

STATE OF MISSISSIPPI MISSISSIPPI
DEPARTMENT OF TRANSPORTATION

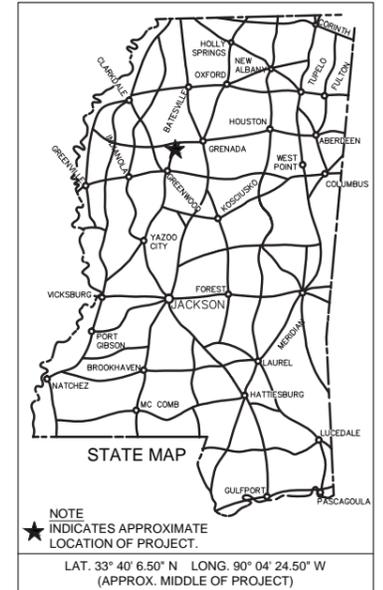
STREAM STABILIZATION AND
COUNTERMEASURES
STATE PROJECT NO. SP-0019-01(008)

BRIDGE NO. 49.4 AT SR 7 ACROSS POTACOCOWA CREEK
COUNTY: CARROLL

FMS 107035/301000

PLAN SCALES
1 IN. = 50 FT.

STATE	PROJECT NUMBER	SHEET NO.
MISS.	SP-0019-01(008)	1



GENERAL INDEX

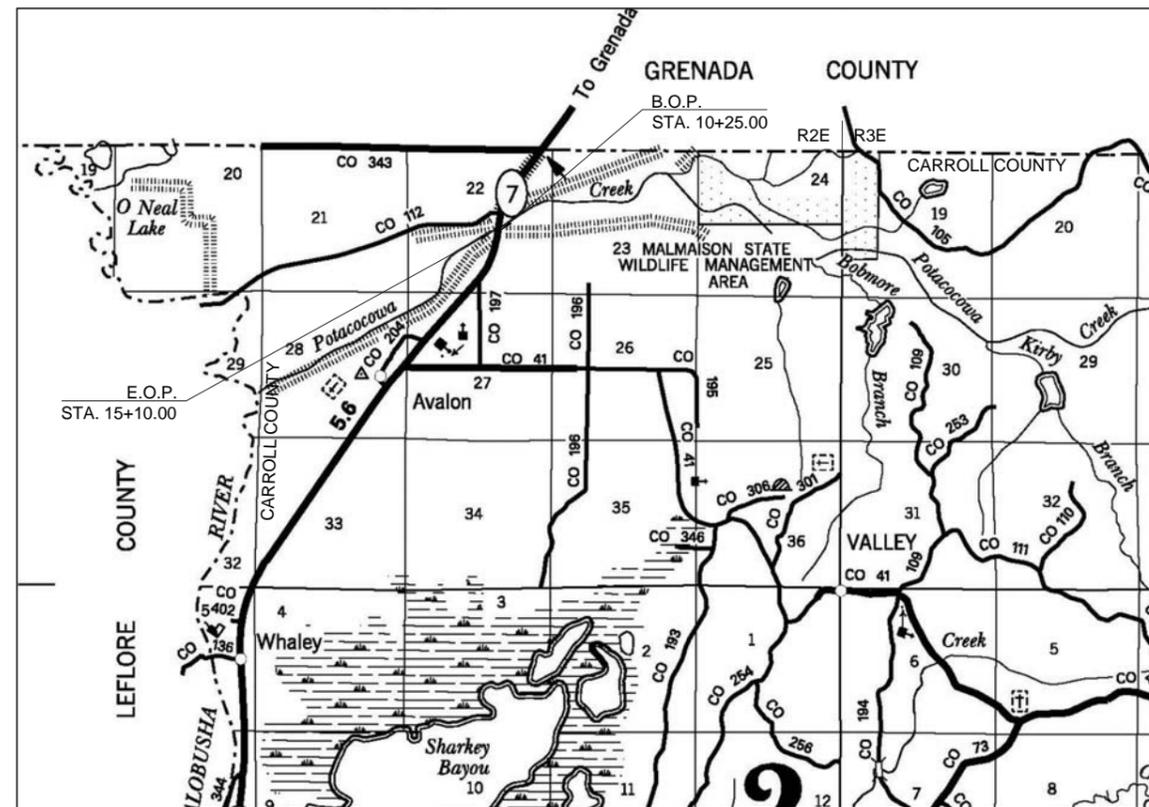
INCLUDED THIS PROJECT	BEGIN WITH SHEET
<input checked="" type="checkbox"/> ROADWAY.....	1
<input type="checkbox"/> PERMANENT SIGNS.....	1001
<input type="checkbox"/> TRAFFIC SIGNALS.....	2001
<input type="checkbox"/> ITS COMPONENTS.....	3001
<input type="checkbox"/> LIGHTING.....	4001
<input type="checkbox"/> (RESERVED).....	5001
<input checked="" type="checkbox"/> ROADWAY STANDARD DWGS.....	6001
<input type="checkbox"/> BRIDGE STANDARD DWGS.....	7001
<input type="checkbox"/> BRIDGE.....	8001
<input checked="" type="checkbox"/> CROSS SECTIONS.....	9001

