



SM No. CMP6000212571

PROPOSAL AND CONTRACT DOCUMENTS

FOR THE CONSTRUCTION OF

16

Overlay approximately 2 miles of SR 57 from Chickasawhay River Bridge to SR 63/57 4-Lane, & approximately 6 miles of SR 594 from SR 63/57 4-Lane to Alabama State Line, known as State Project Nos. MP-6000-21(257) / 306148301 & 302 in Greene County.

Project Completion: Flexible

(STATE DELEGATED)

NOTICE

**BIDDERS MUST PURCHASE A BID PROPOSAL FROM
MDOT CONTRACT ADMINISTRATION DIVISION TO BID
THIS PROJECT.**

Electronic addendum updates will be posted on www.gomdot.com

SECTION 900

OF THE CURRENT 2017 STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION JACKSON, MISSISSIPPI

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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MP-6000-21(257)/306148302 - Greene**

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OF SECTION 905 AS ADDENDA)

04/26/2017 09:46 AM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 901 - ADVERTISEMENT

Electronic bids will be received by the Mississippi Transportation Commission at 10:00 o'clock A.M., Tuesday, May 23, 2017, from the Bid Express Service and shortly thereafter publicly read on the Sixth Floor for:

Overlay approximately 2 miles of SR 57 from Chickasawhay River Bridge to SR 63/57 4-Lane, & approximately 6 miles of SR 594 from SR 63/57 4-Lane to Alabama State Line, known as State Project Nos. MP-6000-21(257) / 306148301 & 302 in Greene County.

The attention of bidders is directed to the predetermined minimum wage rate set by the U. S. Department of Labor under the Fair Labor Standards Act.

The Mississippi Department of Transportation hereby notifies all bidders that it will affirmatively insure that in any contract entered into pursuant to this advertisement, disadvantaged business enterprises will be afforded full opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of race, color, sex, age, disability, religion or national origin in consideration for an award.

The specifications are on file in the offices of the Mississippi Department of Transportation.

Bid proposals must be purchased online at <https://shopmdot.ms.gov>. Specimen proposals may be viewed and downloaded online at no cost at <http://mdot.ms.gov> or purchased online. Proposals are available at a cost of Ten Dollars (\$10.00) per proposal plus a small convenience fee. Cash or checks will not be accepted as payment.

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

MELINDA L. MCGRATH
EXECUTIVE DIRECTOR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Governing Specifications

The current (2017) Edition of the Standard Specifications for Road and Bridge Construction adopted by the Mississippi Transportation Commission is made a part hereof fully and completely as if it were attached hereto, except where superseded by special provisions, or amended by revisions of the Specifications contained within this proposal. Copies of the specification book may be purchased from the MDOT Construction Division, or online at shopmdot/default.aspx?StoreIndex=1.

A reference in any contract document to controlling requirements in another portion of the contract documents shall be understood to apply equally to any revision or amendment thereof included in the contract.

In the event the plans or proposal contain references to the 2004 Edition of the Standard Specifications for Road and Bridge Construction, it is to be understood that such references shall mean the comparable provisions of the 2017 Edition of the Standard Specifications.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 3

CODE: (SP)

DATE: 01/17/2017

SUBJECT: Final Clean-Up

Immediately prior to final inspection for release of maintenance, the Contractor shall pick up, load, transport and properly dispose of all litter from the entire highway right-of-way that is within the termini of the project.

Litter shall include, but not be limited to, solid wastes such a glass, paper products, tires, wood products, metal, synthetic materials and other miscellaneous debris.

Litter removal is considered incidental to other items of work and will not be measured for separate payment.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 9

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Federal Bridge Formula

Bidders are hereby advised that the latest revision of Federal Highway Administration Publication No. FHWA-HOP-06-105, **BRIDGE FORMULA WEIGHTS**, dated August 2006, is made a part of this contract when applicable.

Prior to the preconstruction conference, the Contractor shall advise the Engineer, in writing, what materials, if any, will be delivered to the jobsite via Interstate route(s).

Copies of the **BRIDGE FORMULA WEIGHTS** publication may be obtained by contacting:

Federal Highway Administration
400 7th Street, SW
Washington, DC 20590
(202) 366-2212

or

http://www.ops.fhwa.dot.gov/Freight/publications/brdg_frm_wgths/bridge_formula_all_rev.pdf

An on line **BRIDGE FORMULA WEIGHTS CALCULATOR** is available at

http://ops.fhwa.dot.gov/freight/sw/brdgcalc/calc_page.htm

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 12

CODE: (IS)

DATE: 03/01/2017

SUBJECT: MASH Compliant Devices

Bidders are hereby advised that the Standard Specifications may require certain traffic control and permanent safety hardware devices to meet the requirements of the Manual for Assessing Safety Hardware (MASH). However, devices meeting the requirements of NCHRP Report 350 will be allowed until the mandatory effective date for MASH compliance. The following table shows the effective dates for MASH compliant devices.

Device	Effective Date for MASH Compliance
W-beam barriers, cast-in-place concrete barriers	December 31, 2017
W-beam terminals	June 30, 2018
Cable barriers, cable barrier terminals, crash cushions	December 31, 2018
Bridge rails, transitions, all other longitudinal barriers including portable barriers installed permanently, all other terminals, sign supports, all other breakaway hardware	December 31, 2019

Temporary work zone devices, including portable barriers manufactured after December 31, 2019, must have been successfully tested to the 2016 Edition of MASH. Such devices manufactured on or before this date and successfully tested to NCHRP Report 350 or the 2009 Edition of MASH may continue to be used throughout their normal service lives.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

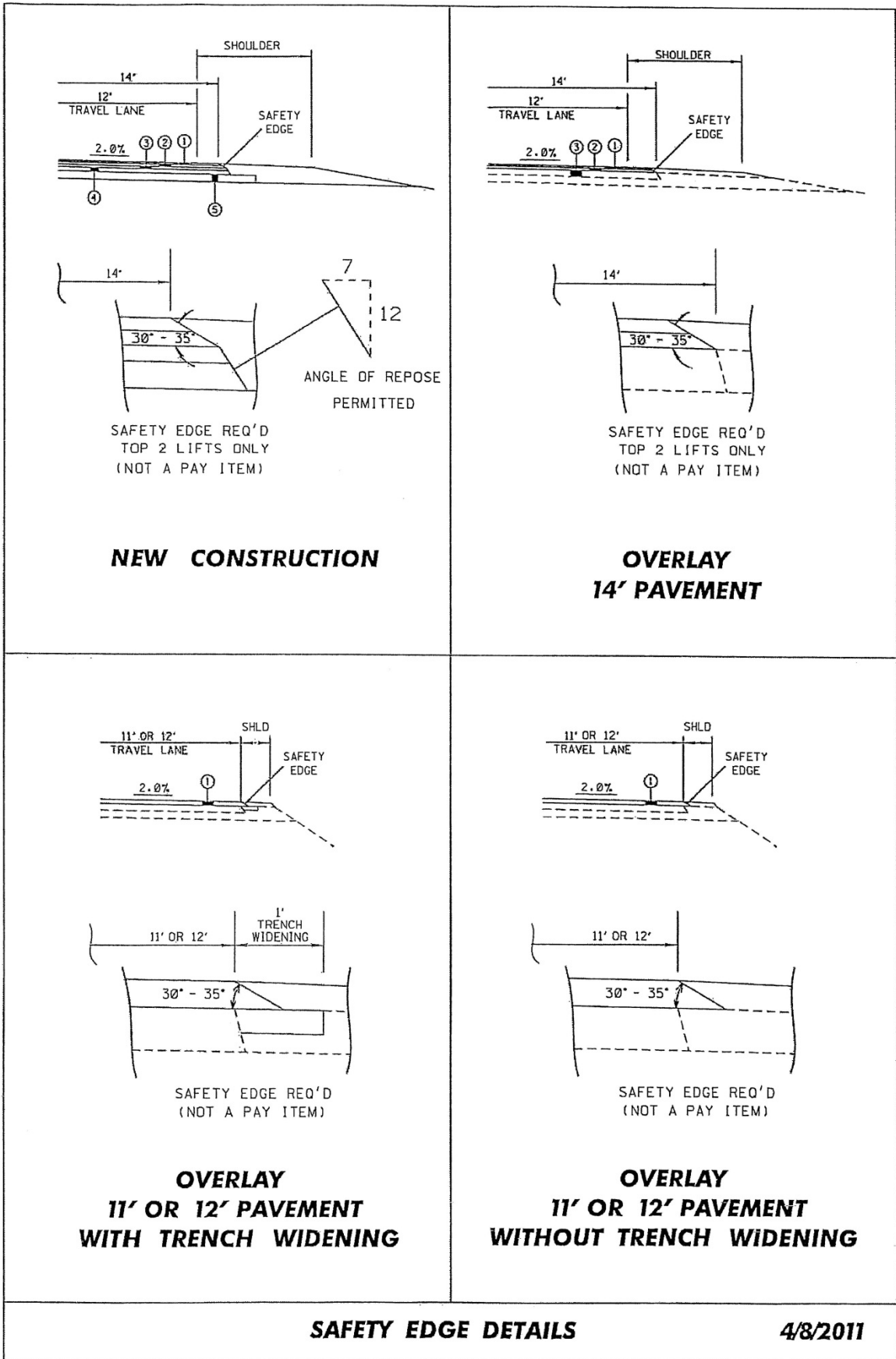
SECTION 904 - NOTICE TO BIDDERS NO. 13

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Safety Edge

Bidders are hereby advised that the Shoulder Wedge (Safety Edge) specified in Section 401, Asphalt Pavements, shall only apply to the top two (2) lifts of asphalt. Open Graded Friction Courses (OGFC) are not to be considered a lift as it pertains to safety edge. Attached is a drawing showing the safety edge. Note that the shoulder dimensions in the bottom two drawings will be less than three feet (3').



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 16

CODE: (SP)

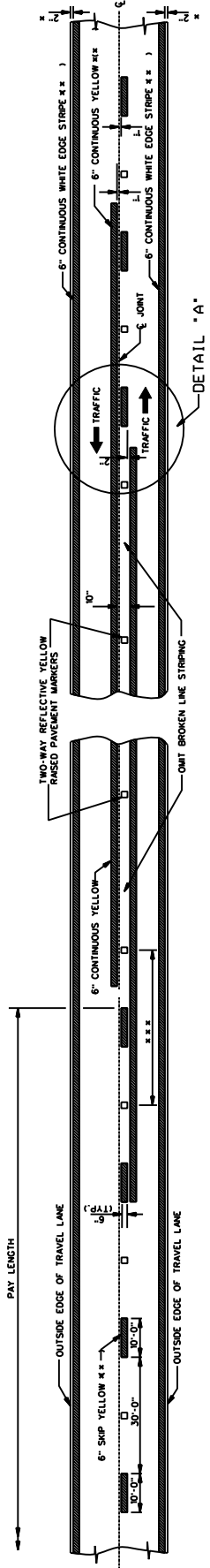
DATE: 03/01/2017

SUBJECT: Standard Drawings

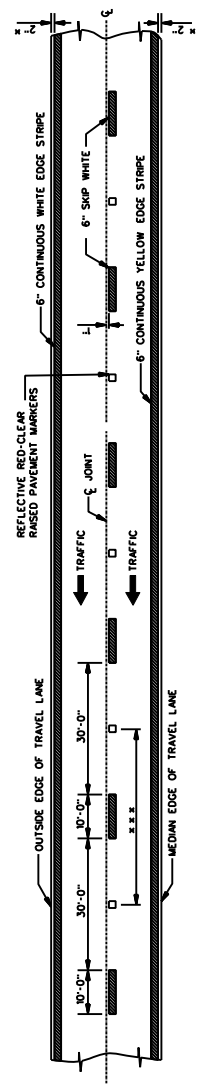
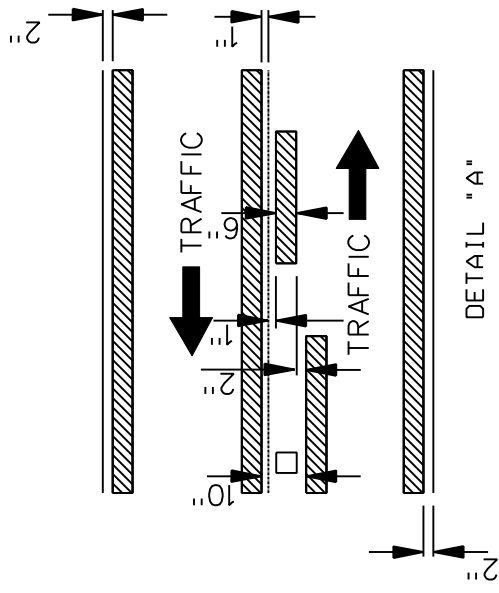
Standard Drawings attached hereto shall govern appropriate items of required work.

Larger copies of Standard Drawings may be purchased from:

MDOT Plans Print Shop
MDOT Shop Complex, Building C, Room 114
2567 North West Street
P.O. Box 1850
Jackson, MS 39215-1850
Telephone: (601) 359-7460
or FAX: (601) 359-7461
or e-mail: plans@mdot.state.ms.us



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



4-LANE WITH ONE-WAY TRAFFIC



GENERAL NOTES:

- x 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
- *** 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	URBAN AREA (U/Lin)	RURAL AREA (U/Lin)
HORIZONTAL CURVES	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	40'-0"
	40'-0"	140'-0"

1. NOTES: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR BASED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP AND 1000' BEHIND (IN DIRECTION OF TRAFFIC) OF THE ENTRANCE RAMP TAPER.

4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED SOURCES OF MATERIALS."

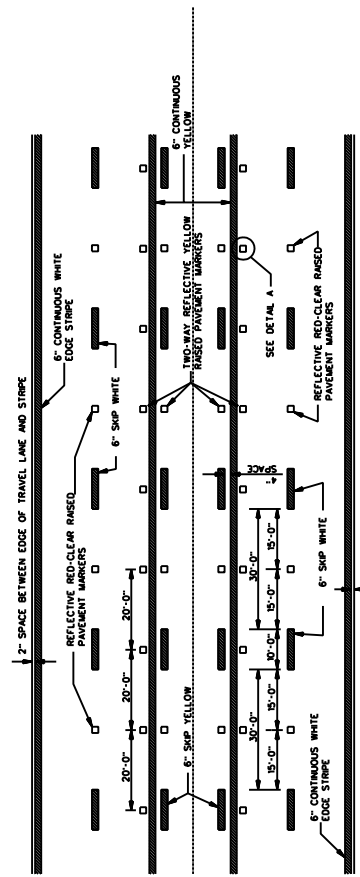
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

PAVEMENT MARKING
DETAILS FOR
2-LANE AND 4-LANE
DIVIDED HIGHWAYS

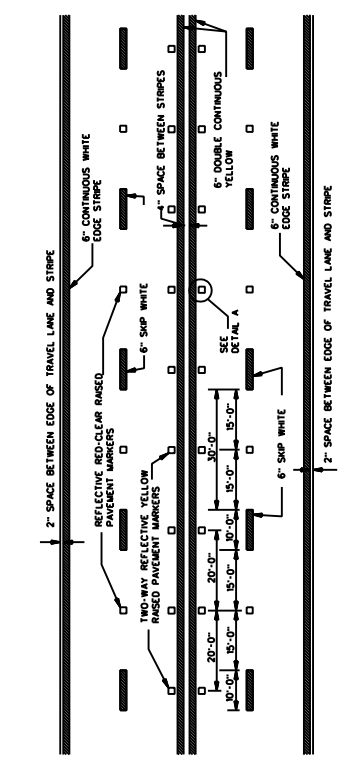
ISSUE DATE: OCTOBER 1, 1998

WORK NUMBER: PM-1
SHEET NUMBER: 100

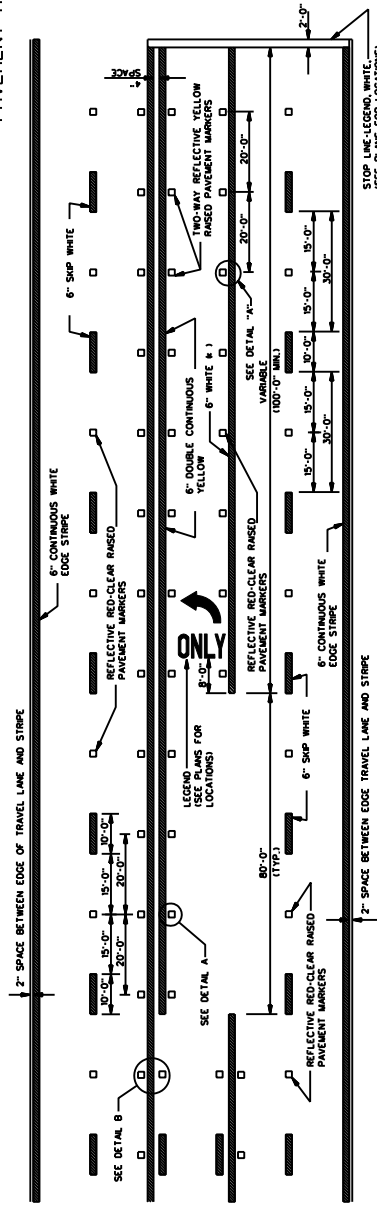
STATE	PROJECT NO.
MISS.	



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 5-LANE SECTION

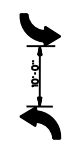


TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS AT LEFT TURN LANES

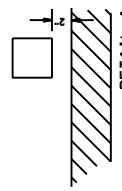
*NOTE: USE DETAIL STRIPING IF LENGTH \leq 150' AT THIS LOCATION, OTHERWISE USE CONTINUOUS STRIPING.



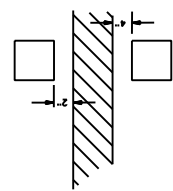
TYPICAL TWO-WAY ARROW INSTALLATION

- NOTES: 1. CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY.
- 2. IF SEGMENT IS LESS THAN 300', PLACE ONE SET OF ARROWS IN CENTER OF SEGMENT.
- 3. IF SEGMENT IS GREATER THAN 300', PLACE FIRST SET OF ARROWS 50' TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS 1250' O.C.

GENERAL NOTE:
1. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MOST APPROVED SOURCE OF MATERIALS.



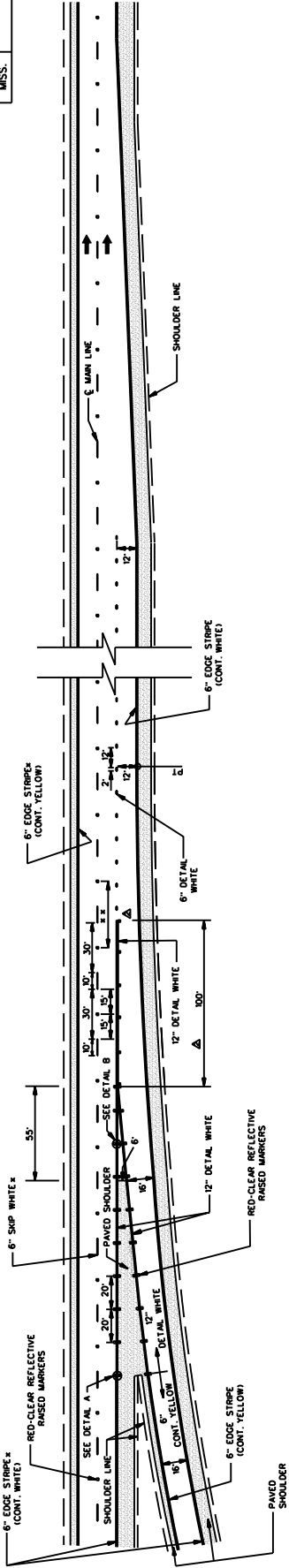
DETAIL A
LATERAL PLACEMENT OF PAVEMENT MARKERS



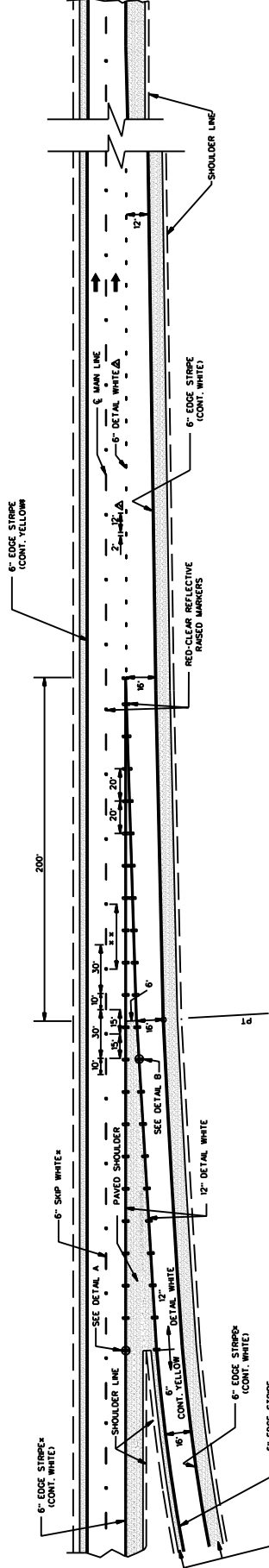
DETAIL B
LATERAL PLACEMENT OF PAVEMENT MARKERS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
PAVEMENT MARKING	
DETAILS FOR	
4-LANE AND 5-LANE	
UNDIVIDED ROADWAYS	
DATE	03/15/16
FILE NAME	SDPM-2.DGN
DESIGNER	CHC/CEL
CHECKER	
DATE	
PROJECT NO.	
SHEET NUMBER	

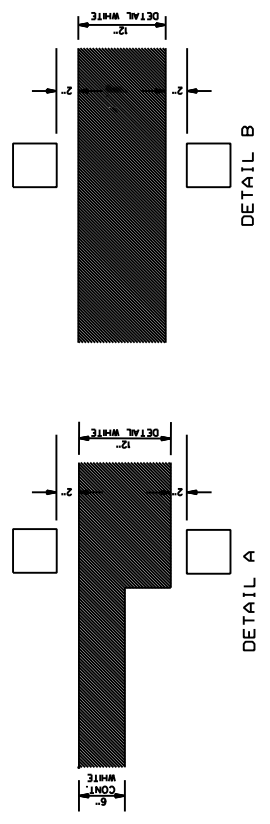
STATE	PROJECT NO.
MISS.	



PARALLEL ENTRANCE RAMP



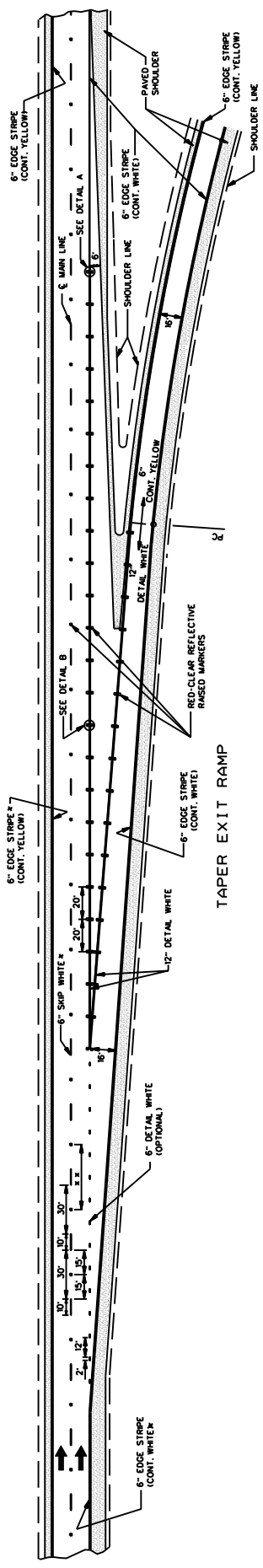
TAPER ENTRANCE RAMP



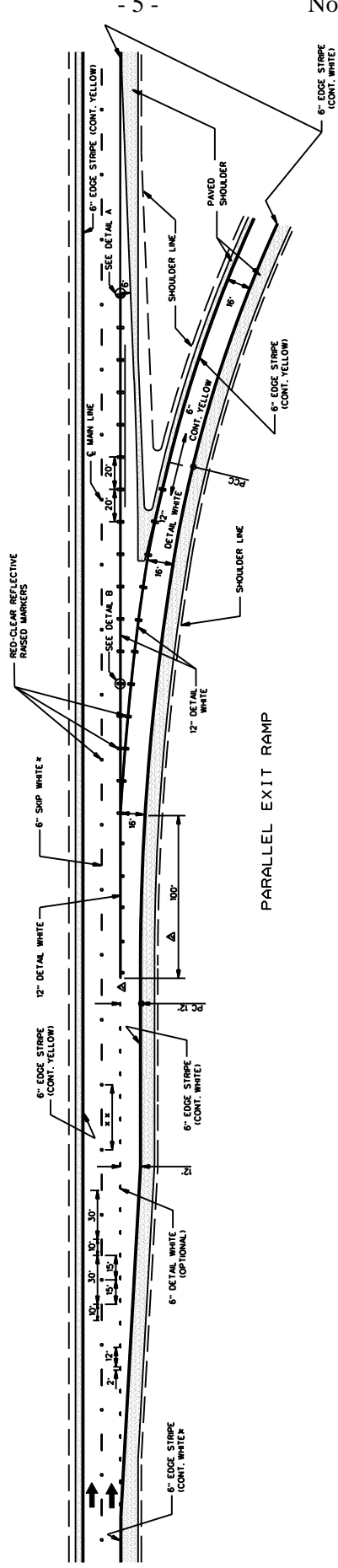
- GENERAL NOTES:
- * 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPES THROUGHOUT THE INTERCHANGE AREA BEGINNING 100' IN THE DIRECTION OF TRAVEL FROM THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - * 2. ON THE MAIN FACILITY, PLACE REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS AT A 40' SPACING ON ALL LANE-LINES THROUGHOUT THE INTERCHANGE AREA BEGINNING 100' IN THE DIRECTION OF TRAVEL FROM THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - * 3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED SOURCES OF MATERIALS."

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION	
PAVEMENT MARKING DETAILS FOR INTERCHANGE RAMP (PARALLEL AND TAPER)	
ISSUE DATE:	OCTOBER 1, 1998
WORKING NUMBER	SDPM-3
SHEET NUMBER	

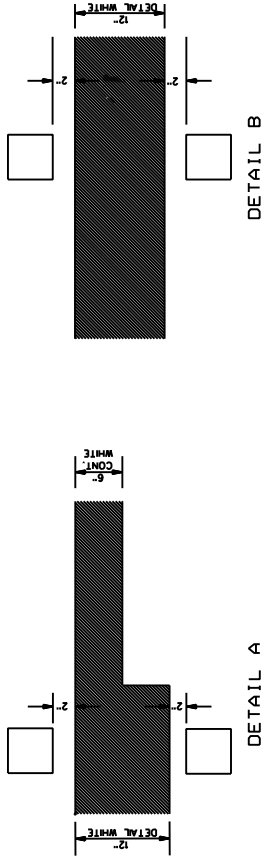
STATE	PROJECT NO.
MISS.	



