

**SECTION 905 -- PROPOSAL (CONTINUED)**

I (We) hereby certify by execution of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.   1   DATED   7/21/2015   ADDENDUM NO.        DATED         
 ADDENDUM NO.        DATED        ADDENDUM NO.        DATED       

Number	Description
1	Revised NTB No. 5668; Amendment EBS Download Required.

TOTAL ADDENDA:   1    
 (Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE \_\_\_\_\_

\_\_\_\_\_  
 Contractor

BY \_\_\_\_\_  
 Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

\_\_\_\_\_  
 President Address

\_\_\_\_\_  
 Secretary Address

\_\_\_\_\_  
 Treasurer Address

The following is my (our) itemized proposal.

STP-0011-02(085) / 10709430 SFM-0011-02(085) / 107106301 Leflore County(ies)

Revised 06/2015

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 - NOTICE TO BIDDERS NO. 5668**

**CODE: (SP)**

**DATE: 07/21/2015**

**SUBJECT: Scope of Work**

**PROJECT: STP-0011-02(085) / 107094301000 – Leflore County  
SFM-0011-02(085) / 107106301000 – Leflore County**

The contract documents do not include an official set of construction plans but may, by reference; include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings". All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

The work to be accomplished using the Pay Items and corresponding specifications set forth in this contract, which is for milling and overlaying of US Highway No.82, beginning at a point 0.34 miles west of US Highway 49E and going easterly for approximately 4.916 miles to the US Highway 49 Overpass, and for milling and overlaying the frontage roads on the north and south side of US Highway 82 from US Highway 49E to US Highway 49 Overpass in Greenwood, Leflore County.

Bidders are advised that cross-slopes for curve superelevations are to be constructed in accordance with information provided by the Department. To assist the contractor in correctly placing the cross slope transitions, the Department will provide at the preconstruction conference the stationing and percent slope information.

It shall be the responsibility of the contractor to protect the roadway and all existing structures, such as bridges and curb, from damage occurring as a result of the contractor's operations. Damages to existing features caused by the contractor's operations shall be repaired or replaced at no cost to the Mississippi Department of Transportation.

At bridge ends and at the end of work day, a taper of one (1) vertical inch for each three (3) horizontal foot shall be provided.

The contractor shall make a utility location request to 811 prior to any excavation, except for trench widening or pavement removal/repair.

In order to expedite the safe movement of traffic and to protect each phase of the work as it is performed, a firm sequence of operations is essential. The work shall be begun and continually prosecuted.

The work shall consist of the following:

### **Section 1 - Frontage Roads:**

1. Cold milling of the existing asphalt pavement to a depth of one and one half (1½) inch and variable. The Reclaimed Asphalt Pavement (RAP) material removed by the milling operation shall become the property of the Contractor with the exception of 10,000 tons or 50% of the total anticipated quantity, whichever is less, and shall be delivered to a site in the median between the east and west bound lanes of US Highway 82 that is approximately 1/2 mile east of Airport Road. Sufficient advance notice shall be given to ensure that MDOT Maintenance personnel will be on hand to direct the delivery. The Contractor shall also provide MDOT

with an operator and the necessary equipment to stockpile the delivery. The cost of which shall be absorbed.

Area	Cold Milling of Bituminous Pavement (SY)
Mainline	112,000

Payment for Cold Milling of Pavement will be made under Pay Item no. 406-A, per square yard, and shall include all cost associated with the milling operation.

NOTE: Milled surfaces are to be covered with surface asphalt within 7 calendar days of removal. The Contractor will be charged a fee of \$5000.00 for each full or partial day in which the milled surface is left uncovered after the 7 calendar days.

NOTE: During this operation and prior to placement of the asphalt, due care shall be required to keep surface water from ponding on the roadway surface; continuous monitoring of the project may be required.

NOTE: During this operation and prior to placement of the asphalt, contractor shall maintain all pot holes.

2. Repair failed areas using the following:
  - 202-B, Removal of Asphalt Pavement, All Depths—for pavement structure
  - 202-B, Removal of Concrete Overlayed w/Asphalt Pavement, All Depths
  - 203-G, Excess Excavation –for material below the pavement structure
  - 907-304-F, Size 610 Crushed Stone Base
  - 907-403-A, Asphalt pavement, ST, 9.5 mm Mixture to replace failed areas
  - 503-C, Saw Cut, Full Depth

NOTE: Replacement shall be made same day as excavation

3. Placement of one and one-half (1 1/2) inches of surface course (ST, 9.5-mm,) on pavement previously milled.

Area	Asphalt Pavement ST, 9.5mm Mixture (tons)
Mainline	9,260

4. Placement of granular material on the shoulders as directed to raise the existing shoulders to the new surface course grade.

NOTE: Shoulders shall be bladed, shaped and compacted throughout the length of the project regardless of whether granular material is required.

NOTE: Granular material not required for the final shape of the shoulders may require removal under the pay item for excess excavation and may include small amounts of asphalt.

NOTE: Due care shall be taken during this operation to blade material to the roadway and away from the ditch line. Material inadvertently bladed to the roadway vegetation shall be removed at no cost to the Mississippi Department of Transportation.

5. Placement of Temporary Traffic Stripe daily as per Special Provisions Nos. 907-618.

- 6. Place permanent pavement markings as required (Thermoplastic Striping and Two-way Yellow Reflective High Performance Raised Markers).

