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17 -



SM No. CMP6011550221

# PROPOSAL AND CONTRACT DOCUMENTS

## FOR THE CONSTRUCTION OF

17

Overlay approximately 7 miles of US 11 from South Apple Dr. to Millard, known as State Project No. MP-6011-55(022) / 306147301 in Pearl River County.

Project Completion: Flexible

**(STATE DELEGATED)**

### NOTICE

**BIDDERS MUST PURCHASE A BID PROPOSAL FROM  
MDOT CONTRACT ADMINISTRATION DIVISION TO BID  
THIS PROJECT.**

Electronic addendum updates will be posted on [www.gomdot.com](http://www.gomdot.com)

# SECTION 900

## OF THE CURRENT

### 2017 STANDARD SPECIFICATIONS

### FOR ROAD AND BRIDGE CONSTRUCTION

JACKSON, MISSISSIPPI

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
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(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET  
OF SECTION 905 AS ADDENDA)

04/26/2017 06:17 AM

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

## SECTION 901 - ADVERTISEMENT

Electronic bids will be received by the Mississippi Transportation Commission at 10:00 o'clock A.M., Tuesday, May 23, 2017, from the Bid Express Service and shortly thereafter publicly read on the Sixth Floor for:

Overlay approximately 7 miles of US 11 from South Apple Dr. to Millard, known as State Project No. MP-6011-55(022) / 306147301 in Pearl River County.

The attention of bidders is directed to the predetermined minimum wage rate set by the U. S. Department of Labor under the Fair Labor Standards Act.

The Mississippi Department of Transportation hereby notifies all bidders that it will affirmatively insure that in any contract entered into pursuant to this advertisement, disadvantaged business enterprises will be afforded full opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of race, color, sex, age, disability, religion or national origin in consideration for an award.

The specifications are on file in the offices of the Mississippi Department of Transportation.

Bid proposals must be purchased online at <https://shopmdot.ms.gov>. Specimen proposals may be viewed and downloaded online at no cost at <http://mdot.ms.gov> or purchased online. Proposals are available at a cost of Ten Dollars (\$10.00) per proposal plus a small convenience fee. Cash or checks will not be accepted as payment.

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

MELINDA L. MCGRATH  
EXECUTIVE DIRECTOR

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 - NOTICE TO BIDDERS NO. 1**

**CODE: (IS)**

**DATE: 03/01/2017**

**SUBJECT: Governing Specifications**

The current (2017) Edition of the Standard Specifications for Road and Bridge Construction adopted by the Mississippi Transportation Commission is made a part hereof fully and completely as if it were attached hereto, except where superseded by special provisions, or amended by revisions of the Specifications contained within this proposal. Copies of the specification book may be purchased from the MDOT Construction Division, or online at [shopmdot/default.aspx?StoreIndex=1](http://shopmdot/default.aspx?StoreIndex=1).

A reference in any contract document to controlling requirements in another portion of the contract documents shall be understood to apply equally to any revision or amendment thereof included in the contract.

In the event the plans or proposal contain references to the 2004 Edition of the Standard Specifications for Road and Bridge Construction, it is to be understood that such references shall mean the comparable provisions of the 2017 Edition of the Standard Specifications.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 3**

**CODE: (SP)**

**DATE: 01/17/2017**

**SUBJECT: Final Clean-Up**

Immediately prior to final inspection for release of maintenance, the Contractor shall pick up, load, transport and properly dispose of all litter from the entire highway right-of-way that is within the termini of the project.

Litter shall include, but not be limited to, solid wastes such a glass, paper products, tires, wood products, metal, synthetic materials and other miscellaneous debris.

Litter removal is considered incidental to other items of work and will not be measured for separate payment.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 9**

**CODE: (IS)**

**DATE: 03/01/2017**

**SUBJECT: Federal Bridge Formula**

Bidders are hereby advised that the latest revision of Federal Highway Administration Publication No. FHWA-HOP-06-105, **BRIDGE FORMULA WEIGHTS**, dated August 2006, is made a part of this contract when applicable.

Prior to the preconstruction conference, the Contractor shall advise the Engineer, in writing, what materials, if any, will be delivered to the jobsite via Interstate route(s).

Copies of the **BRIDGE FORMULA WEIGHTS** publication may be obtained by contacting:

Federal Highway Administration  
400 7<sup>th</sup> Street, SW  
Washington, DC 20590  
(202) 366-2212

or

[http://www.ops.fhwa.dot.gov/Freight/publications/brdg\\_frm\\_wgths/bridge\\_formula\\_all\\_rev.pdf](http://www.ops.fhwa.dot.gov/Freight/publications/brdg_frm_wgths/bridge_formula_all_rev.pdf)

An on line **BRIDGE FORMULA WEIGHTS CALCULATOR** is available at

[http://ops.fhwa.dot.gov/freight/sw/brdgcalc/calc\\_page.htm](http://ops.fhwa.dot.gov/freight/sw/brdgcalc/calc_page.htm)

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 12**

**CODE: (IS)**

**DATE: 03/01/2017**

**SUBJECT: MASH Compliant Devices**

Bidders are hereby advised that the Standard Specifications may require certain traffic control and permanent safety hardware devices to meet the requirements of the Manual for Assessing Safety Hardware (MASH). However, devices meeting the requirements of NCHRP Report 350 will be allowed until the mandatory effective date for MASH compliance. The following table shows the effective dates for MASH compliant devices.

| <b>Device</b>  | <b>Effective Date for MASH Compliance</b> |
|--|---|
| W-beam barriers, cast-in-place concrete barriers   | December 31, 2017                         |
| W-beam terminals   | June 30, 2018                             |
| Cable barriers, cable barrier terminals, crash cushions  | December 31, 2018                         |
| Bridge rails, transitions, all other longitudinal barriers including portable barriers installed permanently, all other terminals, sign supports, all other breakaway hardware | December 31, 2019                         |

Temporary work zone devices, including portable barriers manufactured after December 31, 2019, must have been successfully tested to the 2016 Edition of MASH. Such devices manufactured on or before this date and successfully tested to NCHRP Report 350 or the 2009 Edition of MASH may continue to be used throughout their normal service lives.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 13**

**CODE: (IS)**

**DATE: 03/01/2017**

**SUBJECT: Safety Edge**

Bidders are hereby advised that the Shoulder Wedge (Safety Edge) specified in Section 401, Asphalt Pavements, shall only apply to the top two (2) lifts of asphalt. Open Graded Friction Courses (OGFC) are not to be considered a lift as it pertains to safety edge. Attached is a drawing showing the safety edge. Note that the shoulder dimensions in the bottom two drawings will be less than three feet (3').





**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 – NOTICE TO BIDDERS NO. 16**

**CODE: (SP)**

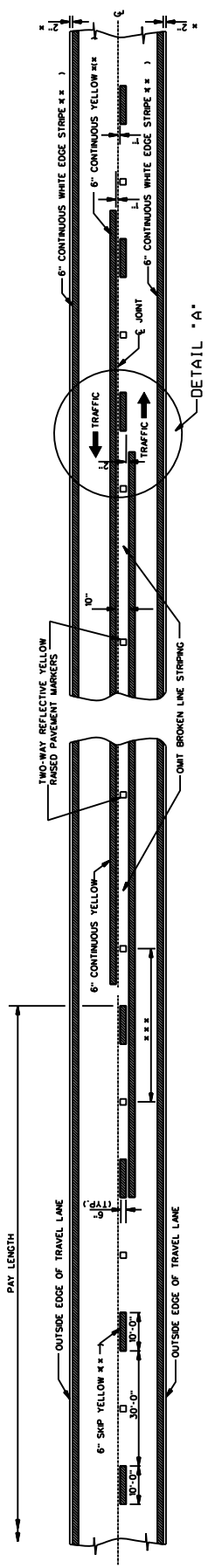
**DATE: 03/01/2017**

**SUBJECT: Standard Drawings**

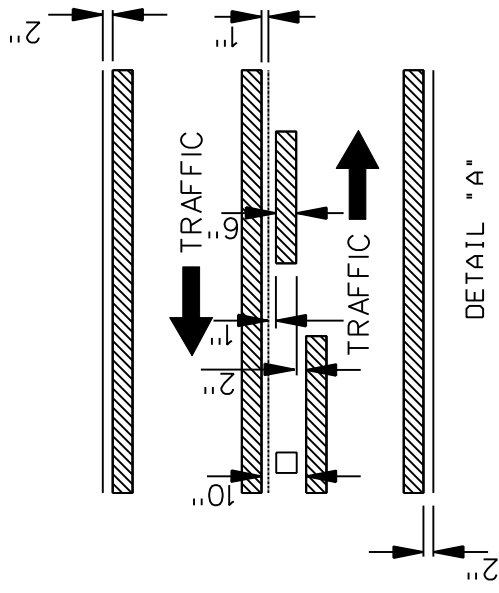
Standard Drawings attached hereto shall govern appropriate items of required work.

Larger copies of Standard Drawings may be purchased from:

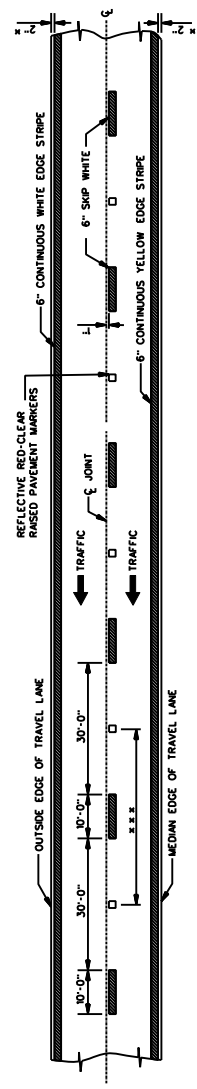
MDOT Plans Print Shop  
MDOT Shop Complex, Building C, Room 114  
2567 North West Street  
P.O. Box 1850  
Jackson, MS 39215-1850  
Telephone: (601) 359-7460  
or FAX: (601) 359-7461  
or e-mail: [plans@mdot.state.ms.us](mailto:plans@mdot.state.ms.us)



TWO-WAY TRAFFIC  
(ASPHALT OR CONCRETE PAVEMENT)



DETAIL "A"



4-LANE WITH ONE-WAY TRAFFIC



GENERAL NOTES:

- \* 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- \*\* 2. EDGE STRIPES SHALL BE SAME MATERIAL AS LANE-LINE STRIPES (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
- \*\*\* 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

| TANGENT SECTIONS   | URBAN AREA (U/Lin) | RURAL AREA (U/Lin) |
|--------------------|--------------------|--------------------|
| HORIZONTAL CURVES  | 40'-0"             | 80'-0"             |
| INTERCHANGE LIMITS | 40'-0"             | 40'-0"             |
|                    | 40'-0"             | 140'-0"            |

- 1. NOTES: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP AND ENDING 1000' BEFORE THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- 4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED SOURCES OF MATERIALS."

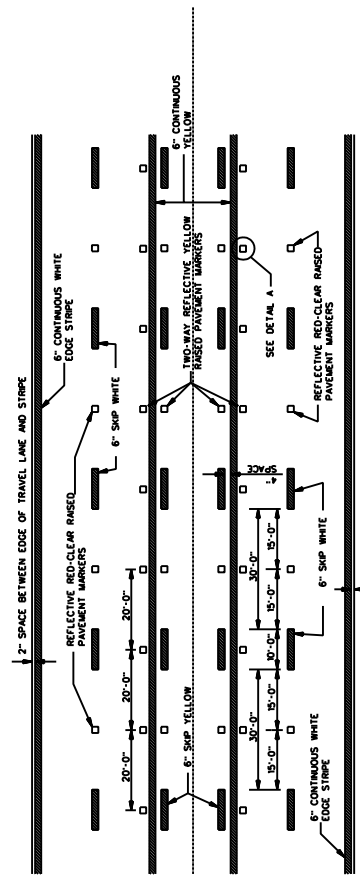
MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

PAVEMENT MARKING  
DETAILS FOR  
2-LANE AND 4-LANE  
DIVIDED HIGHWAYS

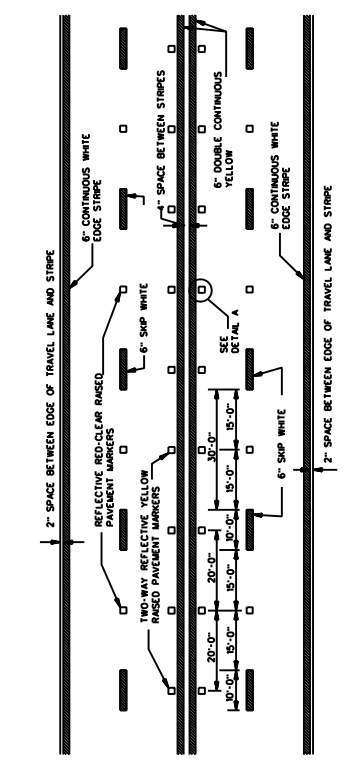
ISSUE DATE: OCTOBER 1, 1998

WORK NUMBER: PM-1  
SHEET NUMBER: 100

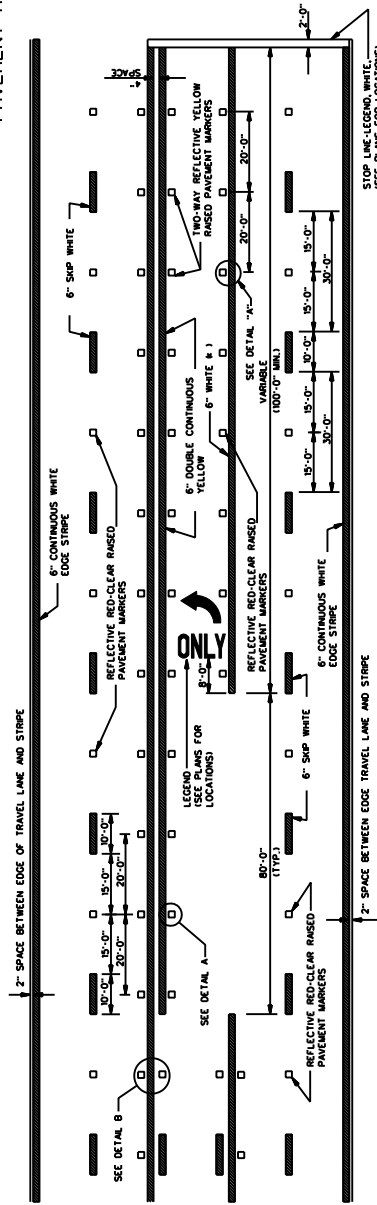
|       |             |
|-------|-------------|
| STATE | PROJECT NO. |
| MISS. |             |



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 5-LANE SECTION

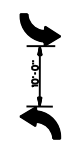


TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION

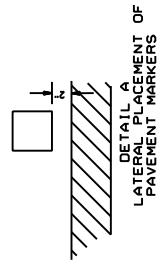


TYPICAL STRIPING AND RAISED PAVEMENT MARKERS AT LEFT TURN LANES

\*NOTE: USE DETAIL STRIPING IF LENGTH  $\leq$  150' AT THIS LOCATION, OTHERWISE USE CONTINUOUS STRIPING.

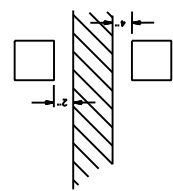


- TYPICAL TWO-WAY ARROW INSTALLATION
1. CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY.
  2. IF SEGMENT IS LESS THAN 300', PLACE ONE SET OF ARROWS IN CENTER OF SEGMENT.
  3. IF SEGMENT IS GREATER THAN 300', PLACE FIRST SET OF ARROWS 50' TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS 1250' O.C.



DETAIL A  
LATERAL PLACEMENT OF PAVEMENT MARKERS

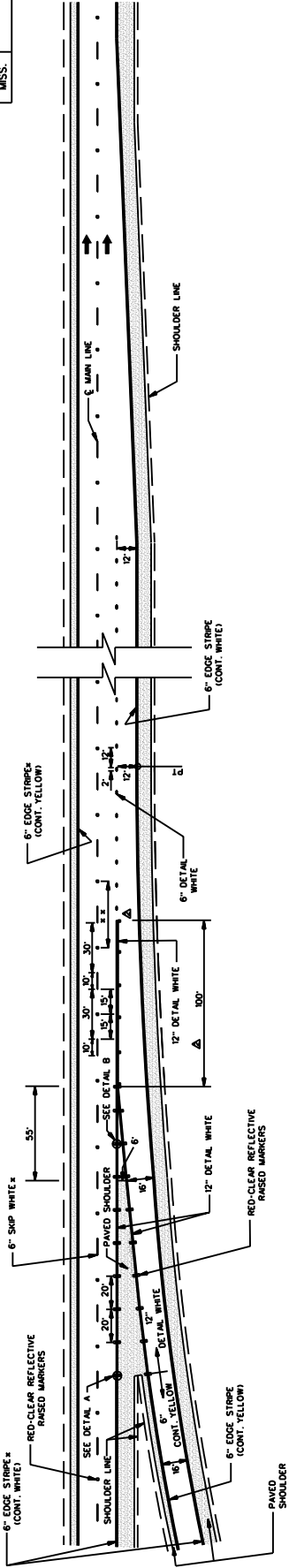
GENERAL NOTE:  
1. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT -APPROVED SOURCES OF MATERIALS.\*



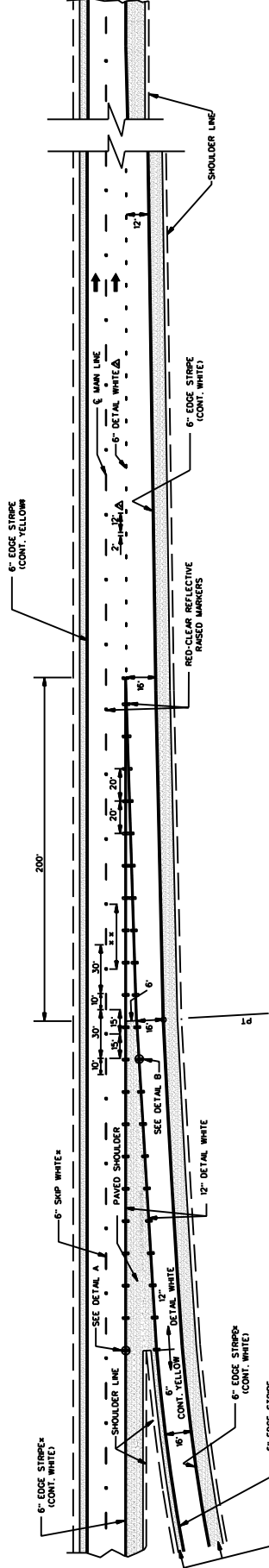
DETAIL B  
LATERAL PLACEMENT OF PAVEMENT MARKERS

|  |            |
|--|------------|
| MISSISSIPPI DEPARTMENT OF TRANSPORTATION |            |
| PAVEMENT MARKING                         |            |
| DETAILS FOR                              |            |
| 4-LANE AND 5-LANE                        |            |
| UNDIVIDED ROADWAYS                       |            |
| DATE                                     | 03/15/16   |
| FILENAME                                 | SDPM-2.DGN |
| DESIGNER                                 | CH2M       |
| CHECKER                                  |            |
| DATE                                     | 03/15/16   |

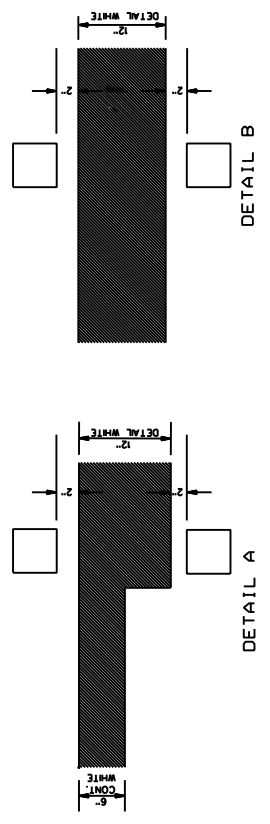
|       |             |
|-------|-------------|
| STATE | PROJECT NO. |
| MISS. |             |



PARALLEL ENTRANCE RAMP



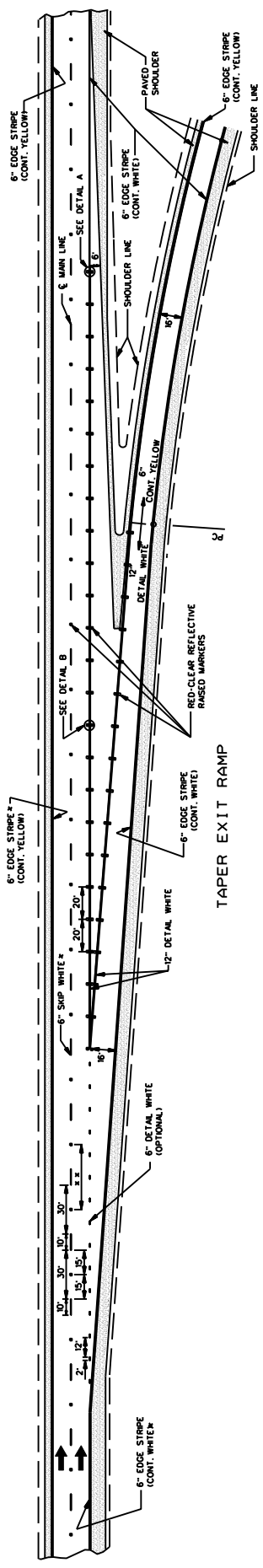
TAPER ENTRANCE RAMP



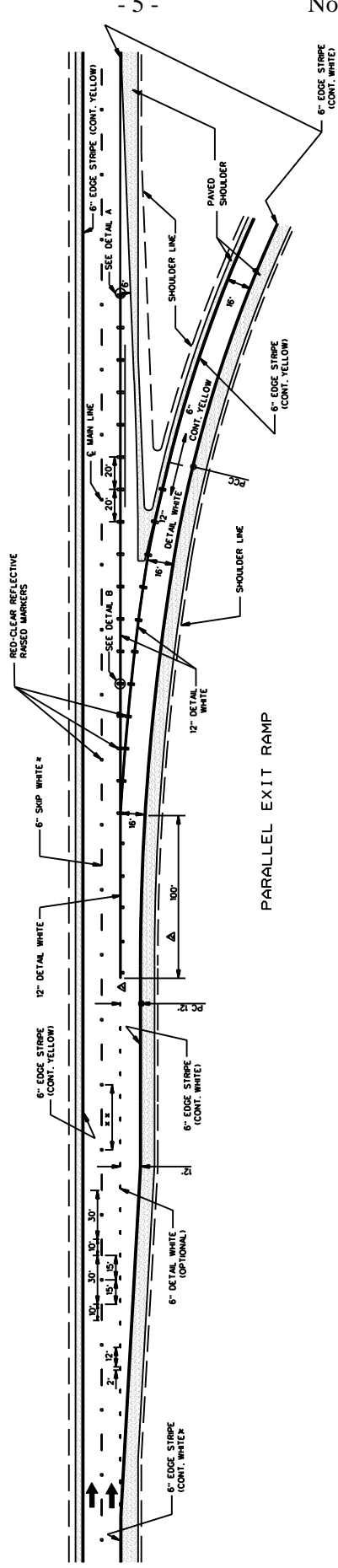
- GENERAL NOTES:
- \*\* 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPES THROUGHOUT THE INTERCHANGE AREA BEGINNING 100' IN THE DIRECTION OF TRAVEL WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELED WAY.
  - \*\* 2. ON THE MAIN FACILITY, PLACE REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS AT A 40' SPACING ON ALL LANE-LINES THROUGHOUT THE INTERCHANGE AREA BEGINNING 100' IN THE DIRECTION OF TRAVEL THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
  - 3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED SOURCES OF MATERIALS."

|   |  |  |  |
|---|--|--|--|
| MISSISSIPPI DEPARTMENT OF TRANSPORTATION<br>ROADWAY DESIGN DIVISION         |  |  | WORKING NUMBER<br>SDPM-3<br>SHEET NUMBER |
| PAVEMENT MARKING<br>DETAILS FOR<br>INTERCHANGE RAMP<br>(PARALLEL AND TAPER) |  |  |  |

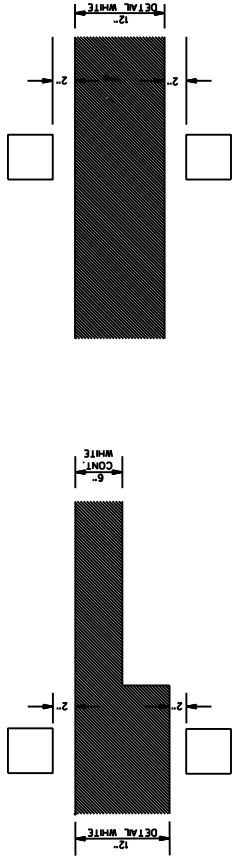
|       |             |
|-------|-------------|
| STATE | PROJECT NO. |
| MISS. |             |



TAPER EXIT RAMP



PARALLEL EXIT RAMP



- GENERAL NOTES:
- # 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LINE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELLED WAY.
  - # 2. ON THE MAINLINE, PLACE REFLECTIVE RED-CLEAR BASED PAVED SHOULDER MARKERS AT 100' INTERVALS THROUGHOUT THE INTERCHANGE AREA BEGINNING 1000' IN ADVANCE IN DIRECTION OF TRAFFIC OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
  - # 3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE MARKERS AS SPECIFIED IN THE MOOT "APPROVED SOURCES OF MATERIALS."

