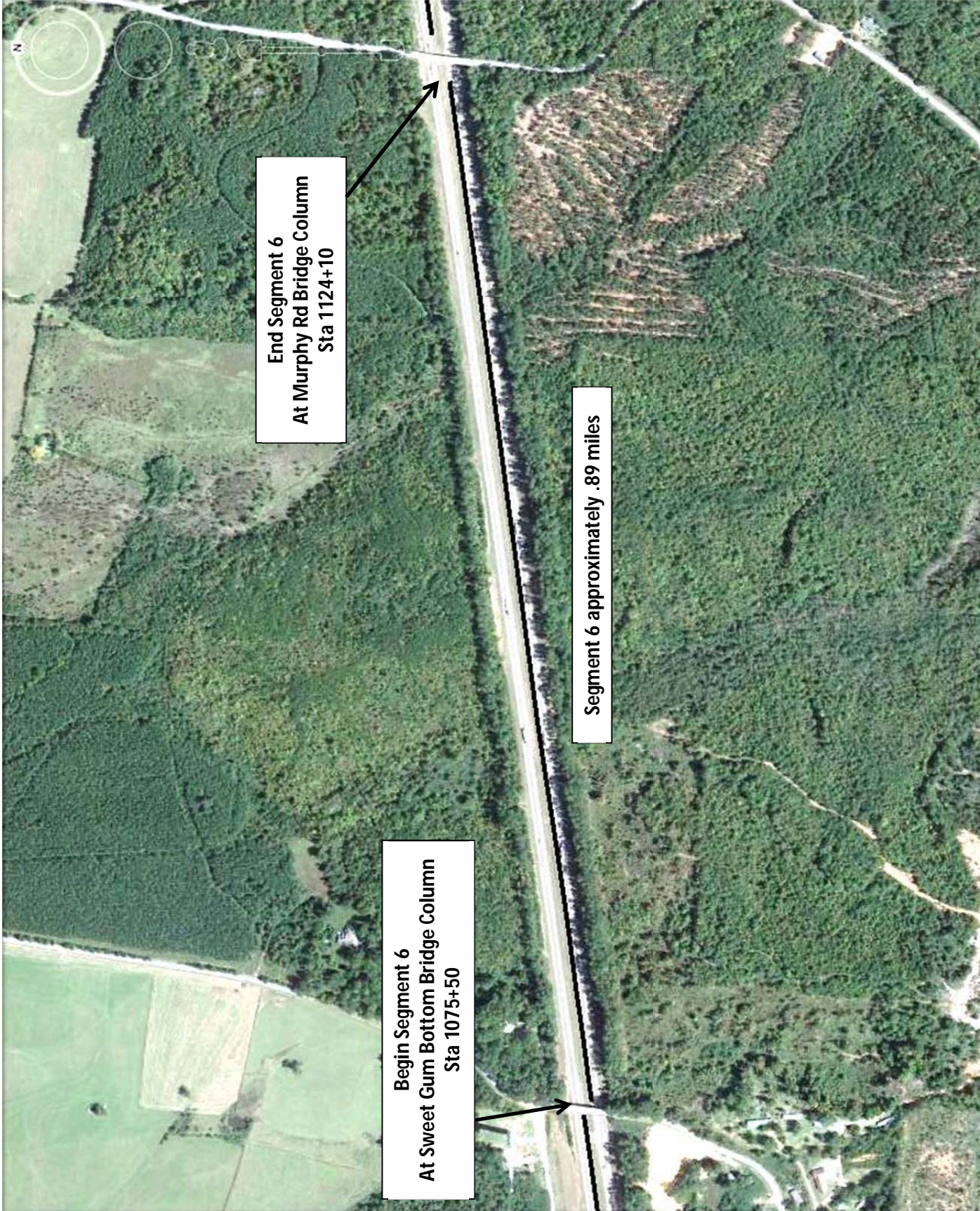
Begin Segment 4
At Knight Parker Rd Bridge
Eastern Bridge End Sta 1002+70

End Segment 4
At US 45 Bridge Column
Sta 1027+85

Cable Rail Location Detail
I-20/I-59 from SR 19/SR 39 Interchange to Dees Rd.
Lauderdale County



Cable Rail Location Detail
I-20/I-59 from SR 19/SR 39 Interchange to Dees Rd.
Lauderdale County

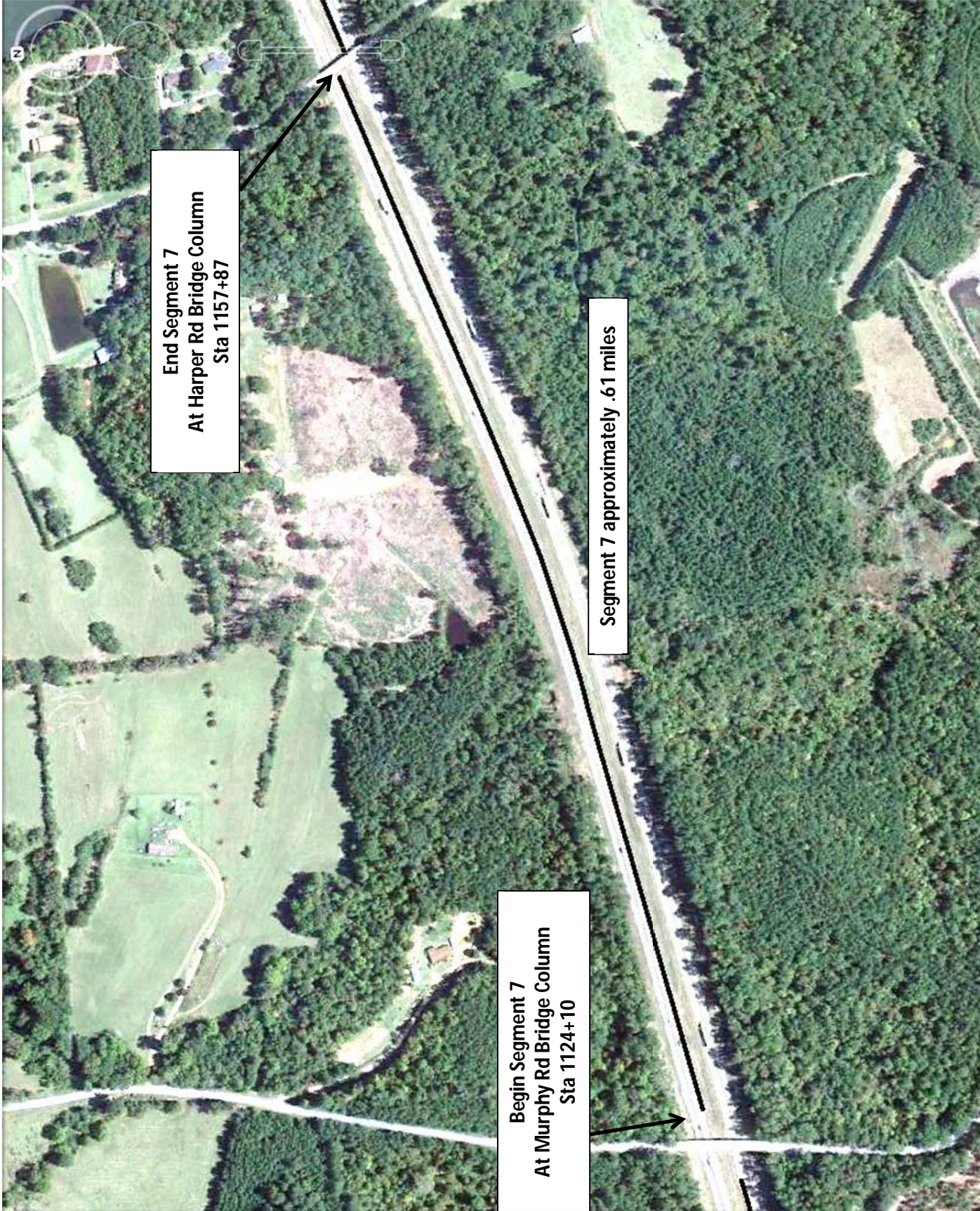


**Begin Segment 6
At Sweet Gum Bottom Bridge Column
Sta 1075+50**

**End Segment 6
At Murphy Rd Bridge Column
Sta 1124+10**

Segment 6 approximately .89 miles

Cable Rail Location Detail
I-20/I-59 from SR 19/SR 39 Interchange to Dees Rd.
Lauderdale County

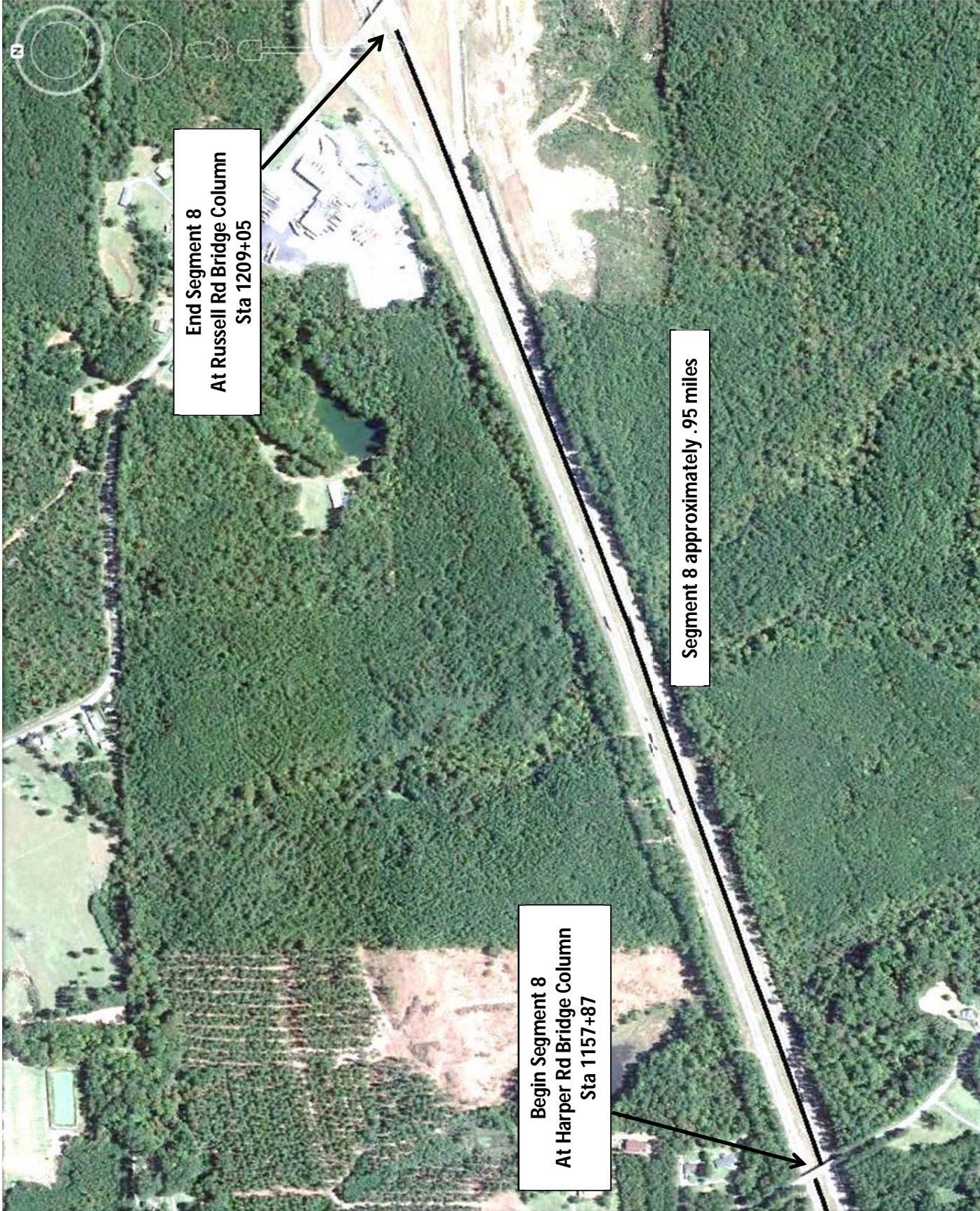


End Segment 7
At Harper Rd Bridge Column
Sta 1157+87

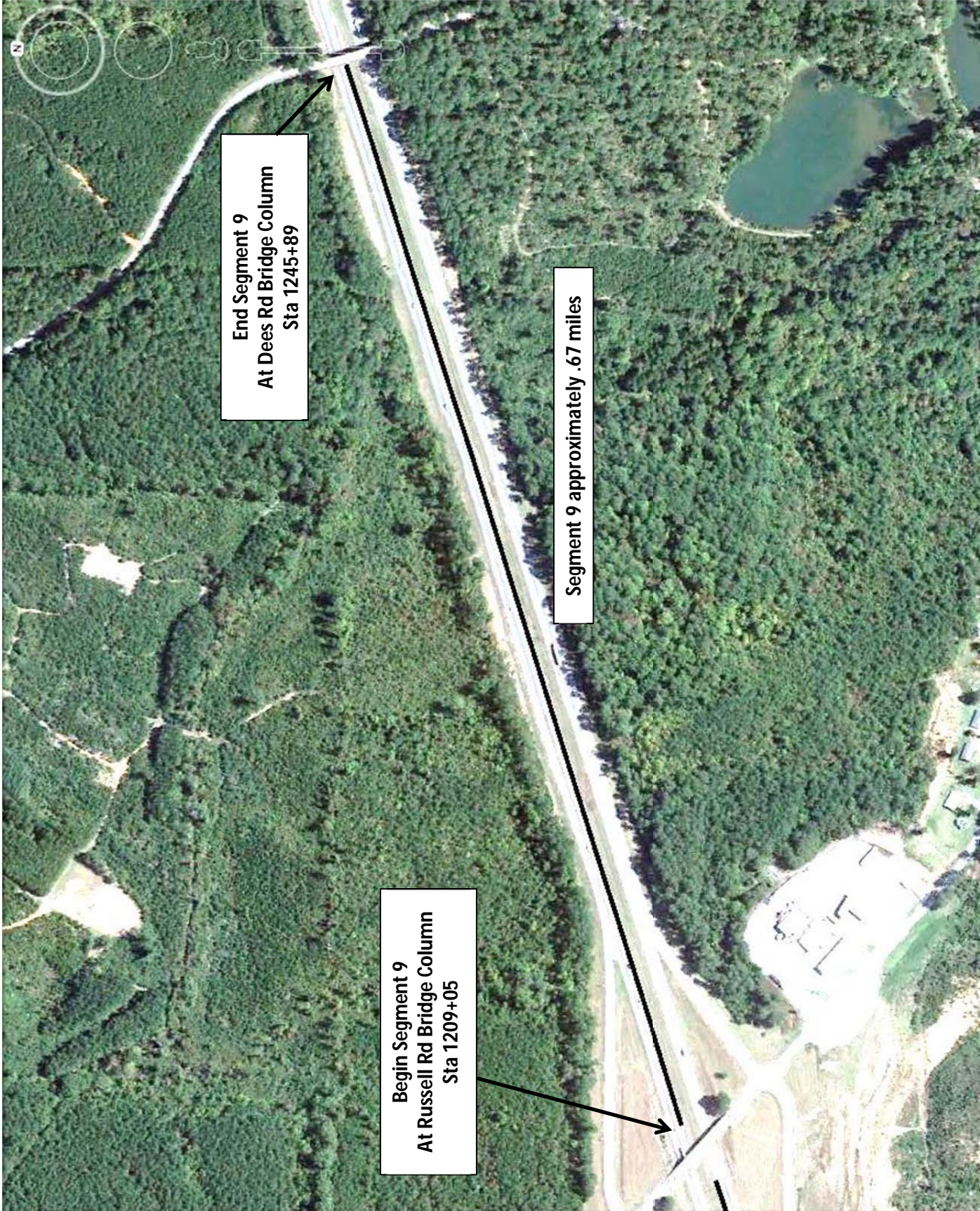
Begin Segment 7
At Murphy Rd Bridge Column
Sta 1124+10

