



SM No. CMP3082760151

PROPOSAL AND CONTRACT DOCUMENTS

FOR THE CONSTRUCTION OF

13

Overlay approximately 6 miles of US 82 from Woodlawn to Leland, known as State Project No. MP-3082-76(015) / 306136301 in Washington County.

Project Completion: Flexible

(STATE DELEGATED)

NOTICE

**BIDDERS MUST PURCHASE A BID PROPOSAL FROM
MDOT CONTRACT ADMINISTRATION DIVISION TO BID
THIS PROJECT.**

Electronic addendum updates will be posted on www.gomdot.com

SECTION 900

OF THE CURRENT

2017 STANDARD SPECIFICATIONS

FOR ROAD AND BRIDGE CONSTRUCTION

JACKSON, MISSISSIPPI

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
TABLE OF CONTENTS**

PROJECT: MP-3082-76(015)/306136301 - Washington

Section 901 - Advertisement

Section 904 - Notice to Bidders

#1	Governing Specification
#3	Final Cleanup
#9	Federal Bridge Formula
#12	MASH Compliant Devices
#13	Safety Edge
#16	Standard Drawings
#83	Contract Time
#84	Scope of Work
#85	Smoothness Tolerances
#113	Tack Coat

Section 907 - Special Provisions

907-618-3 Additional Signing Requirements, w/Supplement

Section 905 - Proposal, Proposal Bid Items, Combination Bid Proposal

State Board of Contractors Requirement

State Certification Regarding Non-Collusion, Debarment and Suspensions

Section 902 - Contract Form

Section 903 - Contract Bond Forms

Progress Schedule

(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

04/26/2017 06:17 AM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 901 - ADVERTISEMENT

Electronic bids will be received by the Mississippi Transportation Commission at 10:00 o'clock A.M., Tuesday, May 23, 2017, from the Bid Express Service and shortly thereafter publicly read on the Sixth Floor for:

Overlay approximately 6 miles of US 82 from Woodlawn to Leland, known as State Project No. MP-3082-76(015) / 306136301 in Washington County.

The attention of bidders is directed to the predetermined minimum wage rate set by the U. S. Department of Labor under the Fair Labor Standards Act.

The Mississippi Department of Transportation hereby notifies all bidders that it will affirmatively insure that in any contract entered into pursuant to this advertisement, disadvantaged business enterprises will be afforded full opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of race, color, sex, age, disability, religion or national origin in consideration for an award.

The specifications are on file in the offices of the Mississippi Department of Transportation.

Bid proposals must be purchased online at <https://shopmdot.ms.gov>. Specimen proposals may be viewed and downloaded online at no cost at <http://mdot.ms.gov> or purchased online. Proposals are available at a cost of Ten Dollars (\$10.00) per proposal plus a small convenience fee. Cash or checks will not be accepted as payment.

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

MELINDA L. MCGRATH
EXECUTIVE DIRECTOR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Governing Specifications

The current (2017) Edition of the Standard Specifications for Road and Bridge Construction adopted by the Mississippi Transportation Commission is made a part hereof fully and completely as if it were attached hereto, except where superseded by special provisions, or amended by revisions of the Specifications contained within this proposal. Copies of the specification book may be purchased from the MDOT Construction Division, or online at shopmdot/default.aspx?StoreIndex=1.

A reference in any contract document to controlling requirements in another portion of the contract documents shall be understood to apply equally to any revision or amendment thereof included in the contract.

In the event the plans or proposal contain references to the 2004 Edition of the Standard Specifications for Road and Bridge Construction, it is to be understood that such references shall mean the comparable provisions of the 2017 Edition of the Standard Specifications.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 3

CODE: (SP)

DATE: 01/17/2017

SUBJECT: Final Clean-Up

Immediately prior to final inspection for release of maintenance, the Contractor shall pick up, load, transport and properly dispose of all litter from the entire highway right-of-way that is within the termini of the project.

Litter shall include, but not be limited to, solid wastes such a glass, paper products, tires, wood products, metal, synthetic materials and other miscellaneous debris.

Litter removal is considered incidental to other items of work and will not be measured for separate payment.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 9

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Federal Bridge Formula

Bidders are hereby advised that the latest revision of Federal Highway Administration Publication No. FHWA-HOP-06-105, **BRIDGE FORMULA WEIGHTS**, dated August 2006, is made a part of this contract when applicable.

Prior to the preconstruction conference, the Contractor shall advise the Engineer, in writing, what materials, if any, will be delivered to the jobsite via Interstate route(s).

Copies of the **BRIDGE FORMULA WEIGHTS** publication may be obtained by contacting:

Federal Highway Administration
400 7th Street, SW
Washington, DC 20590
(202) 366-2212

or

http://www.ops.fhwa.dot.gov/Freight/publications/brdg_frm_wgths/bridge_formula_all_rev.pdf

An on line **BRIDGE FORMULA WEIGHTS CALCULATOR** is available at

http://ops.fhwa.dot.gov/freight/sw/brdgcalc/calc_page.htm

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 12

CODE: (IS)

DATE: 03/01/2017

SUBJECT: MASH Compliant Devices

Bidders are hereby advised that the Standard Specifications may require certain traffic control and permanent safety hardware devices to meet the requirements of the Manual for Assessing Safety Hardware (MASH). However, devices meeting the requirements of NCHRP Report 350 will be allowed until the mandatory effective date for MASH compliance. The following table shows the effective dates for MASH compliant devices.

Device	Effective Date for MASH Compliance
W-beam barriers, cast-in-place concrete barriers	December 31, 2017
W-beam terminals	June 30, 2018
Cable barriers, cable barrier terminals, crash cushions	December 31, 2018
Bridge rails, transitions, all other longitudinal barriers including portable barriers installed permanently, all other terminals, sign supports, all other breakaway hardware	December 31, 2019

Temporary work zone devices, including portable barriers manufactured after December 31, 2019, must have been successfully tested to the 2016 Edition of MASH. Such devices manufactured on or before this date and successfully tested to NCHRP Report 350 or the 2009 Edition of MASH may continue to be used throughout their normal service lives.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 13

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Safety Edge

Bidders are hereby advised that the Shoulder Wedge (Safety Edge) specified in Section 401, Asphalt Pavements, shall only apply to the top two (2) lifts of asphalt. Open Graded Friction Courses (OGFC) are not to be considered a lift as it pertains to safety edge. Attached is a drawing showing the safety edge. Note that the shoulder dimensions in the bottom two drawings will be less than three feet (3').



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 16

CODE: (SP)

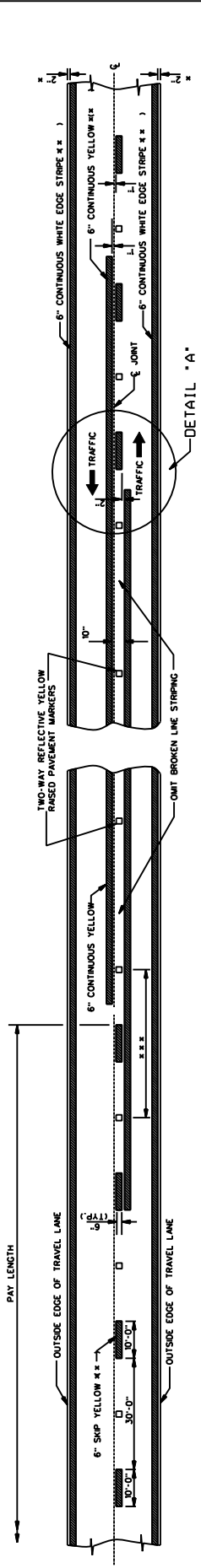
DATE: 03/01/2017

SUBJECT: Standard Drawings

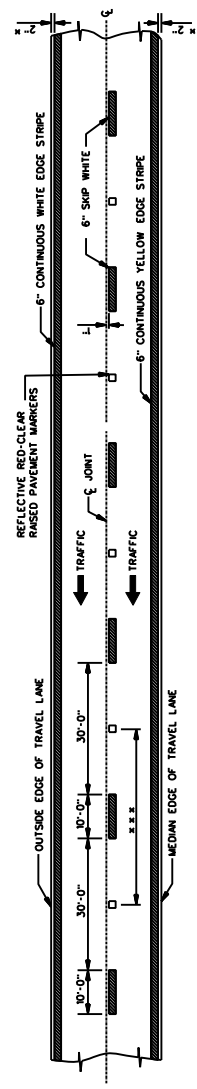
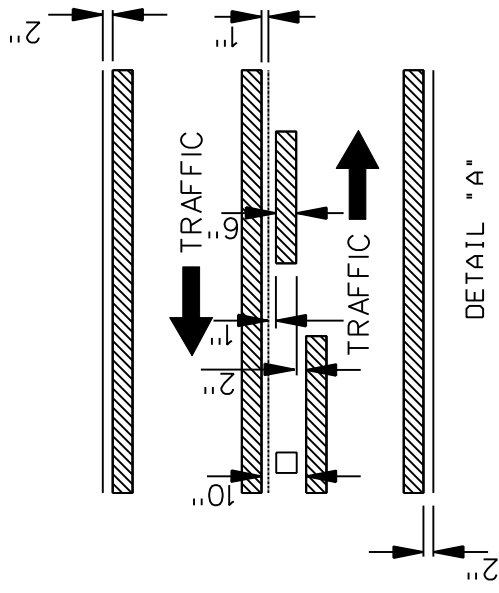
Standard Drawings attached hereto shall govern appropriate items of required work.

Larger copies of Standard Drawings may be purchased from:

MDOT Plans Print Shop
MDOT Shop Complex, Building C, Room 114
2567 North West Street
P.O. Box 1850
Jackson, MS 39215-1850
Telephone: (601) 359-7460
or FAX: (601) 359-7461
or e-mail: plans@mdot.state.ms.us



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



4-LANE WITH ONE-WAY TRAFFIC



GENERAL NOTES:

- * 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- ** 2. EDGE STRIPES SHALL BE SAME MATERIAL AS LANE-LINE STRIPES (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
- *** 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	URBAN AREA (U/Lin)	RURAL AREA (U/Lin)
HORIZONTAL CURVES	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	40'-0"
	40'-0"	140'-0"

1. NOTES: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP AND 1000' BEHIND (IN DIRECTION OF TRAFFIC) OF THE ENTRANCE RAMP TAPER.

4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED SOURCES OF MATERIALS."

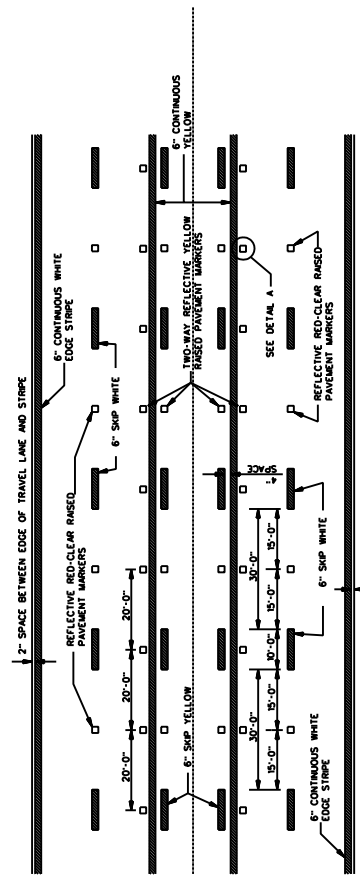
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

PAVEMENT MARKING
DETAILS FOR
2-LANE AND 4-LANE
DIVIDED HIGHWAYS

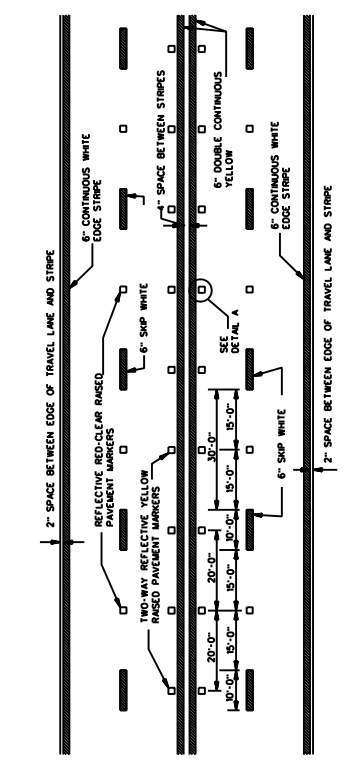
ISSUE DATE: OCTOBER 1, 1998

WORK NUMBER: PM-1
SHEET NUMBER: 100

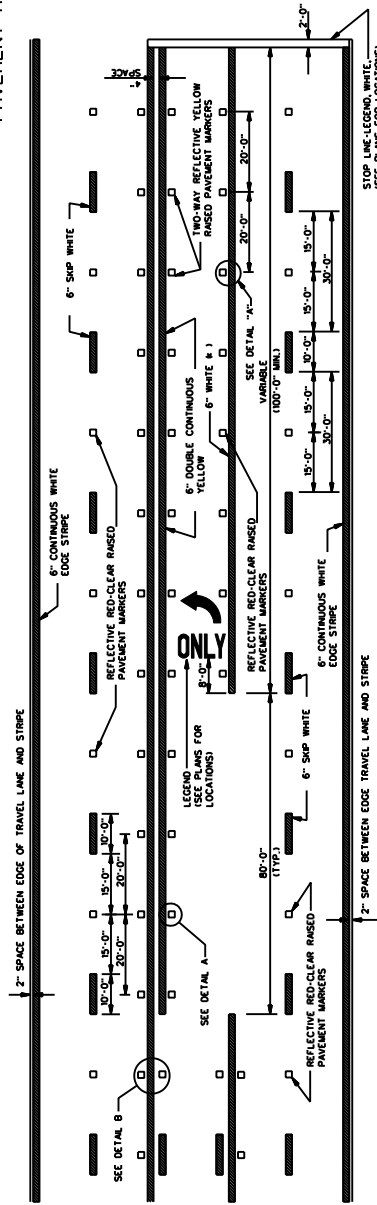
STATE	PROJECT NO.
MISS.	



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 5-LANE SECTION

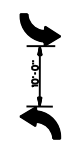


TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS AT LEFT TURN LANES

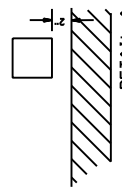
*NOTE: USE DETAIL STRIPING IF LENGTH \leq 150' AT THIS LOCATION, OTHERWISE USE CONTINUOUS STRIPING.



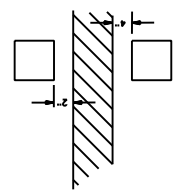
TYPICAL TWO-WAY ARROW INSTALLATION

- NOTES: 1. CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY.
- 2. IF SEGMENT IS LESS THAN 300', PLACE ONE SET OF ARROWS IN CENTER OF SEGMENT.
- 3. IF SEGMENT IS GREATER THAN 300', PLACE FIRST SET OF ARROWS 50' TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS (250' O.C.).

GENERAL NOTE:
1. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MOST APPROVED SOURCE OF MATERIALS.



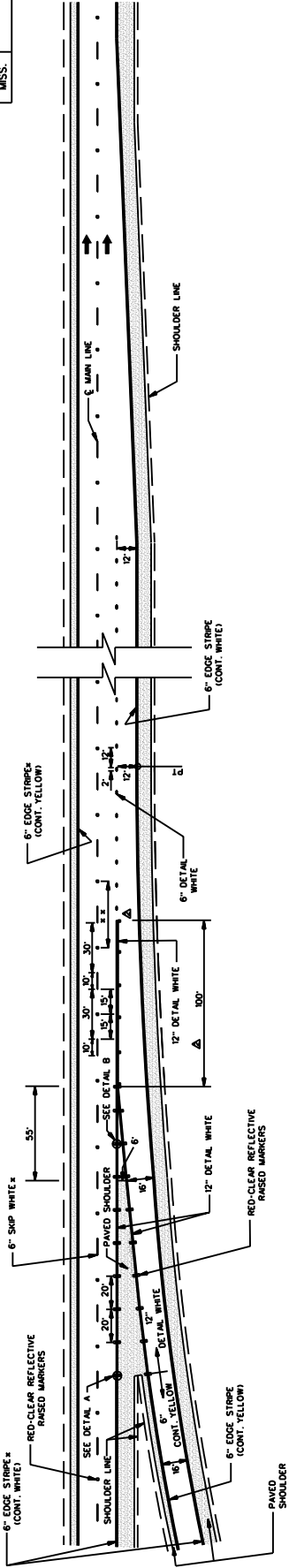
DETAIL A
LATERAL PLACEMENT OF PAVEMENT MARKERS



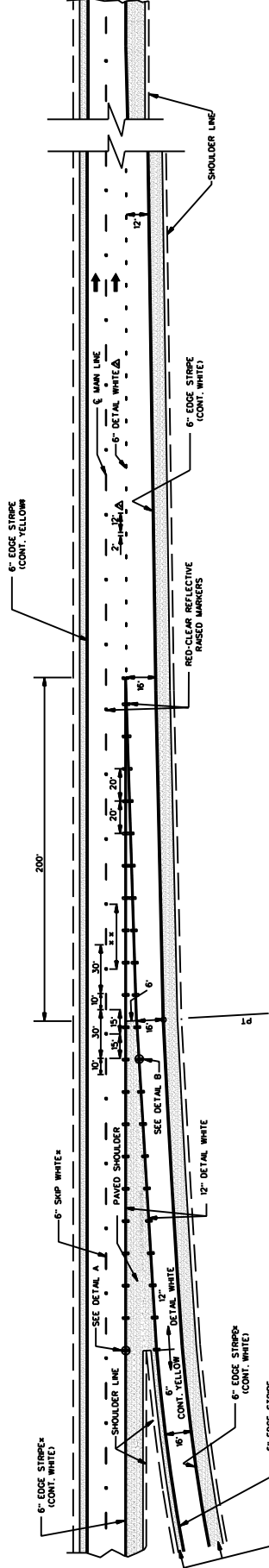
DETAIL B
LATERAL PLACEMENT OF PAVEMENT MARKERS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
PAVEMENT MARKING	
DETAILS FOR	
4-LANE AND 5-LANE	
UNDIVIDED ROADWAYS	
DATE	03/15/16
FILE NAME	SDPM-2.DGN
DESIGNER	CH2M
CHECKER	
DATE	03/15/16

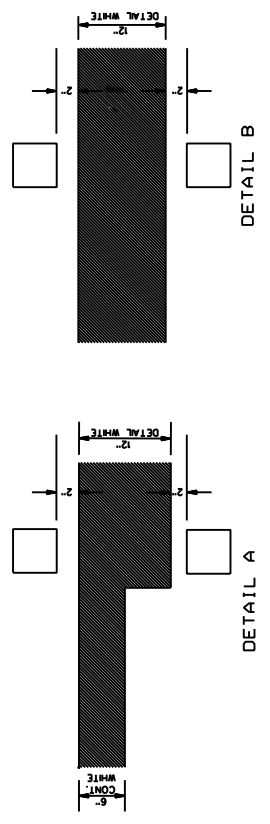
STATE	PROJECT NO.
MISS.	



PARALLEL ENTRANCE RAMP



TAPER ENTRANCE RAMP



- GENERAL NOTES:
- * 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPES THROUGHOUT THE INTERCHANGE AREA BEGINNING 100' IN THE DIRECTION OF TRAVEL FROM THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - * 2. ON THE MAIN FACILITY, PLACE REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS AT A 40' SPACING ON ALL LANE-LINES THROUGHOUT THE INTERCHANGE AREA BEGINNING 100' IN THE DIRECTION OF TRAVEL FROM THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - 3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED SOURCES OF MATERIALS."

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION

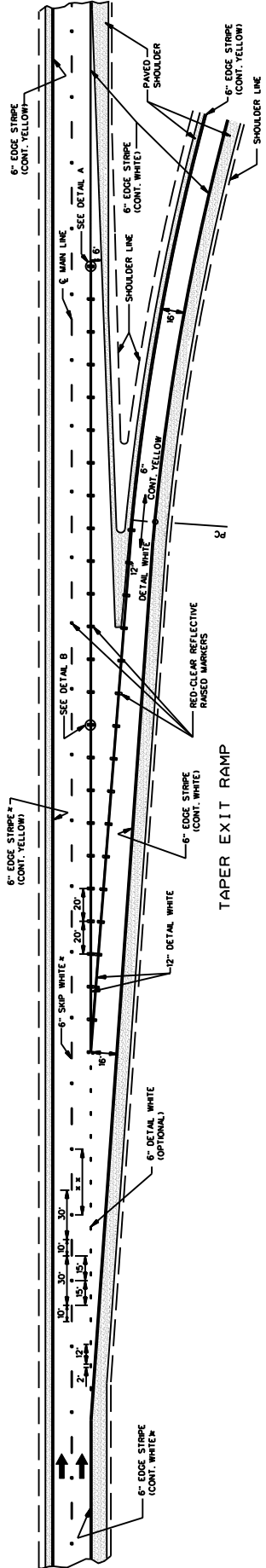
PAVEMENT MARKING
DETAILS FOR
INTERCHANGE RAMP
(PARALLEL AND TAPER)

ISSUE DATE: OCTOBER 1, 1998

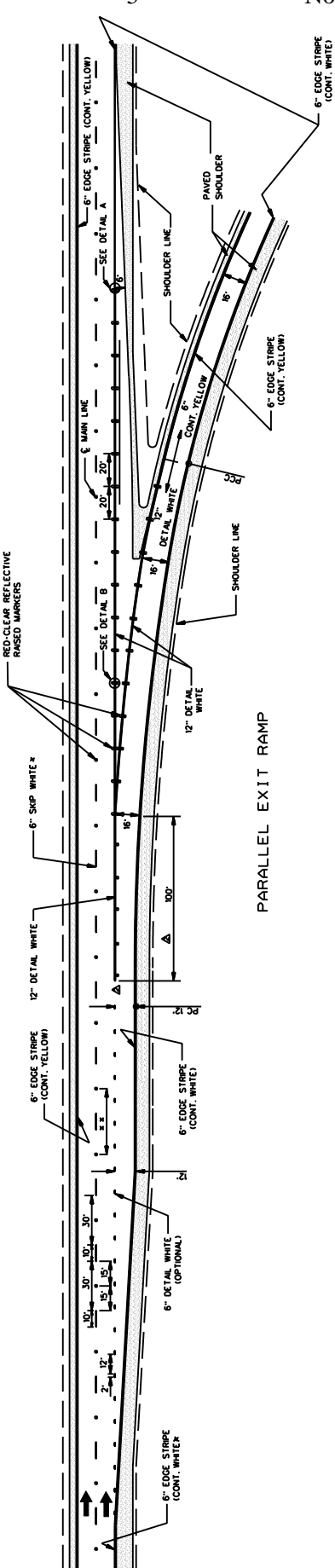
WORKING NUMBER: SDPM-3

SHEET NUMBER: 12

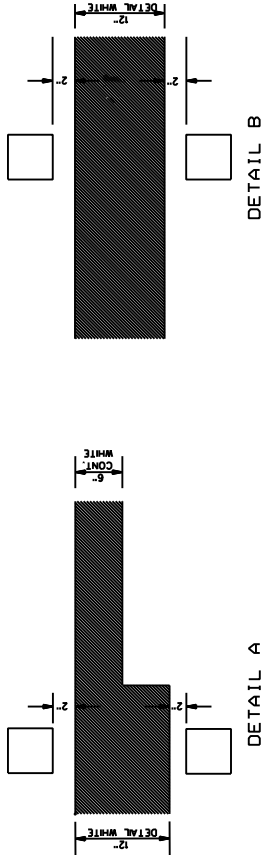
STATE	PROJECT NO.
MISS.	