BRIDGE STRUCTURES REQ'D.

N/A

BOX BRIDGES REQ'D.

N/A



EQUATIONS

N/A

EXCEPTIONS

N/A

LENGTH DATA

	FT.	MI.
LENGTH OF ROADWAY		
LENGTH OF BRIDGES		
LENGTH OF PROJECT (NET)		
LENGTH OF EXCEPTIONS		
LENGTH OF PROJECT (GROSS)		

GPS CONTROL NOTES

HORIZONTAL DATUM: NAD 83 (2011) MS WEST ZONE (US SURVEY FEET)

HORIZONTAL MONUMENT	NORTH	EAST
DF8976	2025632.886	2409688.234
DO9482	1397060.595	2080464.843
DO8516	1567547.096	2805466.939

VERTICAL DATUM: (US SURVEY FEET) NAVD 88

VERTICAL MONUMENT	ELEVATION
DK1750	138.00

ALL AZIMUTHS AND DISTANCES ARE GRID VALUES, US SURVEY FEET

CONVERSION VALUES	PROJECT AVERAGE
GROUND TO GRID (COMBINED) FACTOR	0.99995355
GRID TO GEODETIC AZIMUTH	0° 08' 39.15"

DESIGN CONTROL MPH

= V (SPEED DESIGN)

ADT () = : ADT () =

DHV = : D = % T = %

PERMITS ACQUIRED BY MDT

WETLANDS AND WATERS PERMITS NECESSARY FOR ULTIMATE IMPROVEMENTS ONLY:

	WATERS	WETLANDS
NATIONWIDE #14	<input type="checkbox"/> N	<input type="checkbox"/> N
NATIONWIDE (OTHER)*	<input type="checkbox"/> Y	<input type="checkbox"/> N
GENERAL*	<input type="checkbox"/> N	<input type="checkbox"/> N
INDIVIDUAL (404)*	<input type="checkbox"/> N	<input type="checkbox"/> N

* ACQUISITION OF PERMITS FOR TEMPORARY IMPACTS DURING CONSTRUCTION ARE THE RESPONSIBILITY OF THE CONTRACTOR

STORMWATER PERMIT S

Y REQUIRED, CNOI SUBMITTED BY MDT (DISTURBED AREA=5 ACRES)

S REQUIRED, SCNOI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES)

N NO STORMWATER PERMIT REQUIRED (<1 ACRE)

