

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) enclose a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. 1 DATED 7/2/2014 ADDENDUM NO. DATED
 ADDENDUM NO. DATED ADDENDUM NO. DATED

Number	Description
1	Revised NTB No. 5079; Amendment EBS Download Required.

TOTAL ADDENDA: 1
 (Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

 Contractor

BY _____
 Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.

Revised 09/21/2005

MP-5011-38(008) / 305286301

Lauderdale County(ies)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 5079

CODE: (SP)

DATE: 06/05/2014

SUBJECT: Scope of Work

PROJECT: MP-5011-38(008) / 305286301 -- Lauderdale County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings". All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following:

Mill and overlay 3.3 miles of existing pavement on U.S. Hwy 11 in Lauderdale County from the Clarke/Lauderdale County line (Station 1+99) to the just north of Meehan Savoy Rd. (Station 175+90).

From the BOP at Station 1+99 to the EOP at Station 175+90

Work in this area shall consist of repairing failed transverse joints in the right and left lanes, cleaning and filling transverse joints in JRCP, milling and overlaying the existing pavement, and sawing and sealing transverse joints in the asphalt pavement. Prior to milling and inlay operations, failed joints listed in the attached table shall be removed and repaired full depth to a 3-foot width on either side of JRCP joint (6' total width) and backfilled with 12.5-mm, ST, Leveling, asphalt as per the attached typical sections and details. Any granular base material deemed unsuitable by the Engineer shall be removed as directed. Payment for the excavation of base material will be made using the 203-G Excess Excavation pay item. Saw cuts shall be cut to provide a neat removal and replacement area. No failed joints other than those shown on the attached table shall be added to the Contract without the consent of the District Construction Engineer. Pavement repairs shall be completed as a continuous operation in order to minimize the impact on traffic. Lane closures shall be in place until the failed area has been completely repaired.

Subsequent to the pavement repairs, clean transverse joints in JRCP pavement per attached typical sections and details. It is the Contractor's responsibility to locate joints in need of cleaning and filling. For joints up to 1½" wide, fill with hot applied, fiber reinforced polymer patching material (see attached Special Provision) up to within 1" of the top of the joint and top with 1" of Size No. 89 seal aggregate, meeting the requirements of Subsection 703.14. For joints 1½" and wider, fill with 12.5-mm, ST, Leveling, asphalt. Asphalt required to bring the joints to existing roadway grade is to be included in pay item 907-413-D001 - Cleaning and Filling Joints in PCC Pavement.

Subsequent to failed joint repair and cleaning and filling operations, the pavement shall be milled 1½” and overlaid with 2” and variable 12.5-mm, ST, asphalt where existing cross slope is 2%. Areas where cross slope is 1½% to 2%, correction will be made by placing 2¼” and variable at centerline and 1½” at edge of pavement or as required to obtain 2% slope. The milling should be full depth (2”) at bridge approach slabs to avoid a thin lift of asphalt. Subsequent to the overlay, low shoulders are to be brought to grade as required using Class 5 Group E Granular Material.

It is the intent of this project to mill and overlay the existing pavement in this section and to maintain the current travel lane and paved shoulder widths. No shoulder widening is anticipated in this section. However, paved shoulder widening repairs may be required and will be determined by the Engineer subsequent to the milling of the pavement. An estimated quantity for pavement removal and repair is included in the contract quantities for shoulders that are determined by the Engineer to be removed and replaced. If the pavement widening is to be repaired, the asphalt is to be removed to its entire depth and width outside of the existing concrete pavement limits. The existing underlying material is to be excavated three inches (3”) and graded to a neat line and compacted as required to form a suitable base. The pavement widening shall then be reconstructed using 12.5-mm, ST, asphalt. The maximum lift thickness of 3.5” should be used for the shoulder repairs.

After all overlaying operations are complete, the new asphalt shall be sawed and sealed over all transverse joints.

General Notes:

Milling will not begin until an **approved** asphalt mix design has been received, nor until such time that, in the opinion of the Engineer, weather conditions have been consistently suitable enough to allow placement of the asphalt after the milling operations. Milling will be limited to an area in a single lane established by the Engineer, which in the Engineer’s judgment, can be overlaid the same day. The Contractor will not exceed that area. **The milled lane shall not be opened to traffic until asphalt has been replaced.**

The Reclaimed Asphalt Pavement (RAP) material removed by the milling operation shall become the property of the contractor with the exception of 10,000 tons or 50% of the total anticipated RAP tonnage, whichever is less, to be stockpiled at the MDOT maintenance office yard at the Meridian Maintenance Office The Contractor will be required to coordinate the efforts with the Maintenance Office to effectively stockpile the milled material as directed by the Engineer. All costs associated with the hauling, placing, and stockpiling the state retained material shall be included in the price bid for the cold milling.

Removal of existing cold plastic stripe on the bridge will be required. Also, removal of stripe will be necessary at the EOP in order to tie existing edge stripe to new thermoplastic edge stripe. All permanent stripe will be placed as thermoplastic. Temporary traffic stripe will be required immediately after the milling and/or required overlay and prior to opening area to traffic. Temporary stripe is to be placed in the same location and configuration as the permanent stripe except that it may be offset as required for milling and paving operations.

The intersecting roads within the project limits are to be milled 1½” and overlaid with 2” of 12.5-mm, ST, asphalt to the right-of-way limits. The overlay should be performed as soon as possible to prevent damage to the pavement structure. The Contractor will be responsible for any failures that occur in the pavement structure due to delays in overlaying the roadway.

Potholes that may exist or occur in the existing pavement are to be patched in a timely manner and prior to beginning the asphalt overlay. Patching of potholes shall be considered an absorbed pay item.

Prior to opening lane to traffic, all existing material shall be bladed back to the edge of pavement. All existing material generated from construction shall be used prior to the placement of Class 5 Group E Granular Material.

Temporary pavement transition joints (paper joints) shall be at least three (3) paper widths long, shall be used at milled tie-ins, and shall be adequately maintained. Sand or dirt from the edge of roadway shall not be used as a substitute for treated paper.

Privately owned entrances shall be paved to the shoulder line per the included typical drawing. The asphalt thickness shall be consistent with that of the surface lift. All existing driveway pads shall be overlaid. Existing pads will be milled at a depth of 1½” and paved with 2” of 12.5-mm, ST, asphalt. Additional pads shall be placed at locations as directed by the Engineer. Pads shall be shaped horizontally and vertically to prevent excessive drop-offs. Grading for the placement of new pads shall be done as required. Grading for new pads will not be paid separately. The excavated material shall be retained and used to raise the existing shoulder to match the new pavement elevation. Granular material (Class 5, Group E) shall be provided around the pads to prevent shoulder drop-offs as directed and shall be placed in a timely manner. Drop-offs exceeding 2.5” shall be corrected within two (2) calendar days of the placement of the pad. Stabilizer aggregate shall be used as directed by the Engineer.