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION

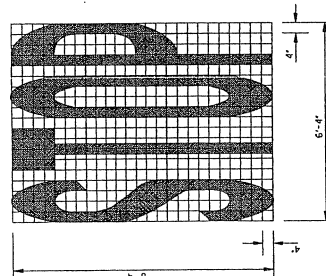
PAVEMENT MARKING  
DETAILS FOR  
INTERCHANGE  
EXIT RAMP  
(PARALLEL AND TAPER)

ISSUE DATE: OCTOBER 1, 1998

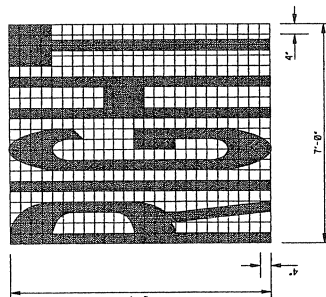
WORKING NUMBER: SDPM-41  
SHEET NUMBER: 13

|                |             |  |  |
|----------------|-------------|--|--|
| STATE<br>MISS. | PROJECT NO. |  |  |
|----------------|-------------|--|--|

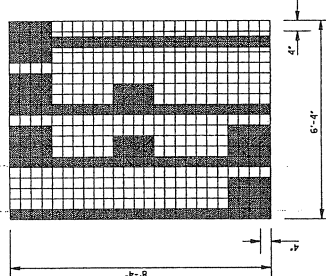
  



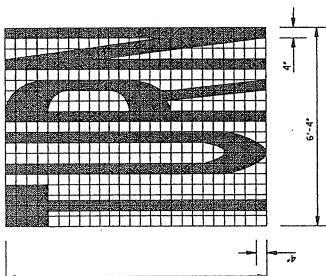
8'-4" x 6'-4"



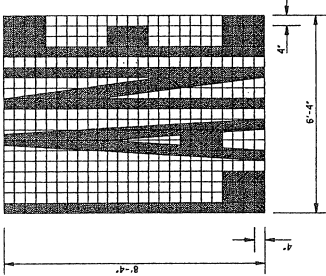
8'-4" x 7'-0"



8'-4" x 6'-4"

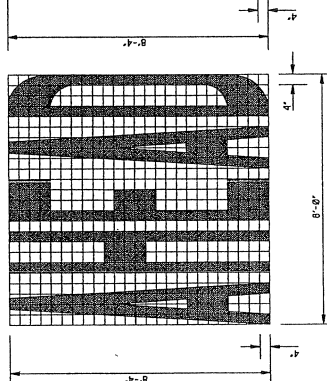


8'-4" x 6'-4"

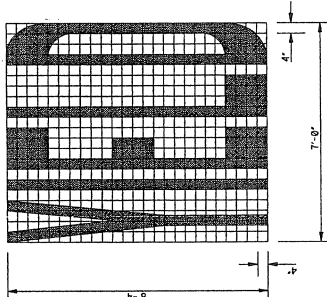


8'-4" x 6'-4"

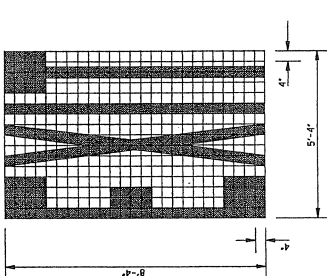
  



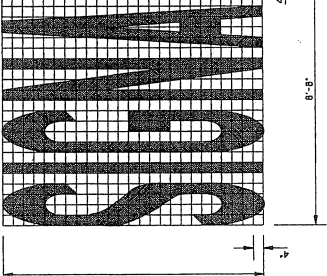
8'-4" x 8'-0"



8'-4" x 7'-0"



8'-4" x 5'-4"



8'-4" x 8'-0"

**GENERAL NOTES:**

- UNLESS OTHERWISE SHOWN ON THE PLANS, ALL PAVEMENT MARKING LEGENDS SHALL BE APPLIED USING HIGH PERFORMANCE POLYURETHANE PAINT.
- TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTORS OF 1/4" OR LESS) AND EXTENDING LETTERS SHALL BE USED FOR ALL LETTERS.
- FOR OTHER DETAILS SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

| LEGEND  | AREA (FT <sup>2</sup> ) |
|---------|-------------------------|
| STOP    | 24.6                    |
| RIGHT   | 35.8                    |
| LEFT    | 35.8                    |
| TRAFFIC | 27.3                    |
| LANE    | 22.7                    |
| AHEAD   | 32.3                    |
| EXIT    | 18.5                    |
| SIGNAL  | 32.5                    |
| SCHOOL  | 35.5                    |

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

**PAVEMENT MARKING  
LEGEND DETAILS**

|                   |  |  |  |  |  |  |  |
|-------------------|--|--|--|--|--|--|--|
| STATE PROJECT NO. |  |  |  |  |  |  |  |
| MISS.             |  |  |  |  |  |  |  |

TURN ARROW

THRU ARROW

COMBINATION ARROW

1-WAY ARROW

**GENERAL NOTES:**

1. UNLESS OTHERWISE SHOWN ON THE PLANS, ALL PAVEMENT MARKING SHALL BE APPLIED USING HIGH PERFORMANCE MATERIALS.
2. TWO HORIZONTAL GAPS CAUSED BY TEMPLATE CONNECTORS OF 1/2" OR LESS AND EXTENDING THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
3. FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM PAVEMENT MARKINGS.
4. PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

| PAY QUANTITIES    |                         |
|-------------------|-------------------------|
| LEGEND/SYMBOL     | AREA (ft <sup>2</sup> ) |
| ONLY              | 22.0                    |
| TURN ARROW        | 18.4                    |
| THRU ARROW        | 21.2                    |
| COMBINATION ARROW | 27.5                    |
| 1-WAY ARROW       | 24.3                    |

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

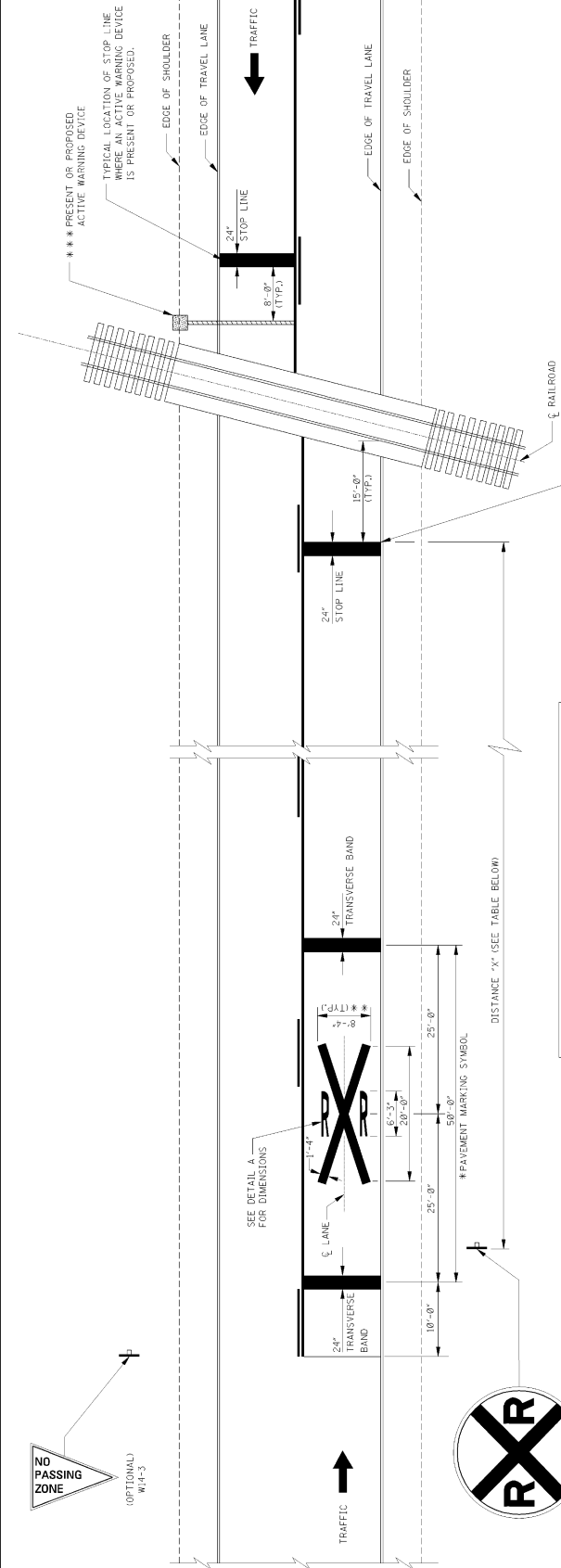
**PAVEMENT MARKING  
LEGEND DETAILS**

WORKING NUMBER  
PM-6

SHEET NUMBER  
125

ISSUE DATE: OCTOBER 1, 1998





\*\*\* PRESENT OR PROPOSED ACTIVE WARNING DEVICE

TYPICAL LOCATION OF STOP LINE WHERE AN ACTIVE WARNING DEVICE IS PRESENT OR PROPOSED.

EDGE OF SHOULDER

EDGE OF TRAVEL LANE

TRAFFIC

24" STOP LINE

8'-0" (TYP)

EDGE OF SHOULDER

EDGE OF TRAVEL LANE

15'-0" (TYP)

24" STOP LINE

24" TRANSVERSE BAND

15'-4" (TYP)

8'-7-1/2" (TYP)

11'-4" (TYP)

6'-3" (TYP)

20'-0" (TYP)

25'-0"

50'-0"

PAVEMENT MARKING SYMBOL

DISTANCE "X" (SEE TABLE BELOW)

RAILROAD

TYPICAL LOCATION OF STOP LINE WHERE NO ACTIVE WARNING DEVICE IS PRESENT OR PROPOSED.

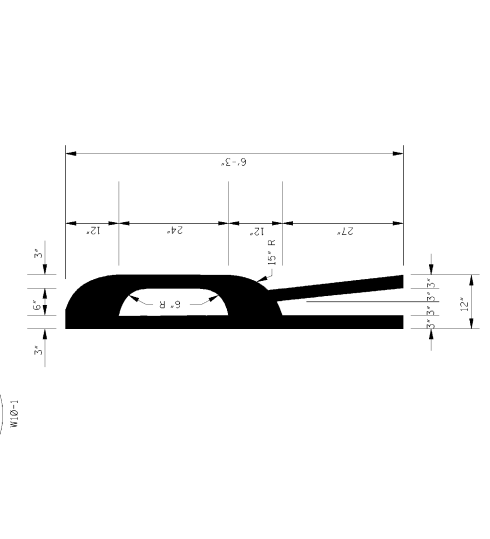
ADVANCE WARNING SIGN PLACEMENT DISTANCE

| POSTED SPEED (mph) | DISTANCE "X" (FT) |       |
|--------------------|-------------------|-------|
|                    | RURAL             | URBAN |
| 20                 | 175               | 100   |
| 25                 | 250               | 100   |
| 30                 | 325               | 100   |
| 35                 | 400               | 150   |
| 40                 | 475               | 225   |
| 45                 | 550               | 300   |
| 50                 | 625               | 375   |
| 55                 | 700               | 450   |
| 60                 | 775               | 550   |

NOTES:

① DISTANCE "X" MAY BE ADJUSTED, IF PROHIBITIVE PHYSICAL CONDITIONS EXIST AT THE DESIGNATED DISTANCE.

② THESE DISTANCES MAY BE ADJUSTED TO A MINIMUM OF 100' IN BUSINESS OR BUSINESS DISTRICTS WHERE LOW SPEEDS ARE PREVALENT.



DETAIL A  
STANDARD "R" PAVEMENT MARKING  
FOR R X R SYMBOL

GENERAL NOTES:

\* 1. A PORTION OF THE PAVEMENT MARKING SYMBOL SHOULD BE DIRECTLY OPPOSITE THE ADVANCE WARNING SIGN (W10-1).

\* 2. WIDTH OF R X R SYMBOL MAY VARY ACCORDING TO LANE WIDTH. HOWEVER, ON MULTI-LANE ROADS, THE TRANSVERSE BANDS AND STOP LINE SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.

\* 3. R X R SYMBOL (63.0 #42), TRANSVERSE BANDS AND STOP LINE SHALL BE PAID FOR AS LEGEND, WHITE (PLASTIC), MATERIAL OPTIONAL FOR OTHER AGENCIES.

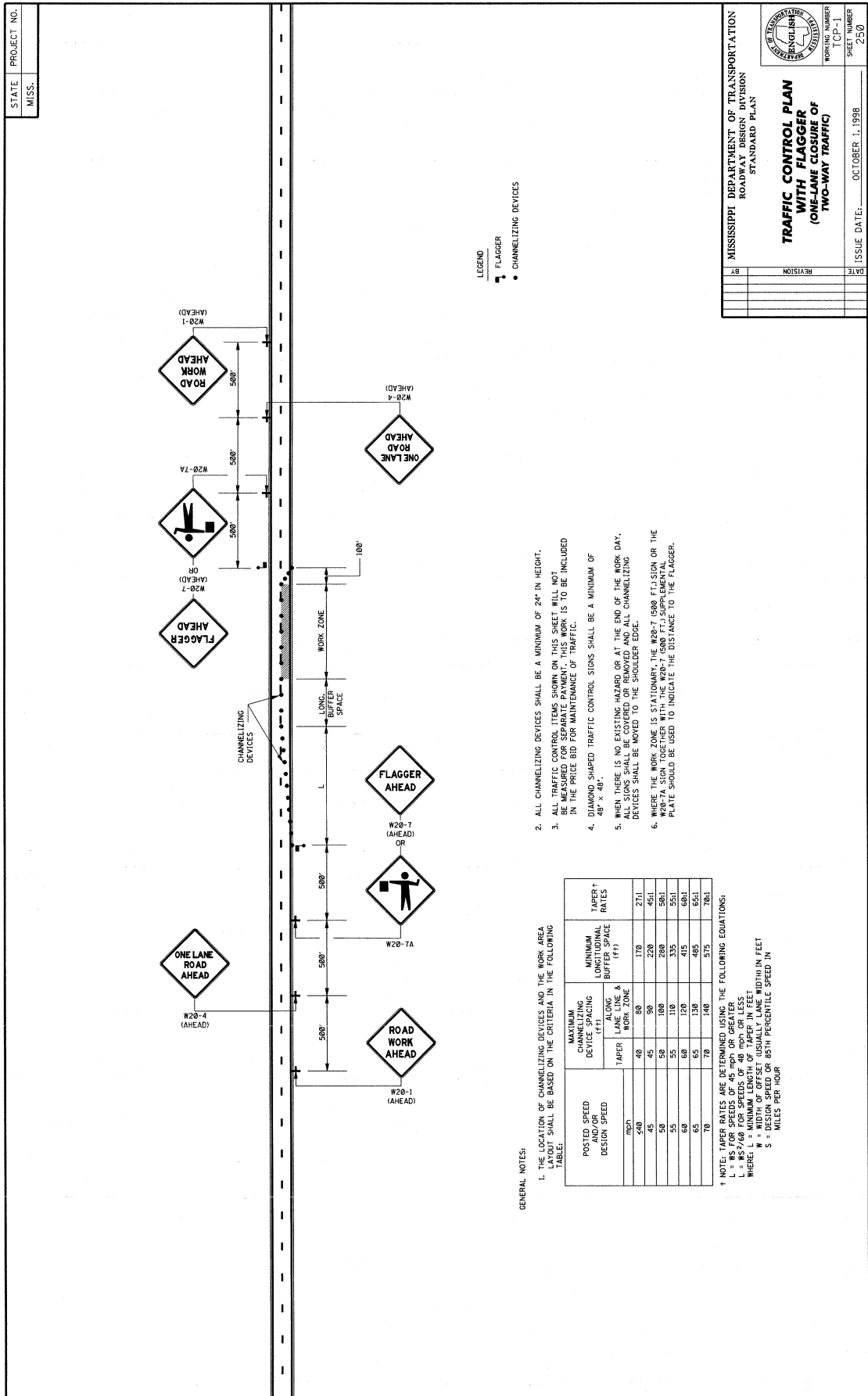
\* 4. REFER TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR LOCATION OF PROPOSED WARNING DEVICES AT RAILROAD-HIGHWAY GRADE CROSSINGS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

**TYPICAL PLACEMENT OF WARNING SIGNS AND PAVEMENT MARKINGS AT RAILROAD HIGHWAY CROSSINGS**

WORKSHEET NO. 100  
SHEET NUMBER 12 OF 18  
ISSUE DATE: OCTOBER 11, 1998

DATE: 10/11/98  
REVISION: 10/11/98  
REVISION NOTE: 10/11/98

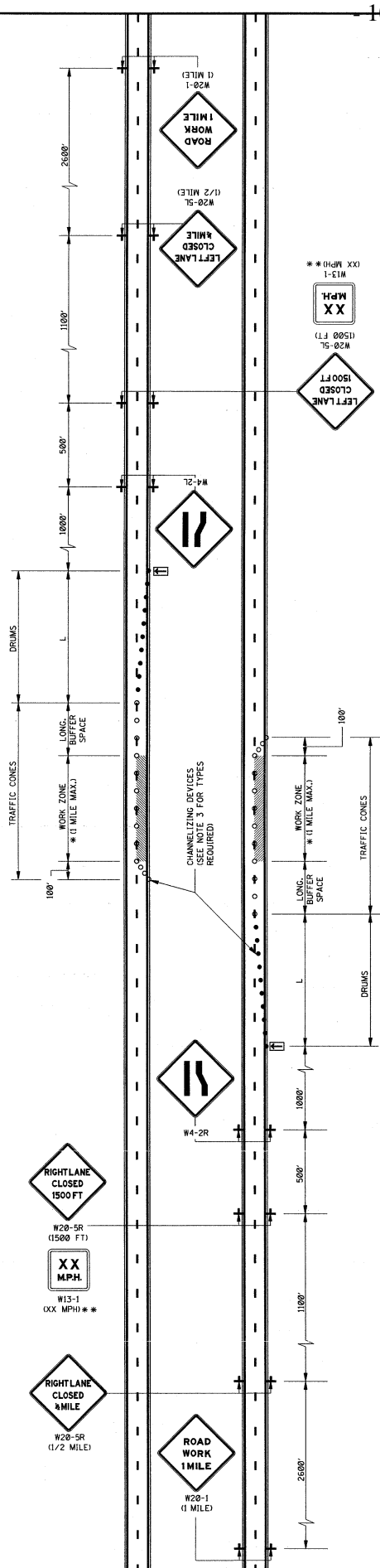


MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
 ROADWAY DESIGN DIVISION  
 STANDARD PLAN

**TRAFFIC CONTROL PLAN WITH FLAGGER (ONE-LANE CLOSURE OF TWO-WAY TRAFFIC)**

WORKING NUMBER: TCP-1  
 SHEET NUMBER: 250  
 ISSUE DATE: OCTOBER 1, 1998

| DATE | REVISION |
|------|----------|
|      |          |
|      |          |
|      |          |



- LEGEND**
- \* OR AS SHOWN ELSEWHERE OF THE PLANS.
  - \*\* THE LEGEND ON W13-1 (XX MPH) SUPPLEMENTAL PLATE SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT.
  - REFLECTORIZED PRE-STANDING PLASTIC DRUMS
  - TRAFFIC CONES (28" HEIGHT)

2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICE TYPES FOR:
  - A. APPROACH TAPER- REFLECTORIZED PLASTIC DRUMS
  - B. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT)
  - C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT)
4. WHEN THERE IS NO EXISTING HAZARD, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE PREVIOUS ZONE.
6. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
7. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".

