

STATE	PROJECT NUMBER	SHEET NO.
MISSISSIPPI	MP-7051-57(016)	1

GENERAL INDEX

INCLUDED THIS PROJECT	BEGIN WITH SHEET
<input checked="" type="checkbox"/> ROADWAY	1
<input type="checkbox"/> PERMANENT SIGNS	1001
<input type="checkbox"/> TRAFFIC SIGNALS	2001
<input type="checkbox"/> ITS COMPONENTS	3001
<input type="checkbox"/> LIGHTING	4001
<input type="checkbox"/> (RESERVED)	5001
<input checked="" type="checkbox"/> ROADWAY STANDARD DWGS	6001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (LRFD)	7001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (STD. SPEC.)	7501
<input type="checkbox"/> BRIDGE	8001
<input type="checkbox"/> CROSS SECTIONS	9001

STATE OF MISSISSIPPI

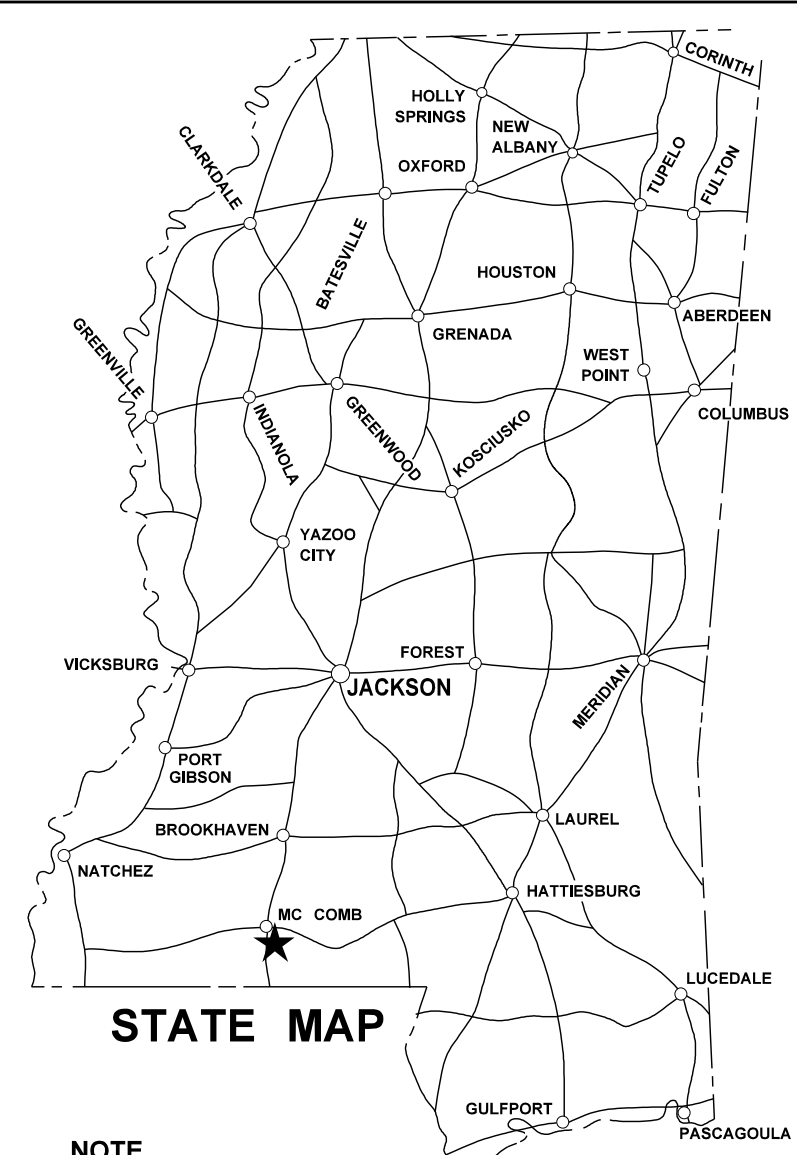
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE OF PROPOSED
STATE HIGHWAY
STATE PROJECT NO. MP-7051-57(016)

US 51
JCT S.R. 48 TO JCT U.S. 98
PIKE COUNTY

FMS. CONST. NO. 306061/301000

SCALES		
PLAN	1 IN. =	100 FT.
PROFILE { HOR. VERT.	1 IN. =	100 FT.
	1 IN. =	10 FT.
LAYOUT	1 IN. =	1,858 FT.



NOTE
★ INDICATES APPROXIMATE
LOCATION OF PROJECT.