**Section 2 – US Highway 82 (Eastbound and Westbound Lanes):**

- 1. Cold milling of the existing asphalt pavement to a depth of two and one half (2½) inch and variable on the mainline and one and one half (1½) inch and variable on other areas. It is the intent of this contract to mill the mainline section down to the concrete surface. The Reclaimed Asphalt Pavement (RAP) material removed by the milling operation shall become the property of the Contractor with the exception of 10,000 tons or 50% of the total anticipated quantity, whichever is less, and shall be delivered to a site in the median between the east and west bound lanes of US Highway 82 that is approximately 1/2 mile east of Airport Road. Sufficient advance notice shall be given to ensure that MDOT Maintenance personnel will be on hand to direct the delivery. The Contractor shall also provide MDOT with an operator and the necessary equipment to stockpile the delivery. The cost of which shall be absorbed.

Area	Cold Milling of Bituminous Pavement (SY)
Mainline	142,000
Frontage Road Entrances	9,900
Intersections	7,960
Turn Lanes	4,300
Crossovers	4,600
Paved Shoulders	54,740
Total	223,500

Payment for Cold Milling of Pavement will be made under Pay Item no. 406-A, per square yard, and shall include all cost associated with the milling operation.

NOTE: Mainline will be milled to the concrete surface.

NOTE: Milled surfaces are to be covered with surface asphalt within 7 calendar days of removal. The Contractor will be charged a fee of \$5000.00 for each full or partial day in which the milled surface is left uncovered after the 7 calendar days.

NOTE: During this operation and prior to placement of the asphalt, due care shall be required to keep surface water from ponding on the roadway surface; continuous monitoring of the project may be required.

NOTE: During this operation and prior to placement of the asphalt, contractor shall maintain all pot holes.

- 2. Repair failed areas using the following:
  - 202-B, Removal of Asphalt Pavement, All Depths—for pavement structure
  - 202-B, Removal of Concrete Pavement, All Depths

- 203-G, Excess Excavation –for material below the pavement structure
- 907-304-F, Size 610 Crushed Stone Base
- 501-D, Expansion Joints, With Dowels
- 907-503-A, 9” and Variable Reinforced Concrete Pavement, Broom Finished
- 503-C, Saw Cut, Full Depth
- 503-C, Concrete for Base Repair, to replace failed areas
- 503-E, Tie Bars, No. 5 Deformed Drilled and Epoxied or Grouted

NOTE: Replacement shall be made same day as excavation

Westbound Lane		
Station	Length	Width
32+80	6	24
60+75	12	12
148+25	8	24
166+00	8	24
178+50	10	24
188+50	3	24
206+00	10	24
215+20	10	24
249+75	20	12
250+25	2	12
270+80	12	24

Eastbound Lane		
Station	Length	Width
49+75	12	12
60+70	15	24
70+75	15	24
80+00	50	12
135+25	10	24
142+00	6	24
168+25	6	24
179+25	6	12
185+75	6	12
194+75	12	24
220+25	6	12
221+80	12	12
254+50	5	12
274+75	20	24

3. All concrete joints are to be cleaned and sealed prior to any asphalt being placed on the roadway surface. Joint cleaning and filling will be paid under Pay Item No. 907-413-D.
4. Remove railroad crossing at approximate mile station 16.2 from right-of-way to right-of-way limit. The removal shall consist of all rails and foundation. Payment for the removal of the crossing will be paid under 202-A, Removal of Obstruction. Backfill the void with six (6) inches of crushed stone and six (6) inches, in (2) lifts, of asphalt pavement (ST, 19mm). The contractor will deliver the salvaged material to the Greenwood Maintenance lot located at 6771 Highway 49 South, Greenwood.
5. Placement of three quarters of an inch (3/4”) and variable of 9.5-mm, HT, Asphalt, Leveling, for slope correction on the previously milled surface.

6. Placement of one and one-half (1 1/2) inches of surface course on pavement previously leveled.

Area	Type Mixture	Quantity (tons)
Mainline	HT, 9.5mm Polymer Modified	11,410
Frontage Road Entrances	HT, 9.5mm	847
Intersections	HT, 9.5mm	681
Turn Lanes	HT, 9.5mm	368
Crossovers	HT, 9.5mm	394
Paved Shoulders	ST, 9.5mm	3,803

NOTE: Contractor shall saw and seal the transverse joint on the mainline surface course.

7. Placement of granular material on the shoulders as directed to raise the existing shoulders to the new surface course grade.

NOTE: Shoulders shall be bladed, shaped and compacted throughout the length of the project regardless of whether granular material is required.

NOTE: Granular material not required for the final shape of the shoulders may require removal under the pay item for excess excavation and may include small amounts of asphalt.

NOTE: Due care shall be taken during this operation to blade material to the roadway and away from the ditch line. Material inadvertently bladed to the roadway vegetation shall be removed at no cost to the Mississippi Department of Transportation.

8. Placement of Temporary Traffic Stripe daily as per Special Provisions Nos. 907-618.

9. Placement of traffic signal loops at the following intersections:

US 49E, Wal-Mart, Park Avenue, Strong Avenue, Claiborne Avenue, Sycamore Avenue and Garrard Avenue. See attachment for details and locations.

Note: All splices shall be made in a junction box or an equivalent watertight enclosure. This item will not be paid for separately and shall be absorbed in other items of work.