TAPER EXIT RAMP



PARALLEL EXIT RAMP



- GENERAL NOTES:
- SEE SHEET PM-1 FOR THE PLACEMENT OF LINE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELLED WAY.
 - ON THE MAINLINE, PLACE REFLECTIVE RED-CLEAR RAISED MARKERS AT THE INTERCHANGE AREA BEGINNING 1000' IN ADVANCE IN DIRECTION OF TRAFFIC OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE MARKERS AS SPECIFIED IN THE MDDI.
- *APPROVED SOURCES OF MATERIALS:

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION

PAVEMENT MARKING
DETAILS FOR
INTERCHANGE
EXIT RAMP
(PARALLEL AND TAPER)

ISSUE DATE: OCTOBER 1, 1998

WORKING NUMBER: SDPM-41
SHEET NUMBER: 13

DATE	BY	REVISION
8/7/98	S.M.R.	UPDATE TO 2008 M/C/D
8/21/98	S.M.R.	REVISOR STRIPE WIDTH

STATE MISS.	PROJECT NO.		
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GENERAL NOTES:

1. UNLESS OTHERWISE SHOWN ON THE PLANS, ALL PAVEMENT MARKING LEGENDS SHALL BE APPLIED USING HIGH PERFORMANCE POLYURETHANE PAINT.
2. TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTORS) OF 1/4" OR LESS AND EXTENDING 1/2" FROM EACH END OF THE LETTER SHALL BE PERMITTED.
3. FOR OTHER DETAILS SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
4. PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

LEGEND	AREA (FT ²)
STOP	24.6
RIGHT	35.8
LEFT	35.8
TRAFFIC	27.3
WIDE	27.3
LANE	22.7
AHEAD	32.3
SCHOOL	32.3
EXIT	18.5
SIGNAL	32.5
SCHOOL	35.5

PAYMENT MARKING LEGEND DETAILS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

BY	REVISION	DATE	ISSUE DATE:	OCTOBER 1, 1998	SHEET NUMBER	124

STATE PROJECT NO.					
MISS.					

TURN ARROW

THRU ARROW

COMBINATION ARROW

1-WAY ARROW

PAY QUANTITIES	
LEGEND/SYMBOL	AREA (ft ²)
ON-1	22.0
TURN ARROW	18.4
THRU ARROW	21.2
COMB. ARROW	27.5
1-WAY ARROW	24.3

GENERAL NOTES:

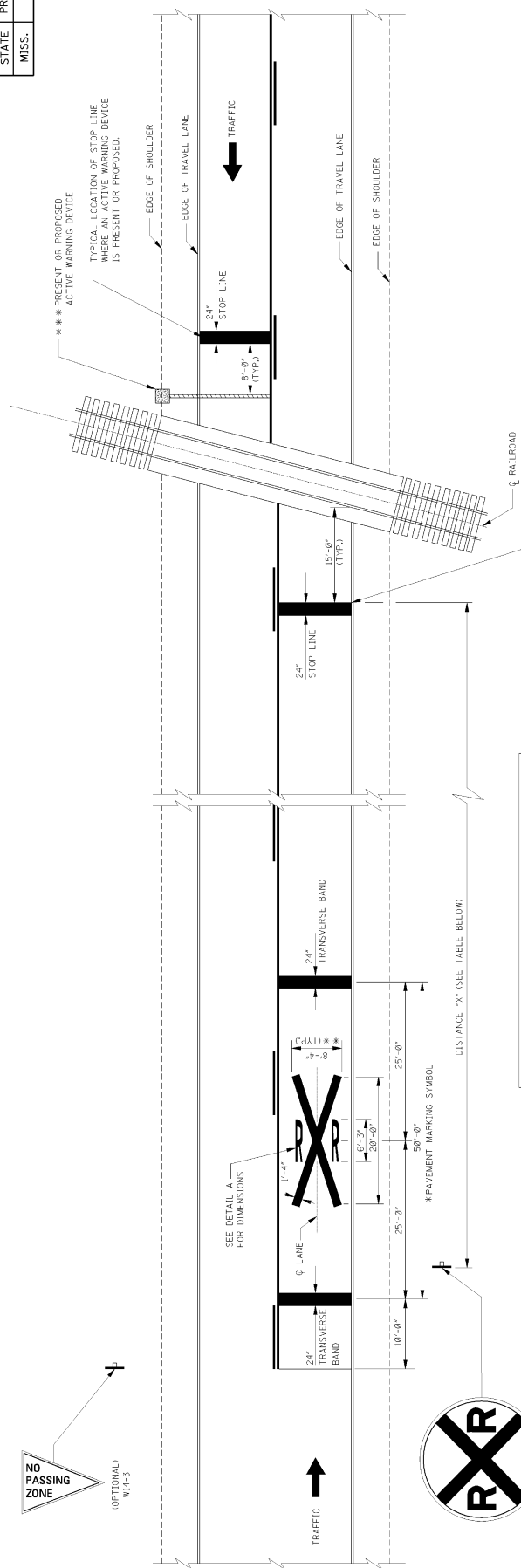
1. UNLESS OTHERWISE SHOWN ON THE PLANS, ALL PAVEMENT MARKING SHALL BE APPLIED USING HIGH PERFORMANCE MATERIALS.
2. TWO HORIZONTAL GAPS CAUSED BY TEMPLATE CONNECTORS OF 1/2" OR LESS AND EXTENDING THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
3. FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM PAVEMENT MARKING LEGENDS.
4. PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
LEGEND DETAILS**

WORKING NUMBER: PM-6
SHEET NUMBER: 125

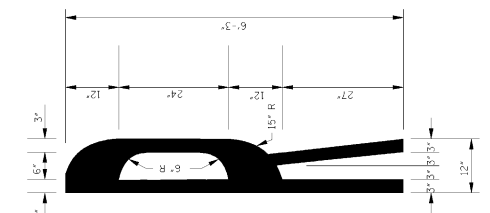
ISSUE DATE: OCTOBER 1, 1998



ADVANCE WARNING SIGN PLACEMENT DISTANCE

POSTED SPEED (mph)	DISTANCE "X" (FT)	
	RURAL	URBAN
20	175	100
25	250	100
30	325	100
35	400	② 150
40	475	② 225
45	550	300
50	625	375
55	700	450
60	775	550

NOTES:
 ① DISTANCE "X" MAY BE ADJUSTED, IF PROHIBITIVE PHYSICAL CONDITIONS EXIST AT THE DESIGNATED DISTANCE.
 ② THESE DISTANCES MAY BE ADJUSTED TO A MINIMUM OF 100 IN. FOR BUSINESS OR BUSINESS DISTRICTS WHERE LOW SPEEDS ARE PREVALENT.



DETAIL A
STANDARD "R" PAVEMENT MARKING
FOR R X R SYMBOL

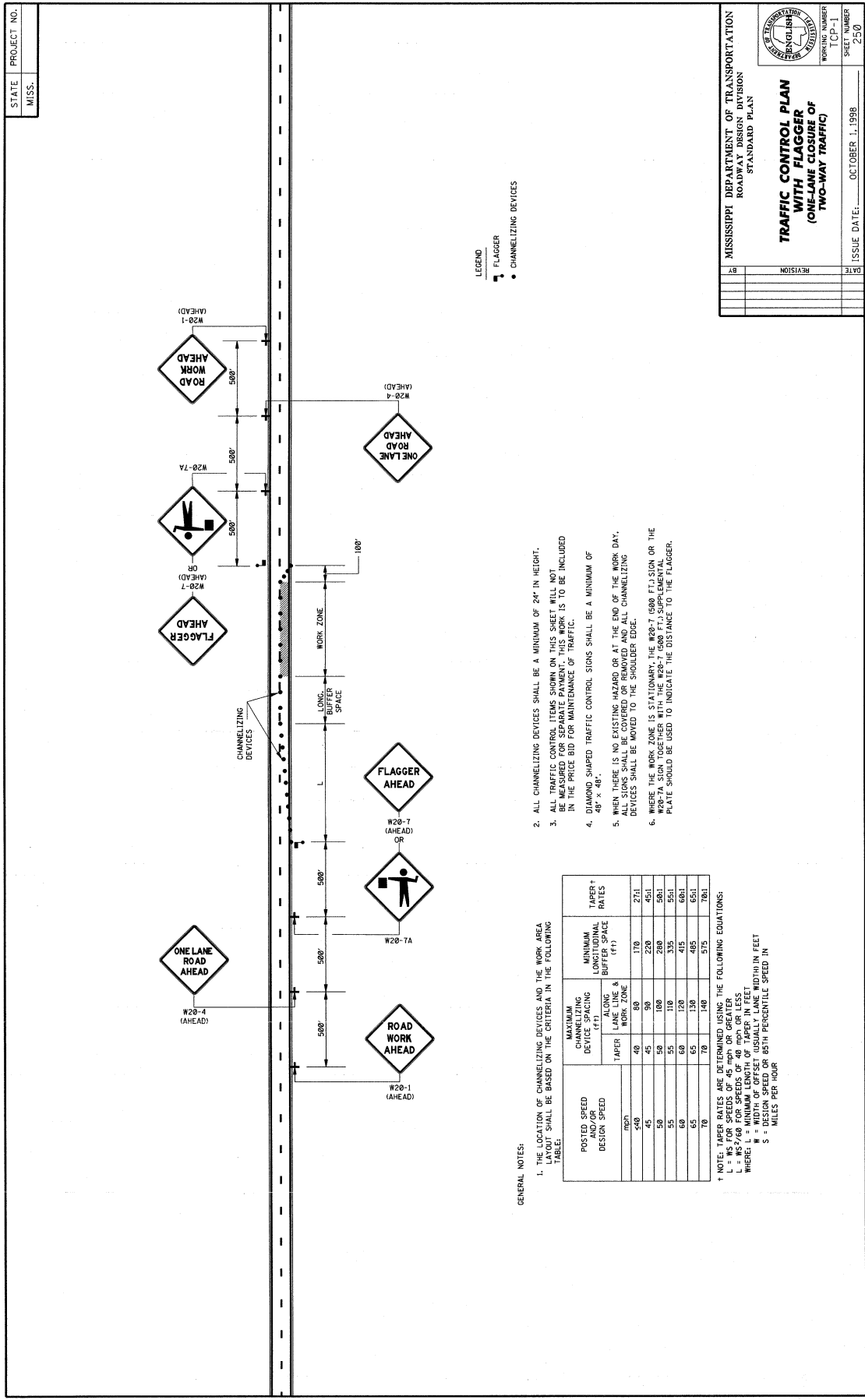
GENERAL NOTES:
 * 1. A PORTION OF THE PAVEMENT MARKING SYMBOL SHOULD BE DIRECTLY OPPOSITE THE ADVANCE WARNING SIGN (W10-1).
 * 2. WIDTH OF R X R SYMBOL MAY VARY ACCORDING TO LANE WIDTH. HOWEVER, ON MULTI-LANE ROADS, THE TRANSVERSE BANDS AND STOP LINE SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.
 * 3. R X R SYMBOL (63.0 ft²), TRANSVERSE BANDS AND STOP LINE SHALL BE PAID FOR AS LEGEND, WHITE (PLASTIC), MATERIAL OPTIONAL FOR OTHER AGENCIES.
 * 4. REFER TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR LOCATION OF PROPOSED WARNING DEVICES AT RAILROAD-HIGHWAY GRADE CROSSINGS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TYPICAL PLACEMENT OF WARNING SIGNS AND PAVEMENT MARKINGS AT RAILROAD HIGHWAY CROSSINGS

WORKSHEET NO. 100
SHEET NUMBER 12 OF 18
ISSUE DATE: OCTOBER 11, 1998

DATE	REVISION
10-18-98	REVISE NOTE

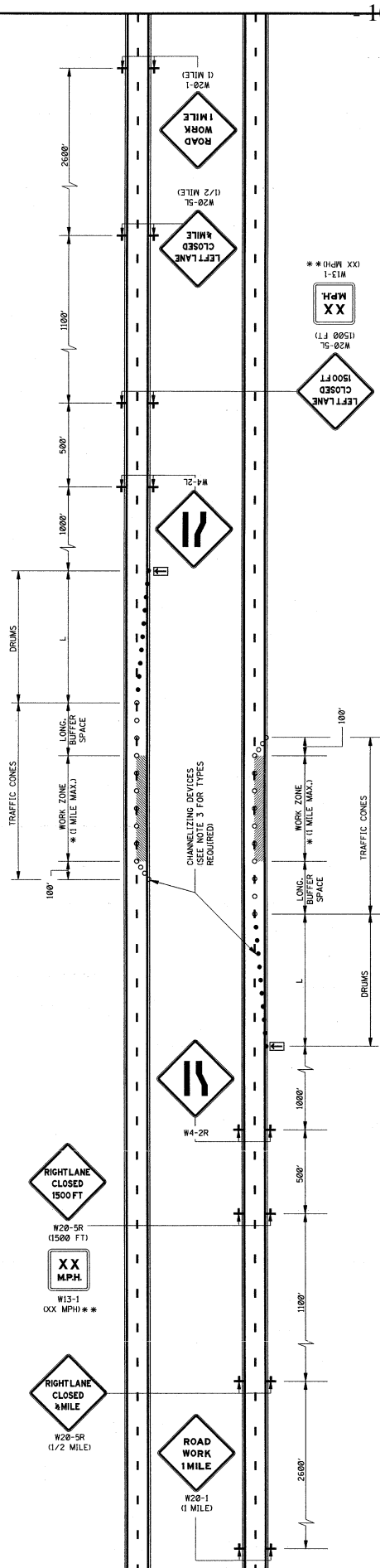


MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN

TRAFFIC CONTROL PLAN WITH FLAGGER (ONE-LANE CLOSURE OF TWO-WAY TRAFFIC)

WORKING NUMBER: TCP-1
 SHEET NUMBER: 250
 ISSUE DATE: OCTOBER 1, 1998

DATE	REVISION	BY



- LEGEND**
- * OR AS SHOWN ELSEWHERE OF THE PLANS.
 - ** THE LEGEND ON W13-1 (XX MPH) SUPPLEMENTAL PLATE SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT.
 - REFLECTORIZED PRE-STANDING PLASTIC DRUMS
 - TRAFFIC CONES (28" HEIGHT)

2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICE TYPES FOR:
 - A. APPROACH TAPER- REFLECTORIZED PLASTIC DRUMS
 - B. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT)
 - C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT)
4. WHEN THERE IS NO EXISTING HAZARD, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE PREVIOUS WORK ZONE.
6. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
7. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".

GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

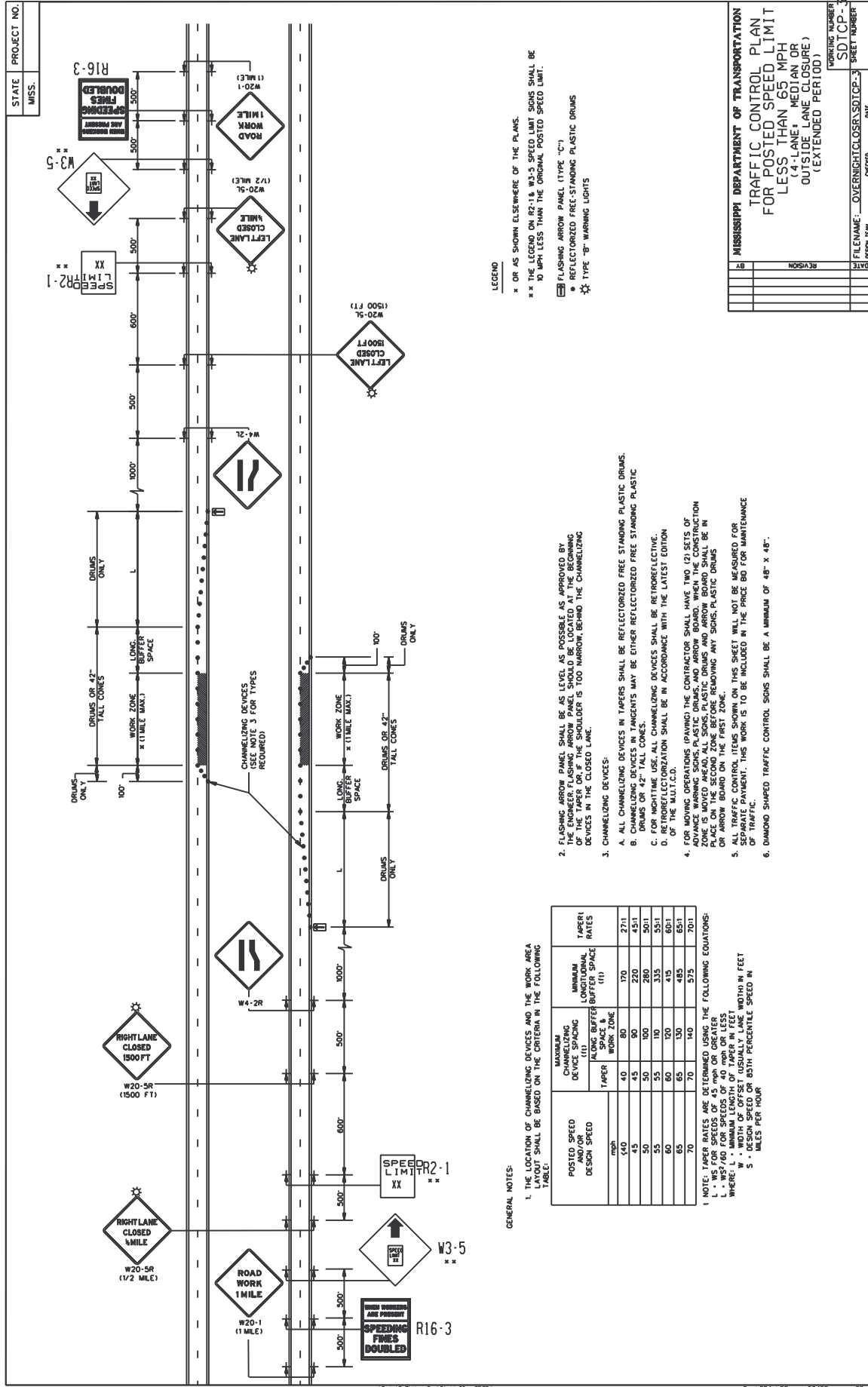
POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (FT)		MINIMUM LONGITUDINAL BUFFER SPACE (ft)	TAPER + TAPER RATES
	ALONG LANE LINE	WORK ZONE		
40	40	80	170	27:1
45	45	90	220	45:1
50	50	100	280	50:1
55	55	110	335	55:1
60	60	120	415	60:1
65	65	130	485	65:1
70	70	140	575	70:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 $L = W^2$ FOR SPEEDS OF 45 MPH OR GREATER
 $L = WS$ FOR SPEEDS OF 45 MPH OR GREATER
 WHERE L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET USUALLY LANE WIDTH IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN

**TRAFFIC CONTROL PLAN
 FOR POSTED SPEED LIMIT
 LESS THAN 65 MPH
 (4-LANE, MEDIAN LANE
 OR CURB AND GUTTER CLOSURE)
 (WORK DAY ONLY)**

WORKING NUMBER: TCP-2
 SHEET NUMBER: 251
 ISSUE DATE: OCTOBER 1, 1998



STATE MISS. PROJECT NO. R16-3

W3-5

XX

TH2-1

LEGEND

- x OR AS SHOWN ELSEWHERE OF THE PLANS.
- ** THE LEGEND ON R2-18 W3-5 SPEED LIMIT SIGNS SHALL BE 10 MPH LESS THAN THE ORIGINAL POSTED SPEED LIMIT.
- ◻ FLASHING ARROW PANEL (TYPE "C")
- ◻ REFLECTORIZED FREE-STANDING PLASTIC DRUMS
- ⊙ TYPE "B" WARNING LIGHTS

2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR AT THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.

3. CHANNELIZING DEVICES:

- A. ALL CHANNELIZING DEVICES IN TAPERS SHALL BE REFLECTORIZED FREE STANDING PLASTIC DRUMS.
 - B. CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER REFLECTORIZED FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.
 - C. FOR NIGHTTIME USE, ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE.
 - D. RETROREFLECTORIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE M.U.T.C.D.
4. FOR MOVING OPERATIONS (TAPERING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF CHANNELIZING DEVICES IN EACH TAPER. CHANNELIZING DEVICES IN THE SECOND ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
5. CHANNELIZING DEVICES AND CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
6. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" x 48".

GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

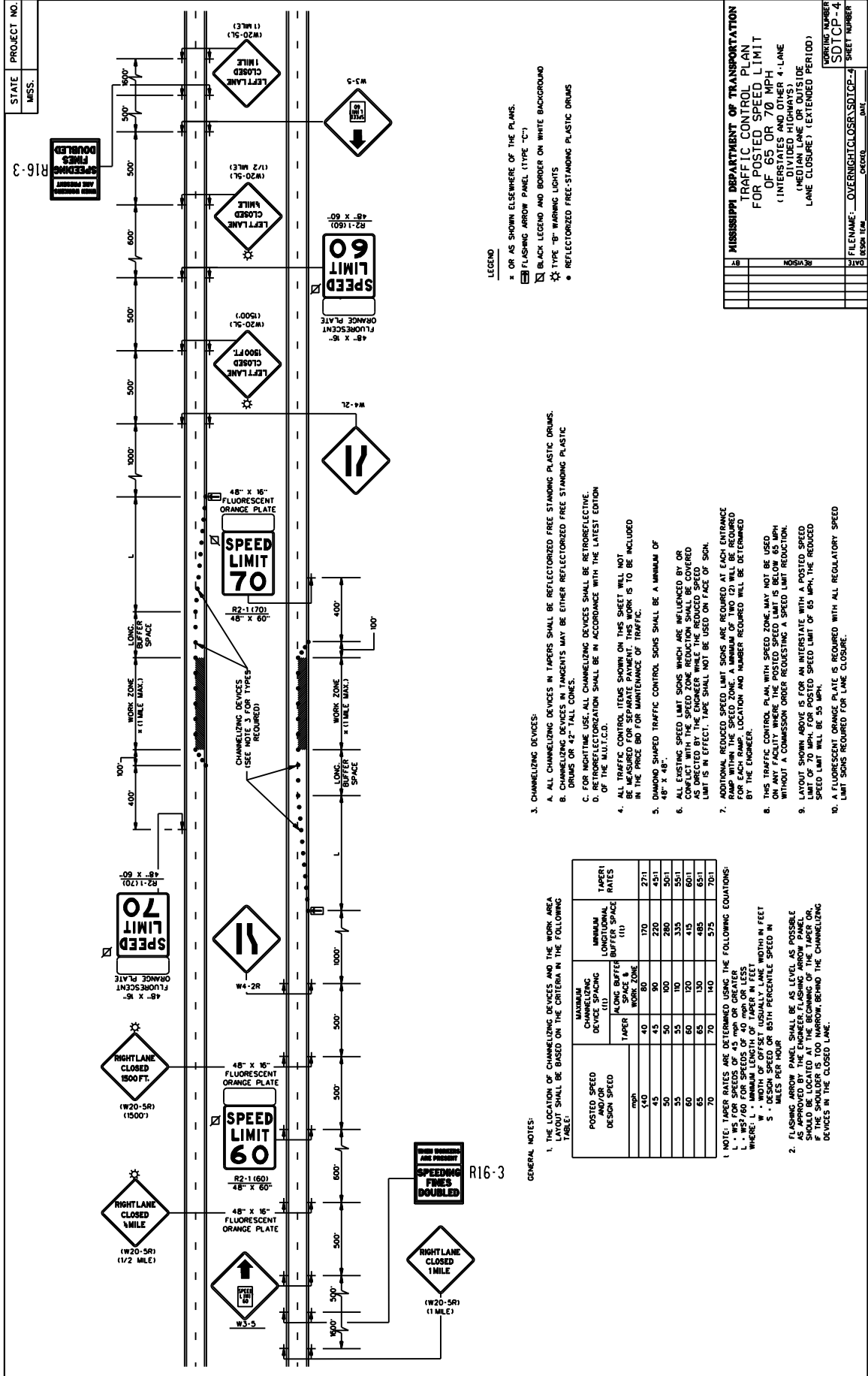
POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICES SPACING ALONG BUFFER SPACE IN TAPER		MINIMUM LONGITUDINAL BUFFER SPACE (L)	TAPER RATES
	(1)	(2)		
40	40	80	170	27:1
45	45	90	220	45:1
50	50	100	280	50:1
55	55	110	335	55:1
60	60	120	415	60:1
65	65	130	485	65:1
70	70	140	575	70:1

1. NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 L = WS FOR SPEEDS OF 45 MPH OR GREATER
 L = WS/40 FOR SPEEDS OF 40 MPH OR LESS
 WHERE:
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 TRAFFIC CONTROL PLAN
 FOR POSTED SPEED LIMIT
 LESS THAN 65 MPH
 (4 LANE MEDIAN OR
 DUAL LANE CLOSURE)
 (EXTENDED PERIOD)

PROJECT NUMBER: SDT CP-3
 SHEET NUMBER: 11

DATE: _____
 REVISION: _____
 DESIGN ENGINEER: _____
 CHECKED: _____



STATE PROJECT NO.
MISS. R16-3

SPEEDING FINES DOUBLED
SEE NOTE 3 FOR TYPES

LEGEND
 * OR AS SHOWN ELSEWHERE OF THE PLANS.
 [Symbol] FLASHING ARROW PANEL (TYPE "C")
 [Symbol] BLACK LEGEND AND BORDER ON WHITE BACKGROUND
 [Symbol] TYPE "B" WARNING LIGHTS
 [Symbol] REFLECTORIZED FREE-STANDING PLASTIC DRUMS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL PLAN
FOR POSTED SPEED LIMIT
OF 65 OR 70 MPH
 (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS)
 (LANE CLOSURE PERIODS)
 (LANE CLOSURE EXTENDED PERIOD)

REPORTING NUMBER: SDTCP-4
 SHEET NUMBER: _____
 DATE: _____
 DESIGNED BY: _____
 CHECKED BY: _____
 FILE NAME: OVERNIGHTCLOSURESDTCP-4

- 3. CHANNELIZING DEVICES:**
- ALL CHANNELIZING DEVICES IN TAPERS SHALL BE REFLECTORIZED FREE STANDING PLASTIC DRUMS.
 - CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER REFLECTORIZED FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.
 - FOR NIGHTTIME USE, ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE.
 - REFLECTORIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL.
- 4. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE INSTALLED UNLESS THE PRICE BID FOR MAINTENANCE OF TRAFFIC:**
- DRAWING SHARED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".
 - ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON FACE OF SIGN.
 - ADDITIONAL REDUCED SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. A MINIMUM OF TWO (2) WILL BE REQUIRED FOR EACH RAMP. LOCATION AND NUMBER REQUIRED WILL BE DETERMINED BY THE ENGINEER.
 - THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE MAY NOT BE USED WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. THE REDUCED SPEED LIMIT WILL BE 55 MPH.
 - A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS REQUIRED FOR LANE CLOSURE.