Segment 7 approximately .61 miles

Cable Rail Location Detail
I-20/I-59 from SR 19/SR 39 Interchange to Dees Rd.
Lauderdale County



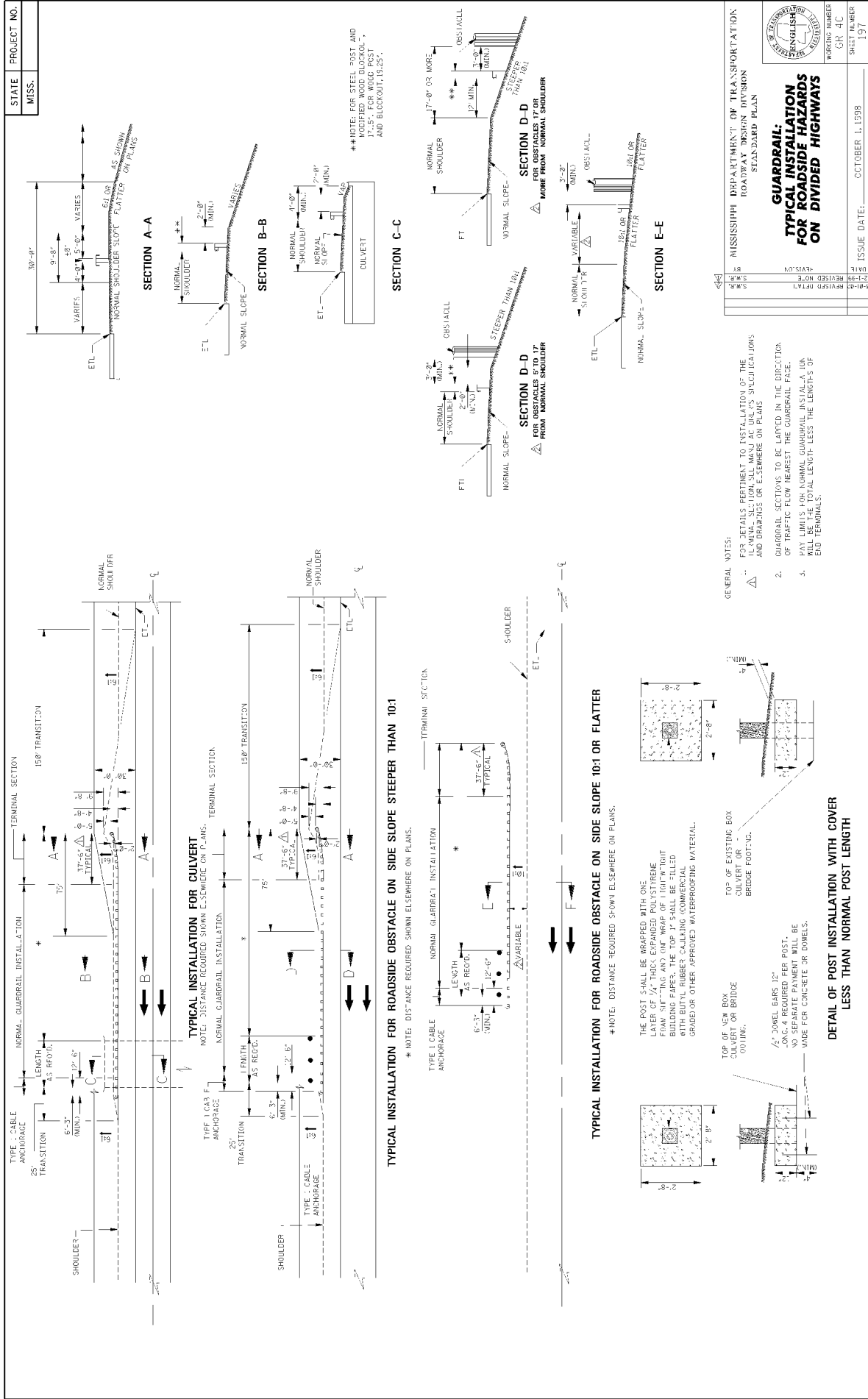
Cable Rail Location Detail
I-20/I-59 from SR 19/SR 39 Interchange to Dees Rd.
Lauderdale County

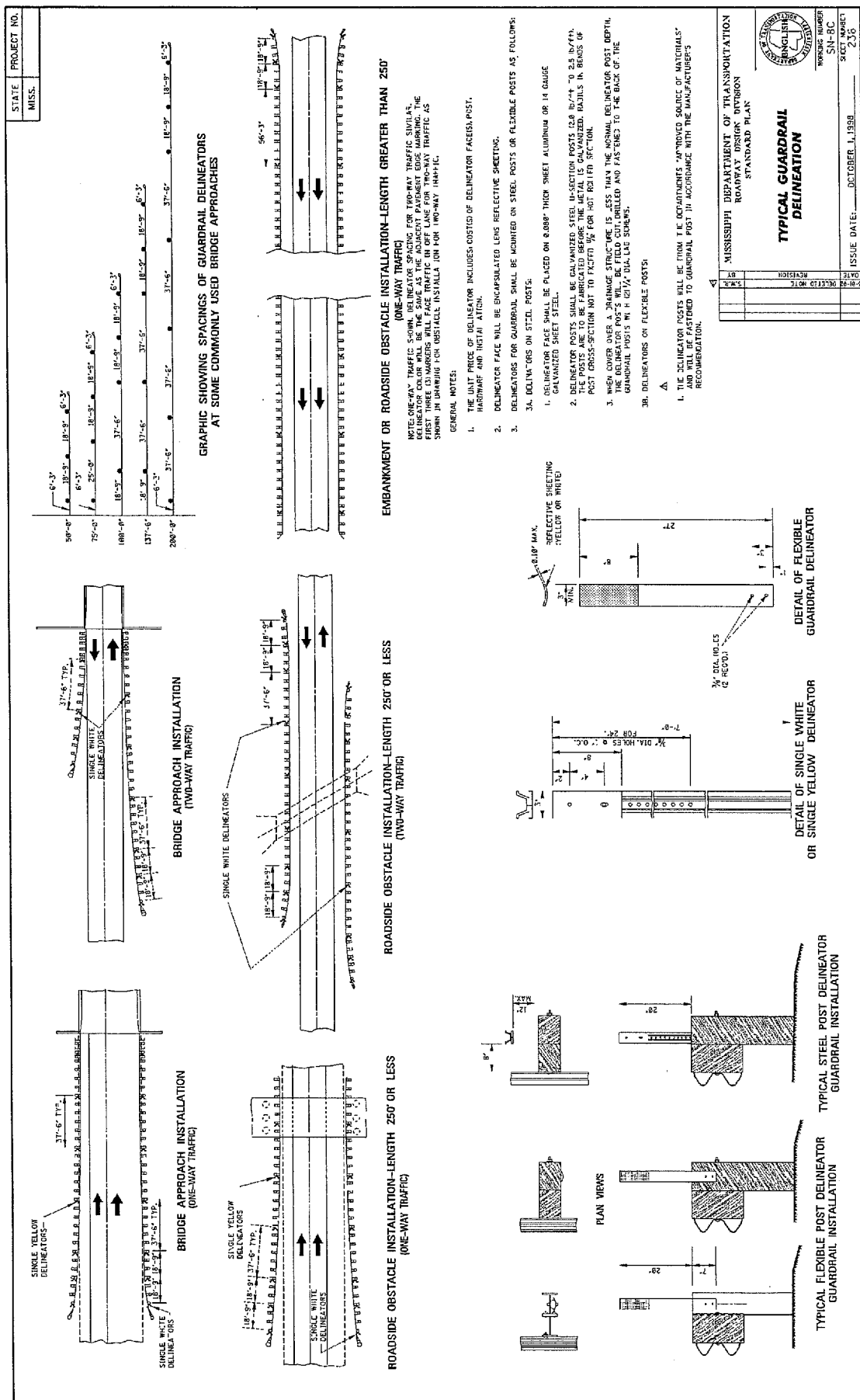


**End Segment 9
At Dees Rd Bridge Column
Sta 1245+89**

Segment 9 approximately .67 miles

**Begin Segment 9
At Russell Rd Bridge Column
Sta 1209+05**





MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 5050

CODE: (SP)

DATE: 05/28/2014

SUBJECT: Adjustments for Bituminous Materials

Bidders are advised that Subsection 907-402.03.1.2, Tack Coat, in Special Provision 907-402, allows the Contractor several options for OGFC tack coat. Regardless of the tack coat used, the monthly material adjustment, as referenced in Section 109 of the Standard Specifications, will be made using the base and current prices of tack coat Grade PG 76-22.

Bidders are also advised that the specifications allow the use of RC-70, RC-250, RC-800, RS-1, RS-2, MC-30, MC-250, MS-2h, LD-7, CQS-1h, and CMS-2h in various other construction operations. If the Contractor uses one of these bituminous materials, the monthly material adjustment will be made using the base and current prices of the materials shown below.

Materials Used	Material Adjustment Made Based on Prices For
RC-70, 250, 800	MC-70
RS-1, 2	CRS-2
MC-30, 250	MC-70
MS-2h	SS-1
LD-7, CQS-1h	CSS-1
CMS-2h	SS-1

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 5053

CODE: (SP)

DATE: 06/03/2014

SUBJECT: Contractor Correspondence

Bidders are advised that all correspondence concerning this project, other than correspondence related to the execution of the contract and sub-contracting, shall be sent to the Project Engineer. The Project Engineer will then forward any necessary correspondence to the appropriate Division. This includes general correspondence, submittals, shop drawings, requests for advancement of materials, etc.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 5080

CODE: (SP)

DATE: 06/10/2014

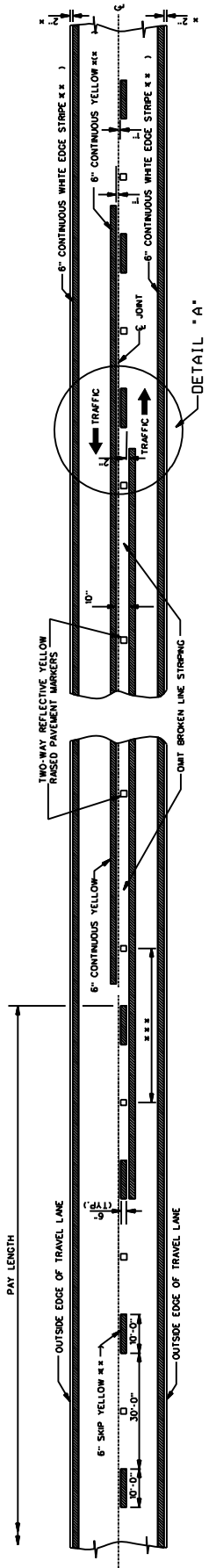
SUBJECT: Standard Drawings

Standard Drawings attached hereto shall govern appropriate items of required work.

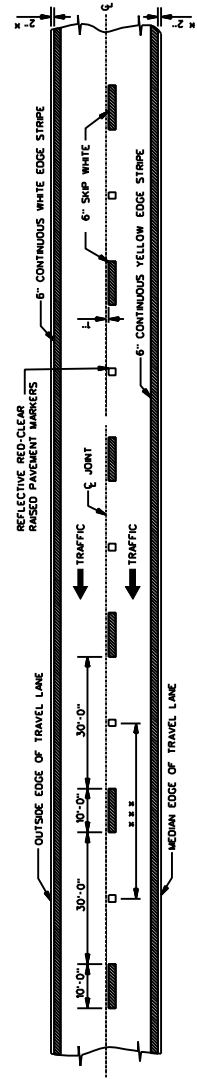
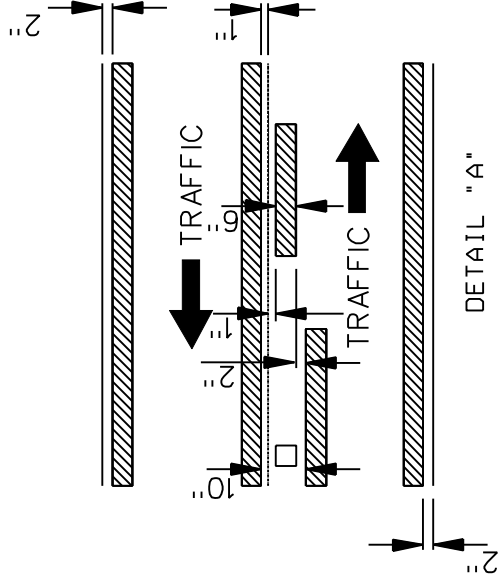
Larger copies of Standard Drawings may be purchased from:

MDOT Plans Print Shop
MDOT Shop Complex, Building C, Room 114
2567 North West Street
P.O. Box 1850
Jackson, MS 39215-1850
Telephone: (601) 359-7460
or FAX: (601) 359-7461
or e-mail: plans@mdot.state.ms.us

STATE	PROJECT NO.
MISS.	