10. Install rumble strips for rumble stripe on the outside of edge of the roadway.

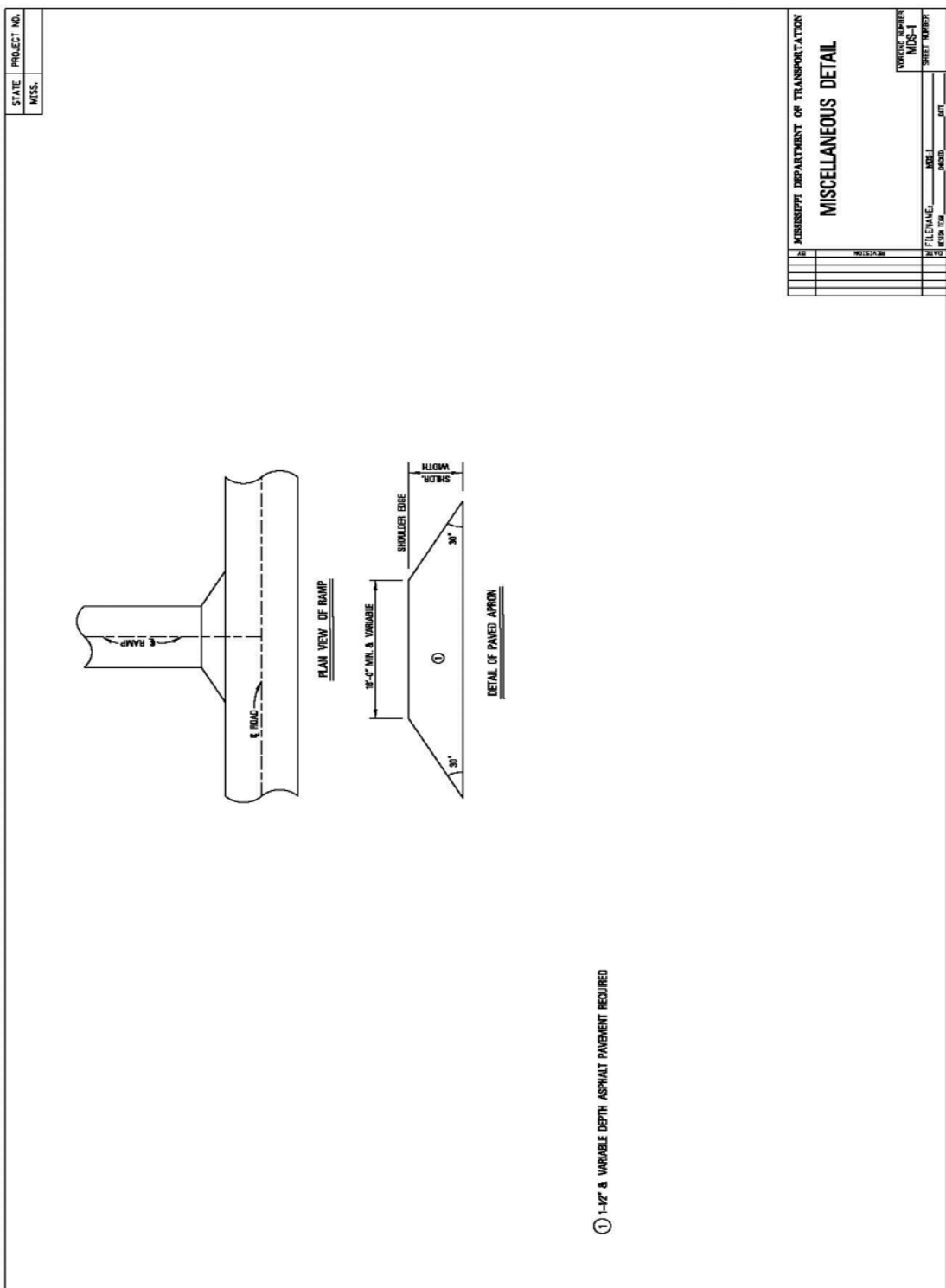
11. Place permanent pavement markings as required (Thermoplastic Striping, Red-Clear Reflective High Performance Raised markers, and Two-way Yellow Reflective High Performance Raised Markers). Placement of a six (6) inch thermoplastic stripe on the inside six (6) inches of the rumble stripe using an atomization method to create a "rumble strip."

12. Replace metal blockouts on existing guardrails. Payment for this work will be made under Pay Item No. 606-B, Guard Rail, Replace Metal Blockout, per Each. Replace terminal end sections. Payment for this work will be made under Pay Item No. 606-E, Guard Rail, Terminal End Section.

The contractor shall provide all signs and traffic handling devices necessary to safely maintain traffic around or through the work areas.

Incidental work such as removing vegetation, shaping and compaction of shoulder, necessary and incidental grading of roadway ditches and other incidental work that is necessary to complete the work will not be measured for separate payment and the cost will be included in the bid items provided.

The Engineer may direct the use of additional cones at County roads or intersections within lane closures and will be absorbed in Maintenance of Traffic.