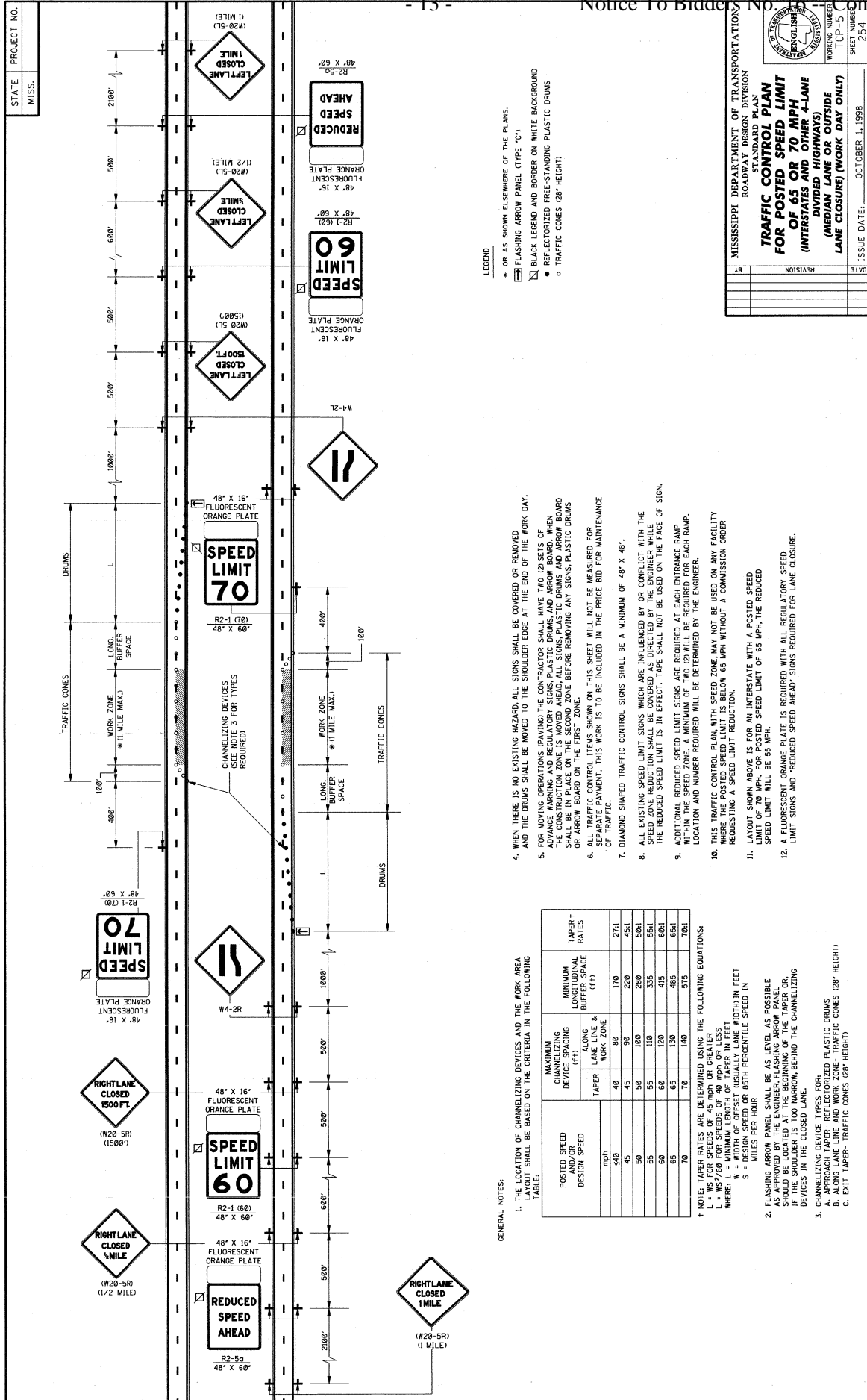
GENERAL NOTES:

- THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICES SPACING ALONG BUFFER SPACE & WORK ZONE	MINIMUM LONGITUDINAL BUFFER SPACE RATES	
		TAPER	TANGENT
70	40	80	170
65	45	80	220
60	55	100	330
55	60	120	410
50	65	130	485
45	70	140	575

1. NOTES: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 L = WS FOR SPEEDS OF 45 MPH OR GREATER
 L = WS²/60 FOR SPEEDS OF 40 MPH OR LESS
 WHERE: L = MINIMUM REQUIRED BUFFER SPACE IN FEET
 W = WIDTH OF POSTED SPEED LIMIT SIGN IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

- FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE. THE PANEL SHALL BE LOCATED IN THE SHOULDER OF THE ROAD OR SHOULD BE PLACED IN THE SHOULDER BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.



GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:
2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE. APPROACH TAPER REFLECTORIZED PLASTIC DRUMS SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICE TYPES FOR:
 - A. APPROACH TAPER- REFLECTORIZED PLASTIC DRUMS
 - B. EXIT TAPER- REFLECTORIZED PLASTIC DRUMS (28" HEIGHT)
 - C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT)
4. WHEN THERE IS NO EXISTING HAZARD, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS PAYING THE CONTRACTOR SHALL HAVE TWO (2) SETS OF CHANNELIZING DEVICES LOCATED AT THE BEGINNING AND END OF THE WORK ZONE. THE CONSTRUCTION ZONE IS MOVED AHEAD ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD OR ARROW BOARD ON THE FIRST ZONE.
6. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
7. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".
8. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON THE FACE OF SIGN.
9. ADDITIONAL REDUCED SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. A MINIMUM OF TWO (2) WILL BE REQUIRED FOR EACH RAMP. LOCATION AND NUMBER REQUIRED WILL BE DETERMINED BY THE ENGINEER.
10. THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
11. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.
12. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS AND "REDUCED SPEED AHEAD" SIGNS REQUIRED FOR LANE CLOSURE.

POSTED SPEED DESIGN SPEED mph	CHANNELIZING DEVICE SPACING (FT)		MINIMUM LONGITUDINAL BUFFER SPACE (FT)	TAPER RATES
	LANE LINE & WORK ZONE	LANE LINE & WORK ZONE		
50	40	80	170	27:1
55	45	90	220	45:1
60	50	100	280	50:1
65	55	110	335	55:1
70	60	120	415	60:1
75	65	130	485	65:1
80	70	140	575	70:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 L = WS FOR SPEEDS OF 45 mph OR GREATER
 L = WS/60 FOR SPEEDS OF 40 mph OR LESS
 WHERE:
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

LEGEND

- OR AS SHOWN ELSEWHERE OF THE PLANS.
- FLASHING ARROW PANEL (TYPE "C")
- BLACK LEGEND AND BORDER ON WHITE BACKGROUND
- REFLECTORIZED FREE-STANDING PLASTIC DRUMS
- TRAFFIC CONES (28" HEIGHT)

STATE PROJECT NO.
MISS.


MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN
**TRAFFIC CONTROL PLAN
FOR POSTED SPEED LIMIT
OF 65 OR 70 MPH
(INTERSTATES AND OTHER 4-LANE
DIVIDED HIGHWAYS)
(MEDIAN LANE OR OUTSIDE
LANE CLOSURE) (WORK DAY ONLY)**

WORKING NUMBER
TCP-5

ISSUE DATE: OCTOBER 1, 1988

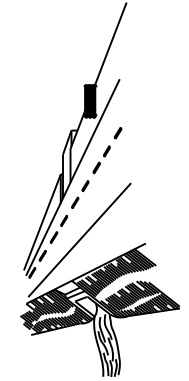
DATE
BY
REVISION

SHEET NUMBER
254



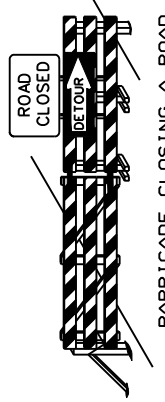
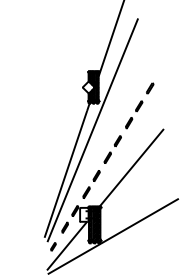
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
HIGHWAY SIGN AND BARRICADE DETAIL FOR CONSTRUCTS

WORKING NUMBER: SDTCP-10
SHEET NUMBER: 10
DATE: 06/21/10
FILE NAME: SDTCP-10_06012012.DGN
PERSON: [blank]
CROSSL: [blank]



WING BARRICADES

- WING BARRICADES ARE TYPE BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A WARNING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
- WING BARRICADES SHOULD BE USED:
 - IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - IN ADVANCE OF ALL BRIDGE OR CULVERT MOVING OPERATIONS.

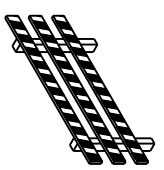


BARRICADE CLOSING A ROAD

BARRICADE CHARACTERISTICS

	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL *	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	35" MIN.	35" MIN.	60" MIN.
NUMBER OF REFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS
TYPE OF FRAME	LIGHT	LIGHT "X" FRAME	POST OR SHD

- * 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- ** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED FACINGS TRAFFIC SHALL HAVE A MINIMUM OF 270 IN OF REFLECTIVE AREA.



TYPE III



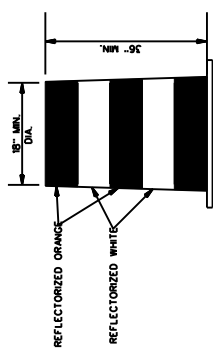
TYPE II



TYPE I

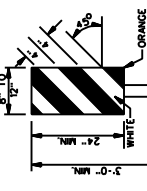
STANDARD BARRICADES

- A TYPE I BARRICADE CONSISTS OF ONE (1) HORIZONTAL RAIL SUPPORTED BY A DEMOUNTABLE POST OR URBAN STREET AND ARTERIALS.
- A TYPE II BARRICADE CONSISTS OF TWO (2) HORIZONTAL RAILS ON A LIGHT "X" FRAME. TYPE II BARRICADES ARE INTENDED FOR USE WHERE THE HAZARD IS LESS SEVERE THAN THAT OF TYPE I BARRICADES.
- TYPE I AND TYPE II BARRICADES ARE INTENDED FOR USE WHERE THE HAZARD IS LESS SEVERE THAN THAT OF TYPE III BARRICADES. TYPE III BARRICADES ARE INTENDED FOR USE ON CONSTRUCTION AND MAINTENANCE PROJECTS AS BARRICADES AND AT ROAD CLOSURES, WHERE THEY MUST REMAIN IN PLACE FOR EXTENDED PERIODS.
- A TYPE III BARRICADE CONSISTS OF THREE (3) HORIZONTAL RAILS SUPPORTED BY FIXED POSTS, A ROAD SHD, A HEAVY DEMOUNTABLE FRAME OR A HEAVY WINGED "X" FRAME.
- TYPE I BARRICADES ARE INTENDED FOR USE ON CONSTRUCTION AND MAINTENANCE PROJECTS AS BARRICADES AND AT ROAD CLOSURES, WHERE THEY MUST REMAIN IN PLACE FOR EXTENDED PERIODS.
- THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).
- DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
- FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.



PLASTIC DRUM STRIPING DETAIL

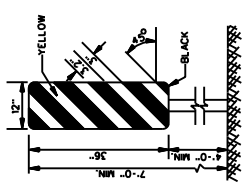
- PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH MUTCD STANDARDS FOR BARRICADES. THE PREDOMINANT COLOR ON DRUMS SHALL BE ORANGE. STRIPES SHALL BE GREEN, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
- DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
- WHERE PRACTICAL PLASTIC DRUMS SHALL BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.



VERTICAL PANEL

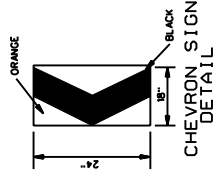
- VERTICAL PANELS CONSIST OF AT LEAST ONE PANEL 8" TO 12" IN WIDTH AND A MINIMUM OF 24" IN HEIGHT.
- THE DIAGONAL STRIPES SHALL SLOPE DOWNWARD IN THE DIRECTION THAT TRAFFIC IS TO PASS THE PANEL. THE MINIMUM OF 35" ABOVE THE ROADWAY ON A SINGLE LIGHTMASS POST.
- VERTICAL PANELS USED ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH-SPEED ROADWAYS SHALL HAVE A MINIMUM OF 270 IN OF RETROREFLECTIVE AREA FACING TRAFFIC.
- FOR TWO-WAY TRAFFIC OPERATIONS, BACK-TO-BACK PANELS SHALL BE USED.

GENERAL NOTES:
1. MARKINGS ON ALL DEVICES SHOWN ON THIS SHEET SHALL BE HIGH INTENSITY REFLECTIVE SHEETING.
2. THE TRAFFIC CONTROL PLAN WILL LIST THE VARIOUS TRAFFIC CONTROL DEVICES REQUIRED FOR EACH PROJECT.



TYPE 3 OBJECT MARKER (OM-3R)

- TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
- THE OM-3R IS SHOWN. THE OM-3R IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
- THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



CHEVRON SIGN DETAIL

- A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
- THE CHEVRON SIGN SHALL BE MOUNTED ON FIXED POST OR ROAD SHD.
- CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHALL BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.

STATE MISS.	PROJECT NO.	
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MOBILE OPERATIONS ON MULTILANE ROAD

MOBILE OPERATIONS ON MULTILANE ROAD

NOTES:

- VEHICLES USED FOR THESE OPERATIONS SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLAOS, SIGNS, OR ARROW PANELS.
- PROTECTION VEHICLE #1 SHOULD BE EQUIPPED WITH AN ARROW PANEL. AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON PROTECTION VEHICLE #1 SO AS NOT TO OBSCURE THE ARROW PANEL.
- PROTECTION VEHICLE #2 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK-MOUNTED ATTENUATOR (TMA).
- PROTECTION VEHICLE #1 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
- WHEN ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, PROTECTION VEHICLE #1 SHOULD BE ELIMINATED.
- ON HIGH-SPEED ROADWAYS, A THIRD PROTECTION VEHICLE SHOULD BE USED (I.e., VEHICLE #1 ON THE SHOULDER (IF PRACTICAL), VEHICLE #2 IN THE CLOSED LANE, AND VEHICLE #3 IN THE CLOSED LANE).
- ARROW PANELS SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
- WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MOBILE OPERATIONS ON TWO-LANE ROAD

MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES:

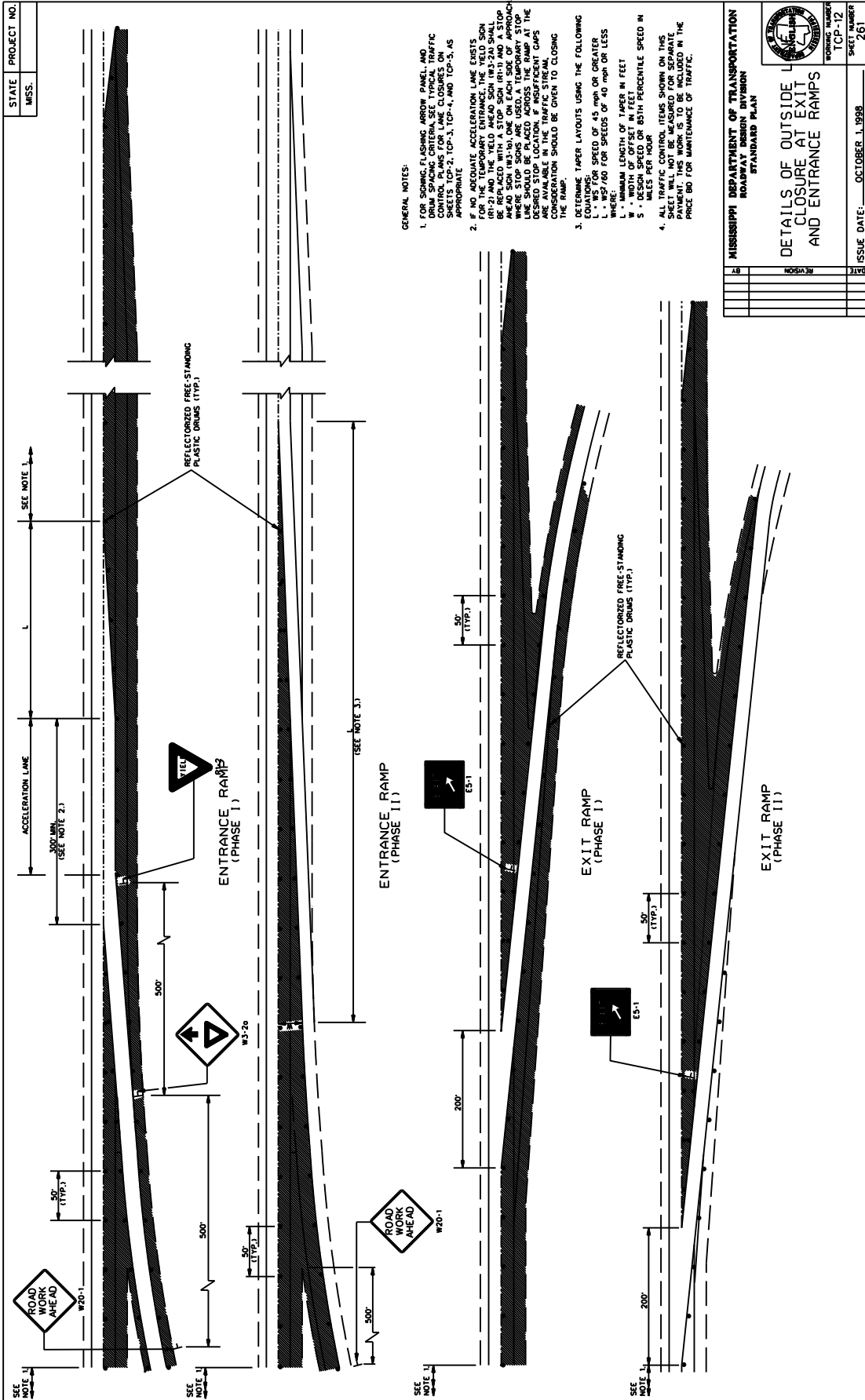
- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND PROTECTION VEHICLES SHOULD BE OPERATED IN THE CENTER TURN LANE. IF THIS CANNOT BE DONE FREQUENTLY, AS AN ALTERNATIVE A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND PROTECTION VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. PROTECTION VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE WORK, PROTECTION VEHICLES SHOULD BE ELIMINATED. PROTECTION VEHICLES SHOULD BE SLOWED AS NEAR AS PRACTICALLY POSSIBLE AS THEY APPROACH THE WORK. SPEEDS AS LOW AS 15 MPH SHOULD BE MAINTAINED. THE PROTECTION VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL PROTECTION VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING WORK OPERATIONS SHOULD BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE PROTECTION VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE PROTECTION VEHICLE SHALL BE EQUIPPED WITH FLASHING BEACONS. THE SIGN PROTECTION AND WORK VEHICLES SHOULD DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

TRAFFIC CONTROL PLAN
MOBILE OPERATIONS
MULTILANE ROADS
AND
TWO-LANE ROADS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN

DATE	BY	REVISION
01-99	REVISOR	1

ISSUE DATE: OCTOBER 1, 1998
 SHEET NUMBER: 260
 PROJECT NUMBER: CP-11



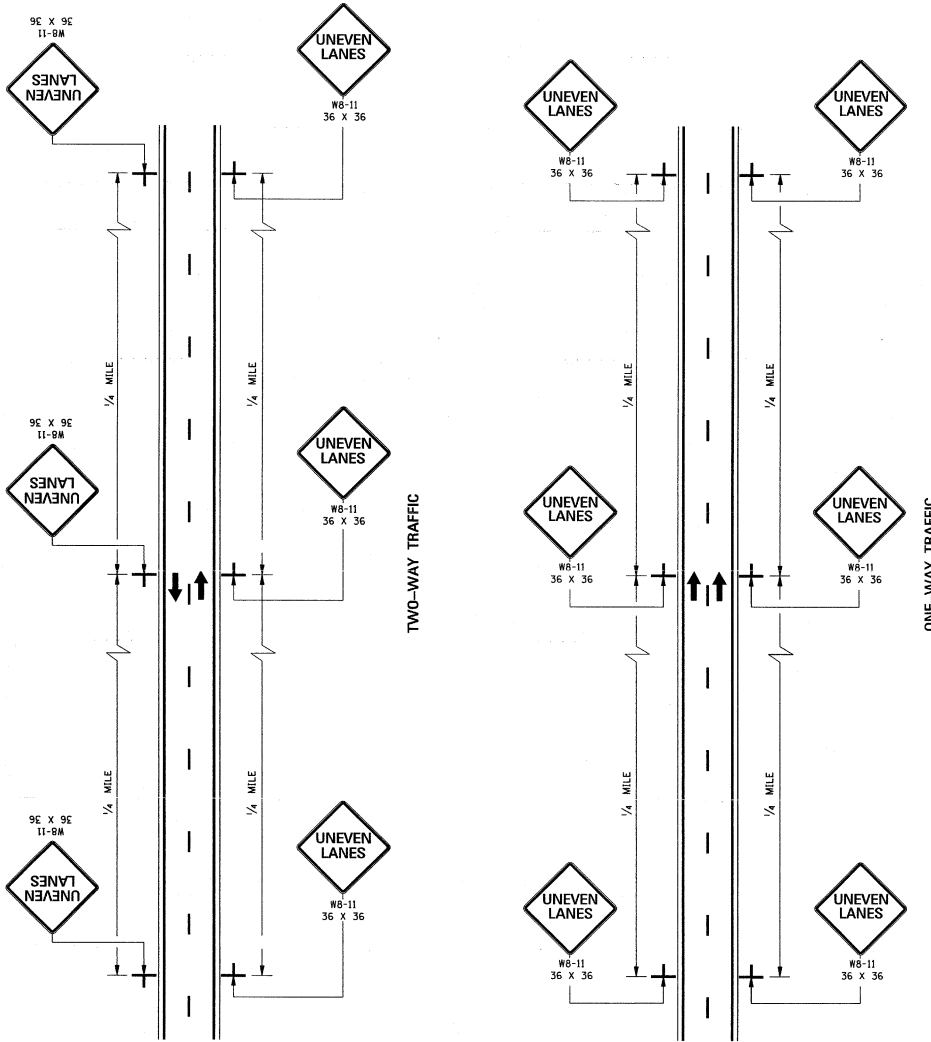
GENERAL NOTES:

- FOR SIGNING, FLASHING ARROW PANEL AND TRAFFIC CONTROL PLANS FOR LANE CLOSURES ON SHEETS TCP-2, TCP-3, TCP-4, AND TCP-5, AS APPROPRIATE.
- IF NO ADEQUATE ACCELERATION LANE EXISTS BEYOND THE RAMP, THE RAMP SHOULD BE REPLACED WITH A STOP SIGN (R11) AND A STOP AHEAD SIGN (W3-10), ONE ON EACH SIDE OF APPROACH. THE STOP SIGN SHOULD BE PLACED AT THE DESIRED STOP LOCATION, IF INSUFFICIENT GAPS EXIST. THE STOP SIGN SHOULD BE PLACED AT THE DESIRED STOP LOCATION, IF INSUFFICIENT GAPS EXIST. THE STOP SIGN SHOULD BE PLACED AT THE DESIRED STOP LOCATION, IF INSUFFICIENT GAPS EXIST.
- DETERMINE TAPER LAYOUTS USING THE FOLLOWING EQUATIONS:
 - $L = 1.47 S V$ FOR SPEEDS OF 40 MPH OR GREATER
 - $L = W S^2 / 100$ FOR SPEEDS OF 40 MPH OR LESS
 WHERE:
 - L = MINIMUM LENGTH OF TAPER IN FEET
 - S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
 - W = TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET
 - V = NUMBER OF VEHICLES PER HOUR TO BE STOPPED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY MAINTENANCE	
STANDARD PLAN	
WORKING NUMBER	TCP-12
SHEET NUMBER	261
ISSUE DATE:	OCTOBER 1, 1998

DETAILS OF OUTSIDE LANE CLOSURE AT EXIT AND ENTRANCE RAMP

STATE	PROJECT NO.
MISS.	

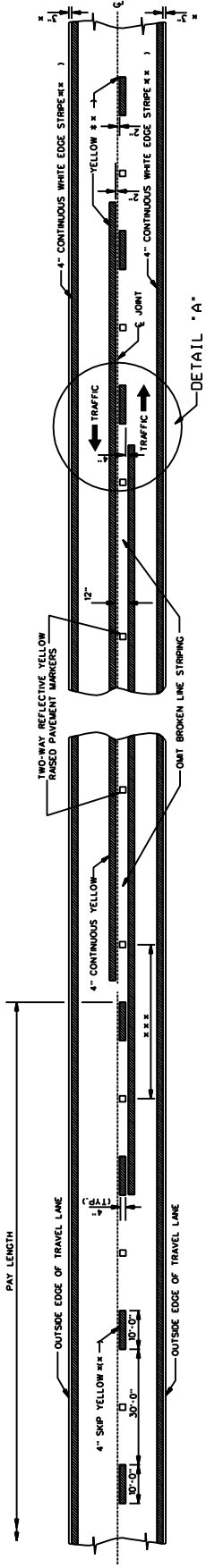


GENERAL NOTES:

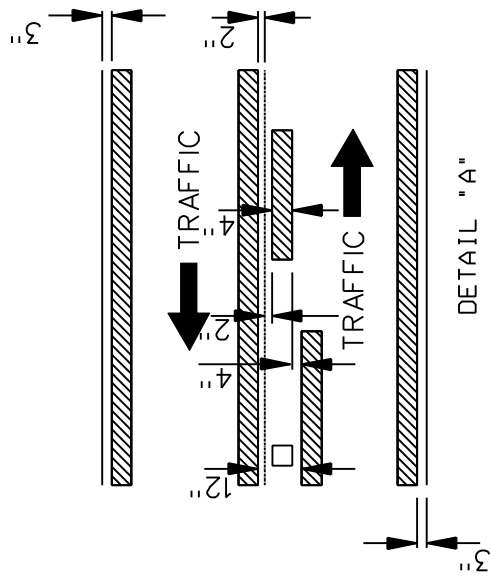
1. UNEVEN LANE LINE SPACING SHALL BE EQUAL TO 1/2" NO SIGNS REQUIRED.
 2. IF GREATER THAN 1/2" AND LESS THAN OR EQUAL TO 2/4" PLACE SIGNS AS SHOWN ON THIS SHEET.
 3. IF GREATER THAN 2/4" TRAFFIC SHOULD NOT BE ALLOWED TO CROSS UNEVEN LANE LINE.
2. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.
 3. THE W8-11 SIGNS SHALL BE SPACED AT 1/4-MILE INTERVALS THROUGHOUT UNEVEN LANE LINE LIMITS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY DESIGN DIVISION	
STANDARD PLAN	
TRAFFIC CONTROL PLANS	
UNEVEN PAVEMENT	
DETAILS	
WORKING NUMBER	CP-14
SHEET NUMBER	263
DATE	OCTOBER 1, 1998
ISSUE DATE:	
BY	REVISION

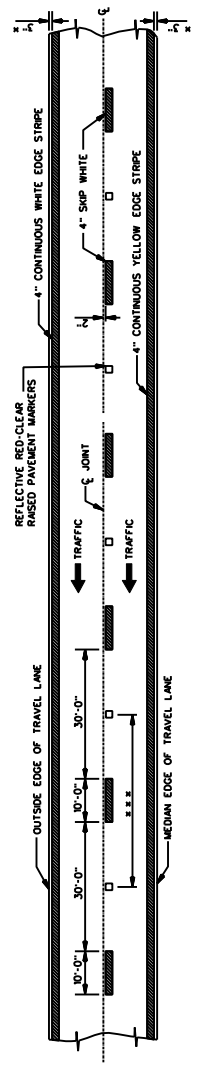
STATE	PROJECT NO.
MISS.	



