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07 -



SM No. CMP2000000841

# PROPOSAL AND CONTRACT DOCUMENTS

## FOR THE CONSTRUCTION OF

07

Raised Pavement Markers on various routes in District 2, known as State Project  
No. MP-2000-00(084) / 306163301 Districtwide.

Project Completion: 03/30/2018

**(STATE DELEGATED)**

### NOTICE

**BIDDERS MUST PURCHASE A BID PROPOSAL FROM  
MDOT CONTRACT ADMINISTRATION DIVISION TO BID  
THIS PROJECT.**

Electronic addendum updates will be posted on [www.gomdot.com](http://www.gomdot.com)

# SECTION 900

## OF THE CURRENT 2017 STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION JACKSON, MISSISSIPPI

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
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**PROJECT: MP-2000-00(084)/306163301 - District 2**

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(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET  
OF SECTION 905 AS ADDENDA)

05/31/2017 11:42 AM

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

## SECTION 901 - ADVERTISEMENT

Electronic bids will be received by the Mississippi Transportation Commission at 10:00 o'clock A.M., Tuesday, June 27, 2017, from the Bid Express Service and shortly thereafter publicly read on the Sixth Floor for:

Raised Pavement Markers on various routes in District 2, known as State Project No. MP-2000-00(084) / 306163301 Districtwide.

The attention of bidders is directed to the predetermined minimum wage rate set by the U. S. Department of Labor under the Fair Labor Standards Act.

The Mississippi Department of Transportation hereby notifies all bidders that it will affirmatively insure that in any contract entered into pursuant to this advertisement, disadvantaged business enterprises will be afforded full opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of race, color, sex, age, disability, religion or national origin in consideration for an award.

The specifications are on file in the offices of the Mississippi Department of Transportation.

Bid proposals must be purchased online at <https://shopmdot.ms.gov>. Specimen proposals may be viewed and downloaded online at no cost at <http://mdot.ms.gov> or purchased online. Proposals are available at a cost of Ten Dollars (\$10.00) per proposal plus a small convenience fee. Cash or checks will not be accepted as payment.

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

MELINDA L. MCGRATH  
EXECUTIVE DIRECTOR

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 - NOTICE TO BIDDERS NO. 1**

**CODE: (IS)**

**DATE: 03/01/2017**

**SUBJECT: Governing Specifications**

The current (2017) Edition of the Standard Specifications for Road and Bridge Construction adopted by the Mississippi Transportation Commission is made a part hereof fully and completely as if it were attached hereto, except where superseded by special provisions, or amended by revisions of the Specifications contained within this proposal. Copies of the specification book may be purchased from the MDOT Construction Division, or online at [shopmdot/default.aspx?StoreIndex=1](http://shopmdot/default.aspx?StoreIndex=1).

A reference in any contract document to controlling requirements in another portion of the contract documents shall be understood to apply equally to any revision or amendment thereof included in the contract.

In the event the plans or proposal contain references to the 2004 Edition of the Standard Specifications for Road and Bridge Construction, it is to be understood that such references shall mean the comparable provisions of the 2017 Edition of the Standard Specifications.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 9**

**CODE: (IS)**

**DATE: 03/01/2017**

**SUBJECT: Federal Bridge Formula**

Bidders are hereby advised that the latest revision of Federal Highway Administration Publication No. FHWA-HOP-06-105, **BRIDGE FORMULA WEIGHTS**, dated August 2006, is made a part of this contract when applicable.

Prior to the preconstruction conference, the Contractor shall advise the Engineer, in writing, what materials, if any, will be delivered to the jobsite via Interstate route(s).