STATE	PROJECT NO.
MISS.	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
<b>MISCELLANEOUS DETAIL</b>	
DATE	WORK NUMBER
REVISION	MDS-1
	SHEET NUMBER
DATE	DATE
FILE NAME	DATE
DESIGN TITLE	DATE



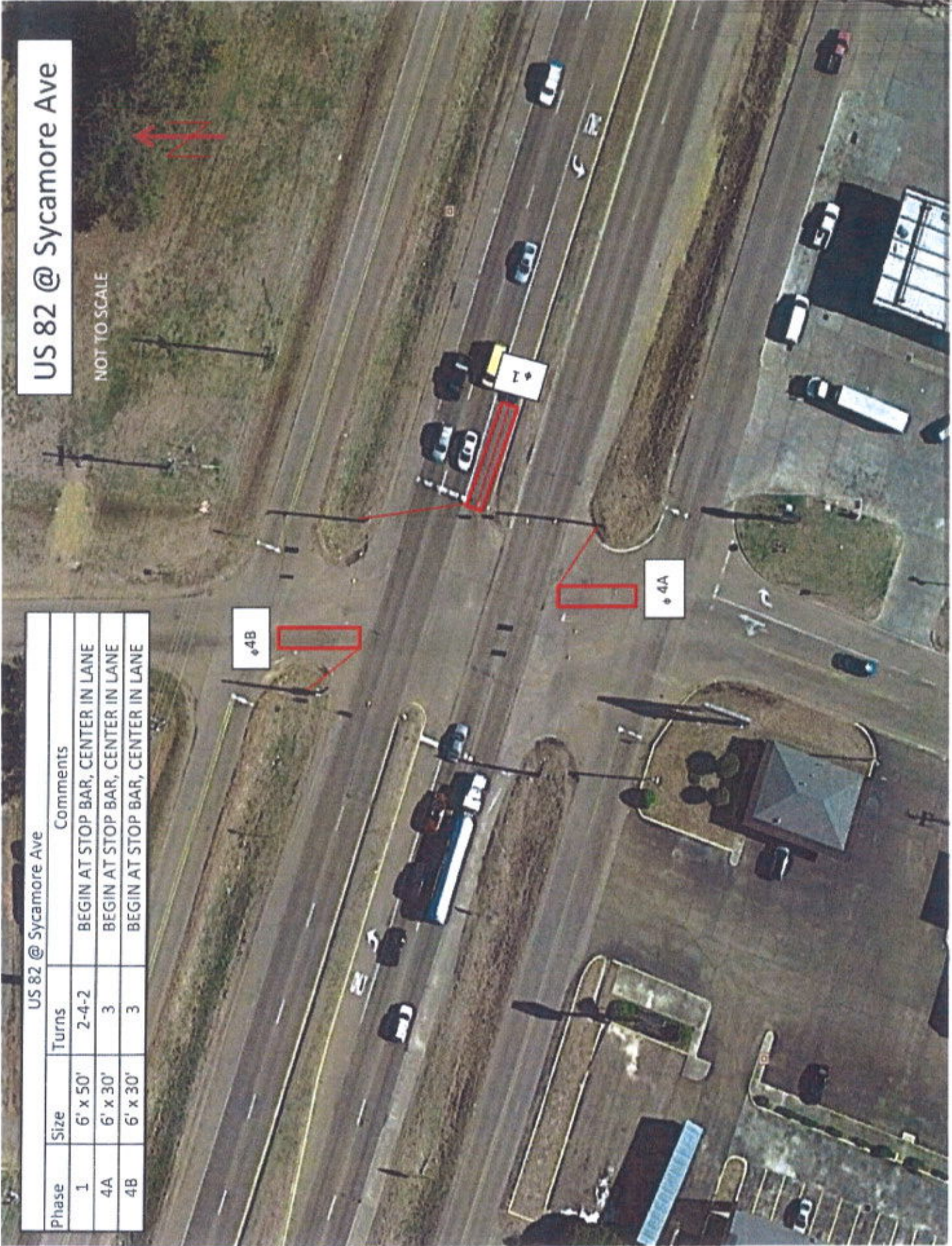