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



DETAIL "A"



4-LANE WITH ONE-WAY TRAFFIC



GENERAL NOTES:
 * 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
 ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
 *** 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	URBAN AREA (U-L-N)	RURAL AREA (U-L-N)
LANE LINES	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	140'-0"

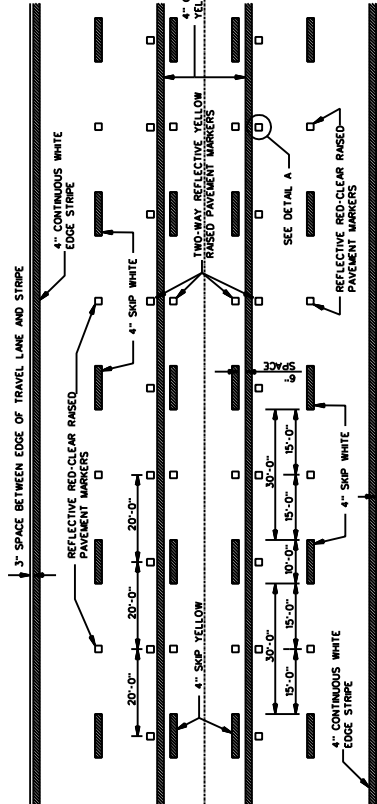
1. NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS SHALL BE USED THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 2. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE MATERIALS AS SPECIFIED IN THE MOOT.
 3. APPROVED SOURCES OF MATERIALS.
 4. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKERS ARE TO REMAIN IN PLACE OVER 3 MONTHS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN

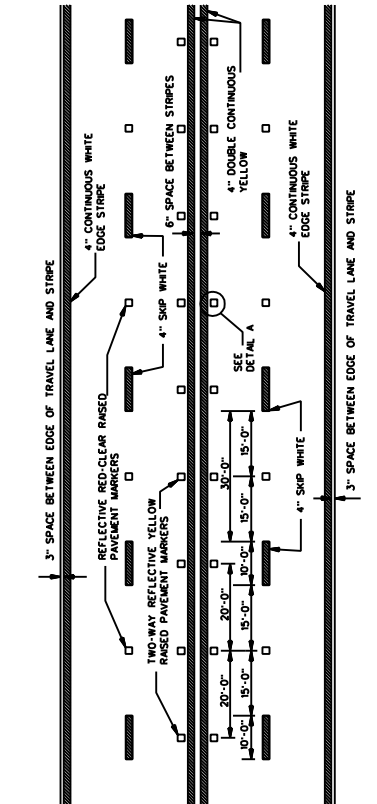
TEMPORARY STRIPING FOR
 TRAFFIC CONTROL
 2-LANE AND 4-LANE
 DIVIDED HIGHWAYS

WORKING NUMBER: TCP-15
 SHEET NUMBER: 264

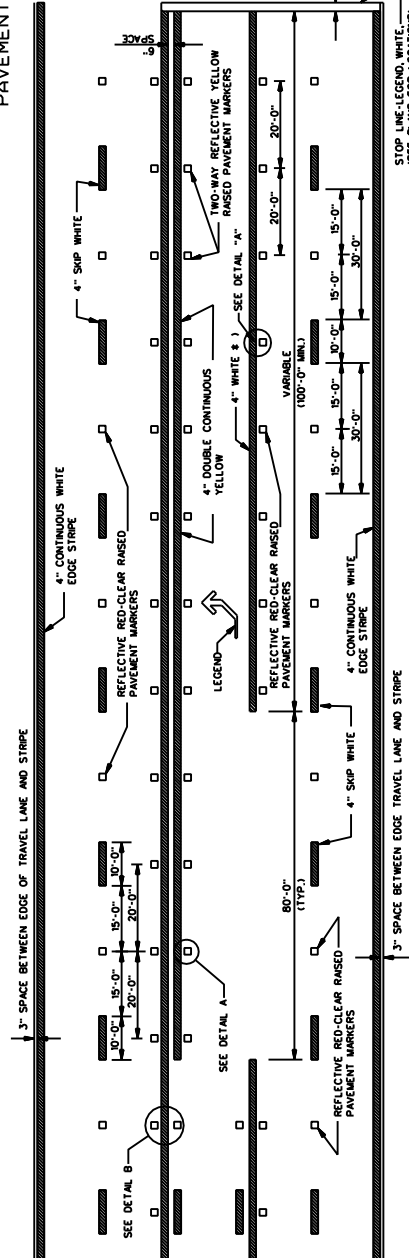
ISSUE DATE: DECEMBER 1, 1999



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 5-LANE SECTION

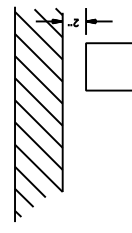


TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION



TYPICAL TWO-WAY ARROW INSTALLATION

- NOTES: 1. CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY.
- 2. IF SEGMENT IS LESS THAN 350', PLACE ONE SET OF ARROWS IN CENTER OF SEGMENT.
- 3. IF SEGMENT IS GREATER THAN 350', PLACE FIRST SET OF ARROWS 50 TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS (250 O.C.).

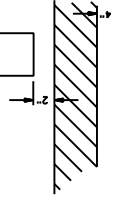


DETAIL A
LATERAL PLACEMENT OF PAVEMENT MARKERS

GENERAL NOTE:

- 1. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MOST APPROVED SOURCE OF MATERIALS.
- 2. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKERS ARE TO REMAIN IN PLACE OVER 3 MONTHS.
- 3. TEMPORARY TURN ARROW TO BE PAID FOR AS TEMPORARY TRAFFIC STRIPE (LEGEND), ESTIMATED AT 10.9 SQ. FT. PER ARROW.

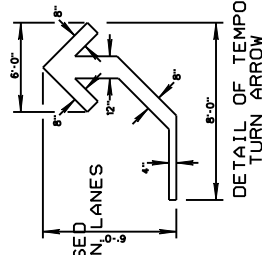
DETAIL B
LATERAL PLACEMENT OF PAVEMENT MARKERS



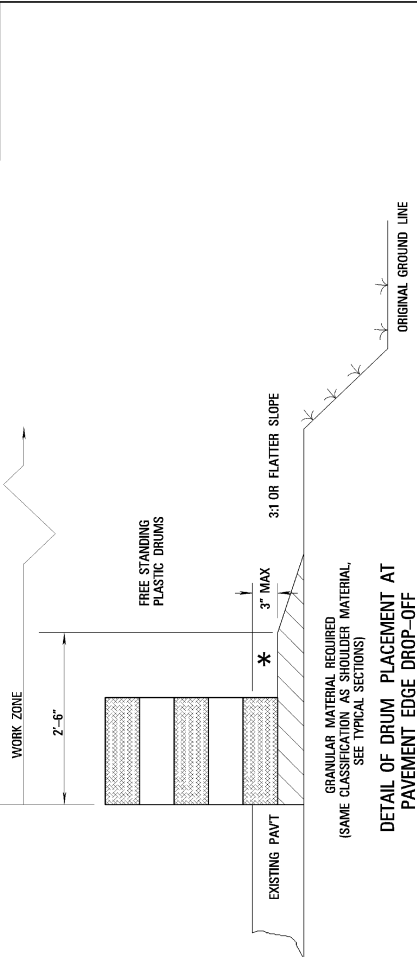
TYPICAL STRIPING AND RAISED PAVEMENT MARKERS AT LEFT TURN LANES

* NOTE: USE DETAIL STRIPING IF LENGTH \leq 150' AT THIS LOCATION, OTHERWISE USE CONTINUOUS STRIPING.

DETAIL OF TEMPORARY TURN ARROW



MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
AS	REVISION
BS	REVISION
CS	REVISION
DS	REVISION
ES	REVISION
FS	REVISION
GS	REVISION
HS	REVISION
IS	REVISION
JS	REVISION
KS	REVISION
LS	REVISION
MS	REVISION
NS	REVISION
OS	REVISION
PS	REVISION
QS	REVISION
RS	REVISION
TS	REVISION
US	REVISION
VS	REVISION
WS	REVISION
XS	REVISION
YS	REVISION
ZS	REVISION
WORKING NUMBER	TC-16
SHEET NUMBER	265
ISSUE DATE:	DECEMBER 1, 1999



DETAIL OF DRUM PLACEMENT AT PAVEMENT EDGE DROP-OFF

- NOTES**
- * A. PAVEMENT EDGE DROP-OFF
 1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-3) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750 ± O.C.).
 2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. COMES MAY BE USED IN PLACE OF DRUMS, PANELS AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE M.U.T.C.D. (1/3 L, WHERE L IS THE TAPER LENGTH IN FEET)
 3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 3:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS/PANELS OR BARRICADES MAY BE USED. IF CONCRETE BARRIERS ARE USED, SPECIAL REFLECTIVE DEVICES OR STEADY BURN LIGHTS SHOULD BE USED FOR OVERNIGHT INSTALLATIONS.
 4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
 5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

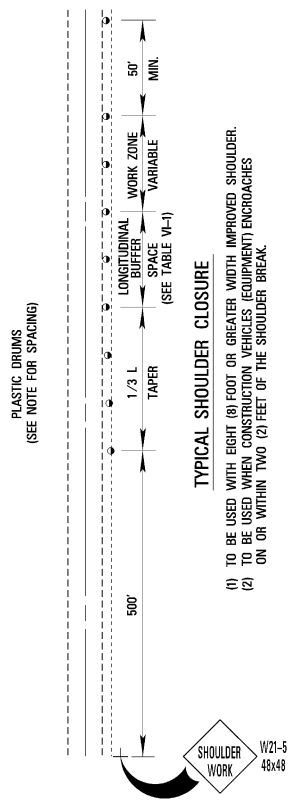
- B. DRUM SPACING**
1. TANGENTS = $2 \times S$
 2. $APPROX = \frac{L}{S} \times W$
 - WHERE L = TAPER LENGTH IN FEET
 - S = SPEED IN MPH (POSTED OR 85 PERCENTILE)
 - W = WIDTH OF OFFSET IN FEET

C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

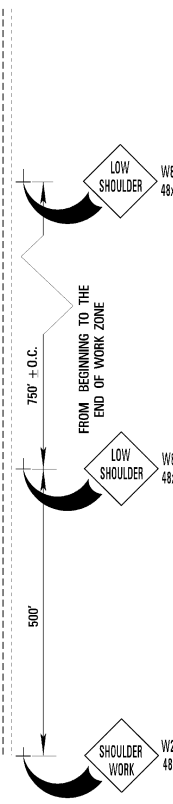
TABLE V-1 GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

SPEED (MPH)	MINIMUM	LENGTH (FEET)
20	35	35
25	45	45
30	55	55
35	65	65
40	75	75
45	85	85
50	95	95
55	105	105
60	115	115
65	125	125
70	135	135
75	145	145
80	155	155
85	165	165

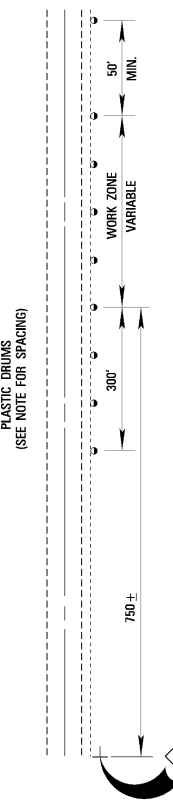
* POSTED SPEED OFF PEAK 85 PERCENTILE SPEED PRIOR TO WORK STARTING OR THE ANTICIPATED GRINDING SPEED IN MPH



TYPICAL SHOULDER CLOSURE



TYPICAL SHOULDER WORK #1



TYPICAL SHOULDER WORK #2

NOTE: WORK OUTSIDE THE (2) FOOT LIMIT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA. OR SEE NOTE A-3 THIS SHEET.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL DETAILS
DRUM PLACEMENT
AND
SHOULDER CLOSURE

PROJECT NO.: _____
COUNTY: _____
FILE NAME: _____
PERSON: _____
DATE: 12-28-93

WORKING NUMBER: _____
TCP NO.: _____
SHEET NUMBER: _____

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 96

CODE: (SP)

DATE: 4/6/2017

SUBJECT: Contract Time

PROJECT: MP-6000-21(257) / 306148301 & 302 – Greene County

The completion of work to be performed by the Contractor for this project will not be a specified date but shall be when all allowable working days are assessed, or any extension thereto as provided in Subsection 108.06. It is anticipated that the Notice of Award will be issued no later than **June 13, 2017**.

The Contractor shall request a Notice to Proceed / Beginning of Contract Time date between the date of the **Execution of Contract and September 7, 2017**.

Should the Contractor not request a Notice to Proceed by **September 7, 2017**, the date for the Notice to Proceed and Beginning of Contract Time will be **September 7, 2017**.

33 Working Days have been allowed for the completion of work on this project.

The progress schedule for this project shows the Notice to Proceed and Beginning of Contract Time starting at the latest possible date. If the Contractor requests a Notice to Proceed earlier than this date, the Contractor shall submit a revised progress schedule showing the work beginning at the new Notice to Proceed and Beginning of Contract Time date.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 97

CODE: (SP)

DATE: 03/22/2017

SUBJECT: Scope of Work

PROJECT: MP-6000-21(257) 306148301 & 302 - Greene County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings". All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following:

OVERLAY STATE ROUTE 57
FROM THE SOUTH END OF CHICKASAWHAY RIVER BRIDGE
TO SR 594, AND SR 594 TO THE ALABAMA LINE

(A) Prior to the overlay, centerline alignment shall be determined by the contractor by measuring the existing roadway at 500-foot intervals in tangent sections, and 100-foot intervals in horizontal curves.

(B) Cold mill 100-foot transitions at the B.O.P. and E.O.P., and bridge ends and along the existing curb and gutter sections (parallel to roadway centerline) at the SR 57 and the SR 57 / 63 overpass areas to provide a smooth transition and any other areas designated by the Project Engineer to ensure smooth transition of new overlay with existing grade. (See Typical Drawings).

(C) The below listed areas have been identified for base repair. They are located as follows:

SR 57 & 63

<u>Location</u>	<u>Lane</u>	<u>Size (feet)</u>	<u>Area (sf)</u>
Station 11±40	Right	55 x 11	605
Station 22+30	Right	35 x 11	385

SR 594

<u>Location</u>	<u>Lane</u>	<u>Size (feet)</u>	<u>Area (sf)</u>
Station 241±70	Left	145 x 11	1,595
Station 277+80	Left	80 x 11	880
Station 283±50	Left	200 x 11	2,200
Station 284±50	Right	65 x 11	715
Station 327±10	Left	55 x 11	605
Station 339±35	Left	201 x 11	2,310
Station 341±10	Right	75 x 11	825
Station 405±08	Left	200 x 11	2,200

These failed areas shall be repaired by full depth removal and backfilling with 19-mm, MT, asphalt. Approximately 508 tons of 19-mm, MT, asphalt will be used for backfill in all removal sections. Additional areas may be added as required by the Project Engineer.

(D) Prior to placement of the asphalt, all raised pavement markers throughout the project shall be removed (cost absorbed). Overlay SR 57/63 & SR 594 with ¾" of Ultra-Thin asphalt. Prior to the overlay, a leveling course of ¾" & variable of 9.5-mm, MT, asphalt will be required. 5,000 tons have been added for this leveling lift. Publicly maintained roads or streets shall be surfaced to the existing R.O.W. only if they have an existing asphalt surface; privately owned entrances shall be surfaced a distance of 10 feet and variable from edge of pavement. Any site grading at local roads or drives will not be measured for separate payment but will be considered an absorbed item.

(E) Smoothness incentive/disincentive will be governed by special provision for MRI (mean roughness index). All incentive/disincentive will be based on theoretical tonnage placed on a 12-foot travel lane.

(F) Temporary and permanent striping shall be placed where existing stripes are located, and shall conform to finished stripe specifications for alignment, neatness, reflectivity, and straightness. When the centerline or turn lane stripe is removed during milling operations, temporary stripe shall be placed prior to opening the roadway to traffic. All permanent pavement markings on asphalt are to be hot thermoplastic. Edge lines will be placed so as to maintain the original lane width.

(G) Raised pavement markers will be placed at 80-foot intervals in tangents and 40-foot intervals in curves along the centerline of roadway. Only flexible adhesive shall be allowed for placement of raised pavement markers meeting the requirements of Subsection 720.03.7.7.

(I) Type 3 object markers are to be placed with two (2) markers per posts at pipe headwalls and one marker per post (two (2) posts each side of roadway) at box headwalls. Object markers will be paid under 630-G pay items as applicable

(J) The existing shoulders shall be raised to match the new pavement with 825B Crushed Stone Base. Placement of the shoulder material(s) on the finished surface course shall not be permitted. The material(s) shall be bladed, rolled, and compacted to a finished slope of 4% where practical. Shoulders with existing adequate shoulder material in place shall be bladed to a slope of 4%, cost of which shall be included in the prices of other items bid.

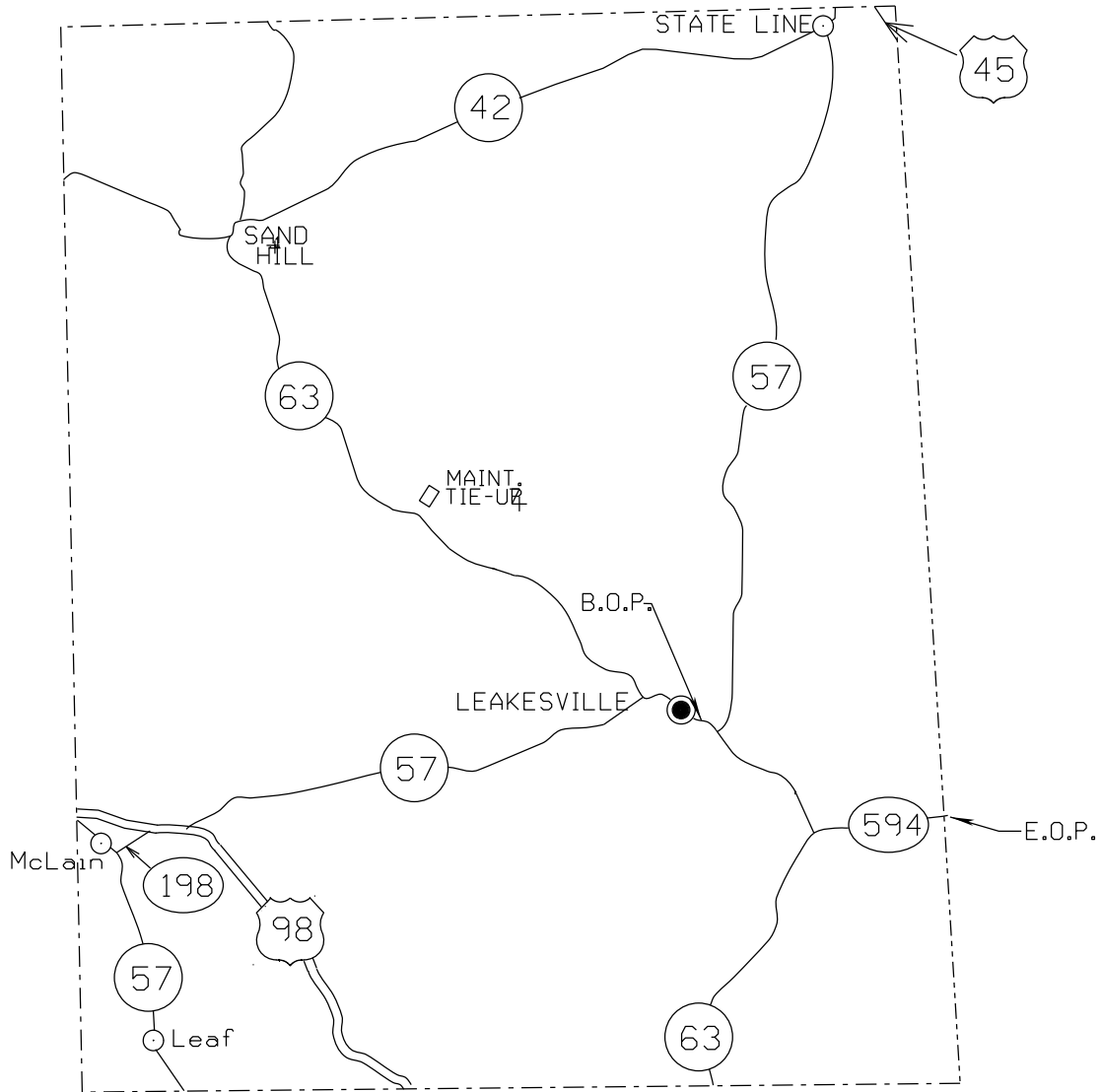
(K) It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged during the life of the contract. No payment will be made for the replacement or repair of damaged items.

(L) Incidental work such as removing vegetation, shaping and compaction of shoulder, removing excess asphalt material, project clean-up, and other incidental work necessary to complete the

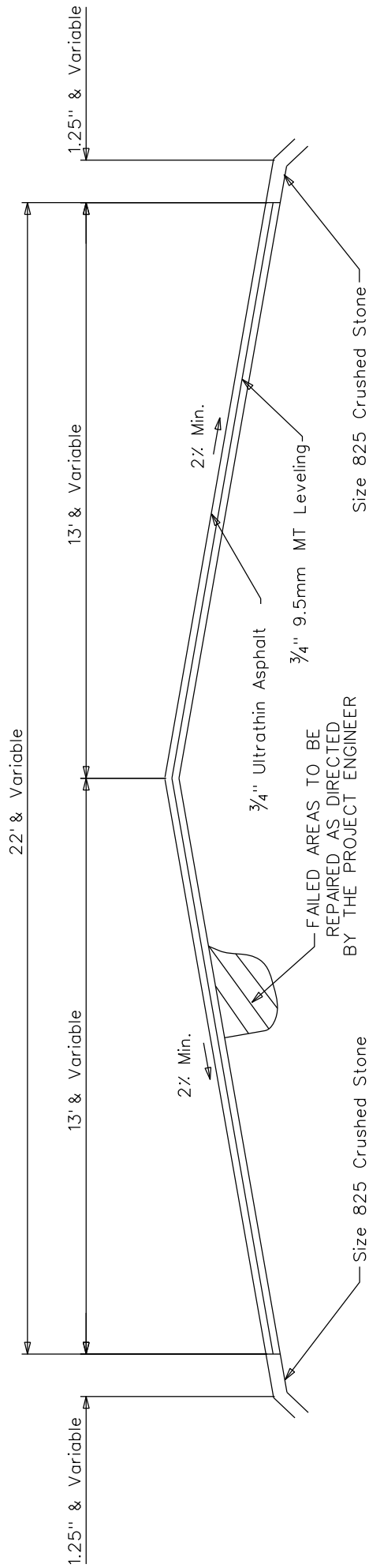
project will not be measured for separate payment, but will be included in other bid items.

(M) If pavement section marking tape is encountered on this project, it shall be located prior to overlaying and placed back in same location after paving operations have ceased. The section marking shall be 8-inch high performance cold plastic detail stripe and shall be four feet (4') in length. The marking shall be centered across the centerline stripe. The cost of this item shall be absorbed in other bid items.

(N) All construction signing as well as plastic traffic drums and barricades are included in the bid for Pay Item 618-A, Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except for R4- 1 and R4-2 signs which shall be black legend and border on white background. The Contractor shall erect and maintain construction signing, and provide all signs and traffic handling devices in accordance with Manual Uniform Traffic Control Devices (MUTCD).



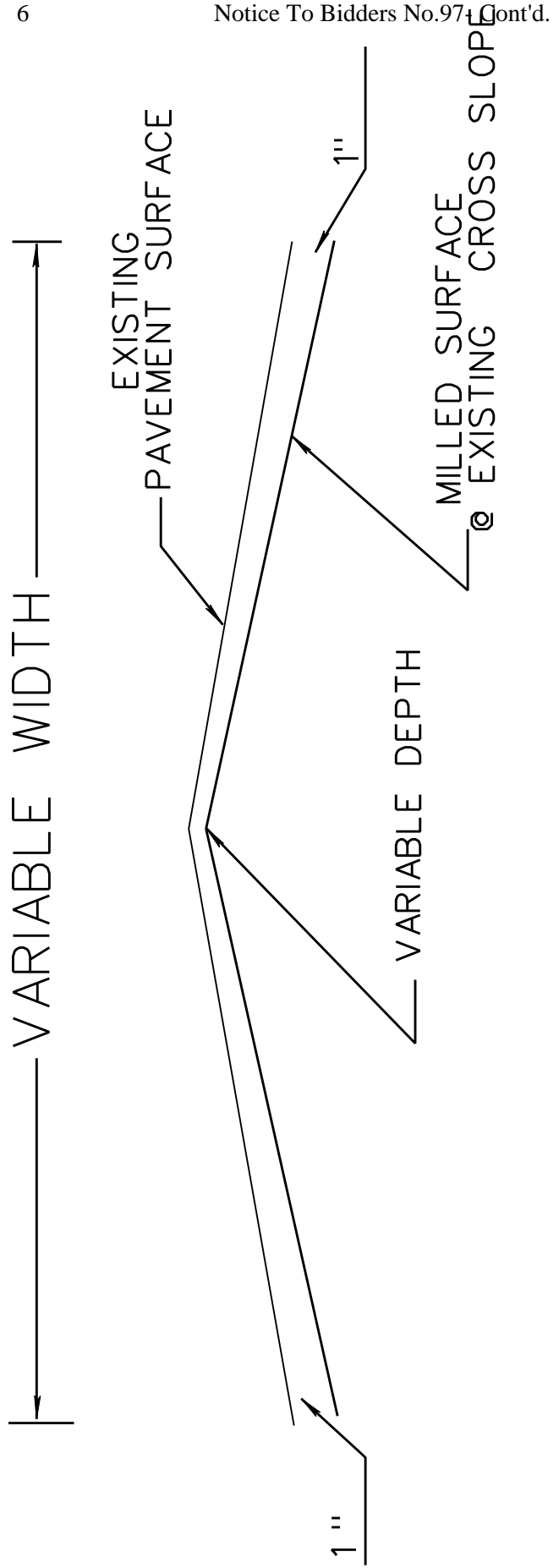
THE ABOVE REFERENCED PROJECT IS FOR LEVELING AND OVERLAYING SR 63 FROM THE EAST END OF THE CHICKASAWHAY RIVER BRIDGE SOUTH TO SR 594, AND LEVELING AND OVERLAYING SR 594 EAST TO THE ALABAMA LINE



NOTES:

- (1) THE EXISTING PAVEMENT EDGE SHALL BE CUT TO A SMOOTH AND NEAR VERTICAL FACE WITH AN APPROVED CUTTING DEVICE. (NOT MEASURED FOR SEPARATE PAY.)
- (2) TRUE CENTERLINE PAVEMENT ALIGNMENT SHALL BE DETERMINED BY THE CONTRACTOR BY MEASURING THE EXISTING ROADWAY AT 500ft. INTERVALS IN TANGENT SECTIONS, AND 100ft. INTERVALS IN HORIZONTAL CURVES.