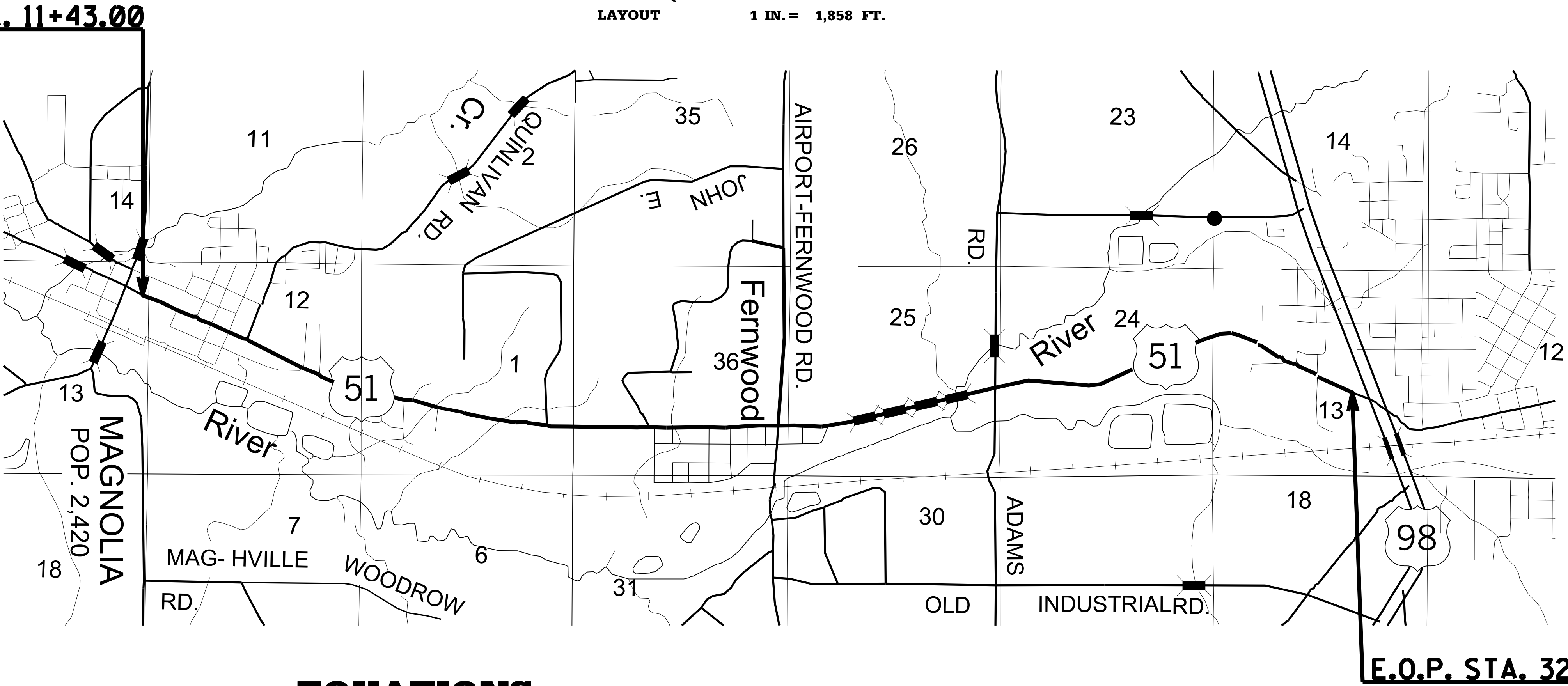
LAT. 31°11'04.44"N LONG. 90°27'02.30"W
(APPROX. MIDDLE OF PROJECT)

DESIGN CONTROL	
MPH =	V (SPEED DESIGN)
ADT () =	: ADT () =
DHV =	: D = % T = %

PERMITS ACQUIRED BY MDOT		
WETLANDS AND WATERS PERMITS (NECESSARY FOR ULTIMATE IMPROVEMENTS ONLY):		
	WATERS	WETLANDS
NATIONWIDE #14	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
NATIONWIDE (OTHER)*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
GENERAL*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
INDIVIDUAL (404)*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
* ACQUISITION OF PERMITS FOR TEMPORARY IMPACTS DURING CONSTRUCTION ARE THE RESPONSIBILITY OF THE CONTRACTOR		
STORMWATER PERMIT <input checked="" type="checkbox"/>		
Y	REQUIRED, CNOI SUBMITTED BY MDOT (DISTURBED AREA = 5 ACRES)	
S	REQUIRED, SCNOI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES)	
N	NO STORMWATER PERMIT REQUIRED (<1 ACRE)	
APPROVED BY: _____		

BRIDGE STRUCTURES REQ'D.