Copies of the **BRIDGE FORMULA WEIGHTS** publication may be obtained by contacting:

Federal Highway Administration  
400 7<sup>th</sup> Street, SW  
Washington, DC 20590  
(202) 366-2212

or

[http://www.ops.fhwa.dot.gov/Freight/publications/brdg\\_frm\\_wgths/bridge\\_formula\\_all\\_rev.pdf](http://www.ops.fhwa.dot.gov/Freight/publications/brdg_frm_wgths/bridge_formula_all_rev.pdf)

An on line **BRIDGE FORMULA WEIGHTS CALCULATOR** is available at

[http://ops.fhwa.dot.gov/freight/sw/brdgcalc/calc\\_page.htm](http://ops.fhwa.dot.gov/freight/sw/brdgcalc/calc_page.htm)

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 12**

**CODE: (IS)**

**DATE: 03/01/2017**

**SUBJECT: MASH Compliant Devices**

Bidders are hereby advised that the Standard Specifications may require certain traffic control and permanent safety hardware devices to meet the requirements of the Manual for Assessing Safety Hardware (MASH). However, devices meeting the requirements of NCHRP Report 350 will be allowed until the mandatory effective date for MASH compliance. The following table shows the effective dates for MASH compliant devices.

<b>Device</b>	<b>Effective Date for MASH Compliance</b>
W-beam barriers, cast-in-place concrete barriers	December 31, 2017
W-beam terminals	June 30, 2018
Cable barriers, cable barrier terminals, crash cushions	December 31, 2018
Bridge rails, transitions, all other longitudinal barriers including portable barriers installed permanently, all other terminals, sign supports, all other breakaway hardware	December 31, 2019

Temporary work zone devices, including portable barriers manufactured after December 31, 2019, must have been successfully tested to the 2016 Edition of MASH. Such devices manufactured on or before this date and successfully tested to NCHRP Report 350 or the 2009 Edition of MASH may continue to be used throughout their normal service lives.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 – NOTICE TO BIDDERS NO. 115**

**CODE: (SP)**

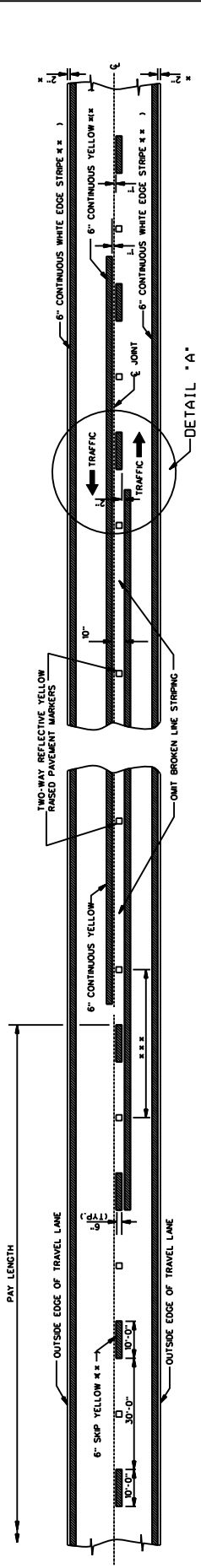
**DATE: 04/19/2017**

**SUBJECT: Standard Drawings**

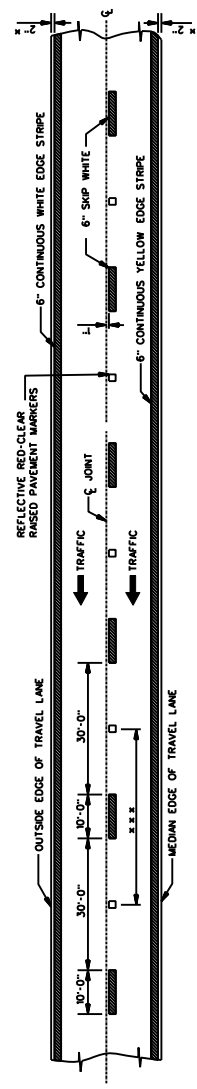
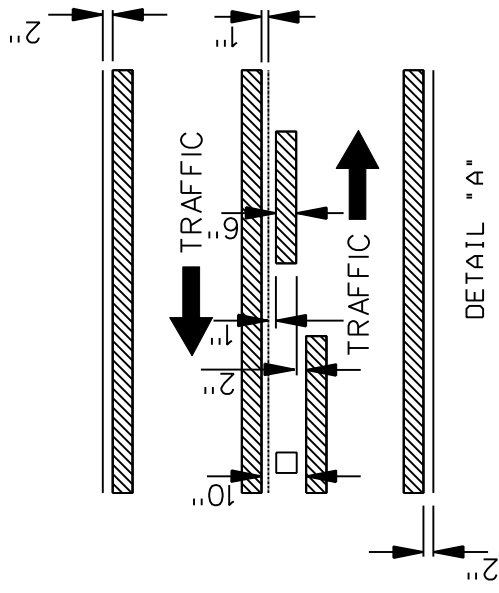
Standard Drawings attached hereto shall govern appropriate items of required work.