**GENERAL NOTES:**

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

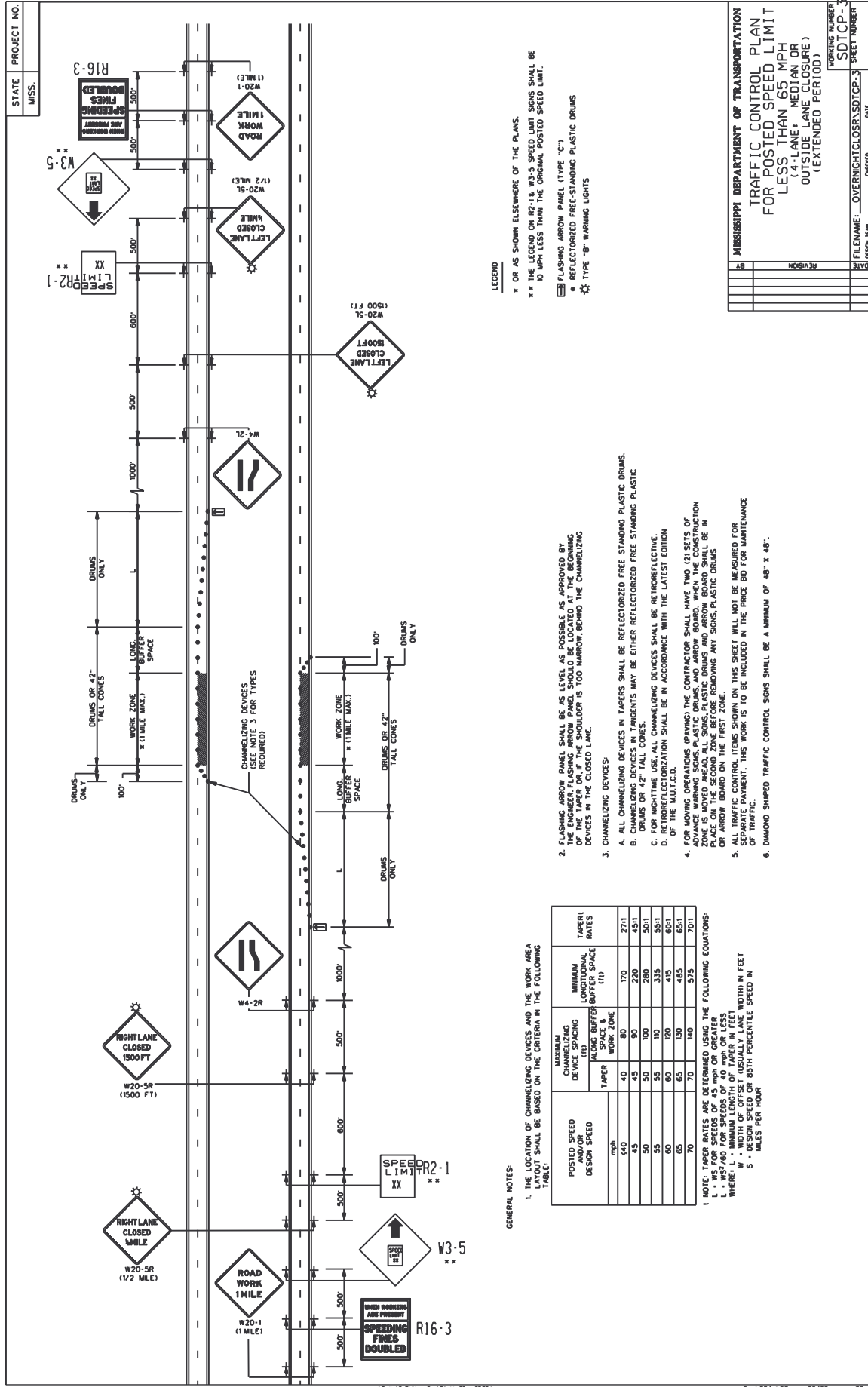
| POSTED SPEED AND/OR DESIGN SPEED | MAXIMUM CHANNELIZING DEVICE SPACING (FT) |           | MINIMUM LONGITUDINAL BUFFER SPACE (ft) | TAPER + TAPER RATES |
|----------------------------------|--|-----------|--|---------------------|
|                                  | ALONG LANE LINE                          | WORK ZONE |  |                     |
| 40                               | 40                                       | 80        | 170                                    | 27:1                |
| 45                               | 45                                       | 90        | 220                                    | 45:1                |
| 50                               | 50                                       | 100       | 280                                    | 50:1                |
| 55                               | 55                                       | 110       | 335                                    | 55:1                |
| 60                               | 60                                       | 120       | 415                                    | 60:1                |
| 65                               | 65                                       | 130       | 485                                    | 65:1                |
| 70                               | 70                                       | 140       | 575                                    | 70:1                |

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:  
 $L = W \cdot S$  FOR SPEEDS OF 45 MPH OR GREATER  
 $L = W \cdot S^2$  FOR SPEEDS OF 40 MPH OR LESS  
 WHERE L = MINIMUM LENGTH OF TAPER IN FEET  
 W = WIDTH OF OFFSET USUALLY LANE WIDTH IN FEET  
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
 ROADWAY DESIGN DIVISION  
 STANDARD PLAN

**TRAFFIC CONTROL PLAN  
 FOR POSTED SPEED LIMIT  
 LESS THAN 65 MPH  
 (4-LANE, MEDIAN LANE  
 OR CURB AND GUTTER CLOSURE)  
 (WORK DAY ONLY)**

WORKING NUMBER: TCP-2  
 SHEET NUMBER: 251  
 ISSUE DATE: OCTOBER 1, 1998

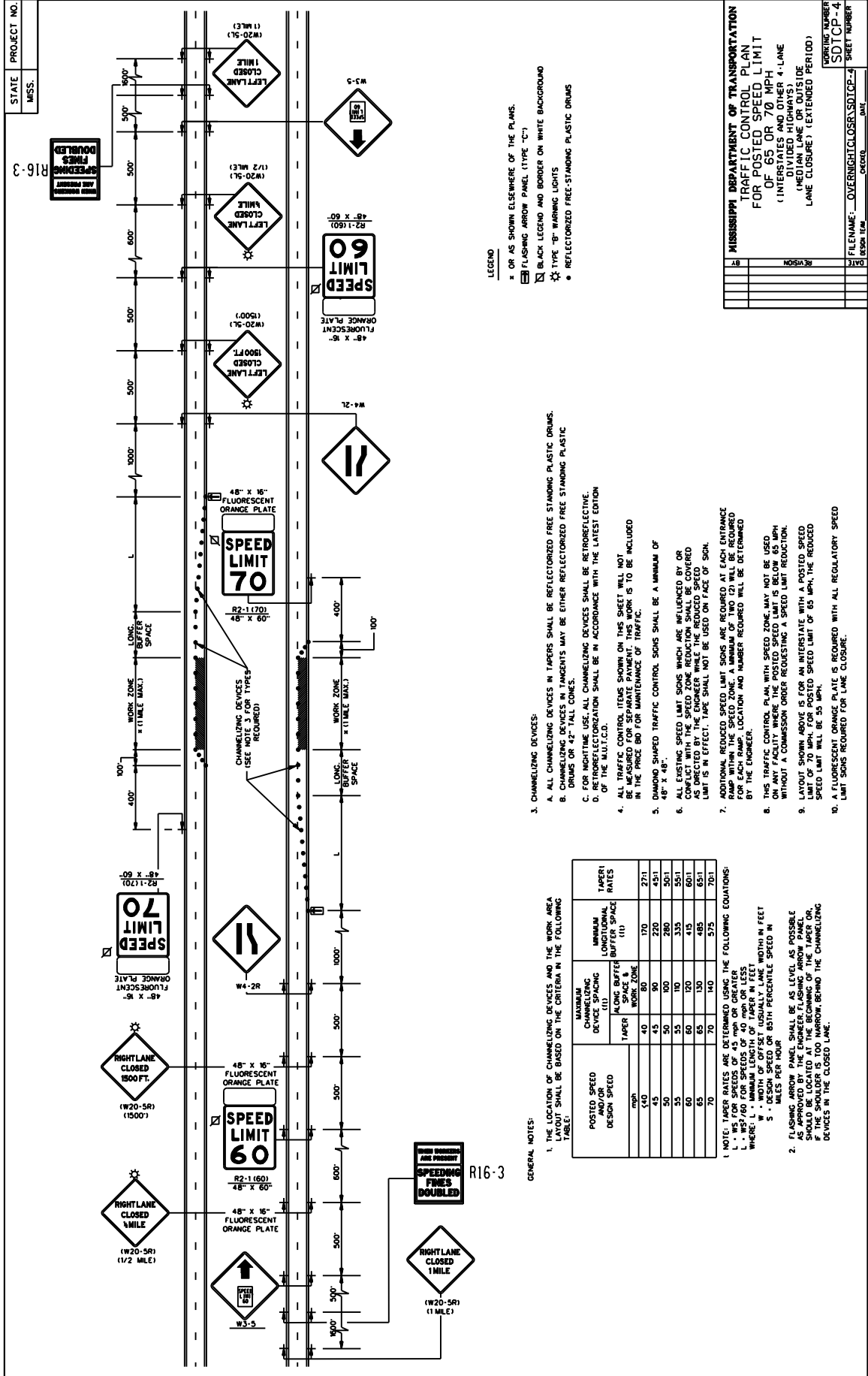


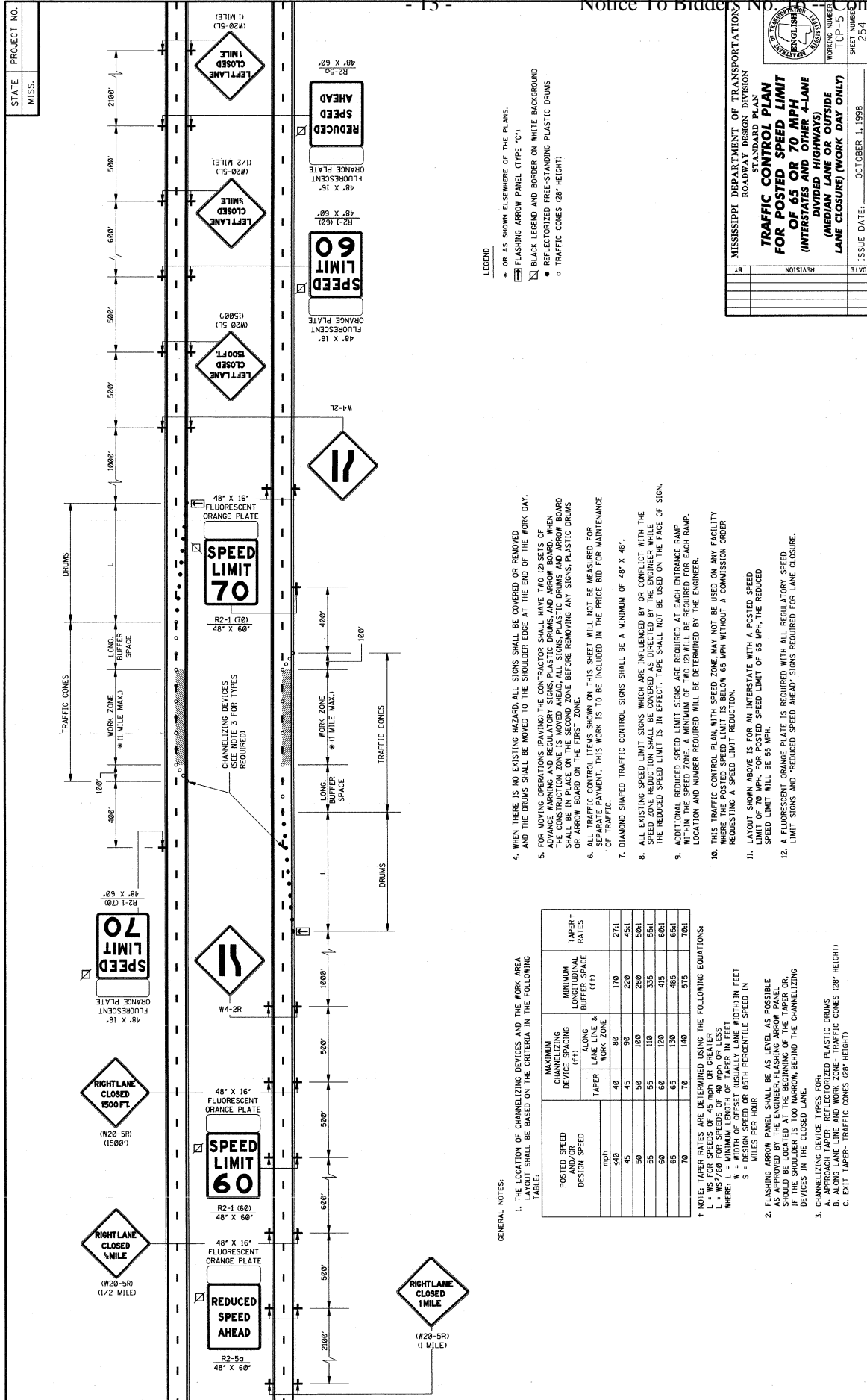
**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**  
**TRAFFIC CONTROL PLAN**  
**FOR POSTED SPEED LIMIT**  
**LESS THAN 65 MPH**  
**(4 LANE MEDIAN OR**  
**DUAL LANE CLOSURE)**  
**(EXTENDED PERIOD)**

PROJECT NUMBER: SDT CP-3  
 SHEET NUMBER: 11

| DATE | REVISION |
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|      |          |

DESIGN FIRM: OVERNIGHT CONSTRUCTION  
 CHECKED: [ ]  
 DATE: [ ]





**GENERAL NOTES:**

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:
2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE. APPROACH TAPER REFLECTORIZED PLASTIC DRUMS SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICE TYPES FOR:
  - A. APPROACH TAPER- REFLECTORIZED PLASTIC DRUMS
  - B. EXIT TAPER- REFLECTORIZED PLASTIC DRUMS (28" HEIGHT)
  - C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT)
4. WHEN THERE IS NO EXISTING HAZARD, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS PAYING THE CONTRACTOR SHALL HAVE TWO (2) SETS OF CHANNELIZING DEVICES AND TRAFFIC CONES TO BE MOVED AHEAD OF THE WORK ZONE. THE CONSTRUCTION ZONE IS MOVED AHEAD ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD OR ARROW BOARD ON THE FIRST ZONE.
6. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
7. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".
8. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON THE FACE OF SIGN.
9. ADDITIONAL REDUCED SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. A MINIMUM OF TWO (2) WILL BE REQUIRED FOR EACH RAMP. LOCATION AND NUMBER REQUIRED WILL BE DETERMINED BY THE ENGINEER.
10. THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
11. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.
12. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS AND "REDUCED SPEED AHEAD" SIGNS REQUIRED FOR LANE CLOSURE.

| POSTED SPEED DESIGN SPEED mph | CHANNELIZING DEVICE SPACING (FT) |                       | MINIMUM LONGITUDINAL BUFFER SPACE (FT) | TAPER RATES |
|-------------------------------|----------------------------------|-----------------------|--|-------------|
|                               | LANE LINE & WORK ZONE            | LANE LINE & WORK ZONE |  |             |
| 50                            | 40                               | 80                    | 170                                    | 27:1        |
| 55                            | 45                               | 90                    | 220                                    | 45:1        |
| 60                            | 50                               | 100                   | 280                                    | 50:1        |
| 65                            | 55                               | 110                   | 335                                    | 55:1        |
| 70                            | 60                               | 120                   | 415                                    | 60:1        |
| 75                            | 65                               | 130                   | 485                                    | 65:1        |
| 80                            | 70                               | 140                   | 575                                    | 70:1        |


† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:  
 L = WS FOR SPEEDS OF 45 mph OR GREATER  
 L = WS/60 FOR SPEEDS OF 40 mph OR LESS  
 WHERE:  
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET  
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

**LEGEND**

- \* OR AS SHOWN ELSEWHERE OF THE PLANS.
- ◻ FLASHING ARROW PANEL (TYPE "C")
- ◻ BLACK LEGEND AND BORDER ON WHITE BACKGROUND
- REFLECTORIZED FREE-STANDING PLASTIC DRUMS
- TRAFFIC CONES (28" HEIGHT)

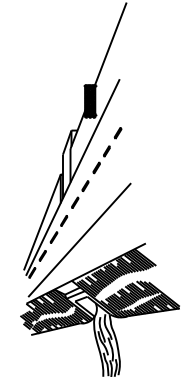
MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
 ROADWAY DESIGN PLAN  
 STANDARD PLAN  
**TRAFFIC CONTROL PLAN  
 FOR POSTED SPEED LIMIT  
 OF 65 OR 70 MPH  
 (INTERSTATES AND OTHER 4-LANE  
 DIVIDED HIGHWAYS)  
 (MEDIAN LANE OR OUTSIDE  
 LANE CLOSURE) (WORK DAY ONLY)**

WORKING NUMBER: TCP-5  
 SHEET NUMBER: 254  
 ISSUE DATE: OCTOBER 1, 1988



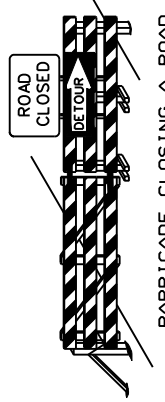
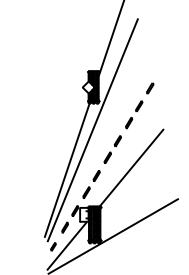
MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
HIGHWAY SIGN AND BARRICADE DETAIL FOR CONSTRUCTS

WORKING NUMBER: SDTCP-10  
SHEET NUMBER: 10  
DATE: 06/21/12  
FILE NAME: SDTCP-10\_06012012.DGN  
PERSON: [blank]  
CROSSLIN: [blank]



**WING BARRICADES**

- WING BARRICADES ARE TYPE BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
- WING BARRICADES SHOULD BE USED:
  - IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
  - IN ADVANCE OF ALL BRIDGE OR CULVERT MOVING OPERATIONS.

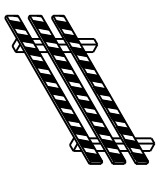


**BARRICADE CLOSING A ROAD**

**BARRICADE CHARACTERISTICS**

|                                    | I                      | II                     | III   |
|------------------------------------|------------------------|------------------------|---|
| WIDTH OF RAIL **                   | 8" MIN. - 12" MAX.     | 8" MIN. - 12" MAX.     | 8" MIN. - 12" MAX.  |
| LENGTH OF RAIL *                   | 24" MIN.               | 24" MIN.               | 48" MIN.  |
| WIDTH OF STRIPE *                  | 6"                     | 6"                     | 6"  |
| HEIGHT                             | 35" MIN.               | 35" MIN.               | 60" MIN.  |
| NUMBER OF REFLECTORIZED RAIL FACES | 2 (ONE EACH DIRECTION) | 4 (TWO EACH DIRECTION) | 3 IF FACING TRAFFIC IN ONE DIRECTION<br>6 IF FACING TRAFFIC IN TWO DIRECTIONS |
| TYPE OF FRAME                      | LIGHT                  | LIGHT "X" FRAME        | POST OR SHD   |

- \* 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- \*\* 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED FACINGS TRAFFIC SHALL HAVE A MINIMUM OF 270 IN OF REFLECTIVE AREA.



**TYPE III**



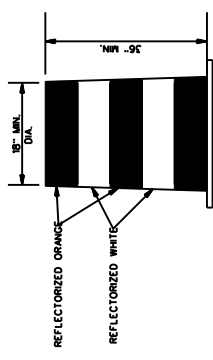
**TYPE II**



**TYPE I**

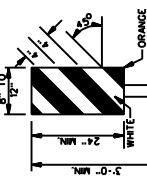
**STANDARD BARRICADES**

- A TYPE BARRICADE CONSISTS OF ONE (1) HORIZONTAL RAIL SUPPORTED BY A DEMOUNTABLE POST OR UPRIGHT. BARRICADES NORMALLY WOULD BE USED ON CONVENTIONAL ROADS OR URBAN STREETS AND ARTERIALS.
- A TYPE BARRICADE CONSISTS OF TWO (2) HORIZONTAL RAILS ON A LIGHT "X" FRAME. TYPE BARRICADES ARE INTENDED FOR USE WHERE THE HAZARD IS LESS SEVERE THAN THAT OF TYPE I AND TYPE II BARRICADES. THEY ARE USED FOR CONTINUOUS DELIMITING OF A RESTRICTED ROADWAY OR FOR TEMPORARY DAYTIME USE.
- A TYPE BARRICADE CONSISTS OF THREE (3) HORIZONTAL RAILS SUPPORTED BY FIXED POSTS, A ROAD SHD, A HEAVY DEMOUNTABLE FRAME OR A HEAVY WINGED "X" FRAME.
- TYPE BARRICADES ARE INTENDED FOR USE ON CONSTRUCTION AND MAINTENANCE PROJECTS AS BARRICADES AND AT ROAD CLOSURES, WHERE THEY MUST REMAIN IN PLACE FOR EXTENDED PERIODS.
- THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).
- DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
- FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.



**PLASTIC DRUM STRIPING DETAIL**

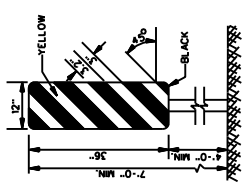
- PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH MUTCD STANDARDS FOR BARRICADES. THE PREDOMINANT COLOR ON DRUMS SHALL BE ORANGE. STRIPES SHALL BE GREEN, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
- DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
- WHERE PRACTICAL PLASTIC DRUMS SHALL BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.



**VERTICAL PANEL**

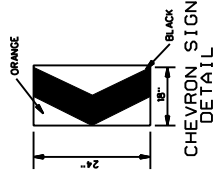
- VERTICAL PANELS CONSIST OF AT LEAST ONE PANEL 8" TO 12" IN WIDTH AND A MINIMUM OF 24" IN HEIGHT.
- THE DIAGONAL STRIPES SHALL SLOPE DOWNWARD IN THE DIRECTION THAT TRAFFIC IS TO PASS THE PANEL. THE MINIMUM OF 35" ABOVE THE ROADWAY ON A SINGLE LIGHTMASS POST.
- VERTICAL PANELS USED ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH-SPEED ROADWAYS SHALL HAVE A MINIMUM OF 270 IN OF RETROREFLECTIVE AREA FACING TRAFFIC.
- FOR TWO-WAY TRAFFIC OPERATIONS, BACK-TO-BACK PANELS SHALL BE USED.