APPROVED BY: _____

P S & E DATE: 06/15/2015

APPROVED: _____

DEPUTY EXECUTIVE DIRECTOR / CHIEF ENGINEER

EXECUTIVE DIRECTOR



DESCRIPTION OF SHEET	REVISION DATE	WKG. NO.	SH. NO.
TITLE SHEET (1)		1	1
DETAILED INDEX (2)			
DETAILED INDEX		DI-1	2
GENERAL NOTES AND SCOPE OF WORK		GN-1	3
TYPICAL SECTIONS (1)			
TYPICAL SECTIONS		TS-1	4
QUANTITY SHEETS (1)			
SUMMARY OF QUANTITIES		SQS-1	5
PLAN AND PROFILE SHEETS (1)			
SITE PLAN		SP-1	6
SPECIAL DESIGN SHEETS (9)			
EROSION CONTROL PLAN		ECP-1	7
TYPICAL TEMPORARY EROSION / SEDIMENT CONTROL APPLICATIONS		ECD-1	8
DETAILS OF SEDIMENT BARRIER APPLICATIONS		ECD-2	9
DETAILS OF SILT FENCE INSTALLATION		ECD-3	10
DETAILS OF EROSION CONTROL WATTLE DITCH CHECK		ECD-6	11
STABILIZED CONSTRUCTION ENTRANCE		ECD-15	12
FLOATING TURBIDITY CURTAIN		ECD-19	13
EROSION CONTROL BLANKET		ECB-1	14
VEGETATION SCHEDULE		VS-1	15
ROADWAY STANDARD DRAWINGS (1)			
TRAFFIC CONTROL PLAN WITH FLAGGER (ONE-LANE CLOSURE OF TWO WAY TRAFFIC)		TCP-1	6250
CROSS SECTIONS (6)			
CROSS SECTIONS		XS-1 - XS-6	9001 - 9006
TOTAL SHEETS (22)			

PS&E PLANS-DATE 06/15/2015		
FMS CON NO. 107035 / 301000		
REVISIONS		
DATE	SHEET NO.	BY



MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE NO. 49.4 AT SR 7 ACROSS POTACOCOWA CREEK STREAM STABILIZATION AND COUNTERMEASURES		
DETAILED INDEX		
PROJECT NO. SP-0019-01(008) COUNTY: CARROLL		WORKING NUMBER DI-1
DATE DESIGNED <u> </u> BY <u> </u>	DATE DETAILED <u> </u> BY <u> </u>	DATE TRACED <u> </u> BY <u> </u>
CHECKED <u> </u> BY <u> </u>	ISSUED <u> </u> BY <u> </u>	DATE <u> </u> <u> </u> <u> </u>
SHEET NUMBER 2		