It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged during the life of the contract. No payment will be made for replacement or repair of damaged items.

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost is to be included in the price bid for Pay Item No. 907-618-A, Maintenance of Traffic unless shown on the Construction Signing Schedule. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background. Standard roadside construction signs and barricades will be paid for using the appropriate pay items.

Roadside construction signs, barrels, etc. shall be placed in accordance with the attached drawings or as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed.

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

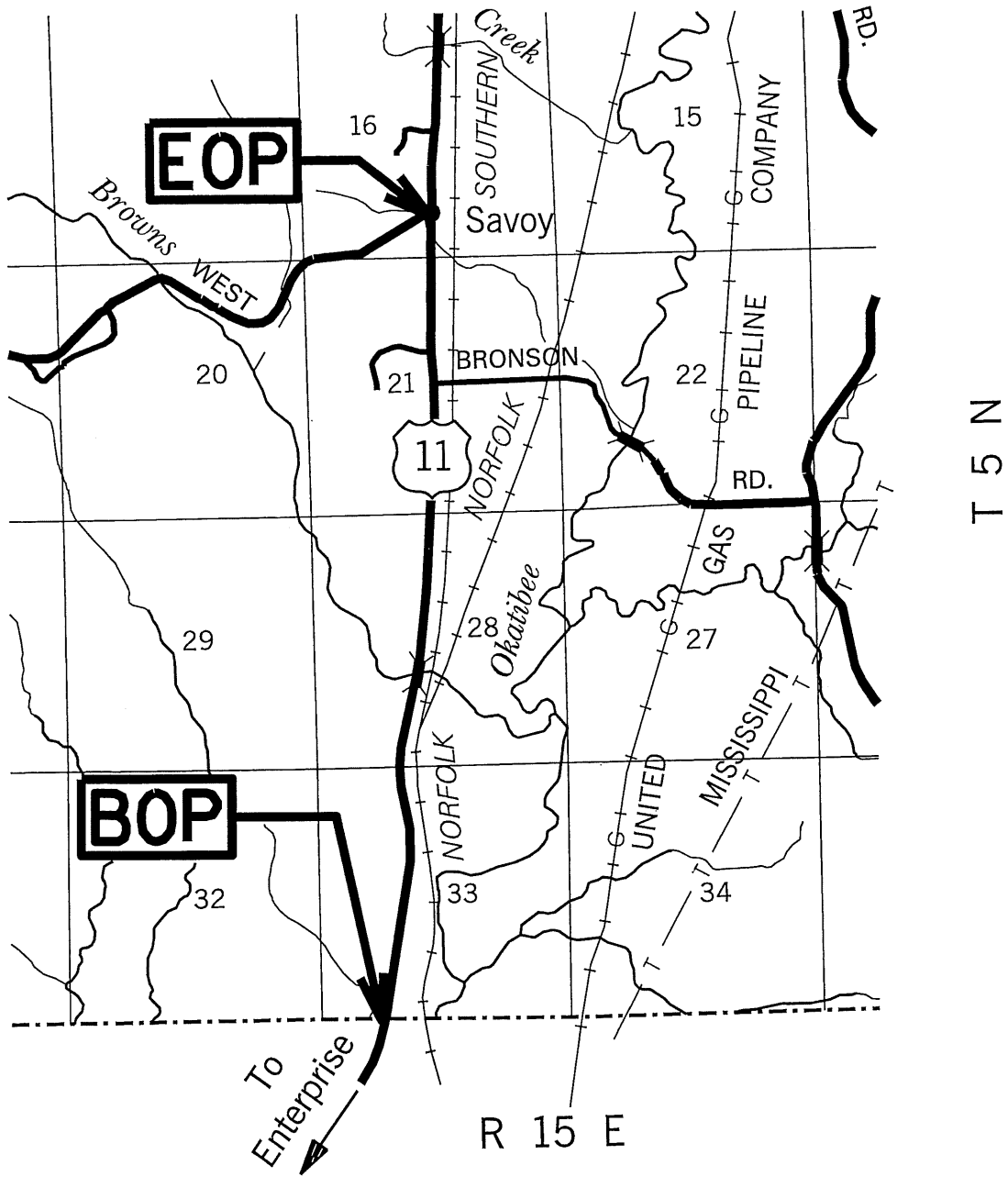
The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal; the cost is to be included in the prices of items bid. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor. As described in the applicable Notice To Bidders, final project cleanup is required and will be completed immediately prior to the final inspection.

Where applicable the existing shoulders are to be raised to match the new pavement elevation by placing variable depth Granular Material (Class 5, Group E) on the existing shoulders once all existing material has been utilized. Placement of the granular material on the finished asphalt course shall not be permitted. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%). Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%). The cost of blading will be an absorbed item and is not to be included in the price of pay items bid.

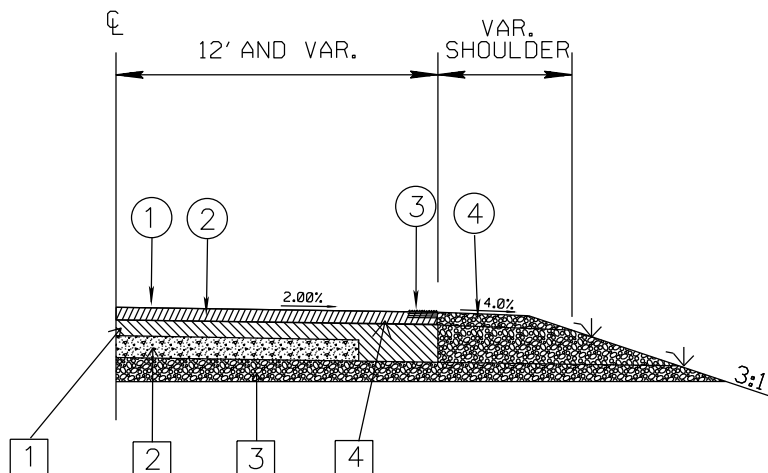
Permanent pavement markers are to be placed in accordance with the attached drawings and Standard Drawings. Two-way yellow markers are to be placed on two-way roads and County Roads. Two-way clear markers are to be placed on County Roads as per the attached detail.

All permanent striping will be thermoplastic. Edge lines shall be placed to accommodate the lane widths shown on the applicable typical sections unless prevented by field conditions. Rumble stripe will be placed from Station 1+99 to 175+90 (E.O.P.) in accordance with the attached detail. Permanent striping on County Roads shall be placed in accordance with the attached drawings or as directed by the Engineer.