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



4-LANE WITH ONE-WAY TRAFFIC



GENERAL NOTES:

- * 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
- *** 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	URBAN AREA	RURAL AREA
(11-in)	40'-0"	(11-in)
HORIZONTAL CURVES	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	40'-0"
		1-40'-0"

1. NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40' SPACING WILL BE REQUIRED ON PAVEMENT SURFACES AT ALL INTERCHANGES. RAISED PAVEMENT MARKERS SHALL BE REQUIRED ON PAVEMENT SURFACES AT ALL INTERCHANGES TO BE INSTALLED 1000' IN ADVANCE IN DIRECTION OF TRAFFIC OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

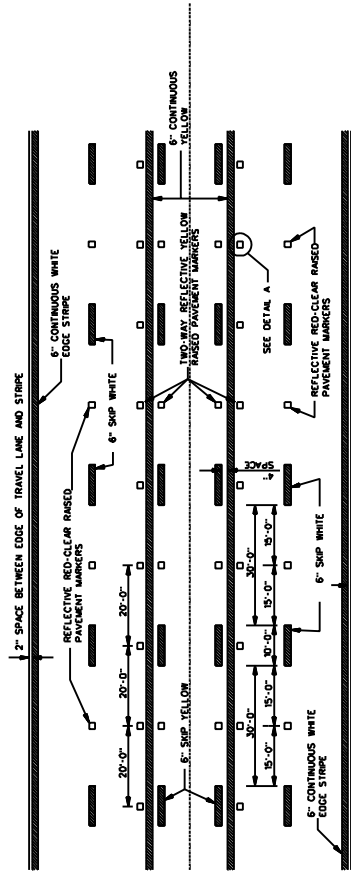
4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOST APPROVED SOURCES OF MATERIALS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

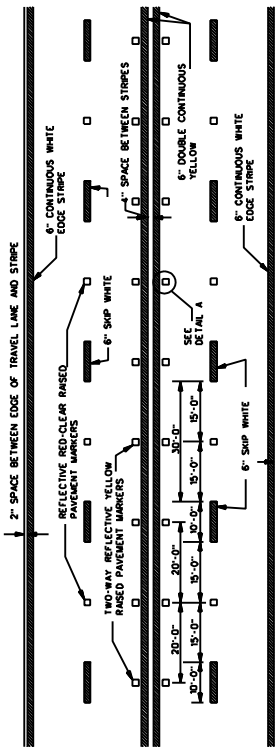
PAVEMENT MARKING
DETAILS FOR
2-LANE AND 4-LANE
DIVIDED HIGHWAYS

DATE: _____
REVISION: _____
DRAWN BY: _____
CHECKED BY: _____
ISSUE DATE: OCTOBER 1, 1998
WORKING NUMBER: PM-1
SHEET NUMBER: 120

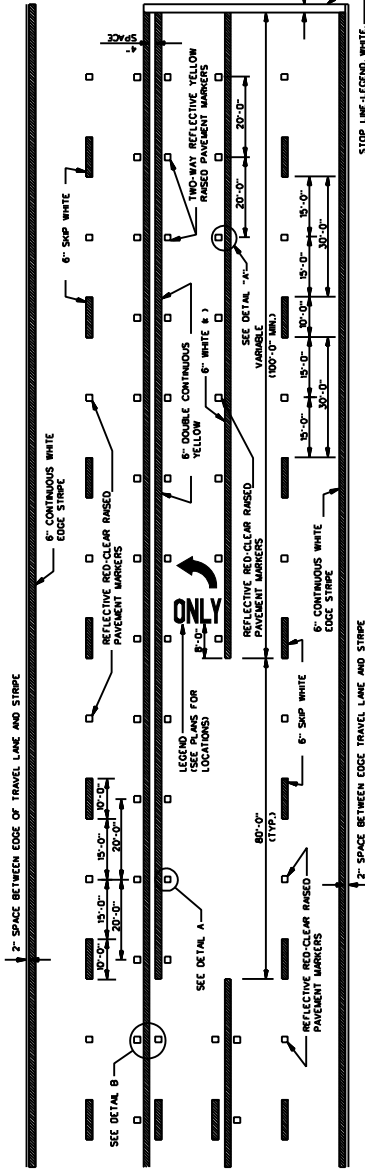
STATE	PROJECT NO.
MISS.	



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 5-LANE SECTION



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION



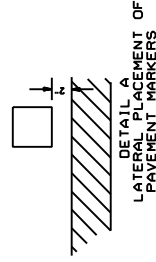
TYPICAL STRIPING AND RAISED PAVEMENT MARKERS AT LEFT TURN LANES

*NOTE: USE DETAIL STRIPING # LENGTH 5' SPD AT THIS LOCATION, OTHERWISE USE CONTINUOUS STRIPING.



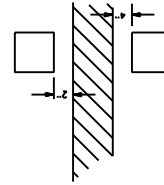
TYPICAL TWO-WAY ARROW INSTALLATION

- NOTES: 1. CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY.
- 2. IF SEGMENT IS LESS THAN 350', PLACE ONE SET OF ARROWS IN CENTER OF SEGMENT.
- 3. IF SEGMENT IS GREATER THAN 350', PLACE FIRST SET OF ARROWS 50' TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS 1250' O.C.



DETAIL A
LATERAL PLACEMENT OF PAVEMENT MARKERS

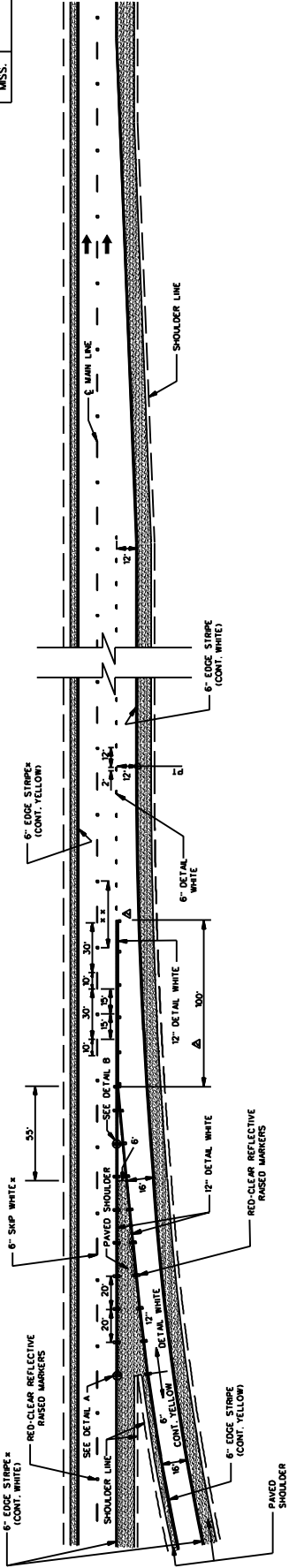
GENERAL NOTE:
1. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MOST APPROVED SOURCE OF MATERIALS.



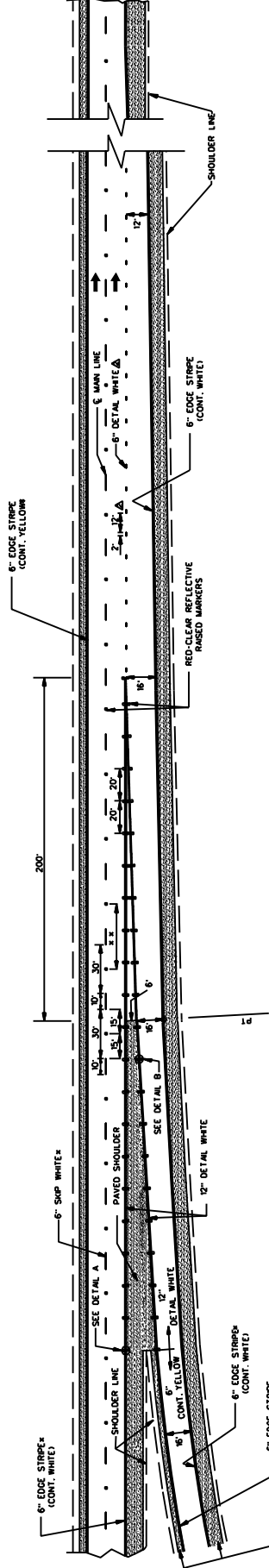
DETAIL B
LATERAL PLACEMENT OF PAVEMENT MARKERS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
PAVEMENT MARKING	
DETAILS FOR	
4-LANE AND 5-LANE	
UNDIVIDED ROADWAYS	
DATE	2/20/18
DESIGNER	SDPM
CHECKER	SDPM
FILE NAME	SDPM-2.DGN
DESIGN TITLE	CRETE
PROJECT NUMBER	5080
SHEET NUMBER	3

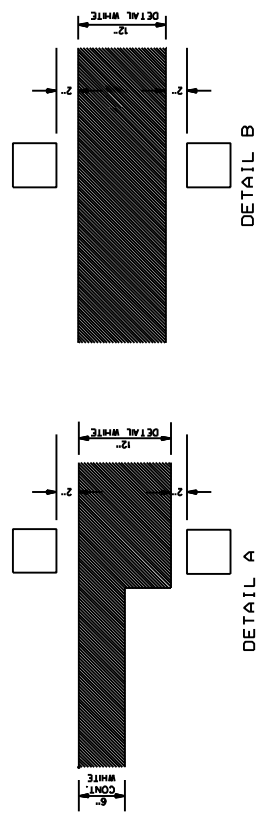
STATE	PROJECT NO.
MISS.	



PARALLEL ENTRANCE RAMP



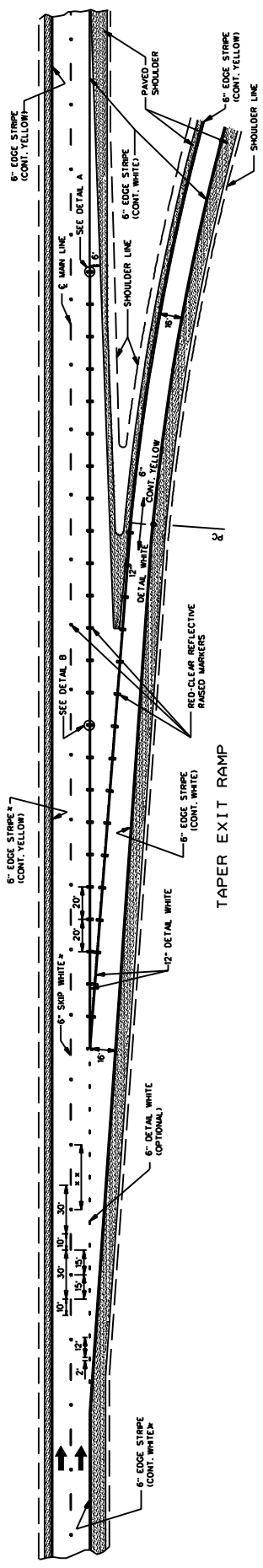
TAPER ENTRANCE RAMP



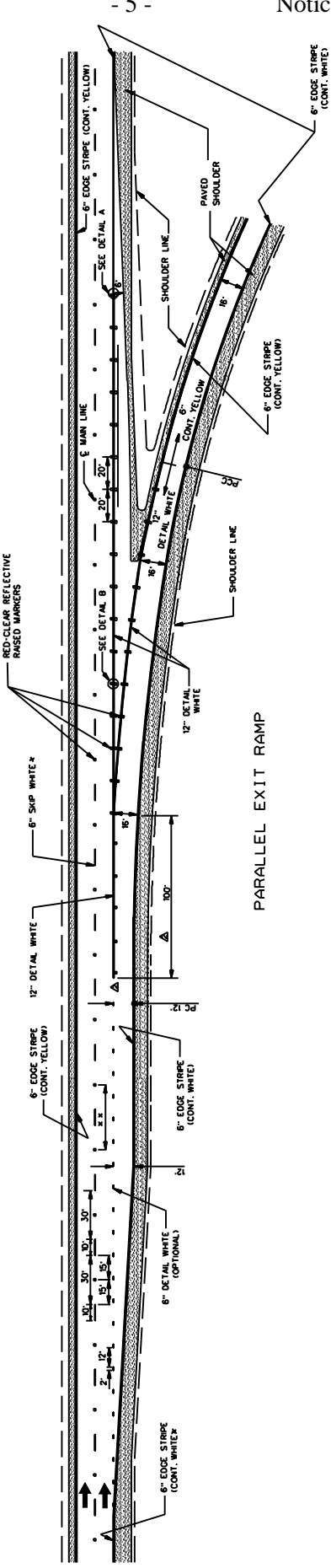
- GENERAL NOTES:
1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPES THROUGHOUT THE INTERCHANGE AREA BEGINNING 100' IN THE MAIN FACILITY AND THROUGHOUT THE INTERCHANGE TAPER AND CONTRAIL THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 2. ON THE MAIN FACILITY, PLACE REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS AT A 40' SPACING ON ALL LANE-LINES THROUGHOUT THE INTERCHANGE AREA BEGINNING 100' IN THE MAIN FACILITY AND THROUGHOUT THE INTERCHANGE TAPER AND CONTRAIL THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED SOURCES OF MATERIALS."

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION	
PAVEMENT MARKING DETAILS FOR INTERCHANGE ENTRANCE RAMP (PARALLEL AND TAPER)	
ISSUE DATE:	OCTOBER 1, 1988
WORKING NUMBER	SDPM-3
SHEET NUMBER	

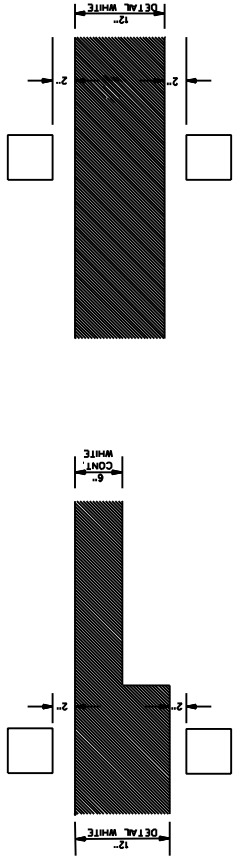
STATE	PROJECT NO.
MISS.	