Larger copies of Standard Drawings may be purchased from:

MDOT Plans Print Shop  
MDOT Shop Complex, Building C, Room 114  
2567 North West Street  
P.O. Box 1850  
Jackson, MS 39215-1850  
Telephone: (601) 359-7460  
or FAX: (601) 359-7461  
or e-mail: [plans@mdot.state.ms.us](mailto:plans@mdot.state.ms.us)



TWO-WAY TRAFFIC  
(ASPHALT OR CONCRETE PAVEMENT)



4-LANE WITH ONE-WAY TRAFFIC



GENERAL NOTES:

- \* 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- \*\* 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
- \*\*\* 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	URBAN AREA (U/Lin)	RURAL AREA (U/Lin)
HORIZONTAL CURVES	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	40'-0"
	40'-0"	140'-0"

NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP AND 1000' BEHIND (IN DIRECTION OF TRAFFIC) OF THE ENTRANCE RAMP TAPER.

4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED SOURCES OF MATERIALS."

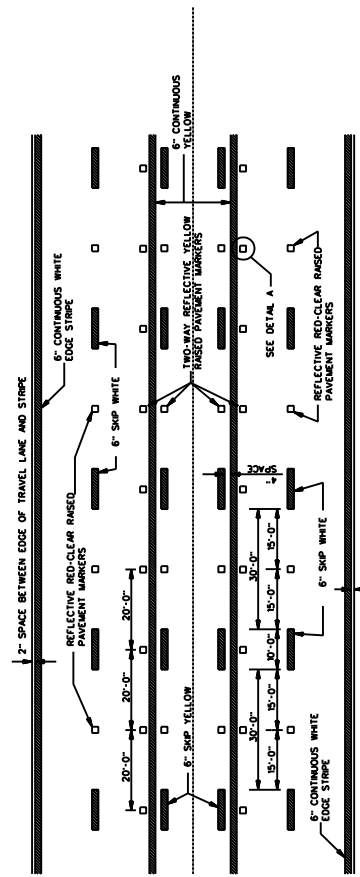
MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

