

**SECTION 905 -- PROPOSAL (CONTINUED)**

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.   1   DATED   8/8/2016        ADDENDUM NO.        DATED         
 ADDENDUM NO.   2   DATED   8/12/2016        ADDENDUM NO.        DATED       

Number	Description
1	Revised Table of Contents; Revised NTB No. 6545; Delete NTB No. 6573; Revised NTB No. 6575; Amendment EBS Download Required.
2	Revised NTB No. 6549; Revised Wage Rates; Amendment EBS Download Required.

TOTAL ADDENDA:   2    
 (Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE \_\_\_\_\_

\_\_\_\_\_  
 Contractor

BY \_\_\_\_\_  
 Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

\_\_\_\_\_  
 President Address

\_\_\_\_\_  
 Secretary Address

\_\_\_\_\_  
 Treasurer Address

The following is my (our) itemized proposal.

IM-0055-02(240)/ 106616301000

Madison County(ies)

Revised 01/26/2016

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 6549

CODE: (SP)

DATE: 06/22/2016

SUBJECT: Scope of Work

PROJECT: IM-0055-02(240) / 106616301 -- Madison County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings". All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following:

Mill and overlay approximately 11.7 miles of existing asphalt pavement on Interstate 55 from SR 463 to 2 miles north of SR 22, beginning at Station 300+00(BOP) to 942+00(EOP). The section from station 458+25 to station 533+00 (Gluckstadt Interchange) will be excluded from this project.

## **I-55 Mainline and Inside shoulders; Nissan ACC/DCC lanes; Nissan Collector/Distributor Roads Sowell Road ACC/DCC lanes ; Sowell Road Collector/Distributor Roads:**

Work in this area shall consist of fine milling the travel lanes, shoulders, guardrail pads to the face of the rail, etc. 1½" and variable to provide for grade profile. Then overlay the inside shoulder and travel lanes with 1½" of 9.5-mm, SMA Asphalt Pavement and 1" of OGFC. Place 1½" of 9.5-mm, ST, Asphalt Pavement on the outside shoulders. Two and one-half inches (2½") of fine milling will be required at overhead bridges to provide for required clearance.

### **Nissan Parkway Interchange:**

Loops, ramps, and inside shoulder will require 1½" of fine milling and inlay with 1½" of 9.5-mm, HT, Asphalt Pavement, Polymer Modified. Outside shoulders will require 1½" of fine milling and inlay with 1½" of 9.5-mm, ST, Asphalt Pavement. No work will be required on Nissan Parkway.

### **SR 463 Interchange:**

No work is required on SR 463 mainline. Ramps, and inside shoulder will require 1½" of fine milling and inlay with 1½" of 9.5-mm, HT, Asphalt Pavement, Polymer Modified. Outside shoulders will require 1½" of fine milling and inlay with 1½" of 9.5-mm, ST, Asphalt Pavement.

### **Sowell Road Interchange:**

Ramps, loops, inside shoulders and mainline on Sowell Rd will require 1½" of fine milling and inlay with 1½" of 9.5-mm, HT, Asphalt Pavement. Outside shoulders will require 1½" of fine

milling and inlay with 1½" of 9.5-mm, ST, Asphalt Pavement. Limits of construction shall be from Calhoun Station Road to Old Jackson Road.

**SR 22 Interchange:**

No work is required on SR 22 mainline. Ramps, loops, and inside shoulders will require 1½" of fine milling and inlay with 1½" of 9.5-mm, HT, Asphalt Pavement. Outside shoulders will require 1½" of fine milling and inlay with 1½" of 9.5-mm, ST, Asphalt Pavement.

**General Notes:** These general notes are applicable to all sites.

**Joint Sealant will be required on the SMA lift as per Special Provision 907-403**

Potholes that may exist are to be patched in a timely manner. Patching of potholes shall be considered an absorbed item.

Any failed areas will be repaired with full depth with 19-mm, HT, asphalt pavement. There are no known failed areas on the project and pay items have been added as a contingency should some develop during construction.

Traffic will not be allowed to run on the milled surface. Milling shall be performed in accordance with the attached drawings. Outside shoulders will not be required to be paved before opening travel lanes to traffic. Approved mix designs must be on hand prior to milling. Milling operations will not commence until such time that, in the opinion of the Engineer, weather conditions have been consistently suitable enough to allow the placement of the asphalt pavement after the milling operations.

**Milling at bridge approach slabs to be done per attached detail sheet.**

**The Reclaimed Asphalt Pavement (RAP) material removed by the milling operation shall become the property of the Contractor with the exception of 10,000 tons or 50% of the total anticipated quantity, whichever is less, and shall be delivered to the Canton Maintenance Headquarters located at 150 Canton One DR., Canton, MS. Sufficient advance notice shall be given to ensure that MDOT Maintenance personnel will be on hand to direct the delivery. The Contractor shall also provide MDOT with an Operator and the necessary equipment to stockpile the delivery. The cost of which shall be absorbed.**

Where applicable the existing shoulders are to be raised to match the new pavement elevation by placing variable depth Granular Material (Class 5, Group E) on the existing shoulders. Placement of the granular material on the finished asphalt course shall not be permitted. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%). Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. **Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%). The cost of blading will be an absorbed item and is not to be included in the price of pay items bid.**

Removal of the existing shoulder material shall be coincident with the milling/overlaying operation to prevent the possible ponding of water. No payment will be made for blading or removal of the existing shoulder material. Any material excavated from the existing shoulder shall be used to raise the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer and will be an absorbed item. Material which cannot be placed in adjacent areas and deemed to be excess excavation by the Engineer shall be removed under Pay Item No. 203-G Excess Excavation.

It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the engineer, any structures damaged during the life of the contract. No payment will be made for replacement or repair of damaged items.

Any signs, mailboxes, etc. that are in conflict with construction of this project shall be removed and relocated by the Contractor as directed by the Engineer, the cost of which is to be absorbed in other items bid.

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost is to be included in the price bid for Pay Item No. 907-618-A, Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background. Standard roadside construction signs and barricades will be paid for using the appropriate pay items.

Roadside construction signs, barrels, etc. shall be placed in accordance with the attached drawings or as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed.

Changeable Message Signs are to be used as directed by the Engineer to help aide in advising the public of lane closures and traffic patterns. Costs for moving and resetting the message boards are to be included in the price bid for Pay Item No. 907-618-A, Maintenance of Traffic.

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal; the cost is to be included in the prices of items bid. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor.

Permanent pavement markers are to be placed in accordance with the attached drawings and Standard Drawings.

Side drains for underdrains are marked by a 12 inches of 6-inch thermoplastic stripe on the shoulders of the Sowell Road interchange and Nissan Drive ramps. These will need to be replaced during the placement of the permanent pavement markings.

**Temporary traffic stripe will be required immediately after the required overlay and prior to opening area to traffic. Temporary stripe is to be placed in the same location and configuration as the permanent stripe.**

Traffic stripe removal pay item was set up to remove stripe from bridges prior placement of permanent stripe.

All permanent striping will be thermoplastic double-drop. Edge lines shall be placed to accommodate the lane widths shown on the applicable typical sections unless prevented by field conditions

The following equations are referenced for this project:

#### **North Lane**

##### **I-55**

$$338+42.48 \text{ BK} = 338+89.96 \text{ AH} \quad - 47.48 \text{ feet}$$

$$440+19.55 \text{ BK} = 440+48.50 \text{ AH} \quad - 28.95 \text{ feet}$$

##### **458+25 – 533+00 - Gluckstadt Interchange**

$$536+41.50 \text{ BK} = 536+17.73 \text{ AH} \quad + 23.77 \text{ feet}$$

$$595+05.90 \text{ BK} = 595+66.30 \text{ AH} \quad - 60.40 \text{ feet}$$

$$658+02.29 \text{ BK} = 658+00.16 \text{ AH} \quad + 2.13 \text{ feet}$$

#### **South Lane**

##### **I-55**

$$339+56.95 \text{ BK} = 340+33.79 \text{ AH} \quad - 76.84 \text{ feet}$$

$$440+56.25 \text{ BK} = 439+14.56 \text{ AH} \quad + 141.69 \text{ feet}$$

##### **458+25 – 533+00 - Gluckstadt Interchange**

$$542+43.27 \text{ BK} = 543+00.86 \text{ AH} \quad - 57.59 \text{ feet}$$

$$588+82.59 \text{ BK} = 588+57.59 \text{ AH} \quad + 25.00 \text{ feet}$$

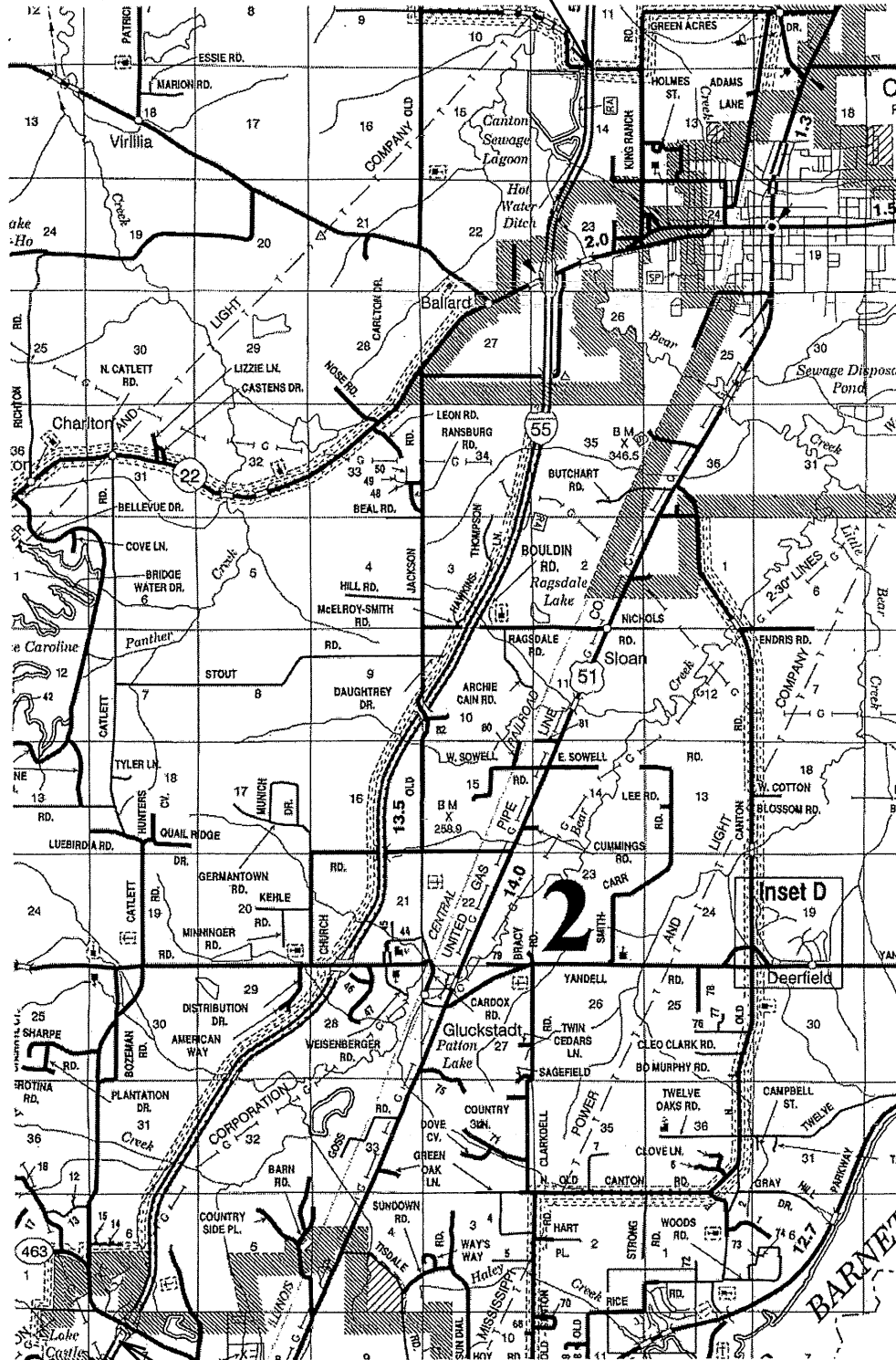
$$649+67.67 \text{ BK} = 649+67.19 \text{ AH} \quad + 0.48 \text{ feet}$$

# I-55 OVERLAY PROJECT

## FROM SR 463 TO 2 MILES NORTH OF SR 22

### MADISON COUNTY

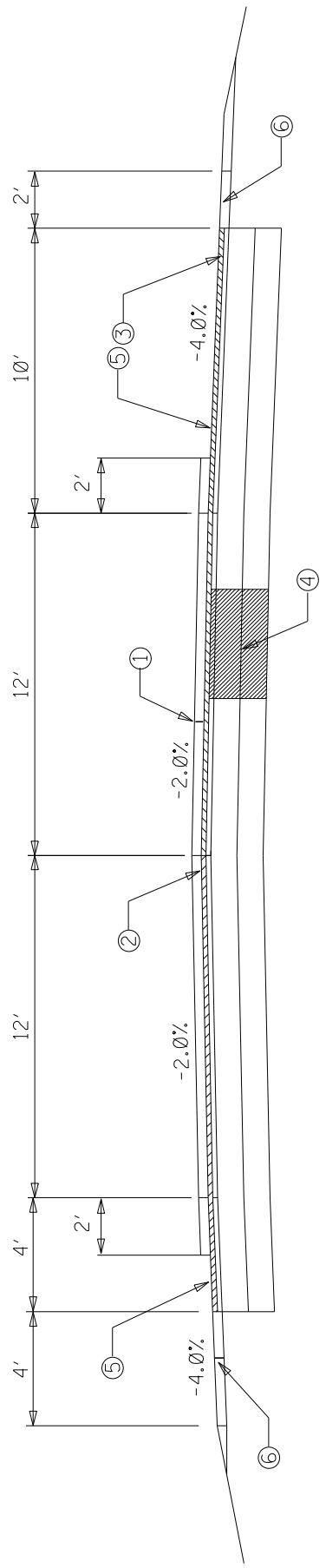
EOP STA 942+00



BOP STA 300+00

IM-0055-02(240)  
MADISON CO.

BOP TO EOP  
I-55 MAINLINE



EXISTING PAVEMENT  
( IN DIRECTION OF TRAFFIC FLOW )

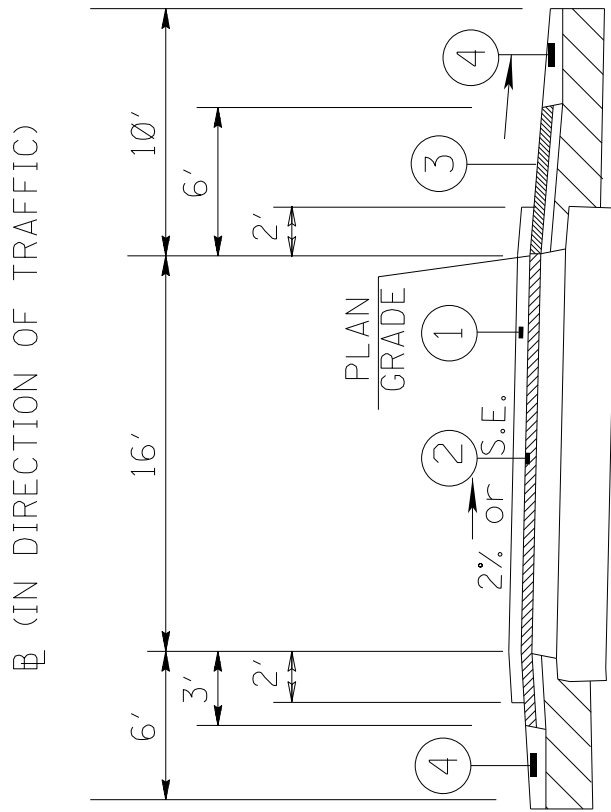
NORTH BOUND SHOWN SOUTH BOUND SHALL BE SIMILAR

- ① 1.00" OPEN GRADE FRICTION COURSE
- ② 1.50" FINE MILLING AND REPLACE WITH 1.50" ASPHALT PAVEMENT, SMA (9.5mm MIXTURE) (1@1.5") \* \*
- ③ 1.50" FINE MILLING AND REPLACE WITH 1.50" ASPHALT PAVEMENT, ST (9.5mm MIXTURE) (1@1.5") \* \*
- ④ REPAIR ANY FAILED AREAS WITH FULL DEPTH ASPHALT PAVEMENT 19mm HT. \*
- ⑤ RUMBLE STRIPS WILL BE REQUIRED AS PER SPECIAL DESIGN SHEET RS-5.
- ⑥ GRANULAR MATERIAL (CLASS 5 GROUP E)

\* \* 2.5" FINE MILLING AT OVERHEAD BRIDGES FOR CLEARANCE.