MP-5011-38(008)
305286301
LAUDERDALE CO, MS



TYPICAL SECTION STATIONS 1+99 TO 175+90



Existing

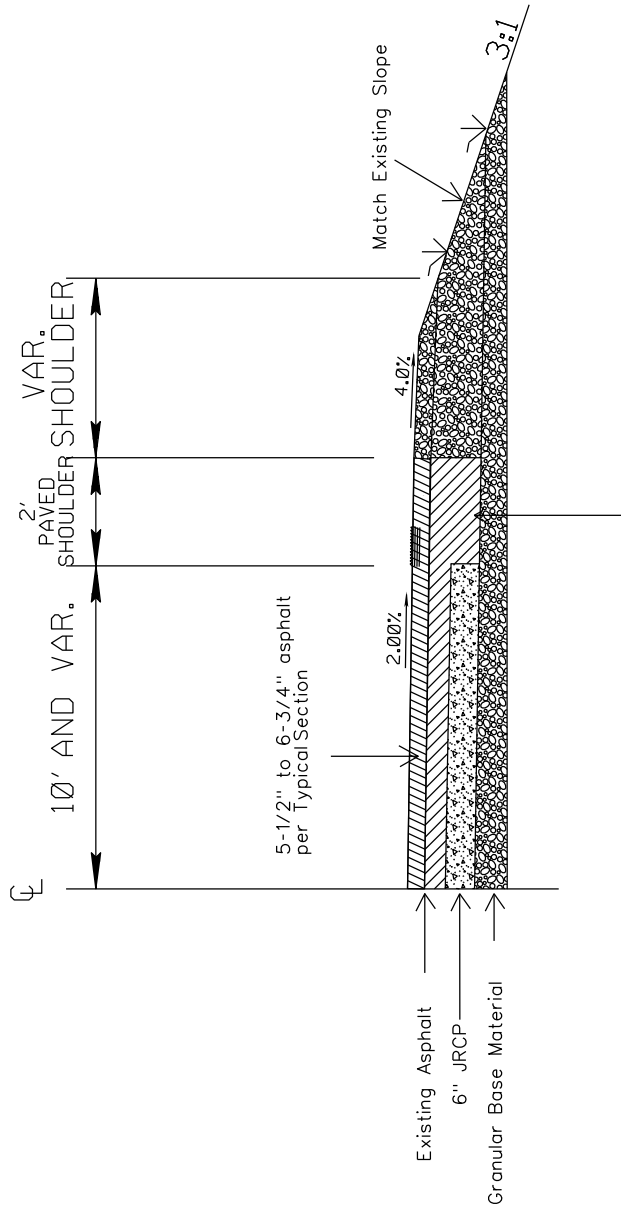
- ① 3-3/4" to 8" and variable of HMA
- ② 6" of Jointed Reinforced Concrete Pavment (JRCP)
- ③ Variable depth Class 5, Group "C" Granular Material
- ④ Existing 1-1/2% Slope

PROPOSED

- ① Mill 1.5" and variable depth.
Place with 12.5mm M_{1x}, ST of variable asphalt pavement, (2-1/4" on Centerline and 1-1/2" at edge of pavement of travel lane or as required to obtain 2% slope at areas that require cross-slope correction)
- ② Repair failed JRCP joints full depth to 3' wide on either side of joint, and place 12.5mm M_{1x}, ST Leveling asphalt pavement. (See tables for locations)
- ③ Place Rumble Stripe on 2' Widening
- ④ Variable depth granular material (Class 5, Group "E") as required.

TYPICAL REPAIR OF FAILED SHOULDER

STATIONS 1+99 TO 175+90

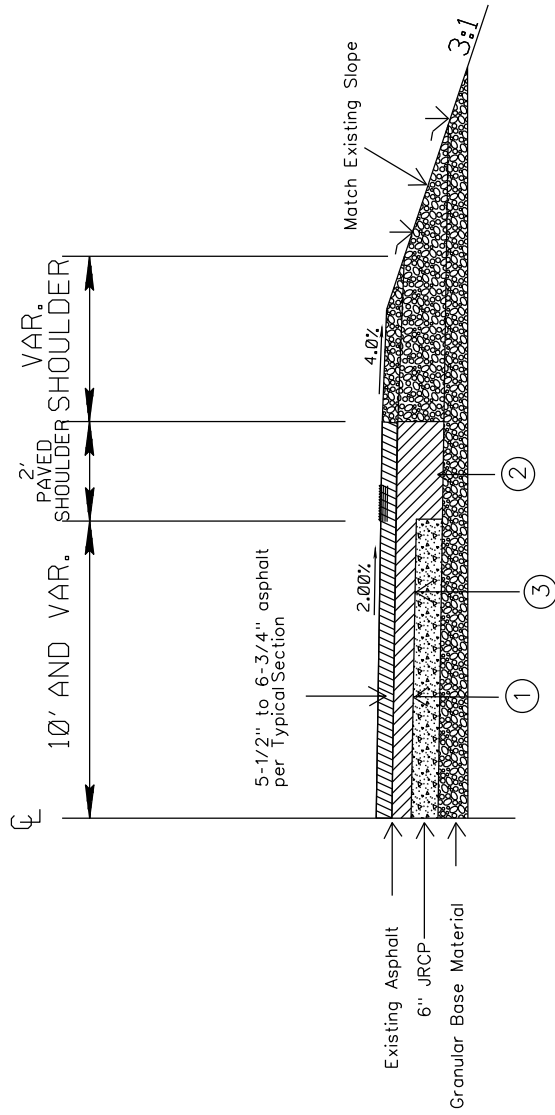


Excavate Existing Granular Material
3" and place 12.5mm, Mix ST Leveling

Note:

In failed areas as determined by the Engineer, excavate existing asphalt widening plus 3" of the underlying base material. Replace with 12.5mm, Mix, ST asphalt pavement to the bottom of the surface lift.