BOX BRIDGES REQ'D.



EQUATIONS

110 + 12.300 = 110 + 20.000	=	-7.70
194 + 92.510 = 194 + 74.000	=	18.51
217 + 07.670 = 217 + 14.330	=	-6.66
233 + 55.060 = 233 + 60.120	=	-5.06
271 + 99.350 = 272 + 02.350	=	-3.00
Total		-3.91

LENGTH DATA

LENGTH OF ROADWAY	31,848.810 FT.	6.032 MI.
LENGTH OF BRIDGES	836.000 FT.	0.158 MI.
LENGTH OF PROJECT (NET)	31,012.810 FT.	5.874 MI.
LENGTH OF EXCEPTIONS	0.000 FT.	0.000 MI.
LENGTH OF PROJECT (GROSS)	31,012.810 FT.	5.874 MI.

EXCEPTIONS

CONVENTIONAL SYMBOLS

COUNTY LINE	-----
TOWN CORPORATION LINE	-----
SECTION LINE	-----
EXISTING ROAD OR TRAVELED WAY	-----
PROPOSED ROAD OR TRAVELED WAY	-----
RAILROAD	-----
SURVEY LINE	-----
BRIDGES	-----

P S & E DATE: 03 /09 /2017


APPROVED:	
DEPUTY EXECUTIVE DIRECTOR / CHIEF ENGINEER	
EXECUTIVE DIRECTOR	



STATE	PROJECT NO.
MISS.	MP-7051-57(016)

DESCRIPTION OF SHEET	WKG. NO.	SH. NO.
TITLE AND LAYOUT SHEET (1)		1
DETAILED INDEX (1)	DI-1	2
GENERAL NOTES (1)	GN-1	3
TYPICAL SECTION SHEETS (2)		
TYPICAL SECTION - U.S. 51 MAINLINE	TS-1	4
TYPICAL SECTION - U.S. 51 MAINLINE	TS-2	5
QUANTITY SHEETS (5)		
SUMMARY OF ESTIMATED QUANTITIES	SQ-1	6
SUMMARY OF ESTIMATED QUANTITIES	SQ-2	7
ESTIMATED QUANTITY FOR U.S. 51 MILLING, ASPHALT, AND GRANULAR MATERIAL	EQ-1	8
ESTIMATED QUANTITY FOR U.S. 51 MILLING, ASPHALT, AND GRANULAR MATERIAL	EQ-2	9
ESTIMATED QUANTITY FOR TRAFFIC CONTROL SIGNING	TCPQ-1	10
PLAN & PROFILE SHEETS (11)		
STA. 10+37 TO STA. 36+00	WK3	11
STA. 36+00 TO STA. 66+00	WK4	12
STA. 66+00 TO STA. 96+00	WK5	13
STA. 96+00 TO STA. 126+00	WK6	14
STA. 126+00 TO STA. 156+00	WK7	15
STA. 156+00 TO STA. 186+00	WK8	16
STA. 186+00 TO STA. 215+00	WK9	17
STA. 215+00 TO STA. 245+00	WK10	18
STA. 245+00 TO STA. 275+00	WK11	19
STA. 275+00 TO STA. 305+00	WK12	20
STA. 305+00 TO STA. 330+64.670	WK13	21
SPECIAL DESIGN SHEETS (10)		
DETAIL OF CONSTRUCTION SIGNING - U.S. 51	DCS-1	22
DETAIL OF CONSTRUCTION SIGNING - U.S. 51	DCS-2	23
2-WAY CLEAR RAISED PAVEMENT MARKERS PLACED ON SIDE ROADS (2-LANE)	CRPMSR-2	24
LOCATION OF R16-3 SIGNS	LRS-1	25
HIGHWAY SIGN AND BARRICADE DETAIL FOR CONSTRUCTION PROJECTS	SDTCP-10	26
TRAFFIC CONTROL DETAILS DRUM PLACEMENT AND SHOULDER CLOSURE	TCP-SC	27
GUARDRAIL: TYPICAL INSTALLATION AT BRIDGE APPROACHES FOR 2-LANE, 2-WAY HIGHWAY	GR-4A-MOD	28
DETAIL FOR CURB AND ISLAND PAINTING	DCIS-1	29
CRG (CURVED RAIL GUARDRAIL) DETAIL	CRG-1-MOD	30
PAVEMENT MARKING DETAIL (YIELD BARS)	PMD-1	31
STANDARD DRAWINGS (12)		
PAVEMENT MARKING DETAILS FOR 2-LANE AND 4-LANE DIVIDED HIGHWAYS	PM-1	6120
PAVEMENT MARKING LEGEND DETAILS	PM-5	6124
PAVEMENT MARKINGS LEGEND DETAILS	PM-6	6125
TYPICAL PLACEMENT OF WARNING SIGNS AND PAVEMENT MARKINGS AT RAILROAD HIGHWAY GRADE CROSSINGS	PM-7	6126
GUARDRAIL: "W" BEAM (WOOD POSTS)	GR-1	6180
GUARDRAIL: "W" BEAM (STEEL POSTS)	GR-1B	6182
GUARDRAIL: BRIDGE END SECTIONS TYPE "A" & "C"	GR-2	6184
TYPICAL GUARDRAIL DELINEATION	SN-8C	6236
TRAFFIC CONTROL PLAN WITH FLAGGER (ONE-LANE CLOSURE OF TWO-WAY TRAFFIC)	TCP-1	6250
TRAFFIC CONTROL PLAN FOR MOBILE OPERATIONS MULTILANE ROADS AND TWO-LANE ROADS	TCP-11	6260
TRAFFIC CONTROL PLANS UNEVEN PAVEMENT DETAILS	TCP-14	6263
TEMPORARY STRIPING FOR 2-LANE AND 4-LANE DIVIDED HIGHWAYS	TCP-15	6264