\* THERE ARE NO KNOWN FAILED AREAS ON THE PROJECT

NISSAN ACC/DCC LANES  
NISSAN COLLECTOR/DISTRIBUTOR ROADS



- ① 1.00" OPEN GRADE FRICTION COURSE
- ② 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, SMA (9.5mm MIXTURE)(1@1.5")\*
- ③ 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, ST (9.5mm MIXTURE)(1@1.5") \*
- ④ CLASS 5 GROUP E GRANULAR MATERIAL AS REQUIRED

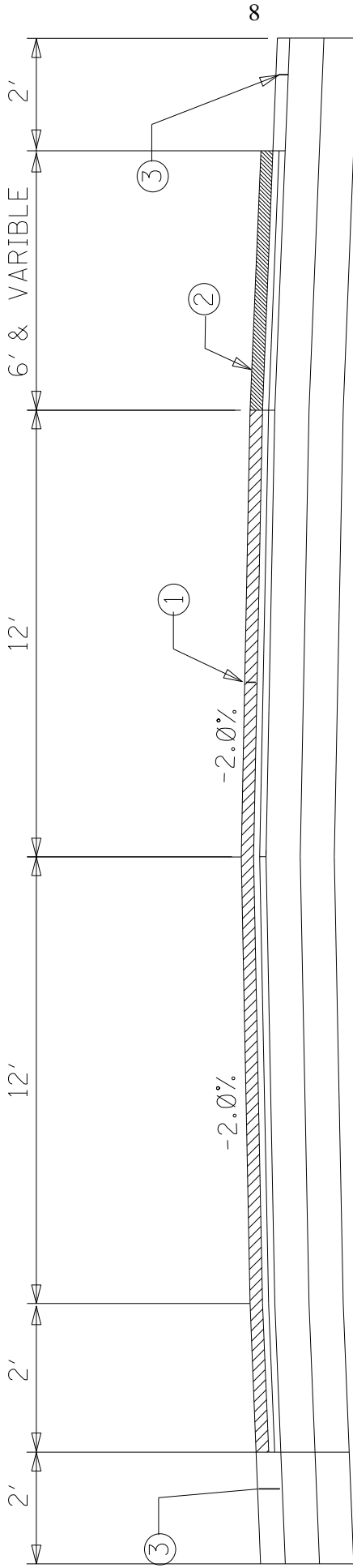
\* \* 2.5" FINE MILLING AT OVERHEAD BRIDGES FOR CLEARANCE.



IM-0055-02(240)  
MADISON CO.

SOWELL RD

CALHOUN STATION PARKWAY TO OLD JACKSON ROAD

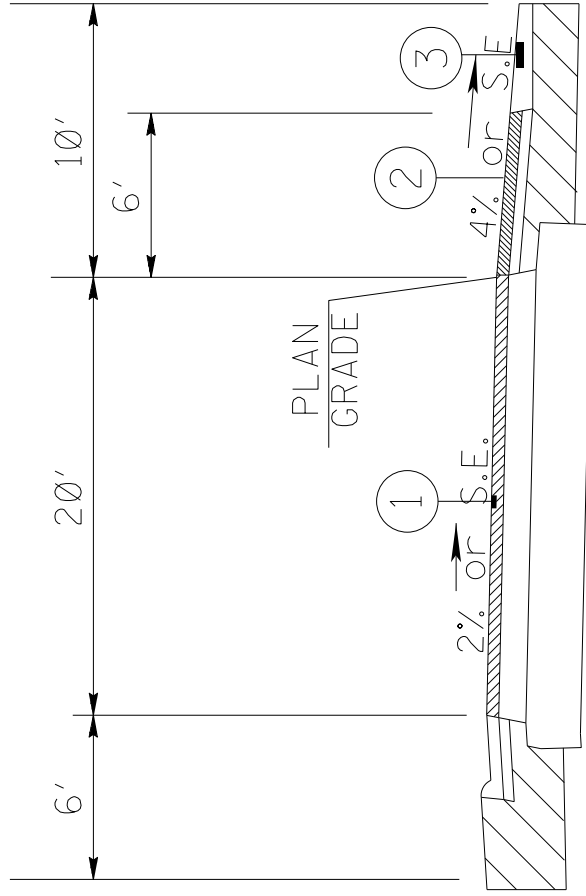


EXISTING PAVEMENT  
( IN DIRECTION OF TRAFFIC FLOW )

EAST BOUND SHOWN WEST BOUND SHALL BE SIMILAR

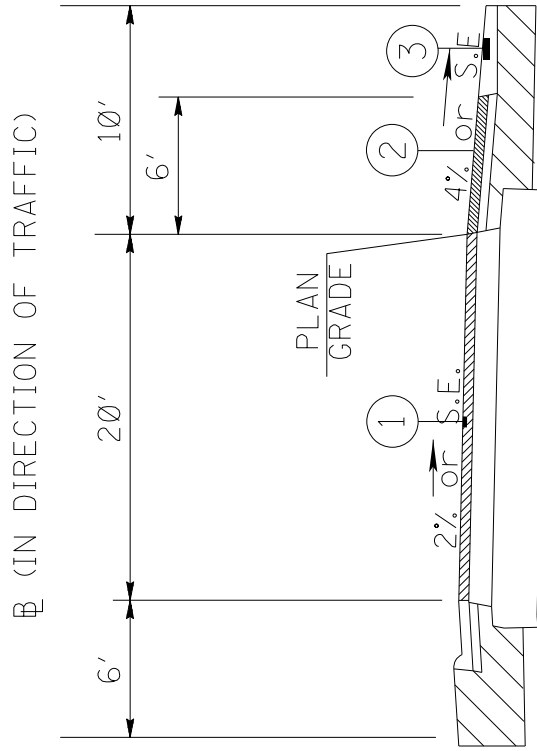
- ① 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, HT (9.5mm MIXTURE)(1@1.5")
- ② 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, ST (9.5mm MIXTURE)(1@1.5")
- ③ CLASS 5 GROUP E GRANULAR MATERIAL AS REQUIRED

NW LOOP RAMP SR 22  
 NW LOOP RAMP SOWELL RD.  
 SW LOOP RAMP SOWELL RD.  
 B (IN DIRECTION OF TRAFFIC)



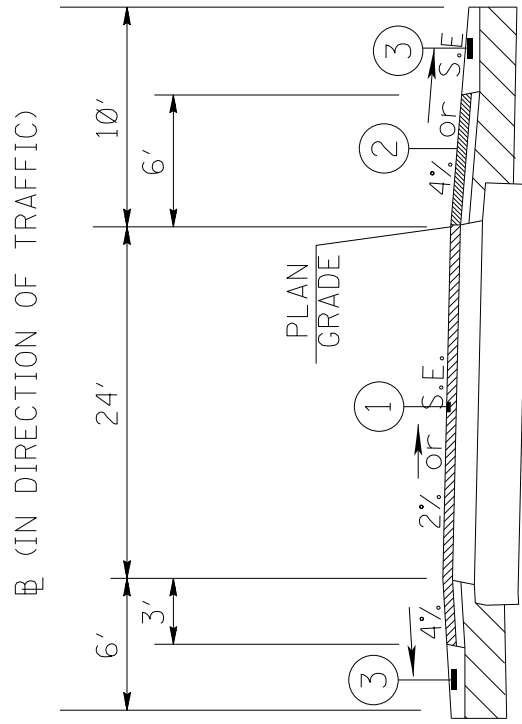
- 1 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, HT (9.5mm MIXTURE)(1@1.5")
- 2 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, ST (9.5mm MIXTURE)(1@1.5")
- 3 CLASS 5 GROUP E GRANULAR MATERIAL AS REQUIRED

LOOP RAMPS - NISSAN PARKWAY



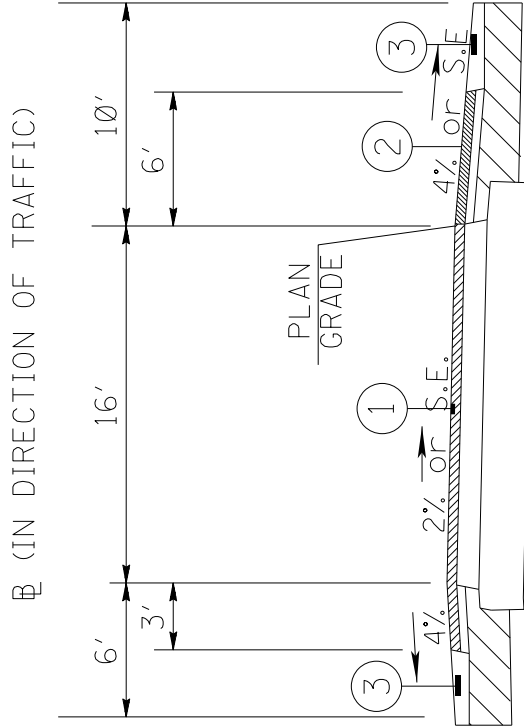
- ① 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, HT (9.5mm MIXTURE)POL YMER MODIFIED(1@1.5")
- ② 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, ST (9.5mm MIXTURE)(1@1.5")
- ③ CLASS 5 GROUP E GRANULAR MATERIAL AS REQUIRED