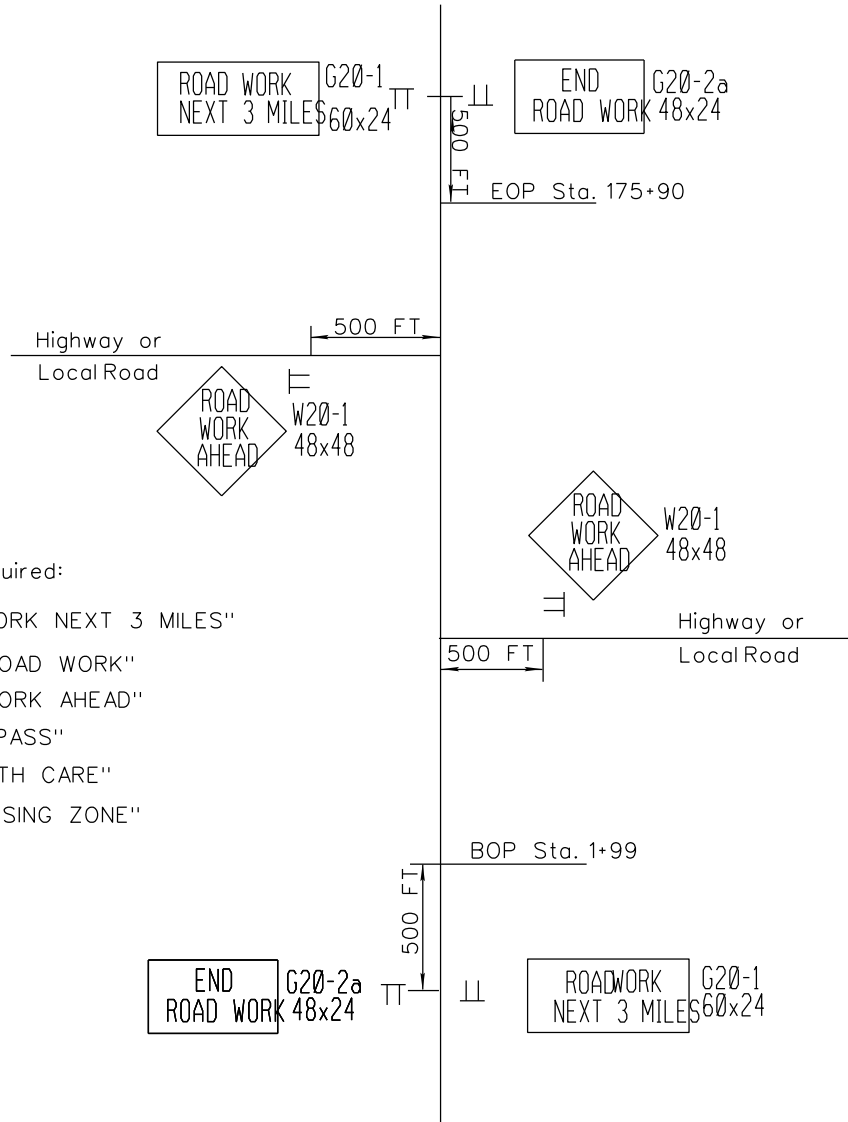
TYPICAL REPAIR OF FAILED JOINTS STATIONS 1+99 TO 175+90



- ① Remove JRCP overlayed with asphalt.
- ② Remove asphalt on 2' wide shoulder.
- ③ Backfill with HMA or WMA 12.5 mm Mix, ST Leveling

NOTE:
Any granular base material deemed unsuitable by the Engineer shall be removed and replaced as directed

CONSTRUCTION SIGNING DETAIL



Traffic Control Signs Required:

- 2 - G20-1 "ROAD WORK NEXT 3 MILES"
- 2 - G20-2a "END ROAD WORK"
- 3 - W20-1 "ROAD WORK AHEAD"
- 21 - R4-1 "DO NOT PASS"
- 3 - R4-2 "PASS WITH CARE"
- 4 - W14-3 "NO PASSING ZONE"

NOTES: One (1) W20-1 "ROAD WORK AHEAD" Sign is Required at each Local Road, street or highway Entering the Project.

Location of additional W20-1 (ROAD WORK AHEAD) signs are shown on the attached table.

G20-1 and G20-2a signs mounted on Type III Double Faced Barricade. Placed a minimum of 500' before the BOP and Eop.

R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE", and W14-3 "NO PASSING ZONE" signs are required in accordance with subsection 618.03.3 and is specified in the MUTCD. If No Passing Zones are 1000 ft. or more, install additional "DO NOT PASS" signs on maximum spacing of 750 ft.

FIELD CONDITIONS MAY REQUIRE SOME SIGNS ON THIS DETAIL TO BE ADJUSTED.

The above shown items will be paid under the appropriate pay items.

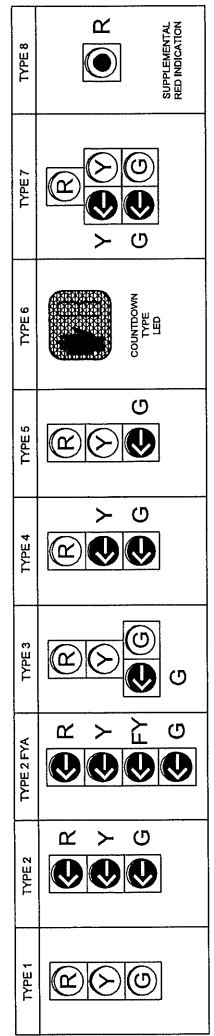
STATE	PROJECT NO.
MISS.	

GENERAL NOTES

- IF IT IS NECESSARY TO RUN ELECTRICAL CABLE FROM ONE SIGNAL HEAD TO ANOTHER, THE CABLE SHALL BE PLACED IN A SEPARATE MESSENGER CABLE LOCATED 1 FOOT MIN. ABOVE THE SIGNAL CABLE.
- VEHICLE LOOP ASSEMBLIES SHALL BE INSTALLED IN THE TOP LAYER OF BINDER OR EXISTING SURFACE BEFORE THE FINAL SURFACE COURSE IS APPLIED.
- PEDESTRIAN PUSHBUTTONS AND SIGNS TO BE INCLUDED IN PAY ITEM FOR TYPE 6 HEADS AT NO ADDITIONAL COST. SIDE OF POLE LOCATIONS OF PUSHBUTTONS MAY BE FIELD ADJUSTED.
- FIELD DRILL AND TAP EXISTING POLES WHERE PEDESTRIAN SIGNALS AND PUSHBUTTONS ARE REQUIRED ON PLANS.
- REFER TO WORKING NUMBER TSD-1 "CONDUIT ENTRANCE DETAIL." FOR CONDUIT AND MESSAGING CABLE CONNECTIONS TO SIGNAL POLES OR CONTROLLERS.
- MESSENGER CABLE AND OTHER SUPPORTING DEVICES WHERE REQUIRED SHALL BE ABSORBED IN THE PAY ITEM FOR ELECTRIC CABLE.
- FOR PROTECTED/PERMITTED LEFT TURN PHASING: TYPE 7 OR 7A TRAFFIC SIGNAL HEADS (FIVE SECTION HEADS) SHALL OPERATE IDENTICALLY AND SIMULTANEOUSLY TO THE CIRCULAR INDICATORS FOR THE LEFT TURN PHASE. A GREEN ARROW OR YELLOW ARROW MAY BE DISPLAYED SIMULTANEOUSLY IN THE SAME FIVE SECTION HEAD. FOR TYPE 2 FVA TRAFFIC SIGNAL HEADS, OPERATION SHALL BE AS FOLLOWS: THE PROTECTED PHASE OF THIS OPERATION SHALL DISPLAY A SOLID GREEN ARROW, FOLLOWED BY A SOLID YELLOW ARROW, AND ENDING WITH A SOLID RED ARROW. THE PERMITTED PHASE SHALL DISPLAY A SOLID RED ARROW, FOLLOWED BY A SOLID YELLOW ARROW AND ENDING WITH A SOLID GREEN ARROW. THERE SHALL BE A DELAY (AS DIRECTED BY THE ENGINEER) BETWEEN THE END OF THE PROTECTED PORTION OF THIS OPERATION AND THE BEGINNING OF THE PERMITTED PORTION OF THIS OPERATION DURING THIS DELAY. THE OPPOSING PHASE THIRD HEADS ARE CAPABLE OF DISPLAYING A GREEN ARROW. THE ENGINEER SHALL DETERMINE WHETHER NECESSARY TO ACCOMPLISH THIS DELAY SHALL BE PROVIDED.
- CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING TEMPORARY TRAFFIC CONTROL AND SIGNALING FOR THE PROJECT AND SHALL BE PAID FOR UNDER PAY ITEM 614-41, TRAFFIC SIGNALS.
- CONTRACTOR IS RESPONSIBLE FOR SCHEDULING FINAL INSPECTION AND SIGNING OF THE PROJECT AND SHALL BE PAID FOR UNDER PAY ITEM 614-41, TRAFFIC SIGNALS.
- CONTRACTOR IS RESPONSIBLE FOR SCHEDULING FINAL INSPECTION AND SIGNING OF THE PROJECT AND SHALL BE PAID FOR UNDER PAY ITEM 614-41, TRAFFIC SIGNALS.
- EMERGENCY VEHICLE PREACTION (WHERE REQUIRED ON THE PLANS) SHALL HAVE A SECURITY ENABLED PHASE SELECTOR.