# US 82 @ Sycamore Ave

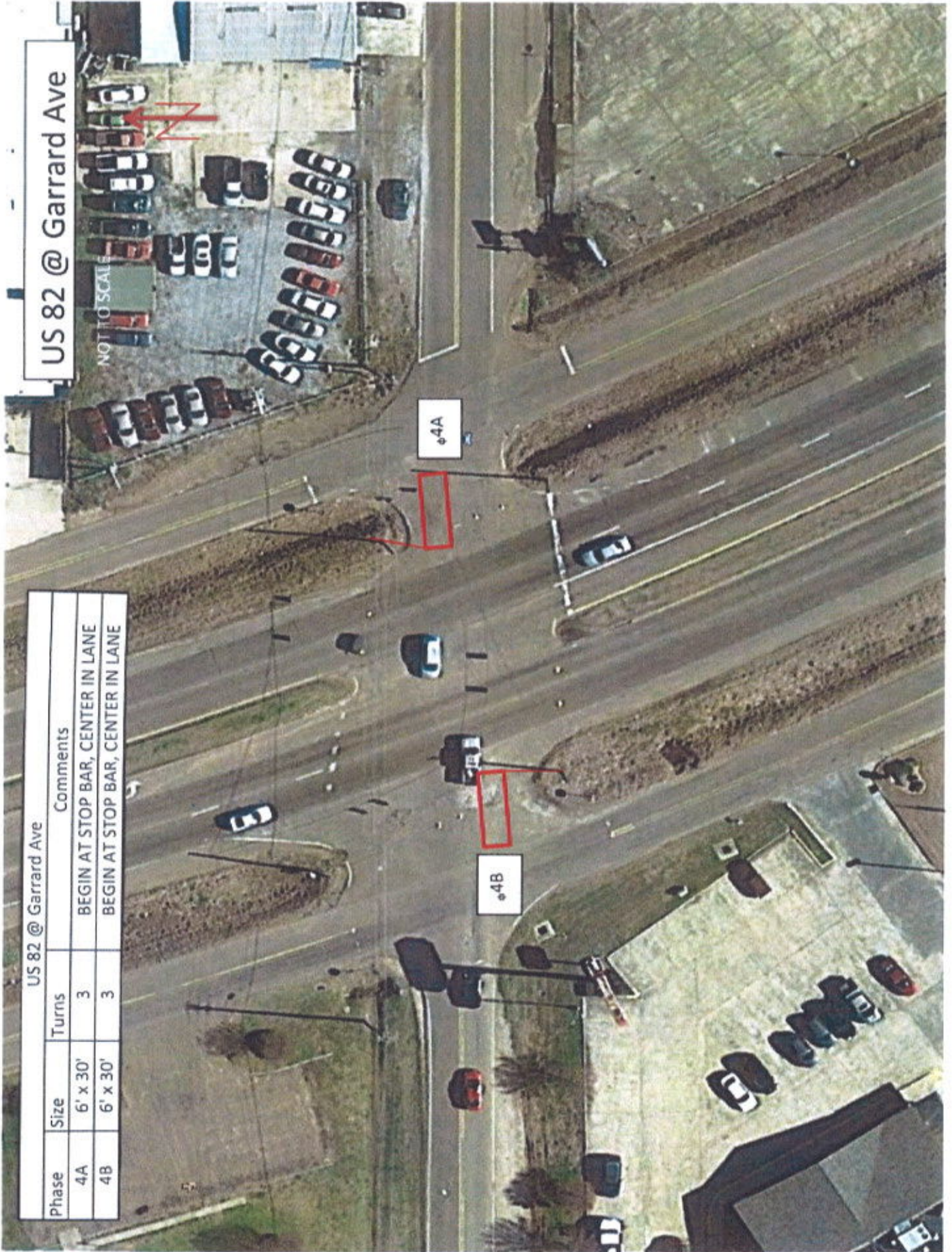
NOT TO SCALE



Phase	Size	Turns	Comments
1	6' x 50'	2-4-2	BEGIN AT STOP BAR, CENTER IN LANE
4A	6' x 30'	3	BEGIN AT STOP BAR, CENTER IN LANE
4B	6' x 30'	3	BEGIN AT STOP BAR, CENTER IN LANE