GENERAL NOTES

1. THE LOCATION AND SPACING OF SIGNS, SHOWN ON THE TRAFFIC CONTROL PLANS, ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.
2. ALL TRAFFIC CONTROL DEVICES ON THIS PROJECT SHALL COMPLY WITH PART VI OF THE MUTCD (LATEST EDITION).
3. ALL PLASTIC DRUMS SHALL HAVE A BALLASTING COLLAR MADE FROM RECYCLED TRUCK TIRES OR OTHER SUITABLE MATERIAL.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING STRUCTURES SUCH AS, BUT NOT LIMITED TO, PIPES, INLETS, APRONS, AND BRIDGES FROM DAMAGE WHICH MIGHT OCCUR DURING CONSTRUCTION. THE CONTRACTOR SHALL REPLACE OR REPAIR, AS DIRECTED BY THE ENGINEER, ANY STRUCTURES DAMAGED DURING THE LIFE OF THE CONTRACT. NO PAYMENT WILL BE MADE FOR REPLACEMENT OR REPAIR OF DAMAGED ITEMS.
5. VOIDS CREATED BY THE REMOVAL OF, BUT NOT LIMITED TO, POSTS, CONCRETE ANCHORS, AND FOOTINGS SHALL BE BACKFILLED AND TAMPED IN ACCORDANCE WITH SECTION 203 OF THE MISSISSIPPI STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
6. UTILITIES ON THE DRAWINGS ARE SHOWN IN THEIR ORIGINAL LOCATION BASED UPON THE BEST INFORMATION AVAILABLE TO THE ENGINEER. THE ENGINEER CAN NOT AND DOES NOT WARRANT THAT THIS INFORMATION IS COMPLETE OR ACCURATE. THE CONTRACTOR MUST COORDINATE DIRECTLY WITH THE INVOLVED UTILITY OWNERS TO HAVE UNDERGROUND UTILITY LINES FIELD LOCATED IN ADVANCE OF CONSTRUCTION.
7. WORK ON STRUCTURES FOR THIS PROJECT REQUIRES EXCAVATION IN THE IMMEDIATE VICINITY OF TRAFFIC AND ADJACENT PROPERTIES. THEREFORE, THE RISK OF A FAILURE OCCURRING DURING EXCAVATION REQUIRES THAT EXTREME CAUTION BE EXERCISED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING WHAT BRACING, SHORING, OR GROUND SUPPORT SYSTEM THAT IS DEEMED NECESSARY TO PREVENT A FAILURE AND PROTECT THE PERSONS WORKING NEAR THE EXCAVATION, THE PUBLIC THAT MAY BE ABOVE THE EXCAVATION OR ANY STRUCTURES ADJACENT TO THE EXCAVATION. ALL COSTS FOR DESIGNING, DRAWING, AND CONSTRUCTING THE FACILITY SHALL BE INCLUDED IN THE PRICE BID FOR CONTRACT ITEMS.
8. WIRE FENCE BACKING WILL BE REQUIRED FOR ALL SILT FENCE. (SEE WK. NO. ECD-3)
9. FLUORESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.
10. ROADWAY SIGNS THAT ARE IN CONFLICT WITH CONSTRUCTION OF THIS PROJECT SHALL BE REMOVED AND RELOCATED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
11. THE EROSION CONTROL DEVICES REFERENCED IN THESE PLANS ARE A MINIMUM REQUIREMENT. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT SILT DOES NOT LEAVE THE RIGHT OF WAY OR CONTAMINATE WATERS OF THE U. S. DURING CONSTRUCTION. THE CONTRACTOR SHALL SUBMIT AN EROSION CONTROL PLAN PRIOR TO COMMENCEMENT OF WORK AND MAINTAIN THE PLAN DURING CONSTRUCTION. ANY ADDITIONAL SILT BASINS NOT SHOWN IN THE PLANS SHALL BE INCLUDED IN THE CONTRACTOR'S EROSION CONTROL PLAN PRIOR TO SUBMITTING FOR APPROVAL.
12. THE CONTRACTOR IS RESPONSIBLE FOR FIELD-VERIFICATION OF EXISTING GRADES AND MAKING ADJUSTMENTS AS NECESSARY WITH THE APPROVAL OF THE PROJECT ENGINEER.