TYPICAL MILLING DIAGRAM



TYPICAL MILLED TRANSITION AT BRIDGE ABUT. OR PAVEMENT IN PLACE

1" & VAR. COLD MILLING
REQ'D AT BRIDGE DECK & ENDS

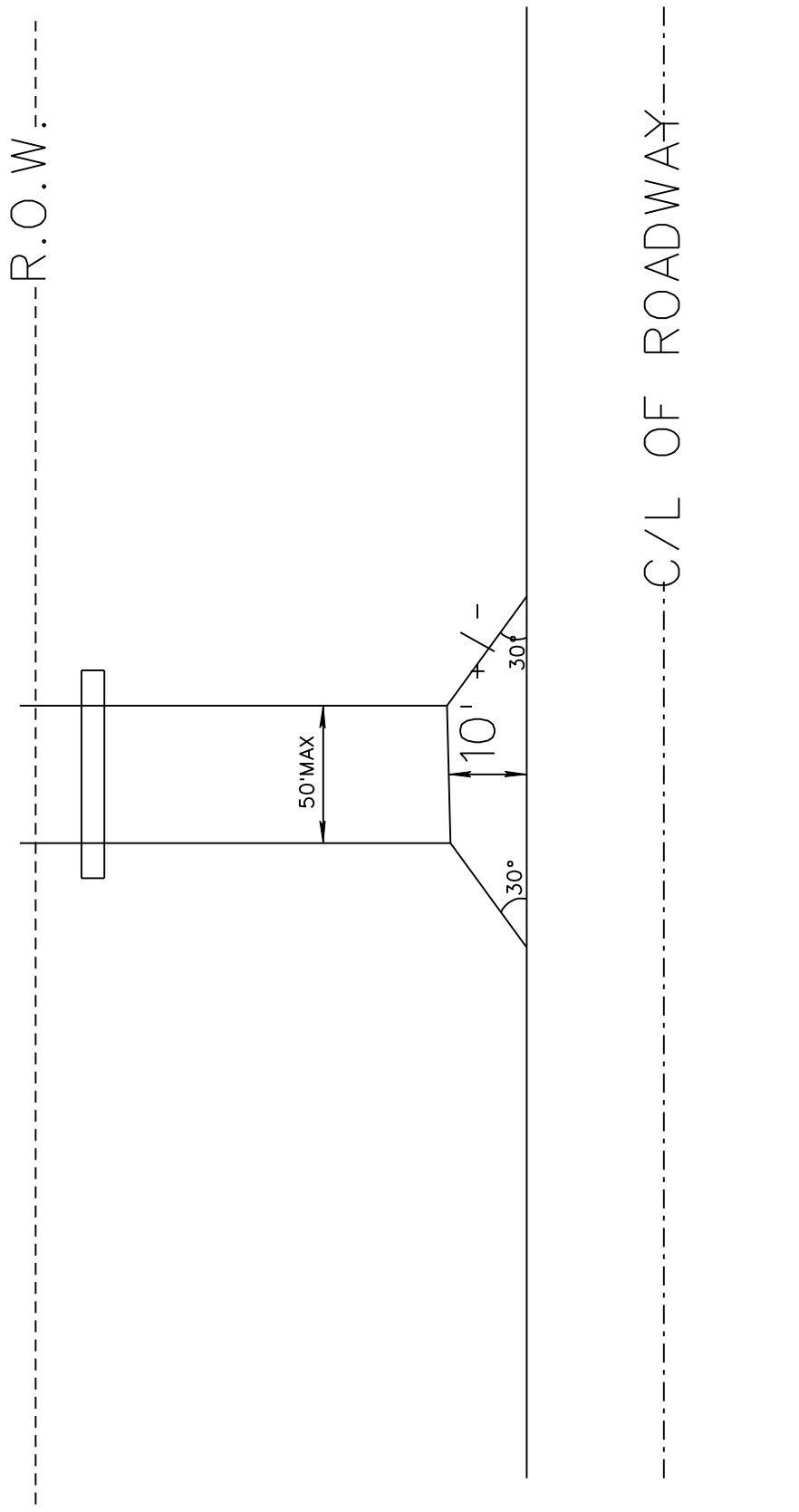
1" & VAR. OVERLAY REQ'D

BRIDGE ABUT.
OR PAVEMENT IN
PLACE

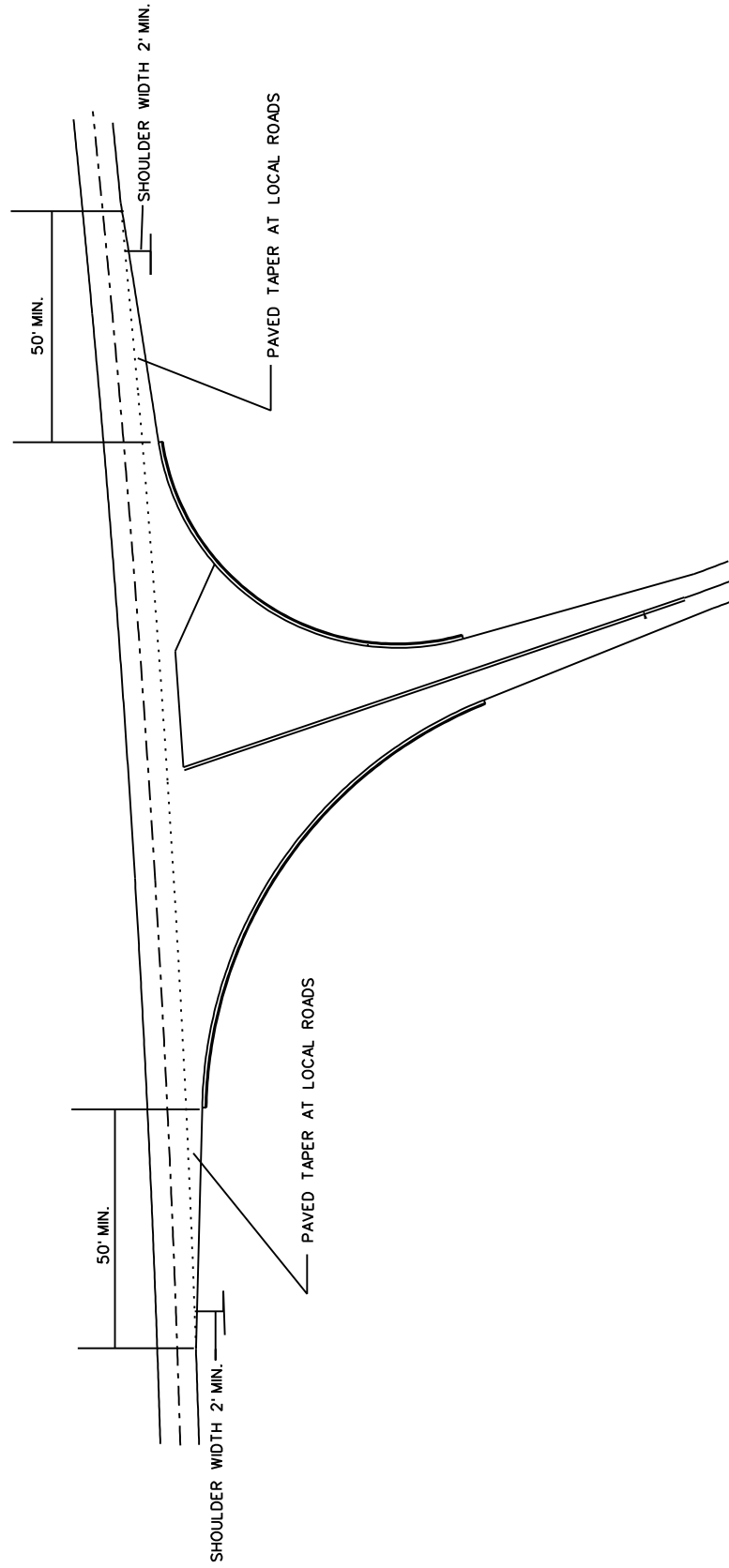
EXISTING ROADWAY
SURFACE IN PLACE

100' & VAR

PRIVATE DRIVEWAY DETAIL



TYPICAL FOR PAVED TAPER AT LOCAL ROADS



STATE	PROJECT NO.
MISS.	

NOTES ON ASSOCIATED ITEMS OF WORK:

202-8298 REMOVAL OF EXISTING JOINT MATERIAL

Descriptions: Shall include the removal of material associated with existing expansion joints. As designated in the detail drawings provided. Overlap joint types shall not be included under this item of work unless otherwise directed by the engineer.

Basis of Payment: Removal of armor and sliding plate joint material will be paid for in linear feet at the contract unit price along the length of the bridge deck on each side of the centerline. The removal of the armor and sliding plate joint material will only be paid for as the length along the centerline of the joint.

808-A001 JOINT PREPARATION

Descriptions: Shall include the work necessary to repair joints in concrete. This includes the removal of old material, as designated in the detail drawings provided. Epoxy mortar shall also be included under this item of work. Removal of old material shall be paid for directly and shall be considered as absorbed under this item of work. All other requirements shall be in accordance with the applicable provisions of Section 808. The specifications and any other sections specified therein.

Basis of Payment: The accepted quantities will be paid for in linear feet at the contract unit price along the length of the bridge deck on each side of the centerline joint.

907-823-0001 SAW CUT, TYPE I & 907-823-0002 SAW CUT, TYPE II

Descriptions: The saw cut depth shall be determined from the manufacturer's specifications. The saw cut type shall be the same as the performed joint seal selected.

Basis of Payment: The accepted quantities will be paid for in linear feet at the contract unit price along the length of the centerline joint on each side of the centerline joint. The contractor's responsibility to ensure that the proper depth is selected based on the manufacturer's recommendations.

907-823-A001 PREFORMED JOINT SEAL, TYPE I

Basis of Payment: The accepted quantities will be paid for in linear feet at the contract unit price along the length of the centerline joint.

907-823-A002 PREFORMED JOINT SEAL, TYPE II

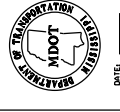
Basis of Payment: The accepted quantities will be paid for in linear feet at the contract unit price along the length of the centerline joint.

EPOXY MORTAR AND POLYMER CONCRETE NOTES:

- Epoxy mortar or polymer concrete may be used, guidelines for use or materials can be found in Section 808 of the specifications.
- Specifications: Mississippi Standard Specifications for Road and Bridge Construction, 2004.
- No change of plans will be permitted except by written engineer approval. Any change to design or construction procedure may be authorized by the bridge engineer provided such changes will not be cause for contract price adjustment.
- Work performed directly and shall therefore be considered an absorbed item of work.

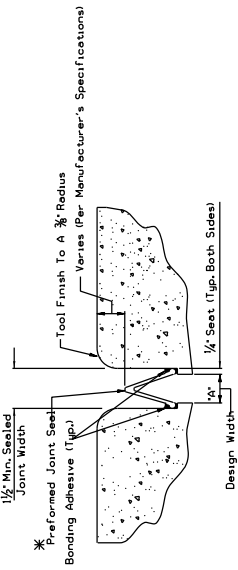
GENERAL NOTES:

- Specifications: Mississippi Standard Specifications for Road and Bridge Construction, 2004.
- No change of plans will be permitted except by written engineer approval. Any change to design or construction procedure may be authorized by the bridge engineer provided such changes will not be cause for contract price adjustment.
- Work performed directly and shall therefore be considered an absorbed item of work.



AB	SNOWSIAJN	COUNTY	WORKING NUMBER
			SHEET NUMBER
			ISSUE DATE
			DATE

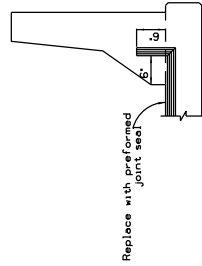
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 JOINT REPAIR
 SLIDING PLATE EXPANSION JOINTS
 PROJECT



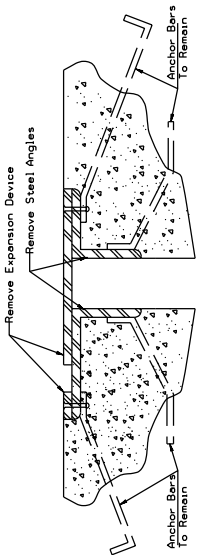
TYPICAL SECTION AT SAWCUT & SEALED JOINT
 Showing Sealed Joint After Sawcut And Repair With Epoxy Mortar

***NOTES:**

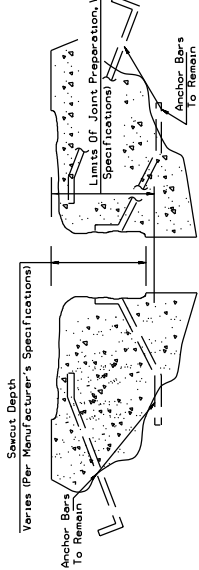
- The preformed joint seal shall be one of the following, installed according to the manufacturer's specifications:
 - A. Silcoflex Joint Sealing System Manufactured By R.J. Watson, Inc. In Alden, NY Type II Model SF-150
 - B. Webco SPS Joint System Manufactured By R.J. Watson Bowen Acme Corporation In Amherst, NY Type II Model SPS-225
 - C. V-Seal Expansion Joint System Manufactured By The Webco Company In North Baltimore, OH Type II Model V-200
- For estimating purposes, the R.J. Watson Silcoflex Joint Sealing System was selected. However, should another supplier be chosen, it is the contractor's responsibility to ensure that the manufacturer's recommendations are followed. Any variances between the specifications provided by the manufacturer and a manufacturer representative shall be present at the time joint sealing begins. Material that the contractor is properly schooled in installation of the joint.
- Joints shall be sealed at their design widths, dimension 'A', which is defined as the actual width of the joint opening. This width does not account for the 1/4" design width greater than or equal to 2" with the maximum design width of expansion material shall be required as directed by the Director of Structures, State Bridge Engineer.



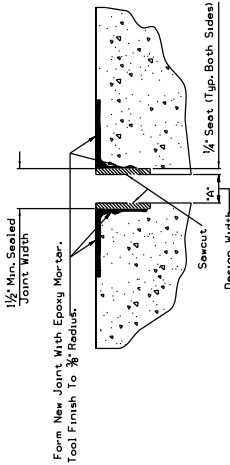
ELEVATION AT END OF SPAN



TYPICAL SECTION AT EXISTING JOINT
 Showing Existing Expansion Devices To Be Removed And Replaced With Preformed Joint Seal



TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL
 Showing Limits of Joint Preparation For Application Of New Joint Seal Materials



TYPICAL SECTION AT SAWCUT & JOINT REPAIR
 Showing Area Where Repairs Are Made After Sawcut With Epoxy Mortar Or Approved Equivalent

STATE	PROJECT NO.
MISS.	

NOTES ON ASSOCIATED ITEMS OF WORK:

202-B208 REMOVAL OF EXISTING JOINT MATERIAL
 Description: Shall include the removal of material associated with the existing joint. The joint shall be removed in accordance with the detail drawings provided. Other joint types shall not be included under this item of work unless otherwise directed by the Engineer.

Basis Of Payments
 Removal of armor and sliding plate joint material will be paid for in accordance with the applicable provisions of Section 808 of the Specifications and any other sections specified therein.

808-A081 JOINT PREPARATION
 Description: Shall include the work necessary to repair joints in preparation for the placement of new expansion material. This shall include the removal of existing material, preparation of existing silicone sealed compression and AC sealed joints as described under this item of work. All other requirements shall be in accordance with the applicable provisions of Section 808 of the Specifications and any other sections specified therein.

Basis Of Payments
 The accepted quantities will be paid for in linear feet at the contract unit price along the length of the bridge deck on each side of the centerline joint, type II.

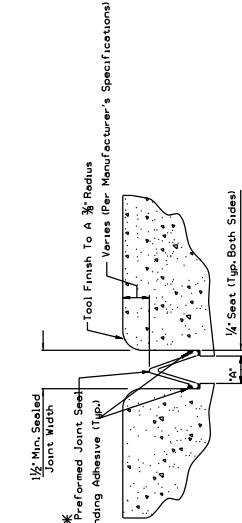
907-823-8001 SAW CUT, TYPE I & 907-823-8002 SAW CUT, TYPE II
 Description: The saw cut depth shall be determined from the manufacturer's specifications. The saw cut depth shall be the same as the performed joint seal selected.

Basis Of Payments
 The accepted quantities will be paid for in linear feet at the contract unit price along the length of the bridge deck on each side of the centerline joint. It is the contractor's responsibility to ensure that the proper depth is selected based on the manufacturer's recommendations.

907-823-A001 PREFORMED JOINT SEAL, TYPE I
907-823-A002 PREFORMED JOINT SEAL, TYPE II
 Description: The accepted quantities will be paid for in linear feet at the contract unit price along the length of the centerline joint.

EPOXY MORTAR AND POLYMER CONCRETE NOTES:
 Either epoxy mortar or polymer concrete may be used. Guidelines for selection of materials can be found in Section 808 of the Specifications.

GENERAL NOTES:
 1. Shop drawings shall conform to the standard specifications for Road and Bridge Construction, 2004.
 2. No change of plans will be permitted except by written order of the Mississippi State Department of Transportation. Minor changes to detail of design or construction procedure may be authorized by the bridge engineer provided such changes will not be cause for contract price adjustment.
 3. Work shall be paid for directly and shall therefore be considered an absorbed item of work.



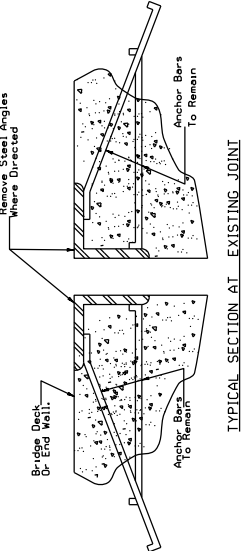
TYPICAL SECTION AT SAWCUT & SEALED JOINT
 Showing Sealed Joint After Sawcut And Repair With Epoxy Mortar

NOTES:

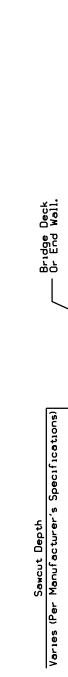
1. The performed joint seal shall be one of the following, installed according to the manufacturer's specifications:
 A. Silcoflex Joint Sealing System Manufactured By R.J. Watson, Inc. In Alden, NY
 Type I: Model SF-150
 Type II: Model SF-400
 B. Webo SPS Joint System Manufactured By Weston Bowman Acme Corporation In Amherst, NY
 Type I: Model SPS-225
 Type II: Model SPS-400
 C. Seal Expansion Joint System Manufactured By U.S. Brown Company In North Baltimore, OH
 Type I: Model SE-200
 Type II: Model V-400

2. For Estimating Purposes, The R.J. Watson Silcoflex Joint Sealing System Was Selected, However, Should Another Supplier Be Chosen, It Is The Contractor's Responsibility To Verify That The Selected System Meets The Requirements For Joint Preparation, Installation, Depth and Width, Adhesive Setting Time, And Any Other Variance Between The Specifications Provided By The Manufacturer, To Ensure That The Contractor Is Properly Trained In Installation Of The Joint Material.

3. Joints Shall Be Sealed At Their Design Width, Dimension W, Which Is Defined As: Seal Required On Both Sides Of The Joint, Performed Joint Seal, Type I, Shall Be Seal Or Design Width Less Than 2". Performed Joint Seal, Type II, Shall Be Used Being 2 1/2". In Cases Where Design Widths Are Greater Than 2 1/2", another type of Seal Shall Be Used As Directed By The Director Of Structures, State Bridge Engineer.



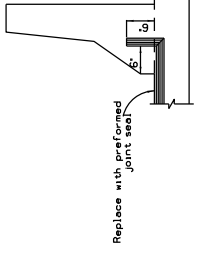
TYPICAL SECTION AT EXISTING JOINT
 Showing Existing Expansion Device To Be Removed And Replaced With Performed Joint Seal.



TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING MATERIAL
 Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials



TYPICAL SECTION AT SAWCUT & JOINT REPAIR
 Showing New Joint Material Being Applied After Sawcut With Epoxy Mortar Or Approved Equivalent



ELEVATION AT END OF SPAN

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

JOINT REPAIR ARMORED EXPANSION JOINTS

PROJECT: COUNTY WORKING NUMBER SHEET NUMBER

DATE: _____

APPROVED BY: _____

DESIGNED BY: _____

DRAWN BY: _____

CHECKED BY: _____

PROJECT ENGINEER: _____



NOTES ON ASSOCIATED ITEMS OF WORK:

262-8298 REMOVAL OF EXISTING JOINT MATERIAL
Description: Shall include the removal of material associated with armor, sliding plate, and neoprene expansion joints, as designed in the detail drawings provided. Other items of work unless otherwise directed by the engineer.

Basis Of Payment: Removal of armor and sliding plate joint material will be paid for in the unit price for the removal of the armor and sliding plate joint material on each side of the centerline joint, while removal of neoprene joint material will be paid for in the unit price for the removal of the centerline of the joint.

808-4001 JOINT PREPARATION

Description: Shall include the work necessary to repair joints in preparation for the placement of new expansion material. Existing joint material shall be removed. Existing armor shall also be included under this item of work. Removal of existing silicone seal, compression, and AC sealed joint materials will not be paid for directly and shall be considered as part of the joint preparation. All work shall be in accordance with the applicable provisions of Section 808 of the specifications and any other sections specified therein.

Basis Of Payment: The accepted quantities will be paid for in linear feet at the contract unit price along the length of the bridge deck on each side of the centerline joint.

987-823-8001 SAW CUT, TYPE I & 987-823-8002 SAW CUT, TYPE II

Description: The saw cut depth shall be determined from the manufacturer's specifications. The saw cut type shall be the same as the performed joint seal selected.

Basis Of Payment: The accepted quantities will be paid for in linear feet at the contract unit price along the length of the bridge deck on each side of the centerline joint. It is the contractor's responsibility to ensure that the depth of the saw cut is selected based on the manufacturer's recommendations.

987-823-4001 PREFORMED JOINT SEAL, TYPE I

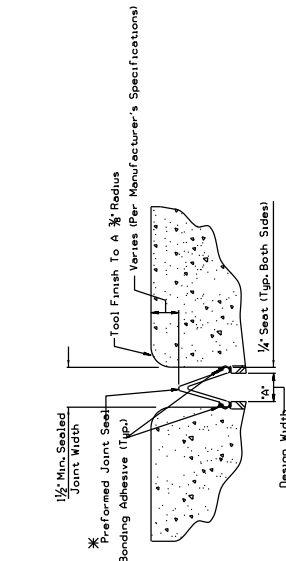
Description: The accepted quantities will be paid for in linear feet at the contract unit price along the length of the centerline joint.

EPOXY MORTAR AND POLYMER CONCRETE NOTES:

Either Epoxy Mortar Or Polymer Concrete May Be Used. Guidelines For Selection Of Materials Can Be Found In Section 808 of the Specifications.

GENERAL NOTES:

1. Specifications for Mississippi Standard Specifications For Road Construction, Mississippi Department of Transportation, 2004. Approval of Plans Will Be Permitted Except By Written Order of the Director of Structures, State Bridge Engineer. Any Changes To The Specifications, Plans, or Details Must Be Authorized By The Bridge Engineer Provided Such Changes Will Not Be Cause For Contract Price Adjustment.
2. Work For Which No Day Item Is Provided In The Proposal Will Be Considered As A Lump Sum Item. The Contractor Shall Be Responsible For Obtaining All Necessary Permits And Shall Therefore Be Considered An Absorbed Item of Work.



TYPICAL SECTION AT SAWCUT & SEALED JOINT
Showing Sealed Joint After Sawcut And Repair With Epoxy Mortar

***NOTES:**

1. The Preformed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications.

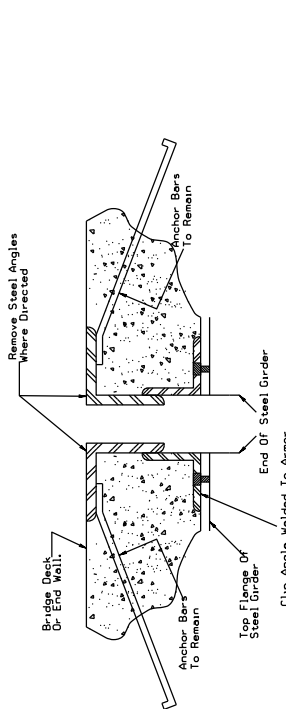
A. Silcoflex Joint Sealing System
www.jointseal.com
Type III, Model SP-400

B. Meko EPS Joint System
Manufactured By Watson Bowman Acme Corporation In Amherst, NY
www.bowmanacme.com
Type III, Model SPS-400

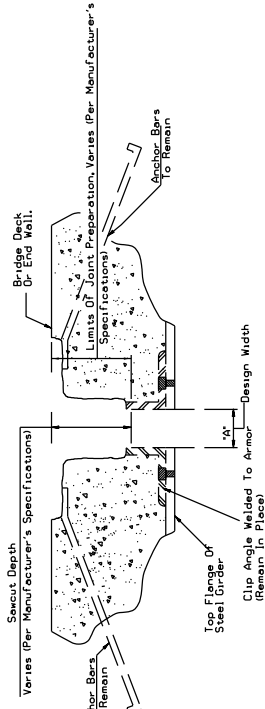
C. V-Seal Expansion Joint System
Manufactured By The D.S. Brown Company In North Baltimore, OH
www.vseal.com
Type II, Model V-300
Type II, Model V-400

2. For Estimating Purposes, The RJ Watson Silcoflex Joint Sealing System Was Selected. However, Should Another Supplier Be Chosen, It Is The Contractor's Responsibility To Provide The Manufacturer's Specifications For The Joint Seal. Other Variations Between The Specifications Provided By The Manufacturer, A 1/2\"/>

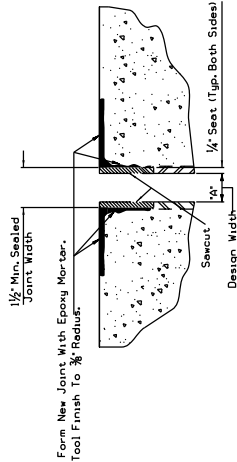
3. Joints Shall Be Sealed At Their Design Width, Dimension 'A', Which Is Defined As The Required On Both Sides Of The Joint. Preformed Joint Seal Type I Shall Be Used For Design Widths Less Than 2'. Preformed Joint Seal Type II Shall Be Used For Design Widths Greater Than 2'. In Cases Where Design Widths Are Greater Than 2', Expansion Material Shall Be Required As Directed By The Director Of Structures, State Bridge Engineer.



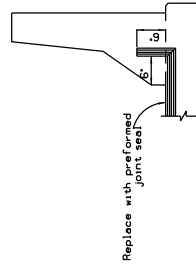
TYPICAL SECTION AT EXISTING JOINT
Showing Existing Expansion Device To Be Removed And Replaced With Preformed Joint Seal



TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING MATERIAL
Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials



TYPICAL SECTION AT SAWCUT & JOINT REPAIR
Showing Area Where Repairs Are Made After Sawcut With Epoxy Mortar Or Approved Equivalent



ELEVATION AT END OF SPAN



MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
JOINT REPAIR	
ARMORED EXPANSION JOINTS	
STEEL GIRDER SPANS	
PROJECT :	
COUNTY	WORKING NUMBER
SHEET NUMBER	
DATE:	

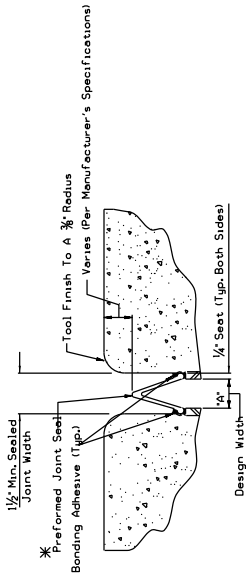
NOTES ON ASSOCIATED ITEMS OF WORKS:
 202-B298 REMOVAL OF EXISTING JOINT MATERIAL
 Description: Shall include the Removal of Material Associated With Armor, Sliding Plates, and Neoprene Expansion Joints, As Designated in the Detail Drawings Provided. Other Work Items Shall Not Be Included Under This Item Of Work Unless Otherwise Directed By the Engineer.
 Basis Of Payment: Removal of Armor and Sliding Plate Joint Material Will Be Paid For Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint, While Removal Of Neoprene Joint Material Will Only Be Paid For As The Length Along The Centerline Of The Joint.