TAPER EXIT RAMP



PARALLEL EXIT RAMP



- GENERAL NOTES:
- SEE SHEET PM-1 FOR THE PLACEMENT OF LINE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELED WAY.
 - ON LINE-LINE MARKINGS, PLACE REFLECTIVE RED-CLEAR BASED PAVEMENT MARKING (RCPM) IN THE INTERCHANGE AREA BEGINNING 1000' IN ADVANCE IN DIRECTION OF TRAFFIC OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE PAVEMENT MARKERS (HPM) AS SHOWN IN THE MOOT "APPROVED SOURCES OF MATERIALS."

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION

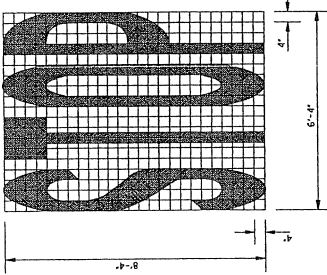
PAVEMENT MARKING
DETAILS FOR
INTERCHANGE
EXIT RAMP
(PARALLEL AND TAPER)

ISSUE DATE: OCTOBER 1, 1998

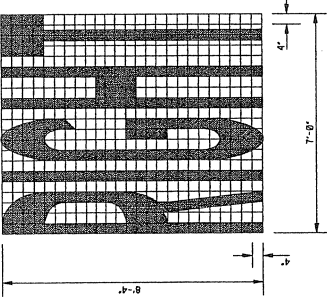
WORKING NUMBER: SDPM-41
SHEET NUMBER: 13

DATE	BY	DESCRIPTION
8/21/98	REVISION	REVISION
8/27/98	UPDATE TO 2008 M/C/D	UPDATE TO 2008 M/C/D

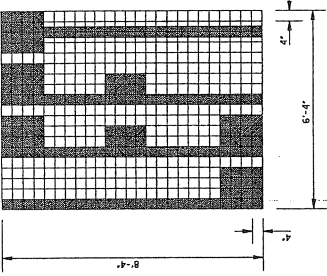
STATE MISS.	PROJECT NO.		
----------------	-------------	--	--



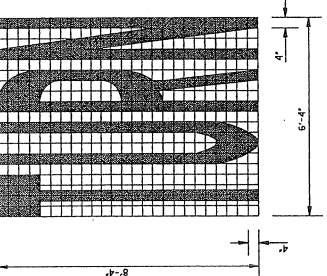
8'-4" x 6'-4"



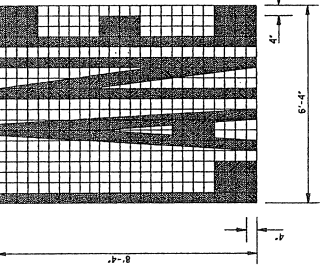
8'-4" x 7'-0"



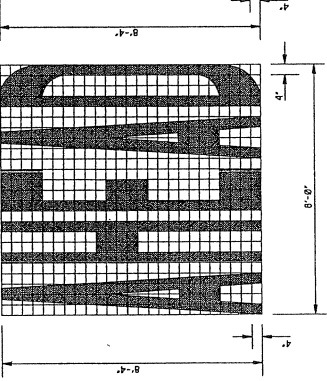
8'-4" x 6'-4"



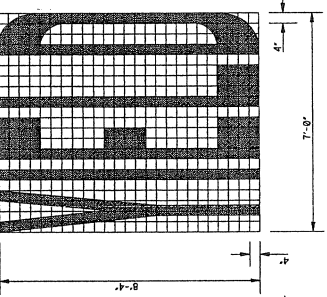
8'-4" x 6'-4"



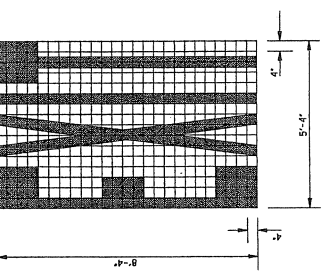
8'-4" x 6'-4"



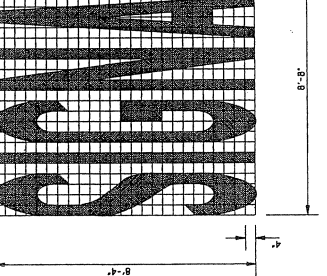
8'-4" x 8'-0"



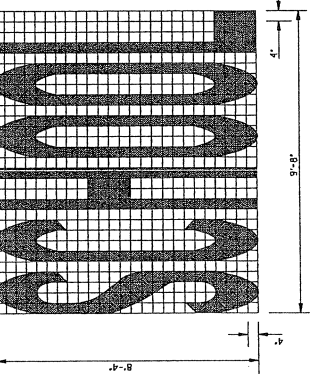
8'-4" x 7'-0"



8'-4" x 5'-4"



8'-4" x 8'-0"



8'-4" x 9'-0"

GENERAL NOTES:

- UNLESS OTHERWISE SHOWN ON THE PLANS, ALL PAVEMENT MARKING LEGENDS SHALL BE APPLIED USING HIGH PERFORMANCE POLYURETHANE PAINT.
- TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTORS OF 1/4" OR LESS) AND EXTENDING LETTERS ARE PERMITTED.
- FOR OTHER DETAILS SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
LEGEND DETAILS**

ISSUE DATE: OCTOBER 1, 1998

REVISION	DATE

REPORT NUMBER: PM-3
SHEET NUMBER: 124

STATE PROJECT NO.					
MISS.					

TURN ARROW

THRU ARROW

COMBINATION ARROW

1-WAY ARROW

GENERAL NOTES:

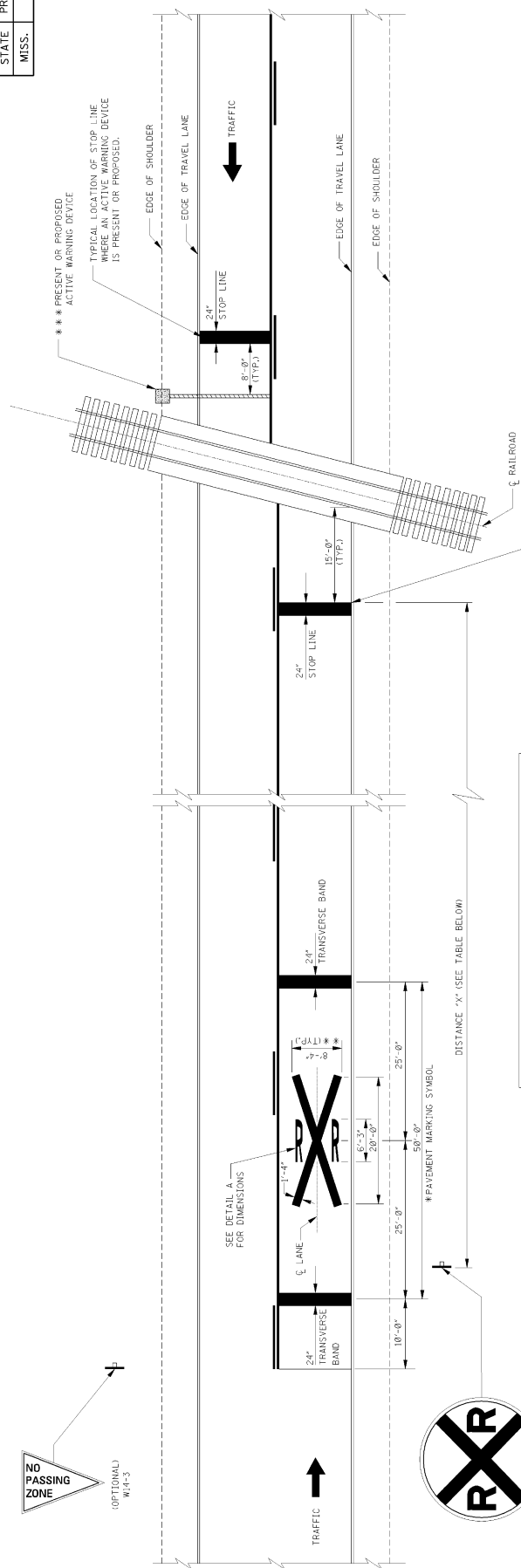
- UNLESS OTHERWISE SHOWN ON THE PLANS, ALL PAVEMENT MARKING SHALL BE APPLIED USING HIGH PERFORMANCE MATERIALS.
- TWO HORIZONTAL GAPS CAUSED BY TEMPLATE CONNECTORS OF 1/2" OR LESS AND EXTENDING THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
- FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM PAVEMENT MARKING LEGENDS.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

PAY QUANTITIES	
LEGEND/SYMBOL	AREA (ft ²)
ONLY	22.0
TURN ARROW	18.4
THRU ARROW	21.2
COMB. ARROW	27.5
1-WAY ARROW	24.3

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
LEGEND DETAILS**

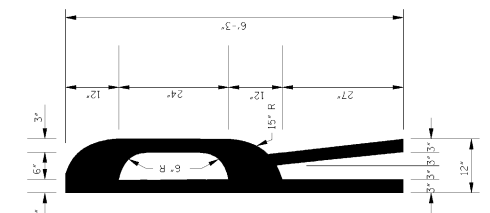
DATE	BY	REVISION	ISSUE DATE:	OCTOBER 1, 1998	SHEET NUMBER	125



ADVANCE WARNING SIGN PLACEMENT DISTANCE

POSTED SPEED (mph)	DISTANCE "X" (FT)	
	RURAL	URBAN
20	175	100
25	250	100
30	325	100
35	400	150
40	475	225
45	550	300
50	625	375
55	700	450
60	775	550

NOTES:
 ① DISTANCE "X" MAY BE ADJUSTED, IF PROHIBITIVE PHYSICAL CONDITIONS EXIST AT THE DESIGNATED DISTANCE.
 ② THESE DISTANCES MAY BE ADJUSTED TO A MINIMUM OF 100' IN BUSINESS OR BUSINESS DISTRICTS WHERE LOW SPEEDS ARE PREVALENT.



DETAIL A
STANDARD "R" PAVEMENT MARKING
FOR R X R SYMBOL

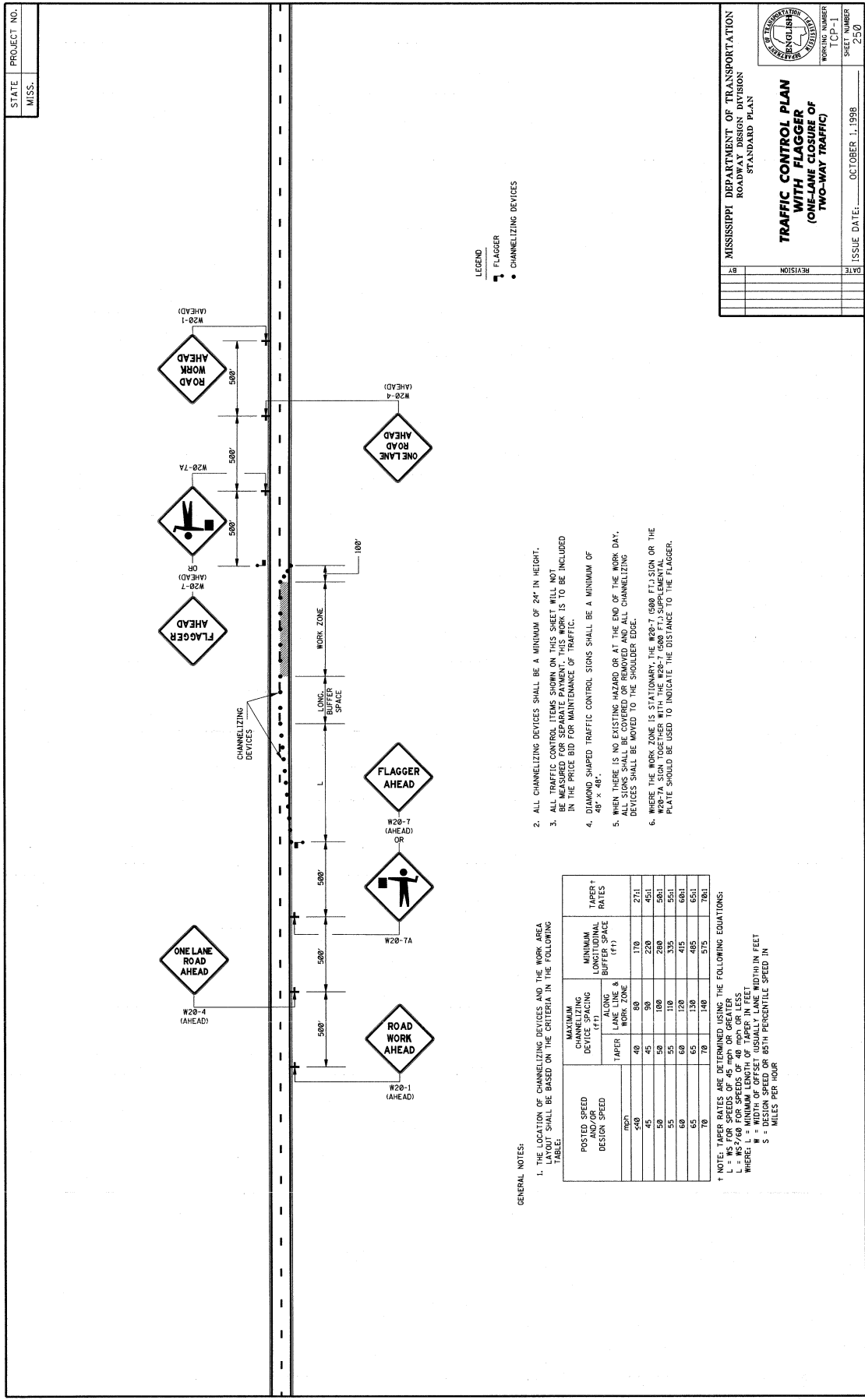
GENERAL NOTES:
 * 1. A PORTION OF THE PAVEMENT MARKING SYMBOL SHOULD BE DIRECTLY OPPOSITE THE ADVANCE WARNING SIGN (W10-1).
 * 2. WIDTH OF R X R SYMBOL MAY VARY ACCORDING TO LANE WIDTH. HOWEVER, ON MULTI-LANE ROADS, THE TRANSVERSE BANDS AND STOP LINE SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.
 * 3. R X R SYMBOL (63.0 ft²), TRANSVERSE BANDS AND STOP LINE SHALL BE PAID FOR AS LEGEND, WHITE (PLASTIC), MATERIAL OPTIONAL FOR OTHER AGENCIES.
 * 4. REFER TO THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" FOR LOCATION OF PROPOSED WARNING DEVICES AT RAILROAD-HIGHWAY GRADE CROSSINGS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TYPICAL PLACEMENT OF WARNING SIGNS AND PAVEMENT MARKINGS AT RAILROAD HIGHWAY CROSSINGS

WORKSHEET NO. 100
SHEET NUMBER 12 OF 18
ISSUE DATE: OCTOBER 11, 1998

DATE	REVISION
10-18-80	REVISE NOTE

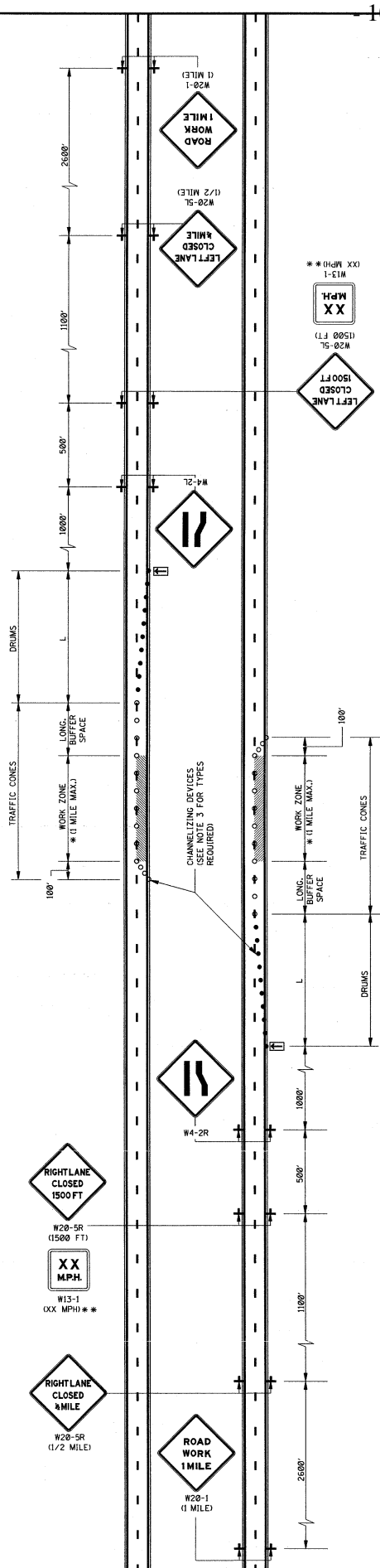


MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN

**TRAFFIC CONTROL PLAN
 WITH FLAGGER
 (ONE-LANE CLOSURE OF
 TWO-WAY TRAFFIC)**

WORKING NUMBER: TCP-1
 SHEET NUMBER: 250
 ISSUE DATE: OCTOBER 1, 1998

DATE	REVISION	BY



- LEGEND**
- * OR AS SHOWN ELSEWHERE OF THE PLANS.
 - ** THE LEGEND ON W13-1 (XX MPH) SUPPLEMENTAL PLATE SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT.
 - REFLECTORIZED PRE-STANDING PLASTIC DRUMS
 - TRAFFIC CONES (28" HEIGHT)

2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICE TYPES FOR:
 - A. APPROACH TAPER- REFLECTORIZED PLASTIC DRUMS
 - B. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT)
 - C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT)
4. WHEN THERE IS NO EXISTING HAZARD, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING SIGNS- PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS- PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE PREVIOUS ZONE.
6. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
7. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".

GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

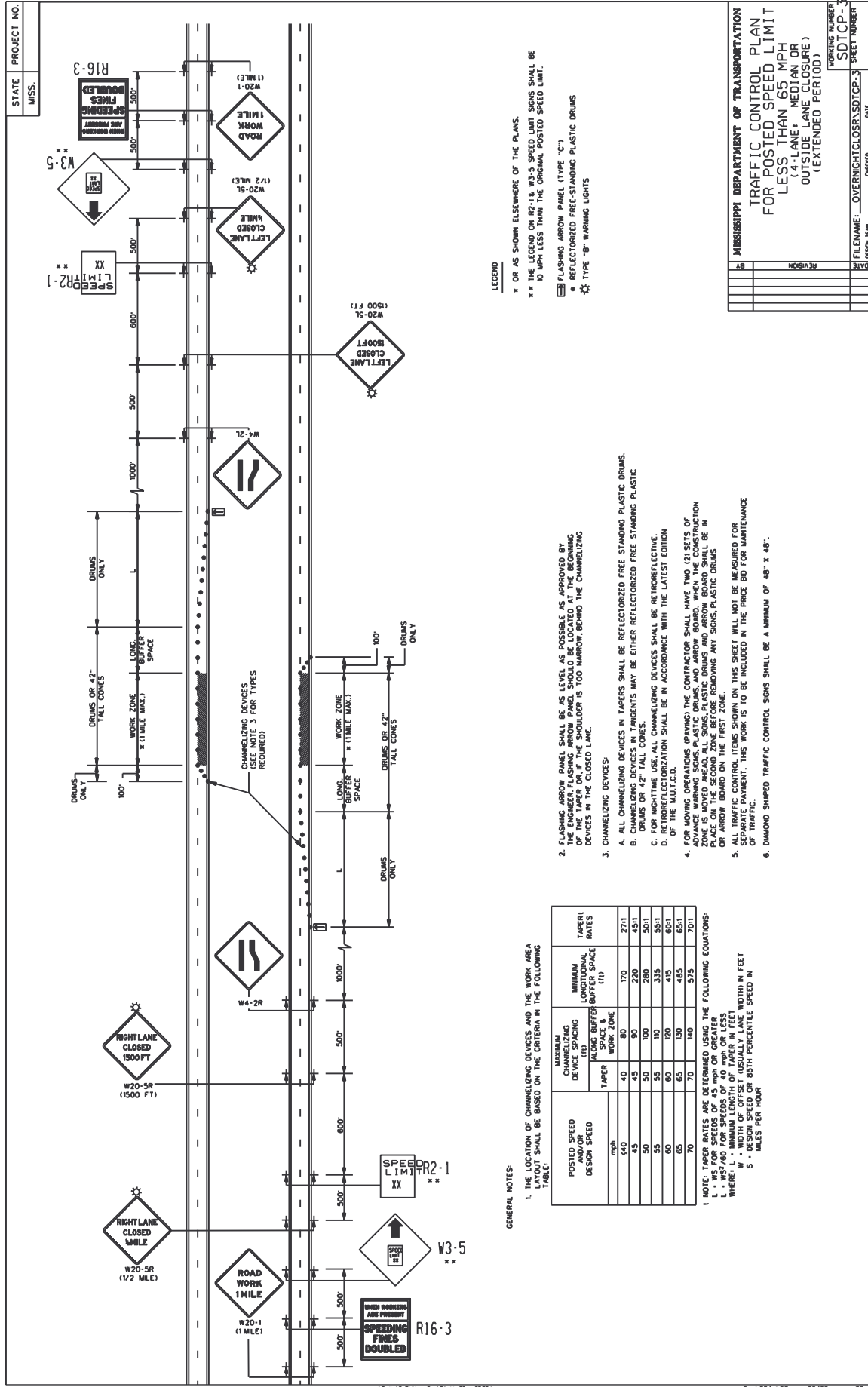
POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (FT)		MINIMUM LONGITUDINAL BUFFER SPACE (ft)	TAPER + TAPER RATES
	ALONG LANE LINE	WORK ZONE		
40	40	80	170	27:1
45	45	90	220	45:1
50	50	100	280	50:1
55	55	110	335	55:1
60	60	120	415	60:1
65	65	130	485	65:1
70	70	140	575	70:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 $L = W^2$ FOR SPEEDS OF 45 MPH OR GREATER
 $L = WS$ FOR SPEEDS OF 30 MPH OR LESS
 WHERE L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET USUALLY LANE WIDTH IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN

**TRAFFIC CONTROL PLAN
 FOR POSTED SPEED LIMIT
 LESS THAN 65 MPH
 (4-LANE, MEDIAN LANE
 OR CURB AND GUTTER CLOSURE)
 (WORK DAY ONLY)**

WORKING NUMBER: TCP-2
 SHEET NUMBER: 251
 ISSUE DATE: OCTOBER 1, 1998



STATE MISS. PROJECT NO. R16-3

W3-5

XX SPEED LIMIT 20-1

W20-1 (1 MILE)

W20-5L (1/2 MILE)

W4-2L

W4-2R

W20-5R (1500 FT)

W20-1 (1 MILE)

W3-5

XX SPEED LIMIT 20-1

R16-3

LEGEND

- x OR AS SHOWN ELSEWHERE OF THE PLANS.
- ** THE LEGEND ON R2-18 W3-5 SPEED LIMIT SIGNS SHALL BE 10 MPH LESS THAN THE ORIGINAL POSTED SPEED LIMIT.
- ◻ FLASHING ARROW PANEL (TYPE "C")
- ◊ REFLECTORIZED FREE-STANDING PLASTIC DRUMS
- ⊛ TYPE "B" WARNING LIGHTS

2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR AT THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.