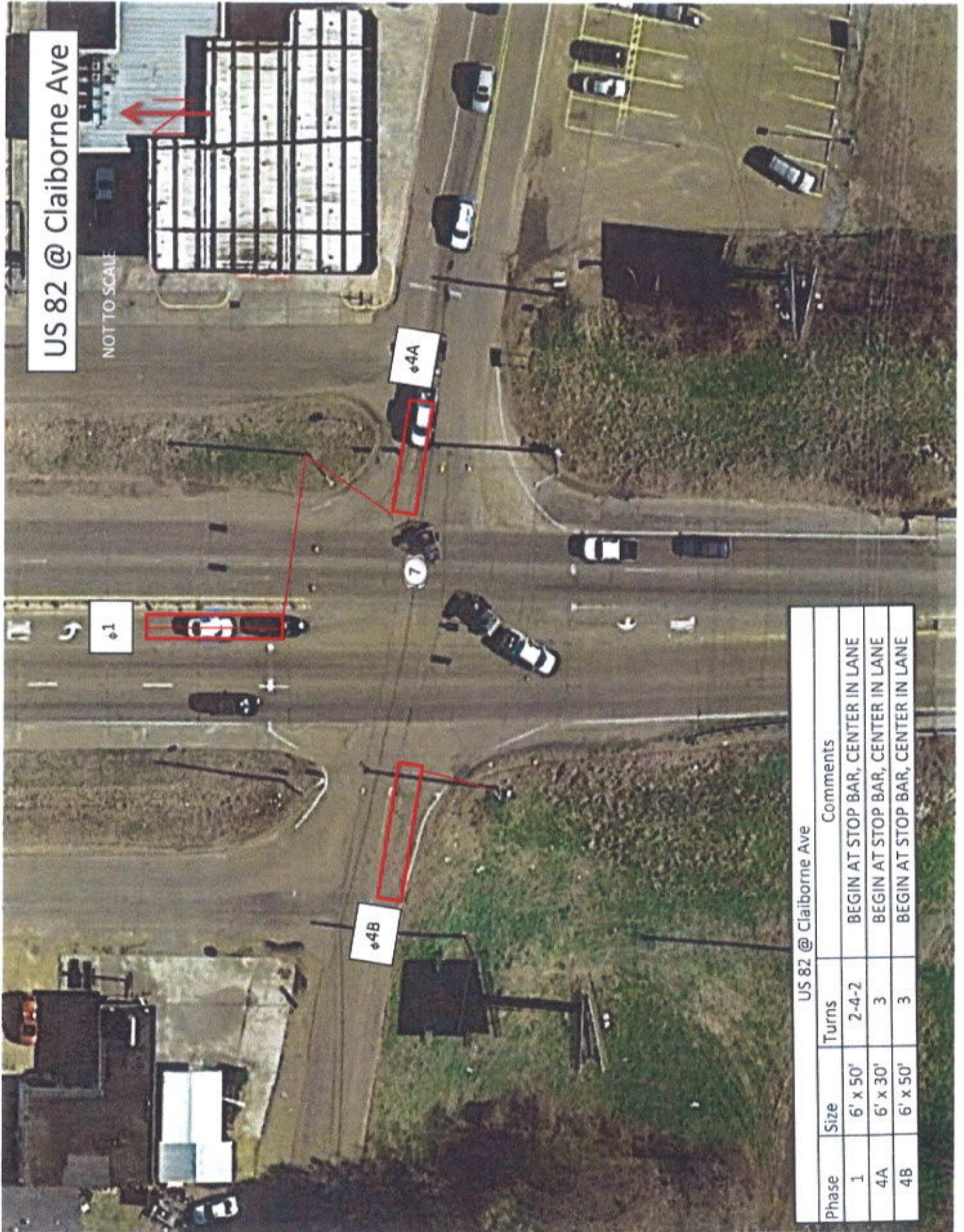




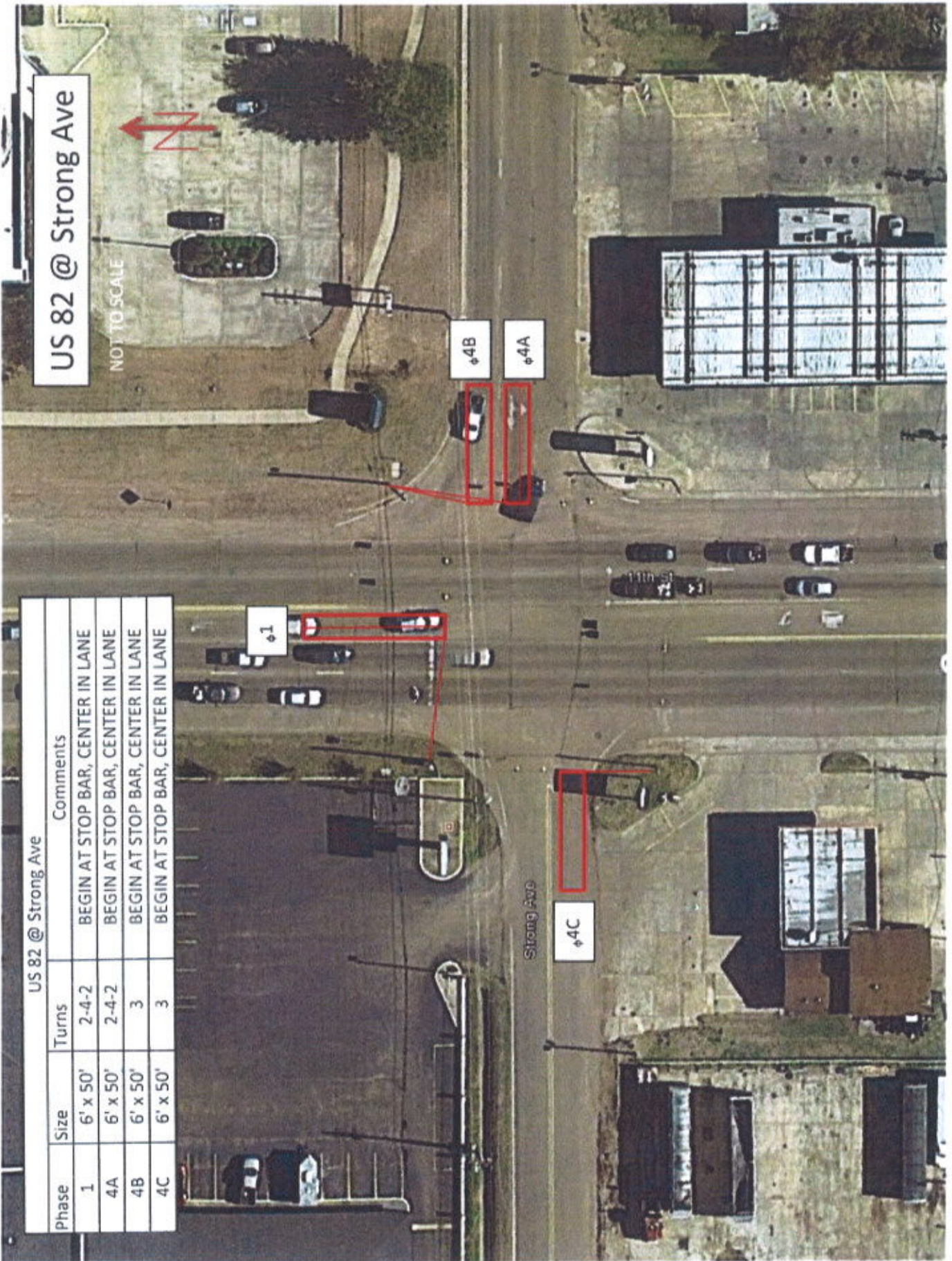


US 82 @ Garrard Ave			
Phase	Size	Turns	Comments
4A	6' x 30'	3	BEGIN AT STOP BAR, CENTER IN LANE
4B	6' x 30'	3	BEGIN AT STOP BAR, CENTER IN LANE