GENERAL NOTES:  
1. MARKINGS ON ALL DEVICES SHOWN ON THIS SHEET SHALL BE HIGH INTENSITY REFLECTIVE SHEETING.  
2. THE TRAFFIC CONTROL PLAN WILL LIST THE VARIOUS TRAFFIC CONTROL DEVICES REQUIRED FOR EACH PROJECT.



**TYPE 3 OBJECT MARKER (OM-3R)**

- TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
- THE OM-3R IS SHOWN. THE OM-3R IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
- THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



**CHEVRON SIGN DETAIL**

- A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
- THE CHEVRON SIGN SHALL BE MOUNTED ON FIXED POST OR ROAD SHD.
- CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHALL BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.

|                |             |  |
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| STATE<br>MISS. | PROJECT NO. |  |
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### MOBILE OPERATIONS ON MULTILANE ROAD

**MOBILE OPERATIONS ON MULTILANE ROAD**

**NOTES:**

- VEHICLES USED FOR THESE OPERATIONS SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLAOS, SIGNS, OR ARROW PANELS.
- PROTECTION VEHICLE #1 SHOULD BE EQUIPPED WITH AN ARROW PANEL. AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON PROTECTION VEHICLE #1 SO AS NOT TO OBSCURE THE ARROW PANEL.
- PROTECTION VEHICLE #2 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK-MOUNTED ATTENUATOR (TMA).
- PROTECTION VEHICLE #1 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
- WHEN ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, PROTECTION VEHICLE #1 SHOULD BE ELIMINATED.
- ON HIGH-SPEED ROADWAYS, A THIRD PROTECTION VEHICLE SHOULD BE USED (I.E., VEHICLE #1 ON THE SHOULDER (IF PRACTICAL), VEHICLE #2 IN THE CLOSED LANE, AND VEHICLE #3 IN THE CLOSED LANE).
- ARROW PANELS SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
- WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

### MOBILE OPERATIONS ON TWO-LANE ROAD

**MOBILE OPERATIONS ON TWO-LANE ROAD**

**NOTES:**

- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND PROTECTION VEHICLES SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLAOS, SIGNS, OR ARROW PANELS. THIS EQUIPMENT SHOULD NOT BE DONE FREQUENTLY, AS AN ALTERNATIVE A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND PROTECTION VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. PROTECTION VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE WORK, PROTECTION VEHICLES SHOULD BE ELIMINATED. THE PROTECTION VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL PROTECTION VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING WORK OPERATION MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE PROTECTION VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE PROTECTION VEHICLE SHALL BE EQUIPPED WITH BEACONS. THE PROTECTION AND WORK VEHICLES SHOULD DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

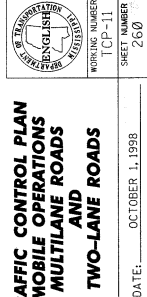
**MOBILE OPERATIONS ON TWO-LANE ROAD**

**MOBILE OPERATIONS ON TWO-LANE ROAD**

**NOTES:**

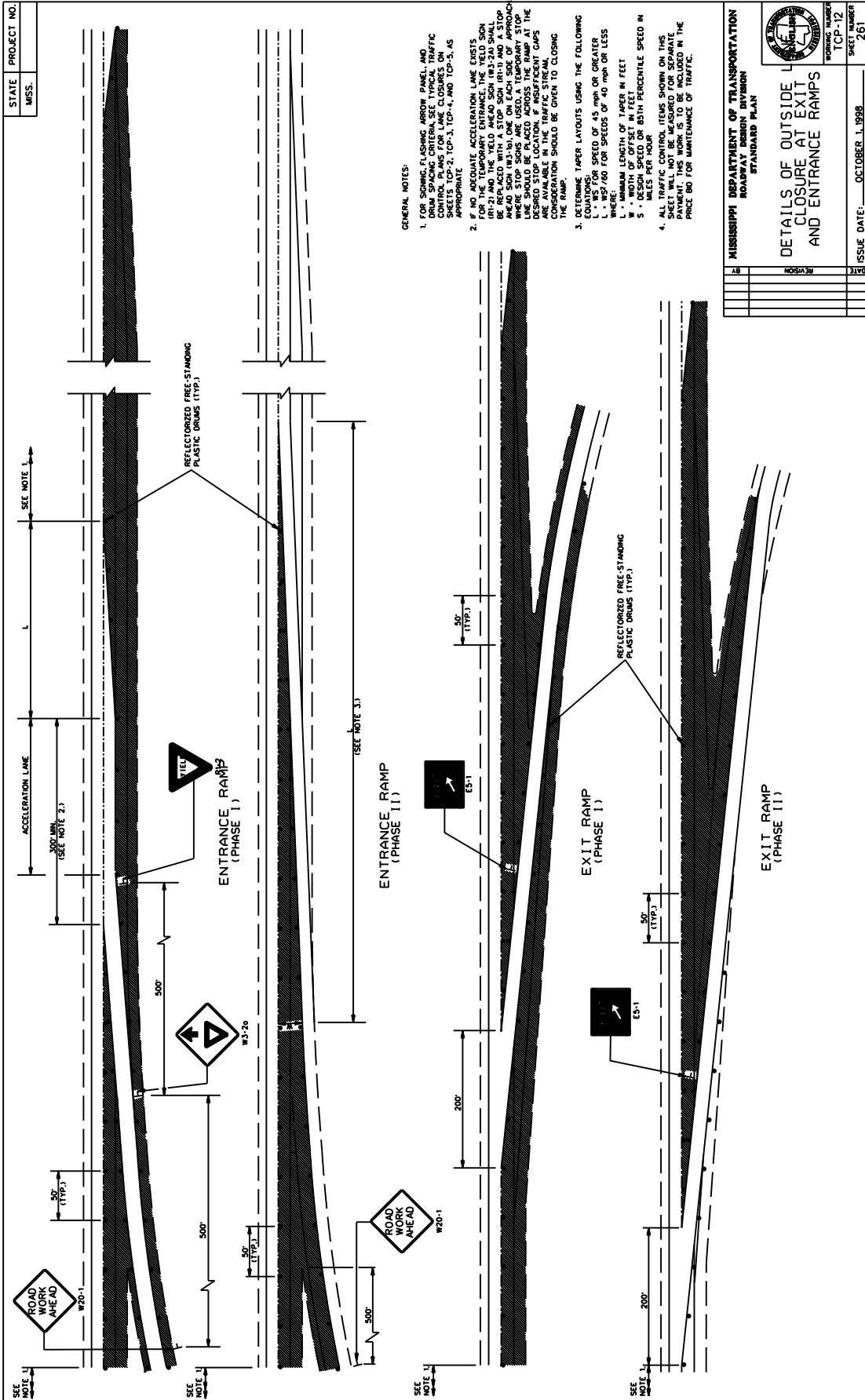
- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND PROTECTION VEHICLES SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLAOS, SIGNS, OR ARROW PANELS. THIS EQUIPMENT SHOULD NOT BE DONE FREQUENTLY, AS AN ALTERNATIVE A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND PROTECTION VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. PROTECTION VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE WORK, PROTECTION VEHICLES SHOULD BE ELIMINATED. THE PROTECTION VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL PROTECTION VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING WORK OPERATION MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE PROTECTION VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE PROTECTION VEHICLE SHALL BE EQUIPPED WITH BEACONS. THE PROTECTION AND WORK VEHICLES SHOULD DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

|  |                 |
|--|-----------------|
| MISSISSIPPI DEPARTMENT OF TRANSPORTATION<br>ROADWAY DESIGN DIVISION<br>STANDARD PLAN |                 |
| DATE   | BY              |
| REVISION   | S.M.R.          |
| REVISED  | CAPTION         |
| 20   | 99              |
| SHEET NUMBER   | ISSUE DATE:     |
| TCP-11   | OCTOBER 1, 1998 |
| 260  |                 |



**TRAFFIC CONTROL PLAN  
MOBILE OPERATIONS  
MULTILANE ROADS  
AND  
TWO-LANE ROADS**





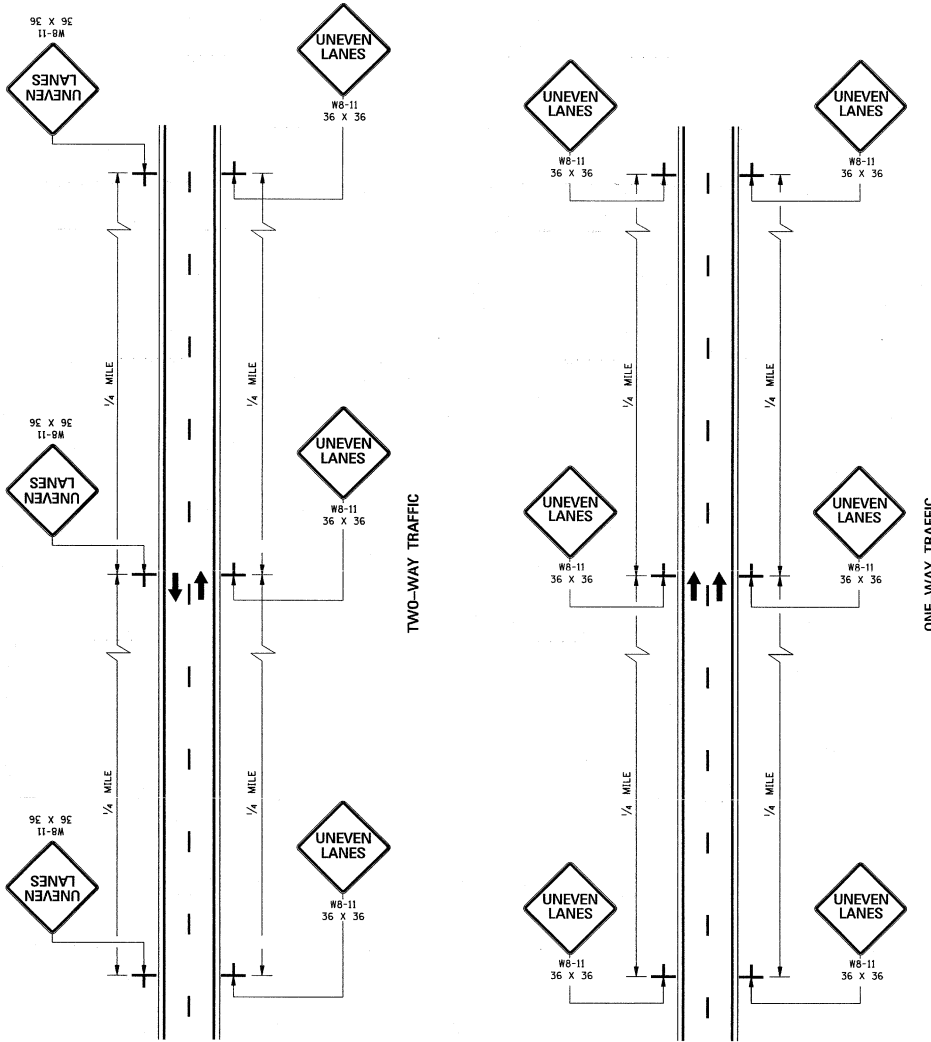
GENERAL NOTES:

- FOR SIGNING, FLASHING ARROW PANEL AND CONTROL PANELS, REFER TO THE SIGNING CONTROL PLANS FOR LANE CLOSURES ON SHEETS TCP-2, TCP-3, TCP-4, AND TCP-5, AS APPROPRIATE.
- IF NO ADEQUATE ACCELERATION LANE EXISTS AT THE ENTRANCE TO THE RAMP, THE RAMP SHALL BE REPLACED WITH A STOP SIGN (R1-1) AND A STOP AHEAD SIGN (W3-26), ONE ON EACH SIDE OF APPROACH. THE STOP SIGN SHALL BE PLACED AT THE DESIRED STOP LOCATION, IF INSUFFICIENT GAPS EXIST. THE STOP SIGN SHALL BE PLACED AT THE DESIRED STOP LOCATION, IF INSUFFICIENT GAPS EXIST. THE STOP SIGN SHALL BE PLACED AT THE DESIRED STOP LOCATION, IF INSUFFICIENT GAPS EXIST.
- DETERMINE TAPER LAYOUTS USING THE FOLLOWING EQUATIONS:
  - $L = 1.47 S^2 / a$  FOR SPEEDS OF 40 MPH OR GREATER
  - $L = .0007 S^3$  FOR SPEEDS OF 40 MPH OR LESS
 WHERE:
  - $L$  = MINIMUM LENGTH OF TAPER IN FEET
  - $S$  = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
  - $a$  = DECELERATION IN FEET PER SECOND
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS DRAWING ARE TO BE INSTALLED FOR USE IN THE PHASE I, UNLESS OTHERWISE NOTED TO BE USED IN THE PHASE II FOR MAINTENANCE OF TRAFFIC.

|  |                 |
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| MISSISSIPPI DEPARTMENT OF TRANSPORTATION |                 |
| ROADWAY IMPROVEMENT DIVISION             |                 |
| STANDARD PLAN                            |                 |
| WORKING NUMBER                           | TCP-12          |
| SHEET NUMBER                             | 261             |
| ISSUE DATE:                              | OCTOBER 1, 1998 |

DETAILS OF OUTSIDE LANE CLOSURE AT EXIT AND ENTRANCE RAMP

|       |             |
|-------|-------------|
| STATE | PROJECT NO. |
| MISS. |             |

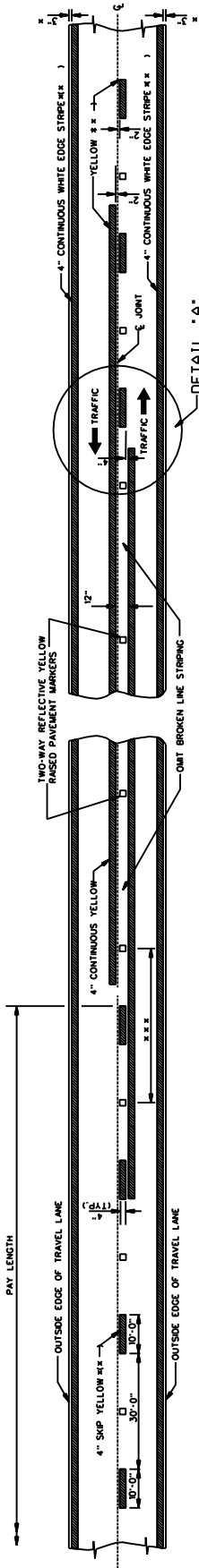


GENERAL NOTES:

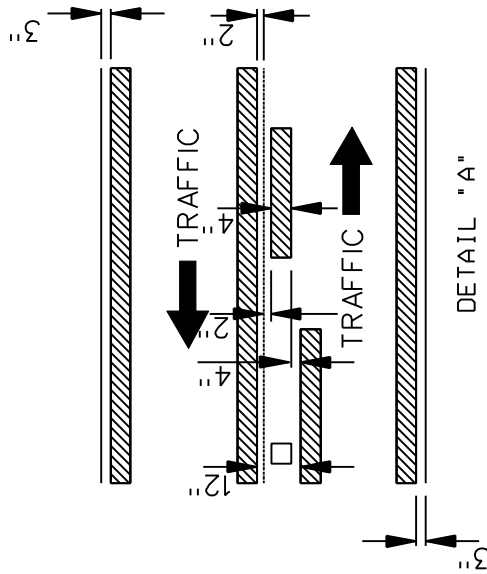
1. UNEVEN LANE LINE SPACING SHALL BE EQUAL TO 1/2" NO SIGNS REQUIRED.
  2. IF GREATER THAN 1/2" AND LESS THAN OR EQUAL TO 2/4" PLACE SIGNS AS SHOWN ON THIS SHEET.
  3. IF GREATER THAN 2/4" TRAFFIC SHOULD NOT BE ALLOWED TO CROSS UNEVEN LANE LINE.
2. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.
  3. THE W8-11 SIGNS SHALL BE SPACED AT 1/4-MILE INTERVALS THROUGHOUT UNEVEN LANE LINE LIMITS.

|  |                 |
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| MISSISSIPPI DEPARTMENT OF TRANSPORTATION |                 |
| ROADWAY DESIGN DIVISION                  |                 |
| STANDARD PLAN                            |                 |
| <b>TRAFFIC CONTROL PLANS</b>             |                 |
| <b>UNEVEN PAVEMENT</b>                   |                 |
| <b>DETAILS</b>                           |                 |
| WORKING NUMBER                           | CP-14           |
| SHEET NUMBER                             | 263             |
| DATE                                     | OCTOBER 1, 1998 |
| BY                                       |                 |
| REVISION                                 |                 |
|  |                 |
|  |                 |
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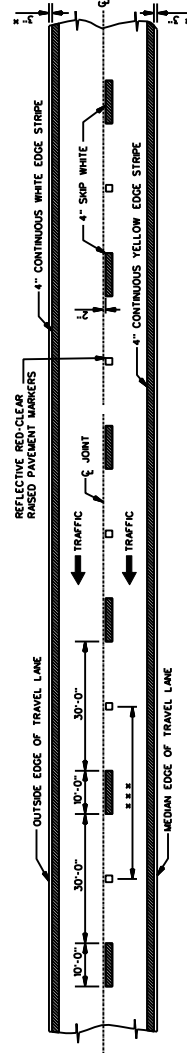
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| STATE | PROJECT NO. |
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TWO-WAY TRAFFIC  
(ASPHALT OR CONCRETE PAVEMENT)



DETAIL "A"



4-LANE WITH ONE-WAY TRAFFIC



GENERAL NOTES:

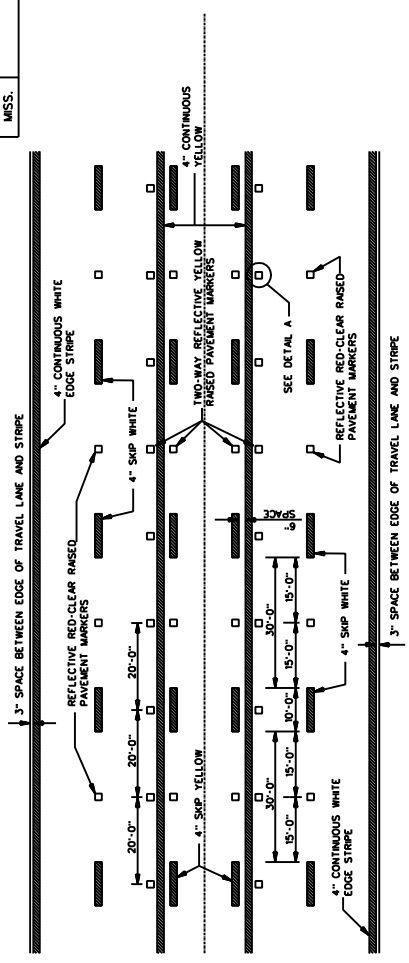
- \* 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- \*\* 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
- \*\*\* 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

| TANGENT SECTIONS   | URBAN AREA (U-L-N) | RURAL AREA (U-L-N) |
|--------------------|--------------------|--------------------|
| LANE LINES         | 40'-0"             | 80'-0"             |
| INTERCHANGE LIMITS | 40'-0"             | 140'-0"            |

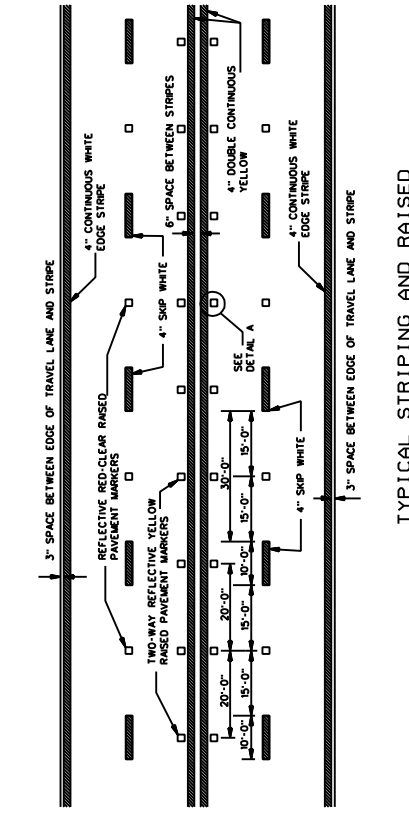
- 1. NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS SHALL BE USED THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- 2. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE MATERIALS AS SPECIFIED IN THE MOOT.
- 3. APPROVED SOURCES OF MATERIALS.
- 4. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKERS ARE TO REMAIN IN PLACE OVER 3 MONTHS.

|  |                  |
|--|------------------|
| MISSISSIPPI DEPARTMENT OF TRANSPORTATION<br>ROADWAY DESIGN DIVISION<br>STANDARD PLAN |                  |
| TEMPORARY STRIPING FOR<br>TRAFFIC CONTROL<br>2-LANE AND 4-LANE<br>DIVIDED HIGHWAYS   |                  |
| WORKING NUMBER   | TCP-15           |
| SHEET NUMBER   | 264              |
| ISSUE DATE:  | DECEMBER 1, 1999 |

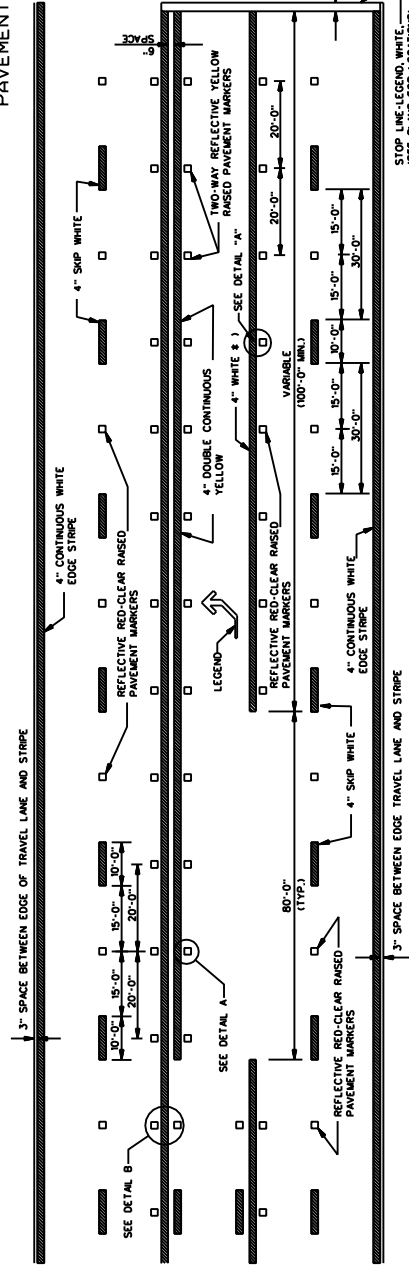
|       |             |
|-------|-------------|
| STATE | PROJECT NO. |
| MISS. |             |



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 5-LANE SECTION



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION



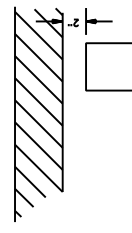
TYPICAL STRIPING AND RAISED PAVEMENT MARKERS AT LEFT TURN LANES

\* NOTE: USE DETAIL STRIPING IF LENGTH  $\leq 150'$  AT THIS LOCATION, OTHERWISE USE CONTINUOUS STRIPING.