3. CHANNELIZING DEVICES:

- A. ALL CHANNELIZING DEVICES IN TAPERS SHALL BE REFLECTORIZED FREE STANDING PLASTIC DRUMS.
 - B. CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER REFLECTORIZED FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.
 - C. FOR NIGHTTIME USE, ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE.
 - D. RETROREFLECTORIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE M.U.T.C.D.
4. FOR MOVING OPERATIONS (TAPERING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF CHANNELIZING DEVICES IN EACH TAPER. CHANNELIZING DEVICES SHALL BE PLACED ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
5. ALL CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
6. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" x 48".

GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

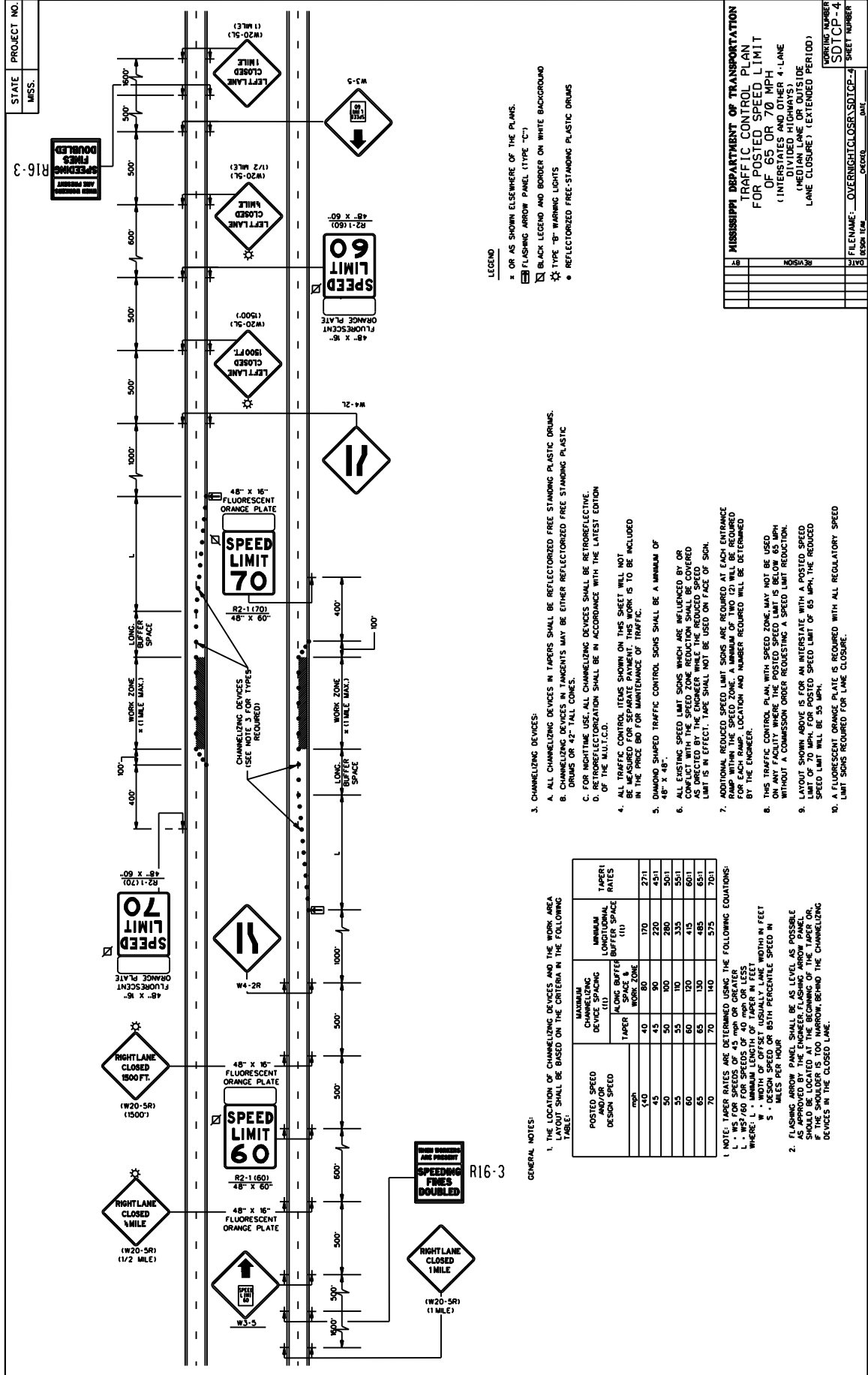
POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICES SPACING ALONG BUFFER SPACE IN TAPER		MINIMUM LONGITUDINAL BUFFER SPACE (L)	TAPER RATES
	(1)	(2)		
40	40	80	170	27:1
45	45	90	220	45:1
50	50	100	280	50:1
55	55	110	335	55:1
60	60	120	415	60:1
65	65	130	485	65:1
70	70	140	575	70:1

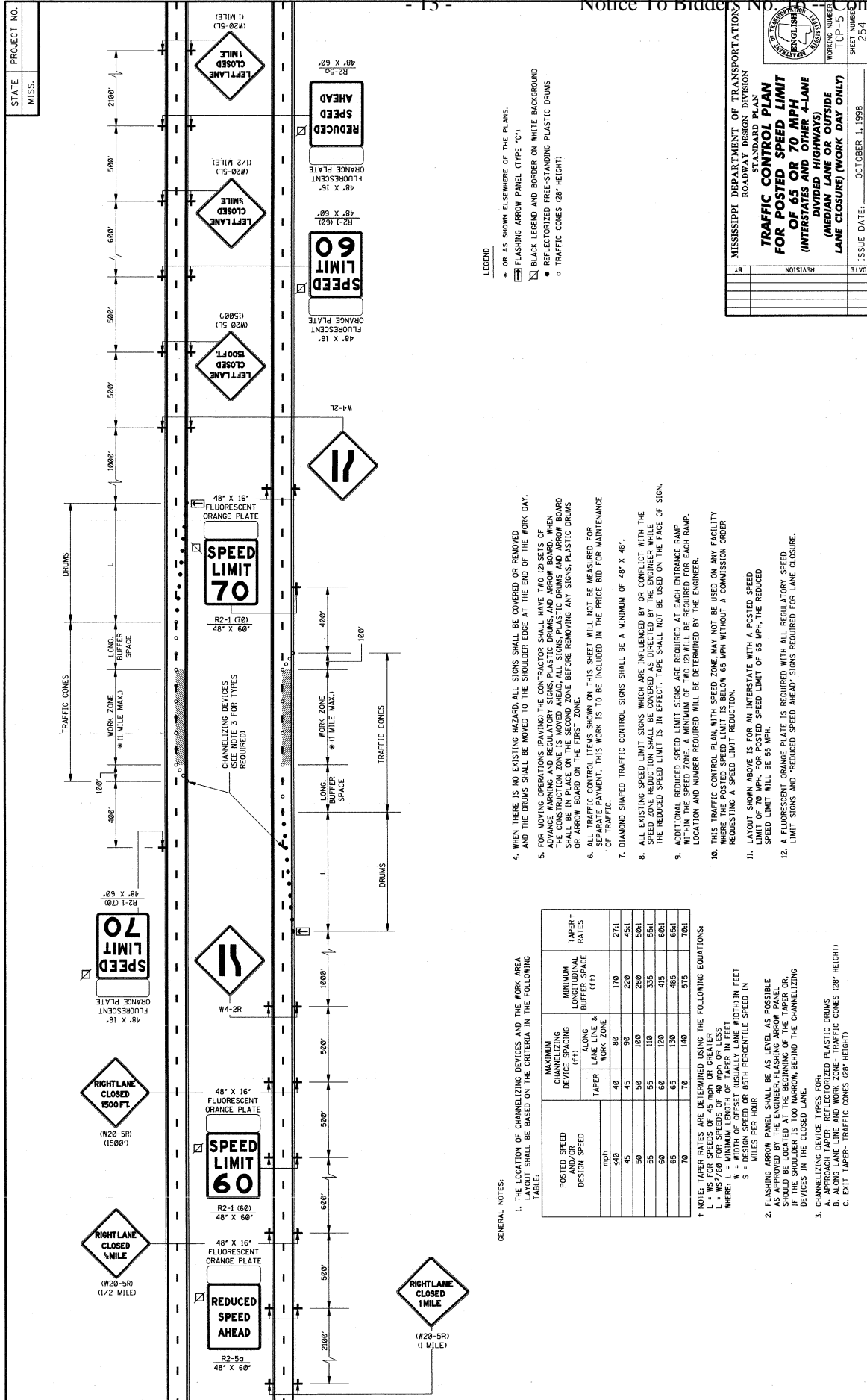
1. NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 L = WS FOR SPEEDS OF 45 MPH OR GREATER
 L = WS/40 FOR SPEEDS OF 40 MPH OR LESS
 WHERE:
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL PLAN
FOR POSTED SPEED LIMIT
LESS THAN 65 MPH
 (4 LANE MEDIAN OR
 DUAL LANE CLOSURE)
 (EXTENDED PERIOD)

PROJECT NUMBER: SDT CP-3
 SHEET NUMBER: 11

DATE: _____
 REVISION: _____
 DESIGN ENGINEER: _____
 CHECKED: _____





GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:
2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE. APPROACH TAPER REFLECTORIZED PLASTIC DRUMS SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICE TYPES FOR:
 - A. APPROACH TAPER- REFLECTORIZED PLASTIC DRUMS
 - B. EXIT TAPER- REFLECTORIZED PLASTIC DRUMS (28" HEIGHT)
 - C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT)
4. WHEN THERE IS NO EXISTING HAZARD, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS PAYING THE CONTRACTOR SHALL HAVE TWO (2) SETS OF CHANNELIZING DEVICES AND TWO (2) SETS OF TRAFFIC CONES. THE CONSTRUCTION ZONE IS MOVED AHEAD ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD OR ARROW BOARD ON THE FIRST ZONE.
6. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
7. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".
8. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON THE FACE OF SIGN.
9. ADDITIONAL REDUCED SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. A MINIMUM OF TWO (2) WILL BE REQUIRED FOR EACH RAMP. LOCATION AND NUMBER REQUIRED WILL BE DETERMINED BY THE ENGINEER.
10. THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
11. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.
12. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS AND "REDUCED SPEED AHEAD" SIGNS REQUIRED FOR LANE CLOSURE.

POSTED SPEED DESIGN SPEED mph	CHANNELIZING DEVICE SPACING (FT)		MINIMUM LONGITUDINAL BUFFER SPACE (FT)	TAPER RATES
	LANE LINE & WORK ZONE	LANE LINE & WORK ZONE		
50	40	80	170	27:1
55	45	90	220	45:1
60	50	100	280	50:1
65	55	110	335	55:1
70	60	120	415	60:1
75	65	130	485	65:1
80	70	140	575	70:1


† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 L = WS FOR SPEEDS OF 45 mph OR GREATER
 L = WS²/60 FOR SPEEDS OF 40 mph OR LESS
 WHERE:
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

LEGEND

- * OR AS SHOWN ELSEWHERE OF THE PLANS.
- FLASHING ARROW PANEL (TYPE "C")
- BLACK LEGEND AND BORDER ON WHITE BACKGROUND
- REFLECTORIZED FREE-STANDING PLASTIC DRUMS
- TRAFFIC CONES (28" HEIGHT)

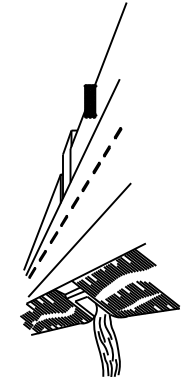
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN PLAN
 STANDARD PLAN
**TRAFFIC CONTROL PLAN
 FOR POSTED SPEED LIMIT
 OF 65 OR 70 MPH
 (INTERSTATES AND OTHER 4-LANE
 DIVIDED HIGHWAYS)
 (MEDIAN LANE OR OUTSIDE
 LANE CLOSURE) (WORK DAY ONLY)**

WORKING NUMBER: TCP-5
 SHEET NUMBER: 254
 ISSUE DATE: OCTOBER 1, 1988



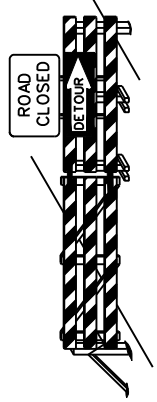
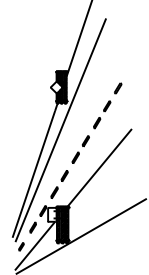
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
HIGHWAY SIGN AND BARRICADE DETAIL FOR CONSTRUCTS

WORKING NUMBER: SDTCP-10
SHEET NUMBER: 10
DATE: 04/16/2012
FILE NAME: SDTCP-10_06012012.DGN
PERSON: [blank]
CROSSLIN: [blank]



WING BARRICADES

- WING BARRICADES ARE TYPE BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A WARNING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
- WING BARRICADES SHOULD BE USED:
 - IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - IN ADVANCE OF ALL BRIDGE OR CULVERT MOVING OPERATIONS.

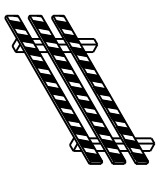


BARRICADE CLOSING A ROAD

BARRICADE CHARACTERISTICS

	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL *	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	35" MIN.	35" MIN.	60" MIN.
NUMBER OF REFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS
TYPE OF FRAME	LIGHT	LIGHT "X" FRAME	POST OR SHD

- * 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- ** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED FACINGS TRAFFIC.



TYPE III



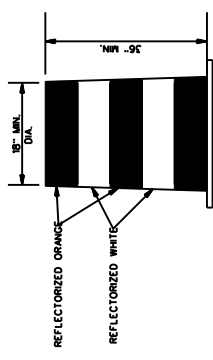
TYPE II



TYPE I

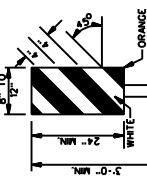
STANDARD BARRICADES

- A TYPE I BARRICADE CONSISTS OF ONE (1) HORIZONTAL RAIL SUPPORTED BY A DEMOUNTABLE FRAME. BARRICADES NORMALLY WOULD BE USED ON CONVENTIONAL ROADS OR URBAN STREETS AND ARTERIALS.
- A TYPE II BARRICADE CONSISTS OF TWO (2) HORIZONTAL RAILS ON A LIGHT "X" FRAME. TYPE II BARRICADES ARE INTENDED FOR USE WHERE THE HAZARD IS LESS SEVERE THAN TYPE I BARRICADES. TYPE II BARRICADES ARE USED FOR TEMPORARY DAYTIME USE, CONTINUOUS DELIMITING OF A RESTRICTED ROADWAY, OR FOR TEMPORARY DAYTIME USE.
- A TYPE III BARRICADE CONSISTS OF THREE (3) HORIZONTAL RAILS SUPPORTED BY FIXED POSTS, A ROAD SHD, A HEAVY DEMOUNTABLE FRAME OR A HEAVY WINGED "X" FRAME.
- TYPE I BARRICADES ARE INTENDED FOR USE ON CONSTRUCTION AND MAINTENANCE PROJECTS AS BARRICADES AND AT ROAD CLOSURES, WHERE THEY MUST REMAIN IN PLACE FOR EXTENDED PERIODS.
- THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).
- DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
- FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.



PLASTIC DRUM STRIPING DETAIL

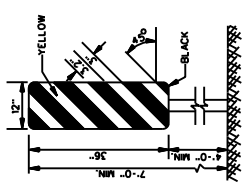
- PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH MUTCD STANDARDS FOR BARRICADES. THE PREDOMINANT COLOR ON DRUMS SHALL BE ORANGE. STRIPES SHALL BE GREEN, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
- DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
- WHERE PRACTICAL PLASTIC DRUMS SHALL BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.



VERTICAL PANEL

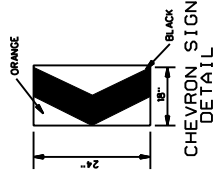
- VERTICAL PANELS CONSIST OF AT LEAST ONE PANEL 8" TO 12" IN WIDTH AND A MINIMUM OF 24" IN HEIGHT.
- THE DIAGONAL STRIPES SHALL SLOPE DOWNWARD IN THE DIRECTION THAT TRAFFIC IS TO PASS THE PANEL. THE MINIMUM OF 35" ABOVE THE ROADWAY ON A SINGLE LIGHTMASS POST.
- VERTICAL PANELS USED ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH-SPEED ROADWAYS SHALL HAVE A MINIMUM OF 70 IN OF RETROREFLECTIVE AREA FACING TRAFFIC.
- FOR TWO-WAY TRAFFIC OPERATIONS, BACK-TO-BACK PANELS SHALL BE USED.

GENERAL NOTES:
1. MARKINGS ON ALL DEVICES SHOWN ON THIS SHEET SHALL BE HIGH INTENSITY REFLECTIVE SHEETING.
2. THE TRAFFIC CONTROL PLAN WILL LIST THE VARIOUS TRAFFIC CONTROL DEVICES REQUIRED FOR EACH PROJECT.



TYPE 3 OBJECT MARKER (OM-3R)

- TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
- THE OM-3R IS SHOWN. THE OM-3R IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
- THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



CHEVRON SIGN DETAIL

- A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
- THE CHEVRON SIGN SHALL BE MOUNTED ON FIXED POST OR ROAD SHD.
- CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHALL BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.