13. ALL ITEMS OF WORK ASSOCIATED WITH THE INSTALLATION OF A CONSTRUCTION ENTRANCE SHALL BE ABSORBED IN OTHER ITEMS OF WORK.
14. IF COLORS ARE USED ON PLAN/PROFILE SHEETS, THEY ARE INTENDED TO VISUALLY EASE THE LOCATION OF ELEMENTS FOR USERS OF THESE DRAWINGS. ALTHOUGH THE INTENT IS TO CATEGORIZE EVERYTHING AS EITHER EXISTING OR PROPOSED, IT IS THE END USER'S RESPONSIBILITY TO ENSURE ALL ELEMENTS ARE INTERPRETED CORRECTLY REGARDLESS OF COLOR.
15. THE TIEBACK SYMBOL INDICATES THE LOCATION OF THE TIEBACK AND IS NOT TO SCALE. SEE THE CROSS SECTIONS FOR DIMENSIONS OF INDIVIDUAL TIEBACKS.
16. CONTRACTOR SHALL MINIMIZE THE AREA TO BE CLEARED WITHIN THE PROJECT RIGHT-OF-WAY AND TEMPORARY CONSTRUCTION EASEMENTS.
17. HIGH WATER STAGES CAN BE EXPECTED TO OCCUR INTERMITTENTLY DEPENDING ON BASIN RAINFALL. INTERRUPTIONS AND/OR DELAYS TO CONSTRUCTION MAY OCCUR WHEN HIGH WATER STAGES AND/OR VELOCITIES MAKE CONSTRUCTION OPERATIONS IMPRACTICABLE. THE PROJECT ENGINEER RESERVES THE RIGHT TO DELAY OPERATIONS WHENEVER, IN HIS OPINION, HIGH WATER STAGES AND/OR VELOCITIES MAKE CONSTRUCTION OPERATIONS IMPRACTICABLE.
18. RIPRAP STONE DELIVERED TO THE WORK SITE, WHICH REQUIRED TEMPORARY STORAGE LANDWARD OF TOP BANK, SHALL BE PLACED IN A CONTAINER SUITABLE FOR STORING THE RIPRAP WITHOUT WASTE, OR A SAND-CLAY-GRAVEL OR CRUSHED STONE PAD MAY BE CONSTRUCTED FOR THE STORAGE AREA AND REMOVED UPON COMPLETION OF THE WORK. IF THE SAND-CLAY-GRAVEL OR CRUSHED STONE PAD METHOD IS USED, THE PAD SHALL HAVE A MINIMUM THICKNESS OF 6 INCHES. UPON COMPLETION OF THE WORK, THE STORAGE AREAS SHALL BE CLEANED OF ALL STORAGE RESIDUES AND RETURNED TO THEIR NATURAL CONDITION. ALL COSTS ASSOCIATED WITH TEMPORARY STORAGE FOR RIPRAP CONTAINERS OR PAD SHALL BE ABSORBED.
19. TEMPORARY STORAGE OF RIPRAP AT THE WORKSITE WILL BE ALLOWED, PROVIDED THE STREAM-SIDE TOE OF THE RIPRAP BE NO CLOSER THAN 50 LINEAR FEET FROM THE CLOSEST EDGE OF THE STREAM'S TOP BANK, AND THE AMOUNT SHALL NOT EXCEED 200 TONS. THE STORAGE STOCKPILE SHALL BE A MAXIMUM OF 12 FEET HIGH AND FORMED BY A SERIES OF LAYERS OF TRUCKLOAD DUMPS, WHERE THE ROCK ESSENTIALLY REMAINS WHERE IT IS PLACED. SUBSEQUENT LAYERS SHALL BE STARTED 10 FEET FROM THE EDGE OF THE PREVIOUS LAYER SO THAT THE ROCK WILL NOT ROLL DOWN THE EDGES OF THE PREVIOUS LAYERS. THE FIRST LAYER SHALL BE A MAXIMUM OF 6 FEET HIGH. ANY RIPRAP WHICH HAS BECOME CONTAMINATED WITH SOIL, DIRT, OR REFUSE AFTER BEING STOCKPILED, WILL NOT BE PUT INTO THE WORK UNLESS THE CONTAMINATING MATERIAL HAS BEEN REMOVED FROM THE RIPRAP PRIOR TO PLACEMENT.
20. MDOT DISTRICT 2 MAINTENANCE HAS PLACED RIPRAP ALONG THE EXISTING CHANNEL BANK FROM ±STA. 11+65 TO ±STA. 12+60 LT. THE EXISTING RIPRAP SHALL NOT BE REMOVED AND SHALL BE INCORPORATED INTO THE PROPOSED RIPRAP IMPROVEMENTS.