ALL RAMPS SR 463



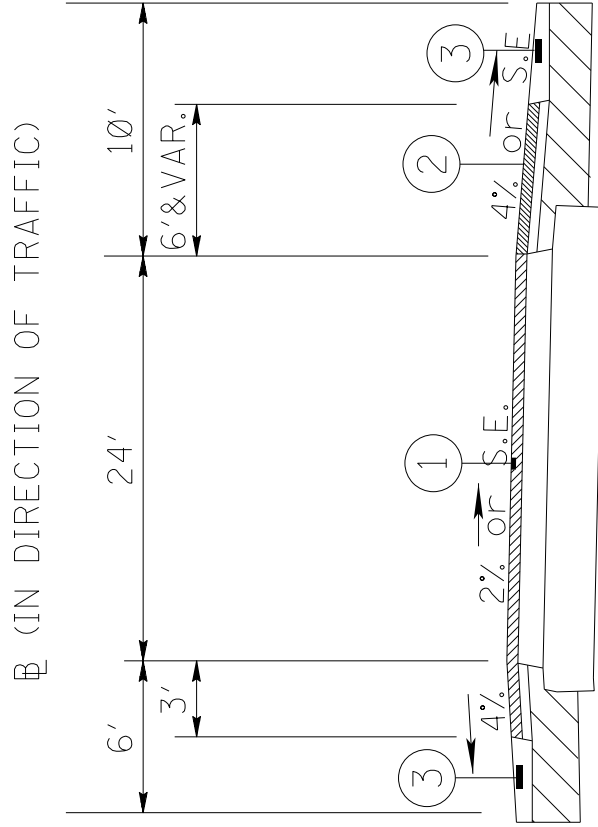
- ① 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, HT (9.5mm MIXTURE)POLYMER MODIFIED(1@1.5")
- ② 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, ST (9.5mm MIXTURE)(1@1.5")
- ③ CLASS 5 GROUP E GRANULAR MATERIAL AS REQUIRED

- NE RAMP NISSAN PARKWAY
- SE RAMP NISSAN PARKWAY
- NW RAMP NISSAN PARKWAY
- SW RAMP NISSAN PARKWAY



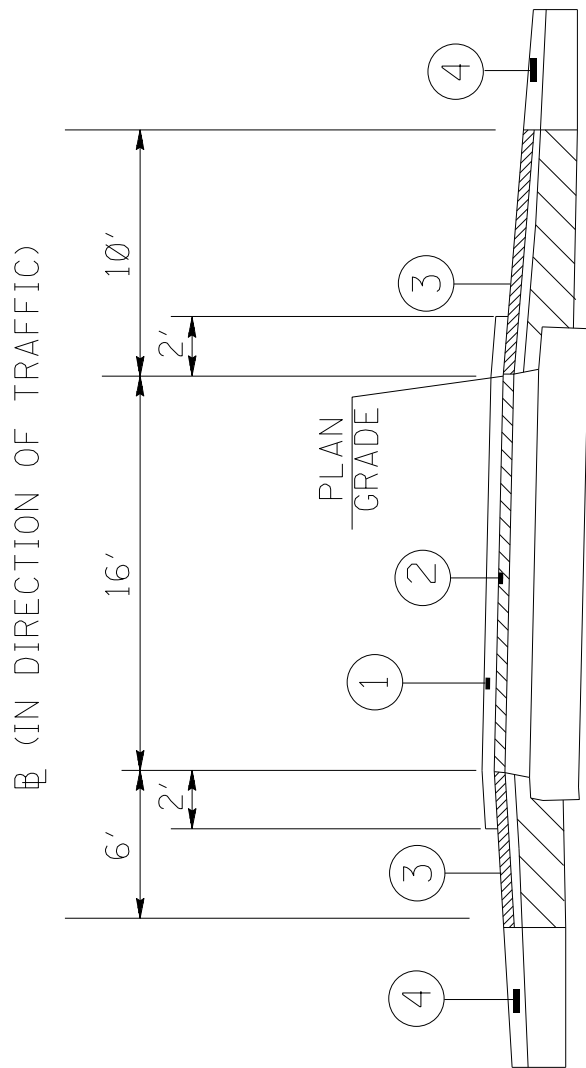
- ① 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, HT (9.5mm MIXTURE)POLYMER MODIFIED(1@1.5")
- ② 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, ST (9.5mm MIXTURE)(1@1.5")
- ③ CLASS 5 GROUP E GRANULAR MATERIAL AS REQUIRED

SE RAMP SR 22  
SE RAMP SOWELL RD



- ① 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, HT (9.5mm MIXTURE)(1@1.5')
- ② 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, ST (9.5mm MIXTURE)(1@1.5')
- ③ CLASS 5 GROUP E GRANULAR MATERIAL AS REQUIRED

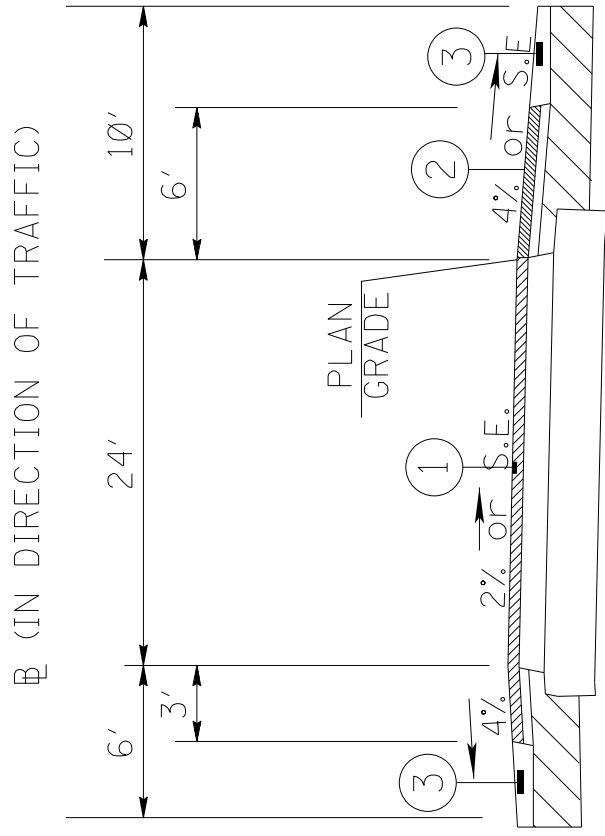
SOWELL ROAD ACC/DCC LANES  
SOWELL ROAD COLLECTOR/DISTRIBUTOR ROAD



- ① 1.00" OPEN GRADE FRICTION COURSE
- ② 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, SMA (9.5mm MIXTURE)(1@1.5")\*
- ③ 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, ST (9.5mm MIXTURE)(1@1.5") \*
- ④ CLASS 5 GROUP E GRANULAR MATERIAL AS REQUIRED

\* \* 2.5" FINE MILLING AT OVERHEAD BRIDGES FOR CLEARANCE.