STATE MISS.	PROJECT NO.	
----------------	-------------	--

MOBILE OPERATIONS ON MULTILANE ROAD

MOBILE OPERATIONS ON MULTILANE ROAD

NOTES:

- VEHICLES USED FOR THESE OPERATIONS SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLAOS, SIGNS, OR ARROW PANELS.
- PROTECTION VEHICLE #1 SHOULD BE EQUIPPED WITH AN ARROW PANEL. AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON PROTECTION VEHICLE #1 SO AS NOT TO OBSCURE THE ARROW PANEL.
- PROTECTION VEHICLE #2 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK-MOUNTED ATTENUATOR (TMA).
- PROTECTION VEHICLE #1 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
- WHEN ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, PROTECTION VEHICLE #1 SHOULD BE ELIMINATED.
- ON HIGH-SPEED ROADWAYS, A THIRD PROTECTION VEHICLE SHOULD BE USED (I.E., VEHICLE #1 ON THE SHOULDER (IF PRACTICAL), VEHICLE #2 IN THE CLOSED LANE, AND VEHICLE #3 IN THE CLOSED LANE).
- ARROW PANELS SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
- WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MOBILE OPERATIONS ON TWO-LANE ROAD

MOBILE OPERATIONS ON TWO-LANE ROAD

OPTIONAL SIGNS FOR SHORT DURATION OPERATION

NOTES:

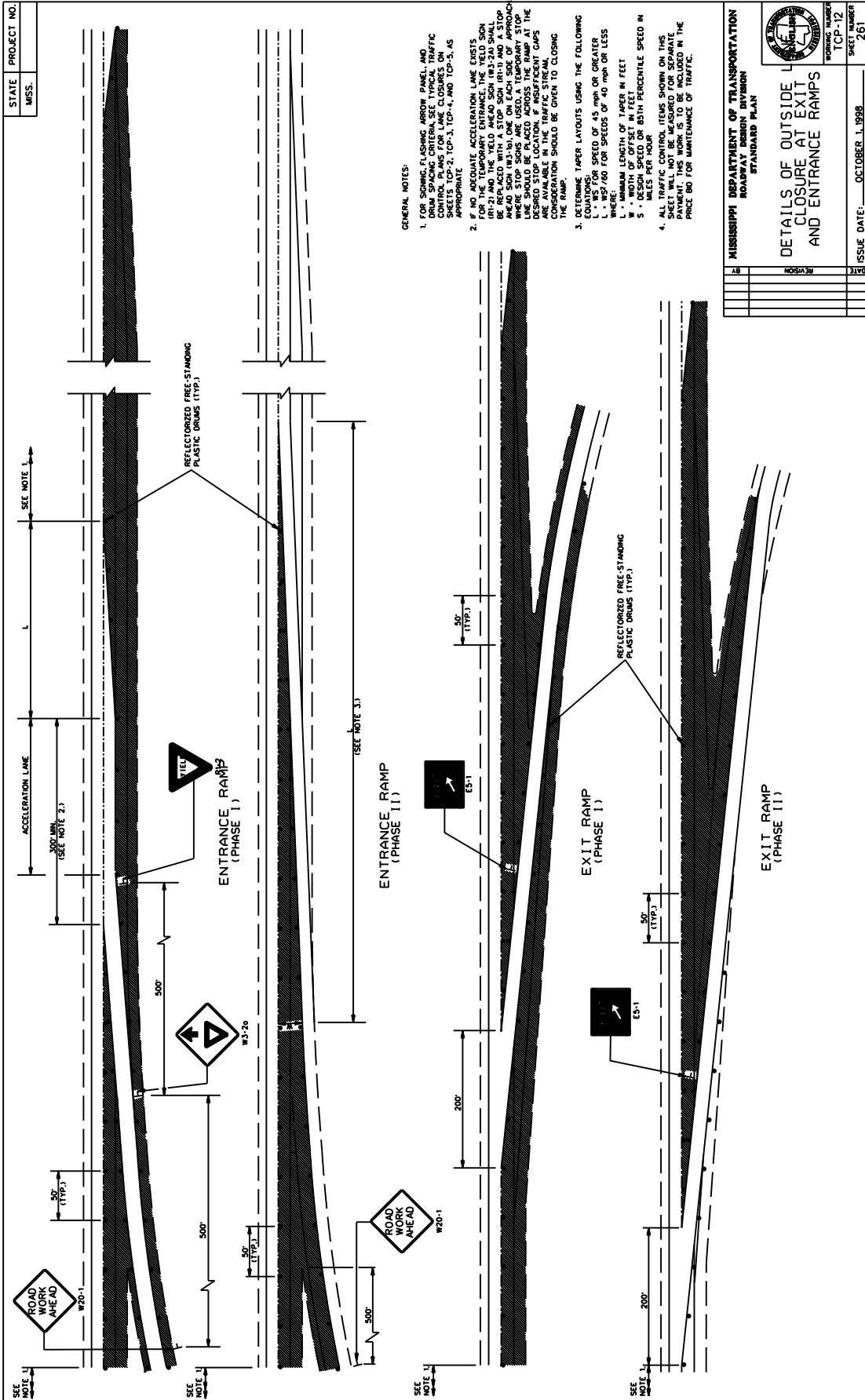
- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND PROTECTION VEHICLES SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLAOS, SIGNS, OR ARROW PANELS. SIGN #1 SHOULD NOT BE DONE FREQUENTLY, AS AN ALTERNATIVE A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND PROTECTION VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. PROTECTION VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE WORK, PROTECTION VEHICLES SHOULD BE ELIMINATED. PROTECTION VEHICLES SHOULD BE SLOWED AS THE WORK VEHICLE. THE PROTECTION VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL PROTECTION VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING WORK OPERATION MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE PROTECTION VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE PROTECTION VEHICLES SHOULD BE EQUIPPED WITH BEACONS. THE PROTECTION AND WORK VEHICLES SHOULD DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

**TRAFFIC CONTROL PLAN
MOBILE OPERATIONS
MULTILANE ROADS
AND
TWO-LANE ROADS**

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

DATE	BY	REVISION
10-1-99	REVISED	CARTON

ISSUE DATE: OCTOBER 1, 1998
SHEET NUMBER: 260
WORKING NUMBER: TCF-11

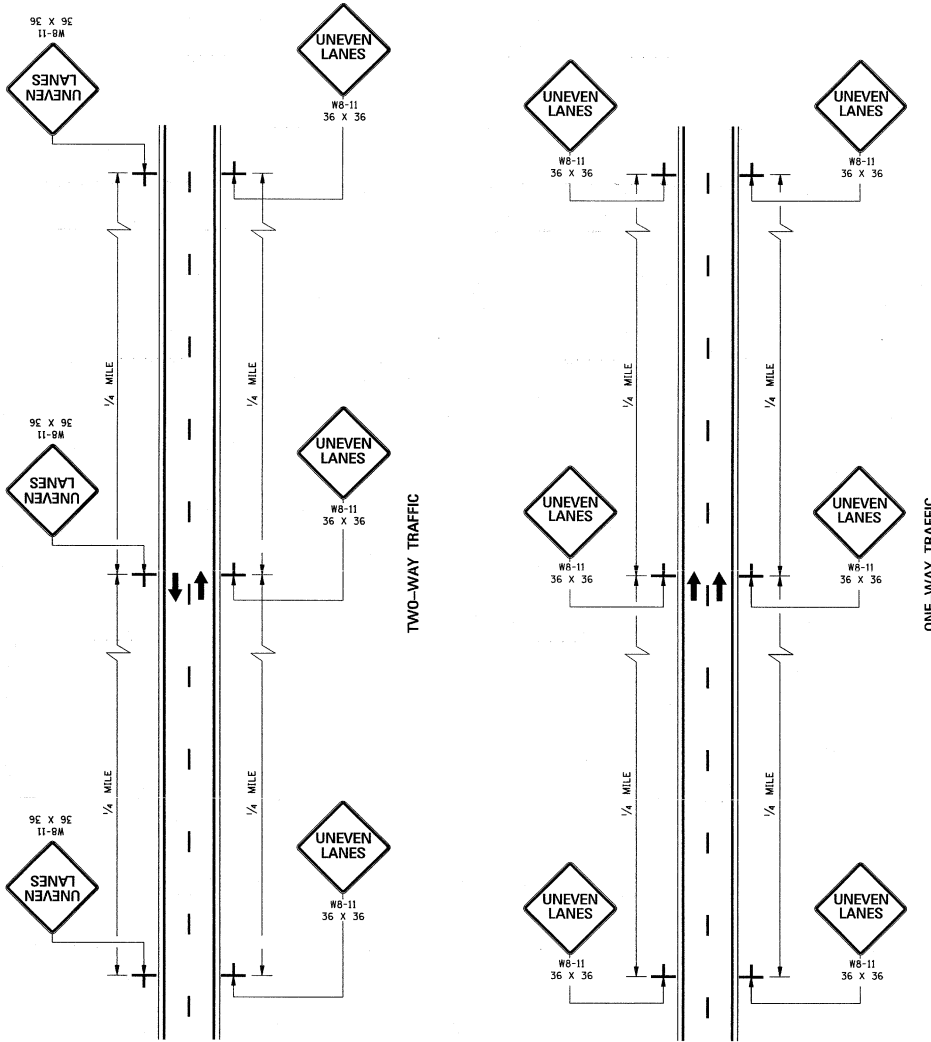


GENERAL NOTES:

- FOR SIGNING, FLASHING ARROW PANEL AND TRAFFIC CONTROL PLANS FOR LANE CLOSURES ON SHEETS TCP-2, TCP-3, TCP-4, AND TCP-5, AS APPROPRIATE.
- IF NO ADEQUATE ACCELERATION LANE EXISTS BEHIND THE RAMP, ONE OR MORE STOP SIGNS (R1-1) AND STOP SIGNS (W3-10), ONE ON EACH SIDE OF APPROACH, SHOULD BE PLACED AT THE END OF THE DESIRED STOP LOCATION. IF INSUFFICIENT GAPS EXIST, TRAFFIC SIGNALS SHOULD BE CONSIDERED. TRAFFIC SIGNAL CONSIDERATION SHOULD BE GIVEN TO CLOSING THE RAMP.
- DETERMINE TAPER LAYOUTS USING THE FOLLOWING EQUATIONS:
 - $L = 1.47 S^2 / a$ FOR SPEEDS OF 45 MPH OR GREATER
 - $L = .0007 S^3$ FOR SPEEDS OF 40 MPH OR LESS
 WHERE:
 - L = MINIMUM LENGTH OF TAPER IN FEET
 - S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS DRAWING ARE TO BE INSTALLED FOR USE IN THE PHASE I LANE CLOSURE. WORK IS TO BE DONE IN THE PHASE II LANE CLOSURE.

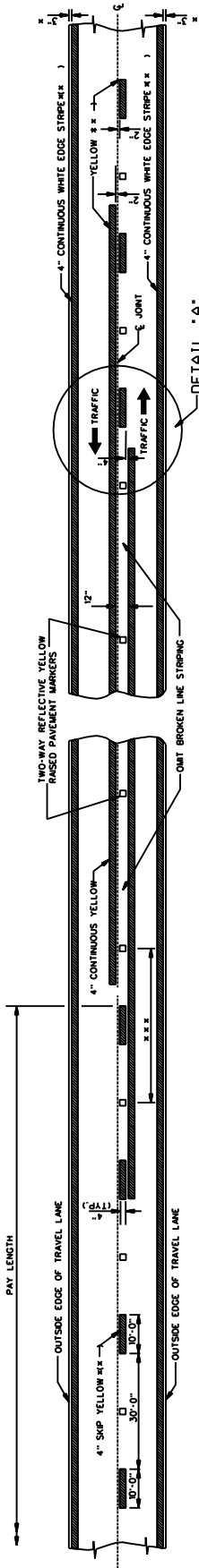
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY IMPROVEMENT DIVISION	
STANDARD PLAN	
DETAILS OF OUTSIDE LANE CLOSURE AT EXIT AND ENTRANCE RAMP	
WORKING NUMBER	TCP-12
SHEET NUMBER	261
ISSUE DATE:	OCTOBER 1, 1998

STATE	PROJECT NO.
MISS.	

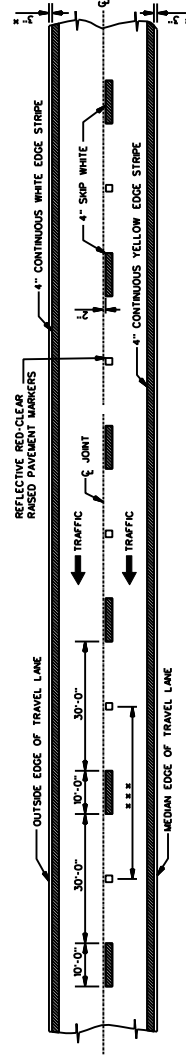
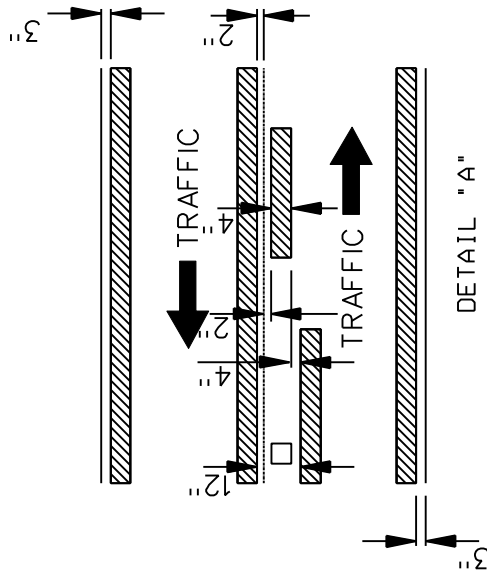


MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN			WORKING NUMBER TCP-14	SHEET NUMBER 263
TRAFFIC CONTROL PLANS UNEVEN PAVEMENT DETAILS			ISSUE DATE: OCTOBER 1, 1998	
DATE	BY	REVISION		

STATE	PROJECT NO.
MISS.	



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



4-LANE WITH ONE-WAY TRAFFIC

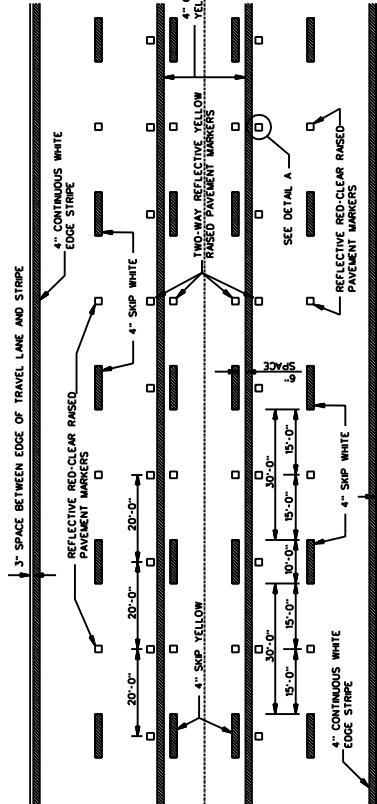


GENERAL NOTES:
 * 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
 ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
 *** 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

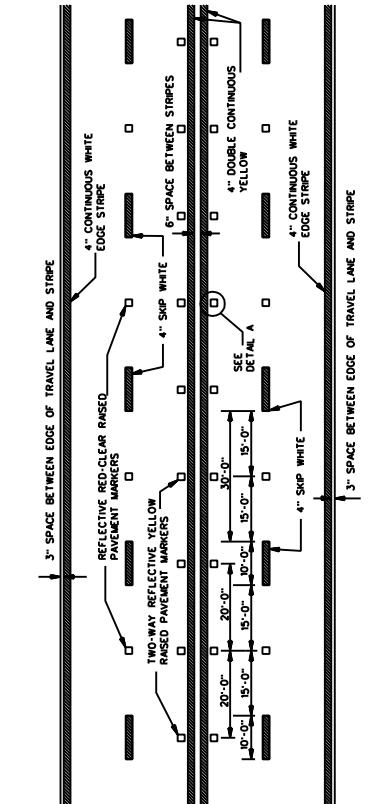
TANGENT SECTIONS	URBAN AREA (U-L-N)	RURAL AREA (U-L-N)
LANE LINES	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	140'-0"

1. NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS SHALL BE USED THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 2. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE MATERIALS AS SPECIFIED IN THE MOOT.
 3. APPROVED SOURCES OF MATERIALS.
 4. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKERS ARE TO REMAIN IN PLACE OVER 3 MONTHS.

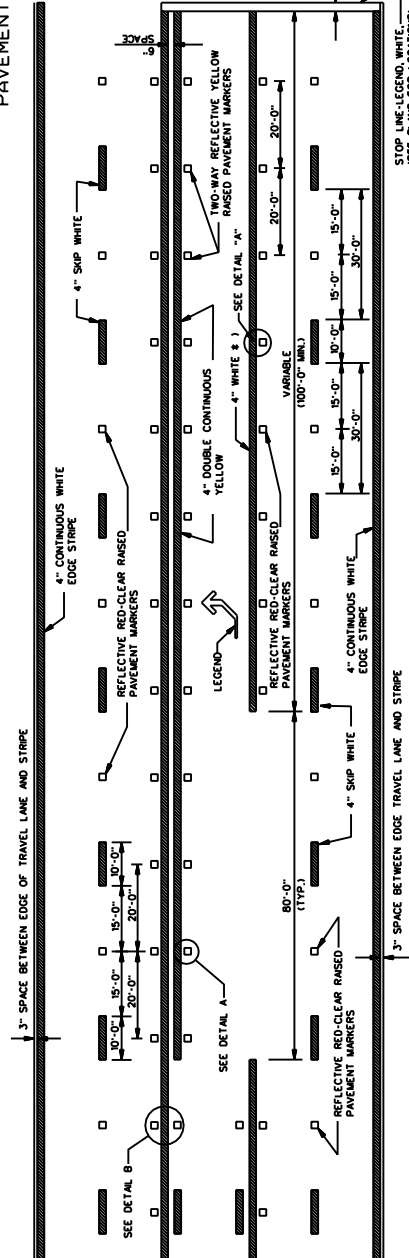
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS	
WORKING NUMBER	TCP-15
SHEET NUMBER	264
ISSUE DATE:	DECEMBER 1, 1999



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 5-LANE SECTION



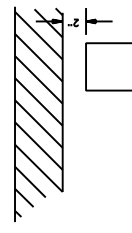
TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS AT LEFT TURN LANES

TYPICAL TWO-WAY ARROW INSTALLATION

- NOTES: 1. CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY.
- 2. IF SEGMENT IS LESS THAN 350', PLACE ONE SET OF ARROWS IN CENTER OF SEGMENT.
- 3. IF SEGMENT IS GREATER THAN 350', PLACE FIRST SET OF ARROWS 50 TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS (250 O.C.).



DETAIL A
LATERAL PLACEMENT OF PAVEMENT MARKERS

GENERAL NOTE:

- 1. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MOST APPROVED SOURCE OF MATERIALS.
- 2. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKERS ARE TO REMAIN IN PLACE OVER 3 MONTHS.
- 3. TEMPORARY TURN ARROW TO BE PAID FOR AS TEMPORARY TRAFFIC STRIPING LEGEND, ESTIMATED AT 10.9 SQ. FT. PER ARROW.

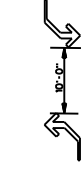


DETAIL B
LATERAL PLACEMENT OF PAVEMENT MARKERS



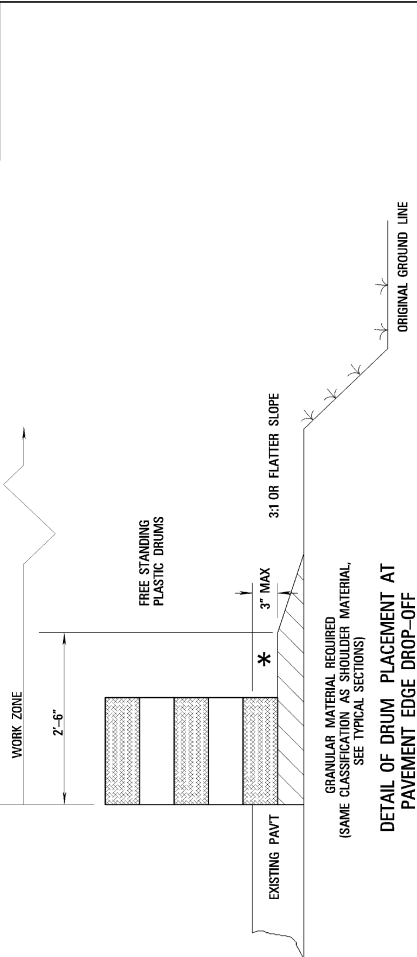
DETAIL OF TEMPORARY TURN ARROW

* NOTE: USE DETAIL STRIPING IF LENGTH < 150' AT THIS LOCATION, OTHERWISE USE CONTINUOUS STRIPING.



STOP LINE LEGEND, WHITE.
(SEE PLANS FOR LOCATIONS)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TEMPORARY STRIPING FOR TRAFFIC CONTROL 4-LANE AND 5-LANE UNDIVIDED ROADWAYS	
WORKING NUMBER TCP-16	SHEET NUMBER 265
ISSUE DATE: DECEMBER 1, 1999	



DETAIL OF DRUM PLACEMENT AT PAVEMENT EDGE DROP-OFF

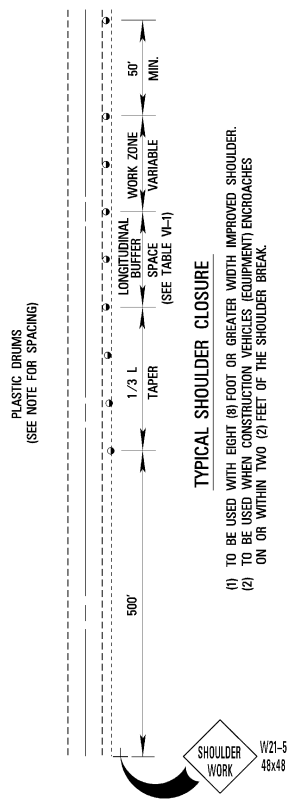
- NOTES**
- * A. PAVEMENT EDGE DROP-OFF
 1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-3) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750 ± 0.0).
 2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. COMES MAY BE USED IN PLACE OF DRUMS, PANELS AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE M.U.T.C.D. (1/3 L, WHERE L IS THE TAPER LENGTH IN FEET)
 3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 3:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS/PANELS OR BARRICADES MAY BE USED. IF CONCRETE BARRIERS ARE USED, SPECIAL REFLECTIVE DEVICES OR STEADY BURN LIGHTS SHOULD BE USED FOR OVERNIGHT INSTALLATIONS.
 4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
 5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

- B. DRUM SPACING**
1. TANGENTS = $2 \times S$
 2. $APPROX = \frac{L}{S} \times W$
WHERE L = TAPER LENGTH IN FEET
S = SPEED IN MPH (POSTED OR 85 PERCENTILE)
W = WIDTH OF OFFSET IN FEET
- C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.**

TABLE V-1 GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

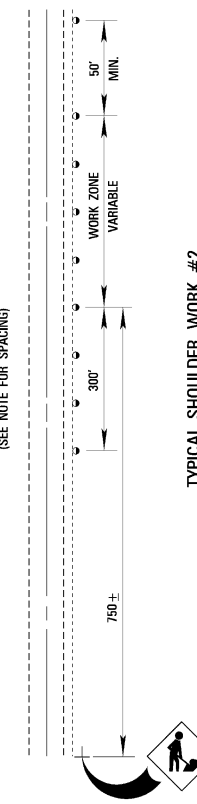
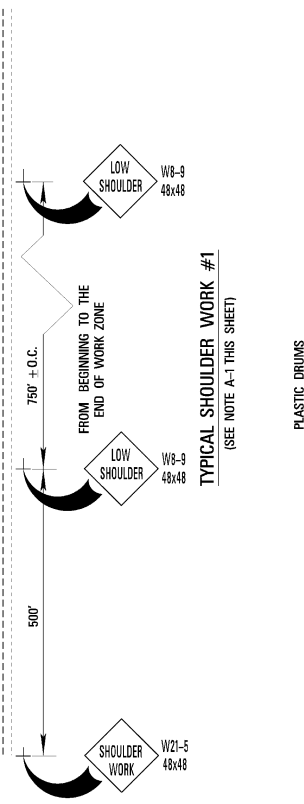
SPEED (MPH)	MINIMUM LENGTH (FEET)	MAXIMUM LENGTH (FEET)
20	25	35
25	35	45
30	45	55
35	55	65
40	65	75
45	75	85
50	85	95
55	95	105
60	105	115
65	115	125
70	125	135
75	135	145
80	145	155
85	155	165
90	165	175
95	175	185
100	185	195

* POSTED SPEED OFF PEAK 85 PERCENTILE SPEED PRIOR TO WORK STARTING OR THE ANTICIPATED GRINDING SPEED IN MPH



TYPICAL SHOULDER CLOSURE

- (1) TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
- (2) TO BE USED WHEN CONSTRUCTION VEHICLES (EQUIPMENT) ENCRUSHES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.