21. CONCRETE DEBRIS LOCATED FROM ±STA. 12+15 TO ±STA. 12+30 SHALL REMAIN IN PLACE AND BE INCORPORATED INTO THE PROPOSED RIPRAP IMPROVEMENTS.
22. HAUL ROADS WILL BE ALLOWED AND ALL ASSOCIATED COSTS SHALL BE ABSORBED IN OTHER ITEMS OF WORK. AREAS DISTURBED FOR HAUL ROADS SHALL BE RETURNED TO CONDITIONS ACCEPTABLE TO THE PROJECT ENGINEER.

SCOPE OF WORK

1. INSTALL AND IMPLEMENT ALL TRAFFIC CONTROL DEVICES AND MEASURES IN ACCORDANCE WITH THE MUTCD, LATEST EDITION.
2. INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES AS REQUIRED BY SITE CONDITIONS THROUGHOUT ALL STAGES OF CONSTRUCTION.
3. THE STREAM BANK AREAS ASSOCIATED WITH THE BANK STABILIZATION WORK, EXCAVATION, AND GRADING SHALL BE CLEARED OF TREES, BRUSH, DRIFT, MISCELLANEOUS DEBRIS, OR OTHER OBSTRUCTIONS THAT WOULD HINDER EXCAVATION OR GRADING AND SUBSEQUENT CONSTRUCTION OPERATIONS. CLEARING SHALL BE LIMITED TO THE ABSOLUTE MINIMUM NECESSARY FOR CONSTRUCTION OF THE WORK. IT IS THE INTENT OF THESE SPECIFICATIONS THAT GROWTH AROUND THE WORK AREA BE PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE. REQUEST TO CLEAR RIGHT-OF-WAY OR STORAGE AREAS SHALL BE SUBMITTED FOR APPROVAL AND WILL BE LIMITED TO APPROVED AREAS.
4. ALL STUMPS EXPOSED DURING EXCAVATION OR GRADING OPERATIONS ASSOCIATED WITH THE BANK STABILIZATION WORK SHALL BE EITHER CUT OFF FLUSH WITH THE FINISHED SLOPE GRADE OR GRUBBED OUT.
5. PRIOR TO PLACING STONE, ALL SNAGS, STUMPS, CUT-OFF ABANDONED PILES, TIMBER DEBRIS WITHIN THE CHANNEL OR OTHER OBSTRUCTIONS SHALL BE REMOVED FROM THE AREA TO BE COVERED BY THE STONE.
6. WHERE INDICATED ON THE DRAWINGS AND AS DIRECTED BY THE PROJECT ENGINEER, LOG JAMS AND/OR TREES, ABANDONED CUT-OFF TIMBER PILES, CONCRETE/ASPHALT DEBRIS AND OTHER OBSTRUCTIONS TO FLOW SHALL BE REMOVED FROM THE STREAM.
7. PERFORM REQUIRED EXCAVATION AND GRADING FOR THE LONGITUDINAL PEAKED STONE DIKE AND STONE PAVING AND OTHER INCIDENTAL EXCAVATION AS REQUIRED. WHERE INDICATED ON THE DRAWINGS, THE NATURAL GROUND SHALL BE EXCAVATED TO PROVIDE FOR PLACEMENT OF STONE. THE FINISHED GRADE SHALL CONFORM TO THE PRESCRIBED GRADE WITHIN THE LIMITS OF PLUS OR MINUS 6 INCHES, AND SHALL PRESENT A NEAT, SMOOTH SURFACE, FREE FROM OBSTRUCTIONS.
8. CONSTRUCTION OF LONGITUDINAL STONE DIKE AND STONE PAVING SHALL BEGIN AT THE UPSTREAM END OF THE PROJECT AND PROCEED TO THE DOWNSTREAM END UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER.
9. CONSTRUCT LONGITUDINAL PEAKED STONE DIKE AND TRANSVERSE STONE TIEBACKS FOLLOWING THE ALIGNMENT AND GRADES AS CLOSELY AS PRACTICABLE.