PAVEMENT MARKING  
DETAILS FOR  
2-LANE AND 4-LANE  
DIVIDED HIGHWAYS

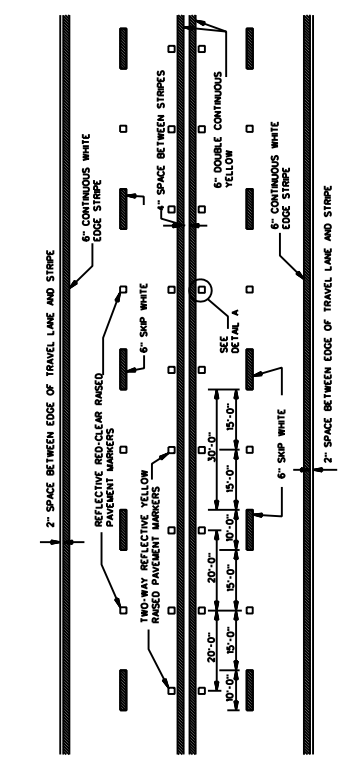
WORK NUMBER: PM-1  
SHEET NUMBER: 100  
ISSUE DATE: OCTOBER 1, 1998



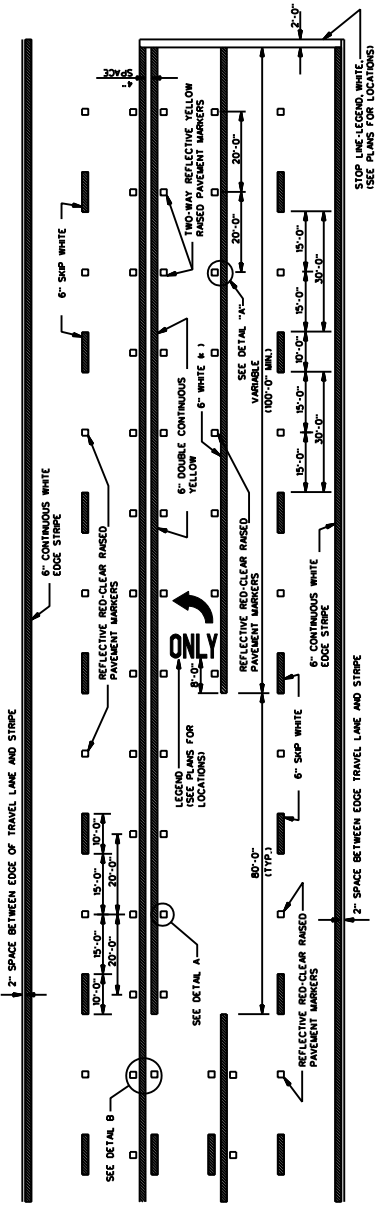
STATE	PROJECT NO.
MISS.	



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 5-LANE SECTION

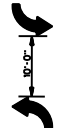


TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS AT LEFT TURN LANES

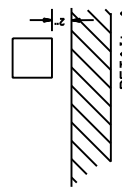
\*NOTE: USE DETAIL STRIPING IF LENGTH  $\leq$  150' AT THIS LOCATION, OTHERWISE USE CONTINUOUS STRIPING.



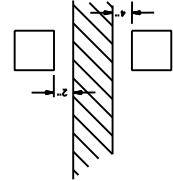
TYPICAL TWO-WAY ARROW INSTALLATION

- NOTES: 1. CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY.
- 2. IF SEGMENT IS LESS THAN 300', PLACE ONE SET OF ARROWS IN CENTER OF SEGMENT.
- 3. IF SEGMENT IS GREATER THAN 300', PLACE FIRST SET OF ARROWS 50' TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS 1250' O.C.