TYPICAL SHOULDER WORK #2

NOTE: WORK OUTSIDE THE (2) FOOT LIMIT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA. OR SEE NOTE A-3 THIS SHEET.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL DETAILS
DRUM PLACEMENT
AND
SHOULDER CLOSURE

PROJECT NO.: _____
COUNTY: _____
FILE NAME: _____
PERSON: _____
DATE: 12-28-93

WORKING NUMBER: _____
TCP: _____
SHEET NUMBER: _____

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 83

CODE: (SP)

DATE: April 6, 2017

SUBJECT: Contract Time

PROJECT: MP-3082-76(015) / 306136301- Washington

The completion of work to be performed by the Contractor for this project will not be a specified date but shall be when all allowable working days are assessed, or any extension thereto as provided in Subsection 108.06. It is anticipated that the Notice of Award will be issued no later than **June 13, 2017**.

The Contractor shall request a Notice to Proceed / Beginning of Contract Time date between the date of the **Execution of Contract and August 10, 2017**.

Should the Contractor not request a Notice to Proceed by **August 10, 2017**, the date for the Notice to Proceed and Beginning of Contract Time will be **August 10, 2017**.

54 Working Days have been allowed for the completion of work on this project.

The progress schedule for this project shows the Notice to Proceed and Beginning of Contract Time starting at the latest possible date. If the Contractor requests a Notice to Proceed earlier than this date, the Contractor shall submit a revised progress schedule showing the work beginning at the new Notice to Proceed and Beginning of Contract Time date.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 84

CODE: (SP)

DATE: 03/23/2017

SUBJECT: Scope of Work

PROJECT: MP-3082-76(015) / 306136301- Washington County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings." All other references to plans in the contract documents and Standard Specification for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following:

US 82
OVERLAY FROM WOODLAWN ROAD (LOG MILE 12.941) TO
THE 4-LANE SECTION AT LELAND (LOG MILE 18.283)

1. The Contractor shall erect and maintain construction signing, provide all signs, set up night time lane closures (if needed), and traffic handling devices in accordance with the Traffic Control Plan. The costs for this work are to be included in the price bid for pay item number 618-A, Maintenance of Traffic. All traffic control devices on this project should comply with the latest version of the MUTCD. Fluorescent orange sheeting shall be used on all construction and traffic control signs except for those designated in the plans to be black legend and border on white background. The Contractor will be required to use 42 inch channelizing cones with 6 inch wide reflective tape and 16 pound vertical panel bases for each cone.
2. Remove any failed areas on the main facility as directed by the Project Engineer using the following construction sequence.
 - a) Saw cut full depth through the asphalt. The saw cut will be absorbed into 202-B - Removal of Pavement, All Types and Depths
 - b) Remove the failed asphalt
 - c) Remove any unsuitable material in the subgrade as directed by the Engineer.
 - d) Backfill and stabilize failed area with crushed stone in lifts to an elevation six inches (6") below the original finished pavement elevation. No lift of crushed stone shall be greater than six inches (6") in thickness.
 - e) Backfill with two lifts of 25-mm, ST, asphalt, 3-inch lifts, for a total of six inches (6"). The final grade of asphalt shall match the existing grade of the highway. All repairs must be complete by the end of the work day and the lane closures must be removed from the roadway so that all lanes of travel are open thereafter.

- f) All dig out areas will require a leveling lift, whose limits will be determined by the Engineer
- 3. It is the Contractor's responsibility to insure the drainage of surface water from milled areas. Temporary wedges of full lane width asphalt shall be placed by the Contractor immediately after the cold milling process to allow the safe transition of traffic. These wedges shall be maintained in a satisfactory condition by the Contractor until the permanent asphalt is placed. All costs for placing and maintaining these wedges shall be absorbed in other pay items.
- 4. Full width milling will be done at several locations on the project. Unless otherwise stated, or directed by the Project Engineer, the depth of milling will be 1½ inches on the appropriate cross slope.
 - a) Mill at the BOP and EOP of the project a distance of 150 feet. Start with a depth of 1½ inches and taper up to 0 inches at 150 feet. Mill bridge ends a minimum of 150 feet. Care shall be taken to insure proper minimum guard rail heights
 - b) Mill Landfill Road 1½ inches with no taper. A full 1½ inches will be milled out to the MDOT Right of Way.
 - c) All other County roads will be milled from 1½ inches at the MDOT Right of Way, tapered up to 0 inches at 20 feet
- 5. Overlay US 82 with 1½ inches and variable 9.5-mm asphalt. This overlay will consist of overlaying the mainline highway, local roads, and asphalt pads. Publicly maintained roads or streets shall be paved to the existing right of way. Privately owned entrances shall be paved a distance of 10 feet and variable from the edge of pavement. Any site grading at local roads, crossovers or drives will not be measured for separate payment but will be considered an absorbed item. Cross slopes shall be increased where practical within contract quantities in an effort to achieve a uniform cross slope of 2%. The existing superelevation rate in horizontal curves is to be maintained as a minimum. Any work to control the laydown equipment for proper placement of the asphalt in the superelevated curves shall be absorbed by the Contractor at no additional cost to the state.
 - a) The mainline will be overlaid using the 9.5-mm, HT, polymer modified asphalt, with the exception of the center turn lane which will be paved with the 9.5-mm, HT, non-polymer modified asphalt.
 - b) The center turn lane will be paved first. Subsequent paving will be done in the travel lanes, downslope towards the outside pavement edge.
 - c) Local Roads and private pads will be paved 1½ inches in depth with the 9.5-mm, HT, non-polymer modified asphalt.

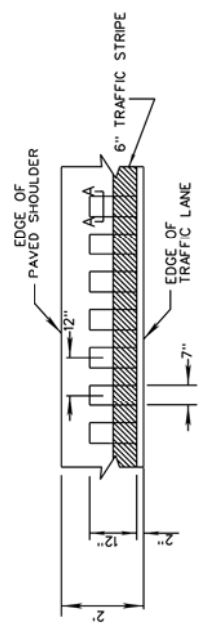
Note: The Contractor will be responsible for traffic control while MDOT personnel conduct density testing on the Asphalt. The cost is to be included in the price bid for pay item No. 618-A, Maintenance of Traffic.

- 6. Temporary striping shall conform to finished stripe specifications for alignment, reflectivity, straightness, and neatness. Temporary stripe shall be placed as needed for safe movement of

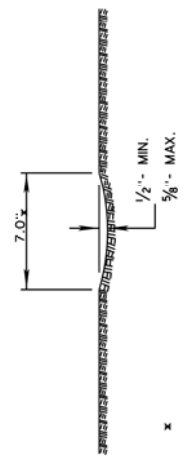
traffic. All permanent pavement markings are to be hot thermoplastic. Edge lines will be placed so as to maintain a 12-foot lane width. Thermoplastic edge lines must be applied by using an atomization method, and centerline stripe and detail stripe must be placed using an extrusion head. The Contractor will mill a 12-inch rumble strip along the edge of pavement and apply a 6-inch thermoplastic stripe on the inside six inches (6") of the rumble strip using an atomization method to create a "Rumble Stripe." (See Rumble Stripe Detail)

7. Raise the existing shoulders to match the new pavement elevation by grading existing material and/or placing any needed granular material, all to be bladed and dressed to a finished slope of 4%. Any existing low shoulders or at any time there is a differential in excess of two inches (2"), the Contractor shall raise the shoulder grade up to the current asphalt grade. The Contractor may pull up existing shoulder material if possible or place new granular material. Incidental work such as removing vegetation, shaping and compacting shoulders (including the base for paved aprons), and other incidental work that is necessary to complete the work will not be measured for separate payment and the cost will be included in the items bid.
8. Raised pavement markers will be placed at 80-foot intervals in tangents and 40-foot intervals in curves and in urban limits along the centerline of roadway. Removal of existing raised pavement markers or rumble bars shall be done before the overlay and shall be considered an absorbed item of work.
9. Replace Guardrail and Guardrail End Sections as per standard drawings and specifications.

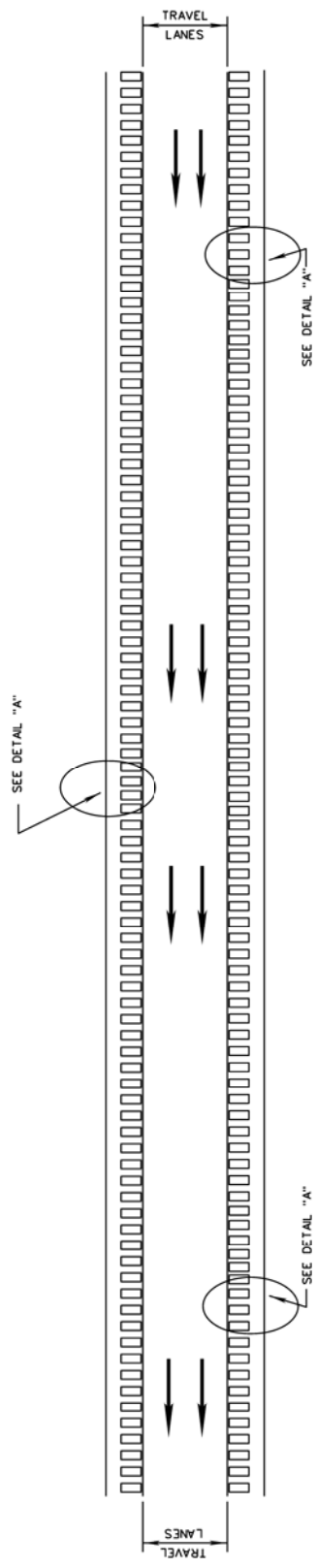
- GENERAL NOTES**
1. GROUND-IN RUMBLE STRIPS SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 2. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED TO MAINLINE ONLY.



DETAIL "A"



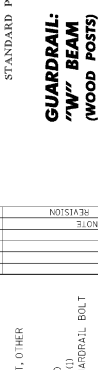
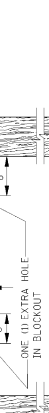
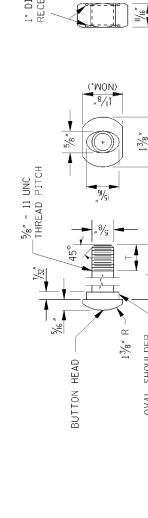
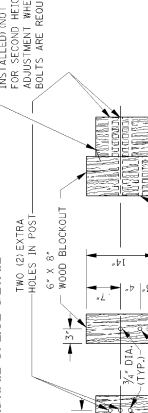
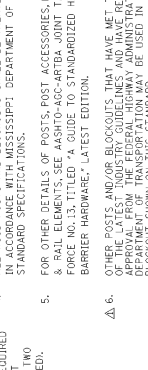
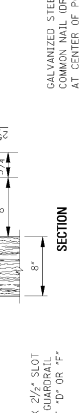
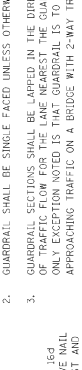
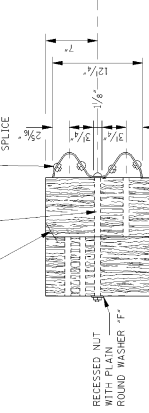
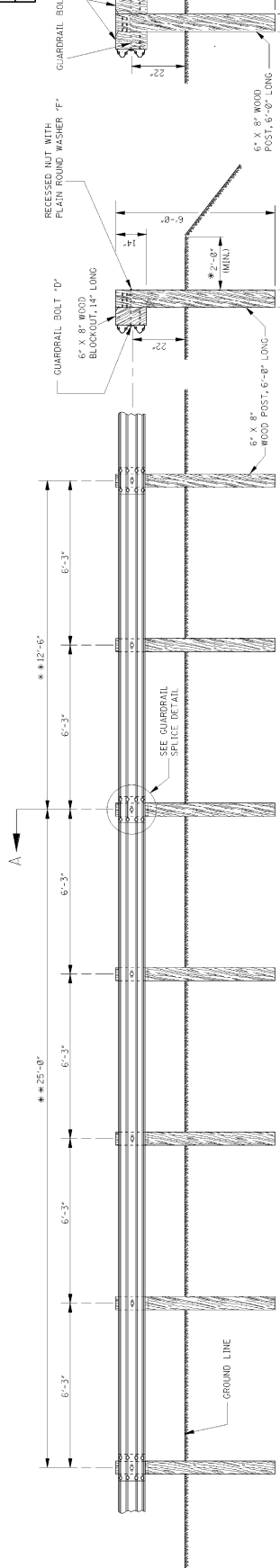
SECTION "A-A"

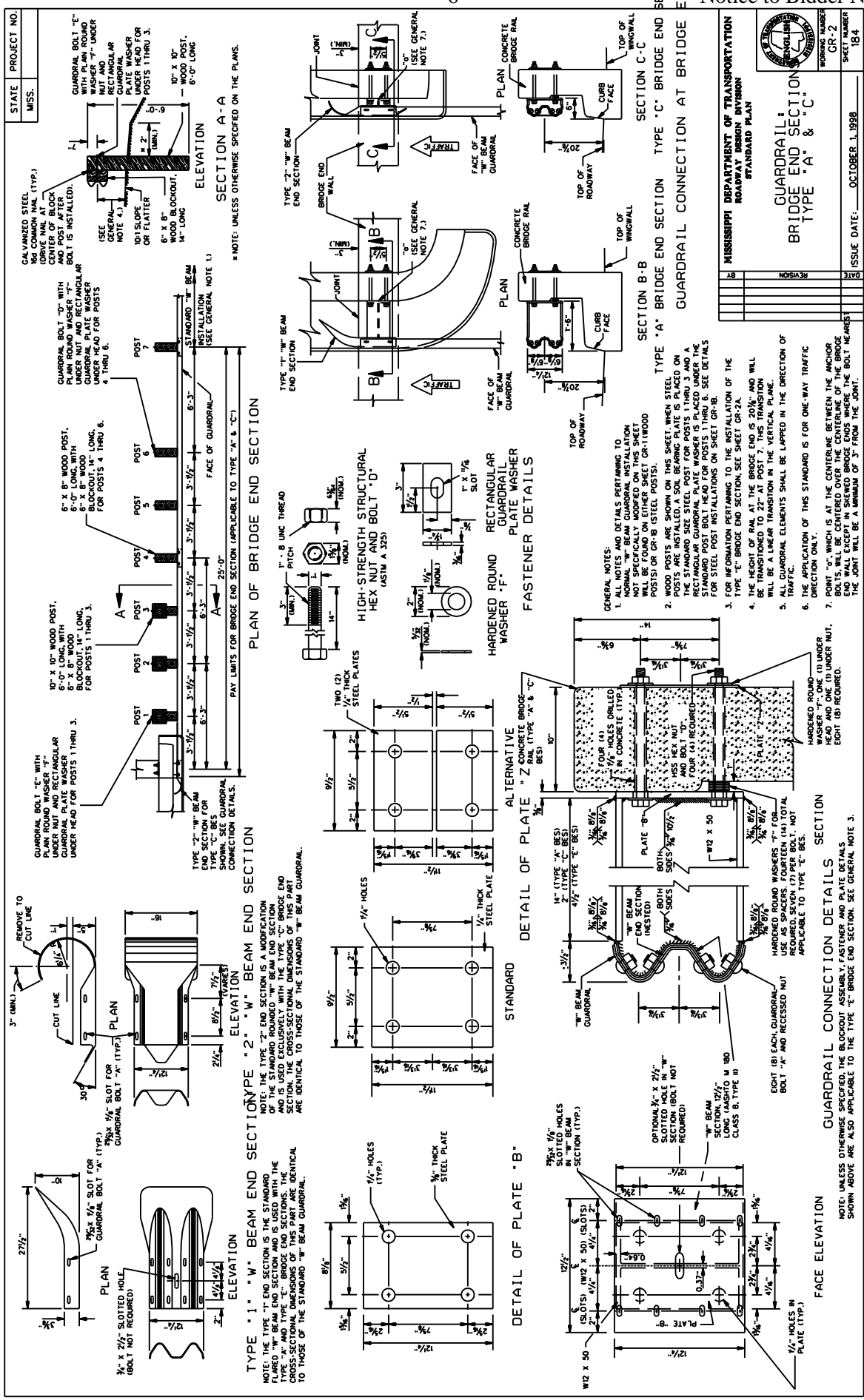


PLAN
NOT TO SCALE
DETAIL OF
RUMBLE STRIPS
(GROUND-IN)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTION	
RUMBLE STRIPS (GROUND-IN)	
BY	REVISION
DATE	FILE NAME:
	DESIGN TEAM: OK0000
	DATE:
	WORKING NUMBER: 88
	SHEET NUMBER:

STATE	PROJECT NO.
MISS.	

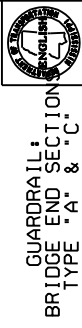




STATE	PROJECT NO.
MISS.	

DATE	ISSUE DATE:
	OCTOBER 1, 1998

REVISION	NO.	DATE

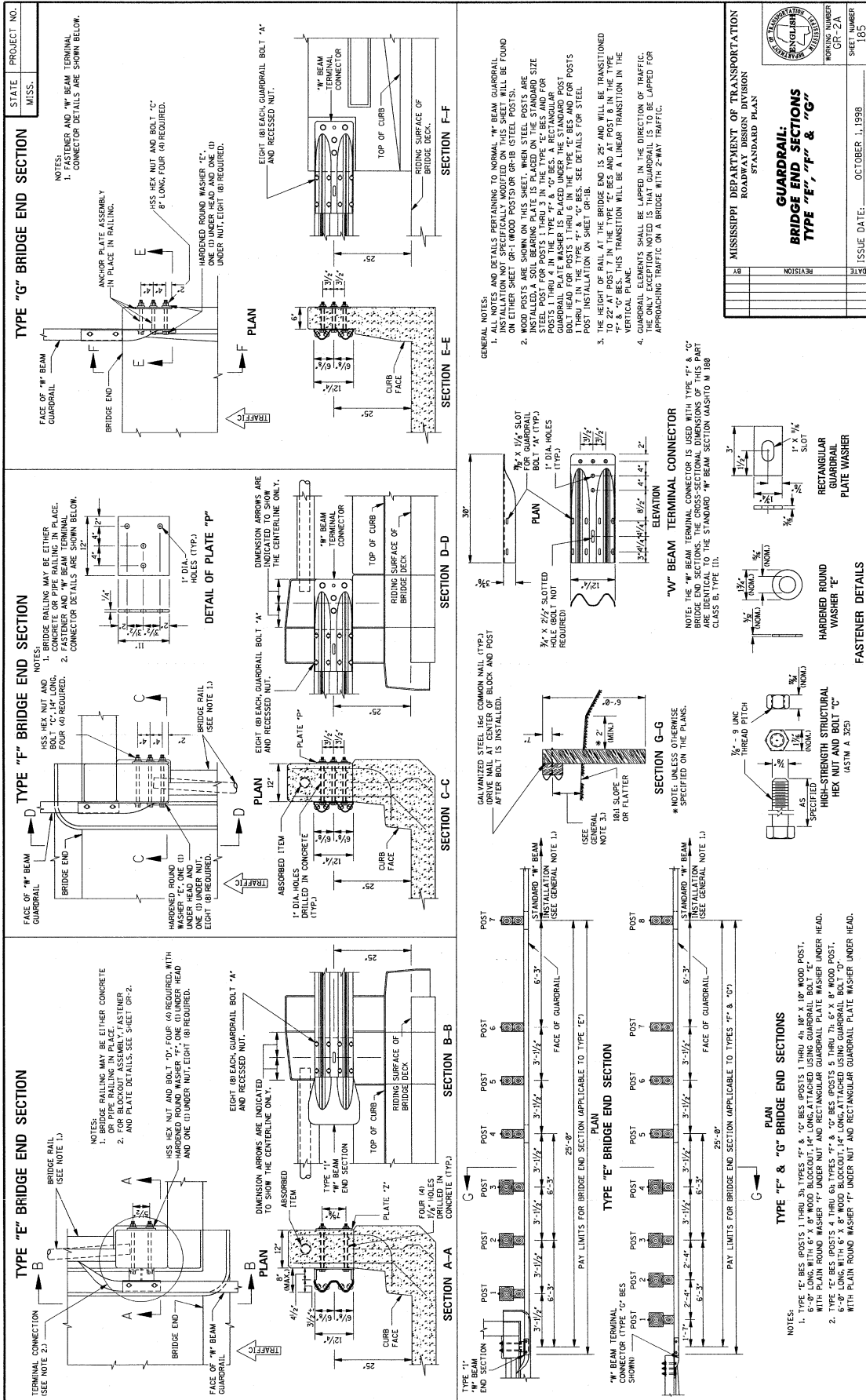


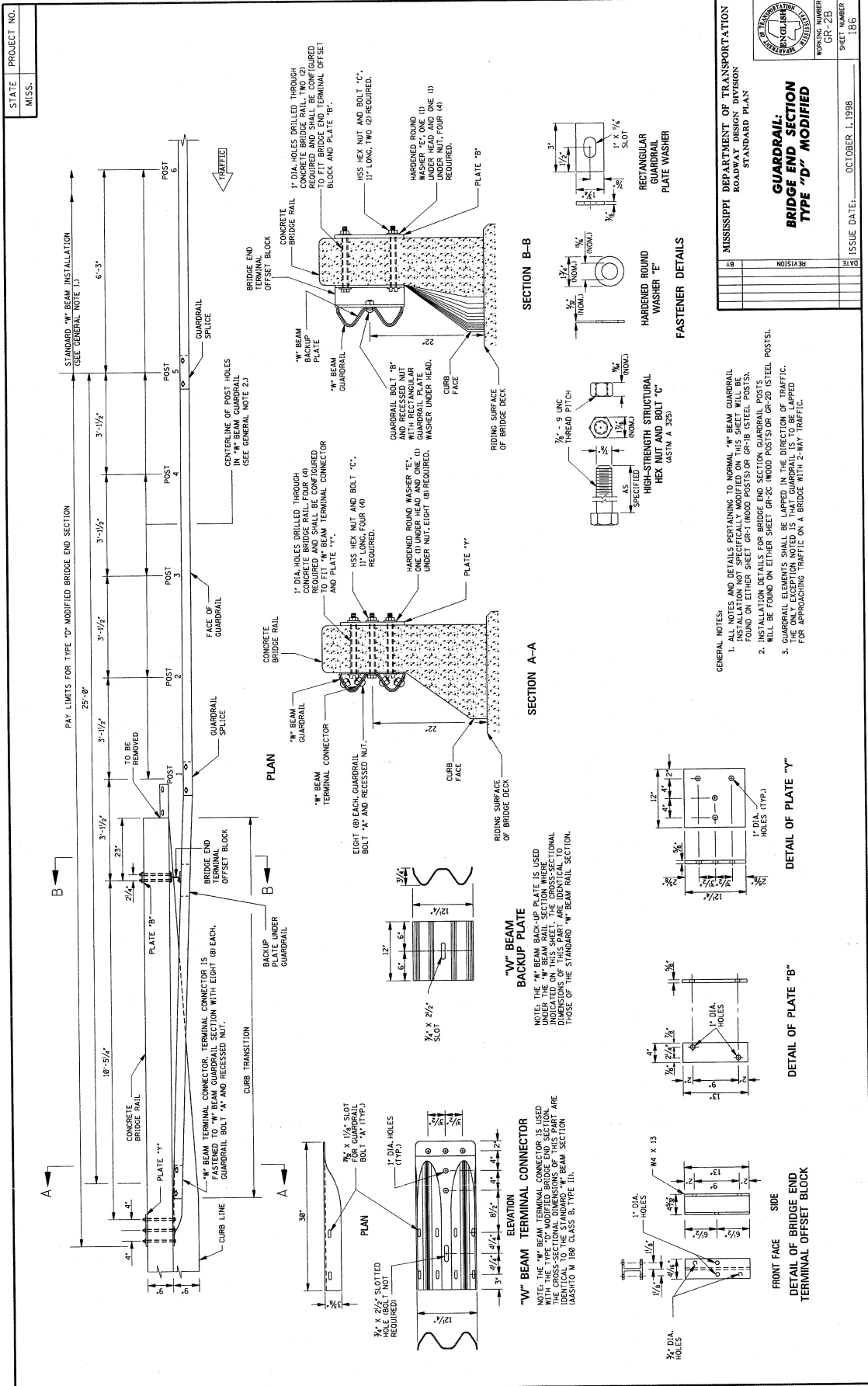
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION

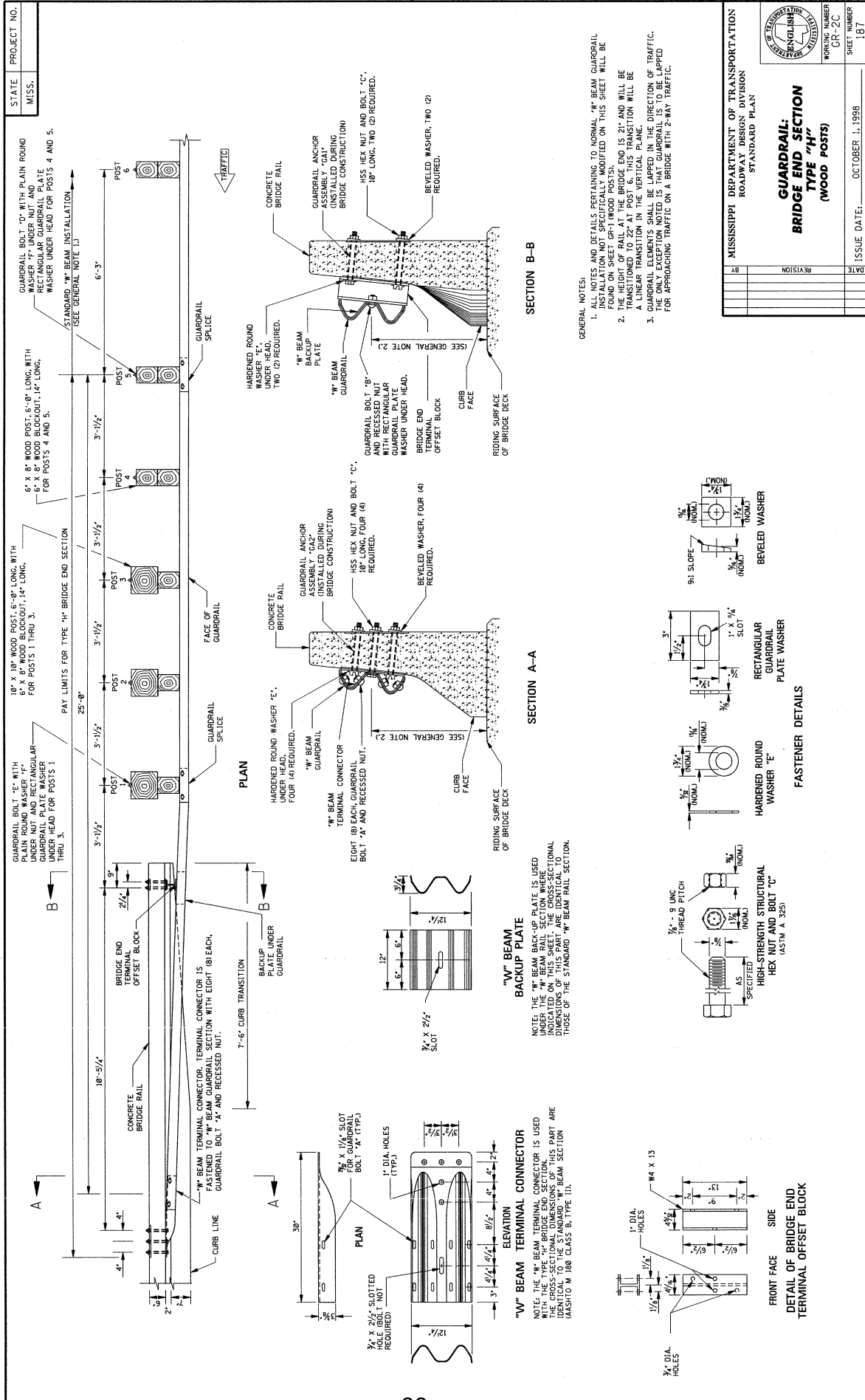
GUARDRAIL
BRIDGE END SECTION
TYPE "A" & "C"

WORKING NUMBER
GR-2

SHEET NUMBER
184





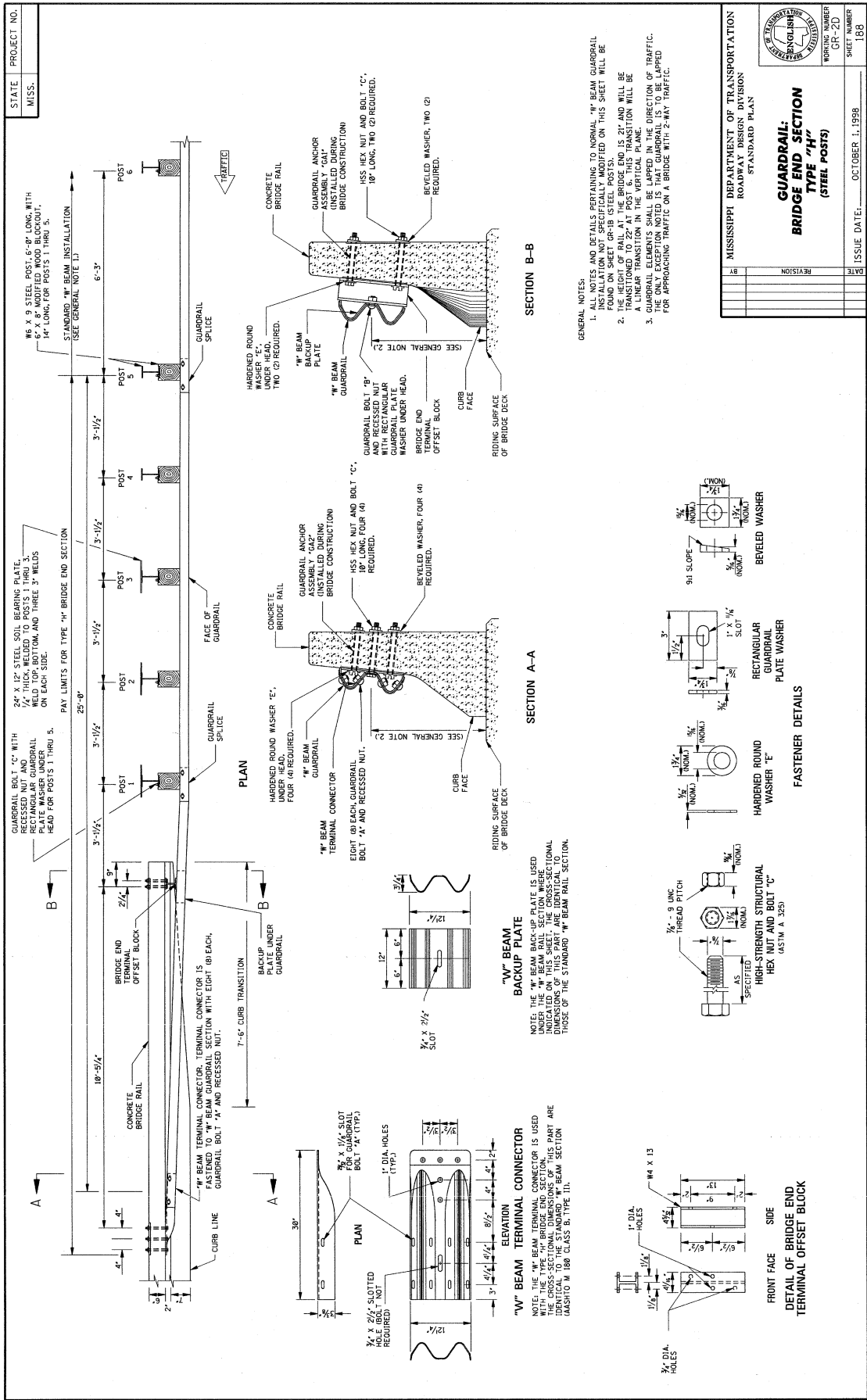


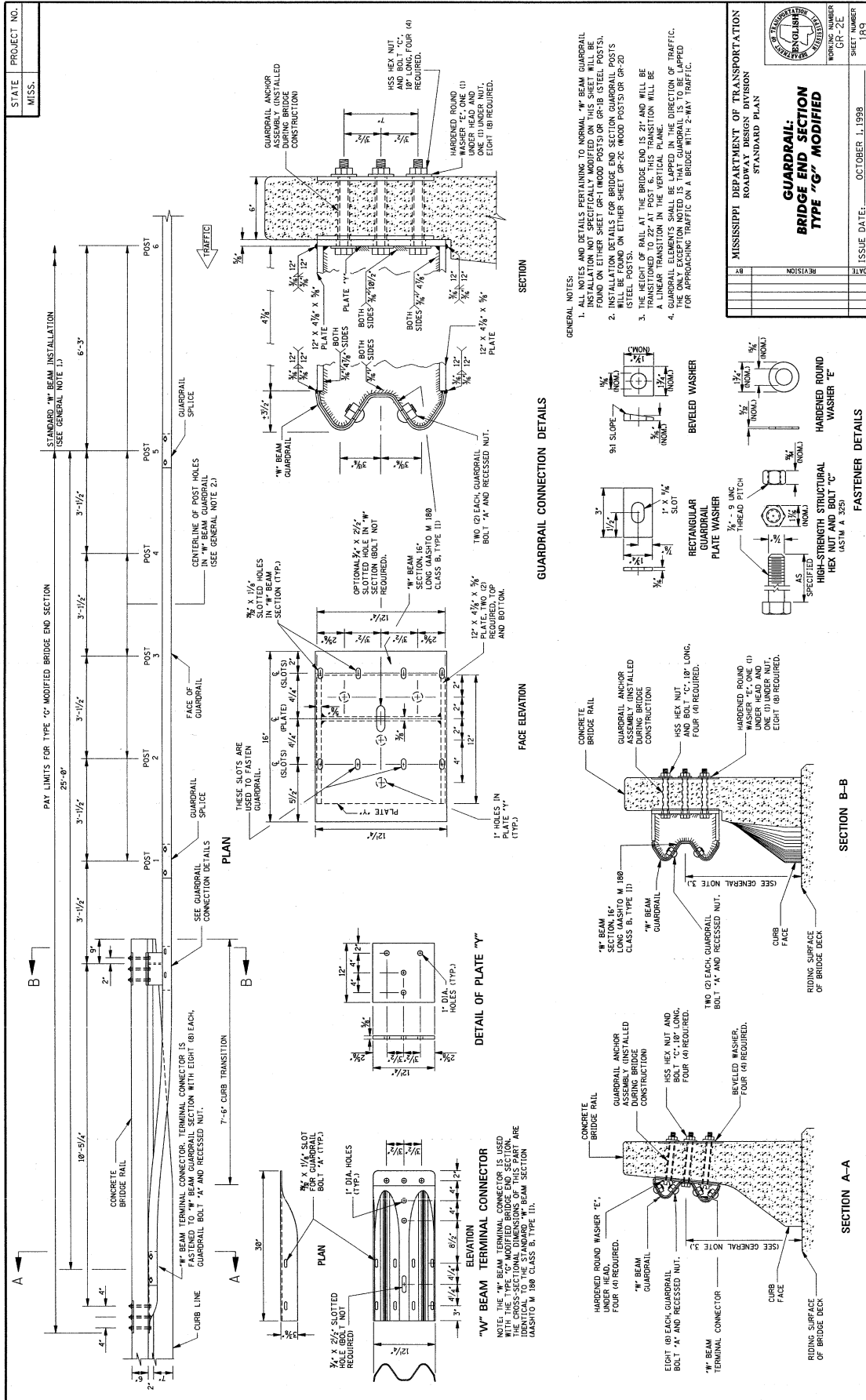
GENERAL NOTES:

1. ALL NOTES AND DETAILS PERTAINING TO NORMAL "W" BEAM GUARDRAIL ARE IDENTICAL TO THOSE INDICATED ON SHEET CR-1 (WOOD POSTS).
2. THE HEIGHT OF RAIL AT THE BRIDGE END IS 21" AND WILL BE TRANSITIONED TO 22" AT POST 6. THIS TRANSITION WILL BE A LINEAR TRANSITION IN THE VERTICAL PLANE.
3. GUARDRAILS SHALL BE INSTALLED IN THE DIRECTION OF TRAFFIC. THE ONLY EXCEPTION NOTED IS THAT GUARDRAILS TO BE APPLIED FOR APPROACHING TRAFFIC ON A BRIDGE WITH 2-WAY TRAFFIC.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
PROJECT NO.	CR-2C
STATE	MISS.
WORKSHEET NUMBER	187
SHEET NUMBER	187
ISSUE DATE:	OCTOBER 1, 1998
DATE	
REVISION	

GUARDRAIL: BRIDGE END SECTION TYPE "H" (WOOD POSTS)



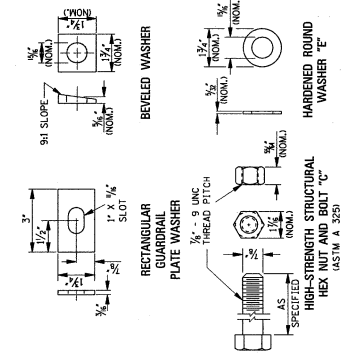


STATE PROJECT NO.	
MISS.	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
GUARDRAIL: BRIDGE END SECTION TYPE "G" MODIFIED	
WORKING NUMBER GR-2E	SHEET NUMBER 189
DATE	ISSUE DATE: OCTOBER 1, 1998
REVISION	

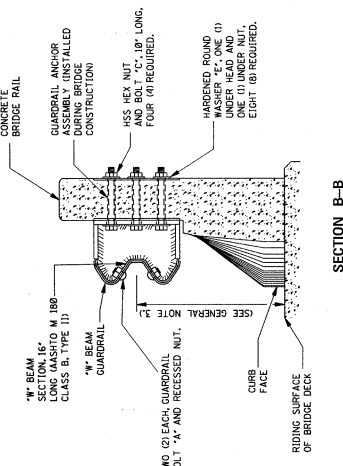
- GENERAL NOTES:
1. INSTALLATION AND DETAILS PERTAINING TO NORMAL "W" BEAM GUARDRAIL FOUND ON EITHER SHEET GR-1 WOOD POSTS OR GR-1B STEEL POSTS.
 2. INSTALLATION DETAILS FOR BRIDGE END SECTION GUARDRAIL POSTS ARE IDENTICAL TO EITHER SHEET GR-2C WOOD POSTS OR GR-2D STEEL POSTS.
 3. THE HEIGHT OF RAIL AT THE BRIDGE END IS 21" AND WILL BE A LINEAR TRANSITION IN THE VERTICAL PLANE.
 4. THE ONLY EXCEPTION NOTED IS THAT GUARDRAIL TO BE LAPPED FOR APPROACHING TRAFFIC ON A BRIDGE WITH 2-WAY TRAFFIC.

GUARDRAIL CONNECTION DETAILS



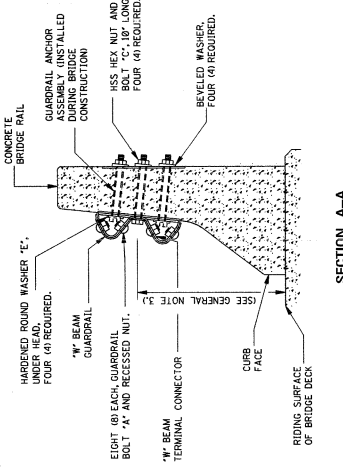
FASTENER DETAILS

FACE ELEVATION



SECTION B-B

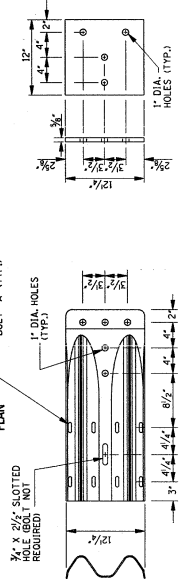
SECTION



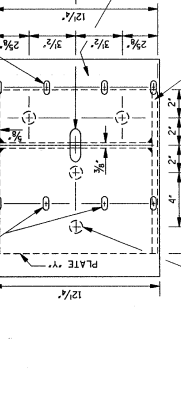
SECTION A-A

"W" BEAM TERMINAL CONNECTOR

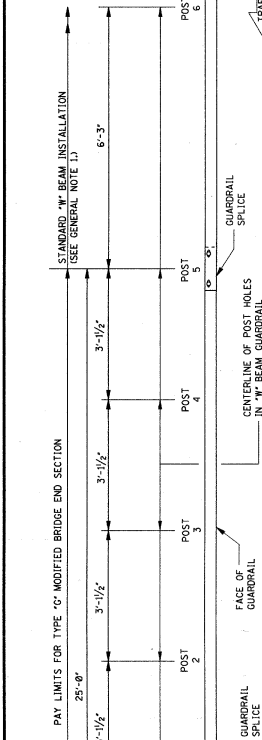
NOTE: THE "W" BEAM TERMINAL CONNECTOR IS USED FOR BRIDGE END SECTION GUARDRAIL. THE CROSS-SECTIONAL DIMENSIONS OF THIS PART ARE IDENTICAL TO THE STANDARD "W" BEAM SECTION (ASHFOT M 180 CLASS B, TYPE II).



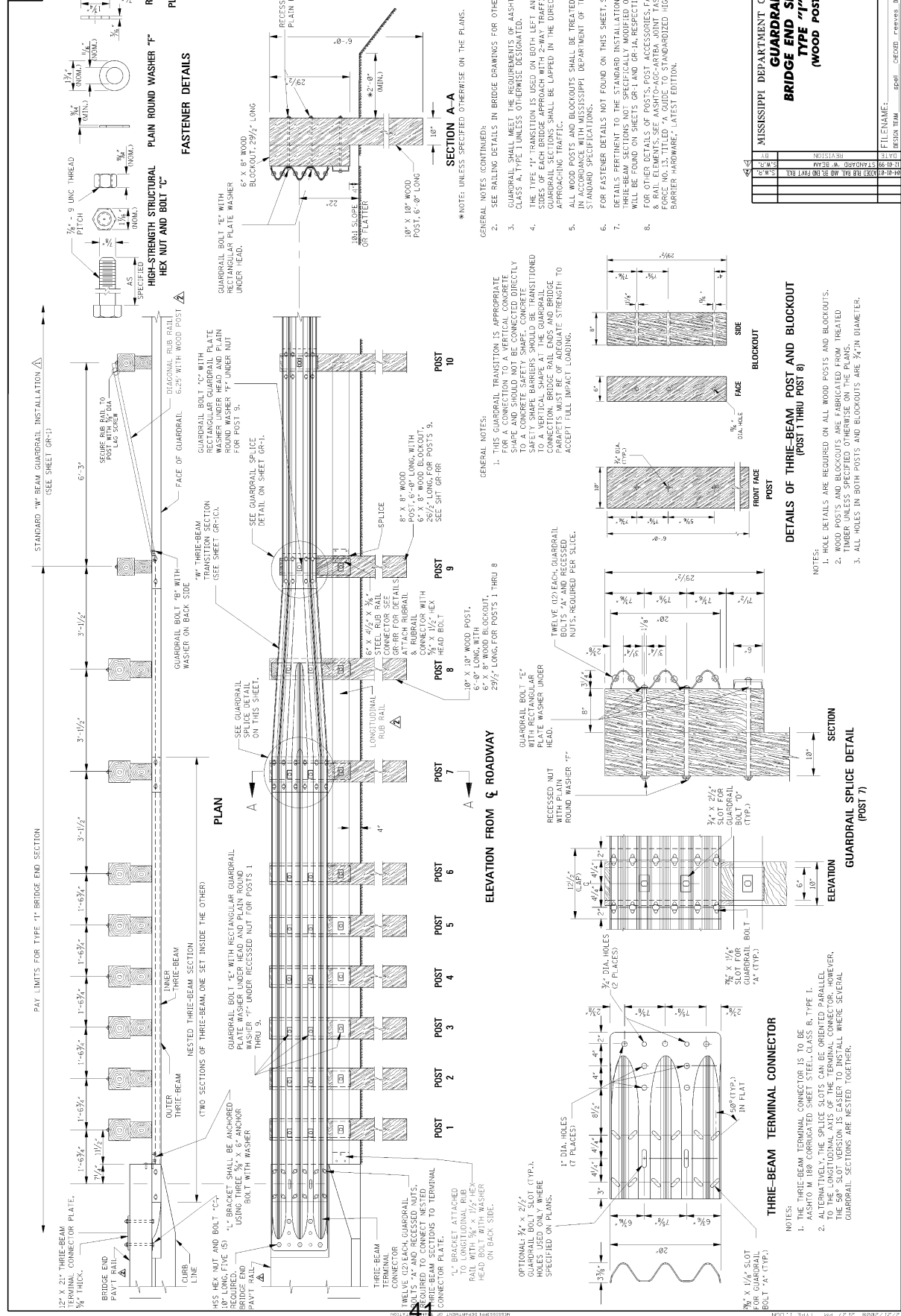
DETAIL OF PLATE "W"



PLAN



STATE	PROJECT NO.
MISS.	



MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
GUARDRAIL: BRIDGE END SECTION	
TYPE "1" (WOOD POSTS)	
WORKING NUMBER	GR-21
SHEET NUMBER	GR-21
FILE NAME	MISSISSIPPI DEPARTMENT OF TRANSPORTATION
DESIGNER	MISSISSIPPI DEPARTMENT OF TRANSPORTATION
CHECKED	MISSISSIPPI DEPARTMENT OF TRANSPORTATION
DATE	MISSISSIPPI DEPARTMENT OF TRANSPORTATION

- GENERAL NOTES (CONTINUED):**
- SEE RAILING DETAILS IN BRIDGE DRAWINGS FOR OTHER DETAILS.
 - GUARDRAIL SHALL MEET THE REQUIREMENTS OF ASHTO M 180, CLASS A, TYPE 1 UNLESS OTHERWISE DESIGNATED.
 - THE TYPE "1" TRANSITION IS USED ON BOTH LEFT AND RIGHT HAND SIDES OF THE BRIDGE. APPROACH WITH 2-WAY TRAFFIC AND THE GUARDRAIL SECTION SHALL BE LAPPED IN THE DIRECTION OF THE APPROACHING TRAFFIC.
 - ALL WOOD POSTS AND BLOCKOUTS SHALL BE TREATED TIMBER IN ACCORDANCE WITH MISSISSIPPI DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS.
 - FOR FASTENER DETAILS NOT FOUND ON THIS SHEET SEE SHEET GR-1.
 - DETAILS PERTINENT TO THE STANDARD INSTALLATION OF "W" AND "THREE-BEAM" SECTIONS NOT SPECIFICALLY MODIFIED ON THIS SHEET SHALL BE USED FOR ALL OTHER SECTIONS OF POSTS, FASTENERS, FASTENINGS & RAIL ELEMENTS; SEE ASHTO-ACC-ARBA JOINT TASK FORCE NO. 13, TITLED "A GUIDE TO STANDARDIZED HIGHWAY BARRIER HARDWARE", LATEST EDITION.

- GENERAL NOTES:**
- THIS GUARDRAIL TRANSITION IS APPROPRIATE TO A CONCRETE SAFETY SHAPE, CONCRETE SAFETY SHAPE BARRIERS SHOULD BE TRANSITIONED TO THIS GUARDRAIL TYPE AT THE BRIDGE CONNECTION. BRIDGE RAIL ENDS AND BRIDGE PARAPETS MUST BE OF ADEQUATE STRENGTH TO ACCEPT FULL IMPACT LOADING.
 - THIS GUARDRAIL TRANSITION IS APPROPRIATE TO A CONCRETE SAFETY SHAPE, CONCRETE SAFETY SHAPE BARRIERS SHOULD BE TRANSITIONED TO THIS GUARDRAIL TYPE AT THE BRIDGE CONNECTION. BRIDGE RAIL ENDS AND BRIDGE PARAPETS MUST BE OF ADEQUATE STRENGTH TO ACCEPT FULL IMPACT LOADING.

- DETAILS OF THREE-BEAM POST AND BLOCKOUT (POST 1 THRU POST 8)**
- NOTES:**
- WALL DETAILS ARE REQUIRED ON ALL WOOD POSTS AND BLOCKOUTS.
 - WOOD POSTS AND BLOCKOUTS ARE FABRICATED FROM TREATED TIMBER UNLESS SPECIFIED OTHERWISE ON THE PLANS.
 - ALL HOLES IN BOTH POSTS AND BLOCKOUTS ARE 3/4" IN DIAMETER.

THREE-BEAM TERMINAL CONNECTOR

NOTES:

- THE THREE-BEAM TERMINAL CONNECTOR IS TO BE ASHTO M 180 CORRUGATED SHEET STEEL, CLASS B, TYPE 1.
- ALTERNATIVELY, THE SPALICE SLOTS CAN BE ORIENTED PARALLEL TO THE LONGITUDINAL AXIS OF THE TERMINAL CONNECTOR. HOWEVER, THIS CONNECTION IS NOT RECOMMENDED FOR THE FULL WHERE SEVERAL GUARDRAIL SECTIONS ARE NESTED TOGETHER.

PLAN LIMITS FOR TYPE "1" BRIDGE END SECTION

PLAN

GUARDRAIL BOLT "C" WITH RECTANGULAR GUARDRAIL PLATE UNDER NUT FOR POSTS 1 THRU 9.

GUARDRAIL BOLT "C" WITH RECTANGULAR GUARDRAIL PLATE UNDER NUT FOR POST 10.

GUARDRAIL BOLT "C" WITH RECTANGULAR GUARDRAIL PLATE UNDER NUT FOR POST 9.

GUARDRAIL BOLT "C" WITH RECTANGULAR GUARDRAIL PLATE UNDER NUT FOR POST 8.

GUARDRAIL BOLT "C" WITH RECTANGULAR GUARDRAIL PLATE UNDER NUT FOR POST 7.

GUARDRAIL BOLT "C" WITH RECTANGULAR GUARDRAIL PLATE UNDER NUT FOR POST 6.

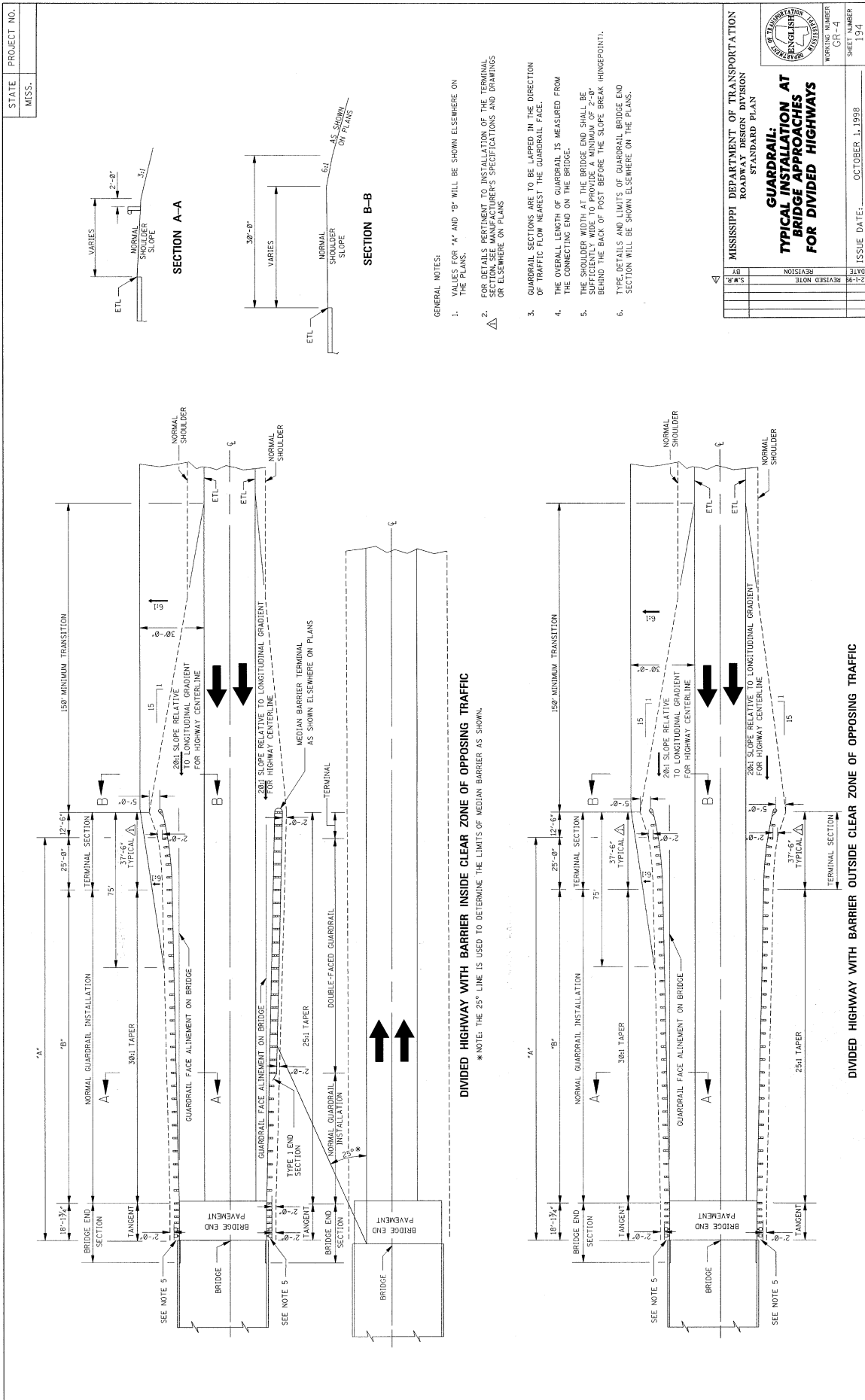
GUARDRAIL BOLT "C" WITH RECTANGULAR GUARDRAIL PLATE UNDER NUT FOR POST 5.

GUARDRAIL BOLT "C" WITH RECTANGULAR GUARDRAIL PLATE UNDER NUT FOR POST 4.

GUARDRAIL BOLT "C" WITH RECTANGULAR GUARDRAIL PLATE UNDER NUT FOR POST 3.

GUARDRAIL BOLT "C" WITH RECTANGULAR GUARDRAIL PLATE UNDER NUT FOR POST 2.

GUARDRAIL BOLT "C" WITH RECTANGULAR GUARDRAIL PLATE UNDER NUT FOR POST 1.



STATE	PROJECT NO.
MISS.	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
GUARDRAIL: TYPICAL INSTALLATION AT BRIDGE APPROACHES FOR DIVIDED HIGHWAYS	
DATE	ISSUE DATE: OCTOBER 1, 1998
REVISION	
REVISED NOTE	
CR	WORK NUMBER
CR	CR NUMBER
CR	SHEET NUMBER
CR	194

GENERAL NOTES:

- VALUES FOR "A" AND "B" WILL BE SHOWN ELSEWHERE ON THE PLANS.
- FOR DETAILS PERTINENT TO INSTALLATION OF THE TERMINAL SECTION, SEE MANUFACTURER'S SPECIFICATIONS AND DRAWINGS OR ELSEWHERE ON PLANS.
- GUARDRAIL SECTIONS ARE TO BE LAPPED IN THE DIRECTION OF TRAFFIC FLOW NEAREST THE GUARDRAIL FACE.
- THE OVERALL LENGTH OF GUARDRAIL IS MEASURED FROM THE CONNECTING END ON THE BRIDGE.
- THE SHOULDER WIDTH AT THE BRIDGE END SHALL BE SUFFICIENTLY WIDE TO PROVIDE A MINIMUM OF 2'-0" BEHIND THE BACK OF POST BEFORE THE SLOPE BREAK (HINGEPOINT).
- TYPE DETAILS AND LIMITS OF GUARDRAIL BRIDGE END SECTION WILL BE SHOWN ELSEWHERE ON THE PLANS.

DIVIDED HIGHWAY WITH BARRIER INSIDE CLEAR ZONE OF OPPOSING TRAFFIC

* NOTE: THE 25' LINE IS USED TO DETERMINE THE LIMITS OF MEDIAN BARRIER AS SHOWN.

DIVIDED HIGHWAY WITH BARRIER OUTSIDE CLEAR ZONE OF OPPOSING TRAFFIC

STATE MISS.	PROJECT NO.	
----------------	-------------	--

STEEL TUBE ANCHOR

NOTE: TERMINAL POST SHOULD BE ABLE TO BE MADE OF S4S TIMBER WITH ACTUAL INSIDE DIMENSIONS OF THIS GUARANTIZED TUBE CANNOT BE LESS THAN 7/2" X 9/2".

TERMINAL POST

NOTE: TERMINAL POST SHALL BE MADE OF S4S TIMBER WITH STRESS GRADE OF 1200 lb-ft/in².

ANCHOR PLATE ASSEMBLY DETAILS

"W" BEAM TERMINAL CONNECTOR PLATE

NOTES:
 1. THE "W" BEAM TERMINAL CONNECTOR IS TO BE AASHTO M 188 CORRUGATED SHEET STEEL CLASS B, TYPE I.
 2. SPLICE-BOLT SLOTS MAY ALSO BE ORIENTED AT 90° TO THE FLAT INSTEAD OF 0° AS SHOWN.

SOIL PLATE

NOTE: 2 REQUIRED

BEARING PLATE

END PLATE

BRACKET

ANCHOR PLATE

CABLE ANCHOR ASSEMBLY

GUARDRAIL BOLT

GUARDRAIL NUT

RECESSED NUT

HEX NUT AND BOLT

RECTANGULAR GUARDRAIL PLATE WASHER

HEX NUT

PLAIN ROUND WASHER

CABLE ANCHOR ASSEMBLY

BREAKAWAY TERMINAL POST SLEEVE

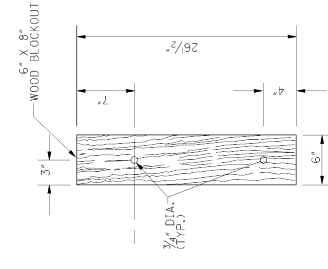
SHELF ANGLE BRACKET

BOLT	L (MIN.)	T
"A"	1/2"	1"
"B"	2"	1 3/4"
"D"	18"	4"

WASHER	A (NOM.)	B (NOM.)	C (NOM.)
"F"	1/2"	1 3/4"	3/8"
"H"	1/2"	2"	3/8"

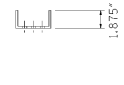
NOTES:
 1. L GUARDRAIL BOLTS ARE 3/8" - 11 UNC THREAD PITCH.
 2. IF ANY BOLT EXTENDS MORE THAN 1/4" FROM THE NUT, THE BOLT SHOULD BE TRIMMED BACK.

FASTENER DETAILS

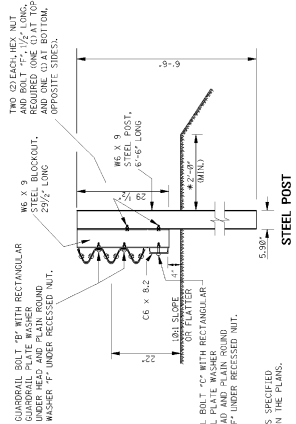


WOOD BLOCKOUT

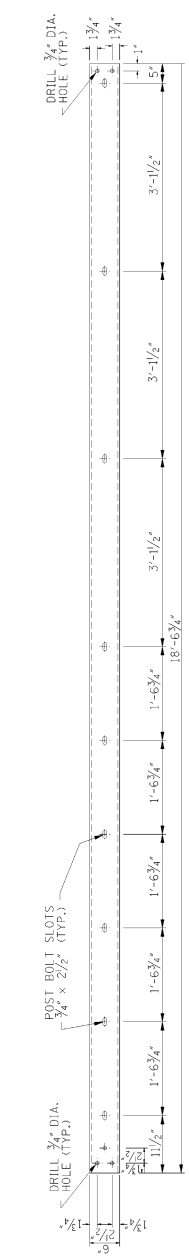
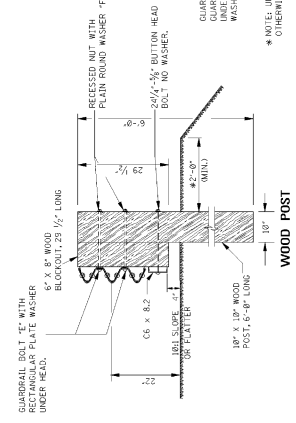
BLOCKOUT FOR POST 9 OF TYPE 1" BRIDGE END SECTION



CROSS-SECTION VIEW OF C6 x 8.2

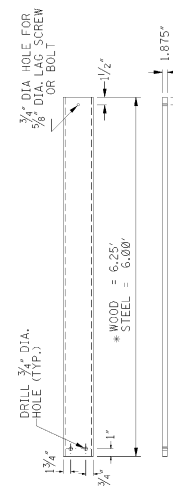


PROFILE VIEW OF POSTS

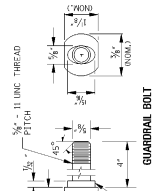


LONGITUDINAL RUBRAIL

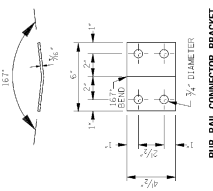
C6 x 8.2



DIAGONAL RUBRAIL



T-BRACKET



RUB RAIL CONNECTOR BRACKET

POSTS 1 THROUGH 6 REQUIRE AN ADDITIONAL HOLE TO ATTACH LOWER BLOCKOUTS. STEEL POST REQUIRE 2 HOLES ON EITHER SIDE OF THE POST.
RUBRAIL BLOCKOUTS ARE TO BE SECURED WITH 3/4" BUTTONHEAD BOLT FOR STEEL POST AND 2 1/2" BOLT FOR WOOD POST. (SEE PROFILE VIEW FOR LENGTHS)
RUBRAIL & LONG BLOCKOUT (POST 9) ARE NOT REQUIRED IN CURBS & GUTTER SECTIONS OF ROADWAY.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
GUARDRAIL: RUB RAIL HARDWARE SHEET	
WORKING NUMBER: GR-R-10	SHEET NUMBER: 1
FILE NUMBER: 111-25-415	DATE: 04-21-97
CHECKED:	DESIGNED:
BY:	REVISION:

LOCATION OF FAILED AREAS				
Begin Station	End Station	Rt or Lt Lane	Length (ft)	Width (ft)
406+94	426+01	Lt	1907	6
416+97	421+97	Rt	500	6
389+17	393+17	Lt	400	6
380+70	382+50	Rt	180	6
375+65	376+85	Rt	120	6
Anne Drive-In Road		Lt	15	5
324+70	332+13	Lt	743	6
322+75	323+25	Rt	50	6
290+47	291+47	Lt	100	6
Landfill Road		Lt	75	10
256+64	259+14	Rt	250	6
Box Culvert @ 255+55		Rt	50	10
			50	12
244+76	245+96	Rt	120	6
178+52	180+52	Rt	200	6
161+19	161+99	Rt	80	14
161+99	163+69	Rt	170	6
161+19	162+39	Lt	120	6

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904- NOTICE TO BIDDERS NO. 85

CODE: (SP)

DATE: 03/29/2016

SUBJECT: Smoothness Tolerances

Bidders are hereby advised that the smoothness tolerances for this project shall meet the requirements of a Category C project according to Subsection 907-403.03.2.1. Bidders are responsible for the collection of a preliminary smoothness profile prior to any work being performed.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 113

CODE: (SP)

DATE: 04/18/2017

SUBJECT: Tack Coat

Bidders are advised that in addition to the products listed on the Department's APL as referenced in Subsection 401.03.1.2 on page 256, the Contractor may use one of the following as a tack coat.