[illegible]

					BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION					
					REVISION	DETAILED INDEX					
						COUNTY: PIKE					
						PROJECT NUMBER: MP-7051-57(016)					
											
						WORKING NUMBER DI-1					
					DATE	FILENAME: \02\Index.dgn					
					DESIGN TEAM	CHECKED	DATE	SHEET NUMBER 2			

STATE	PROJECT NO.
MISS.	MP-7051-57(061)

GENERAL NOTES

- 1

THE LOCATION AND SPACING OF SIGNS AS SHOWN ON THE TRAFFIC CONTROL PLANS ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.
- 2

ALL TRAFFIC CONTROL DEVICES ON THIS PROJECT SHALL COMPLY WITH THE MUTCD (LATEST EDITION).
- 3

FLUORESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.
- 4

ALL PLASTIC DRUMS SHALL HAVE A BALLASTING COLLAR MADE FROM RECYCLED TRUCK TIRES OR SUITABLE MATERIAL.
- 5

ALL SIGNS AND DELINEATORS THAT CONFLICT WITH THE CONSTRUCTION OF THIS PROJECT SHALL BE REMOVED AND RESET BY THE CONTRACTOR; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- 6

THE GRASS ON EXISTING SHOULDERS SHALL BE REMOVED PRIOR TO THE PLACEMENT OF SHOULDER MATERIAL BY LIGHTLY BLADING OR CLOSELY MOWING; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- 7

WHERE MILLING OF THE ROADWAY IS REQUIRED, THE CONTRACTOR SHALL PROVIDE OUTLETS IN THE EXISTING SHOULDER AT SUFFICIENT INTERVALS TO PREVENT POOLING OR STANDING WATER ON MILLED SURFACE; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- 8

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT EXISTING STRUCTURES SUCH AS PIPES, INLETS, APRONS, BRIDGES, ETC., FROM DAMAGE WHICH MIGHT OCCUR DURING CONSTRUCTION. THE CONTRACTOR SHALL REPLACE OR REPAIR, AS DIRECTED BY THE ENGINEER, ANY STRUCTURES DAMAGED DURING THE LIFE OF THE CONTRACT. NO PAYMENT WILL BE MADE FOR REPLACEMENT OR REPAIR OF DAMAGED ITEMS.
- 9

THE CONTRACTOR SHALL REMOVE EXISTING PAVEMENT MARKERS PRIOR TO PLACING ASPHALT; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- 10

TEMPORARY STRIPING IS REQUIRED IMMEDIATELY AFTER MILLING, PRELIMINARY LEVELING AND OVERLAYING OPERATION; WEATHER PERMITTED OR PRIOR TO OPENING THE AREA TO TRAFFIC; TEMPORARY STRIPING SHALL BE PLACED IN THE SAME LOCATION AND LAYOUT AS PERMANENT STRIPE. CARE SHOULD BE TAKING WHEN REMOVING THE STRIPE ON THE BRIDGES TO AVOID DAMAGE TO BRIDGE JOINT MATERIAL. ANY DAMAGE TO THE BRIDGE JOINTS WILL BE REPAIRED BY THE CONTRACTOR AND PAYMENT WILL BE ABSORBED UNDER THE REMOVAL OF STRIPE.
- 11

ALL ASPHALT AND CONCRETE CURBS ALONG RAMPS, LOCAL ROADS, ETC. FROM B.O.P. TO E.O.P. SHALL BE PAINTED (TWO APPLICATIONS) WITH WHITE TRAFFIC PAINT AND TRAFFIC BEADS PER DCIS-1; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- 12

IF THE ASPHALT CURB ALONG THE LOCAL ROAD IS SEVERELY DAMAGED, THE ENTIRE ASPHALT CURB WILL BE REMOVED AS DIRECTED BY THE ENGINEER; COST TO BE ABSORBED IN OTHER PAY ITEMS.