- SW RAMP SOWELL RD
- NW RAMP SOWELL RD
- NE RAMP SOWELL RD
- NE RAMP SR22
- NW RAMP SR22
- SW RAMP SR22

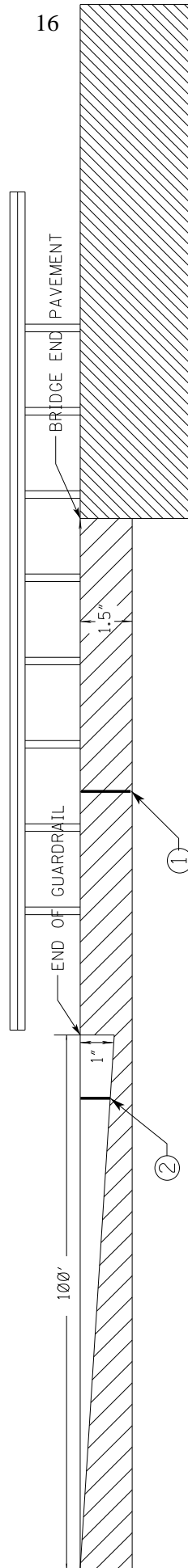


- ① 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, HT (9.5mm MIXTURE) (1@1.5")
- ② 1.5" FINE MILLING AND REPLACE WITH 1.5" ASPHALT PAVEMENT, ST (9.5mm MIXTURE) (1@1.5")
- ③ CLASS 5 GROUP E GRANULAR MATERIAL AS REQUIRED



106616/301000  
MADISON COUNTY

OGFC TRANSITION AT  
BRIDGE ENDS DETAIL

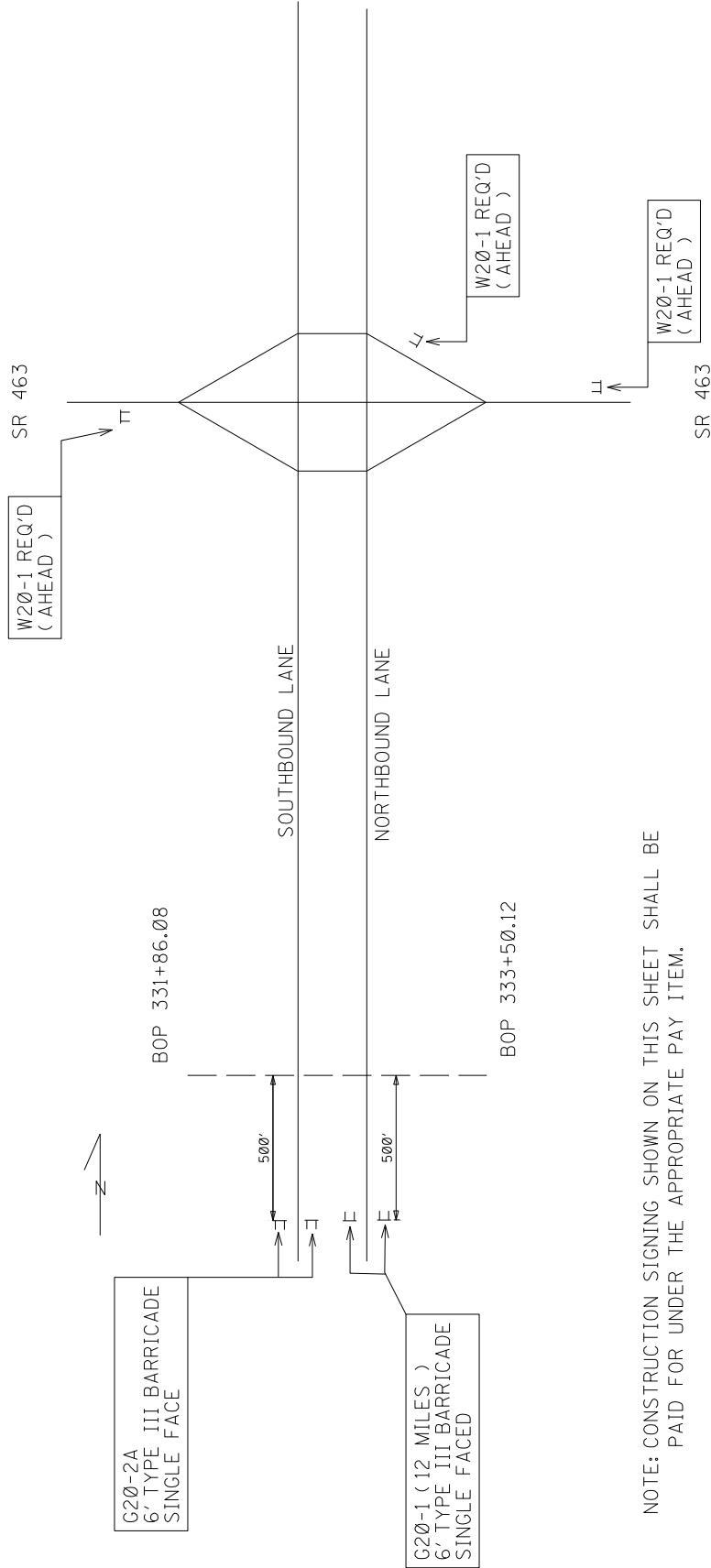


Notice To Bidders No.6549- Cont'd.

- ① 1.5" FINE MILLING AND REPLACE WITH 1.5" SMA ASPHALT PAVEMENT, SMA (9.5mm MIXTURE)(1@1.5")
- ② 1" AND VARIABLE FINE MILLING & FILL WITH 1" OPEN GRADE FRICTION COURSE.

106616/301000  
MADISON COUNTY  
CONSTRUCTION SIGN SCHEDULE

SECTION 1

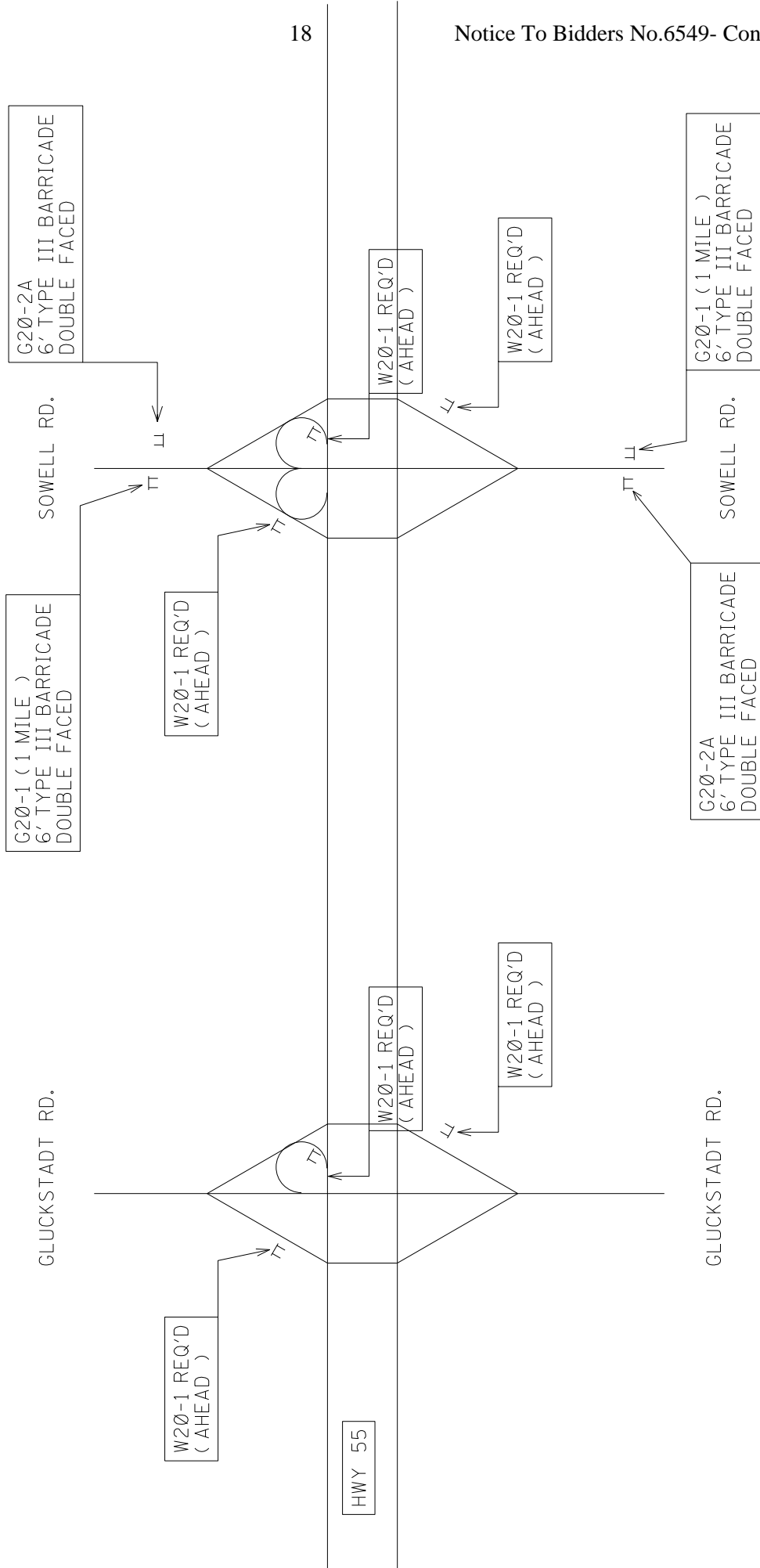


NOTE: CONSTRUCTION SIGNING SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER THE APPROPRIATE PAY ITEM.