- CSS-1
- CSS-1h
- SS-1
- SS-1h

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

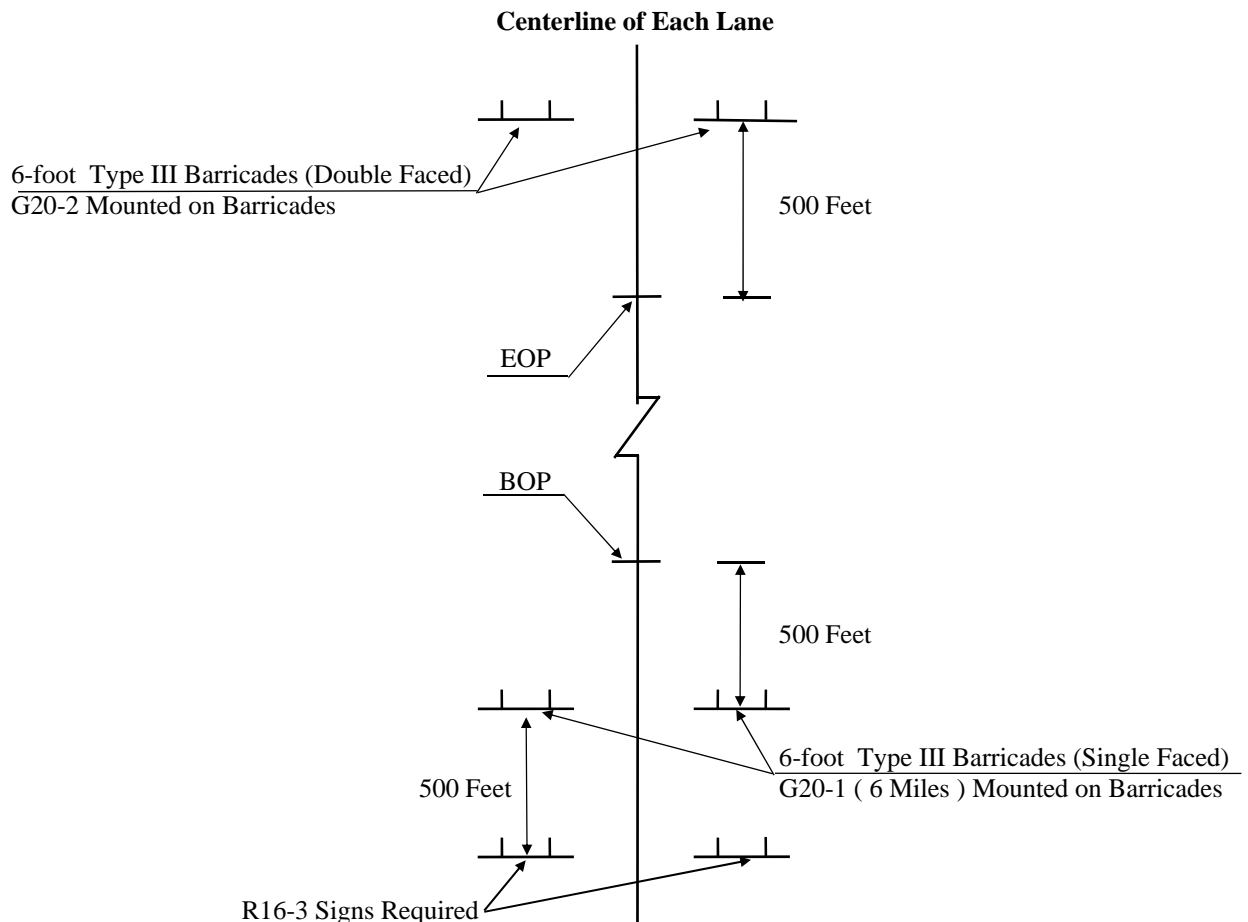
SUPPLEMENT TO SPECIAL PROVISION NO. 907-618-3

DATE: 03/23/2017

PROJECT: MP-3082-76(015) / 306136301 - Washington County

After the first paragraph of Subsection 907-618.01.2 on page 1, add the following.

Additional signs will be required as follows.



ADDITIONAL TRAFFIC CONTROL SIGNS REQUIRED:

- 11 - W20-1 "AHEAD" signs required. One (1) sign is required at each local road or street entering the project.
- 10 - R16-3 "SPEEDING FINES DOUBLED" signs required.

R16-3 signs shall be spaced in accordance with sheet titled "Location of R16-3 Signs".

All Construction signing is included in the bid for Pay Item 618-A, Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except for R16-3 which shall be black legend and border on white background.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-618-3

CODE: (SP)

DATE: 01/17/2017

SUBJECT: Additional Signing Requirements

Section 618, Maintenance of Traffic and Traffic Control Plan, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-618.01.2--Traffic Control Plan. At the end of Subsection 618.01.2 on page 441, add the following:

For compliance with the traffic control plan, the Contractor will be required to install and maintain construction signs at various locations throughout the project. Payment for these signs will be included in the price bid for pay item no. 618-A, Maintenance of Traffic per lump sum.

SECTION 905 - PROPOSAL

Date _____

Mississippi Transportation Commission
Jackson, Mississippi

Sirs: The following proposal is made on behalf of _____
_____ of _____

for constructing the following designated project(s) within the time(s) hereinafter specified.

The plans are composed of drawings and blue prints on file in the offices of the Mississippi Department of Transportation, Jackson, Mississippi.

The Specifications are the current Standard Specifications of the Mississippi Department of Transportation approved by the Federal Highway Administration, except where superseded or amended by the plans, Special Provisions and Notice(s) to Bidders attached hereto and made a part thereof.

I (We) certify that I (we) possess a copy of said Standard and any Supplemental Specifications.

Evidence of my (our) authority to submit the Proposal is hereby furnished. The proposal is made without collusion on the part of any person, firm or corporation. I (We) certify that I (we) have carefully examined the Plans, the Specifications, including the Special Provisions and Notice(s) to Bidders, herein, and have personally examined the site of the work. On the basis of the Specifications, Special Provisions, Notice(s) to Bidders, and Plans, I (we) propose to furnish all necessary machinery, tools, apparatus and other means of construction and do all the work and furnish all the materials in the manner specified. I (We) understand that the quantities mentioned herein are approximate only and are subject to either increase or decrease, and hereby propose to perform any increased or decreased quantities of work at the unit prices bid, in accordance with the above.

I (We) acknowledge that this proposal will be found irregular and/or non-responsive unless a certified check, cashier's check, or Proposal Guaranty Bond in the amount as required in the Advertisement (or, by law) is submitted electronically with the proposal or is delivered to the Contract Administration Engineer prior to the bid opening time specified in the advertisement.

INSTRUCTION TO BIDDERS: Alternate and Optional Items on Bid Schedule.

1. Two or more items entered opposite a single unit quantity WITHOUT DEFINITE DESIGNATION AS "ALTERNATE ITEMS" are considered as "OPTIONAL ITEMS". Bidders may or may not indicate on bids the Optional Item proposed to be furnished or performed WITHOUT PREJUDICE IN REGARD TO IRREGULARITY OF BIDS.
2. Items classified on the bid schedule as "ALTERNATE ITEMS" and/or "ALTERNATE TYPES OF CONSTRUCTION" must be preselected and indicated on bids. However, "Alternate Types of Construction" may include Optional Items to be treated as set out in Paragraph 1, above.
3. Optional items not preselected and indicated on the bid schedule MUST be designated in accordance with Subsection 102.06 prior to or at the time of execution of the contract.
4. Optional and Alternate items designated must be used throughout the project.

I (We) further propose to perform all "force account or extra work" that may be required of me (us) on the basis provided in the Specifications and to give such work my (our) personal attention in order to see that it is economically performed.

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) shall submit electronically with our proposal or deliver prior to the bid opening time a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.

Overlay approximately 6 miles of US 82 from Woodlawn to Leland, known as State Project No. MP-3082-76(015) / 306136301 in Washington County.

Line no.	Item Code	Adj Code	Quantity	Units	Description[Fixed Unit Price]
Roadway Items					
0010	202-B150		1,885	Linear Feet	Removal of Guard Rail Including Post, Blockouts & Hardware
0020	202-B188		3,578	Square Yard	Removal of Pavement, All Types and Depths
0030	203-G001	(E)	1,193	Cubic Yard	Excess Excavation, FM, AH
0040	304-B005	(GT)	1,686	Ton	Granular Material, Class 5, Group E
0050	403-A009	(BA1)	1,184	Ton	25-mm, ST, Asphalt Pavement
0060	403-A013	(BA1)	3,500	Ton	9.5-mm, HT, Asphalt Pavement
0070	403-B010	(BA1)	410	Ton	9.5-mm, HT, Asphalt Pavement, Leveling
0080	403-D007	(BA1)	13,000	Ton	9.5-mm, HT, Asphalt Pavement, Polymer Modified
0090	406-A002		2,916	Square Yard	Cold Milling of Bituminous Pavement, All Depths
0100	407-A001	(A2)	14,615	Gallon	Asphalt for Tack Coat
0110	423-A001		10	Mile	Rumble Strips, Ground In
0120	606-B001		1,375	Linear Feet	Guard Rail, Class A, Type 1
0130	606-B010		220	Linear Feet	Guard Rail, Class A, Type 1, Thrie Beam
0140	606-E007		8	Each	Guard Rail, Terminal End Section, Non-Flared
0150	606-G004		16	Each	Special Sections, Thrie Beam, Transition Section
0160	618-A001		1	Lump Sum	Maintenance of Traffic
0170	618-B001		1	Square Feet	Additional Construction Signs (\$10.00)
0180	619-A1004		10	Mile	Temporary Traffic Stripe, Continuous White, Paint
0190	619-A2004		10	Mile	Temporary Traffic Stripe, Continuous Yellow, Paint
0200	619-A3003		10	Mile	Temporary Traffic Stripe, Skip White, Paint
0210	619-A4004		10	Mile	Temporary Traffic Stripe, Skip Yellow, Paint
0220	619-A5002		1,211	Linear Feet	Temporary Traffic Stripe, Detail, Paint White
0230	619-A5002		568	Linear Feet	Temporary Traffic Stripe, Detail, Paint Yellow
0240	619-A6003		3,477	Square Feet	Temporary Traffic Stripe, Legend, Paint
0250	619-A6004		945	Linear Feet	Temporary Traffic Stripe, Legend, Paint
0260	620-A001		1	Lump Sum	Mobilization
0270	626-A001		10	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip White
0280	626-C002		10	Mile	6" Thermoplastic Double Drop Edge Stripe, Continuous White
0290	626-D001		10	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip Yellow
0300	626-E001		10	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow
0310	626-G004		1,211	Linear Feet	Thermoplastic Double Drop Detail Stripe, White
0320	626-G005		568	Linear Feet	Thermoplastic Double Drop Detail Stripe, Yellow
0330	626-H001		3,477	Square Feet	Thermoplastic Double Drop Legend, White
0340	626-H002		840	Linear Feet	Thermoplastic Double Drop Legend, White

Line no.	Item Code	Adj Code	Quantity	Units	Description[Fixed Unit Price]
0350	627-J001		230	Each	Two-Way Clear Reflective High Performance Raised Markers
0360	627-K001		1,337	Each	Red-Clear Reflective High Performance Raised Markers
0370	627-L001		4,003	Each	Two-Way Yellow Reflective High Performance Raised Markers
0380	630-F012		56	Each	Delineators, Post Mounted, Single White
ALTERNATE GROUP AA NUMBER 1					
0390	304-F001	(GT)	2,013	Ton	3/4" and Down Crushed Stone Base
ALTERNATE GROUP AA NUMBER 2					
0400	304-F002	(GT)	2,013	Ton	Size 610 Crushed Stone Base
ALTERNATE GROUP AA NUMBER 3					
0410	304-F003	(GT)	2,013	Ton	Size 825B Crushed Stone Base

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

CONDITIONS FOR COMBINATION BID

If a bidder elects to submit a combined bid for two or more of the contracts listed for this month's letting, the bidder must complete and execute these sheets of the proposal in each of the individual proposals to constitute a combination bid. In addition to this requirement, each individual contract shall be completed, executed and submitted in the usual specified manner.

Failure to execute this Combination Bid Proposal in each of the contracts combined will be just cause for each proposal to be received and evaluated as a separate bid.

It is understood that the Mississippi Transportation Commission not only reserves the right to reject any and all proposals, but also the right to award contracts upon the basis of lowest separate bids or combination bids most advantageous to the State.

It is further understood and agreed that the Combination Bid Proposal is for comparison of bids only and that each contract shall operate in every respect as a separate contract in accordance with its proposal and contract documents.

I (We) agree to complete each contract on or before its specified completion date.

COMBINATION BID PROPOSAL

This proposal is tendered as one part of a Combination Bid Proposal utilizing option ___* of Subsection 102.11 on the following contracts:

* Option to be shown as either (a), (b), or (c).

	<u>Project No.</u>	<u>County</u>	<u>Project No.</u>	<u>County</u>
1.	_____	_____	6.	_____
2.	_____	_____	7.	_____
3.	_____	_____	8.	_____
4.	_____	_____	9.	_____
5.	_____	_____	10.	_____

(a) If Combination A has been selected, your Combination Bid is complete.

(b) If Combination B has been selected, then complete the following page.

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
1.					
2.					
3.					
4.					
5.					
6.					
7.					
8.					

For Informational Purposes Only

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
9.					
10.					

(c) If Combination C has been selected, then initial and complete ONE of the following.

_____ I (We) desire to be awarded work not to exceed a total monetary value of \$ _____.

_____ I (We) desire to be awarded work not to exceed _____ number of contracts.



TO: EXECUTIVE DIRECTOR, MISSISSIPPI DEPARTMENT OF TRANSPORTATION
JACKSON, MISSISSIPPI

CERTIFICATE

If awarded this contract, I (we) contemplate that portions of the contract will be sublet. I (we) certify that those subcontracts which are equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

I (we) agree that this notification of intent DOES NOT constitute APPROVAL of the subcontracts.

_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)

NOTE: Failure to complete the above DOES NOT preclude subsequent subcontracts. Subsequent subcontracts, if any, equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

Contractor _____

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
CERTIFICATION

I, _____,
(Name of person signing bid)

individually, and in my capacity as _____ of
(Title of person signing bid)

(Name of Firm, partnership, or Corporation)

do hereby certify under penalty of perjury under the laws of the United States and the State of Mississippi

that _____, Bidder
(Name of Firm, Partnership, or Corporation)

on Project No. **MP-3082-76(015)/ 306136301000**

in **Washington** County(ies), Mississippi, has not either directly or indirectly entered into any agreement, participated in any collusion; or otherwise taken any action in restraint of free competitive bidding in connection with this contract; nor have any of its corporate officers or principal owners.

Except as noted hereafter, it is further certified that said legal entity and its corporate officers, principal owners, managers, auditors and others in a position of administering federal funds are not currently under suspension, debarment, voluntary exclusion or determination of ineligibility; nor have a debarment pending; nor been suspended, debarred, voluntarily excluded or determined ineligible within the past three years by the Mississippi Transportation Commission, the State of Mississippi, any other State or a federal agency; nor been indicted, convicted or had a civil judgment rendered by a court of competent jurisdiction in any matter involving fraud or official misconduct within the past three years.

Do exceptions exist and are made a part thereof? Yes / No

Any exceptions shall address to whom it applies, initiating agency and dates of such action.

Note: Exceptions will not necessarily result in denial of award but will be considered in determining bidder responsibility. Providing false information may result in criminal prosecution or administrative sanctions.

All of the foregoing is true and correct.

(1/2016 S)

SECTION 902

CONTRACT FOR MP-3082-76(015)/ 306136301000

LOCATED IN THE COUNTY(IES) OF Washington

STATE OF MISSISSIPPI,
COUNTY OF HINDS

This contract entered into by and between the Mississippi Transportation Commission on one hand, and the undersigned contractor, on the other witnesseth;

That, in consideration of the payment by the Mississippi Transportation Commission of the prices set out in the proposal hereto attached, to the undersigned contractor, such payment to be made in the manner and at the time of times specified in the specifications and the special provisions, if any, the undersigned contractor hereby agrees to accept the prices stated in the proposal in full compensation for the furnishing of all materials and equipment and the executing of all the work contemplated in this contract.

It is understood and agreed that the advertising according to law, the Advertisement, the instructions to bidders, the proposal for the contract, the specifications, the revisions of the specifications, the special provisions, and also the plans for the work herein contemplated, said plans showing more particularly the details of the work to be done, shall be held to be, and are hereby made a part of this contract by specific reference thereto and with like effect as if each and all of said instruments had been set out fully herein in words and figures.

It is further agreed that for the same consideration the undersigned contractor shall be responsible for all loss or damage arising out of the nature of the work aforesaid; or from the action of the elements and unforeseen obstructions or difficulties which may be encountered in the prosecution of the same and for all risks of every description connected with the work, exceptions being those specifically set out in the contract; and for faithfully completing the whole work in good and workmanlike manner according to the approved Plans, Specifications, Special Provisions, Notice(s) to Bidders and requirements of the Mississippi Department of Transportation.

It is further agreed that the work shall be done under the direct supervision and to the complete satisfaction of the Executive Director of the Mississippi Department of Transportation, or his authorized representatives, and when Federal Funds are involved subject to inspection at all times and approval by the Federal Highway Administration, or its agents as the case may be, or the agents of any other Agency whose funds are involved in accordance with those Acts of the Legislature of the State of Mississippi approved by the Governor and such rules and regulations issued pursuant thereto by the Mississippi Transportation Commission and the authorized Federal Agencies.

The Contractor agrees that all labor as outlined in the Special Provisions may be secured from list furnished by

It is agreed and understood that each and every provision of law and clause required by law to be inserted in this contract shall be deemed to be inserted herein and this contract shall be read and enforced as though it were included herein, and, if through mere mistake or otherwise any such provision is not inserted, then upon the application of either party hereto, the contract shall forthwith be physically amended to make such insertion.

The Contractor agrees that he has read each and every clause of this Contract, and fully understands the meaning of same and that he will comply with all the terms, covenants and agreements therein set forth.

Witness our signatures this the ___ day of _____, _____.

Contractor(s)

By _____

MISSISSIPPI TRANSPORTATION COMMISSION

Title _____

By _____

Signed and sealed in the presence of:
(names and addresses of witnesses)

Executive Director

Secretary to the Commission

Award authorized by the Mississippi Transportation Commission in session on the ___ day of _____, _____, Minute Book No. _____, Page No. _____.

Revised 8/06/2003

SECTION 903
PERFORMANCE AND PAYMENT BOND

CONTRACT BOND FOR: MP-3082-76(015)/ 306136301000

LOCATED IN THE COUNTY(IES) OF: Washington

STATE OF MISSISSIPPI,
COUNTY OF HINDS

Know all men by these presents: that we, _____

Principal, a _____ (Contractor)

residing at _____ in the State of _____

and _____

(Surety)

residing at _____ in the State of _____,

authorized to do business in the State of Mississippi, under the laws thereof, as surety, effective as of the contract date

shown below, are held and firmly bound unto the State of Mississippi in the sum of _____

_____ Dollars, lawful money of the United States of America, to be paid to it for which payment well and truly to be made, we bind ourselves, our heirs, administrators, successors, or assigns jointly and severally by these presents.

The conditions of this bond are such, that whereas the said _____

_____ principal, has (have) entered into a contract with the Mississippi Transportation Commission, bearing the date of _____ day of _____ A.D. _____ hereto annexed, for the construction of certain projects(s) in the State of Mississippi as mentioned in said contract in accordance with the Contract Documents therefor, on file in the offices of the Mississippi Department of Transportation, Jackson, Mississippi.

Now therefore, if the above bounden _____ in all things shall stand to and abide by and well and truly observe, do keep and perform all and singular the terms, covenants, conditions, guarantees and agreements in said contract, contained on his (their) part to be observed, done, kept and performed and each of them, at the time and in the manner and form and furnish all of the material and equipment specified in said contract in strict accordance with the terms of said contract which said plans, specifications and special provisions are included in and form a part of said contract and shall maintain the said work contemplated until its final completion and acceptance as specified in Subsection 109.11 of the approved specifications, and save harmless said Mississippi Transportation Commission from any loss or damage arising out of or occasioned by the negligence, wrongful or criminal act, overcharge, fraud, or any other loss or damage whatsoever, on the part of said principal (s), his (their) agents, servants, or employees in the performance of said work or in any manner connected therewith, and shall be liable and responsible in a civil action instituted by the State at the instance of the Mississippi Transportation Commission or any officer of the State authorized in such cases, for double any amount in money or property, the State may lose or be overcharged or otherwise defrauded of, by reason of wrongful or criminal act, if any, of the Contractor(s), his (their) agents or employees, and shall promptly pay the said agents, servants and employees and all persons furnishing labor, material, equipment or supplies therefor, including premiums incurred, for Surety Bonds, Liability Insurance, and Workmen's Compensation Insurance; with the additional obligation that such Contractor shall promptly make payment of all taxes, licenses, assessments, contributions, damages,

any liquidated damages which may arise prior to any termination of said principal's contract, any liquidated damages which may arise after termination of the said principal's contract due to default on the part of said principal, penalties and interest thereon, when and as the same may be due this state, or any county, municipality, board, department, commission or political subdivision: in the course of the performance of said work and in accordance with Sections 31-5-51 et seq. Mississippi Code of 1972, and other State statutes applicable thereto, and shall carry out to the letter and to the satisfaction of the Executive Director of the Mississippi Department of Transportation, all, each and every one of the stipulations, obligations, conditions, covenants and agreements and terms of said contract in accordance with the terms thereof and all of the expense and cost and attorney's fee that may be incurred in the enforcement of the performance of said contract, or in the enforcement of the conditions and obligations of this bond, then this obligation shall be null and void, otherwise to be and remain in full force and virtue.

_____	_____
(Contractors) Principal	Surety
By _____	By _____
	(Signature) Attorney in Fact
	Address _____

Title _____	_____
(Contractor's Seal)	(Printed) MS Agent

	(Signature) MS Agent
	Address _____

	(Surety Seal)

	Mississippi Insurance ID Number



BID BOND

KNOW ALL MEN BY THESE PRESENTS, that we _____
Contractor

Address

City, State ZIP

As principal, hereinafter called the Principal, and _____
Surety

a corporation duly organized under the laws of the state of _____

as Surety, hereinafter called the Surety, are held and firmly bound unto State of Mississippi, Jackson, Mississippi

As Obligee, hereinafter called Obligee, in the sum of **Five Per Cent (5%) of Amount Bid**

Dollars(\$ _____)

for the payment of which sum will and truly to be made, the said Principal and said Surety, bind ourselves, our heirs, executors, administrators, successors and assigns, jointly and severally, firmly by these presents.

WHEREAS, the Principal has submitted a bid for **Overlay approximately 6 miles of US 82 from Woodlawn to Leland, known as State Project No. MP-3082-76(015) / 306136301 in Washington County.**

NOW THEREFORE, the condition of this obligation is such that if the aforesaid Principal shall be awarded the contract, the said Principal will, within the time required, enter into a formal contract and give a good and sufficient bond to secure the performance of the terms and conditions of the contract, then this obligation to be void; otherwise the Principal and Surety will pay unto the Obligee the difference in money between the amount of the bid of the said Principal and the amount for which the Obligee legally contracts with another party to perform the work if the latter amount be in excess of the former, but in no event shall liability hereunder exceed the penal sum hereof.

Signed and sealed this _____ day of _____, 20__

(Witness)

(Principal) (Seal)

By: _____
(Name) (Title)

(Surety) (Seal)

(Witness)

By: _____
(Attorney-in-Fact)

(MS Agent)

Mississippi Insurance ID Number