TYPICAL TWO-WAY ARROW INSTALLATION

- NOTES: 1. CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY.
- 2. IF SEGMENT IS LESS THAN 350', PLACE ONE SET OF ARROWS IN CENTER OF SEGMENT.
- 3. IF SEGMENT IS GREATER THAN 350', PLACE FIRST SET OF ARROWS 50 TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS (250 O.C.).

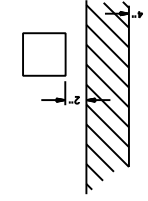
DETAIL A  
LATERAL PLACEMENT OF PAVEMENT MARKERS



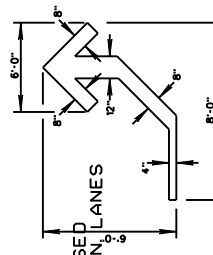
GENERAL NOTE:

- 1. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MOST APPROVED SOURCE OF MATERIALS.
- 2. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKERS ARE TO REMAIN IN PLACE OVER 3 MONTHS.
- 3. TEMPORARY TURN ARROW TO BE PAID FOR AS TEMPORARY TRAFFIC STRIPING LEGEND, ESTIMATED AT 10.9 SQ. FT. PER ARROW.

DETAIL B  
LATERAL PLACEMENT OF PAVEMENT MARKERS



DETAIL OF TEMPORARY TURN ARROW

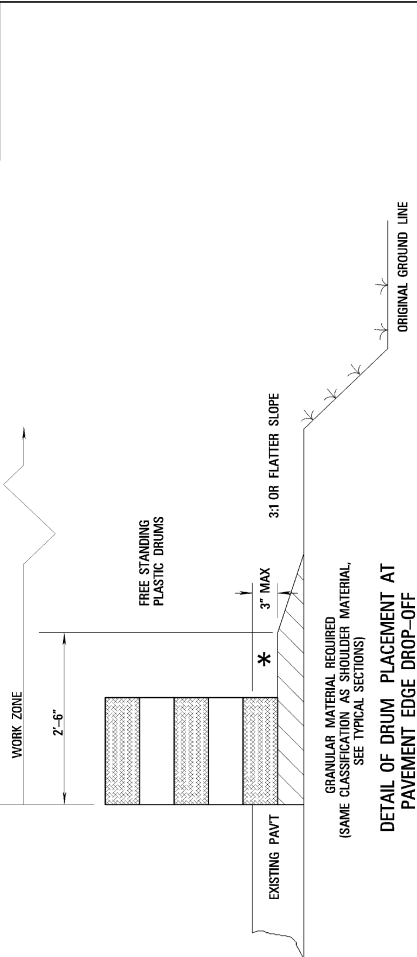


MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

TEMPORARY STRIPING FOR TRAFFIC CONTROL  
4-LANE AND 5-LANE UNDIVIDED ROADWAYS

WORKING NUMBER: TCP-16  
SHEET NUMBER: 265

ISSUE DATE: DECEMBER 1, 1999



DETAIL OF DRUM PLACEMENT AT PAVEMENT EDGE DROP-OFF

- NOTES**
- \* A. PAVEMENT EDGE DROP-OFF
    1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-3) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750 ± O.C.).
    2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. COMES MAY BE USED IN PLACE OF DRUMS, PANELS AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE M.U.T.C.D. (1/3 L, WHERE L IS THE TAPER LENGTH IN FEET)
    3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 3:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS/PANELS OR BARRICADES MAY BE USED. IF CONCRETE BARRIERS ARE USED, SPECIAL REFLECTIVE DEVICES OR STEADY BURN LIGHTS SHOULD BE USED FOR OVERNIGHT INSTALLATIONS.
    4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
    5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

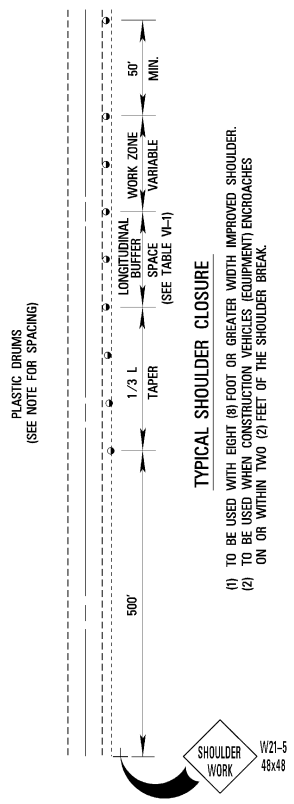
- B. DRUM SPACING**
1. TANGENTS =  $2 \times S$
  2.  $APPROX = \frac{L}{S} \times W$   
WHERE L = TAPER LENGTH IN FEET  
S = SPEED IN MPH (POSTED OR 85 PERCENTILE)  
W = WIDTH OF OFFSET IN FEET

C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

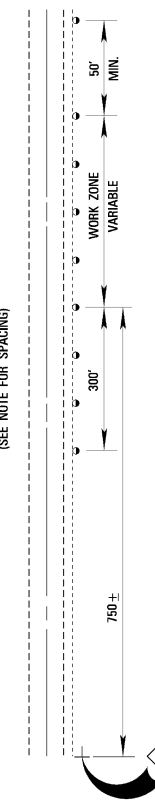
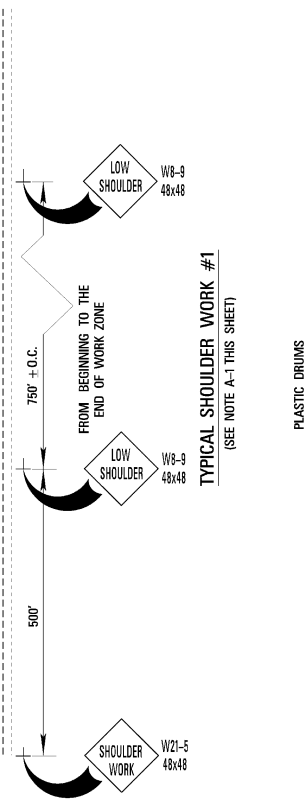
TABLE V-1 GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

| SPEED (MPH) | LENGTH (FEET) |
|-------------|---------------|
| 20          | 35            |
| 25          | 45            |
| 30          | 55            |
| 35          | 65            |
| 40          | 75            |
| 45          | 85            |
| 50          | 95            |
| 55          | 105           |
| 60          | 115           |
| 65          | 125           |
| 70          | 135           |
| 75          | 145           |
| 80          | 155           |
| 85          | 165           |
| 90          | 175           |
| 95          | 185           |
| 100         | 195           |
| 110         | 215           |
| 120         | 235           |
| 130         | 255           |
| 140         | 275           |
| 150         | 295           |
| 160         | 315           |
| 170         | 335           |
| 180         | 355           |
| 190         | 375           |
| 200         | 395           |
| 210         | 415           |
| 220         | 435           |
| 230         | 455           |
| 240         | 475           |
| 250         | 495           |

\* POSTED SPEED OFF PEAK 85 PERCENTILE SPEED PRIOR TO WORK STARTING OR THE ANTICIPATED GRINDING SPEED IN MPH



- TYPICAL SHOULDER CLOSURE**
- (1) TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
  - (2) TO BE USED WHEN CONSTRUCTION VEHICLES (EQUIPMENT) ENCRUSHES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.



- TYPICAL SHOULDER WORK #2**
- NOTE: WORK OUTSIDE THE (2) FOOT LIMIT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA. OR SEE NOTE A-3 THIS SHEET.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
TRAFFIC CONTROL DETAILS  
DRUM PLACEMENT  
AND  
SHOULDER CLOSURE

PROJECT NO.: \_\_\_\_\_  
COUNTY: \_\_\_\_\_  
FILE NAME: \_\_\_\_\_  
PERSON: \_\_\_\_\_  
DATE: 12-28-20

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 103**

**CODE: (SP)**

**DATE:** April 11, 2017

**SUBJECT:** Contract Time

**PROJECT:** MP-6011-55(022) / 306147301- Pearl River

The completion of work to be performed by the Contractor for this project will not be a specified date but shall be when all allowable working days are assessed, or any extension thereto as provided in Subsection 108.06. It is anticipated that the Notice of Award will be issued no later than **June 13, 2017**.

The Contractor shall request a Notice to Proceed / Beginning of Contract Time date between the date of the **Execution of Contract and September 7, 2017**.

Should the Contractor not request a Notice to Proceed by **September 7, 2017**, the date for the Notice to Proceed and Beginning of Contract Time will be **September 7, 2017**.

**37** Working Days have been allowed for the completion of work on this project.

The progress schedule for this project shows the Notice to Proceed and Beginning of Contract Time starting at the latest possible date. If the Contractor requests a Notice to Proceed earlier than this date, the Contractor shall submit a revised progress schedule showing the work beginning at the new Notice to Proceed and Beginning of Contract Time date.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 104**

**CODE: (SP)**

**DATE: 03/22/2017**

**SUBJECT: Scope of Work**

**PROJECT: MP-6011-55(022) / 306147301 - Pearl River County**

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled "Standard Drawings". All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following:

**OVERLAY U. S. HIGHWAY 11**  
**FROM SOUTH APPLE DRIVE TO MILLARD - 6.6 MILES**

(A) Prior to the overlay, centerline alignment shall be determined by the Contractor by measuring the existing roadway at 500-foot intervals in tangent sections, and 100-foot intervals in horizontal curves.

(B) Cold mill a 100-foot transition at the BOP, through the two box culvert locations, a 100-foot transition at the EOP, local roads to ROW as directed by the Project Engineer, and any other areas designated by the Project Engineer to ensure smooth transition of new overlay with existing grade. (See Typical Drawings).

(C) One area has been identified for base repair. It is located as follows:

| <u>Approximate Location</u> | <u>Lane</u>    | <u>Size</u> | <u>Area (sf)</u> |
|-----------------------------|----------------|-------------|------------------|
| 169+75                      | Left and Right | 26' x 30'   | 780              |

This failed area shall be repaired by full depth removal and backfilling with asphalt 19-mm, ST, asphalt (estimated 40 tons).

(D) Widening of the roadway will be required at the Cemetery Road intersection. The widening will be both north and south of the intersection approximately 300 feet in each direction and 12 feet wide. This will allow for the addition of left turn lanes as shown in the attached drawings.

(E) Prior to placement of the asphalt, all raised pavement markers throughout the project shall be removed (cost absorbed). Overlay Highway 11 and all side roads to State ROW with 1" of 9.5-mm, MT, asphalt. A ¾" leveling lift of 9.5-mm, MT, leveling asphalt shall be required on the mainline from BOP to EOP or as directed by the Project Engineer. Publicly maintained roads or streets shall be surfaced to the existing ROW; privately owned entrances shall be surfaced a distance of 10 feet and variable from edge of pavement. Any site grading at local

roads or drives will not be measured for separate payment but will be considered an absorbed item. Cross slopes shall be increased where practical with contract quantities in an effort to achieve a uniform cross slope of 2%.

It is estimated that 5,500 tons of 9.5-mm, MT, asphalt will be used for the mainline and 4,000 tons of 9.5-mm, MT, asphalt will be used for local roads and driveways.

(F) The Mean Roughness Index (MRI) shall be used to determine the pavement smoothness.

(G) Sawing and sealing of transverse joints will be required in the surface asphalt. The Contractor will be responsible to locate/mark these joints prior to construction.

(H) Temporary and permanent striping shall be placed where existing stripes are located and shall conform to finished stripe specifications for alignment, neatness, reflectivity, and straightness. When the centerline or turn lane stripe is removed during milling operations, temporary stripe shall be placed prior to opening the roadway to traffic. All permanent pavement markings on asphalt are to be hot thermoplastic. Edge lines will be placed so as to maintain the original lane width.

(I) Raised pavement markers will be placed at 80-foot intervals in tangents and 40-foot intervals in curves along the centerline of roadway.

(J) Raise the existing shoulders to match the new pavement with Size 825B Crushed Stone Base. Placement of the shoulder material(s) on the finished surface course shall not be permitted. The material(s) shall be bladed, rolled, and compacted to a finished slope of 4% where practical.

(K) It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the Project Engineer, any structures damaged during the life of the contract. No payment will be made for the replacement or repair of damaged items.

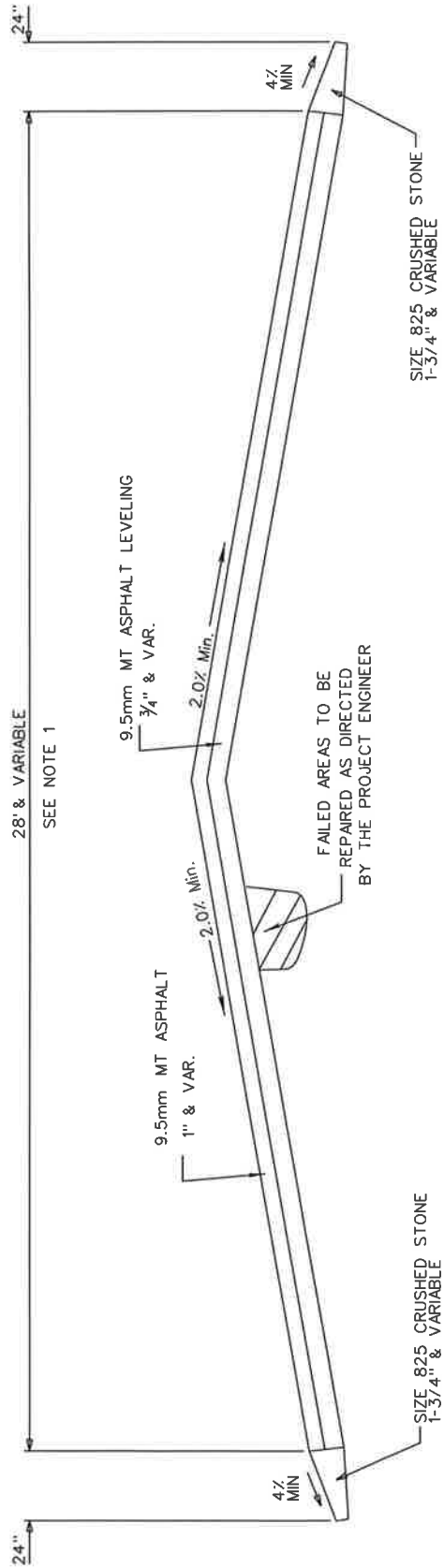
(L) Incidental work such as removing vegetation, shaping and compaction of shoulder, removing excess asphalt material, project clean-up, and other incidental work necessary to complete the project will not be measured for separate payment, but will be included in other bid items.

(M) If pavement section marking tape is encountered on this project, it shall be located prior to overlaying and placed back in the same location after paving operations have ceased. The section marking shall be 8-inch high performance cold plastic detail stripe and shall be four feet (4') in length. The marking shall be centered across the centerline stripe. The cost of this item shall be absorbed in other bid items.

(N) All Construction signing as well as Plastic Traffic Drums and Barricades are included in the bid for Pay Item 618-A, Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except for R4-1 and R4-2 signs which shall be black legend and border on white background. The Contractor shall erect and maintain construction

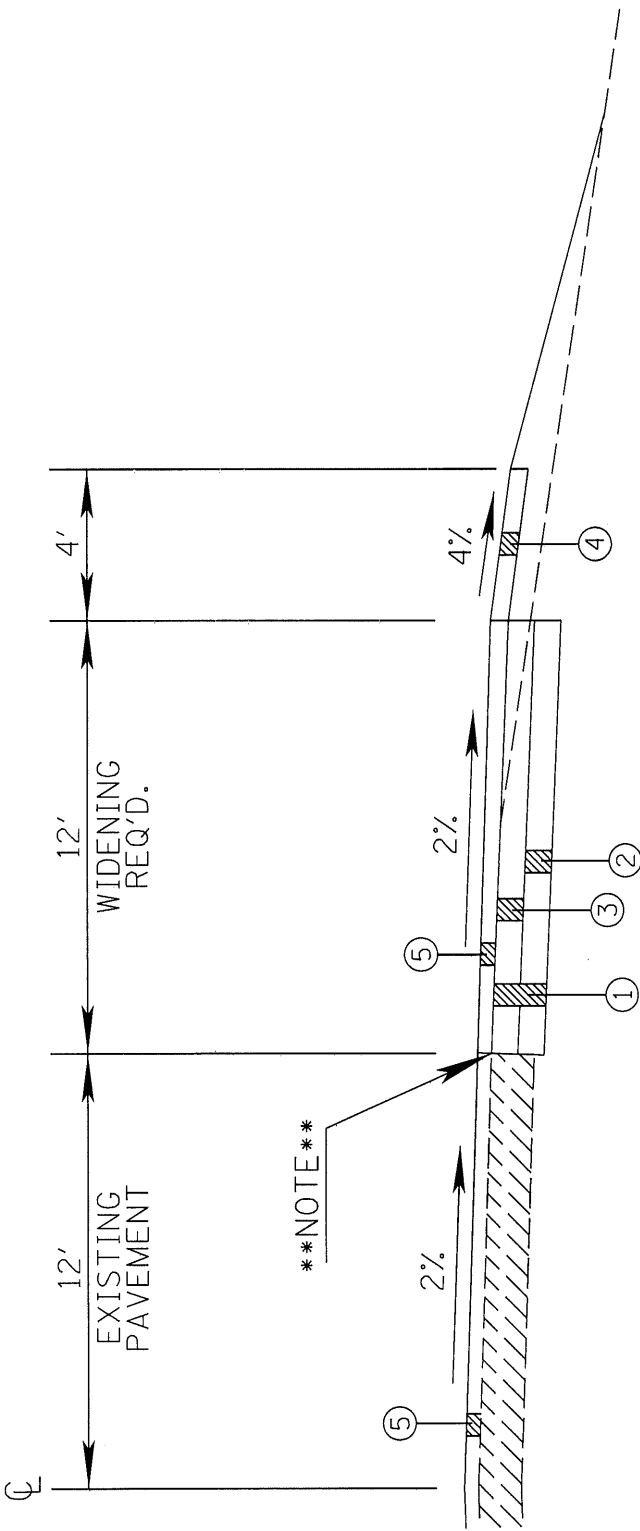


signing, and provide all signs and traffic handling devices in accordance with Manual Uniform Traffic Control Devices (MUTCD).



NOTES:

- (1) TRUE CENTERLINE PAVEMENT ALIGNMENT SHALL BE DETERMINED BY THE CONTRACTOR BY MEASURING THE EXISTING ROADWAY AT 500ft. INTERVALS IN TANGENT SECTIONS, AND 100ft. INTERVALS IN HORIZONTAL CURVES.