808-A001 JOINT PREPARATION
 Description: Shall include The Work Necessary To Repair Joints In Preparation For The Placement Of New Expansion Material, As Designated In The Detail Drawings Provided. Epoxy Mortar Or Other Material Specified Under This Item Of Work Shall Be Applied To The Bridge Deck On Each Side Of The Centerline Joint. Materials Will Not Be Paid For Directly And Shall Be Considered As Absorbed Under This Item Of Work. All Other Requirements Specified In The Detail Drawings Provided Shall Apply To The Work. Specifications And Any Other Sections Specified Therein.
 Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint.
 907-823-8001 SAW CUT, TYPE I & 907-823-8002 SAW CUT, TYPE II
 Description: The Saw Cut Depth Shall Be Determined From The Manufacturer's Specifications. The Saw Cut Type Shall Be The Same As The Preformed Joint Seal Selected.

Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At Each Side Of The Centerline Joint. The Contractor Is Responsible To Ensure That The Proper Depth Is Selected Based On The Manufacturer's Recommendations.
 907-823-A001 PREFORMED JOINT SEAL, TYPE I
 907-823-A002 PREFORMED JOINT SEAL, TYPE II
 Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Centerline Joint.

EPOXY MORTAR AND POLYMER CONCRETE NOTES:
 Either Epoxy Mortar Or Polymer Concrete May Be Used. Guidelines For Selection Of Materials Can Be Found In Section 808 of the Specifications.
GENERAL NOTES:
 1. Specifications, Mississippi Standard Specifications For Road Construction, Section 808.
 2. No Change Of Plans Will Be Permitted Except By Written Approval Of The Director Of Structures, State Bridge Engineer. All Changes Must Be Approved By The Construction Commission. The Contractor Shall Be Responsible For Obtaining Such Approval. Work For Which No Pay Item Is Provided In The Proposal Will Be Considered As Absorbed Item Of Work.

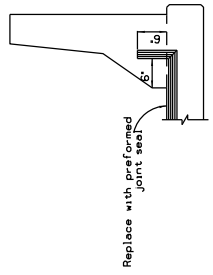
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 JOINT REPAIR
 SLIDING PLATE EXPANSION JOINTS
 STEEL GIRDER SPANS
 PROJECT : COUNTY WORKING NUMBER
 SHEET NUMBER



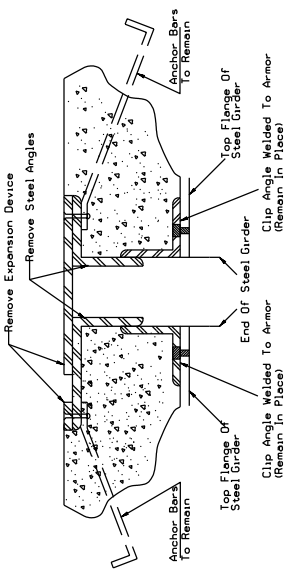
TYPICAL SECTION AT SAWCUT & SEALED JOINT
 Showing Sealed Joint at Top, Sawcut and Repair With Epoxy Mortar

***NOTES:**
 1. The Preformed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:
 A. Silcoflex Joint Sealing System Manufactured By R.J. Watson, Inc. In Alden, NY
 Type I: Model SF-150
 Type II: Model SF-400
 B. Webo SPS Joint System Manufactured By Webo Corporation In Amherst, NY
 www.webo.com
 Type I: Model SPS-225
 Type II: Model SPS-400
 C. V-Seal Expansion Joint System Manufactured By The U.S. Brom Company In North Baltimore, OH
 www.usbrom.com
 Type I: Model V-300
 Type II: Model V-400

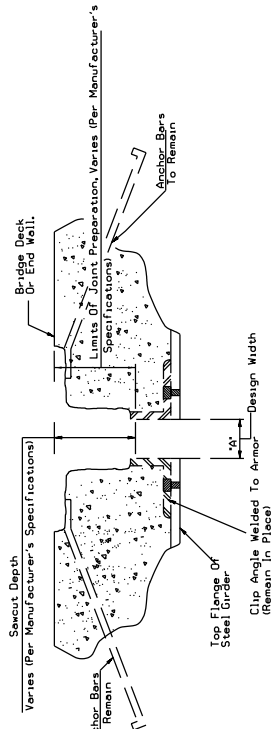
2. For Estimating Purposes, The UJ Watson Silcoflex Joint Sealing System Was Assumed To Be Used. The Contractor Shall Be Responsible To Ensure That The Manufacturer's Recommendations Are Followed For The Installation Of The Joint Seal. Any Other Variations Between The Manufacturer's Recommendations And A Manufacturer Representative Shall Be Present At The Time Joint Sealing Begins. It Is The Contractor's Responsibility To Ensure That The Joint Seal Is Properly Installed In The Joint Material.
 3. Joints Shall Be Sealed At Their Design Width Dimension W, Which Is Defined As The Actual Width Of The Joint Opening. This Width Does Not Account For The 1/2" Seat Required On Both Sides Of The Joint. Preformed Joint Seal, Type I, Shall Be Used For Design Widths Greater Than Or Equal To 2". With The Maximum Design Width Being 2 1/2". In Cases Where Design Widths Are Greater Than 2 1/2", another Type Of Seal Shall Be Required As Directed By The Director Of Structures, State Bridge Engineer.



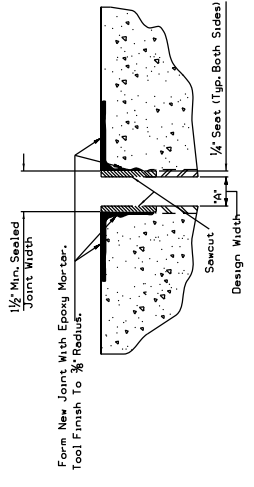
ELEVATION AT END OF SPAN
 Replace with preformed joint seal



TYPICAL SECTION AT EXISTING JOINT
 Showing Existing Joint Seal To Be Removed And Replaced With Preformed Joint Seal



TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING MATERIAL
 Showing Limits Of Joint Preparation For Application Of New Joint Seal Material



TYPICAL SECTION AT SAWCUT & JOINT REPAIR
 Showing Area Where Repairs Are Made After Sawcut With Epoxy Mortar Or Approved Equivalent

STATE	PROJECT NO.
MISS.	
<p>NOTES ON ASSOCIATED ITEMS OF WORK:</p> <p>202-B298 REMOVAL OF EXISTING JOINT MATERIAL Description: Shall Include The Removal Of Material Associated With Existing Expansion Joints. As Shown On The Detail Drawings Provided. Other Joint Types Shall Not Be Included Under This Item Of Work Unless Otherwise Directed By The Engineer. Basis Of Payment: Removal Of Armor And Sliding Plate Joint Material Will Be Paid For In Linear Feet At The Contract Unit Price. Material To Be Removed From The Centerline Joint, While Removal Of Neoprene Joint Material Will Only Be Paid For As The Length Along The Centerline Of The Joint.</p> <p>808-A001 JOINT PREPARATION Description: Shall Include The Work Necessary To Repair Joints In Preparation For The Placement Of New Expansion Material. As Designated In The Detail Drawings Provided. Epoxy Mortar Or Existing Silicone Sealed Compression And AC Sealed Joint Materials Will Not Be Paid For Directly And Shall Be Considered As Absorbed Under This Item Of Work. All Other Requirements As Specified In Section 808 Of The Specifications And Any Other Sections Specified Therein. Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint.</p> <p>907-823-8001 SAW CUT, TYPE I & 907-823-8002 SAW CUT, TYPE II Description: The Saw Cut Depth Shall Be Determined From The Manufacturer's Specifications. The Saw Cut Type Shall Be The Same As The Performed Joint Seal Selected. Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint. It Is The Contractor's Responsibility To Verify That The Depth Is Selected Based On The Manufacturer's Recommendations.</p> <p>907-823-4001 PREFORMED JOINT SEAL, TYPE I 907-823-4002 PREFORMED JOINT SEAL, TYPE II Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Centerline Joint.</p> <p>EPDXY MORTAR AND POLYMER CONCRETE NOTES: Either Epoxy Mortar Or Polymer Concrete May Be Used. Guidelines For Selection Of Materials Can Be Found In Section 808 of the Specifications. GENERAL NOTES: 1. Specifications, Mississippi Standard Specifications For Road Construction, Shall Apply Unless Otherwise Indicated. 2. No Change Of Plans Will Be Permitted Except By Written Approval Of The Director Of Structures, State Bridge Engineer. Any Changes To Details, Design Or Construction Procedure Must Be Approved In Advance. Any Changes To Details And Specifications Will Not Be Cause For Contract Price Adjustment. 3. Work For Which No Pay Item Is Provided In The Proposal Will Be Considered As Absorbed Into The Contract Unit Price For The Corresponding Item of Work.</p>	
<p>MISSISSIPPI DEPARTMENT OF TRANSPORTATION JOINT REPAIR NEOPRENE EXPANSION JOINTS PROJECT COUNTY WORKING NUMBER SHEET NUMBER</p>	

NOTES ON ASSOCIATED ITEMS OF WORK:

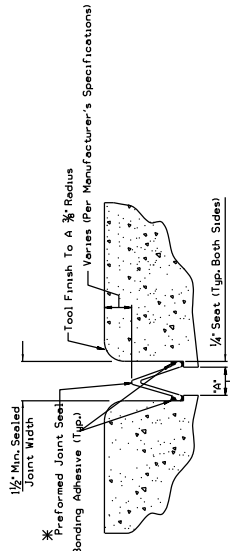
202-B298 REMOVAL OF EXISTING JOINT MATERIAL
 Description: Shall Include The Removal Of Material Associated With Existing Expansion Joints. As Shown On The Detail Drawings Provided. Other Joint Types Shall Not Be Included Under This Item Of Work Unless Otherwise Directed By The Engineer.
 Basis Of Payment: Removal Of Armor And Sliding Plate Joint Material Will Be Paid For In Linear Feet At The Contract Unit Price. Material To Be Removed From The Centerline Joint, While Removal Of Neoprene Joint Material Will Only Be Paid For As The Length Along The Centerline Of The Joint.

808-A001 JOINT PREPARATION
 Description: Shall Include The Work Necessary To Repair Joints In Preparation For The Placement Of New Expansion Material. As Designated In The Detail Drawings Provided. Epoxy Mortar Or Existing Silicone Sealed Compression And AC Sealed Joint Materials Will Not Be Paid For Directly And Shall Be Considered As Absorbed Under This Item Of Work. All Other Requirements As Specified In Section 808 Of The Specifications And Any Other Sections Specified Therein.
 Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint.

907-823-8001 SAW CUT, TYPE I & 907-823-8002 SAW CUT, TYPE II
 Description: The Saw Cut Depth Shall Be Determined From The Manufacturer's Specifications. The Saw Cut Type Shall Be The Same As The Performed Joint Seal Selected.
 Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint. It Is The Contractor's Responsibility To Verify That The Depth Is Selected Based On The Manufacturer's Recommendations.

907-823-4001 PREFORMED JOINT SEAL, TYPE I
907-823-4002 PREFORMED JOINT SEAL, TYPE II
 Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Centerline Joint.

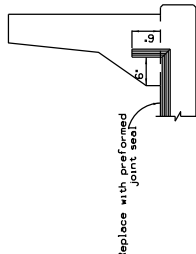
EPDXY MORTAR AND POLYMER CONCRETE NOTES:
 Either Epoxy Mortar Or Polymer Concrete May Be Used. Guidelines For Selection Of Materials Can Be Found In Section 808 of the Specifications.
GENERAL NOTES:
 1. Specifications, Mississippi Standard Specifications For Road Construction, Shall Apply Unless Otherwise Indicated.
 2. No Change Of Plans Will Be Permitted Except By Written Approval Of The Director Of Structures, State Bridge Engineer. Any Changes To Details, Design Or Construction Procedure Must Be Approved In Advance. Any Changes To Details And Specifications Will Not Be Cause For Contract Price Adjustment.
 3. Work For Which No Pay Item Is Provided In The Proposal Will Be Considered As Absorbed Into The Contract Unit Price For The Corresponding Item of Work.



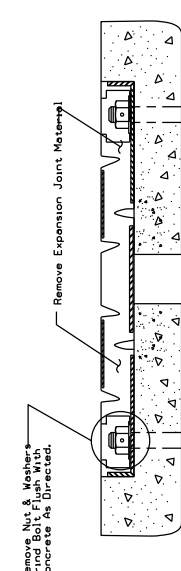
TYPICAL SECTION AT SAWCUT & SEALED JOINT
 Showing Sealed Joint After Sawcut And Repair With Epoxy Mortar

***NOTES:**
 1. The Performed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:
 A. Silicoflex Joint Sealing System
 www.silicoflex.com
 Type I: Model SF-1000
 Type II: Model SF-1000
 B. Mape SPS Joint System
 www.mape.com
 Manufactured By Watson Bowman Acme Corporation In Amherst, NY
 Type I: Model SP-400
 Type II: Model SP-400
 C. V-Seal Expansion Joint System
 Manufactured By The U.S. Brown Company In North Baltimore, OH
 Type I: Model V-300
 Type II: Model V-400

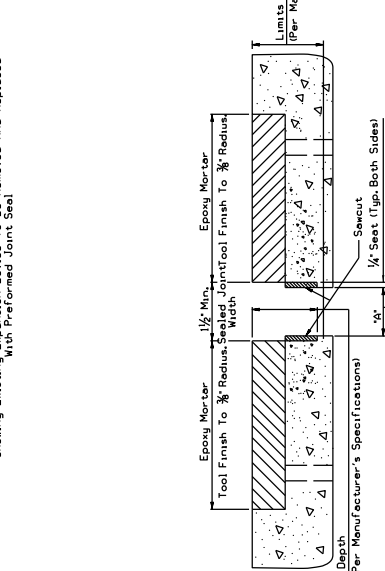
2. For Estimating Purposes, The RJ Watson Silicoflex Joint Sealing System Was Selected. However, Should Another Supplier Be Chosen, It Is The Contractor's Responsibility To Verify That The System Selected Meets The Requirements For Joint Preparation, Installation, Depth And Width, Adhesive Setting Times, And A Manufacturer Representative Shall Be Present At The Time Joint Sealing Begins To Ensure That The Contractor Is Properly Schooled In Installation Of The Joint Material.
 3. Joints Shall Be Sealed At Their Design Widths Dimension W, Which Is Defined As Seal Required On Both Sides Of The Joint. Performed Joint Seal Type I Shall Be Used For Design Widths Greater Than Or Equal To 2". With The Minimum Design Width Being 2 1/2". In Cases Where Design Widths Are Greater Than 2 1/2", another Type Of Seal Shall Be Required As Directed By The Director Of Structures, State Bridge Engineer.



ELEVATION AT END OF SPAN



TYPICAL SECTION AT EXISTING JOINT
 Showing Existing Expansion Device To Be Removed And Replaced With Preformed Joint Seal



TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL
 Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials

STATE	PROJECT NO.
MISS.	

NOTES ON ASSOCIATED ITEMS OF WORK:

888-4001 JOINT PREPARATION

Description: Shall include the work necessary to repair joints in preparation for the placement of new expansion material. All joints in the deck drawings for road, epoxy mortar shall be prepared in accordance with the provisions of existing silicone sealed, compression and AC sealed joint materials will not be paid for directly and shall be considered as part of the existing joint materials. All other joints shall be in accordance with the applicable provisions of Section 888 of the Specifications and any other sections specified therein.

Basis of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint.

987-823-8001 SAW CUT, TYPE I & 987-823-8002 SAW CUT, TYPE II

Description: The Saw Cut Depth Shall Be Determined From The Manufacturer's Specifications. The Saw Cut Type Shall Be The Same As The Preformed Joint Seal Selected.

Basis of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint. The Preformed Joint Seal Selected Shall Be Based On The Manufacturer's Recommendations.

987-823-4001 PREFORMED JOINT SEAL, TYPE I

987-823-4002 PREFORMED JOINT SEAL, TYPE II

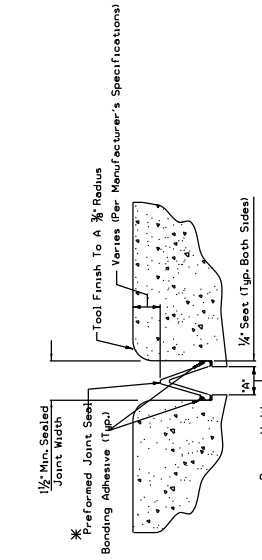
Basis of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Centerline Joint.

EPOXY MORTAR AND POLYMER CONCRETE NOTES:

Either Epoxy Mortar Or Polymer Concrete May Be Used. Guidelines For Selection Of Materials Can Be Found In Section 888 of the Specifications.

GENERAL NOTES:

1. Specifications: Mississippi Standard Specifications For Road And Bridge Construction, 2004.
2. No work shall be done until the project has been approved by the Director Of The Department Of Transportation. The Director Of Structures Shall Bridge Engineer. Minor Changes To Detail Of Design Or Construction Procedure May Be Authorized By The Bridge Engineer. Provided Such Changes Will Not Affect The Safety Or Structural Integrity Of The Work. Work For Which No Pay Item Is Provided In The Proposal Will Not Be Paid For Directly And Shall Therefore Be Considered An Absorbed Item Of Work.



TYPICAL SECTION AT SAWCUT & SEALED JOINT

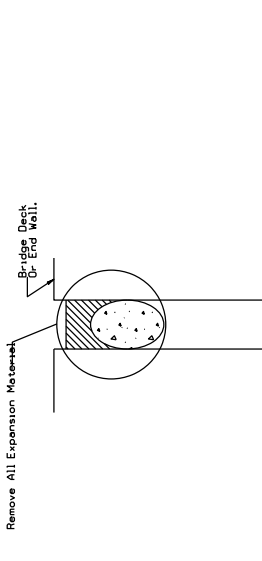
Showing Sealed Joint After Sawcut And Repair With Epoxy Mortar

***NOTES:**

1. The Preformed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:
 A. Silcoflex Joint Sealing System Manufactured By R.J. Watson, Inc. In Alden, NY
 Type I: Model SF-150
 Type II: Model SF-400
 B. Webo SPS Joint System Manufactured By Watson Bowman Acme Corporation In Amherst, NY
 Type I: Model SPS-235
 Type II: Model SPS-400
 www.watson.com
 www.watson.com
 Type I: Model V-400
 Type II: Model V-400

2. For Estimating Purposes, The R.J. Watson Silcoflex Joint Sealing System Was Selected. However, Should Another Supplier Be Chosen, It Is The Contractor's Responsibility To Ensure That The Manufacturer's Recommendations Followed. Any Other Vendors Between The Specifications Provided By The Manufacturer, The Contractor Shall Be Responsible For Obtaining The Necessary Approvals To Ensure That The Contractor Is Properly Trained In Installation Of The Joint Material.

3. Joints Shall Be Sealed At Their Design Widths, Dimension 'a', Which Is Defined As The Actual Width Of The Joint Opening. This Width Does Not Account For The Sealant. The Actual Width Of The Joint Opening Shall Be Determined From The Manufacturer's Specifications. For Design Widths Less Than 2", Preformed Joint Seal, Type II, Shall Be Used (Per Section 888). In Cases Where Design Widths Are Greater Than 2", Another Type Of Expansion Material Shall Be Required As Directed By The Director Of Structures, State Bridge Engineer.

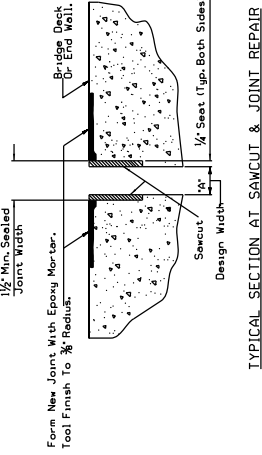


TYPICAL SECTION AT EXISTING JOINT

Showing Existing Existing Material To Be Removed And Replaced With Preformed Joint Seal

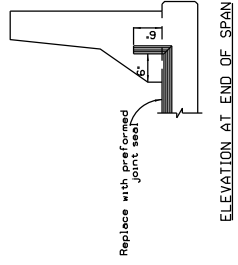
TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL

Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials



TYPICAL SECTION AT SAWCUT & JOINT REPAIR

Showing Area Where Repairs Are Made After Sawcut, With Epoxy Mortar Or Approved Equivalent



MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
JOINT REPAIR	
SILICONE SEALED EXPANSION JOINTS	
PROJECT: COUNTY	
WORKING NUMBER	SHEET NUMBER
DATE	SCALE
BY	CHECKED
IN CHARGE	DESIGNED
APPROVED	APPROVED

STATE	PROJECT NO.
MISS.	

NOTES ON ASSOCIATED ITEMS OF WORK:

808-A081 JOINT PREPARATION

Description: Shall include the Work Necessary To Repair Joints In Preparation For The Placement Of New Expansion Material, As Designated In The Detail Drawings Provided. Epoxy Mortar Shall Also Be Included Under This Item Of Work. Removal Of Existing Mortar From Joints To Be Repaired Shall Be Considered As Absorbed Under This Item Of Work. All Other Requirements As Indicated On The Specifications And On Other Sections Specified Therein.

Basis of Payments: The Accepted Quantities Will Be Paid For In Linear Feet At On Each Side Of The Center-Line Joint.

907-823-B001 SAW CUT, TYPE I & 907-823-B002 SAW CUT, TYPE II

Description: The Saw Cut Depth Shall Be Determined From The Manufacturer's Specifications. The Saw Cut Depth Shall Be The Same As The Performed Joint Seal Selected.

Basis of Payments: The Accepted Quantities Will Be Paid For In Linear Feet At On Each Side Of The Center-Line Joint. It Is The Contractor's Responsibility To Ensure That The Proper Depth Is Selected Based On The Manufacturer's Recommendations.

907-823-A001 PREFORMED JOINT SEAL, TYPE I

907-823-A002 PREFORMED JOINT SEAL, TYPE II

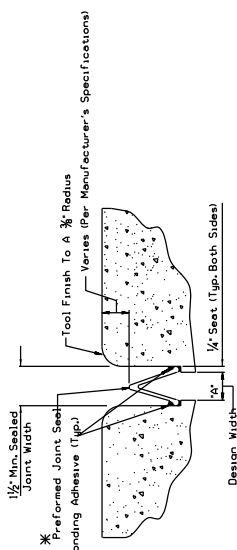
Basis of Payments: The Accepted Quantities Will Be Paid For In Linear Feet At On Each Side Of The Center-Line Joint.

EPOXY MORTAR AND POLYMER CONCRETE NOTES:

Either Epoxy Mortar Or Polymer Concrete May Be Used. Guidelines For Selection Of Materials Can Be Found In Section 808 of the Specifications.

GENERAL NOTES:

1. Specifications Mississippi Standard Specifications For Road And Bridge Construction, 2004.
2. No Change Of Plans Will Be Permitted Except By Written Instruction From The Engineer. The Contractor Shall Be Responsible For Minor Changes To Detail Of Design Or Construction Procedure. Any Changes To Detail Of Design Or Construction Procedure May Be Authorized By The Bridge Engineer Provided Such Changes Do Not Affect The Contract Price In Any Manner.
3. Work Will Be Paid For Directly In Lump Sum. The Contractor Will Not Be Paid For Directly And Shall Therefore Be Considered An Absorbed Item of Work.

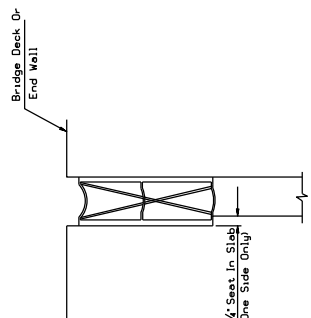


TYPICAL SECTION AT SAWCUT & SEALED JOINT

Showing Sealed Joint After Sawcut And Repair With Epoxy Mortar

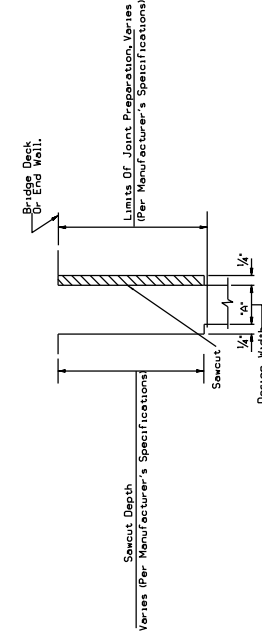
***NOTES:**

1. The Performed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:
 - A. Silicoflex Joint Sealing System manufactured by R.J. Watson, Inc. in Alden, NY www.rjwatson.com Type I; Model SF-100 Type II; Model SF-400
 - B. Mape SPS Joint System manufactured by Mape Corporation in Amherst, NY www.mapecorp.com Type I; Model SPS-225 Type II; Model SPS-420
 - C. V-Seal Expansion Joint System manufactured by The D.S. Brown Company in North Baltimore, OH www.vseal.com Type I; Model V-300 Type II; Model V-400
2. For Estimating Purposes, The RJ Watson Silicoflex Joint Sealing System Was Selected, However, It Should Another Supplier Be Chosen, It Is The Contractor's Responsibility To Verify That The Supplier's Product Meets The Requirements For Joint Preparation, Installation, Depth And Width, Adhesive Setting Times, And All Other Variances Between The Specifications Provided By The Manufacturer, And The Specifications Provided By The State Bridge Engineer. It Is The Contractor's Responsibility To Ensure That The Contractor Is Properly Trained In Installation Of The Joint Sealing Material.
3. Joints Shall Be Sealed At Their Design Widths, Dimension "A", Which Is Defined As Seal Required On Both Sides Of The Joint. Performed Joint Seal, Type I, Shall Be Used For Design Widths Less Than 2". Performed Joint Seal, Type II, Shall Be Used For Design Widths Greater Than 2". Performed Joint Seal, Type III, Shall Be Used For Design Widths Greater Than 2 1/2". In Cases Where Design Widths Are Greater Than 2 1/2", another Type Of Seal Shall Be Used. Seal Widths Shall Be Required As Directed By The Director Of Structures, State Bridge Engineer.



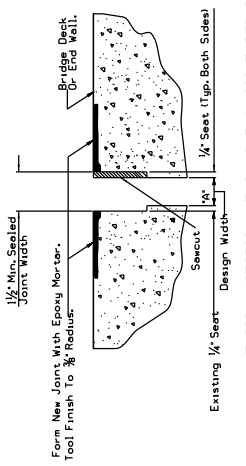
TYPICAL SECTION AT EXISTING JOINT

Showing Existing Expansion Device To Be Removed And Replaced With Performed Joint Seal.



TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL

Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials



TYPICAL SECTION AT SAWCUT & JOINT REPAIR

Showing New Joint With Epoxy Mortar Repair Made After Sawcut With Epoxy Mortar Or Approved Equivalent

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
JOINT REPAIR	
COMPRESSION EXPANSION JOINTS	
PROJECT :	
COUNTY :	
DATE	WORKING NUMBER
DESIGNED BY	SHEET NUMBER
CHECKED BY	
IN CHARGE	
SUPERVISOR OF STRUCTURES DIVISION BRIDGE ENGINEERING SECTION	



STATE	PROJECT NO.
MISS.	