TAPER EXIT RAMP



PARALLEL EXIT RAMP



- GENERAL NOTES:
- SEE SHEET PM-1 FOR THE PLACEMENT OF LINE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELED WAY.
 - PAVEMENT MARKING SHALL BE HIGH PERFORMANCE REFLECTIVE RED CLEAR BASED THROUGHOUT THE INTERCHANGE AREA BEGINNING 1000' IN ADVANCE IN DIRECTION OF TRAFFIC OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED MARKERS AS SHOWN AND LISTED IN THE MOOT "APPROVED SOURCES OF MATERIALS."

DATE	BY	REVISION
10/1/99	SM	UPDATE TO 2008 M/C/D
	SM	REVIS STRIPE WIDTH
	SM	REVISION

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION

PAVEMENT MARKING
DETAILS FOR
INTERCHANGE
EXIT RAMPS
(PARALLEL AND TAPER)

ISSUE DATE: OCTOBER 1, 1999

WORKING NUMBER: SDPM-41

SHEET NUMBER: _____

STATE PROJECT NO.
MISS.

STATE PROJECT NO.
MISS.

8'-4" x 6'-4"

8'-4" x 6'-4"

8'-4" x 6'-4"

8'-4" x 6'-4"

8'-4" x 6'-4"

GENERAL NOTES:

- UNLESS OTHERWISE SHOWN ON THE PLANS, ALL PAVEMENT MARKING LEGENDS SHALL BE CONSTRUCTED USING HIGH PERFORMANCE MATERIALS.
- TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTORS) OF 1/2" FULL WIDTH ARE PERMITTED IN EACH LETTER.
- FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

LEGEND	AREA (SF)
STOP	26.8
RIGHT	26.8
LEFT	19.5
TURN	27.3
ALWAYS	27.3
YIELD	26.8
EXIT	18.5
SIGNAL	32.5
SCHOOL	26.5

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
LEGEND DETAILS**

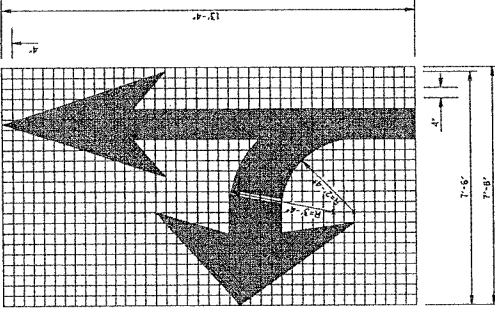
ISSUE DATE: OCTOBER 1, 1990

DATE

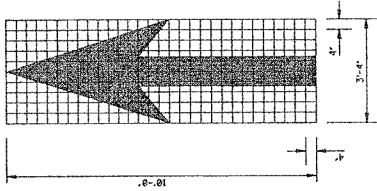
SHEET NUMBER

124

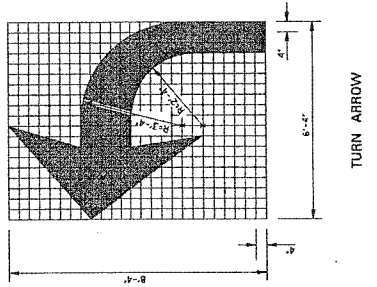
STATE PROJECT NO.
MISS.



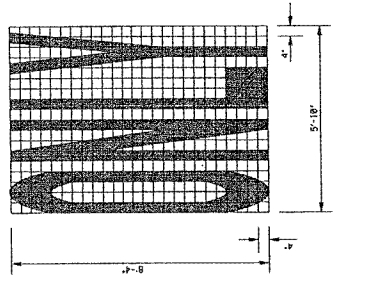
COMBINATION ARROW



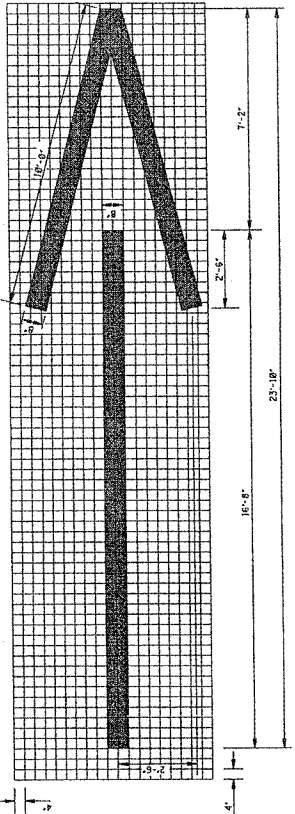
THRU ARROW



TURN ARROW



1-WAY ARROW



GENERAL NOTES:

1. UNLESS OTHERWISE SHOWN ON THE PLANS, ALL PAVEMENT MARKING LEGENDS SHALL BE APPLIED USING HIGH PERFORMANCE MATERIALS.
2. TWO HORIZONTAL GAPS CAUSED BY TEMPLATE CONNECTORS OF 1/2" OR LESS AND EXTENDING THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
3. FOR OTHER DETAILS, SEE THE MANUAL OR UNIFORM PAVEMENT MARKING LEGENDS.
4. PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

PAY QUANTITIES	
LEGEND/SYMBOL	AREA (ft ²)
ONLY	22.0
TURN ARROW	16.4
THRU ARROW	21.2
COMBINATION ARROW	24.3

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

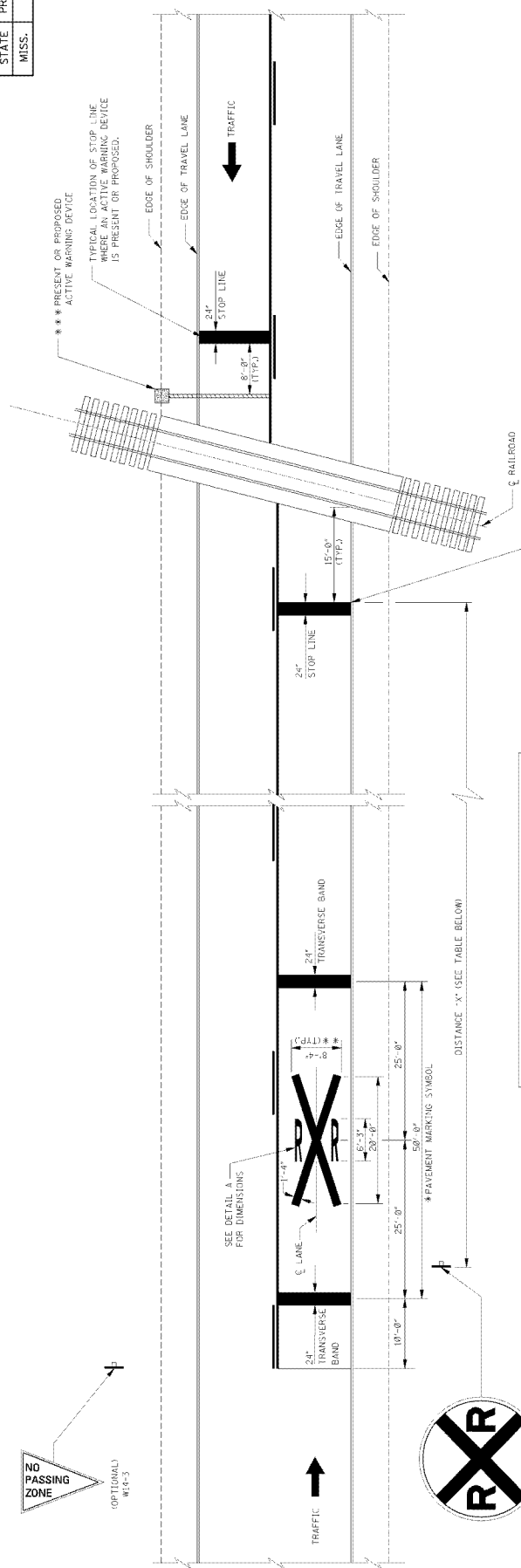
**PAVEMENT MARKING
LEGEND DETAILS**

DATE: _____ BY: _____ REVISION: _____

ISSUE DATE: OCTOBER 1, 1998

WORKING NUMBER: PM-6
SHEET NUMBER: 125

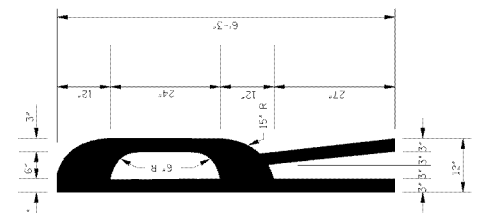
STATE PROJECT NO.
MISS.



ADVANCE WARNING SIGN PLACEMENT DISTANCE

POSTED SPEED (mph)	DISTANCE "X" (FT)	
	RURAL	URBAN
20	175	100
25	250	100
30	325	100
35	400	150
40	475	225
45	550	300
50	625	375
55	700	450
60	775	550

NOTES:
 ① DISTANCE "X" MAY BE ADJUSTED IF PROHIBITIVE PHYSICAL CONDITIONS EXIST AT THE DESIGNATED DISTANCE.
 ② THESE DISTANCES MAY BE ADJUSTED TO A MINIMUM OF 100' IN RESTRICTED AREAS OR BUSINESS DISTRICTS WHERE LOW SPEEDS ARE PREVALENT.



DETAIL A
STANDARD "R" PAVEMENT MARKING
FOR R X R SYMBOL

TYPICAL LOCATION OF STOP LINE WHERE NO ACTIVE WARNING DEVICE IS PRESENT OR PROPOSED.

GENERAL NOTES:

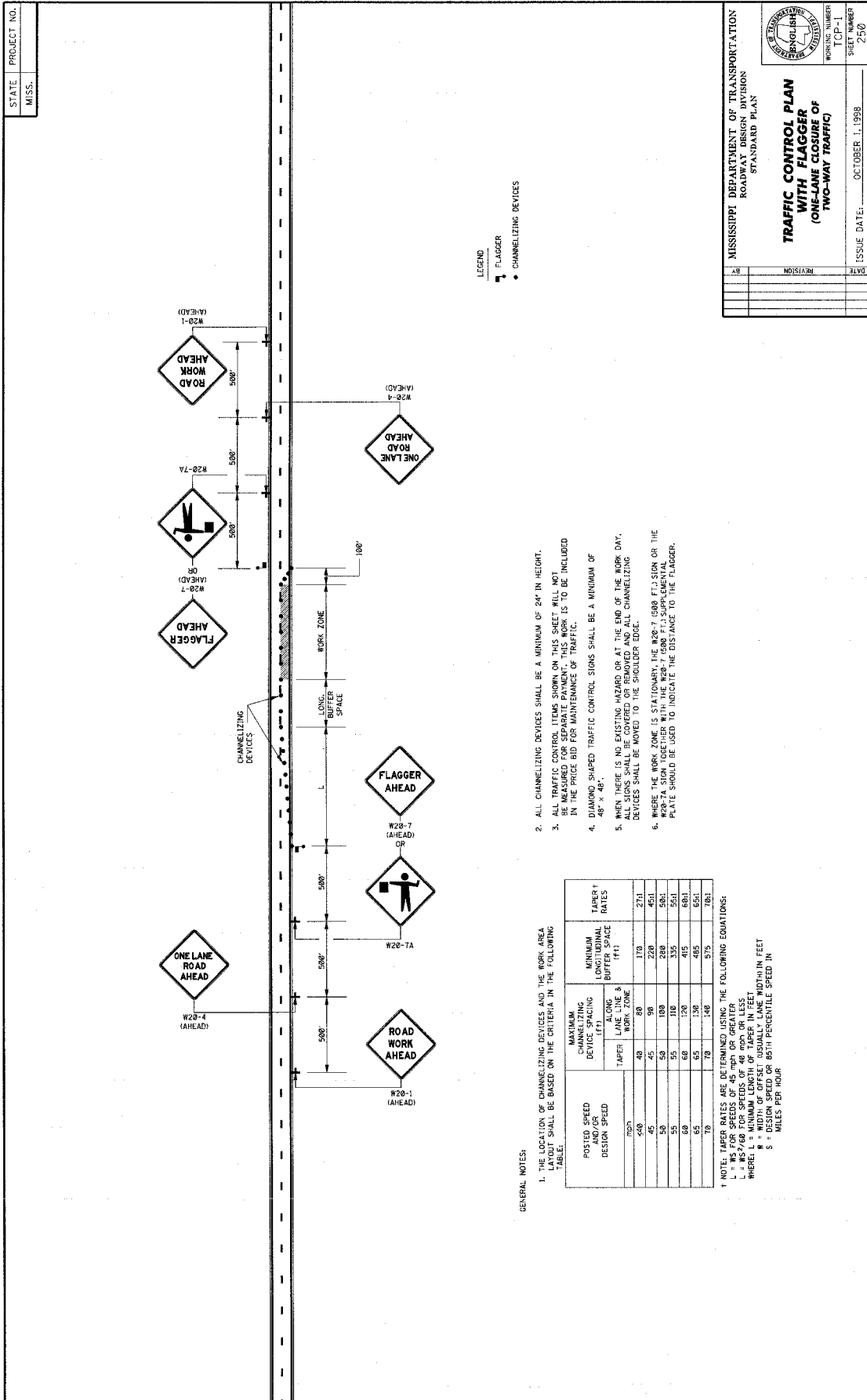
- * 1. A PORTION OF THE PAVEMENT MARKING SYMBOL SHOULD BE DIRECTLY OPPOSITE THE ADVANCE WARNING SIGN (WD-1).
- ** 2. WIDTH OF R X R SYMBOL MAY VARY ACCORDING TO LANE WIDTH. HOWEVER, ON MULTI-LANE ROADS, THE TRANSVERSE BANDS AND STOP LINE SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.
- Δ 3. R X R SYMBOL (63.0 ± 4.2), TRANSVERSE BANDS AND STOP LINE SHALL BE PAID FOR AS LEGEND, WHITE (PLASTIC), MATERIAL OPTIONAL FOR OTHER AGENCIES.
- ** * 4. REFER TO THE 'MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES' FOR LOCATION OF PROPOSED WARNING DEVICES AT RAILROAD-HIGHWAY GRADE CROSSINGS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TYPICAL PLACEMENT OF WARNING SIGNS AND PAVEMENT MARKINGS AT RAILROAD HIGHWAY GRADE CROSSINGS

WORKSHEET NO. 12
SHEET NUMBER 12
ISSUE DATE: OCTOBER 11, 1998

DATE	BY	REVISION



STATE PROJECT NO.
MISS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TRAFFIC CONTROL PLAN
WITH FLAGGER
(ONE-LANE CLOSURE OF
TWO-WAY TRAFFIC)**