\*\*NOTE\*\*

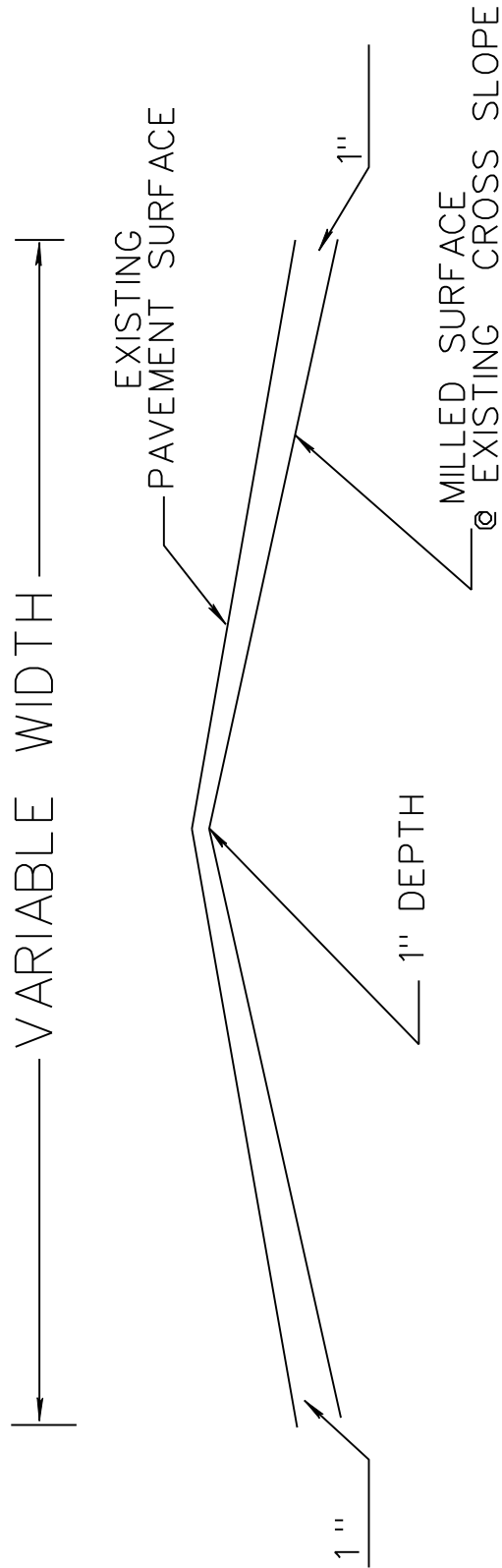
- ① 12" DEPTH UNDERCUT REQ'D MATERIAL FROM UNDERCUT TO BE USED FOR WIDENED SHOULDER (ABSORBED ITEM)
- ② 6" DEPTH CRUSHED STONE BASE REQ'D
- ③ 6" DEPTH ASPHALT REQ'D 2@3" (MT), 19mm MIX
- ④ 1 3/4" DEPTH CRUSHED STONE BASE REQ'D
- ⑤ LEVELING AND OVERLAY WITH MAINLINE

\*\*NOTE\*\*

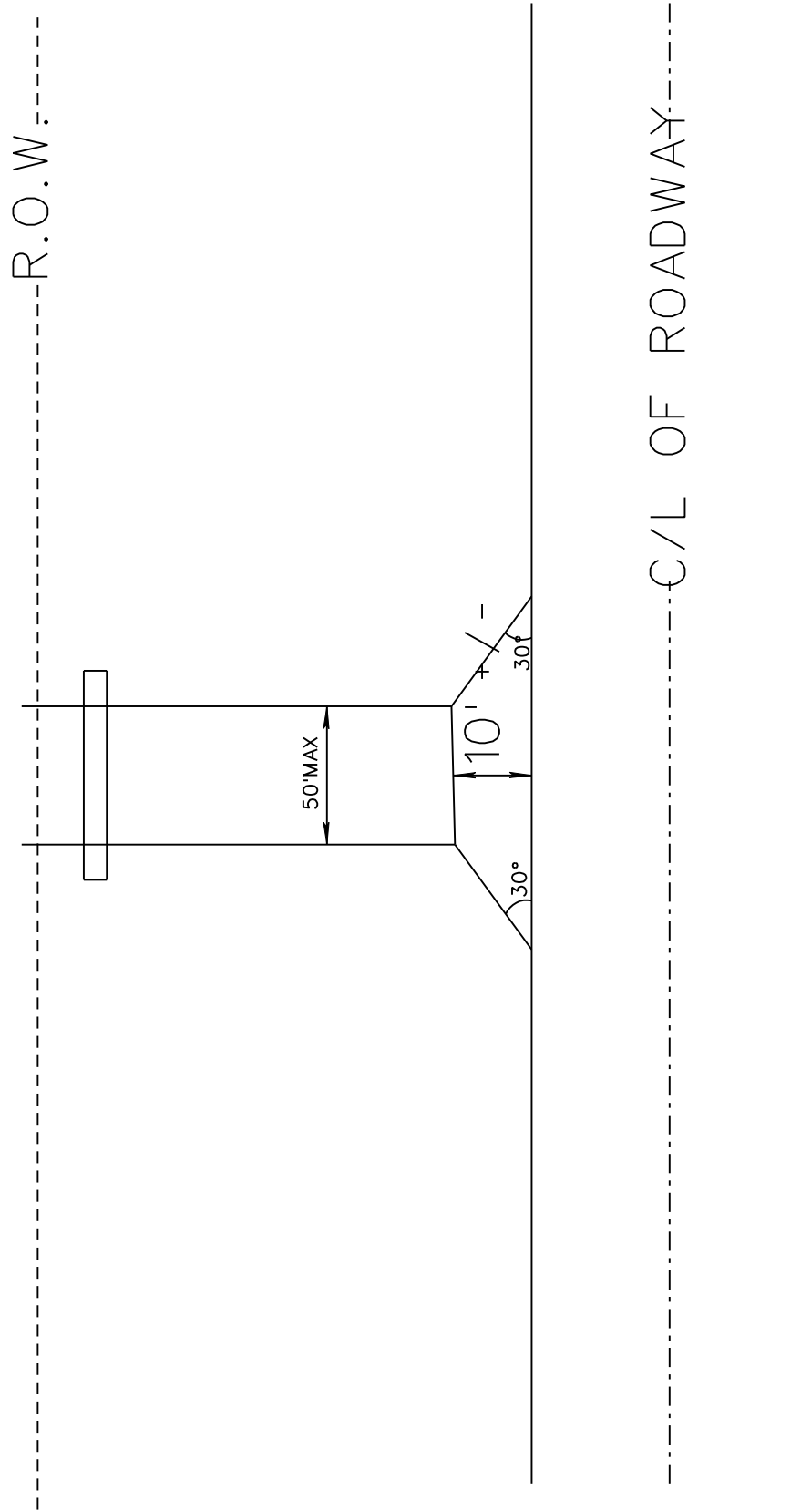
THE EXISTING PAVEMENT EDGE SHALL BE CUT TO A SMOOTH AND NEAR VERTICAL FACE WITH AN APPROVED CUTTING DEVICE. (NOT TO BE MEASURED FOR SEPARATE PAY)

TYPICAL FOR WIDENING

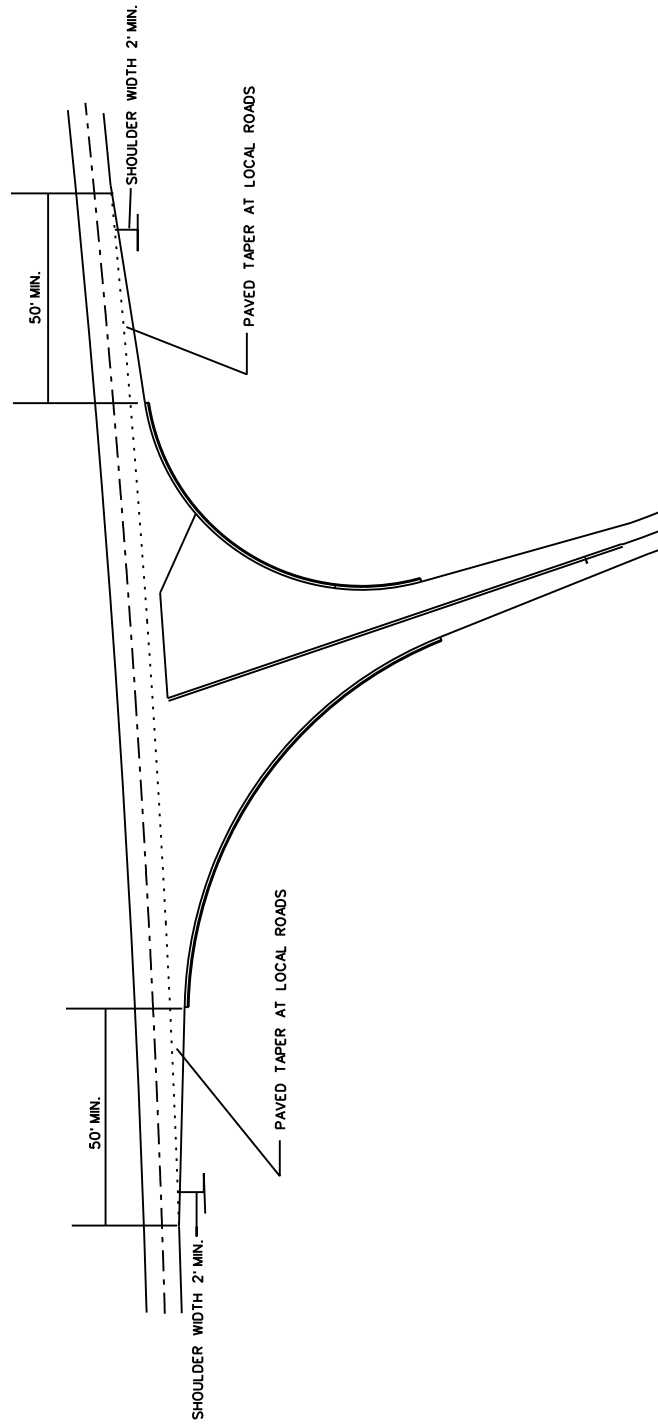
# TYPICAL MILLING DIAGRAM



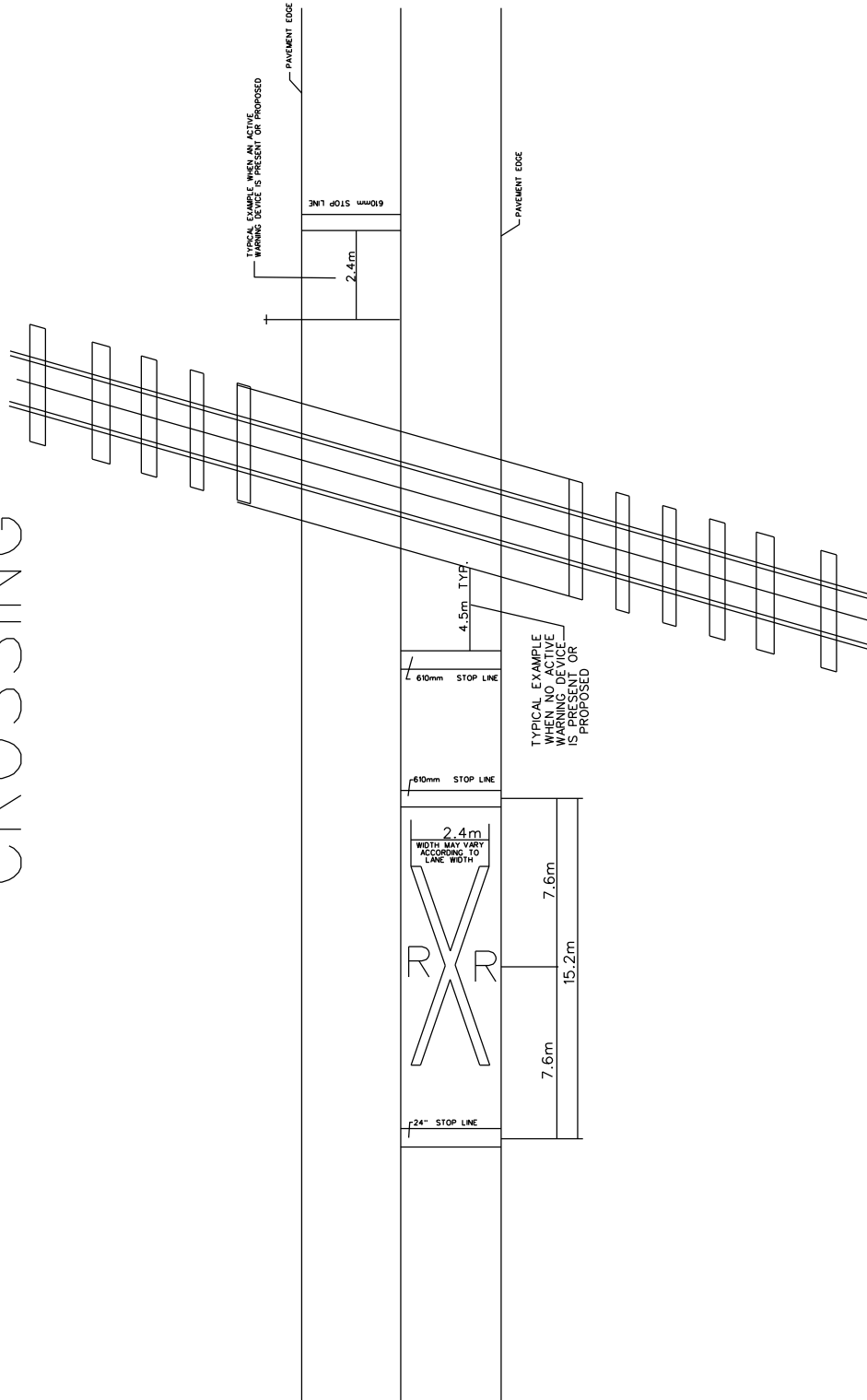
PRIVATE DRIVEWAY DETAIL



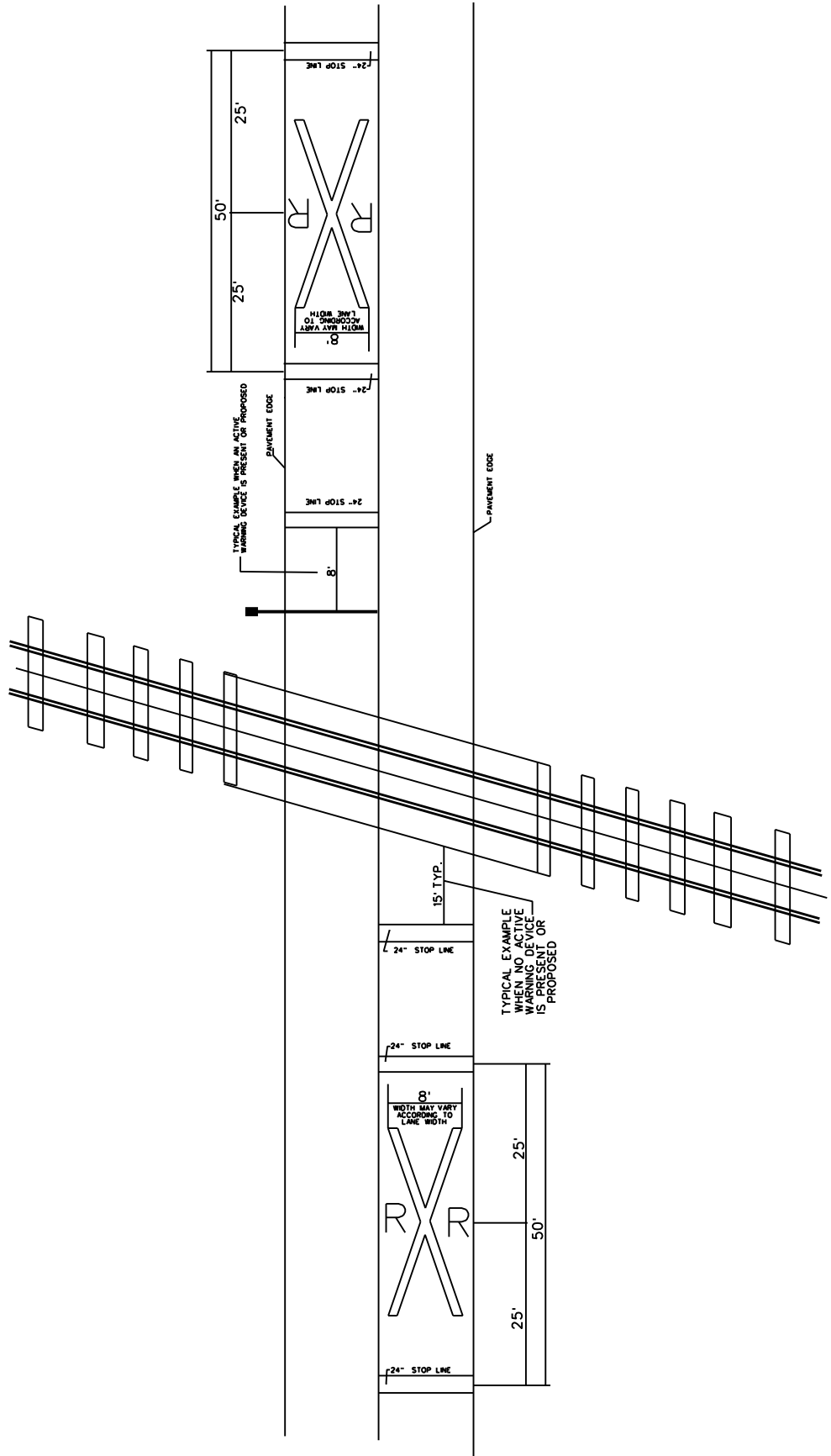
# TYPICAL FOR PAVED TAPER AT LOCAL ROADS



# TYPICAL STRIPING AT RAILROAD CROSSING

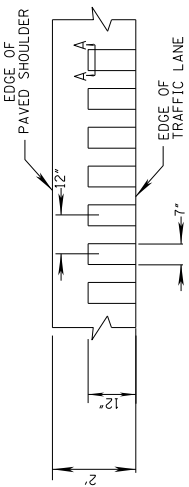


# TYPICAL STRIPING AT RAILROAD CROSSING

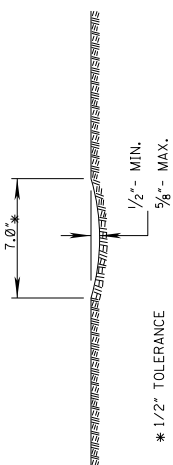




- GENERAL NOTES
1. GROUND-IN RUMBLE STRIPS SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
  2. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED TO MAINLINE ONLY.

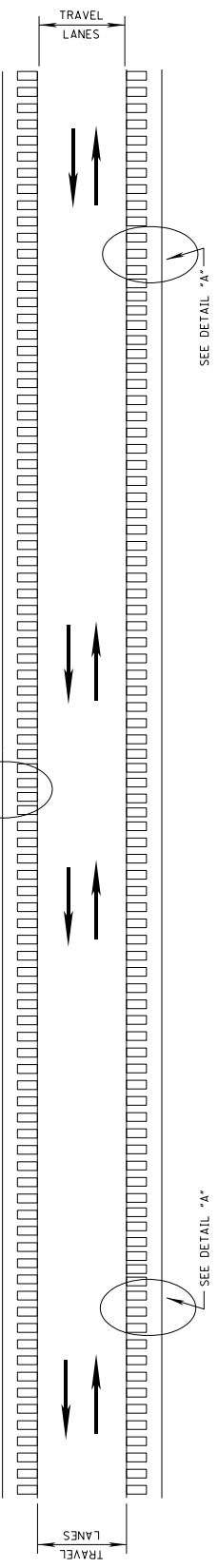


**DETAIL "A"**



**SECTION "A-A"**

\* 1/2" TOLERANCE

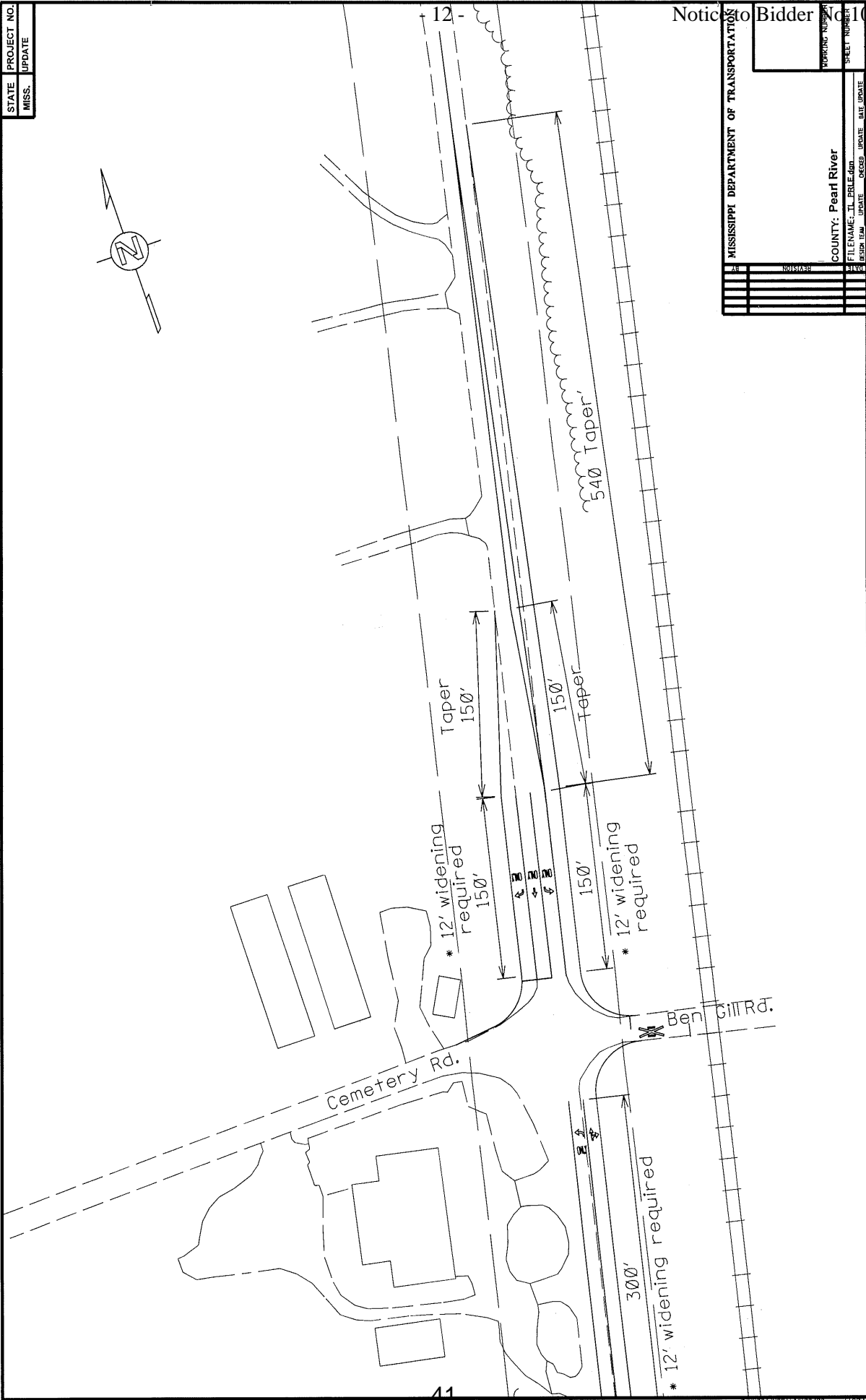
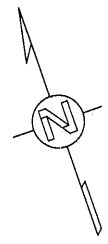


**PLAN**  
NOT TO SCALE  
DETAIL OF  
RUMBLE STRIPS  
(GROUND-IN)

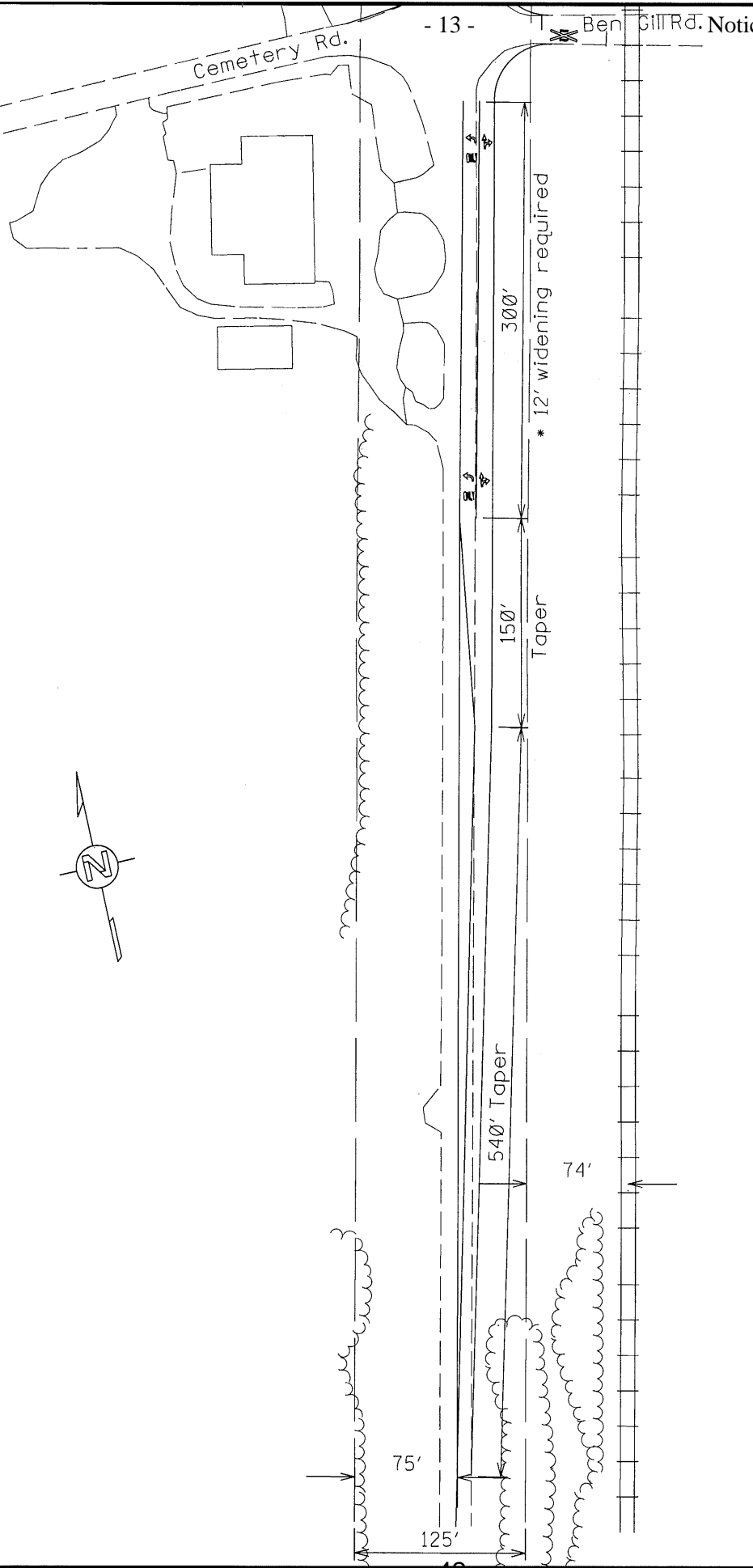
|  |             |
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| MISSISSIPPI DEPARTMENT OF TRANSPORTATION |             |
| TYPICAL SECTION                          |             |
| RUMBLE STRIPS (GROUND-IN)                |             |
| WORKING NUMBER                           | RS          |
| FILE NAME                                | DESIGN TEAM |
| DATE                                     | CREATED     |
| BY                                       | REVISION    |
|  |             |
|  |             |
|  |             |

|       |             |
|-------|-------------|
| STATE | PROJECT NO. |
| MISS. | UPDATE      |

|  |                |
|--|----------------|
| MISSISSIPPI DEPARTMENT OF TRANSPORTATION |                |
| WORKING NUMBER                           | SHEET NUMBER   |
| COUNTY: Pearl River                      |                |
| FILENAME: JI_PRL104                      |                |
| ESOR: JPM                                | UPDATE         |
| DESIGN: JPM                              | UPDATE         |
| DATE: 10/10/00                           | DATE: 10/10/00 |



STATE PROJECT NO.  
MISS.