US 82 @ Strong Ave			
Phase	Size	Turns	Comments
1	6' x 50'	2-4-2	BEGIN AT STOP BAR, CENTER IN LANE
4A	6' x 50'	2-4-2	BEGIN AT STOP BAR, CENTER IN LANE
4B	6' x 50'	3	BEGIN AT STOP BAR, CENTER IN LANE
4C	6' x 50'	3	BEGIN AT STOP BAR, CENTER IN LANE

US 82 @ Strong Ave

NOT TO SCALE

φ1

φ4B

φ4A

φ4C

Strong Ave















NO.	REVISION

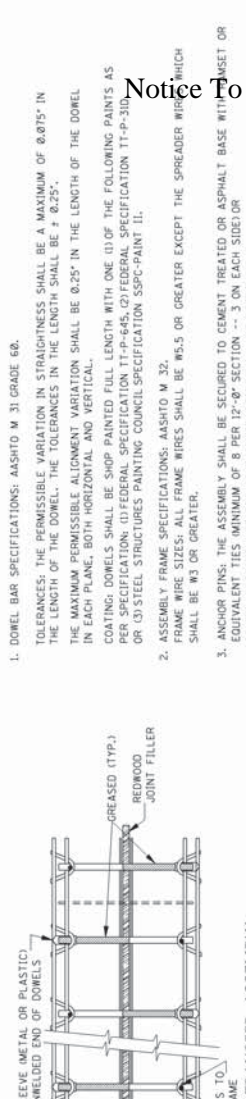
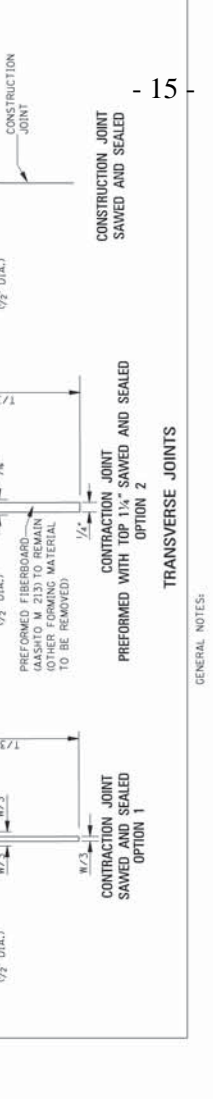
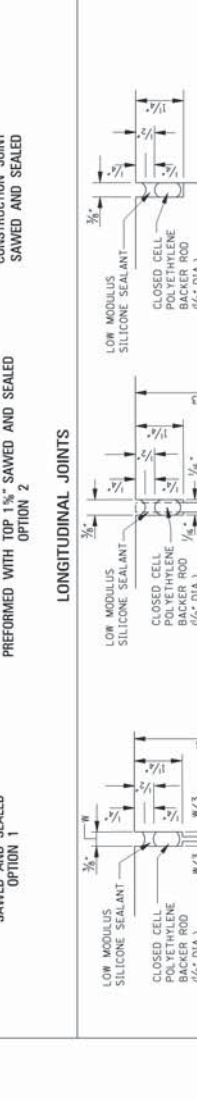
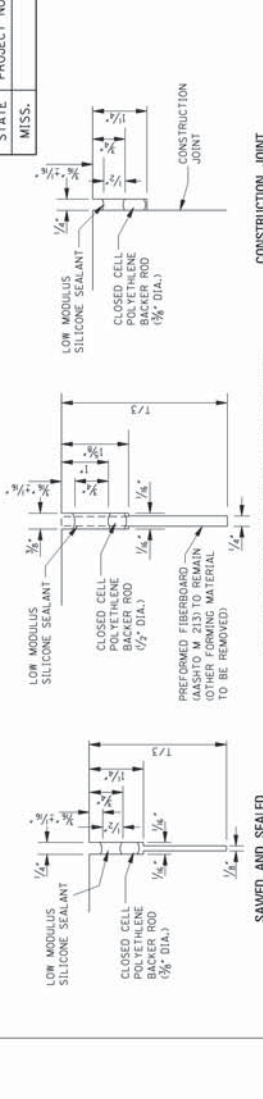
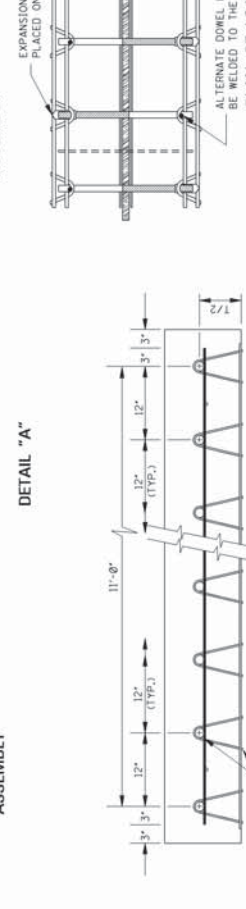
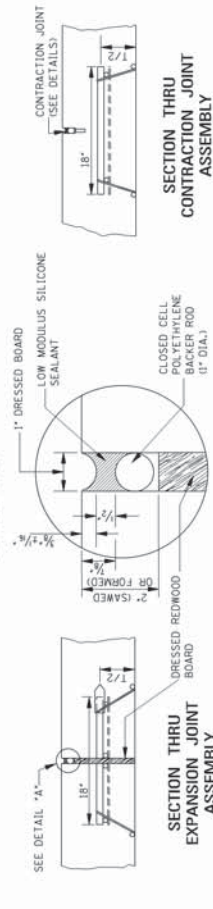
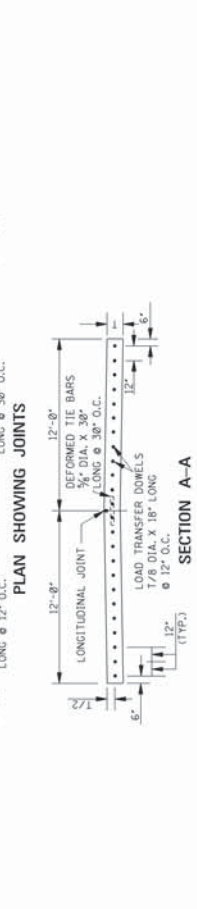
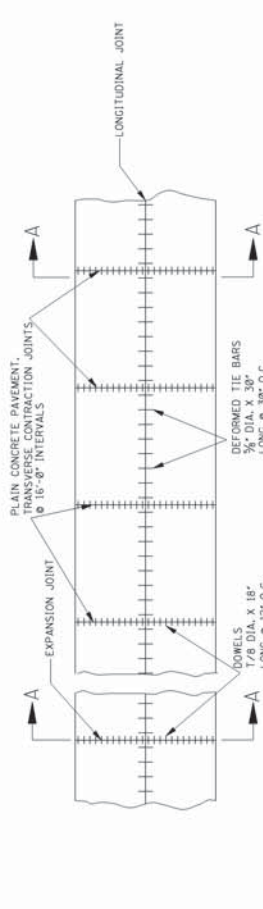