WORKING NUMBER: TCP-1
SHEET NUMBER: 250
ISSUE DATE: OCTOBER 1, 1998

DATE	REVISION

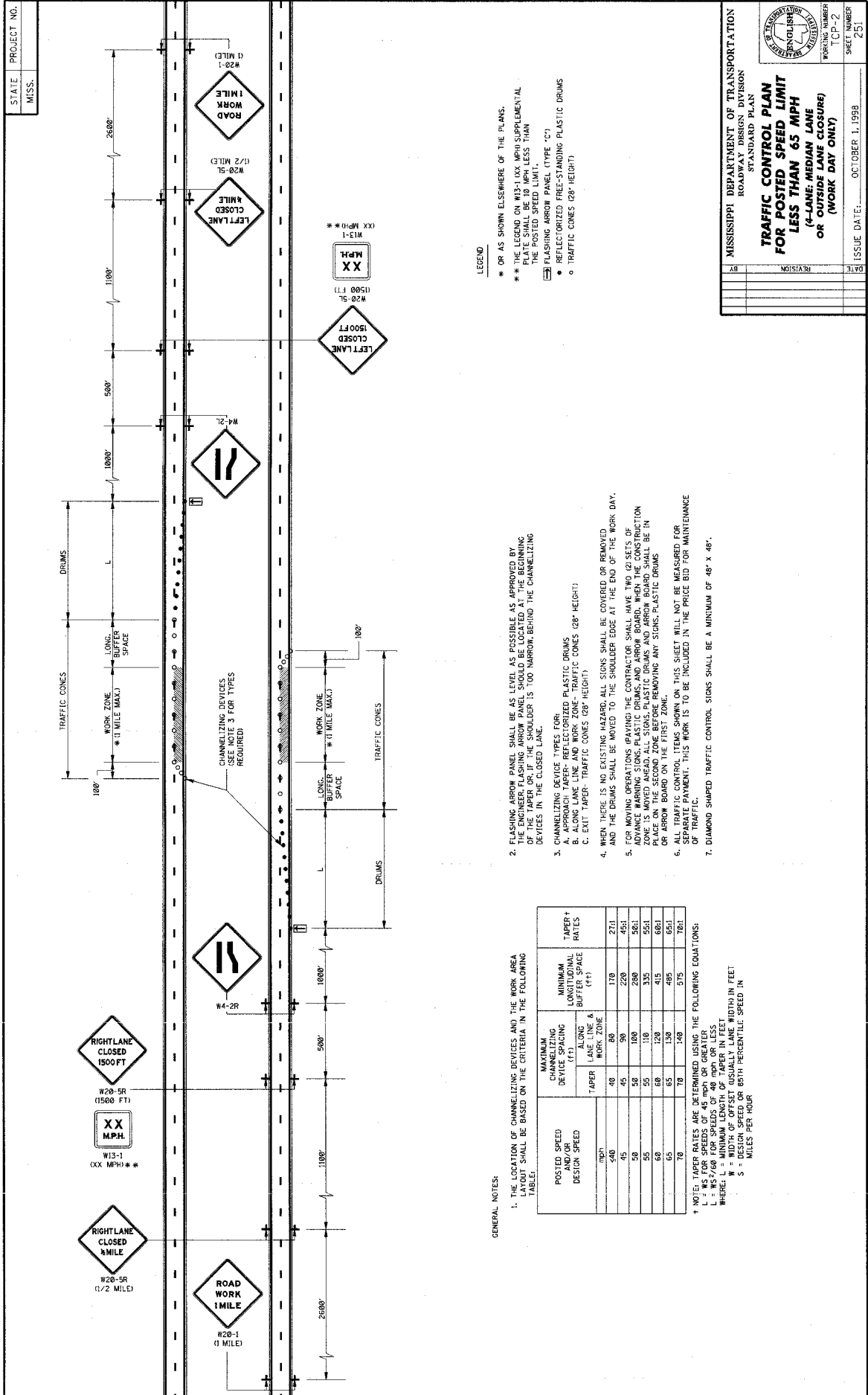
LEGEND
 FLAGGER
 CHANNELIZING DEVICES

2. ALL CHANNELIZING DEVICES SHALL BE A MINIMUM OF 24" IN HEIGHT.
3. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
4. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".
5. WHEN THERE IS NO CASTING HAZARD OR AT THE END OF THE WORK DAY, ALL SIGNS SHALL BE COVERED OR REMOVED AND ALL CHANNELIZING DEVICES SHALL BE MOVED TO THE SHOULDER EDGE.
6. WHERE THE WORK ZONE IS STATIONARY, THE W20-7 (500 FT.) SIGN OR THE W20-7A SIGN TOGETHER WITH THE W20-7 (500 FT.) SUPPLEMENTAL PLATE SHOULD BE USED TO INDICATE THE DISTANCE TO THE FLAGGER.

GENERAL NOTES:
 1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	CHANNELIZING DEVICES SPACING		MINIMUM LONGITUDINAL BUFFER SPACE (FT.)	TAPER †
	ALONG WORK ZONE	ALONG BUFFER SPACE		
20	40	80	170	270
40	45	90	220	450
50	50	100	280	500
55	55	110	335	550
60	60	120	415	600
65	65	130	485	650
70	70	140	575	700

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 L = WS FOR SPEEDS OF 45 MPH OR GREATER
 L = WS FOR SPEEDS OF 30 MPH OR GREATER
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR



STATE PROJECT NO.
MISS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN
**TRAFFIC CONTROL PLAN
FOR POSTED SPEED LIMIT
LESS THAN 65 MPH
(4-LANE; MEDIUM LANE
OR OUTSIDE LANE CLOSURE)
(WORK DAY ONLY)**

WORKING NUMBER
TCP-2
SHEET NUMBER
251

ISSUE DATE: OCTOBER 1, 1999

LEGEND

- * OR AS SHOWN ELSEWHERE OF THE PLANS.
- ** THE LEGEND ON W13-1XX MPH SUPPLEMENTAL PLATE SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT.
- ◻ FLASHING ARROW PANEL (TYPE "C")
- REFLECTORIZED FREE-STANDING PLASTIC DRUMS
- TRAFFIC CONES (28" HEIGHT)

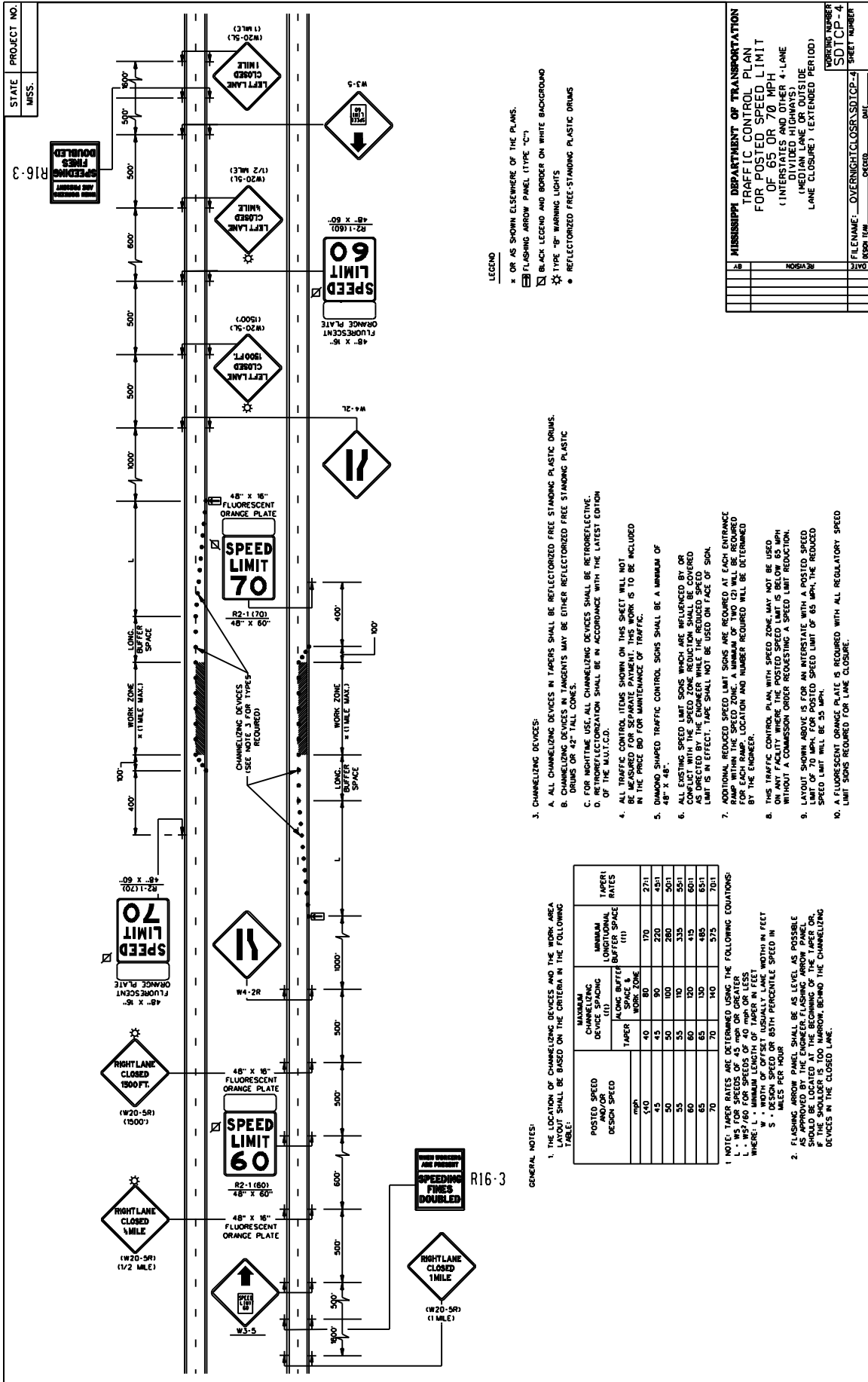
2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANELS SHOULD BE LOCATED AT THE BEGINNING OF THE BUFFER OF THE SHOULDER IS 100' NARROWER BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICE TYPES FOR:
 - A. ALONG LANE LINE AND WORK ZONE - TRAFFIC CONES (28" HEIGHT)
 - B. EXIT TAPER - TRAFFIC CONES (28" HEIGHT)
4. WHEN THERE IS NO EXISTING HAZARD, ALL SIGNS SHALL BE COVERED OR REMOVED, AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS BEHIND THE CONTRACTOR SHOULDER, TWO SETS OF DRUMS SHALL BE USED TO PROTECT THE WORK ZONE. THE FIRST SET OF DRUMS SHALL BE MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
6. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
7. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".

GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

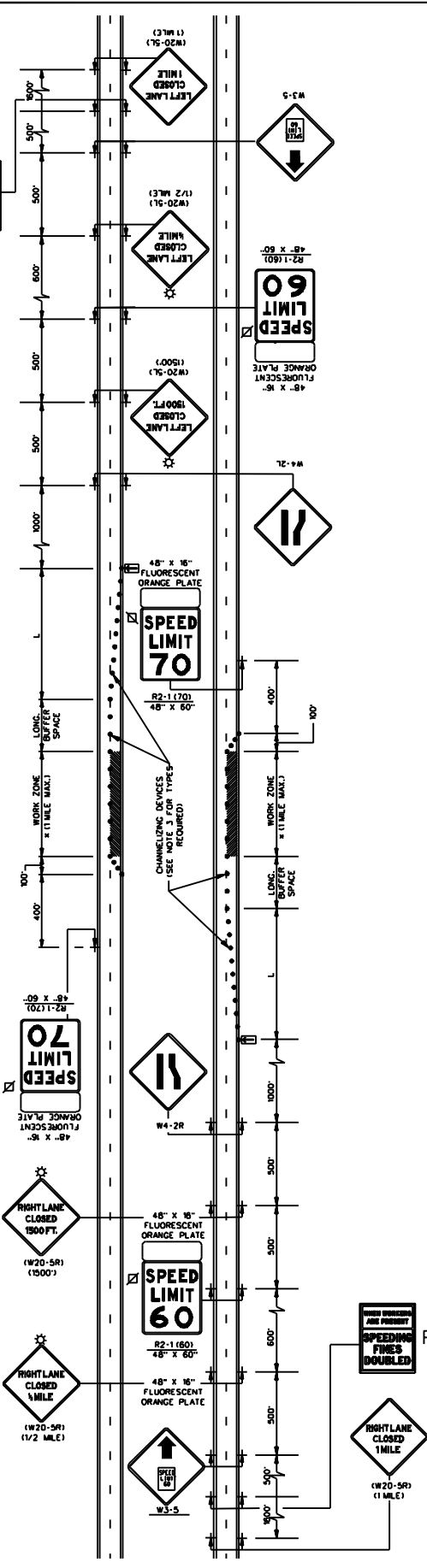
POSTED SPEED (MPH) OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (FT)		MINIMUM LONGITUDINAL BUFFER SPACE (FT)	TAPER RATES
	LANE	WORK ZONE		
40	40	80	170	27:1
45	45	90	220	45:1
50	50	100	280	36:1
55	55	110	335	55:1
60	60	120	415	68:1
65	65	130	495	65:1
70	70	140	575	78:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 L = WS FOR SPEEDS OF 45 MPH OR GREATER
 L = WS^{2/60} FOR SPEEDS OF 40 MPH OR LESS
 WHERE: W = WIDTH OF OFFSET USUALLY LANE WIDTH IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR



STATE PROJECT NO.
MISS. R16-3

SPEEDING FINES DOUBLED
SEE NOTE 1



- LEGEND**
- * OR AS SHOWN ELSEWHERE OF THE PLANS.
 - ◻ FLASHING ARROW PANEL (TYPE "C")
 - ◻ BLACK LEGEND AND BORDER ON WHITE BACKGROUND
 - ⊗ TYPE "B" WARNING LIGHTS
 - ◉ REFLECTORIZED FREE-STANDING PLASTIC DRUMS