SIGN TYPE	NO. OF SIGNS
G20-1	4
G20-2A	4
W20-1	20
R16-3	34

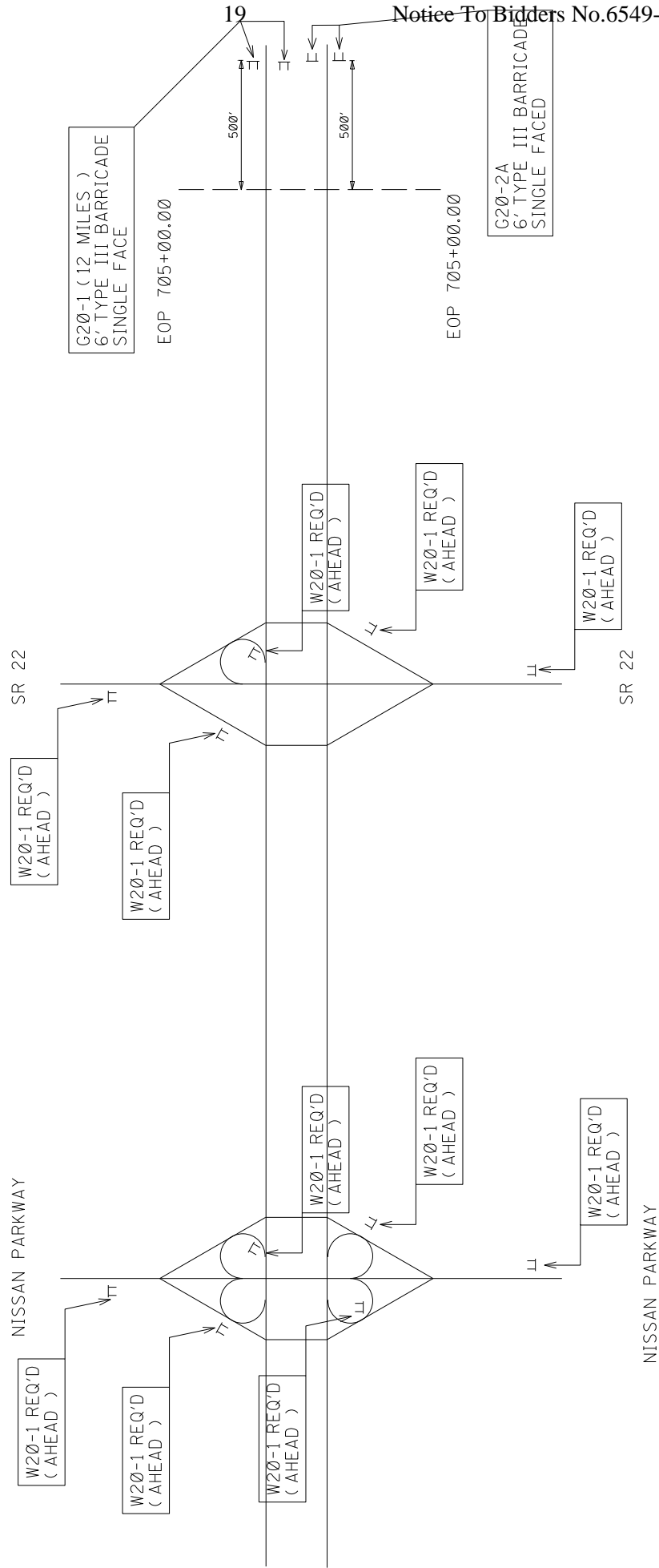
106616/301000  
MADISON COUNTY  
CONSTRUCTION SIGN SCHEDULE

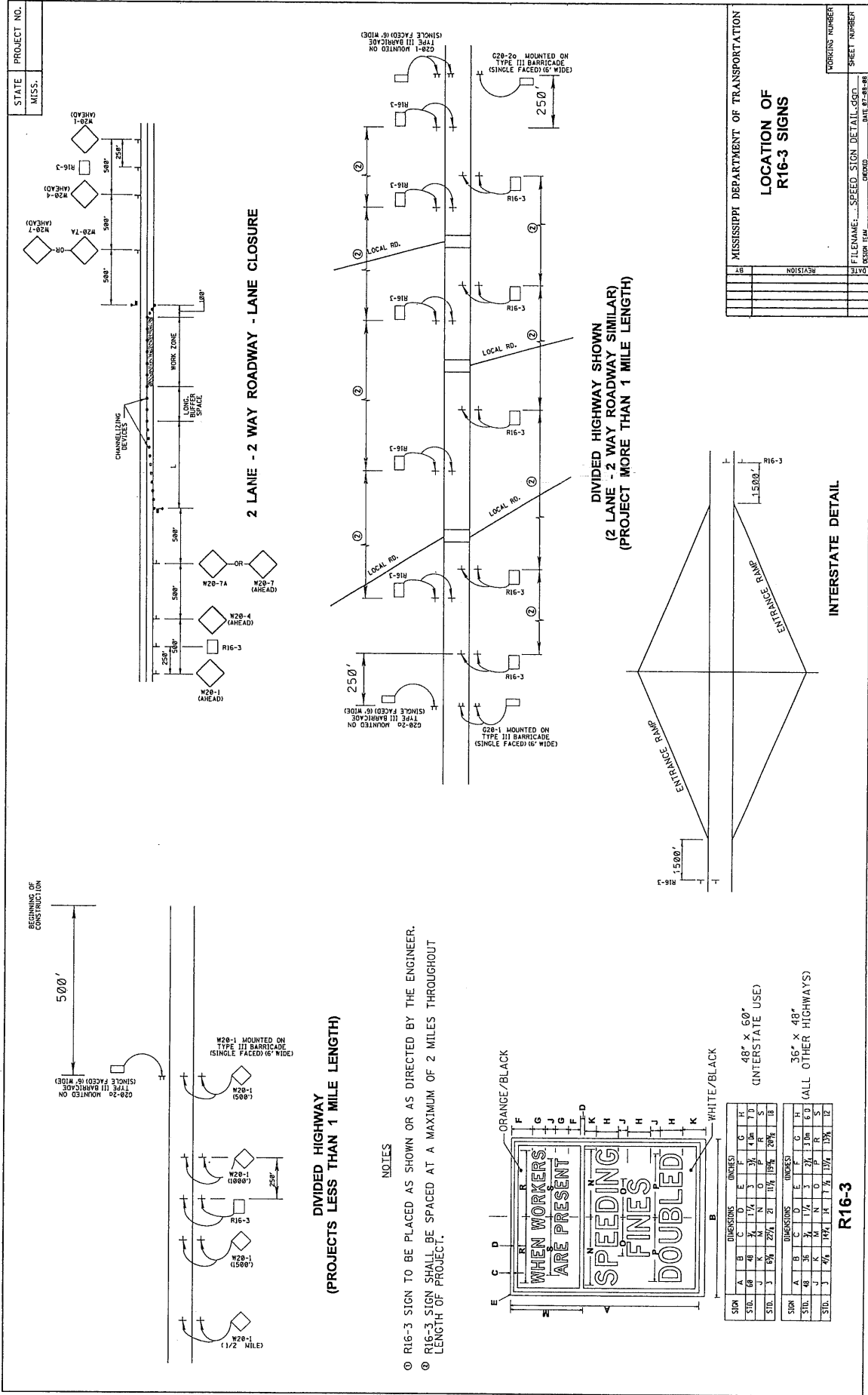
SECTION 2



106616/301000  
MADISON COUNTY  
CONSTRUCTION SIGN SCHEDULE

SECTION 3





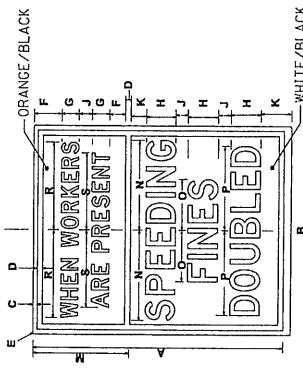
**2 LANE - 2 WAY ROADWAY - LANE CLOSURE**

**DIVIDED HIGHWAY SHOWN  
(2 LANE - 2 WAY ROADWAY SIMILAR)  
(PROJECT MORE THAN 1 MILE LENGTH)**

**DIVIDED HIGHWAY  
(PROJECTS LESS THAN 1 MILE LENGTH)**

**NOTES**

- ⊙ R16-3 SIGN TO BE PLACED AS SHOWN OR AS DIRECTED BY THE ENGINEER.
- ⊙ R16-3 SIGN SHALL BE SPACED AT A MAXIMUM OF 2 MILES THROUGHOUT LENGTH OF PROJECT.