- 13

ALL LOCAL ROADS TO BE PAVED TO THE R.O.W. LIMITS OR AS DIRECTED.
- 14

VOIDS CREATED BY THE REMOVAL OF POSTS, CONCRETE ANCHORS, FOOTINGS, ETC. SHALL BE BACKFILLED AND TAMPED IN ACCORDANCE WITH SECTION 203 OF THE MISSISSIPPI STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- 15

ALL RAMPS AND DRIVEWAYS TO BE PAVED TO THE SHOULDER LINE, MINIMUM, OR FURTHER, AS DIRECTED WITHIN THE LIMITS OF THE R.O.W.
- 16

GRANULAR MATERIAL WILL NOT BE ALLOWED TO BE PLACED DIRECTLY ON THE SURFACE LIFT OF ASPHALT, BUT MUST BE PLACED DIRECTLY ON THE GRAVEL SHOULDER OR A ROAD WIDENER MACHINE USED AND APPROVED BY THE PROJECT ENGINEER.
- 17

PORTABLE SIGNS MAY BE USED IN LOCATATIONS WHERE POSTS CANNOT BE PLACED.
- 18

THE SAWCUT JOINTS SHALL BE DIRECTLY OVER THE EXISTING PCC PAVEMENT JOINT AND SHALL BE ACCURATELY LOCATED BY A METHOD EMPLOYING PINS AND STRINGLINE. THE PINS SHALL BE ACCURATELY LOCATED PRIOR TO PAVING. DETAILS OF THE CONTRACTOR'S METHOD FOR LOCATING THE SAWCUTS SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER. THE ENTIRE SAWING AND SEALING OPERATION SHALL BE COMPLETED WITHIN SEVEN (7) DAYS AFTER THE PLACEMENT OF THE FINAL WEARING COURSE.
- 19


A CN RAILROAD CROSSING EXISTS AT STA. 114+06 +/- . THE CONTRACTOR IS REQUIRED TO COORDINATE WITH CN RAILROAD AT LEAST 10 WORKING DAYS PRIOR TO ANY WORK ON THE RAILROAD PROPERTY. THE CONTACT PERSON FOR CN RAILROAD IS JOHN DINNING @ (601)914-2658. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PAY ALL BILLS ASSOCIATED WITH RAILROAD FLAGGING AND CABLE LOCATING.
- 20

EXISTING ASPHALT IS REQUIRED TO BE REMOVED FROM THE CURB AND GUTTER THROUGHOUT THE PROJECT.
- 21

BIDDERS ARE ADVISED THAT HARD COPIES OF ANY ADDENDA FOR THIS PROJECT WILL NO LONGER BE MAILED. ALL ADDENDA FOR THIS PROJECT WILL BE POSTED TO www.mdot.ms.gov UNDER THE PROPOSAL ADDENDA COLUMN. IT IS THE BIDDER'S RESPONSIBILITY TO CHECK AND SEE IF ANY ADDENDA HAVE BEEN POSTED FOR THIS PROJECT. PLEASE CONTACT CONTRACT ADMINISTRATION DIVISION AT 601-359-7700 FOR ANY QUESTIONS REGARDING ELECTRONIC ADDENDA.
- 22

MAINLINE ASPHALT OPERATIONS SHALL BEGIN NO LATER THAN 72 HOURS AFTER THE MAINLINE ROADWAY HAS BEEN MILLED FROM STA. 11+43 - STA. 18+00.

3/9/2017 10:44:56 AM 4003 GEN. NOTES.DGN PLAN DIVISION ROADWAY MISSISSIPPI DEPARTMENT OF TRANSPORTATION

					BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
					REVISION	GENERAL NOTES	
					DATE	PROJ. NO.: MP-7051-57(061) COUNTY: PIKE	
					DESIGN TEAM	FILENAME: (03)gen.notes.dgn	 WORKING NUMBER GN-1 SHEET NUMBER 3
					CHECKED	DATE	