GENERAL NOTE:  
1. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT -APPROVED SOURCES OF MATERIALS.\*



DETAIL A  
LATERAL PLACEMENT OF PAVEMENT MARKERS

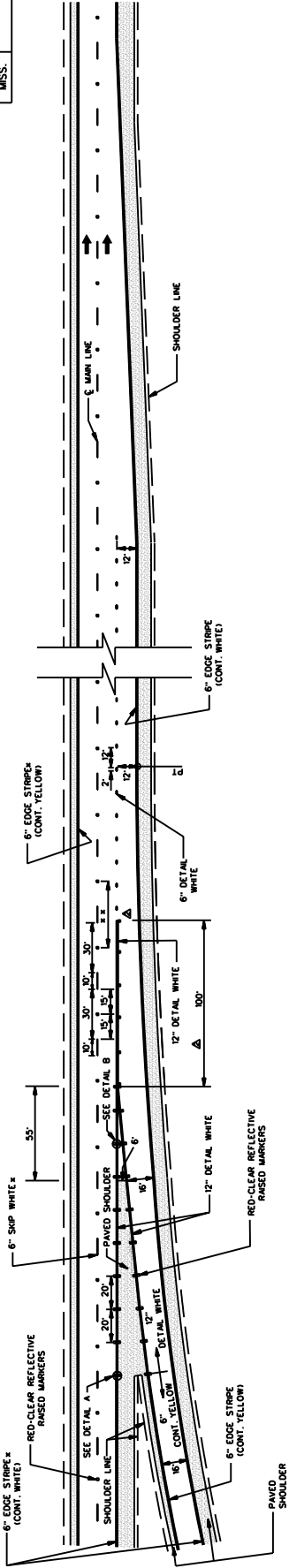


DETAIL B  
LATERAL PLACEMENT OF PAVEMENT MARKERS

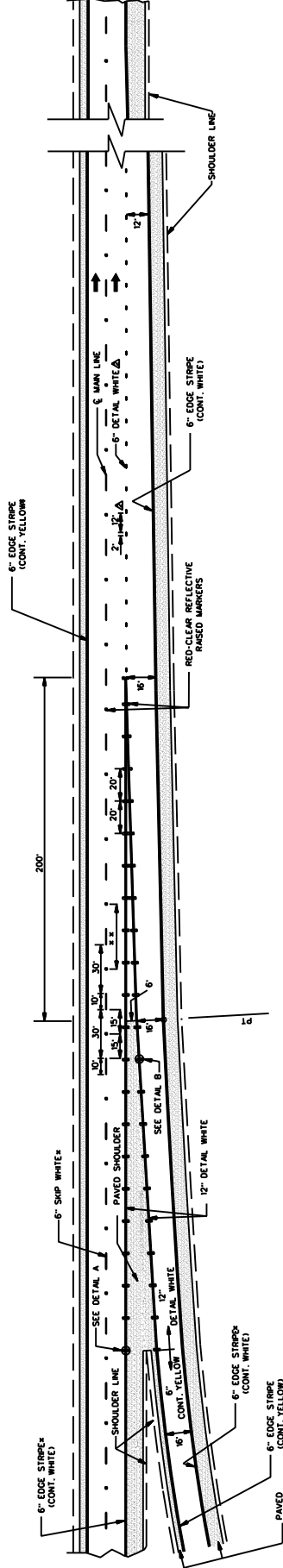
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
PAVEMENT MARKING	
DETAILS FOR	
4-LANE AND 5-LANE	
UNDIVIDED ROADWAYS	
DATE	03/15/14
FILE NAME	SDPM-2.DGN
DESIGNER	CH2M
CHECKER	
DATE	03/15/14

NUMBER FOR SDPM-2 SHEET NUMBER

STATE	PROJECT NO.
MISS.	