NOTE: FOR USE OF OTHER CHAIR SUPPORTS, SUBMIT DRAWINGS TO THE CONSTRUCTION ENGINEER FOR APPROVAL.

GENERAL NOTES:  
 1. THE LOT SIZE FOR CONFORMANCE DETERMINATION SHALL BE 20' OF PAVEMENT IN EACH LANE. CHAIR SPACINGS SHALL NOT BE GREATER THAN 42" CENTER TO CENTER (LONGITUDINAL) AND 24" (TRANSVERSE). ADDITIONAL CHAIRS SHALL BE USED IF NECESSARY TO MEET PLACEMENT REQUIREMENTS.



NOTE: LONGITUDINAL AND TRANSVERSE BARS SHALL BE SECURELY FASTENED TOGETHER BY ANY MEANS. AT INTERSECTIONS OF LONGITUDINAL AND TRANSVERSE BARS, THE BARS SHALL BE DOUBLE FASTENED. LONGITUDINAL BARS SHALL BE DOUBLE FASTENED TO ADJACENT BARS. ANY SATISFACTORY METHOD OR DEVICE FOR HOLDING THE BARS FIRMLY IN POSITION DURING THE PLACEMENT OF THE CONCRETE WILL BE ACCEPTABLE.



GENERAL NOTES:

1. DOWEL BAR SPECIFICATIONS: AASHTO M 31 GRADE 60. TOLERANCES: THE PERMISSIBLE VARIATION IN STRAIGHTNESS SHALL BE A MAXIMUM OF 0.075" IN THE LENGTH OF THE DOWEL. THE TOLERANCES IN THE LENGTH SHALL BE ± 0.25". THE MAXIMUM PERMISSIBLE ALIGNMENT VARIATION SHALL BE 0.25" IN THE LENGTH OF THE DOWEL IN EACH PLANE, BOTH HORIZONTAL AND VERTICAL. COATING: DOWELS SHALL BE SHOP PAINTED FULL LENGTH WITH ONE (1) OF THE FOLLOWING PAINTS AS PER SPECIFICATION: (1) FEDERAL SPECIFICATION TT-P-645, (2) FEDERAL SPECIFICATION TT-P-310, OR (3) STEEL STRUCTURES PAINTING COUNCIL SPECIFICATION SSPC-PAIN 11.
2. ASSEMBLY FRAME SPECIFICATIONS: AASHTO M 32. FRAME WIRE SIZES: ALL FRAME WIRES SHALL BE #5.5 OR GREATER EXCEPT THE SPREADER WIRE WHICH SHALL BE #3 OR GREATER.
3. ANCHOR PINS: THE ASSEMBLY SHALL BE SECURED TO CEMENT TREATED OR ASPHALT BASE WITH ANCHOR PIN EQUIVALENT TIES (MINIMUM OF 8 PER 12'-0" SECTION -- 3 ON EACH SIDE) OR OTHER APPROVED METHODS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SECURING THE ENTIRE ASSEMBLY IN SUCH A MANNER AS TO PREVENT DISPLACEMENT.
4. FOR CONSIDERATION OF USE OF OTHER LOAD TRANSFER ASSEMBLIES, THE CONTRACTOR SHALL SUBMIT DRAWINGS TO THE ENGINEER FOR APPROVAL.
5. PREFORMED FILLER IS NOT AN ALTERNATE FOR REDWOOD FILLER AT EXPANSION JOINTS UNLESS SPECIFICALLY REQUIRED ON THE PLANS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

**CONCRETE PAVEMENT JOINTS**

WORKING NUMBER  
PJ-1

SHEET NUMBER  
6104

ISSUE DATE: OCTOBER 1, 1998

DATE	REVISION