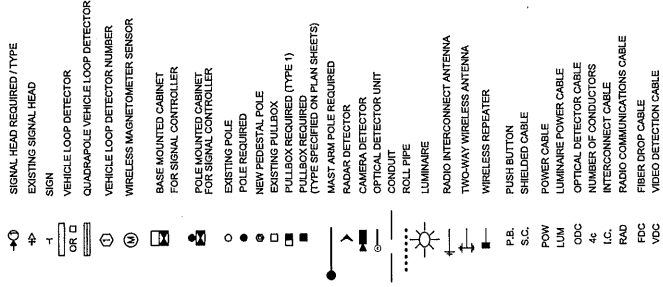
- INTERCONNECT CABLE SHALL BE EITHER IMSA 402 OR IMSA 403 CABLE. THE NUMBER AND NUMBER OF CONDUCTORS AS SHOWN ON PLANS.
- SIGNAL SUPPLY CABLE SHALL BE IMSA 20-1-1891 SIGNAL CABLE. THE NUMBER AND NUMBER OF CONDUCTORS AS SHOWN ON PLANS.
- POWER SUPPLY CABLE SHALL BE IMSA 20-1-17 CONDUCTOR CABLE, STRANDED, AND NUMBER AS SHOWN ON PLANS.
- DETECTOR/SHIELDER CABLE SHALL BE IMSA 89-3 SIGNAL CABLE. THE NUMBER AND NUMBER OF CONDUCTORS, UNLESS OTHERWISE NOTED ON THE PLANS.
- POLES, SIGNAL HEADS, EQUIPMENT BOXES, PULLBOXES AND CONTROLLERS SHALL BE INSTALLED IN THE LOCATIONS AND DIRECTIONS SHOWN ON PLANS. HOWEVER, SIGNAL HEAD OR POLE LOCATIONS SHALL BE WITHIN REQUIREMENTS OUTLINED IN THE SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS AND HIGHWAY DESIGN AND OPERATIONAL PRACTICES RELATED TO HIGHWAY SAFETY.
- POLES AND FOUNDATIONS OF EXISTING SIGNAL INSTALLATIONS SHALL BE CUT OFF 6" BELOW GROUND OR REMOVED AND AREA RESTORED TO MATCH ADJACENT SURFACE AS DIRECTED BY THE ENGINEER.
- LOOP AMPLIFIERS SHALL BE REQUIRED AS SHOWN ON PLANS WHERE TWO OR MORE LOOPS ARE CONNECTED TO THE SAME CHANNEL. THEY SHALL BE WIRED IN SERIES.
- THE CONTRACTOR SHALL PROVIDE MOST ARM POLE DESIGN CERTIFICATION AND CALCULATIONS AS CONTAINED IN SECTION 201. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING MOST ARM POLES SHALL BE 2001 AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS AND HIGHWAY DESIGN AND OPERATIONAL PRACTICES. CATEGORY II DO NOT CONSIDER GALLONING OR TRUCK FORCES. BASED UPON MAPS IN THE 2007 AASHTO SPECIFICATION, USE UPSHELF TRUCK ARM.
- DETERMINATION OF REQUIRED SIZES, LENGTHS AND GAUGES OF CABLES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR IN ACCORDANCE WITH THE PLANS AND SECTION 722.02 OF THE STANDARD SPECIFICATIONS, UNLESS OTHERWISE SPECIFIED IN PLANS OR SPECIFICATIONS.
- THE TOP OF THE STRAIN POLE FOUNDATION SHALL BE 6" ABOVE THE GROUND. THE CONTRACTOR SHALL PROVIDE PILES TO BE DRIVEN TO THE REQUIRED DEPTH. THE CONTRACTOR SHALL PROVIDE VERTICAL CLEARANCE OF THE TRAFFIC SIGNAL HEADS WITHOUT EXTENDING THE FOUNDATION ABOVE THE GROUND LINE OF THE POINT WHERE THE POLE IS LOCATED, EVEN THOUGH THIS MAY BE BELOW THE FINISHED GRADE OF THE ROADWAY.
- ALL STRAIN POLES AT AN INTERSECTION SHALL BE THE SAME DIAMETER AND UTILIZE THE SAME BOLT CIRCLE SPACING.
- POLE FOUNDATIONS AND BASE MOUNTED CABINET FOUNDATIONS, GRADE SHALL BE ESTABLISHED TO .3" OF EDGE OF PAVEMENT ELEVATION AS DIRECTED BY THE ENGINEER.
- TRAFFIC SIGNAL CABINETS AND CONTROLLERS SHALL BE WIRED TO THE POWER COMPANY SERVICE POINTS IN ACCORDANCE WITH THE PHASE SEQUENCE DIAGRAM.
- ALL EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED AND ABSORBED. UNLESS OTHERWISE NOTED IN THE PLANS, THE EXISTING POLES, CABINETS, CONTROLLERS, TRAFFIC SIGNAL HEADS, AND OTHER DEVICES SHALL REMAIN ON THE EXISTING FOUNDATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PICKUP BY STATE FORCES OR AS NOTED ON PLANS. ALL OTHER SIGNAL EQUIPMENT SHALL BECOME THE PROPERTY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ELECTRICAL SERVICE FROM THE POWER COMPANY SERVICE POINT TO THE POWER COMPANY SERVICE POINT. THE CONTRACTOR SHALL RUN THE POWER COMPANY SERVICE POINT AERIAL TO THE SIGNAL POLE NEAREST THE CONTROLLER. THE SERVICE SHALL THEN RUN TO THE CONTROLLER AS SHOWN ON PLANS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DIRECTLY TO THE POWER SERVICE PANEL. THEN TO THE CONTROLLER. THE CONTRACTOR SHALL MAKE APPLICATION WITH THE POWER COMPANY IN ADVANCE OF NEEDING THE SERVICE. INSTALLATION OF NEW SERVICE POLE (IF NEEDED) IS THE RESPONSIBILITY OF THE CONTRACTOR AND IS COST ABSORBED.