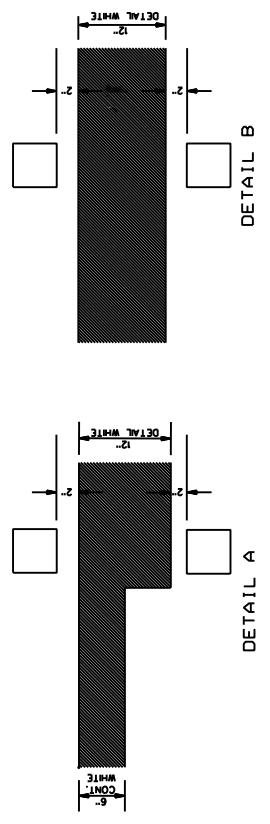


PARALLEL ENTRANCE RAMP



TAPER ENTRANCE RAMP

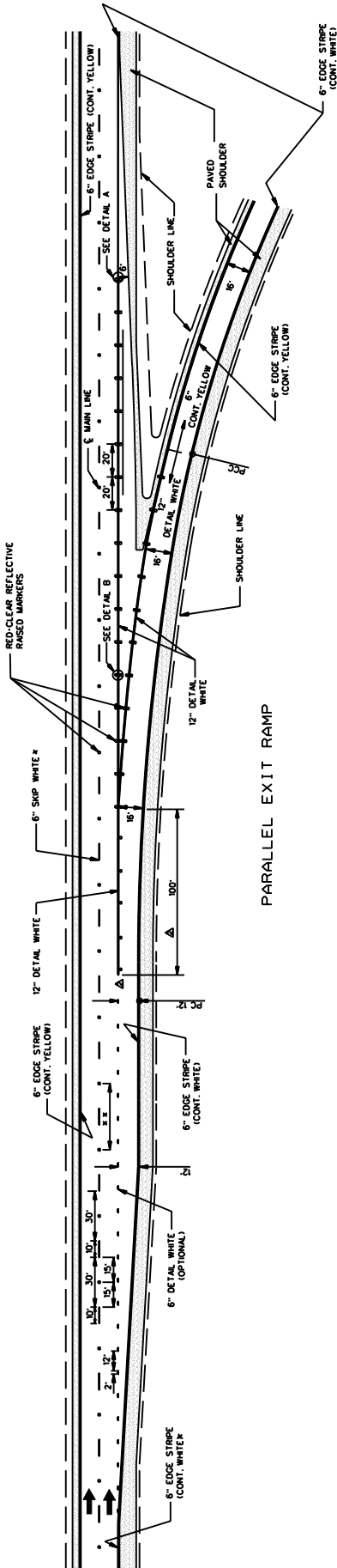
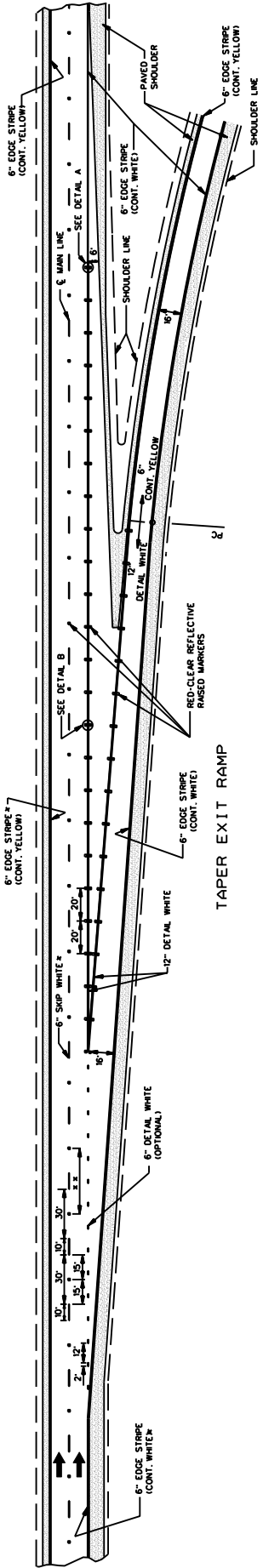
- GENERAL NOTES:
1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPES AND RED-CLEAR REFLECTIVE RAISED MARKERS. THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELED WAY.
  2. ON THE MAIN FACILITY, PLACE REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS AT A 40' SPACING ON ALL LANE-LINES THROUGHOUT THE INTERCHANGE AREA BEGINNING 300' IN THE MAIN FACILITY FROM THE INTERCHANGE POINT AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
  3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED SOURCES OF MATERIALS."



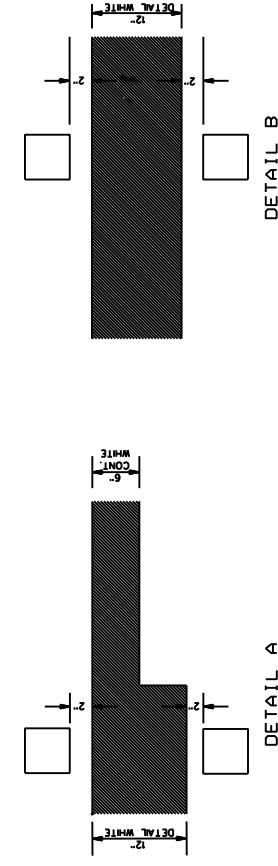
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION	
PAVEMENT MARKING DETAILS FOR INTERCHANGE RAMP (PARALLEL AND TAPER)	
ISSUE DATE:	OCTOBER 1, 1998
WORKING NUMBER	SDPM-3
SHEET NUMBER	



STATE	PROJECT NO.
MISS.	



- GENERAL NOTES:**
- SEE SHEET PM-1 FOR THE PLACEMENT OF LINE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELED WAY.
  - ON THE MAINLINE, PLACE RED REFLECTIVE RED-CLEAR BASED PAVEMENT MARKERS AT THE INTERCHANGE AREA BEGINNING 1000' IN ADVANCE IN DIRECTION OF TRAFFIC OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
  - PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE PAVEMENT MARKERS AS SPECIFIED IN THE MOOT "APPROVED SOURCES OF MATERIALS."



MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION

WORKING NUMBER  
SDPM-41

SHEET NUMBER

ISSUE DATE: OCTOBER 1, 1998

	R/7/8	UPDATE TO 2009 M/ICD	S.M.R.	S.M.R.	S.M.R.
	R/7/9	REVISOR STRIKE WITH			
	R/7/10				

STATE MISS.	PROJECT NO.		
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**GENERAL NOTES:**

- UNLESS OTHERWISE SHOWN ON THE PLANS, ALL PAVEMENT MARKING LEGENDS SHALL BE APPLIED USING HIGH PERFORMANCE POLYURETHANE PAINT.
- TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTORS OF 1/4" OR LESS) AND EXTENDING LETTERS SHALL BE USED TO MAINTAIN A MINIMUM WIDTH OF 1/2" BETWEEN LETTERS.
- FOR OTHER DETAILS SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