NOTES ON ASSOCIATED ITEMS OF WORK:

888-A801 JOINT PREPARATION

Description: Shall include the work necessary to repair joints in preparation for the placement of new expansion material. The joint shall be prepared by saw cutting, grinding, and cleaning. The joint shall be prepared to a depth of 1/2" below the surface of the concrete. The joint shall be prepared to a width of 1/2" on each side of the joint. The joint shall be prepared to a depth of 1/2" below the surface of the concrete. The joint shall be prepared to a width of 1/2" on each side of the joint. The joint shall be prepared to a depth of 1/2" below the surface of the concrete. The joint shall be prepared to a width of 1/2" on each side of the joint.

Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet Along Each Side Of The Centerline Joint.

987-823-8001 SAW CUT, TYPE I & 987-823-8002 SAW CUT, TYPE II

Description: The Saw Cut Depth Shall Be Determined From The Manufacturer's Specifications. The Saw Cut Type Shall Be The Same As The Performed Joint Seal Selected.

Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet Along Each Side Of The Centerline Joint. It Is The Contractor's Responsibility To Provide The Manufacturer's Recommendations Based On The Manufacturer's Recommendations.

987-823-4001 PREFORMED JOINT SEAL, TYPE I
987-823-4002 PREFORMED JOINT SEAL, TYPE II

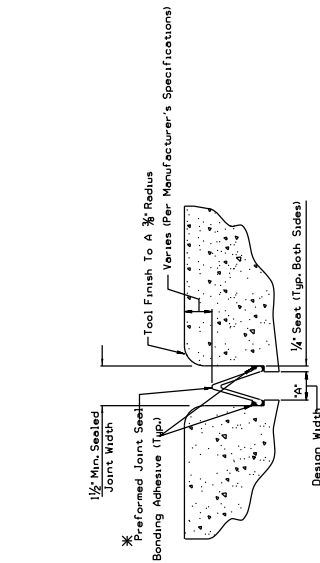
Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet Along Each Side Of The Centerline Joint.

EPOXY MORTAR AND POLYMER CONCRETE NOTES:

Either Epoxy Mortar Or Polymer Concrete May Be Used. Guidelines For Selection Of Materials Can Be Found In Section 888 of the Specifications.

GENERAL NOTES:

1. Specifications, Mississippi Standard Specifications For Road And Bridge Construction, 2004.
2. No substitutions will be allowed without the written approval of the Director Of Structures State Bridge Procedure. Minor Changes To Detail Of Design Or Construction Procedure May Be Authorized By The Bridge Engineer Provided Such Changes Do Not Affect The Structural Integrity Of The Work.
3. Work For Which No Pay Item Is Provided In The Proposal Will Not Be Paid For Directly And Shall Therefore Be Considered An Absorbed Item Of Work.

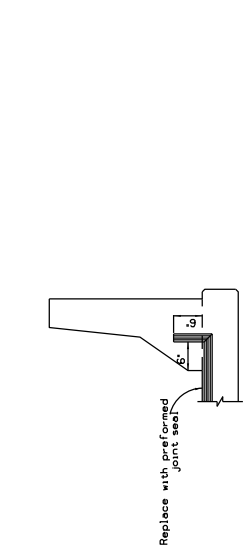


TYPICAL SECTION AT SAWCUT & SEALED JOINT

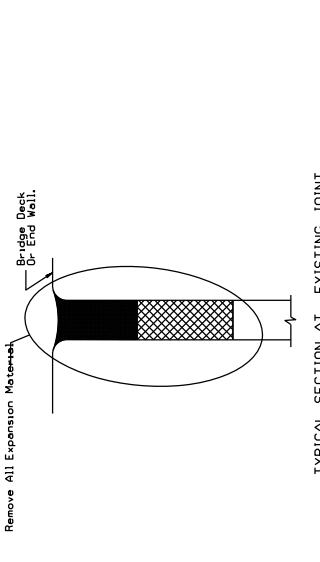
Showing Sealed Joint After Sawcut And Repair With Epoxy Mortar.

***NOTES:**

1. The Performed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:
A. Silcoflex Joint Sealing System
www.silcoflex.com
Type I: Model SF-100
Type II: Model SF-100
B. RJ Watson Joint System
www.rjwatson.com
Type I: Model SPS-225
Type II: Model SPS-400
C. V-Seal Expansion Joint System
www.vseal.com
Type I: Model V-300
Type II: Model V-480
2. For Estimating Purposes, The RJ Watson Silcoflex Joint Sealing System Was Selected. However, Should Another Supplier Be Chosen, It Is The Contractor's Responsibility To Provide The Manufacturer's Recommendations For Joint Preparation, Installation, Depth And Width, Adhesive Setting Times, And All Other Variables Between The Manufacturer's Recommendations. The Contractor Shall Ensure That The Contractor Is Properly Schooled In Installation Of The Joint Material.
3. Joints Shall Be Sealed At Their Design Width, Dimension 'A', Which Is Defined As: Seal Required On Both Sides Of The Joint. Performed Joint Seal, Type I, Shall Be Used For Design Widths Less Than 2". Performed Joint Seal, Type II, Shall Be Used For Design Widths Greater Than 2". In Cases Where Design Widths Are Greater Than 2", Another Type Of Expansion Material Shall Be Required As Directed By The Director Of Structures, State Bridge Engineer.



ELEVATION AT END OF SPAN

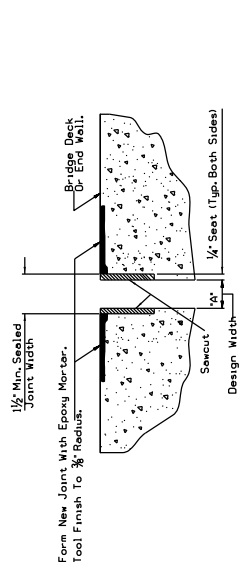


TYPICAL SECTION AT EXISTING JOINT

Showing Existing Expansion Material To Be Removed And Replaced With Performed Joint Seal

TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL

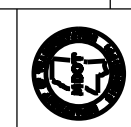
Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials



TYPICAL SECTION AT SAWCUT & JOINT REPAIR

Showing New Joint With Epoxy Mortar. Seal Material To Be Replaced After Sawcut With Epoxy Mortar Or Approved Equivalent.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
JOINT REPAIR	
AC SEALED EXPANSION JOINTS	
PROJECT :	COUNTY :
WORKING NUMBER :	SHEET NUMBER :



DATE:

STATE	MISS.
PROJECT NO.	

NOTES ON ASSOCIATED ITEMS OF WORKS

987-824-PP097 BRIDGE REPAIR, ENDWALL REPAIR

Description: Shall Include The Work Necessary To Remove And Replace The Damaged Endwall, As Designated In The Detail Drawings Provided. Instead Of Limiting The Repair To The Damaged Section, The Specified Depth Of Endwall Shall Be Removed Along The Entire Width Of The Bridge Deck. Basis of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Width Of The Bridge Deck.

Damage Caused To Other Elements Of The Structure Or Roadway While Completing This Item Of Work Shall Be Repaired By The Contractor At No Cost To The Department.

Prior to Placing New Concrete, All Concrete Surfaces That Will Be In Contact With The New Concrete Shall Be Painted With An Approved Epoxy Binder-Designed To Bond New Concrete To Old.

New Concrete Shall Be High Early Strength Concrete, As Follows:

- The concrete mixture design shall be furnished by the Contractor, for approval by the Materials Division. Mixture design parameters are as follows:
- Required Strength: 2500 psi in 24 hours
- Maximum Slump: 6 inches

Non-chloride based accelerator may be used if the ambient temperature is 50°F or less, but shall not be used if the ambient temperature is greater than 80°F. Synthetic structural fibers shall be used. The Contractor shall select a manufacturer's list of products and the manufacturer's recommendations shall be followed for the dosage rate.

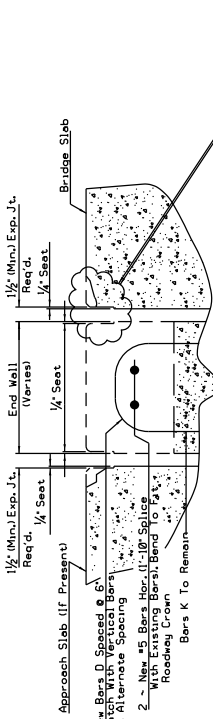
Curing is to be continuous until 2500 psi is attained. Traffic is to be permitted on the concrete when the contractor may use the Maturity Method per Section 907-804 to estimate the concrete compressive strength for the purposes of releasing the repair area to traffic. However, final acceptance of the in-place concrete shall be determined using concrete placement, two cylinders are to be tested at 8, 16, and 24 hour intervals. The two remaining cylinders shall be used to determine the 28-day compressive strength of the concrete.

The Removal Of Existing Expansion Material May Require Any Number Of Pay Items Listed Below. Once The Expansion Device Is Identified, Refer To The Associated Items Of Work.

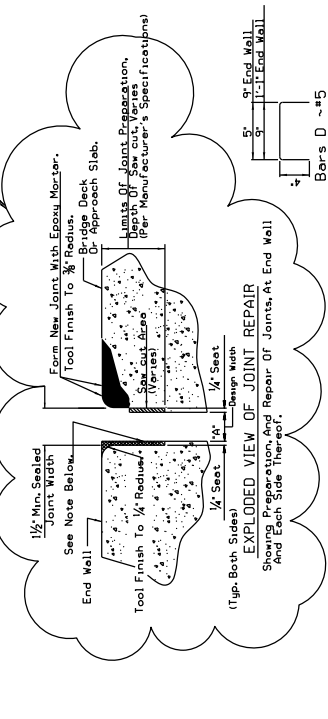
- 202-8238 REMOVAL OF EXISTING JOINT MATERIAL
- 907-823-B001 SAW CUT, TYPE I
- 907-823-B002 SAW CUT, TYPE II
- 907-823-A001 PREFORMED JOINT SEAL, TYPE I
- 907-823-A002 PREFORMED JOINT SEAL, TYPE II

GENERAL NOTES:

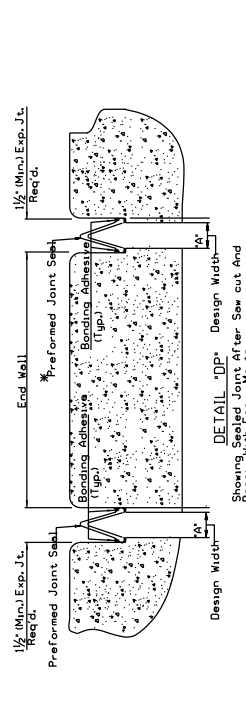
- Specifications: Mississippi Standard Specifications For Road And Bridge Construction, 2004, Revised Except By Written Approval Of The Director Of Structures, State Bridge Engineer. Minor Changes To Detail Of Design Or Construction Procedure Will Not Be Cause For Contract Price Adjustment.
- Work For Which No Pay Item Is Provided In The Proposal Will Be Considered An Absorbed Item of Work, Fully And Shall Therefore Be Considered An



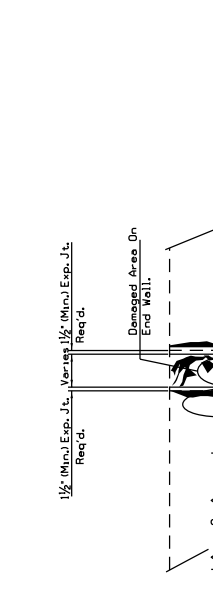
DETAIL 'D'
Showing Repair Details Of End Wall Reinforcing Steel And Expansion Joint Formation.



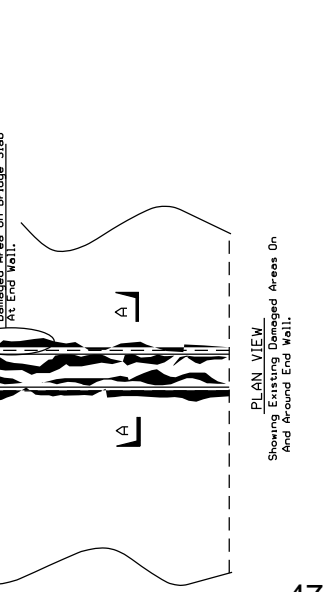
EXPLODED VIEW OF JOINT REPAIR
Showing Preparation And Repair Of Joints At End Wall And Each Side Thereof.



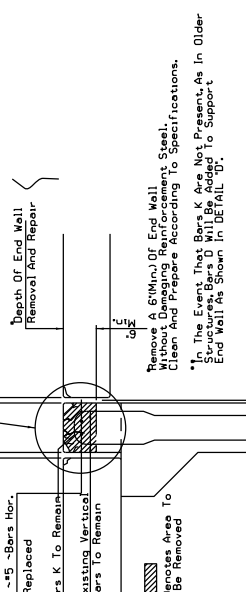
DETAIL 'DP'
Showing Sealed Joint After Saw cut And Repair With Epoxy Mortar



PLAN VIEW
Showing Existing Damaged Areas On And Around End Wall.



ELEVATION (SECTION A-A)
Showing Details Of Removal Of Damaged End Wall.

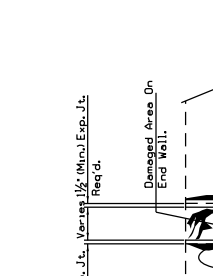


***NOTES:**

- The Preformed Joint Seal Shall Be One Of The Following Installed According To The Manufacturer's Specifications:
A. Manufactured By J. Watson, Inc. In Albany, NY
Type II, Model SP-100
B. Meko SPS Joint System
Manufactured By Meko-Boman Ane Corporation In Amherst, NY
Type II, Model SPS-200
C. V-J Seal Expansion Joint System
Manufactured By The U.S. Boman Company In North Baltimore, MD
Type II, Model V-200

2. For Estimating Purposes, The RJ Watson Silcoflex Joint Sealing System Was Responsible To Ensure That The Manufacturer's Recommendations Are Followed For Joint Preparation, Installation Depths And Widths, Adhesive Setting Times, And For Grouting And Sealing. A Manufacturer Representative Shall Be Present At The Time Joint Sealing Begins To Ensure That The Contractor Is Properly Schooled In Installation Of The Joint Material.

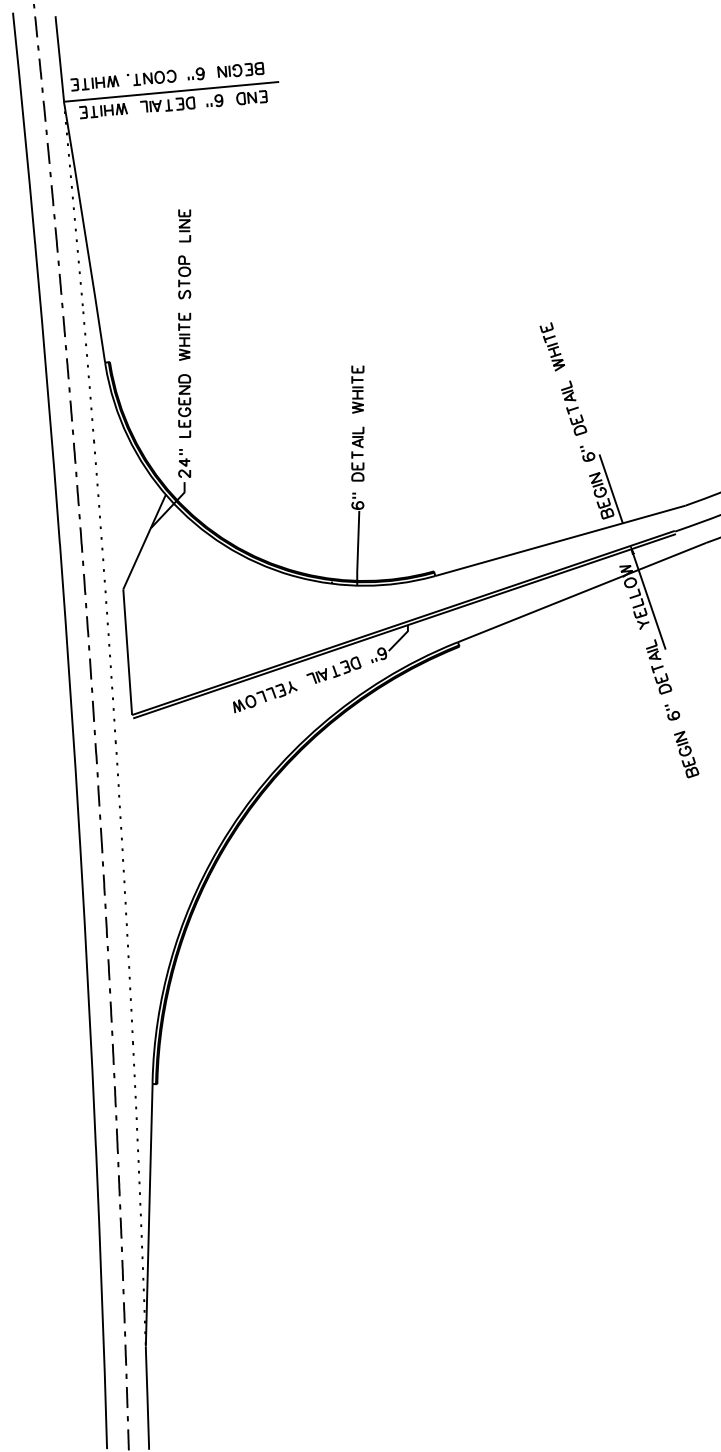
3. Joints Shall Be Sealed At Their Design Width, Dimension W, Which Is Defined As Seal Required On Both Sides Of The Joint. Preformed Joint Seal, Type I, Shall Be Used For Design Widths Less Than 2 1/2\"/>



MISSISSIPPI DEPARTMENT OF TRANSPORTATION
PROJECT: END WALL REPAIR
COUNTY: _____
WORKING NUMBER: _____
SHEET NUMBER: _____

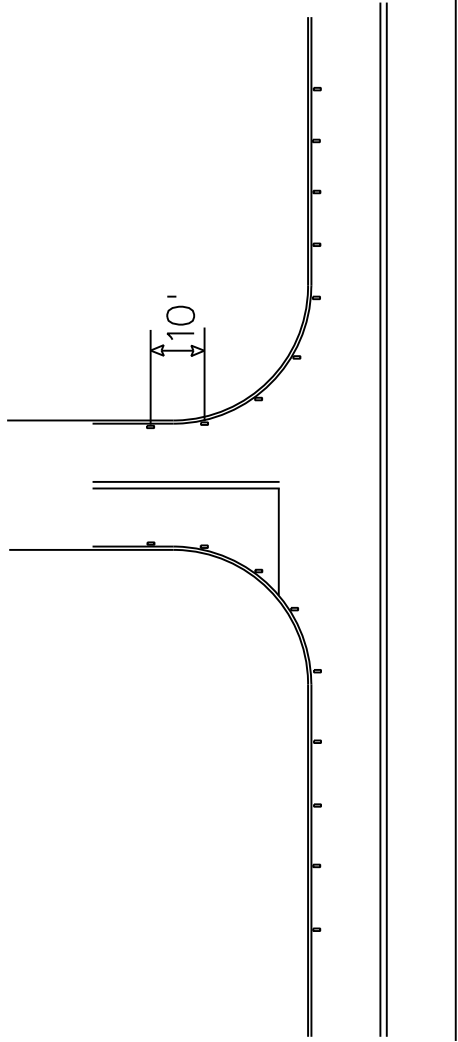
DATE: _____

TYPICAL STRIPING FOR SIMPLE INTERSECTION AT LOCAL ROADS



STATE	PROJECT NO.
MISS.	

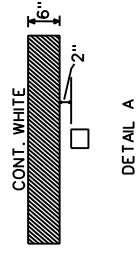
TYPICAL FOR RAISED PAVEMENT MARKERS
PLACED ON SIDE ROAD RADIUS



NOTE 1. MARKERS SHALL BE PLACED EVERY 10 FEET.

NOTE 2. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.

NOTE 3. MARKERS SHALL BE HIGH PERFORMANCE TWO WAY CLEAR.



MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
2-LANE	
RAISED PAVEMENT MARKERS	
PLACED ON SIDE ROADS	
WORKING NUMBER	
SHEET NUMBER	
FILE NAME	
ISSUE 12/11	CHECKED DATE
BY	REVISION

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 113

CODE: (SP)

DATE: 04/18/2017

SUBJECT: Tack Coat

Bidders are advised that in addition to the products listed on the Department's APL as referenced in Subsection 401.03.1.2 on page 256, the Contractor may use one of the following as a tack coat.

- CSS-1
- CSS-1h
- SS-1
- SS-1h

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

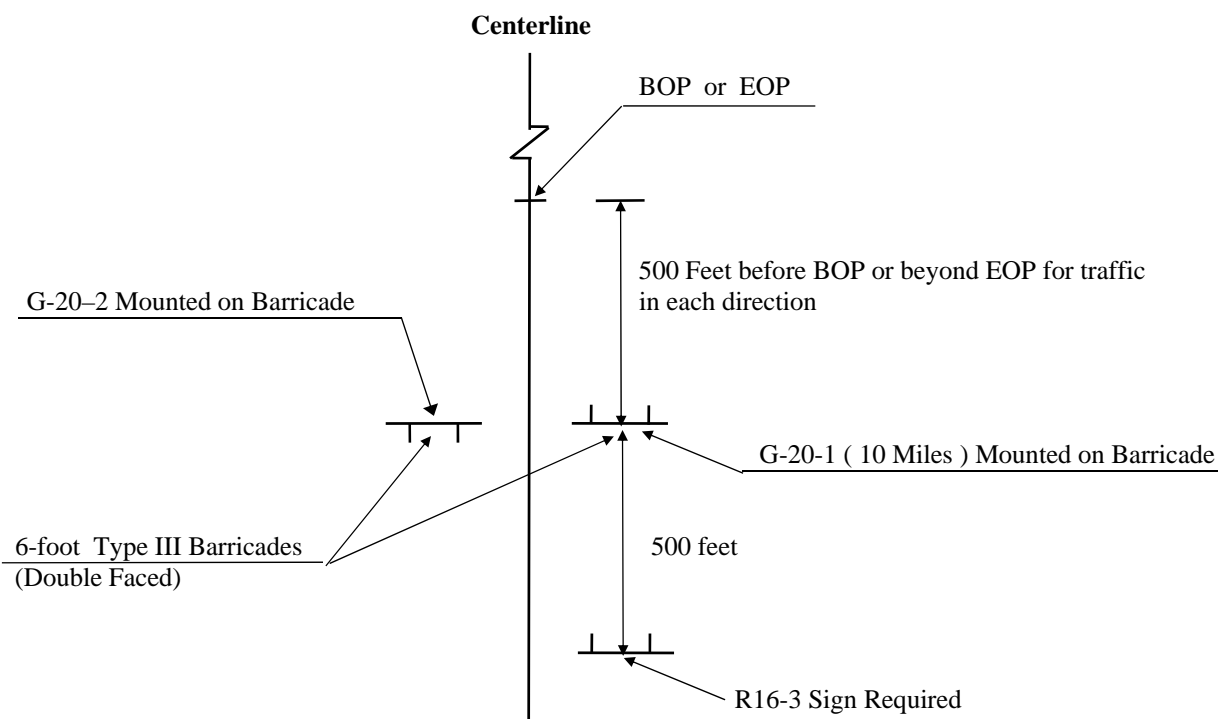
SUPPLEMENT TO SPECIAL PROVISION NO. 907-618-3

DATE: 03/22/2017

PROJECT: MP-6000-21(257) 306148301 & 302 - Greene County

After the first paragraph of Subsection 907-618.01.2 on page 1, add the following.

Additional signs will be required as follows.



ADDITIONAL TRAFFIC CONTROL SIGNS REQUIRED:

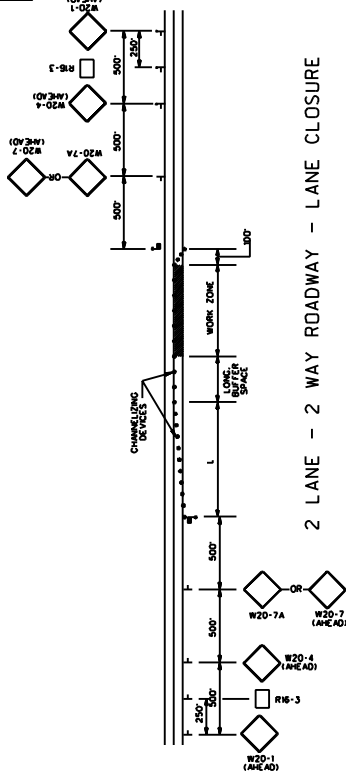
- 13 - W20-1 "AHEAD" signs required. One (1) W20-1 "AHEAD" sign is required at each local road or street entering the project.
- 88 - R4-1 "DO NOT PASS" signs required.
- 46 - R4-2 "PASS WITH CARE" signs required.
- 34 - W14-3 "NO PASSING ZONE" signs required.
- 12 - R16-3 "SPEEDING FINES DOUBLED" signs required.

R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE", and W14-3 "NO PASSING ZONE" signs are required in accordance with Subsection 618.03.3, this drawing, and as specified in the Manual on Uniform Traffic Control Devices. If no passing zones are 1,000 feet or more, additional "DO NOT PASS" signs shall be installed.

R16-3 signs shall be spaced in accordance with sheet titled "Location of R16-3 Signs".

All Construction signing is included in the bid for Pay Item 618-A, Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except for R16-3, R4-1 and R4-2 signs which shall be black legend and border on white background.

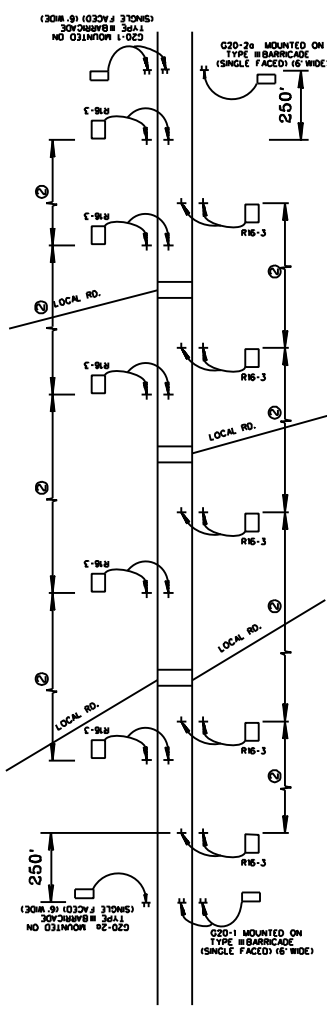
STATE PROJECT NO.
MISS.