- 3. CHANNELIZING DEVICES:**
- A. ALL CHANNELIZING DEVICES IN TANGERS SHALL BE REFLECTORIZED FREE STANDING PLASTIC DRUMS.
 - B. CHANNELIZING DEVICES IN TANGERS MAY BE EITHER REFLECTORIZED FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.
 - C. FOR NIGHTTIME USE, ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE.
 - D. RETROREFLECTORIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MUTCD.
- 4. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE REFLECTORIZED UNLESS SPECIFICALLY NOTED OTHERWISE IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.**
- 5. DAMAGED SHIPPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" x 48".**
- 6. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE UNLIT BY OR ALL EXISTING SPEED REDUCTION SIGNS WHICH ARE UNLIT AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT, TAPE SHALL NOT BE USED ON FACE OF SIGN.**
- 7. ADDITIONAL REDUCED SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. A MINIMUM OF TWO (2) WILL BE REQUIRED FOR EACH RAMP. LOCATION AND NUMBER REQUIRED WILL BE DETERMINED BY THE ENGINEER.**
- 8. THIS TRAFFIC CONTROL PLAN, WITH SPEED ZONE, MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.**
- 9. ALL REFLECTORIZED ORANGE PLATE SIGNS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS REQUIRED FOR LANE CLOSURE.**

GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA TAPER SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

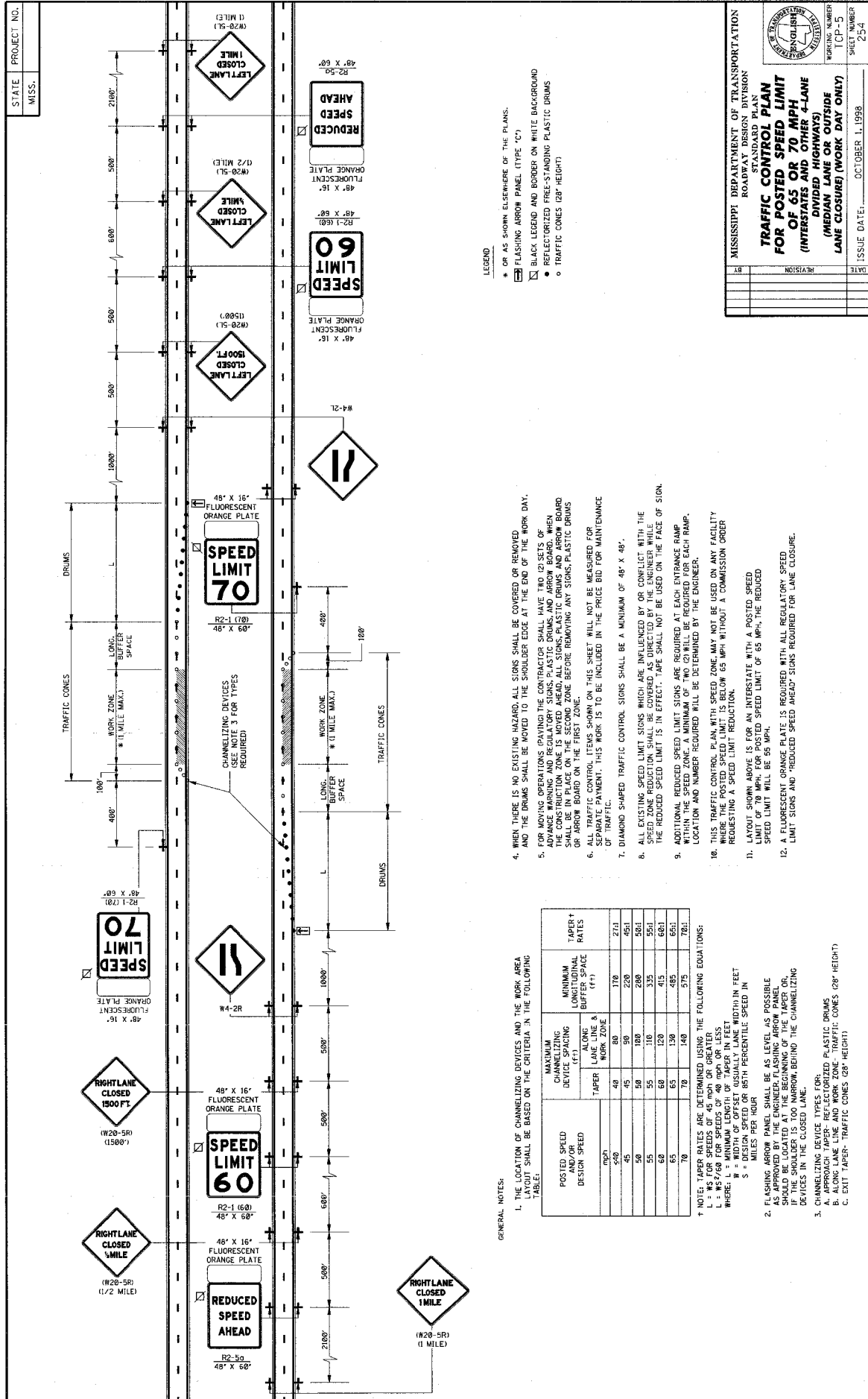
POSTED SPEED AND/OR DESIGN SPEED mph	MAXIMUM CHANNELIZING DEVICE SPACING TAPER SPACE & BUFFER WORK ZONE	MINIMUM TAPER RATES	
		MINIMUM TAPER RATES	TAPER RATES
40	40	170	2711
45	50	220	4511
50	60	280	5611
55	70	330	6711
60	80	380	7811
65	90	430	8911
70	100	480	10011

- 1. NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:**
- L = WS FOR SPEEDS OF 45 mph OR GREATER
 - L = WS/60 FOR SPEEDS OF 40 mph OR LESS
 - WHERE L = MINIMUM LENGTH OF TAPER IN FEET
 - W = WIDTH OF TAPER IN FEET
 - S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
- 2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANELS SHALL BE 100' FROM THE EDGE OF THE SHOULDER OR 100' FROM THE EDGE OF THE SHOULDER BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.**

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL PLAN
FOR POSTED SPEED LIMIT
OF 65 OR 70 MPH
(INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS, MEDIAN LANE OR OUTSIDE LANE CLOSURE) (TEXTURED PERIOD)

DATE: _____
BY: _____
CHECKED: _____
DESIGNED: _____

FILE NAME: OVERNIGHTCLOSARS.DWG
SHEET NUMBER: SDTCP-4



STATE PROJECT NO.
MISS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN
TRAFFIC CONTROL PLAN
FOR POSTED SPEED LIMIT
OF 65 OR 70 MPH
(INTERSTATES AND OTHER 4-LANE
DIVIDED HIGHWAYS)
(MEDIAN LANE OR OUTSIDE
LANE CLOSURE) (WORK DAY ONLY)

WORKING NUMBER: TPC-5
SHEET NUMBER: 254
ISSUE DATE: OCTOBER 1, 1998

LEGEND

- * OR AS SHOWN ELSEWHERE OF THE PLANS.
- FLASHING ARROW PANEL (TYPE "C")
- BLACK LEGEND AND BORDER ON WHITE BACKGROUND
- REFLECTORIZED FREE-STANDING PLASTIC DRUMS
- TRAFFIC CONES (28" HEIGHT)

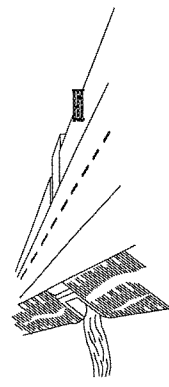
4. WHEN THERE IS NO EXISTING HAZARD, ALL SIGNS SHALL BE COVERED OR REMOVED, AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF THE CONSTRUCTION ZONE IS MOVED AHEAD ALL SIGNS, PLASTIC DRUMS, AND ARROW BOARD OR ARROW BOARD ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
6. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
7. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".
8. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON THE FACE OF SIGN.
9. ADDITIONAL REDUCED SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. A MINIMUM OF TWO (2) WILL BE REQUIRED FOR EACH RAMP. LOCATION AND NUMBER REQUIRED WILL BE DETERMINED BY THE ENGINEER.
10. THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE, MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
11. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.
12. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS AND "REDUCED SPEED AHEAD" SIGNS REQUIRED FOR LANE CLOSURE.

GENERAL NOTES:
1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA WIDTH SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED (mph)	CHANNELIZING DEVICE SPACING (FT)		MINIMUM BUFFER SPACE (FT)	TAPER RATES
	LANE LINE & TAPER	WORK ZONE		
50	40	80	170	27:1
45	45	90	220	45:1
50	50	100	280	50:1
55	55	110	335	55:1
60	60	120	415	60:1
65	65	130	485	65:1
70	70	140	575	70:1

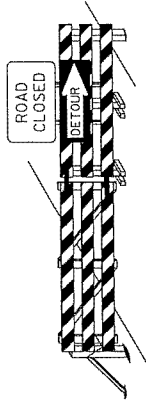
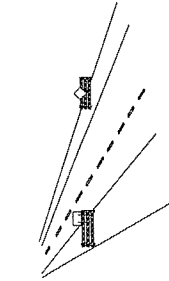
- † NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 $L = W^2$ FOR SPEEDS OF 45 MPH OR GREATER
 $L = 1.5W^2$ FOR SPEEDS OF 55 MPH OR GREATER
 WHERE:
 W = MINIMUM LENGTH OF TAPER IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE AND SHALL BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
 3. CHANNELIZING DEVICE TYPES (TOP):
 A. APPROACH TAPER - REFLECTORIZED PLASTIC DRUMS (28" HEIGHT)
 B. APPROACH TAPER - REFLECTORIZED PLASTIC CONES (28" HEIGHT)
 C. EXIT TAPER - TRAFFIC CONES (28" HEIGHT)

STATE	PROJECT NO.
MISS.	



WING BARRICADES

1. WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
2. WING BARRICADES SHOULD BE USED:
 - a. IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - b. IN ADVANCE OF ALL BRIDGE OR CULVERT REPAIRING OPERATIONS.

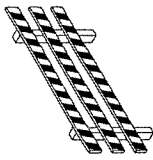


BARRICADE CLOSING A ROAD

BARRICADE CHARACTERISTICS

	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE #	6"	36" MIN.	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF REFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 (IF FACING TRAFFIC IN ONE DIRECTION) 6 (IF FACING TRAFFIC IN TWO DIRECTIONS)
TYPE OF FRAME	LIGHT	LIGHT "A" FRAME	POST OR SKID

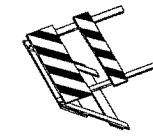
- * 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- ** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS SHALL HAVE A MINIMUM OF 270 1/2" OF REFLECTIVE AREA FACING TRAFFIC.



TYPE III

STANDARD BARRICADES

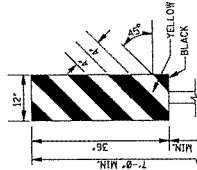
1. A TYPE I BARRICADE CONSISTS OF ONE (1) HORIZONTAL RAIL SUPPORTED BY A DEMOUNTABLE FRAME OR A LIGHT "A" FRAME. A TYPE I BARRICADE NORMALLY WOULD BE USED ON CONVENTIONAL ROADS OR URBAN STREETS AND ARTERIALS.
2. A TYPE II BARRICADE CONSISTS OF TWO (2) HORIZONTAL RAILS ON A LIGHT "A" FRAME. TYPE II BARRICADES ARE INTENDED FOR USE ON EXPRESSWAYS AND FREEWAYS AND OTHER HIGH-SPEED ROADWAYS.
3. TYPE I AND TYPE II BARRICADES ARE INTENDED FOR USE WHERE THE HAZARD IS RELATIVELY SMALL AS, FOR EXAMPLE, ON CITY STREETS, OR FOR THE MORE OR LESS CONTINUOUS DELIMITING OF A RESTRICTED ROADWAY, OR FOR TEMPORARY OBTAINING USE. CONTINUOUS DELIMITING OF A RESTRICTED ROADWAY, OR FOR TEMPORARY OBTAINING USE.
4. A TYPE III BARRICADE CONSISTS OF THREE (3) HORIZONTAL RAILS SUPPORTED BY FIXED POSTS, A RIGID SKID, A HEAVY DEMOUNTABLE FRAME OR A HEAVY, RINGED "A" FRAME.
5. TYPE III BARRICADES ARE INTENDED FOR USE ON CONSTRUCTION AND MAINTENANCE PROJECTS AS WING BARRICADES AND AT ROAD CLOSURES, WHERE THEY MUST REMAIN IN PLACE FOR EXTENDED PERIODS.
6. THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).
7. DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
8. FOR ADDITIONAL INFORMATION OR DETAILS, SEE METHOD, LATEST EDITION.
9. BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES WHICH REQUIRE CRASHWORTHINESS ACCEPTANCE LETTERS. TO DATE, 2-IN. THICK TIMBER RAILS HAVE NOT BEEN SUCCESSFULLY CRASH TESTED. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE: http://safety.fhwa.dot.gov/roadway_dept/policy_guide/road_how_to_build/cr2.ctm



TYPE II



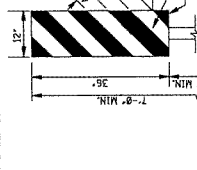
TYPE I



VERTICAL PANEL

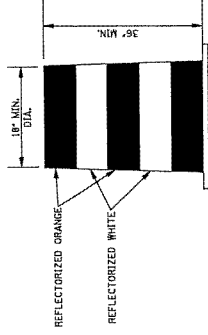
1. VERTICAL PANELS CONSIST OF AT LEAST ONE PANEL 8" TO 12" IN WIDTH AND A MINIMUM OF 24" IN HEIGHT.
2. THE DIAGONAL STRIPES SHALL SLOPE DOWNWARD FROM LEFT TO RIGHT. THE PANEL SHALL BE MOUNTED WITH THE TOP A MINIMUM OF 36" ABOVE THE ROADWAY ON A SINGLE LIGHTMASS POST.
3. VERTICAL PANELS USED ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH-SPEED ROADWAYS SHALL HAVE A MINIMUM OF 270 1/2" OF RETROREFLECTIVE AREA FACING TRAFFIC.
4. FOR TWO-WAY TRAFFIC OPERATIONS, BACK-TO-BACK PANELS SHALL BE USED.

- GENERAL NOTES:
1. ALL DEVICES SHOWN ON THIS SHEET SHALL BE HIGH INTENSITY REFLECTIVE SHEETING.
 2. THE TRAFFIC CONTROL PLAN WILL LIST THE VARIOUS TRAFFIC CONTROL DEVICES REQUIRED FOR EACH PROJECT.