DETAIL OF TYPICAL TRAFFIC SIGNAL HEADS

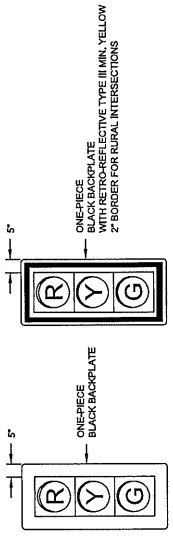


- NOTE:
- ALL SIGNAL HEADS SHALL BE BLACK IN COLOR UNLESS OTHERWISE NOTED ON THE PLANS.
 - ALL SIGNAL HEADS SHALL BE LEAD UNLESS OTHERWISE NOTED ON THE PLANS. TYPE 'X' SIGNAL HEAD IS TO BE OPTICALLY PROGRAMMED VIA LOWERS.
 - LETTER 'R' ON HEAD TYPES MEANS RIGHT TURN ARROWS.
 - SYMBOL NUMBERS SHALL BE FULLY ILLUMINATED (NO OUTLINE SYMBOLS ALLOWED).
 - TYPE 7 SIGNAL HEAD SHALL BE FURNISHED WITH R10-12 SIGN WHEN INDICATED ON PLANS.
 - TYPE 4 & 5 SIGNAL HEADS SHALL BE FURNISHED WITH R10-12 SIGN.
 - FOR SPAN WIRE INSTALLATION, THE HOUSING FOR THE RED INDICATION OF A TYPE 7 HEAD, SHALL BE ALUMINUM.

SIGNAL PLAN LEGEND

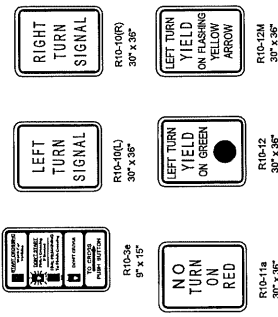


DETAIL OF TRAFFIC SIGNAL WITH BACKPLATE



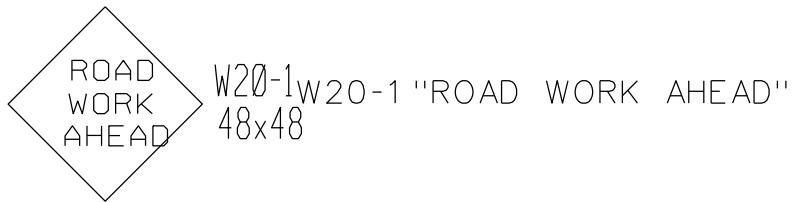
- NOTE:
- ALL SIGNAL HEADS SHALL INCLUDE BACKPLATES UNLESS OTHERWISE NOTED ON TRAFFIC SIGNAL INSTALLATION SHEETS.

DETAIL OF TYPICAL TRAFFIC SIGNAL SIGNS



- NOTE:
- ALUMINUM SIGN BLANKS ARE TO BE ALLOY 5052-H38 AND 0.08" (NOMINAL) THICK.
 - THE SIGNS SHALL BE SUPPLIED WITH MOUNTING BRACKETS AS REQUIRED.
 - NUMBER 12 PLATED JACK CHAINS SHALL BE ATTACHED TO THE BOTTOM OF ALL SPAN WIRE MOUNTED SIGNS.
 - CHAINS SHALL BE ATTACHED TO SIGN AND TETHER USING "S" HOOKS.
 - THE SIZE OF THE SIGN BLANK, LEGEND, BORDER AND THE COLOR OF THE BACKGROUND AND LEGEND IS TO CONFORM TO THE MUTCD.
 - THE BACKGROUND SHALL BE REFLECTORIZED USING TYPE IX SHEETINGS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
DETAIL OF TRAFFIC SIGNAL HEADS, TRAFFIC SIGNAL SIGNS, AND GENERAL NOTES	
PROJECT NO.:	
COUNTY:	
WORKING NUMBER	TSD-1
SHEET NUMBER	
DATE	
DESIGN TEAM	CHECKER
FILE NAME	
DATE	
7/10/17	
3/16/17	
REVISION	



HWY 11 from Clarke/Lauderdale Co. line to Savoy
Additional W20-1 (ROAD WORK AHEAD) Signs
"ONE" SIGN REQUIRED AT EACH COUNTY ROAD LISTED BELOW

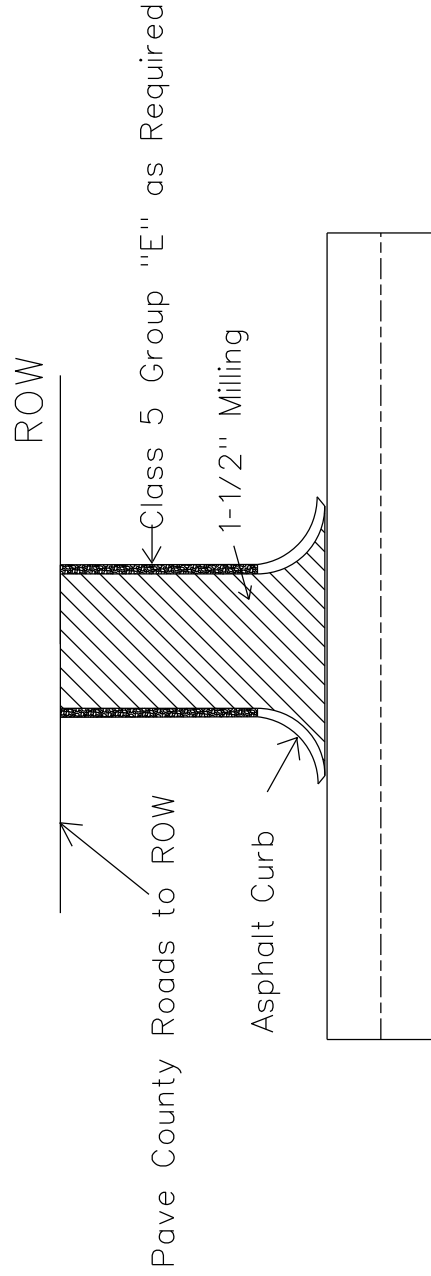
Bronson Rd.

Brown's Creek Rd.


Meehan Savoy Rd.