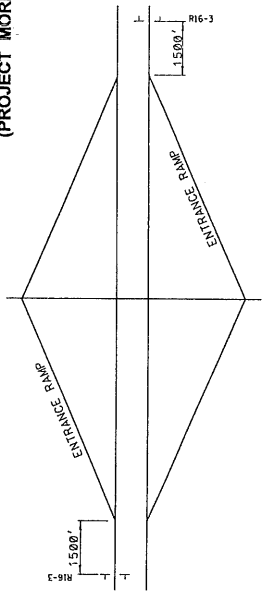


DIMENSIONS		INCHES	
SIGN	A	48	50
	B	36	48
	C	1 1/4	1 1/4
	D	1 1/4	1 1/4
	E	1 1/4	1 1/4
	F	1 1/4	1 1/4
	G	1 1/4	1 1/4
	H	1 1/4	1 1/4
	I	1 1/4	1 1/4
	J	1 1/4	1 1/4
	K	1 1/4	1 1/4
	L	1 1/4	1 1/4
	M	1 1/4	1 1/4
	N	1 1/4	1 1/4
	O	1 1/4	1 1/4
	P	1 1/4	1 1/4
	Q	1 1/4	1 1/4
	R	1 1/4	1 1/4
	S	1 1/4	1 1/4

48" x 50"  
(INTERSTATE USE)

36" x 48"  
(ALL OTHER HIGHWAYS)

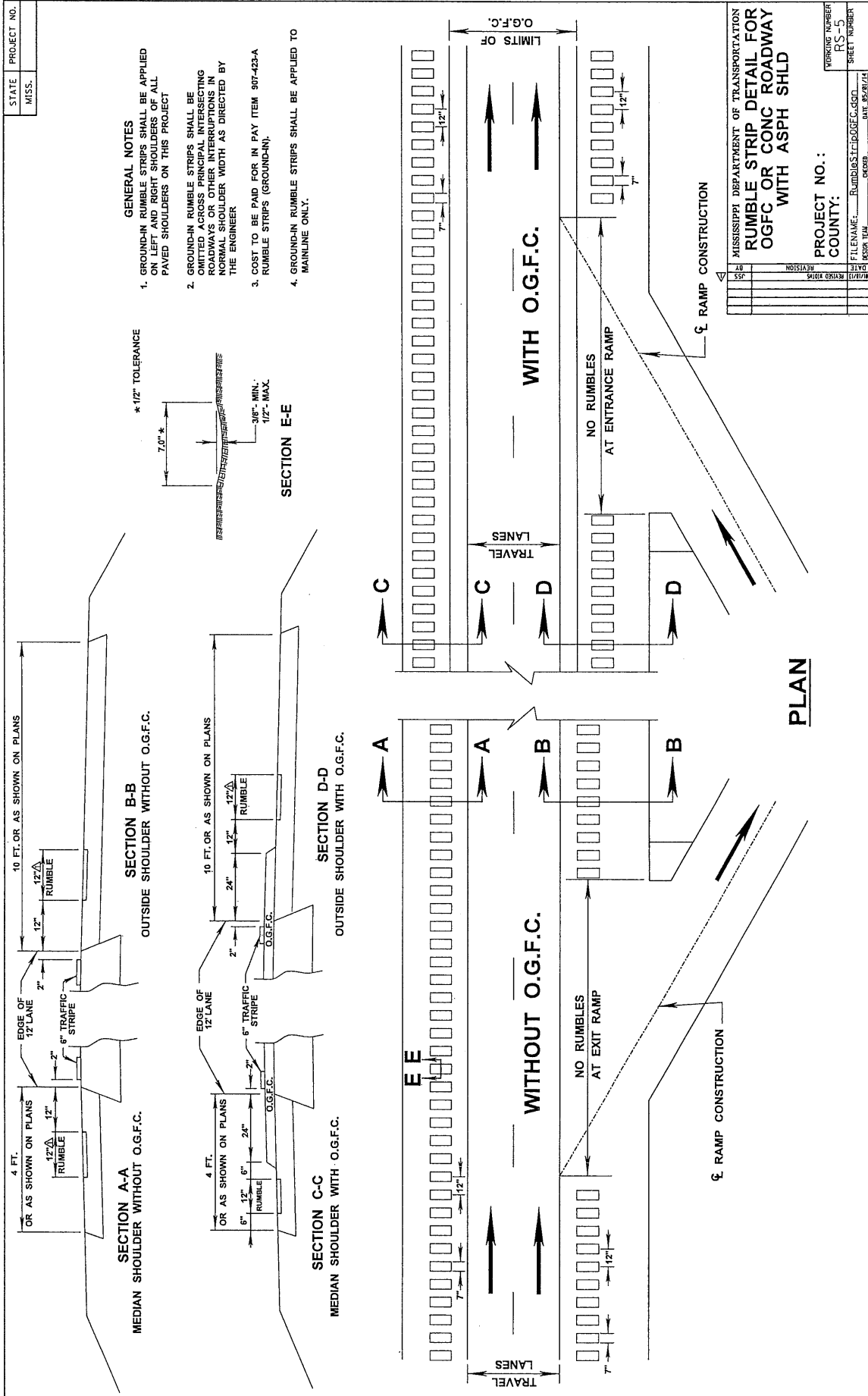
**R16-3**

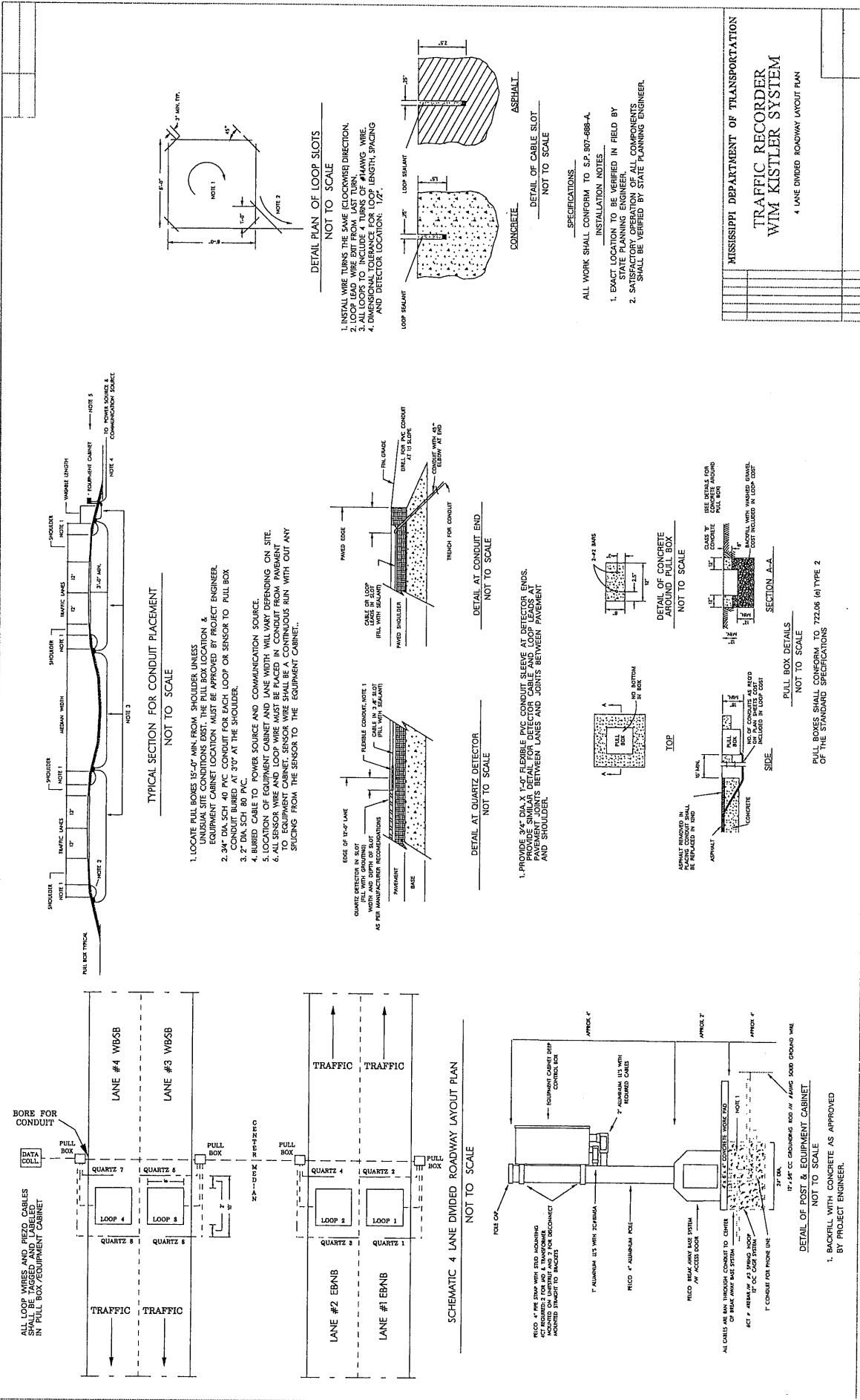


MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
DATE	REVISION
BY	
FILENAME: ...SPEED SIGN DETAIL.DWG	
DATE 07-21-08	
WORKING NUMBER	
SHEET NUMBER	

**LOCATION OF  
R16-3 SIGNS**

STATE	PROJECT NO.
MISS.	





MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
TRAFFIC RECORDER  
WIM KISTLER SYSTEM  
4 LANE DIVIDED ROADWAY LAYOUT PLAN

General Decision Number: MS160241 07/22/2016 MS241

Superseded General Decision Number: MS20150241

State: Mississippi

Construction Type: Highway

County: Madison County in Mississippi.

HIGHWAY CONSTRUCTION PROJECTS

Note: Under Executive Order (EO) 13658, an hourly minimum wage of \$10.15 for calendar year 2016 applies to all contracts subject to the Davis-Bacon Act for which the solicitation was issued on or after January 1, 2015. If this contract is covered by the EO, the contractor must pay all workers in any classification listed on this wage determination at least \$10.15 (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in calendar year 2016. The EO minimum wage rate will be adjusted annually. Additional information on contractor requirements and worker protections under the EO is available at [www.dol.gov/whd/govcontracts](http://www.dol.gov/whd/govcontracts).

Modification Number	Publication Date
0	01/08/2016
1	07/22/2016

\* ELEC0480-010 07/01/2016

	Rates	Fringes
TRAFFIC SIGNALIZATION		
Electrician.....	\$ 24.60	3%+7.48
-----		
SUMS2010-060 08/04/2014		

	Rates	Fringes
CARPENTER (Form Work Only).....	\$ 12.32	0.00
CEMENT MASON/CONCRETE FINISHER...	\$ 12.85	0.39
ELECTRICIAN.....	\$ 24.04	5.87
HIGHWAY/PARKING LOT STRIPING:		
Truck Driver (Line Striping Truck).....	\$ 10.86	0.00
INSTALLER - GUARDRAIL.....	\$ 12.07	0.00
INSTALLER - SIGN.....	\$ 11.54	0.00
IRONWORKER, REINFORCING.....	\$ 15.52	0.00

LABORER: Common or General,  
Including Asphalt Raking,



Shoveling, Spreading; and Grade Checking.....	\$ 10.38	0.00
LABORER: Flagger.....	\$ 9.91	0.00
LABORER: Luteman.....	\$ 12.88	0.00
LABORER: Mason Tender - Cement/Concrete.....	\$ 11.27	0.00
LABORER: Pipelayer.....	\$ 13.44	0.00
LABORER: Laborer-Cones/ Barricades/Barrels - Setter/Mover/Sweeper.....	\$ 10.26	0.00
OPERATOR: Asphalt Spreader.....	\$ 14.71	0.00
OPERATOR: Backhoe/Excavator/Trackhoe.....	\$ 14.37	0.00
OPERATOR: Bobcat/Skid Steer/Skid Loader.....	\$ 11.64	0.00
OPERATOR: Broom/Sweeper.....	\$ 10.48	0.00
OPERATOR: Bulldozer.....	\$ 13.88	0.00
OPERATOR: Concrete Saw.....	\$ 15.50	0.00
OPERATOR: Crane.....	\$ 15.00	0.00
OPERATOR: Distributor.....	\$ 10.95	0.00
OPERATOR: Grader/Blade.....	\$ 14.28	0.00
OPERATOR: Grinding/Grooving Machine.....	\$ 15.90	0.00
OPERATOR: Loader.....	\$ 13.28	0.00
OPERATOR: Mechanic.....	\$ 13.00	0.00
OPERATOR: Milling Machine.....	\$ 14.68	0.00
OPERATOR: Mixer.....	\$ 14.25	0.00
OPERATOR: Oiler.....	\$ 12.35	0.00
OPERATOR: Paver (Asphalt, Aggregate, and Concrete).....	\$ 11.74	0.00
OPERATOR: Roller (All Types)....	\$ 10.85	0.00
OPERATOR: Scraper.....	\$ 12.25	0.00
OPERATOR: Tractor.....	\$ 10.56	0.00
TRUCK DRIVER: Flatbed Truck.....	\$ 14.06	0.00

TRUCK DRIVER: Lowboy Truck.....	\$ 12.08	0.00
TRUCK DRIVER: Mechanic.....	\$ 13.00	0.00
TRUCK DRIVER: Water Truck.....	\$ 10.00	0.00
TRUCK DRIVER: Dump Truck (All Types).....	\$ 11.02	0.00
TRUCK DRIVER: Semi/Trailer Truck.....	\$ 12.50	0.00

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WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

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Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (ii)).

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The body of each wage determination lists the classification and wage rates that have been found to be prevailing for the cited type(s) of construction in the area covered by the wage determination. The classifications are listed in alphabetical order of "identifiers" that indicate whether the particular rate is a union rate (current union negotiated rate for local), a survey rate (weighted average rate) or a union average rate (weighted union average rate).

#### Union Rate Identifiers

A four letter classification abbreviation identifier enclosed in dotted lines beginning with characters other than "SU" or "UAVG" denotes that the union classification and rate were prevailing for that classification in the survey. Example: PLUM0198-005 07/01/2014. PLUM is an abbreviation identifier of the union which prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. 07/01/2014 is the effective date of the most current negotiated rate, which in this example is July 1, 2014.

Union prevailing wage rates are updated to reflect all rate changes in the collective bargaining agreement (CBA) governing this classification and rate.

#### Survey Rate Identifiers

Classifications listed under the "SU" identifier indicate that

no one rate prevailed for this classification in the survey and the published rate is derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As this weighted average rate includes all rates reported in the survey, it may include both union and non-union rates. Example: SULA2012-007 5/13/2014. SU indicates the rates are survey rates based on a weighted average calculation of rates and are not majority rates. LA indicates the State of Louisiana. 2012 is the year of survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. 5/13/2014 indicates the survey completion date for the classifications and rates under that identifier.

Survey wage rates are not updated and remain in effect until a new survey is conducted.

#### Union Average Rate Identifiers

Classification(s) listed under the UAVG identifier indicate that no single majority rate prevailed for those classifications; however, 100% of the data reported for the classifications was union data. EXAMPLE: UAVG-OH-0010 08/29/2014. UAVG indicates that the rate is a weighted union average rate. OH indicates the state. The next number, 0010 in the example, is an internal number used in producing the wage determination. 08/29/2014 indicates the survey completion date for the classifications and rates under that identifier.

A UAVG rate will be updated once a year, usually in January of each year, to reflect a weighted average of the current negotiated/CBA rate of the union locals from which the rate is based.

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#### WAGE DETERMINATION APPEALS PROCESS

1.) Has there been an initial decision in the matter? This can be:

- \* an existing published wage determination
- \* a survey underlying a wage determination
- \* a Wage and Hour Division letter setting forth a position on a wage determination matter
- \* a conformance (additional classification and rate) ruling

On survey related matters, initial contact, including requests for summaries of surveys, should be with the Wage and Hour Regional Office for the area in which the survey was conducted because those Regional Offices have responsibility for the Davis-Bacon survey program. If the response from this initial contact is not satisfactory, then the process described in 2.) and 3.) should be followed.

With regard to any other matter not yet ripe for the formal process described here, initial contact should be with the Branch of Construction Wage Determinations. Write to:

Branch of Construction Wage Determinations  
Wage and Hour Division  
U.S. Department of Labor  
200 Constitution Avenue, N.W.  
Washington, DC 20210

2.) If the answer to the question in 1.) is yes, then an interested party (those affected by the action) can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Write to:

Wage and Hour Administrator  
U.S. Department of Labor  
200 Constitution Avenue, N.W.  
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and by any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3.) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board  
U.S. Department of Labor  
200 Constitution Avenue, N.W.  
Washington, DC 20210

4.) All decisions by the Administrative Review Board are final.

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END OF GENERAL DECISION