TYPE 3 OBJECT MARKER (OM-3R)

1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
2. THE OM-3R IS SHOWN. THE OM-3L IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
3. THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



PLASTIC DRUM STRIPING DETAIL

1. PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR MARKING THE EDGE OF THE ROADWAY. DRUMS SHALL BE CONSISTENT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT COLOR ON DRUMS SHALL BE ORANGE WITH FOUR (4) REFLECTORIZED, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
2. DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
3. WHERE PRACTICAL PLASTIC DRUMS SHALL BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS

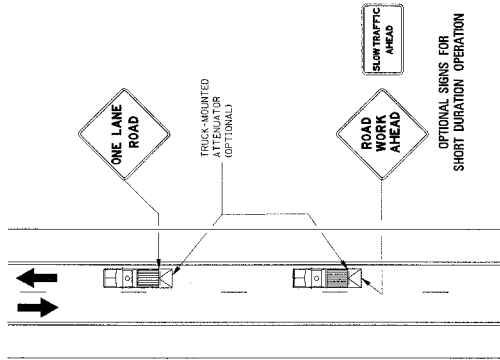
WORKING NUMBER: SDTCP-10

SHEET NUMBER: _____

ISSUE DATE: 10-04-2011

STATE PROJECT NO.
MISS. _____

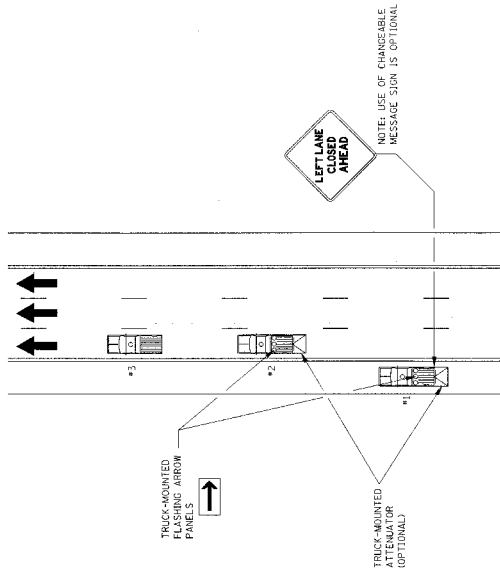
MOBILE OPERATIONS ON TWO-LANE ROAD



OPTIONAL SIGNS FOR SHORT DURATION OPERATION

MOBILE OPERATIONS ON TWO-LANE ROAD

MOBILE OPERATIONS ON MULTILANE ROAD

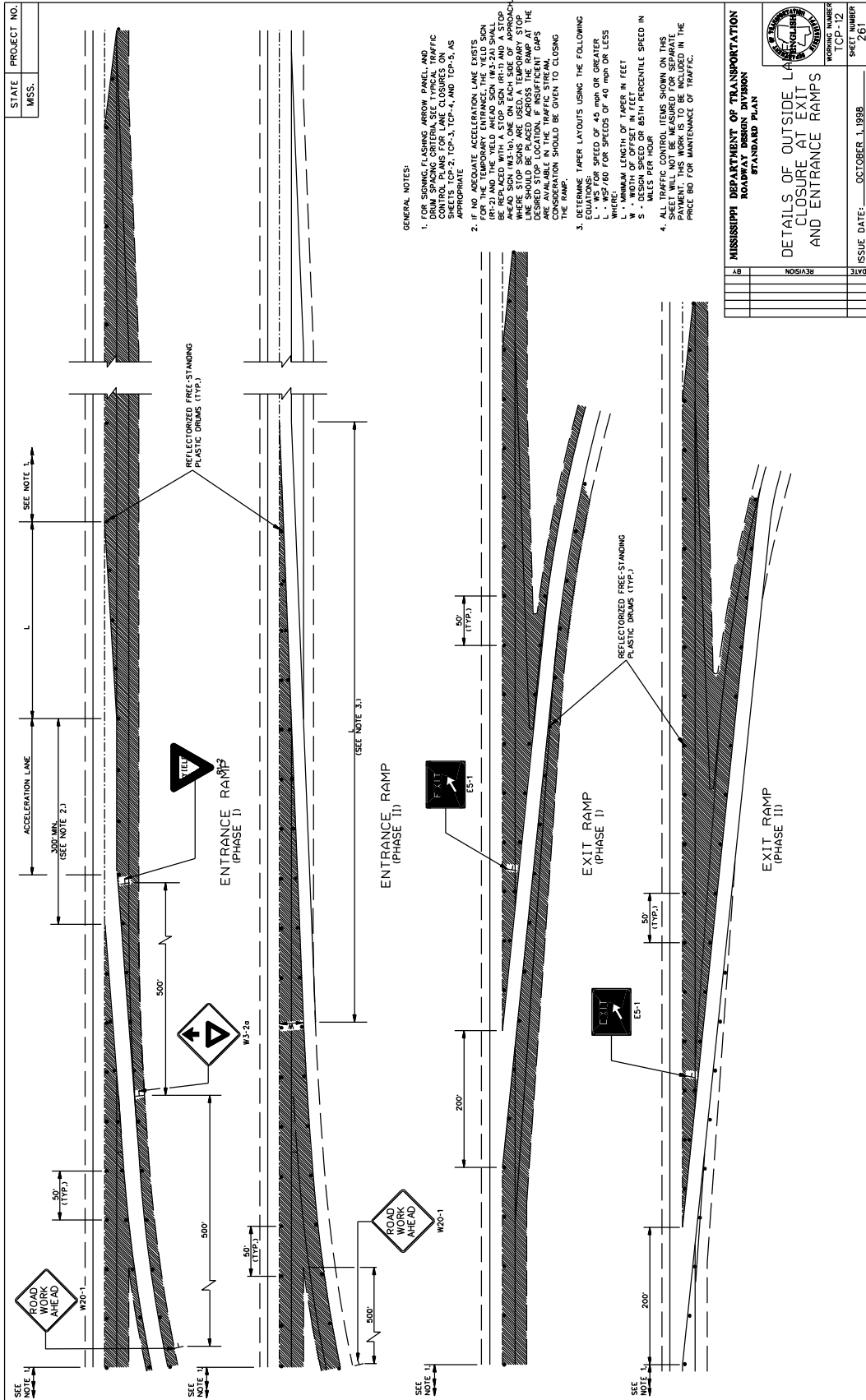


MOBILE OPERATIONS ON MULTILANE ROAD

- NOTES:
1. BEING PRACTICAL AND WHEN NEEDED, THE WORK AND PROTECTION VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY, AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
 2. THE DISTANCE BETWEEN THE WORK AND PROTECTION VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. PROTECTION VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE WARNING IS PROVIDED, PROTECTION VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE PROTECTION VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
 3. ADDITIONAL PROTECTION VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
 4. A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE PROTECTION VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
 5. THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS AND THE PROTECTION VEHICLE SHALL BE EQUIPPED WITH THE MINIMUM REQUIRED FLASHING LIGHTS. PROTECTION AND WORK VEHICLES MOUNTED ON THE REAR, ADJACENT TO THE SIGN, PROTECTION AND WORK VEHICLES SHOULD DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
 6. VEHICLE-MOUNTED SIGNS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 49" ABOVE THE PAVEMENT. BEACONS SHOULD BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
 7. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

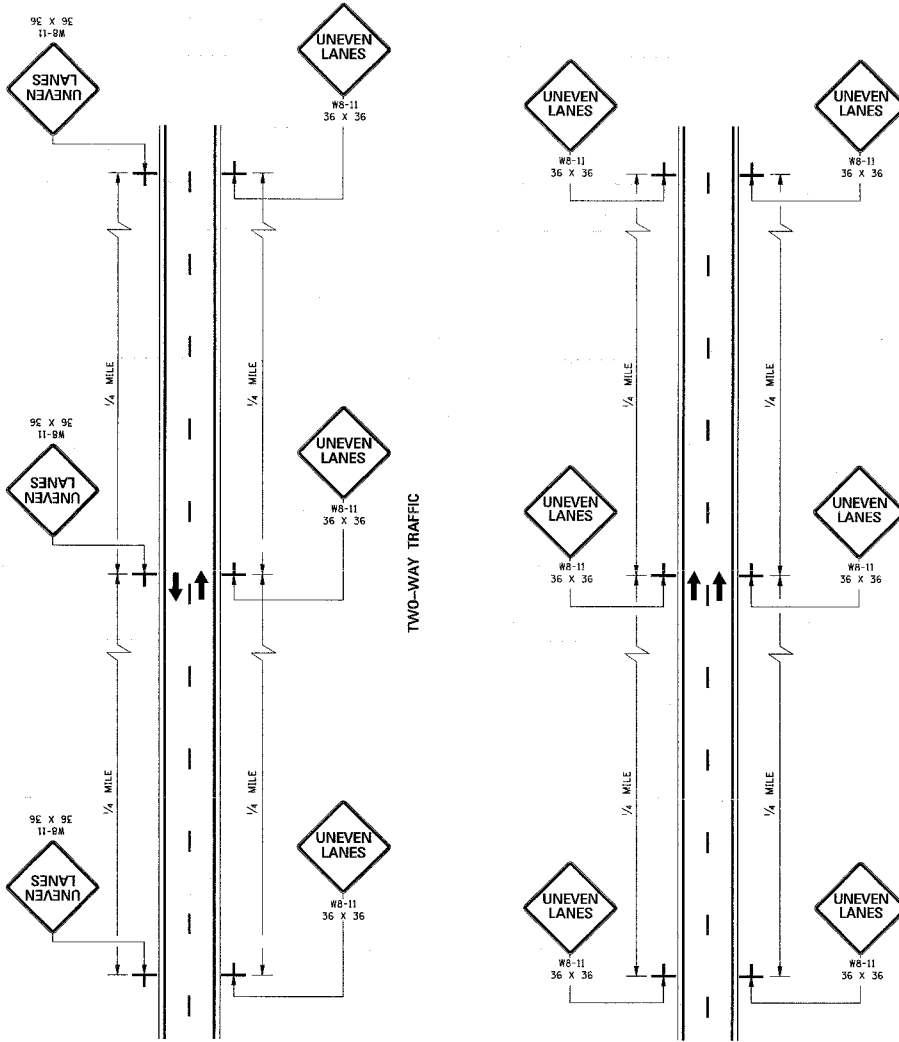
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO-LANE ROADS	
NO. 213	REVISED
NO. 212	NO. 211
NO. 211	NO. 210
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NO. 4	NO. 3
NO. 3	NO. 2
NO. 2	NO. 1
NO. 1	NO. 0

ISSUE DATE: OCTOBER 1, 1998
DRAWN BY: _____
CHECKED BY: _____
DATE: _____
SCALE: _____
SHEET NO. 260



STATE MISS. PROJECT NO.

- GENERAL NOTES:
1. UNEVEN LANE LINE SIGNS SHALL BE EQUAL TO 1/2" NO. SIGNS REQUIRED.
 2. IF GREATER THAN 1/2" AND LESS THAN OR EQUAL TO 2 1/4", PLACE SIGNS AS SHOWN ON THIS SHEET.
 3. IF GREATER THAN 2 1/4", TRAFFIC SHOULD NOT BE ALLOWED TO CROSS UNEVEN LANE LINE.
 4. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.
 5. THE W8-11 SIGNS SHALL BE SPACED AT 1/4-MILE INTERVALS THROUGHOUT UNEVEN LANE LINE LIMITS.



MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

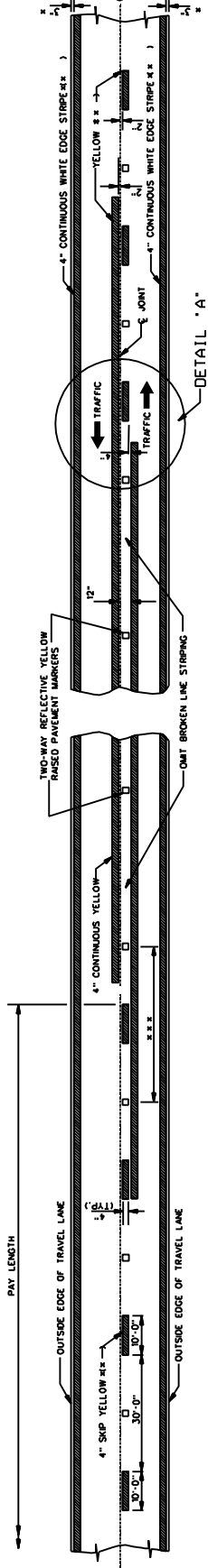
**TRAFFIC CONTROL PLANS
UNEVEN PAVEMENT
DETAILS**

WORKING NUMBER: CP-14
SHEET NUMBER: 263

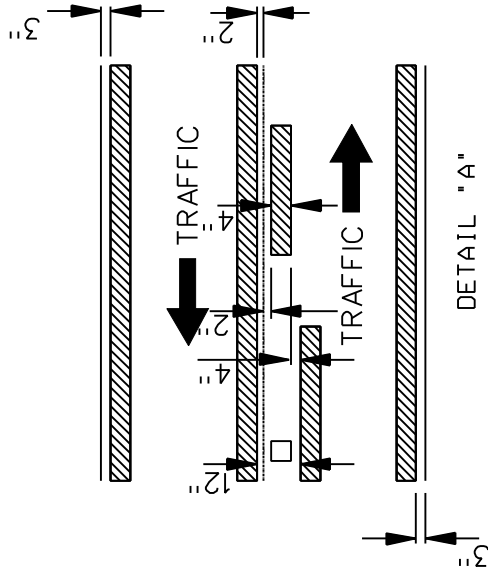
ISSUE DATE: OCTOBER 1, 1998

DATE	REVISION

STATE PROJECT NO.
MISS.



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)

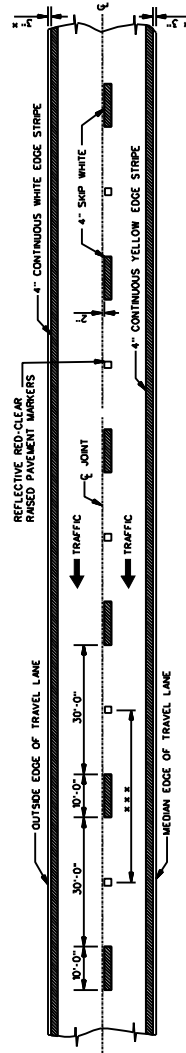


GENERAL NOTES:

- x 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- ** 2. EDGE STRIPING SHALL BE SAME MATERIAL AS LANE LINE STRIPING (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
- *** 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS

TANGENT SECTIONS	URBAN AREA (11-11)	RURAL AREA (11-12)
HORIZONTAL CURVES	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	140'-0"

1. NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 100' IN ADVANCE IN DIRECTION OF TRAFFIC OF THE EXIT RAMP AND ENDING 100' IN ADVANCE OF THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPE.
4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED SOURCES OF MATERIALS."
5. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS.