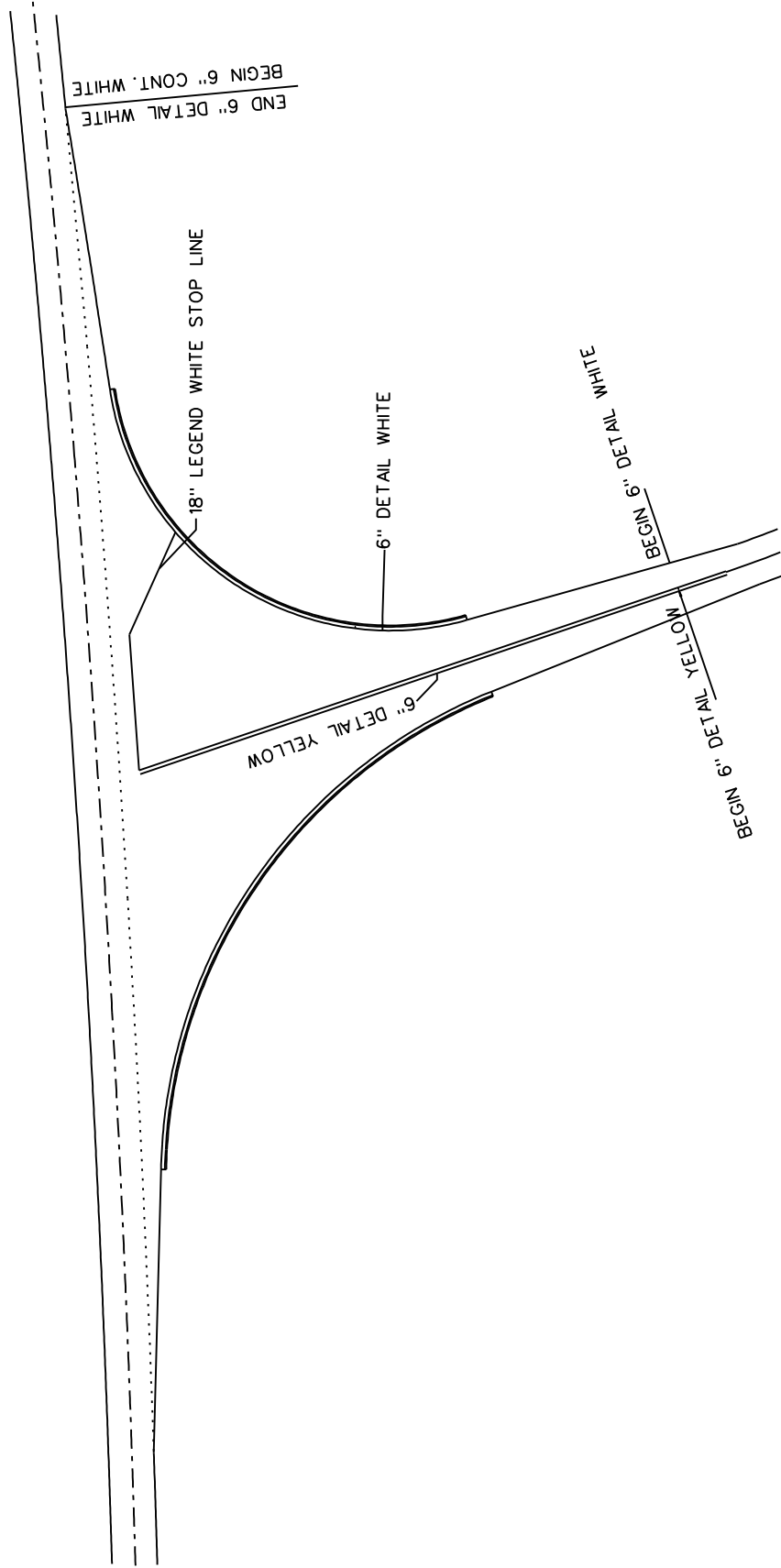
- 13 -

Ben CTRD. Notice

Bidder No. 04 -- Cont'd

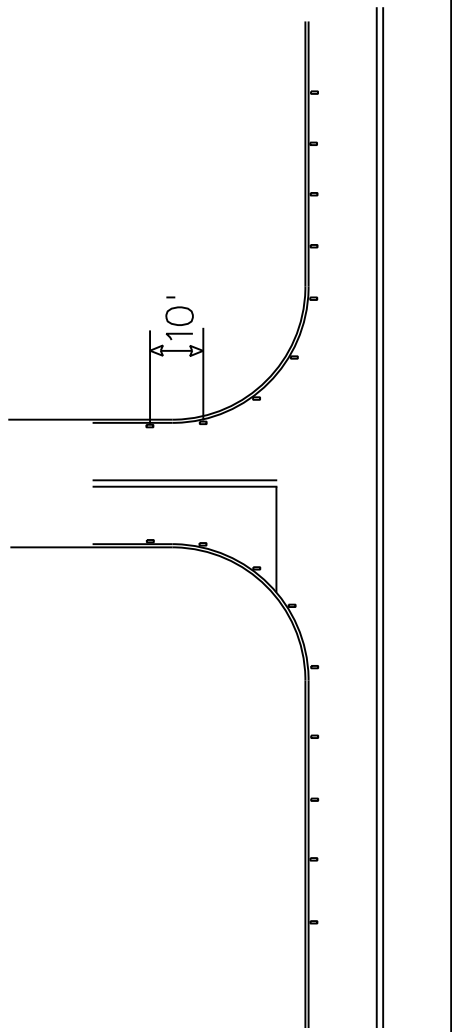
|  |        |
|--|--------|
| MISSISSIPPI DEPARTMENT OF TRANSPORTATION |        |
| WORKING NUMBER                           |        |
| SHEET NUMBER                             |        |
| COUNTY: Pearl River                      |        |
| FILENAME: I.L.PRL.EJOB                   |        |
| DESIGN TEAM                              | UPDATE |
| DESIGNED                                 | UPDATE |
| DATE                                     | UPDATE |

# TYPICAL STRIPING FOR SIMPLE INTERSECTION AT LOCAL ROADS



|       |             |
|-------|-------------|
| STATE | PROJECT NO. |
| MISS. |             |

# TYPICAL FOR RAISED PAVEMENT MARKERS PLACED ON SIDE ROAD RADIUS



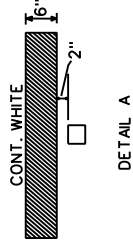
NOTE 1. MARKERS SHALL BE PLACED EVERY 10 FEET.

NOTE 2. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.

NOTE 3. MARKERS SHALL BE HIGH PERFORMANCE TWO WAY CLEAR.

NOTE 4. 5 MARKERS SHALL BE PLACED ALONG MAINLINE EDGE STRIPE.

NOTE 5. MARKERS FOR COUNTY RD. SHALL BEGIN AT RADIUS AND CONTINUE DOWN EDGE STRIPE A DISTANCE OF 40 FEET.



DETAIL A

|  |              |
|--|--------------|
| MISSISSIPPI DEPARTMENT OF TRANSPORTATION |              |
| 2-LANE                                   |              |
| RAISED PAVEMENT MARKERS                  |              |
| PLACED ON SIDE ROADS                     |              |
| DATE                                     | DATE         |
| FILE NAME:                               | FILE NAME:   |
| DRAWING NUMBER                           | SHEET NUMBER |

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 113**

**CODE: (SP)**

**DATE: 04/18/2017**

**SUBJECT: Tack Coat**

Bidders are advised that in addition to the products listed on the Department's APL as referenced in Subsection 401.03.1.2 on page 256, the Contractor may use one of the following as a tack coat.

- CSS-1
- CSS-1h
- SS-1
- SS-1h

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

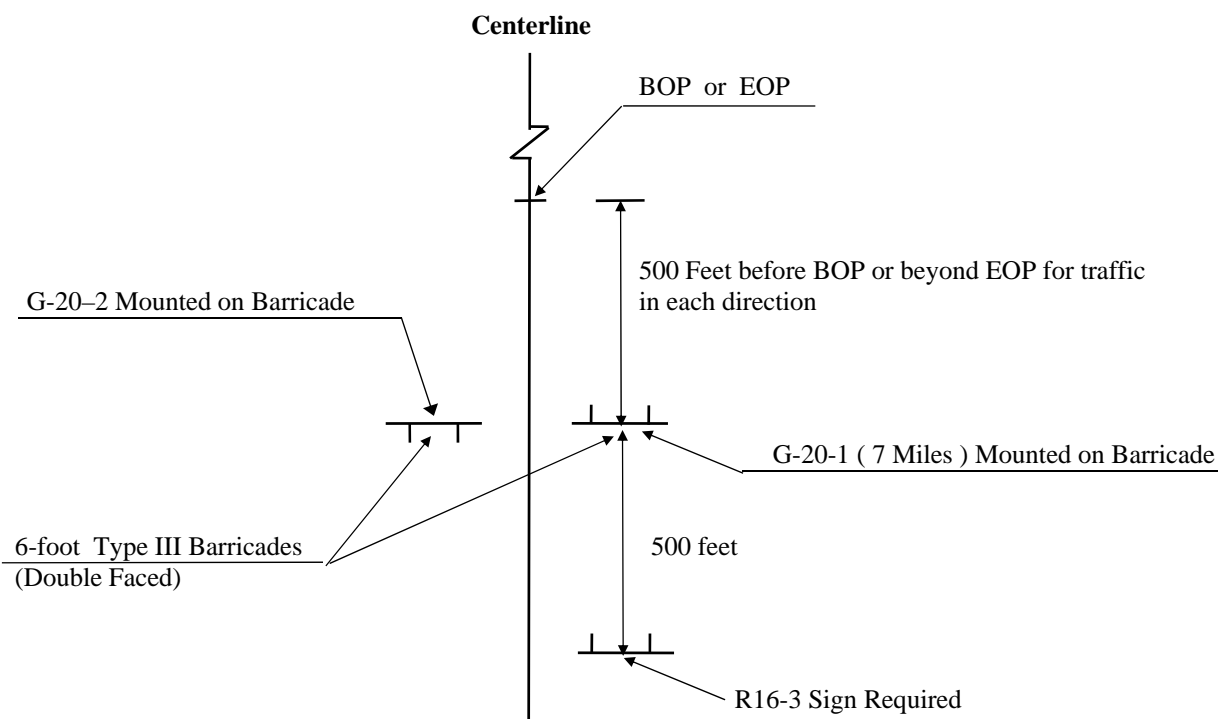
## SUPPLEMENT TO SPECIAL PROVISION NO. 907-618-3

**DATE:** 03/22/2017

**PROJECT:** MP-6011-55(022) / 306147301 - Pearl River County

After the first paragraph of Subsection 907-618.01.2 on page 1, add the following.

Additional signs will be required as follows.



### ADDITIONAL TRAFFIC CONTROL SIGNS REQUIRED:

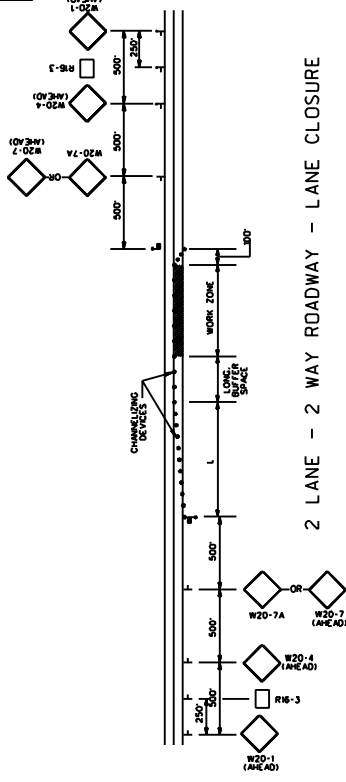
- 13 - W20-1 "AHEAD" signs required. One (1) W20-1 "AHEAD" sign is required at each local road or street entering the project.
- 21 - R4-1 "DO NOT PASS" signs required.
- 20 - R4-2 "PASS WITH CARE" signs required.
- 24 - W14-3 "NO PASSING ZONE" signs required.
- 2 - R16-3 "SPEEDING FINES DOUBLED" signs required.

R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE", and W14-3 "NO PASSING ZONE" signs are required in accordance with Subsection 618.03.3, this drawing, and as specified in the Manual on Uniform Traffic Control Devices. If no passing zones are 1,000 feet or more, additional "DO NOT PASS" signs shall be installed.

R16-3 signs shall be spaced in accordance with sheet titled "Location of R16-3 Signs".

All Construction signing is included in the bid for Pay Item 618-A, Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except for R16-3, R4-1 and R4-2 signs which shall be black legend and border on white background.

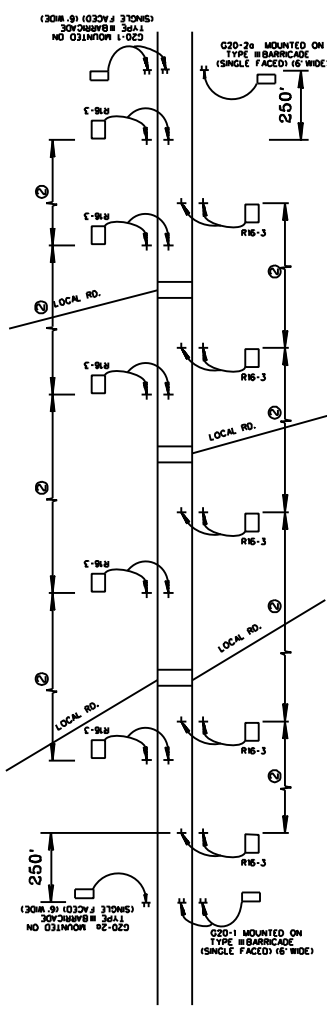
STATE PROJECT NO.  
MISS.



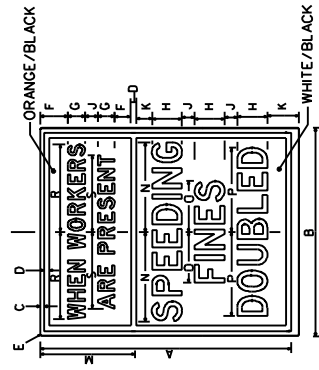
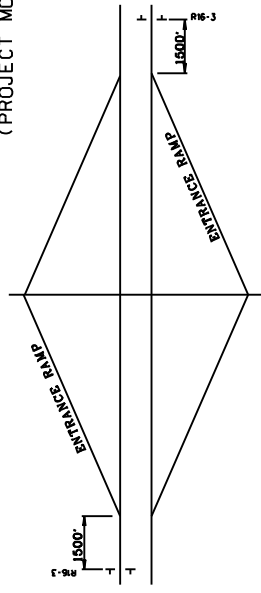
2 LANE - 2 WAY ROADWAY - LANE CLOSURE

DIVIDED HIGHWAY  
(PROJECTS LESS THAN 1 MILE LENGTH)

- NOTES
- Ø R16-3 SIGN TO BE PLACED AS SHOWN OR AS DIRECTED BY THE ENGINEER.
  - ⊙ R16-3 SIGN SHALL BE SPACED AT A MAXIMUM OF 2 MILES THROUGHOUT LENGTH OF PROJECT.



DIVIDED HIGHWAY SHOWN  
(2 LANE - 2 WAY ROADWAY SIMILAR)  
(PROJECT MORE THAN 1 MILE LENGTH)



| SIGN | DIMENSIONS (INCHES) |       |       |       |       |       |       |       |       |       |       |       |       |
|------|---------------------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|
| S10  | A                   | B     | C     | D     | E     | F     | G     | H     | I     | J     | K     | L     | M     |
|      | 60                  | 48    | 3/4   | 1/4   | 3/4   | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 |
|      | 1 1/2               | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 |
| S11  | A                   | B     | C     | D     | E     | F     | G     | H     | I     | J     | K     | L     | M     |
|      | 60                  | 48    | 3/4   | 1/4   | 3/4   | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 |
|      | 1 1/2               | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 | 1 1/2 |

48" x 60" (INTERSTATE USE)

36" x 48" (ALL OTHER HIGHWAYS)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

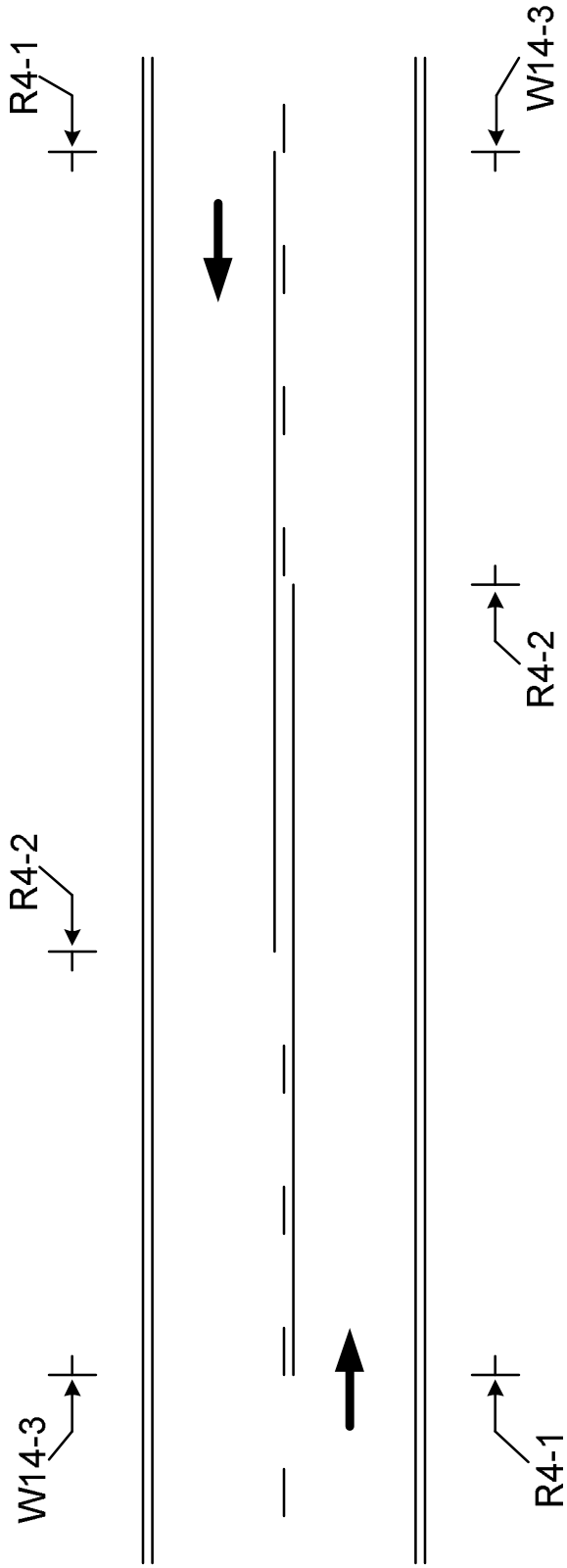
LOCATION OF R16-3 SIGNS

|          |    |      |
|----------|----|------|
| REVISION | BY | DATE |
|          |    |      |
|          |    |      |
|          |    |      |

WORKING NUMBER: \_\_\_\_\_  
SHEET NUMBER: \_\_\_\_\_  
CHECKED: \_\_\_\_\_  
DATE: 02/08/09

FILENAME: SPEED\_SIGN\_DETAIL.DWG  
PROJECT: \_\_\_\_\_





The W14-3, No Passing Zone sign, shall be placed on the left side of the road at the beginning of each no passing zone.