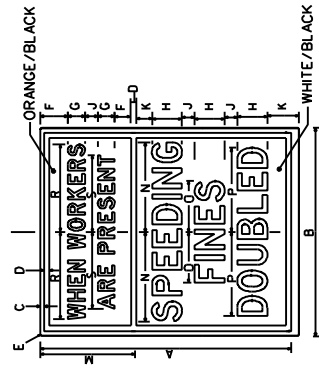
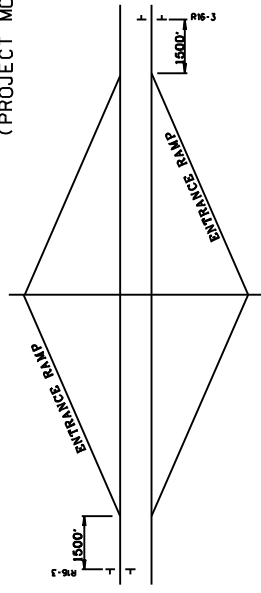
2 LANE - 2 WAY ROADWAY - LANE CLOSURE

DIVIDED HIGHWAY
(PROJECTS LESS THAN 1 MILE LENGTH)

- NOTES
- 1. R16-3 SIGN TO BE PLACED AS SHOWN OR AS DIRECTED BY THE ENGINEER.
 - 2. R16-3 SIGN SHALL BE SPACED AT A MAXIMUM OF 2 MILES THROUGHOUT LENGTH OF PROJECT.



DIVIDED HIGHWAY SHOWN
(2 LANE - 2 WAY ROADWAY SIMILAR)
(PROJECT MORE THAN 1 MILE LENGTH)



SIZE	DIMENSIONS (INCHES)												
S10	A	B	C	D	E	F	G	H	I	J	K	L	M
	60	48	3/4	1/4	3/4	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2
	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2
S10	A	B	C	D	E	F	G	H	I	J	K	L	M
	48	36	3/4	1/4	3/4	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2
	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2	1 1/2

48" x 60" (INTERSTATE USE)
36" x 48" (ALL OTHER HIGHWAYS)

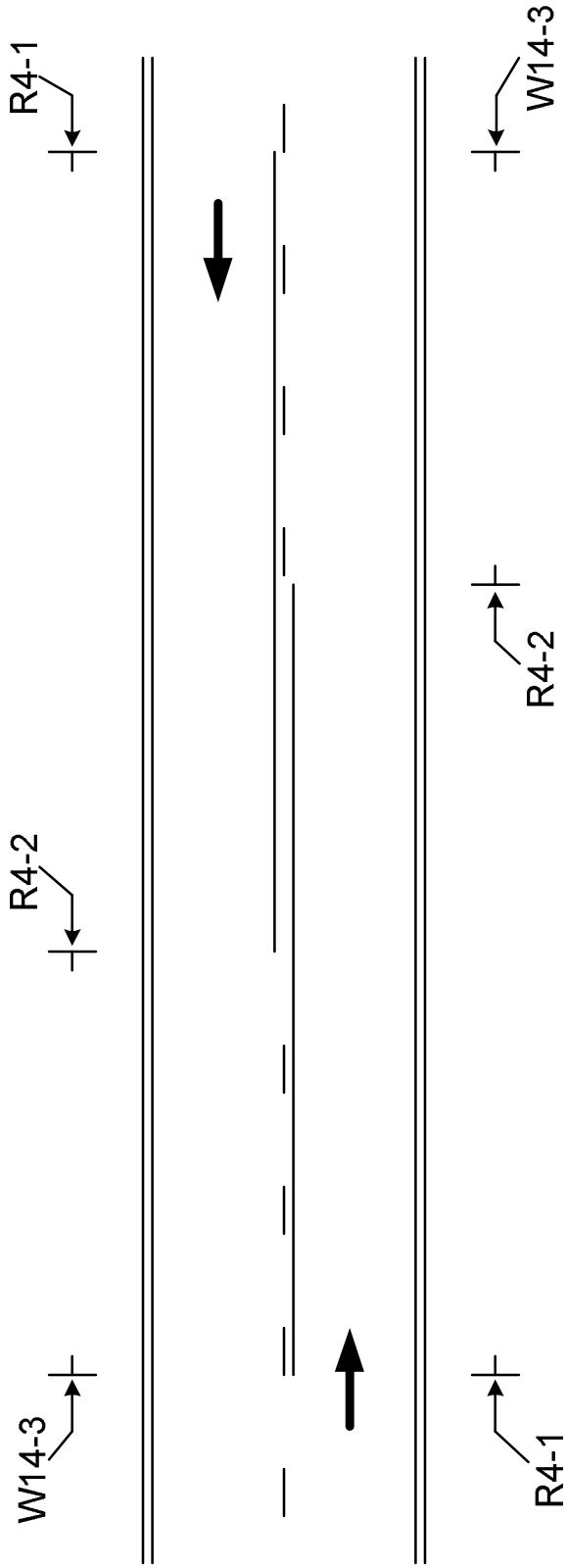
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

LOCATION OF R16-3 SIGNS

REVISION	BY	DATE

WORKING NUMBER
SHEET NUMBER

FILE NAME: SPEED_SIGN_DETAIL.DWG
CHECKED: DATE: 02/08/09



The W14-3, No Passing Zone sign, shall be placed on the left side of the road at the beginning of each no passing zone.

The R4-1, Do Not Pass signs, shall be placed on the right side of the road at the beginning of the no passing zone. Additional R4-1 signs shall be placed right and left in increments of 750 to 1000 feet throughout the length of the no passing zone.

The R4-2, Pass With Care sign, shall be placed on the right side of the road at the end of the no passing zone.

The R4-1, R4-2 and W14-3 signs are to be used when standard pavement markings are not in place. The signs may also be used to emphasize pavement markings.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-618-3

CODE: (SP)

DATE: 01/17/2017

SUBJECT: Additional Signing Requirements

Section 618, Maintenance of Traffic and Traffic Control Plan, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-618.01.2--Traffic Control Plan. At the end of Subsection 618.01.2 on page 441, add the following:

For compliance with the traffic control plan, the Contractor will be required to install and maintain construction signs at various locations throughout the project. Payment for these signs will be included in the price bid for pay item no. 618-A, Maintenance of Traffic per lump sum.

SECTION 905 - PROPOSAL

Date _____

Mississippi Transportation Commission
Jackson, Mississippi

Sirs: The following proposal is made on behalf of _____
_____ of _____

for constructing the following designated project(s) within the time(s) hereinafter specified.

The plans are composed of drawings and blue prints on file in the offices of the Mississippi Department of Transportation, Jackson, Mississippi.

The Specifications are the current Standard Specifications of the Mississippi Department of Transportation approved by the Federal Highway Administration, except where superseded or amended by the plans, Special Provisions and Notice(s) to Bidders attached hereto and made a part thereof.

I (We) certify that I (we) possess a copy of said Standard and any Supplemental Specifications.

Evidence of my (our) authority to submit the Proposal is hereby furnished. The proposal is made without collusion on the part of any person, firm or corporation. I (We) certify that I (we) have carefully examined the Plans, the Specifications, including the Special Provisions and Notice(s) to Bidders, herein, and have personally examined the site of the work. On the basis of the Specifications, Special Provisions, Notice(s) to Bidders, and Plans, I (we) propose to furnish all necessary machinery, tools, apparatus and other means of construction and do all the work and furnish all the materials in the manner specified. I (We) understand that the quantities mentioned herein are approximate only and are subject to either increase or decrease, and hereby propose to perform any increased or decreased quantities of work at the unit prices bid, in accordance with the above.

I (We) acknowledge that this proposal will be found irregular and/or non-responsive unless a certified check, cashier's check, or Proposal Guaranty Bond in the amount as required in the Advertisement (or, by law) is submitted electronically with the proposal or is delivered to the Contract Administration Engineer prior to the bid opening time specified in the advertisement.

INSTRUCTION TO BIDDERS: Alternate and Optional Items on Bid Schedule.

1. Two or more items entered opposite a single unit quantity WITHOUT DEFINITE DESIGNATION AS "ALTERNATE ITEMS" are considered as "OPTIONAL ITEMS". Bidders may or may not indicate on bids the Optional Item proposed to be furnished or performed WITHOUT PREJUDICE IN REGARD TO IRREGULARITY OF BIDS.
2. Items classified on the bid schedule as "ALTERNATE ITEMS" and/or "ALTERNATE TYPES OF CONSTRUCTION" must be preselected and indicated on bids. However, "Alternate Types of Construction" may include Optional Items to be treated as set out in Paragraph 1, above.
3. Optional items not preselected and indicated on the bid schedule MUST be designated in accordance with Subsection 102.06 prior to or at the time of execution of the contract.
4. Optional and Alternate items designated must be used throughout the project.

I (We) further propose to perform all "force account or extra work" that may be required of me (us) on the basis provided in the Specifications and to give such work my (our) personal attention in order to see that it is economically performed.

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) shall submit electronically with our proposal or deliver prior to the bid opening time a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
_____ Secretary	_____ Address
_____ Treasurer	_____ Address

The following is my (our) itemized proposal.

Overlay approximately 2 miles of SR 57 from Chickasawhay River Bridge to SR 63/57 4-Lane, & approximately 6 miles of SR 594 from SR 63/57 4-Lane to Alabama State Line, known as State Project Nos. MP-6000-21(257) / 306148301 & 302 in Greene County.

Line no.	Item Code	Adj Code	Quantity	Units	Description[Fixed Unit Price]
Roadway Items					
0010	202-B188		1,441	Square Yard	Removal of Pavement, All Types and Depths
0020	202-B240		970	Linear Feet	Removal of Traffic Stripe
0030	304-F003	(GT)	1,180	Ton	Size 825B Crushed Stone Base
0040	403-A005	(BA1)	508	Ton	19-mm, MT, Asphalt Pavement
0050	403-B011	(BA1)	5,000	Ton	9.5-mm, MT, Asphalt Pavement, Leveling
0060	406-A002		4,681	Square Yard	Cold Milling of Bituminous Pavement, All Depths
0070	407-A001	(A2)	24,656	Gallon	Asphalt for Tack Coat
0080	411-A001	(BA1)	5,431	Ton	Ultra Thin Asphalt Pavement
0090	618-A001		1	Lump Sum	Maintenance of Traffic
0100	618-B001		2	Square Feet	Additional Construction Signs (\$10.00)
0110	619-A1001		32	Mile	Temporary Traffic Stripe, Continuous White
0120	619-A2001		18	Mile	Temporary Traffic Stripe, Continuous Yellow
0130	619-A4002		7	Mile	Temporary Traffic Stripe, Skip Yellow
0140	619-A5001		24,721	Linear Feet	Temporary Traffic Stripe, Detail
0150	619-A6001		622	Square Feet	Temporary Traffic Stripe, Legend
0160	619-A6002		148	Linear Feet	Temporary Traffic Stripe, Legend
0170	620-A001		1	Lump Sum	Mobilization
0180	626-C002		17	Mile	6" Thermoplastic Double Drop Edge Stripe, Continuous White
0190	626-D003		4	Mile	6" Thermoplastic Traffic Stripe, Skip Yellow
0200	626-E004		9	Mile	6" Thermoplastic Traffic Stripe, Continuous Yellow
0210	626-G002		5,215	Linear Feet	Thermoplastic Detail Stripe, White
0220	626-G003		20,540	Linear Feet	Thermoplastic Detail Stripe, Yellow
0230	626-H004		945	Square Feet	Thermoplastic Legend, White
0240	626-H005		148	Linear Feet	Thermoplastic Legend, White
0250	627-J001		280	Each	Two-Way Clear Reflective High Performance Raised Markers
0260	627-K001		45	Each	Red-Clear Reflective High Performance Raised Markers
0270	627-L001		1,690	Each	Two-Way Yellow Reflective High Performance Raised Markers
0280	630-G005		24	Each	Type 3 Object Markers, OM-3R or OM-3L, Post Mounted
0290	630-G006		24	Each	Type 3 Object Markers, OM-3R or OM-3L, 2 Markers Per Post, Post Mounted

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

CONDITIONS FOR COMBINATION BID

If a bidder elects to submit a combined bid for two or more of the contracts listed for this month's letting, the bidder must complete and execute these sheets of the proposal in each of the individual proposals to constitute a combination bid. In addition to this requirement, each individual contract shall be completed, executed and submitted in the usual specified manner.

Failure to execute this Combination Bid Proposal in each of the contracts combined will be just cause for each proposal to be received and evaluated as a separate bid.

It is understood that the Mississippi Transportation Commission not only reserves the right to reject any and all proposals, but also the right to award contracts upon the basis of lowest separate bids or combination bids most advantageous to the State.

It is further understood and agreed that the Combination Bid Proposal is for comparison of bids only and that each contract shall operate in every respect as a separate contract in accordance with its proposal and contract documents.

I (We) agree to complete each contract on or before its specified completion date.

COMBINATION BID PROPOSAL

This proposal is tendered as one part of a Combination Bid Proposal utilizing option ___* of Subsection 102.11 on the following contracts:

* Option to be shown as either (a), (b), or (c).

	<u>Project No.</u>	<u>County</u>	<u>Project No.</u>	<u>County</u>
1.	_____	_____	6.	_____
2.	_____	_____	7.	_____
3.	_____	_____	8.	_____
4.	_____	_____	9.	_____
5.	_____	_____	10.	_____

(a) If Combination A has been selected, your Combination Bid is complete.

(b) If Combination B has been selected, then complete the following page.

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
1.					
2.					
3.					
4.					
5.					
6.					
7.					
8.					

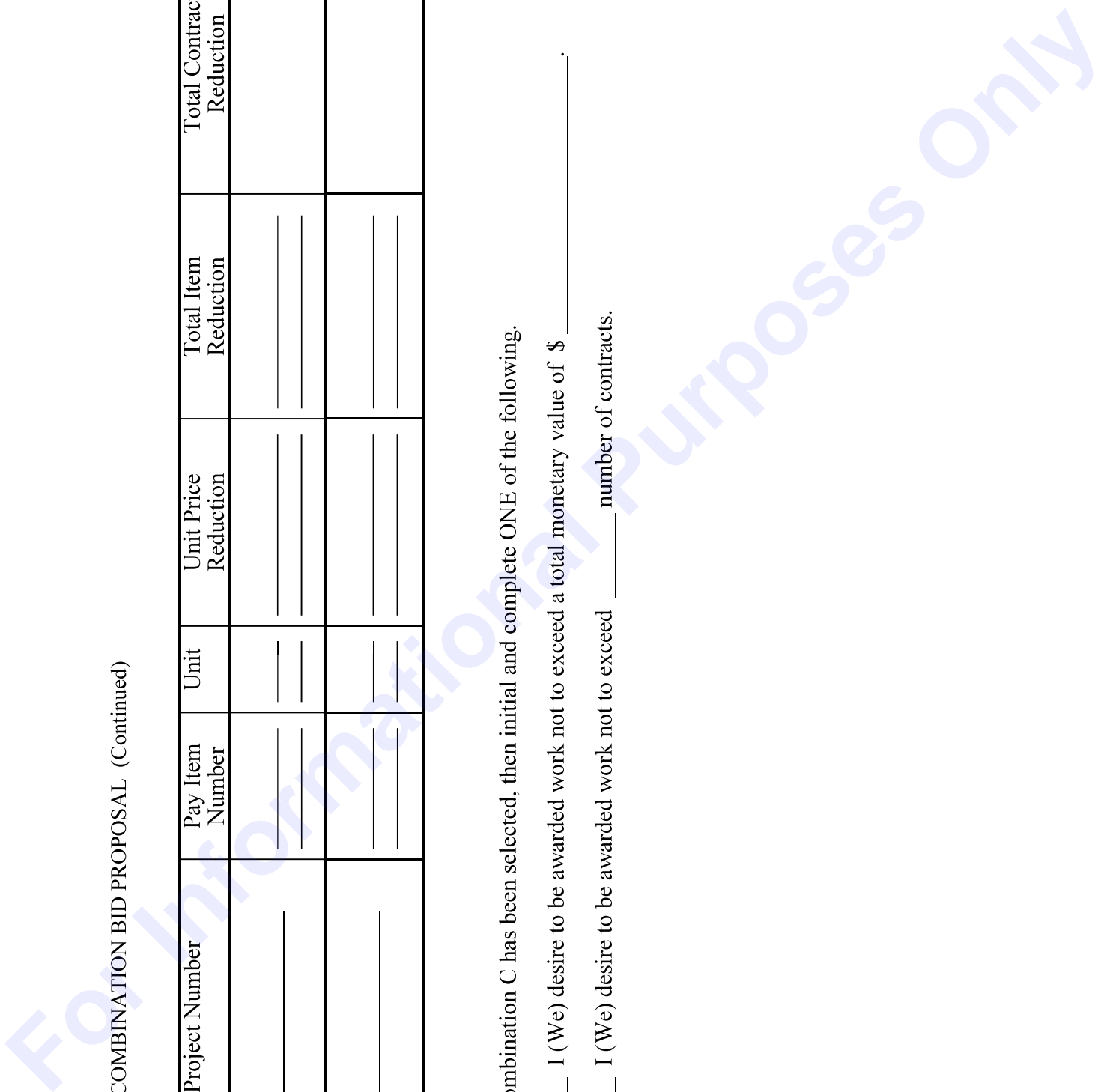
For Informational Purposes Only

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
9.					
10.					

(c) If Combination C has been selected, then initial and complete ONE of the following.

- _____ I (We) desire to be awarded work not to exceed a total monetary value of \$ _____.
- _____ I (We) desire to be awarded work not to exceed _____ number of contracts.



TO: EXECUTIVE DIRECTOR, MISSISSIPPI DEPARTMENT OF TRANSPORTATION
JACKSON, MISSISSIPPI

CERTIFICATE

If awarded this contract, I (we) contemplate that portions of the contract will be sublet. I (we) certify that those subcontracts which are equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

I (we) agree that this notification of intent DOES NOT constitute APPROVAL of the subcontracts.

_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)

NOTE: Failure to complete the above DOES NOT preclude subsequent subcontracts. Subsequent subcontracts, if any, equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

Contractor _____

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
CERTIFICATION

I, _____,
(Name of person signing bid)

individually, and in my capacity as _____ of
(Title of person signing bid)

(Name of Firm, partnership, or Corporation)

do hereby certify under penalty of perjury under the laws of the United States and the State of Mississippi

that _____, Bidder
(Name of Firm, Partnership, or Corporation)

on Project No. **MP-6000-21(257)/ 306148301000 & MP-6000-21(257)/ 306148302000**

in **Greene** _____ County(ies), Mississippi, has not either directly or indirectly entered into any agreement, participated in any collusion; or otherwise taken any action in restraint of free competitive bidding in connection with this contract; nor have any of its corporate officers or principal owners.

Except as noted hereafter, it is further certified that said legal entity and its corporate officers, principal owners, managers, auditors and others in a position of administering federal funds are not currently under suspension, debarment, voluntary exclusion or determination of ineligibility; nor have a debarment pending; nor been suspended, debarred, voluntarily excluded or determined ineligible within the past three years by the Mississippi Transportation Commission, the State of Mississippi, any other State or a federal agency; nor been indicted, convicted or had a civil judgment rendered by a court of competent jurisdiction in any matter involving fraud or official misconduct within the past three years.

Do exceptions exist and are made a part thereof? Yes / No

Any exceptions shall address to whom it applies, initiating agency and dates of such action.

Note: Exceptions will not necessarily result in denial of award but will be considered in determining bidder responsibility. Providing false information may result in criminal prosecution or administrative sanctions.

All of the foregoing is true and correct.

(1/2016 S)

SECTION 902

CONTRACT FOR MP-6000-21(257)/ 306148301000 & MP-6000-21(257)/ 306148302000

LOCATED IN THE COUNTY(IES) OF Greene

STATE OF MISSISSIPPI,
COUNTY OF HINDS

This contract entered into by and between the Mississippi Transportation Commission on one hand, and the undersigned contractor, on the other witnesseth;

That, in consideration of the payment by the Mississippi Transportation Commission of the prices set out in the proposal hereto attached, to the undersigned contractor, such payment to be made in the manner and at the time of times specified in the specifications and the special provisions, if any, the undersigned contractor hereby agrees to accept the prices stated in the proposal in full compensation for the furnishing of all materials and equipment and the executing of all the work contemplated in this contract.

It is understood and agreed that the advertising according to law, the Advertisement, the instructions to bidders, the proposal for the contract, the specifications, the revisions of the specifications, the special provisions, and also the plans for the work herein contemplated, said plans showing more particularly the details of the work to be done, shall be held to be, and are hereby made a part of this contract by specific reference thereto and with like effect as if each and all of said instruments had been set out fully herein in words and figures.

It is further agreed that for the same consideration the undersigned contractor shall be responsible for all loss or damage arising out of the nature of the work aforesaid; or from the action of the elements and unforeseen obstructions or difficulties which may be encountered in the prosecution of the same and for all risks of every description connected with the work, exceptions being those specifically set out in the contract; and for faithfully completing the whole work in good and workmanlike manner according to the approved Plans, Specifications, Special Provisions, Notice(s) to Bidders and requirements of the Mississippi Department of Transportation.

It is further agreed that the work shall be done under the direct supervision and to the complete satisfaction of the Executive Director of the Mississippi Department of Transportation, or his authorized representatives, and when Federal Funds are involved subject to inspection at all times and approval by the Federal Highway Administration, or its agents as the case may be, or the agents of any other Agency whose funds are involved in accordance with those Acts of the Legislature of the State of Mississippi approved by the Governor and such rules and regulations issued pursuant thereto by the Mississippi Transportation Commission and the authorized Federal Agencies.

The Contractor agrees that all labor as outlined in the Special Provisions may be secured from list furnished by

It is agreed and understood that each and every provision of law and clause required by law to be inserted in this contract shall be deemed to be inserted herein and this contract shall be read and enforced as though it were included herein, and, if through mere mistake or otherwise any such provision is not inserted, then upon the application of either party hereto, the contract shall forthwith be physically amended to make such insertion.

The Contractor agrees that he has read each and every clause of this Contract, and fully understands the meaning of same and that he will comply with all the terms, covenants and agreements therein set forth.

Witness our signatures this the ___ day of _____, _____.

Contractor(s)

By _____

MISSISSIPPI TRANSPORTATION COMMISSION

Title _____

By _____

Signed and sealed in the presence of:
(names and addresses of witnesses)

Executive Director

Secretary to the Commission

Award authorized by the Mississippi Transportation Commission in session on the ___ day of _____, _____, Minute Book No. _____, Page No. _____.

SECTION 903
PERFORMANCE AND PAYMENT BOND

CONTRACT BOND FOR: MP-6000-21(257)/ 306148301000 & MP-6000-21(257)/ 306148302000

LOCATED IN THE COUNTY(IES) OF: Greene

STATE OF MISSISSIPPI,
COUNTY OF HINDS

Know all men by these presents: that we, _____

(Contractor)

Principal, a _____

residing at _____ in the State of _____

and _____

(Surety)
residing at _____ in the State of _____,

authorized to do business in the State of Mississippi, under the laws thereof, as surety, effective as of the contract date

shown below, are held and firmly bound unto the State of Mississippi in the sum of _____

(\$ _____) Dollars, lawful money of the United States of America, to be paid to it for which

payment well and truly to be made, we bind ourselves, our heirs, administrators, successors, or assigns jointly and severally by these presents.

The conditions of this bond are such, that whereas the said _____

principal, has (have) entered into a contract with the Mississippi Transportation Commission, bearing the date of

_____ day of _____ A.D. _____ hereto annexed, for the construction of certain projects(s) in

the State of Mississippi as mentioned in said contract in accordance with the Contract Documents therefor, on file in the

offices of the Mississippi Department of Transportation, Jackson, Mississippi.

Now therefore, if the above bounden _____

in all things shall stand to and abide by and well and truly observe, do keep and perform all and singular the terms, covenants, conditions, guarantees and agreements in said contract, contained on his (their) part to be observed, done, kept and performed and each of them, at the time and in the manner and form and furnish all of the material and equipment specified in said contract in strict accordance with the terms of said contract which said plans, specifications and special provisions are included in and form a part of said contract and shall maintain the said work contemplated until its final completion and acceptance as specified in Subsection 109.11 of the approved specifications, and save harmless said Mississippi Transportation Commission from any loss or damage arising out of or occasioned by the negligence, wrongful or criminal act, overcharge, fraud, or any other loss or damage whatsoever, on the part of said principal (s), his (their) agents, servants, or employees in the performance of said work or in any manner connected therewith, and shall be liable and responsible in a civil action instituted by the State at the instance of the Mississippi Transportation Commission or any officer of the State authorized in such cases, for double any amount in money or property, the State may lose or be overcharged or otherwise defrauded of, by reason of wrongful or criminal act, if any, of the Contractor(s), his (their) agents or employees, and shall promptly pay the said agents, servants and employees and all persons furnishing labor, material, equipment or supplies therefor, including premiums incurred, for Surety Bonds, Liability Insurance, and Workmen's Compensation Insurance; with the additional obligation that such Contractor shall promptly make payment of all taxes, licenses, assessments, contributions, damages,

any liquidated damages which may arise prior to any termination of said principal's contract, any liquidated damages which may arise after termination of the said principal's contract due to default on the part of said principal, penalties and interest thereon, when and as the same may be due this state, or any county, municipality, board, department, commission or political subdivision: in the course of the performance of said work and in accordance with Sections 31-5-51 et seq. Mississippi Code of 1972, and other State statutes applicable thereto, and shall carry out to the letter and to the satisfaction of the Executive Director of the Mississippi Department of Transportation, all, each and every one of the stipulations, obligations, conditions, covenants and agreements and terms of said contract in accordance with the terms thereof and all of the expense and cost and attorney's fee that may be incurred in the enforcement of the performance of said contract, or in the enforcement of the conditions and obligations of this bond, then this obligation shall be null and void, otherwise to be and remain in full force and virtue.

_____	_____
(Contractors) Principal	Surety
By _____	By _____
	(Signature) Attorney in Fact
	Address _____

Title _____	_____
(Contractor's Seal)	(Printed) MS Agent

	(Signature) MS Agent
	Address _____

	(Surety Seal)

	Mississippi Insurance ID Number



BID BOND

KNOW ALL MEN BY THESE PRESENTS, that we _____
Contractor

Address

City, State ZIP

As principal, hereinafter called the Principal, and _____
Surety

a corporation duly organized under the laws of the state of _____

as Surety, hereinafter called the Surety, are held and firmly bound unto State of Mississippi, Jackson, Mississippi

As Obligee, hereinafter called Obligee, in the sum of **Five Per Cent (5%) of Amount Bid**

Dollars(\$ _____)

for the payment of which sum will and truly to be made, the said Principal and said Surety, bind ourselves, our heirs, executors, administrators, successors and assigns, jointly and severally, firmly by these presents.

WHEREAS, the Principal has submitted a bid for **Overlay approximately 2 miles of SR 57 from Chickasawhay River Bridge to SR 63/57 4-Lane, & approximately 6 miles of SR 594 from SR 63/57 4-Lane to Alabama State Line, known as State Project Nos. MP-6000-21(257) / 306148301 & 302 in Greene County.**

NOW THEREFORE, the condition of this obligation is such that if the aforesaid Principal shall be awarded the contract, the said Principal will, within the time required, enter into a formal contract and give a good and sufficient bond to secure the performance of the terms and conditions of the contract, then this obligation to be void; otherwise the Principal and Surety will pay unto the Obligee the difference in money between the amount of the bid of the said Principal and the amount for which the Obligee legally contracts with another party to perform the work if the latter amount be in excess of the former, but in no event shall liability hereunder exceed the penal sum hereof.

Signed and sealed this _____ day of _____, 20__

(Witness)

(Principal) (Seal)

By: _____
(Name) (Title)

(Witness)

(Surety) (Seal)

By: _____
(Attorney-in-Fact)

(MS Agent)

Mississippi Insurance ID Number