4-LANE WITH ONE-WAY TRAFFIC

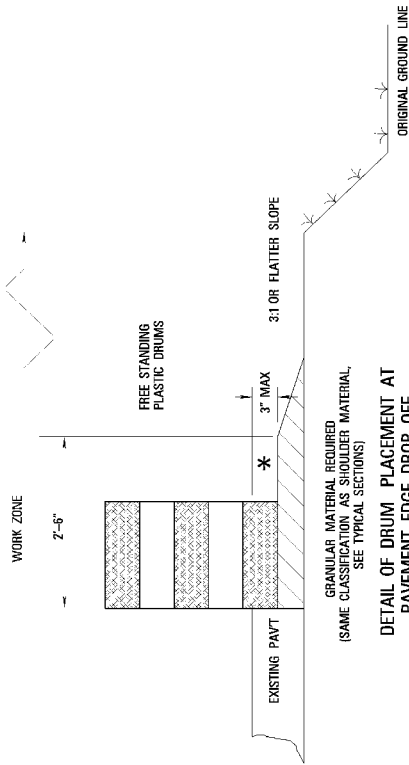
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TEMPORARY STRIPING FOR
TRAFFIC CONTROL
2-LANE AND 4-LANE
DIVIDED HIGHWAYS

WORKING NUMBER
TCP-15

SHEET NUMBER
264

ISSUE DATE: DECEMBER 1, 1989



GRANULAR MATERIAL REQUIRED
(SAME CLASSIFICATION AS SHOULDER MATERIAL,
SEE TYPICAL SECTIONS)

**DETAIL OF DRUM PLACEMENT AT
PAVEMENT EDGE DROP-OFF**

NOTES

- * A. PAVEMENT EDGE DROP-OFF
 1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750 ± 0.0).
 2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. CONES MAY BE USED IN PLACE OF DRUMS, PANELS AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE M.U.T.C.D. (1/3 L, WHERE L IS THE TAPER LENGTH IN FEET)
 3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 3:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS/PANELS OR BARRICADES MAY BE USED.
 4. FOR TEMPORARY CONDITIONS DROP OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
 5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

B. DRUM SPACING

- 1. TANGENTS = $2 \times S$
- 2. CURVES = $L \times W$
- WHERE $L = \frac{S^2}{W}$
- L = TAPER LENGTH IN FEET
- S = SPEED IN MPH (POSTED OR 85 PERCENTILE)
- W = WIDTH OF OFFSET IN FEET

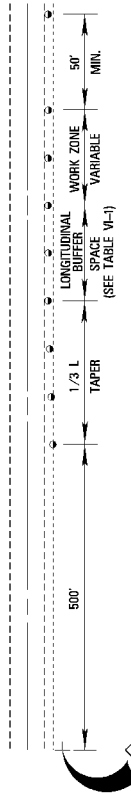
C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

TABLE VI-1 GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

SPEED (MPH)	LENGTH (FEET)
20	25
25	35
30	50
35	70
40	100
45	150
50	200
55	250
60	300
65	350
70	400
75	450
80	500
85	550

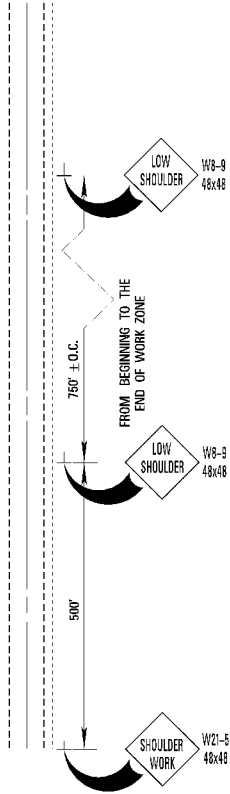
* POSTED SPEED OFF-PEAK 85 PERCENTILE SPEED
PRIOR TO WORK STARTING OR THE ANTICIPATED
GROUNDING SPEED IN MPH.

PLASTIC DRUMS
(SEE NOTE FOR SPACING)



TYPICAL SHOULDER CLOSURE

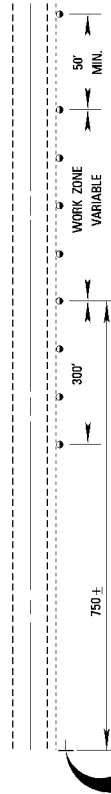
- (1) TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
- (2) TO BE USED WHEN CONSTRUCTION VEHICLES (EQUIPMENT) ENCRUSHES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.



TYPICAL SHOULDER WORK #1

(SEE NOTE A-1 THIS SHEET)

PLASTIC DRUMS
(SEE NOTE FOR SPACING)



TYPICAL SHOULDER WORK #2

NOTE: WORK OUTSIDE THE (2) FOOT LIMIT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA, OR SEE NOTE A-3 THIS SHEET.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL DETAILS
DRUM PLACEMENT
AND
SHOULDER CLOSURE

PROJECT NO.:
COUNTY:
DATE: 02.28.23

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904- NOTICE TO BIDDERS NO. 5140

CODE: (SP)

DATE: 07/15/2014

SUBJECT: General Notes for Cable Rail Installation

PROJECT: HSIP-0020-01(214) / 106780301, HSIP-0020-01(215) / 106780302, & HSIP-0020-02(086) / 1607803030 – Hinds, Rankin, & Lauderdale Counties

Bidders are hereby advised of the following notes that relate to construction and installation of the cable barrier:

- 1) The Contractor shall install the cable barrier system as shown on the Typical Section Sheets attached and according to Special Provision No. 907-606-4 and the manufacturer's specifications.
- 2) All Terminal Sections at guardrail locations are to be placed behind the guardrail as far as practical in order to protect them from being struck by traffic.
- 3) The Contractor shall stake each section as indicated above for placement of the cable barrier system. The Contractor, along with the Project Engineer, shall then check for any possible obstructions such as drainage structures, paved ditches, trees, etc. that would interfere with the installation of the cable barrier system. If any obstructions are found, the Project Engineer will direct the Contractor on how to avoid the obstruction.
- 4) Obstructions such as bridge columns and high-mast lights will require protection on each lane whenever there is less than nine feet (9') of clearance between the obstruction and the cable barrier. In these cases, the barrier shall extend at least 175 feet beyond the obstruction on each side.
- 5) Breaks in the cable barrier will be required periodically to allow for mowing access on each side of the cable barrier. These breaks are to be spaced approximately every one (1) mile. The rail will be installed on each side of the median centerline and will overlap for a distance of approximately 175 feet. There will be a minimum distance of 25 feet between the two lines of rail to allow space for a mower to move between the barriers. The location of these breaks will be determined based on field conditions and approved by the Project Engineer prior to installation.
- 6) When transitioning to avoid obstructions or create a mower access break, a 50:1 taper shall be used.
- 7) The Contractor will be responsible for verifying the location of all existing wiring, conduit, etc. prior to installation. In the event that existing wiring, conduit, etc., is damaged, the Contractor shall be responsible for all cost for repairs.

- 8) The Contractor shall excavate a 3-foot wide by 4-inch thick trench for the Vegetation Control Pad. The Vegetation Control Pad shall be on the same slope and flush with natural ground. All material that comes from the grading for the vegetation control pad shall be site graded in a way not to interfere with drainage or adversely change the existing typical sections. Only the minimum excavation necessary to construct the vegetation pad will be allowed.
- 9) The concrete for the vegetation control pad shall be Class "C". Wire Mesh Reinforcement (6 X 6 – W1.4 X W1.4), or Reinforced Fibers as per Notice to Bidders No. 640, is required.
- 10) All incidental items, such as but not limited to site grading, finishing, materials, equipment, and labor associated with the vegetation control pad, shall be included in the price for Pay Item 221-A, Portland Cement Concrete Paved Ditch.
- 11) The Contractor shall take precautions necessary to keep from damaging the roadside. Any rutting or other damage shall be repaired by the Contractor at no additional cost to the Department.
- 12) The Contractor is responsible to ensure that growth and coverage is achieved on any areas that the Contractor disturbs. Any grassing that is required is to be done according to the governing specifications and shall be included in the cost of the pay items included for the cable barrier system.
- 13) Any material delivery vehicle that is required to exit or enter the highway for delivery of materials (including concrete) shall do so under a lane closure.
- 14) The Contractor shall install a shoulder closure in accordance with the attached drawing at any time the Contractor is performing work or has equipment within 30 feet of the edge of traffic.
- 15) The Contractor shall repair any posts that are damaged by the traveling public. Payment will be made under Pay Item 907-606-I001, Cable Barrier Post Repair. The Contractor will be required to furnish the entire plan quantity for this Pay Item. Any unused repair posts shall become the property of the Mississippi Department of Transportation.
- 16) The Contractor shall erect and maintain construction signing and provide all signs and traffic handling devices necessary to safely maintain traffic around and through the work areas in accordance with the Standard Drawings and the latest edition of the MUTCD. All construction signs will be measured for payment under Pay Items 619-D1 and 619-D2.
- 17) The area that is to receive crushed stone shall be excavated four inches (4") prior to placement of the crushed stone. The excavated material will be paid for under pay item 203-G, Excess Excavation.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SUPPLEMENT TO SPECIAL PROVISION NO. 907-107-14

DATE: 07/15/2014

SUBJECT: Contractor's Protection Plan

PROJECT: HSIP-0020-01(214) / 106780301, HSIP-0020-01(215) / 106780302, & HSIP-0020-02(086) / 1607803030 – Hinds, Rankin, & Lauderdale Counties

907-107.22.1--Contractor's Erosion Control Plan. Delete the first sentence of the second paragraph of Subsection 907-107.22.1 on page 1, and substitute the following.

The time between the Notice of Award and Notice to Proceed/Beginning of Contract Time in the proposal, has been allowed for the submittal and concurrence of the Contractor's erosion control plan, MDOT's review of the plan, and any revisions that may be necessary.

Delete subparagraphs 1, 2, and 3 of Subsection 907-107.22.1 on pages 1 and 2, and substitute the following.

The Contractor shall submit an erosion and siltation control plan that shows a typical protection plan for median inlets and any other drainage structure as necessary on the project. A site specific erosion and siltation control plan shall be provided for any site that has sensitive areas that may require special attention.

After the last paragraph of Subsection 907-107.22.1 on page 2, add the following.

As soon as the ECP has been approved, a copy of the SWPPP (Narrative, ECP with updates) shall be available on the project at all times. The Contractor shall provide and install a weatherproof enclosure, such as a mailbox, on the project at a location that will be readily accessible to the Engineer or others who may want to review the project ECP. The cost of installing and maintaining this enclosure shall be included in the prices bid for the various erosion control pay items and no direct payment will be made for this work.

Installing Median Cable Barriers, known as Federal Aid Project Nos. HSIP-0020-01(214), HSIP-0020-01(215), and HSIP-0020-02(086) / 106780301, 302, & 303 in Hinds, Rankin, and Lauderdale Counties.

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
					Roadway Items
0010	203-G004	(E)	10,998	Cubic Yard	Excess Excavation, LVM, AH
	Changed 07/14/2014				
0020	221-A001	(S)	6,600	Cubic Yard	Portland Cement Concrete Paved Ditch
0030	234-A001		1,000	Linear Feet	Temporary Silt Fence
0040	606-B001		750	Linear Feet	Guard Rail, Class A, Type 1
0050	606-C003		2	Each	Guard Rail, Cable Anchor, Type 1
0060	606-E003		2	Each	Guard Rail, Terminal End Section, Non-Flared
0070	618-A001		1	Lump Sum	Maintenance of Traffic
0080	619-D1001		128	Square Feet	Standard Roadside Construction Signs, Less than 10 Square Feet
0090	619-D2001		3,060	Square Feet	Standard Roadside Construction Signs, 10 Square Feet or More
0100	619-G4001		192	Linear Feet	Barricades, Type III, Single Faced
0110	620-A001		1	Lump Sum	Mobilization
0120	630-F002		18	Each	Delineators, Guard Rail, Yellow
0130	630-G002		2	Each	Type 3 Object Markers, OM-3R or OM-3L, Post Mounted
0140	907-234-D001		5	Each	Inlet Siltation Guard
0150	907-237-A003		5,000	Linear Feet	Wattles, 20"
0160	907-304-F003	(GT)	14,431	Ton	3/4" and Down Crushed Stone Base
	Changed 07/14/2014				
0170	907-606-G001		178,339	Linear Feet	Cable Barrier
0180	907-606-H001		74	Each	Cable Barrier Terminal Section
0190	907-606-I001		550	Each	Cable Barrier Post Repair

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
OFFICE OF CIVIL RIGHTS
JACKSON, MISSISSIPPI

LIST OF FIRMS SUBMITTING QUOTES

I/we received quotes from the following firms on Project No: **HSIP-0020-01(214), HSIP-0020-01(215), & HSIP-0020-02(086) / 106780301, 302, & 303**

County: **Hinds, Rankin, & Lauderdale** Letting Date: **July 22, 2014**

Disadvantaged Business Enterprise (DBE) Regulations as stated in 49 CFR 26.11 require the Mississippi Department of Transportation (MDOT) to create and maintain a comprehensive list of all firms quoting/bidding subcontracts on prime contracts and quoting/bidding subcontracts on federally-funded transportation projects. For every firm, we require the following information:

Firm Name: _____
Contact Name/Title: _____
Firm Mailing Address _____
Phone Number: _____
_____ DBE Firm _____ Non-DBE Firm

Firm Name: _____
Contact Name/Title: _____
Firm Mailing Address _____
Phone Number: _____
_____ DBE Firm _____ Non-DBE Firm

Firm Name: _____
Contact Name/Title: _____
Firm Mailing Address _____
Phone Number: _____
_____ DBE Firm _____ Non-DBE Firm

Firm Name: _____
Contact Name/Title: _____
Firm Mailing Address _____
Phone Number: _____
_____ DBE Firm _____ Non-DBE Firm

Firm Name: _____
Contact Name/Title: _____
Firm Mailing Address _____
Phone Number: _____
_____ DBE Firm _____ Non-DBE Firm

SUBMITTED BY (Signature)

FIRM NAME

Submit this form to **Contract Administration as a part of your bid package**. If at least one copy of this form is not **signed** and included as part of the bid packet, your bid will be deemed irregular. Question regarding this form shall be directed to www.gomdot.com under the current letting webpage. Please make and add copies of this form when needed or attach additional sheets containing the information required by this form and add these sheets to the bid package.