10. CONSTRUCT TRANSVERSE STONE TIEBACKS FROM THE LONGITUDINAL PEAKED STONE DIKE TOWARDS LAND UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER.
11. STONE SHALL BE PLACED IN THE LONGITUDINAL PEAKED STONE DIKE, TRANSVERSE TIEBACKS, AND SLOPE PAVING BY SKIP, GRAPPLE, HAND, OR OTHER APPROVED METHOD, IN SUCH A MANNER AS TO PRODUCE A REASONABLY WELL GRADED MASS OF STONE WITH THE MINIMUM PRACTICABLE PERCENTAGE OF VOIDS. STONE SHALL NOT BE DROPPED THROUGH AIR FROM A HEIGHT GREATER THAN 3 FEET. A VARIATION OF 20 PERCENT ABOVE OR BELOW THE SPECIFIED AVERAGE THICKNESS OF THE STONE PAVING SHOWN ON THE DRAWINGS WILL BE ALLOWED. BULLDOZING STONE INTO EXCAVATED TRENCHES WILL NOT BE PERMITTED.
12. THOSE PORTIONS OF TRANSVERSE STONE TIEBACKS WHICH ARE LANDWARD OF HIGH-TOP BANK SHALL BE BACKFILLED AS SHOWN ON THE DRAWINGS. BACKFILL MAY BE COMPLETED USING MATERIAL OBTAINED FROM EXCAVATION FOR THE DIKES AND STONE TIEBACKS. BACKFILL SHALL BE PLACED IN LAYERS NOT TO EXCEED 24 INCHES IN THICKNESS, AND EACH LAYER SHALL BE COMPACTED BY ONE COMPLETE PASS OF A CRAWLER TYPE TRACTOR WEIGHING AT LEAST 20,000 POUNDS AND EXERTING A TREAD PRESSURE OF NOT LESS THAN 6 PSI. CRAWLER TYPE TRACTORS WILL NOT BE CONSIDERED TO BE COMPACTING WHEN SPREADING MATERIAL. BACKFILL SHALL BE SLOPED TO DRAIN LANDWARD AND GRADED TO A SMOOTH SURFACE TRANSITION INTO THE SURROUNDING SURFACES.
13. CONSTRUCT RIPRAP REVETMENT ALONG THE EXISTING TOE OF SLOPE BENEATH THE BRIDGE AS SHOWN. STONE PAVING SHALL COMMENCE AT THE TOE OF SLOPE AND PROCEED UP THE SLOPE TO THE REQUIRED ELEVATION OR TOP BANK. STONE SHALL NOT BE DUMPED FROM THE TOP OF BANK ONTO THE SLOPE. GRADING OF THE EXISTING SLOPE SHALL BE LIMITED TO THE ABSOLUTE MINIMUM NECESSARY REQUIRED FOR THE PLACING OF RIPRAP AT THE NATURAL ANGLE OF REPOSE OR FLATTER SLOPE.
14. PERFORM SEEDING, FERTILIZING, AND MULCHING OF ALL DISTURBED AREAS IN ACCORDANCE WITH THE SPECIFICATIONS.
15. TEMPORARY GRASSING SHALL BE REQUIRED PER THE MDEQ SMALL CONSTRUCTION NOTICE OF INTENT (SCNOI). TEMPORARY GRASSING COSTS SHALL BE ABSORBED IN OTHER ITEMS OF WORK.

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MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE NO. 49.4 AT SR 7 ACROSS POTACOCOWA CREEK STREAM STABILIZATION AND COUNTERMEASURES		
GENERAL NOTES AND SCOPE OF WORK		
PROJECT NO. SP-0019-01(008) COUNTY: CARROLL		WORKING NUMBER GN-1
DATE DESIGNED _____ CHECKED _____	BY REVISION _____ _____ _____ _____	SHEET NUMBER 3
DATE DESIGNED _____ CHECKED _____	BY REVISION _____ _____ _____ _____	SHEET NUMBER 3