Failed Joint Locations				
LT LN	STA		RT LN	STA
	03+64			07+53
	08+85			08+85
	25+42			17+56
	28+28			22+70
	36+35			28+28
	36+56			53+75
	88+98			61+57
	103+50			92+98
	109+50			99+70
	118+70			109+50
	127+94			118+70
	128+15			140+00
	140+00			143+10
	145+75			145+75
	150+10			154+00
	150+30			158+02
	153+85			158+65
				160+06
				169+30

Milling and Paving Detail County Roads



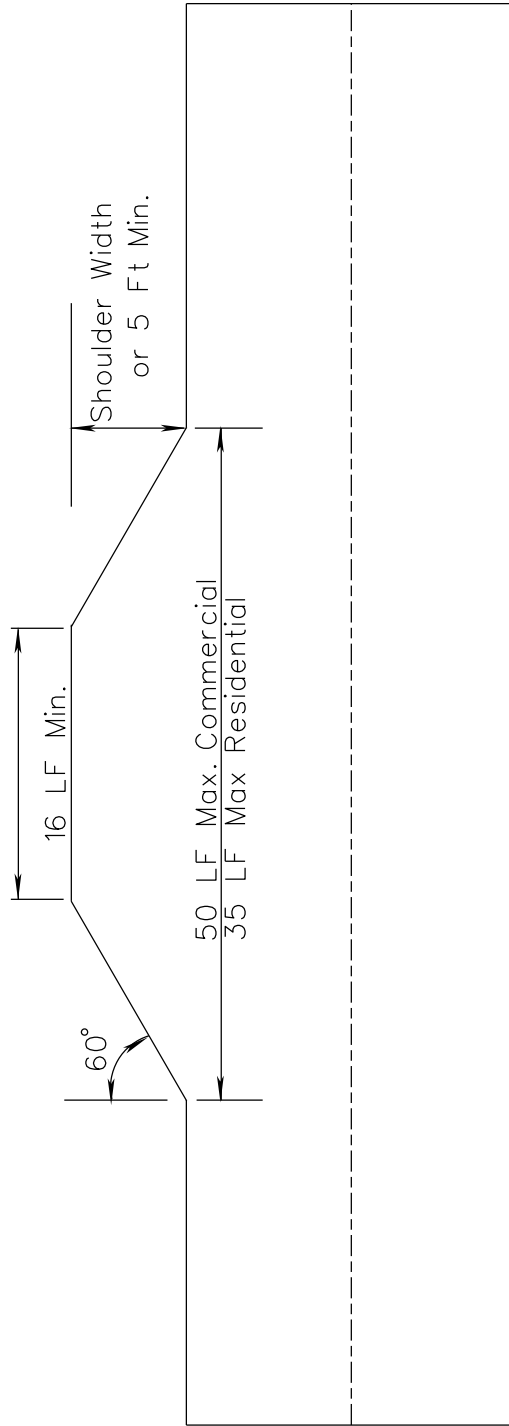
Notes:

- Mill limits of county/local roads at a depth of 1-1/2".
- Place 2" of 12.5mm, ST, Mixture to tie to mainline overlay.
- Milling/Paving area = 

County Roads:

- Bronson Rd.
- Brown's Creek Rd.
- Meehan Savoy Rd.

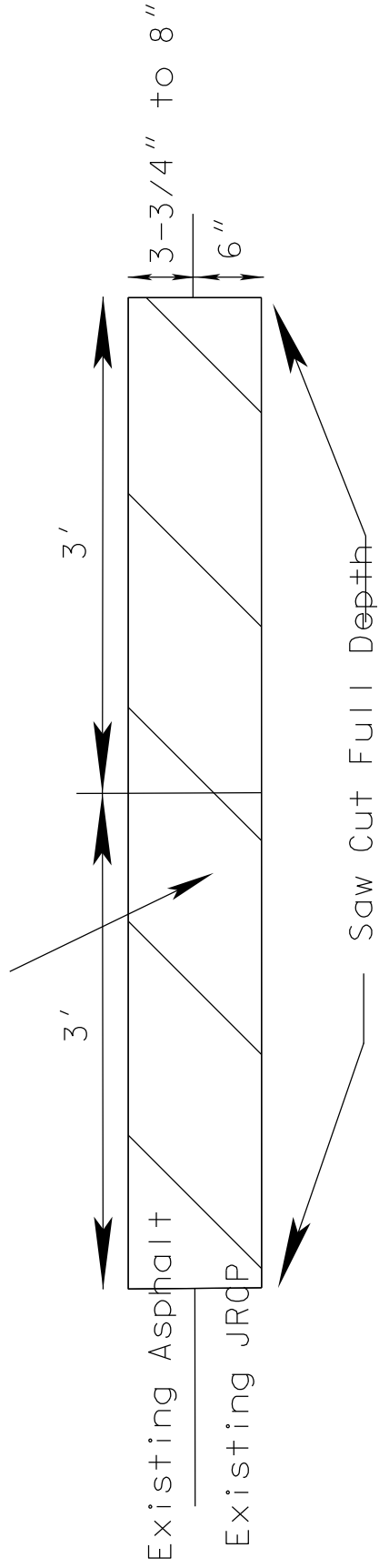
TYPICAL RAMP/PAD DETAIL



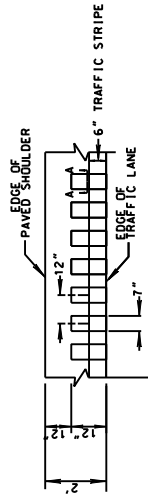
Lauderdale County
MP-5011-38(008)
305286/301000

Concrete Paving Repair Detail

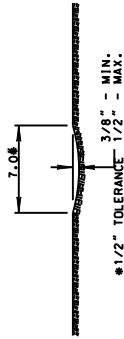
Replace with 12.5mm Asphalt Mix, ST



STATE	PROJECT NO.
MISS.	