The R4-1, Do Not Pass signs, shall be placed on the right side of the road at the beginning of the no passing zone. Additional R4-1 signs shall be placed right and left in increments of 750 to 1000 feet throughout the length of the no passing zone.

The R4-2, Pass With Care sign, shall be placed on the right side of the road at the end of the no passing zone.

The R4-1, R4-2 and W14-3 signs are to be used when standard pavement markings are not in place. The signs may also be used to emphasize pavement markings.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SPECIAL PROVISION NO. 907-618-3**

**CODE: (SP)**

**DATE: 01/17/2017**

**SUBJECT: Additional Signing Requirements**

Section 618, Maintenance of Traffic and Traffic Control Plan, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

**907-618.01.2--Traffic Control Plan.** At the end of Subsection 618.01.2 on page 441, add the following:

For compliance with the traffic control plan, the Contractor will be required to install and maintain construction signs at various locations throughout the project. Payment for these signs will be included in the price bid for pay item no. 618-A, Maintenance of Traffic per lump sum.

# SECTION 905 - PROPOSAL

Date \_\_\_\_\_

Mississippi Transportation Commission  
Jackson, Mississippi

Sirs: The following proposal is made on behalf of \_\_\_\_\_  
\_\_\_\_\_ of \_\_\_\_\_

for constructing the following designated project(s) within the time(s) hereinafter specified.

The plans are composed of drawings and blue prints on file in the offices of the Mississippi Department of Transportation, Jackson, Mississippi.

The Specifications are the current Standard Specifications of the Mississippi Department of Transportation approved by the Federal Highway Administration, except where superseded or amended by the plans, Special Provisions and Notice(s) to Bidders attached hereto and made a part thereof.

I (We) certify that I (we) possess a copy of said Standard and any Supplemental Specifications.

Evidence of my (our) authority to submit the Proposal is hereby furnished. The proposal is made without collusion on the part of any person, firm or corporation. I (We) certify that I (we) have carefully examined the Plans, the Specifications, including the Special Provisions and Notice(s) to Bidders, herein, and have personally examined the site of the work. On the basis of the Specifications, Special Provisions, Notice(s) to Bidders, and Plans, I (we) propose to furnish all necessary machinery, tools, apparatus and other means of construction and do all the work and furnish all the materials in the manner specified. I (We) understand that the quantities mentioned herein are approximate only and are subject to either increase or decrease, and hereby propose to perform any increased or decreased quantities of work at the unit prices bid, in accordance with the above.

I (We) acknowledge that this proposal will be found irregular and/or non-responsive unless a certified check, cashier's check, or Proposal Guaranty Bond in the amount as required in the Advertisement (or, by law) is submitted electronically with the proposal or is delivered to the Contract Administration Engineer prior to the bid opening time specified in the advertisement.

**INSTRUCTION TO BIDDERS: Alternate and Optional Items on Bid Schedule.**

1. Two or more items entered opposite a single unit quantity WITHOUT DEFINITE DESIGNATION AS "ALTERNATE ITEMS" are considered as "OPTIONAL ITEMS". Bidders may or may not indicate on bids the Optional Item proposed to be furnished or performed WITHOUT PREJUDICE IN REGARD TO IRREGULARITY OF BIDS.
2. Items classified on the bid schedule as "ALTERNATE ITEMS" and/or "ALTERNATE TYPES OF CONSTRUCTION" must be preselected and indicated on bids. However, "Alternate Types of Construction" may include Optional Items to be treated as set out in Paragraph 1, above.
3. Optional items not preselected and indicated on the bid schedule MUST be designated in accordance with Subsection 102.06 prior to or at the time of execution of the contract.
4. Optional and Alternate items designated must be used throughout the project.

I (We) further propose to perform all "force account or extra work" that may be required of me (us) on the basis provided in the Specifications and to give such work my (our) personal attention in order to see that it is economically performed.

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) shall submit electronically with our proposal or deliver prior to the bid opening time a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

**SECTION 905 -- PROPOSAL (CONTINUED)**

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Respectfully Submitted,

DATE \_\_\_\_\_

\_\_\_\_\_  
Contractor

BY \_\_\_\_\_  
Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

\_\_\_\_\_  
President Address

\_\_\_\_\_  
Secretary Address

\_\_\_\_\_  
Treasurer Address

The following is my (our) itemized proposal.

Overlay approximately 7 miles of US 11 from South Apple Dr. to Millard, known as State Project No. MP-6011-55(022) / 306147301 in Pearl River County.

| Line no.             | Item Code | Adj Code | Quantity | Units       | Description[Fixed Unit Price]                              |
|----------------------|-----------|----------|----------|-------------|--|
| <b>Roadway Items</b> |           |          |          |             |  |
| 0010                 | 202-B188  |          | 90       | Square Yard | Removal of Pavement, All Types and Depths                  |
| 0020                 | 304-F003  | (GT)     | 4,000    | Ton         | Size 825B Crushed Stone Base                               |
| 0030                 | 403-A006  | (BA1)    | 290      | Ton         | 19-mm, ST, Asphalt Pavement                                |
| 0040                 | 403-A014  | (BA1)    | 9,500    | Ton         | 9.5-mm, MT, Asphalt Pavement                               |
| 0050                 | 403-B011  | (BA1)    | 4,000    | Ton         | 9.5-mm, MT, Asphalt Pavement, Leveling                     |
| 0060                 | 406-A002  |          | 2,600    | Square Yard | Cold Milling of Bituminous Pavement, All Depths            |
| 0070                 | 407-A001  | (A2)     | 13,000   | Gallon      | Asphalt for Tack Coat                                      |
| 0080                 | 413-E001  |          | 23,200   | Linear Feet | Sawing and Sealing Transverse Joints in Asphalt Pavement   |
| 0090                 | 423-A001  |          | 13       | Mile        | Rumble Strips, Ground In                                   |
| 0100                 | 618-A001  |          | 1        | Lump Sum    | Maintenance of Traffic                                     |
| 0110                 | 618-B001  |          | 1        | Square Feet | Additional Construction Signs (\$10.00)                    |
| 0120                 | 619-A1001 |          | 26       | Mile        | Temporary Traffic Stripe, Continuous White                 |
| 0130                 | 619-A2001 |          | 17       | Mile        | Temporary Traffic Stripe, Continuous Yellow                |
| 0140                 | 619-A4002 |          | 8        | Mile        | Temporary Traffic Stripe, Skip Yellow                      |
| 0150                 | 620-A001  |          | 1        | Lump Sum    | Mobilization   |
| 0160                 | 626-B004  |          | 4        | Mile        | 6" Thermoplastic Traffic Stripe, Continuous White          |
| 0170                 | 626-C002  |          | 13       | Mile        | 6" Thermoplastic Double Drop Edge Stripe, Continuous White |
| 0180                 | 626-E004  |          | 9        | Mile        | 6" Thermoplastic Traffic Stripe, Continuous Yellow         |
| 0190                 | 626-G002  |          | 2,000    | Linear Feet | Thermoplastic Detail Stripe, White                         |
| 0200                 | 626-H004  |          | 495      | Square Feet | Thermoplastic Legend, White                                |
| 0210                 | 626-H005  |          | 1,250    | Linear Feet | Thermoplastic Legend, White                                |
| 0220                 | 627-J001  |          | 240      | Each        | Two-Way Clear Reflective High Performance Raised Markers   |
| 0230                 | 627-K001  |          | 30       | Each        | Red-Clear Reflective High Performance Raised Markers       |
| 0240                 | 627-L001  |          | 525      | Each        | Two-Way Yellow Reflective High Performance Raised Markers  |

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

**CONDITIONS FOR COMBINATION BID**

If a bidder elects to submit a combined bid for two or more of the contracts listed for this month's letting, the bidder must complete and execute these sheets of the proposal in each of the individual proposals to constitute a combination bid. In addition to this requirement, each individual contract shall be completed, executed and submitted in the usual specified manner.

Failure to execute this Combination Bid Proposal in each of the contracts combined will be just cause for each proposal to be received and evaluated as a separate bid.

It is understood that the Mississippi Transportation Commission not only reserves the right to reject any and all proposals, but also the right to award contracts upon the basis of lowest separate bids or combination bids most advantageous to the State.

It is further understood and agreed that the Combination Bid Proposal is for comparison of bids only and that each contract shall operate in every respect as a separate contract in accordance with its proposal and contract documents.

I (We) agree to complete each contract on or before its specified completion date.

\*\*\*\*\*

**COMBINATION BID PROPOSAL**

This proposal is tendered as one part of a Combination Bid Proposal utilizing option \_\_\_\* of Subsection 102.11 on the following contracts:

\* Option to be shown as either (a), (b), or (c).

|    | <u>Project No.</u> | <u>County</u> | <u>Project No.</u> | <u>County</u> |
|----|--------------------|---------------|--------------------|---------------|
| 1. | _____              | _____         | 6.                 | _____         |
| 2. | _____              | _____         | 7.                 | _____         |
| 3. | _____              | _____         | 8.                 | _____         |
| 4. | _____              | _____         | 9.                 | _____         |
| 5. | _____              | _____         | 10.                | _____         |

(a) If Combination A has been selected, your Combination Bid is complete.

(b) If Combination B has been selected, then complete the following page.

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

| Project Number | Pay Item Number | Unit | Unit Price Reduction | Total Item Reduction | Total Contract Reduction |
|----------------|-----------------|------|----------------------|----------------------|--------------------------|
| 1.             |                 |      |                      |                      |                          |
| 2.             |                 |      |                      |                      |                          |
| 3.             |                 |      |                      |                      |                          |
| 4.             |                 |      |                      |                      |                          |
| 5.             |                 |      |                      |                      |                          |
| 6.             |                 |      |                      |                      |                          |
| 7.             |                 |      |                      |                      |                          |
| 8.             |                 |      |                      |                      |                          |

For Informational Purposes Only

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

| Project Number | Pay Item Number | Unit | Unit Price Reduction | Total Item Reduction | Total Contract Reduction |
|----------------|-----------------|------|----------------------|----------------------|--------------------------|
| 9.             |                 |      |                      |                      |                          |
|                |                 |      |                      |                      |                          |
|                |                 |      |                      |                      |                          |
| 10.            |                 |      |                      |                      |                          |
|                |                 |      |                      |                      |                          |
|                |                 |      |                      |                      |                          |

(c) If Combination C has been selected, then initial and complete ONE of the following.

\_\_\_\_\_ I (We) desire to be awarded work not to exceed a total monetary value of \$ \_\_\_\_\_.

\_\_\_\_\_ I (We) desire to be awarded work not to exceed \_\_\_\_\_ number of contracts.





TO: EXECUTIVE DIRECTOR, MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
JACKSON, MISSISSIPPI

**CERTIFICATE**

If awarded this contract, I (we) contemplate that portions of the contract will be sublet. I (we) certify that those subcontracts which are equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

I (we) agree that this notification of intent DOES NOT constitute APPROVAL of the subcontracts.

|                      |           |
|----------------------|-----------|
| _____                | _____     |
| (Individual or Firm) | (Address) |
| _____                | _____     |
| (Individual or Firm) | (Address) |
| _____                | _____     |
| (Individual or Firm) | (Address) |
| _____                | _____     |
| (Individual or Firm) | (Address) |

NOTE: Failure to complete the above DOES NOT preclude subsequent subcontracts. Subsequent subcontracts, if any, equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

Contractor \_\_\_\_\_

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**  
**CERTIFICATION**

I, \_\_\_\_\_,  
(Name of person signing bid)

individually, and in my capacity as \_\_\_\_\_ of  
(Title of person signing bid)

\_\_\_\_\_  
(Name of Firm, partnership, or Corporation)

do hereby certify under penalty of perjury under the laws of the United States and the State of Mississippi

that \_\_\_\_\_, Bidder  
(Name of Firm, Partnership, or Corporation)

on Project No. **MP-6011-55(022)/ 306147301000**

in **Pearl River** \_\_\_\_\_ County(ies), Mississippi, has not either directly or indirectly entered into any agreement, participated in any collusion; or otherwise taken any action in restraint of free competitive bidding in connection with this contract; nor have any of its corporate officers or principal owners.

Except as noted hereafter, it is further certified that said legal entity and its corporate officers, principal owners, managers, auditors and others in a position of administering federal funds are not currently under suspension, debarment, voluntary exclusion or determination of ineligibility; nor have a debarment pending; nor been suspended, debarred, voluntarily excluded or determined ineligible within the past three years by the Mississippi Transportation Commission, the State of Mississippi, any other State or a federal agency; nor been indicted, convicted or had a civil judgment rendered by a court of competent jurisdiction in any matter involving fraud or official misconduct within the past three years.

Do exceptions exist and are made a part thereof?            Yes / No

Any exceptions shall address to whom it applies, initiating agency and dates of such action.

Note: Exceptions will not necessarily result in denial of award but will be considered in determining bidder responsibility. Providing false information may result in criminal prosecution or administrative sanctions.

All of the foregoing is true and correct.

(1/2016 S)

SECTION 902

CONTRACT FOR MP-6011-55(022)/ 306147301000

LOCATED IN THE COUNTY(IES) OF Pearl River

STATE OF MISSISSIPPI,  
COUNTY OF HINDS

This contract entered into by and between the Mississippi Transportation Commission on one hand, and the undersigned contractor, on the other witnesseth;

That, in consideration of the payment by the Mississippi Transportation Commission of the prices set out in the proposal hereto attached, to the undersigned contractor, such payment to be made in the manner and at the time of times specified in the specifications and the special provisions, if any, the undersigned contractor hereby agrees to accept the prices stated in the proposal in full compensation for the furnishing of all materials and equipment and the executing of all the work contemplated in this contract.

It is understood and agreed that the advertising according to law, the Advertisement, the instructions to bidders, the proposal for the contract, the specifications, the revisions of the specifications, the special provisions, and also the plans for the work herein contemplated, said plans showing more particularly the details of the work to be done, shall be held to be, and are hereby made a part of this contract by specific reference thereto and with like effect as if each and all of said instruments had been set out fully herein in words and figures.

It is further agreed that for the same consideration the undersigned contractor shall be responsible for all loss or damage arising out of the nature of the work aforesaid; or from the action of the elements and unforeseen obstructions or difficulties which may be encountered in the prosecution of the same and for all risks of every description connected with the work, exceptions being those specifically set out in the contract; and for faithfully completing the whole work in good and workmanlike manner according to the approved Plans, Specifications, Special Provisions, Notice(s) to Bidders and requirements of the Mississippi Department of Transportation.

It is further agreed that the work shall be done under the direct supervision and to the complete satisfaction of the Executive Director of the Mississippi Department of Transportation, or his authorized representatives, and when Federal Funds are involved subject to inspection at all times and approval by the Federal Highway Administration, or its agents as the case may be, or the agents of any other Agency whose funds are involved in accordance with those Acts of the Legislature of the State of Mississippi approved by the Governor and such rules and regulations issued pursuant thereto by the Mississippi Transportation Commission and the authorized Federal Agencies.

The Contractor agrees that all labor as outlined in the Special Provisions may be secured from list furnished by

It is agreed and understood that each and every provision of law and clause required by law to be inserted in this contract shall be deemed to be inserted herein and this contract shall be read and enforced as though it were included herein, and, if through mere mistake or otherwise any such provision is not inserted, then upon the application of either party hereto, the contract shall forthwith be physically amended to make such insertion.

The Contractor agrees that he has read each and every clause of this Contract, and fully understands the meaning of same and that he will comply with all the terms, covenants and agreements therein set forth.

Witness our signatures this the \_\_\_ day of \_\_\_\_\_, \_\_\_\_\_.

\_\_\_\_\_  
Contractor(s)

By \_\_\_\_\_

MISSISSIPPI TRANSPORTATION COMMISSION

Title \_\_\_\_\_

By \_\_\_\_\_

Signed and sealed in the presence of:  
(names and addresses of witnesses)

Executive Director

\_\_\_\_\_

\_\_\_\_\_

Secretary to the Commission

Award authorized by the Mississippi Transportation Commission in session on the \_\_\_ day of \_\_\_\_\_, \_\_\_\_\_, Minute Book No. \_\_\_\_\_, Page No. \_\_\_\_\_.

**SECTION 903**  
**PERFORMANCE AND PAYMENT BOND**

CONTRACT BOND FOR: MP-6011-55(022)/ 306147301000

LOCATED IN THE COUNTY(IES) OF: Pearl River

STATE OF MISSISSIPPI,  
COUNTY OF HINDS

Know all men by these presents: that we, \_\_\_\_\_  
\_\_\_\_\_  
(Contractor)

Principal, a \_\_\_\_\_

residing at \_\_\_\_\_ in the State of \_\_\_\_\_

and \_\_\_\_\_

(Surety)  
residing at \_\_\_\_\_ in the State of \_\_\_\_\_,

authorized to do business in the State of Mississippi, under the laws thereof, as surety, effective as of the contract date shown below, are held and firmly bound unto the State of Mississippi in the sum of \_\_\_\_\_

\_\_\_\_\_ Dollars, lawful money of the United States of America, to be paid to it for which payment well and truly to be made, we bind ourselves, our heirs, administrators, successors, or assigns jointly and severally by these presents.

The conditions of this bond are such, that whereas the said \_\_\_\_\_

\_\_\_\_\_ principal, has (have) entered into a contract with the Mississippi Transportation Commission, bearing the date of \_\_\_\_\_ day of \_\_\_\_\_ A.D. \_\_\_\_\_ hereto annexed, for the construction of certain projects(s) in the State of Mississippi as mentioned in said contract in accordance with the Contract Documents therefor, on file in the offices of the Mississippi Department of Transportation, Jackson, Mississippi.

Now therefore, if the above bounden \_\_\_\_\_ in all things shall stand to and abide by and well and truly observe, do keep and perform all and singular the terms, covenants, conditions, guarantees and agreements in said contract, contained on his (their) part to be observed, done, kept and performed and each of them, at the time and in the manner and form and furnish all of the material and equipment specified in said contract in strict accordance with the terms of said contract which said plans, specifications and special provisions are included in and form a part of said contract and shall maintain the said work contemplated until its final completion and acceptance as specified in Subsection 109.11 of the approved specifications, and save harmless said Mississippi Transportation Commission from any loss or damage arising out of or occasioned by the negligence, wrongful or criminal act, overcharge, fraud, or any other loss or damage whatsoever, on the part of said principal (s), his (their) agents, servants, or employees in the performance of said work or in any manner connected therewith, and shall be liable and responsible in a civil action instituted by the State at the instance of the Mississippi Transportation Commission or any officer of the State authorized in such cases, for double any amount in money or property, the State may lose or be overcharged or otherwise defrauded of, by reason of wrongful or criminal act, if any, of the Contractor(s), his (their) agents or employees, and shall promptly pay the said agents, servants and employees and all persons furnishing labor, material, equipment or supplies therefor, including premiums incurred, for Surety Bonds, Liability Insurance, and Workmen's Compensation Insurance; with the additional obligation that such Contractor shall promptly make payment of all taxes, licenses, assessments, contributions, damages,

any liquidated damages which may arise prior to any termination of said principal's contract, any liquidated damages which may arise after termination of the said principal's contract due to default on the part of said principal, penalties and interest thereon, when and as the same may be due this state, or any county, municipality, board, department, commission or political subdivision: in the course of the performance of said work and in accordance with Sections 31-5-51 et seq. Mississippi Code of 1972, and other State statutes applicable thereto, and shall carry out to the letter and to the satisfaction of the Executive Director of the Mississippi Department of Transportation, all, each and every one of the stipulations, obligations, conditions, covenants and agreements and terms of said contract in accordance with the terms thereof and all of the expense and cost and attorney's fee that may be incurred in the enforcement of the performance of said contract, or in the enforcement of the conditions and obligations of this bond, then this obligation shall be null and void, otherwise to be and remain in full force and virtue.

|                         |                                 |
|-------------------------|---------------------------------|
| _____                   | _____                           |
| (Contractors) Principal | Surety                          |
| By _____                | By _____                        |
|                         | (Signature) Attorney in Fact    |
|                         | Address _____                   |
|                         | _____                           |
| Title _____             | _____                           |
| (Contractor's Seal)     | (Printed) MS Agent              |
|                         | _____                           |
|                         | (Signature) MS Agent            |
|                         | Address _____                   |
|                         | _____                           |
|                         | _____                           |
|                         | (Surety Seal)                   |
|                         | _____                           |
|                         | Mississippi Insurance ID Number |



# BID BOND

KNOW ALL MEN BY THESE PRESENTS, that we \_\_\_\_\_  
Contractor

\_\_\_\_\_  
Address

\_\_\_\_\_  
City, State ZIP

As principal, hereinafter called the Principal, and \_\_\_\_\_  
Surety

a corporation duly organized under the laws of the state of \_\_\_\_\_

as Surety, hereinafter called the Surety, are held and firmly bound unto State of Mississippi, Jackson, Mississippi

As Obligee, hereinafter called Obligee, in the sum of **Five Per Cent (5%) of Amount Bid**

Dollars(\$ \_\_\_\_\_ )

for the payment of which sum will and truly to be made, the said Principal and said Surety, bind ourselves, our heirs, executors, administrators, successors and assigns, jointly and severally, firmly by these presents.

WHEREAS, the Principal has submitted a bid for **Overlay approximately 7 miles of US 11 from South Apple Dr. to Millard, known as State Project No. MP-6011-55(022) / 306147301 in Pearl River County.**

NOW THEREFORE, the condition of this obligation is such that if the aforesaid Principal shall be awarded the contract, the said Principal will, within the time required, enter into a formal contract and give a good and sufficient bond to secure the performance of the terms and conditions of the contract, then this obligation to be void; otherwise the Principal and Surety will pay unto the Obligee the difference in money between the amount of the bid of the said Principal and the amount for which the Obligee legally contracts with another party to perform the work if the latter amount be in excess of the former, but in no event shall liability hereunder exceed the penal sum hereof.

Signed and sealed this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_

\_\_\_\_\_  
(Witness)

\_\_\_\_\_  
(Principal) (Seal)

By: \_\_\_\_\_  
(Name) (Title)

\_\_\_\_\_  
(Surety) (Seal)

\_\_\_\_\_  
(Witness)

By: \_\_\_\_\_  
(Attorney-in-Fact)

\_\_\_\_\_  
(MS Agent)

\_\_\_\_\_  
Mississippi Insurance ID Number