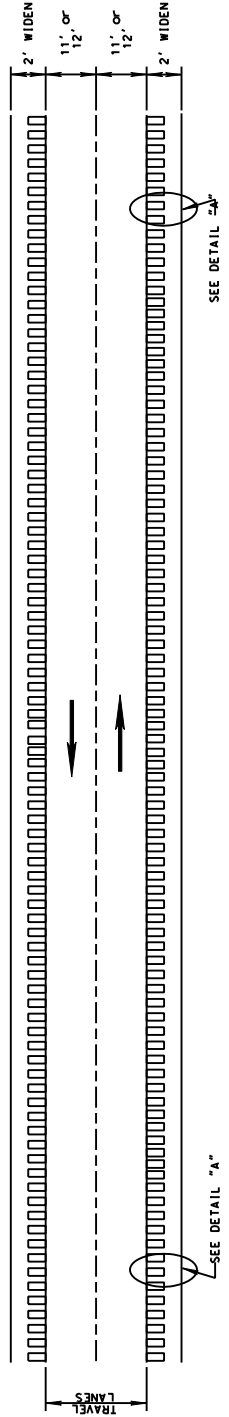


DETAIL "A"



SECTION "A-A"

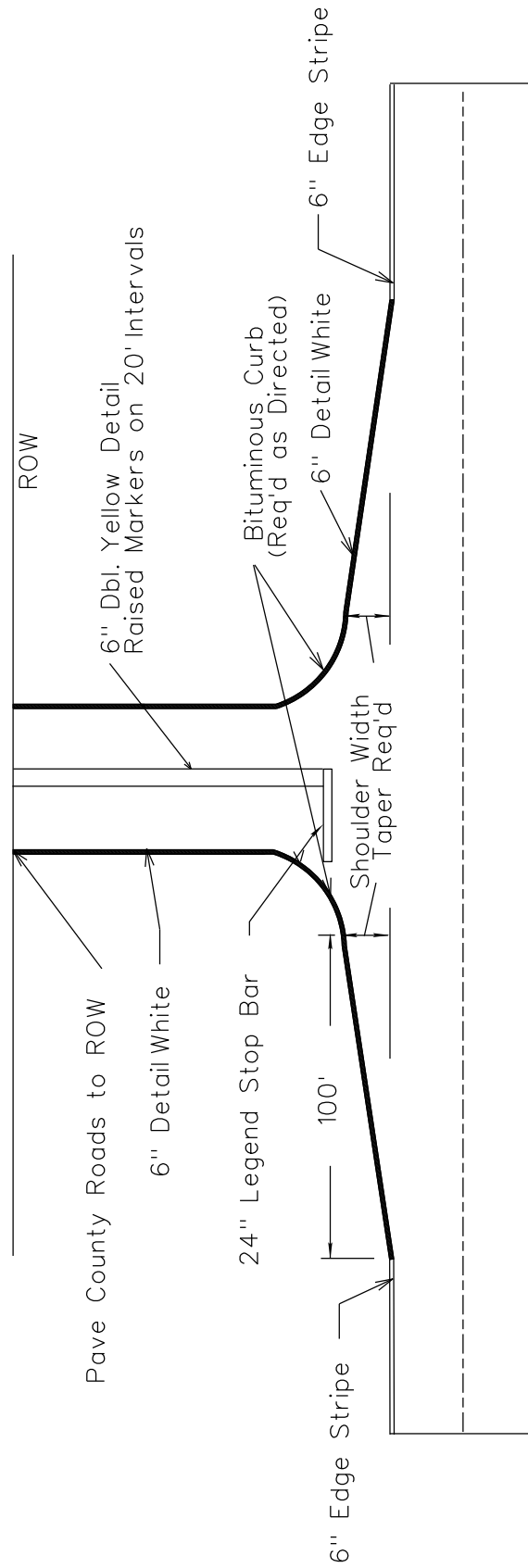
- GENERAL NOTES
1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT.
 2. GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS BRIDGES, INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER.
 3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS.
 4. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.
 5. DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.



PLAN
NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
SPECIAL DESIGN: RUMBLE STRIPES (GROUND-IN) 2 LANE	
WORKING NUMBER RS-2L	SHEET NUMBER
FILE NAME: RSM2L1M.L	DATE: 03/04/11
BY	REVISION

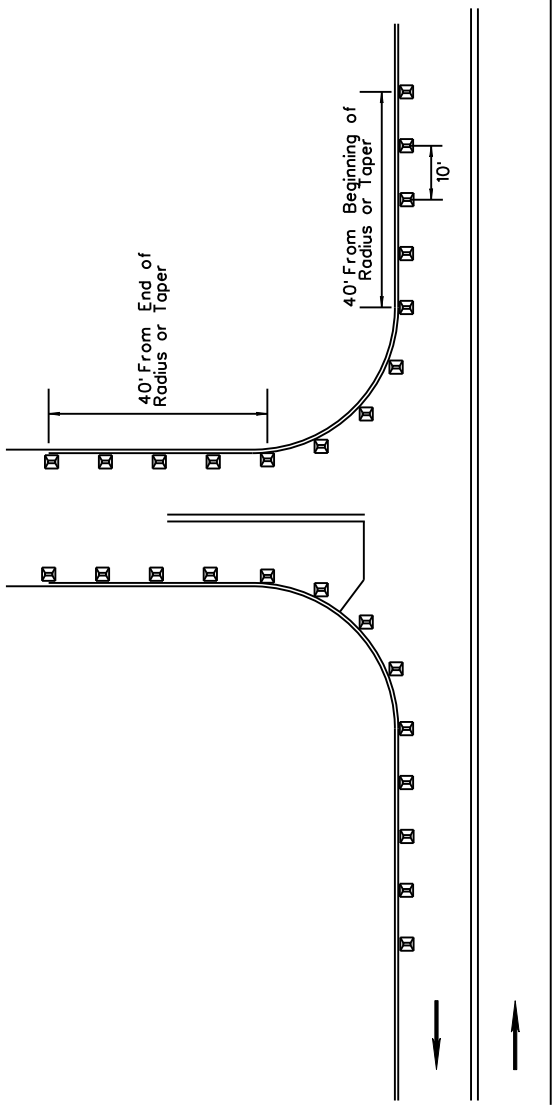
Typical Section - County Roads



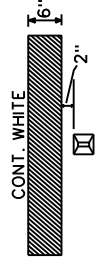
*SEE ATTACHED SHEET FOR TWO-WAY CLEAR RPM DETAIL

STATE	PROJECT NO.
MISS.	STP-0213-000026

TYPICAL FOR RAISED PAVEMENT MARKERS
 PLACED ON SIDE ROAD RADIUS
 2-LANE, 2-WAY TRAFFIC



- 18 -



DETAIL A

→ DIRECTION OF TRAFFIC

NOTE 1: MARKERS SHALL BE PLACED EVERY 10 FEET.

NOTE 2: MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.

NOTE 3: MARKERS SHALL BE HIGH PERFORMANCE TWO WAY CLEAR.

NOTE 4: FIVE (5) MARKERS SHALL BE PLACED ALONG MAINLINE EDGE STRIPE.

NOTE 5: MARKERS FOR COUNTY ROADS SHALL CONTINUE DOWN THE EDGE STRIPE A DISTANCE OF 40 FEET.

NOTE 6: MARKERS SHALL NOT BE ROTATED WHEN BEING PLACED ALONG RADIUS OF LOCAL ROAD.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
2-LANE, 2-WAY	
2-WAY CLEAR RAISED PAVEMENT MARKERS PLACED ON SIDE ROADS	
PROJECT NO.:	
COUNTY:	
FILE NAME:	SP5SIDEORRPM.DGN
DESIGNER:	
DATE:	
WORKING NUMBER:	CRPMSR-2
SHEET NUMBER:	

5079