LEGEND	AREA (FT <sup>2</sup> )
STOP	24.6
RIGHT	18.8
LANE	27.3
AWAY	22.7
AHEAD	32.3
EXIT	18.5
SIGNAL	32.5
SCHOOL	35.5

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

**PAVEMENT MARKING LEGEND DETAILS**

REVISION					
DATE	ISSUE DATE:	OCTOBER 1, 1998	SHEET NUMBER	124	124

STATE PROJECT NO.					
MISS.					

TURN ARROW

THRU ARROW

COMBINATION ARROW

1-WAY ARROW

GENERAL NOTES:

- UNLESS OTHERWISE SHOWN ON THE PLANS, ALL PAVEMENT MARKING SHALL BE APPLIED USING HIGH PERFORMANCE MATERIALS.
- TWO HORIZONTAL GAPS CAUSED BY TEMPLATE CONNECTORS OF 1/2" OR LESS AND EXTENDING THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
- FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM PAVEMENT MARKING LEGENDS.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

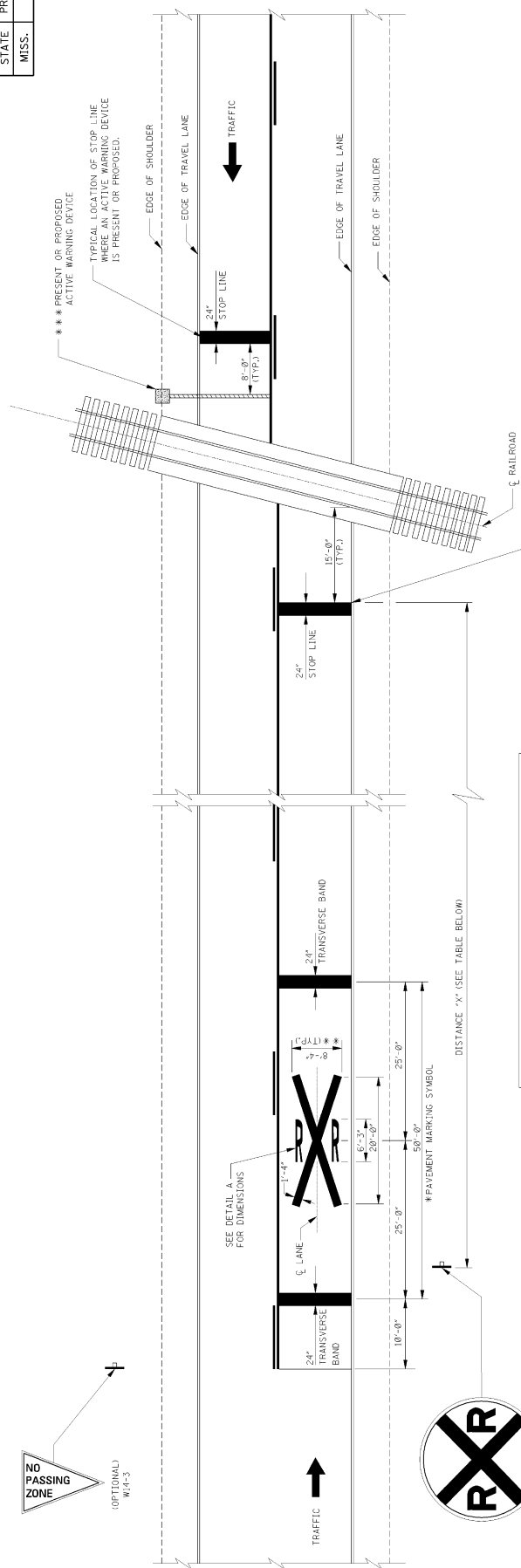
PAY QUANTITIES	
LEGEND/SYMBOL	AREA (ft <sup>2</sup> )
ONLY	22.0
TURN ARROW	18.4
THRU ARROW	21.2
COMBINATION ARROW	27.5
1-WAY ARROW	24.3

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

**PAVEMENT MARKING  
LEGEND DETAILS**

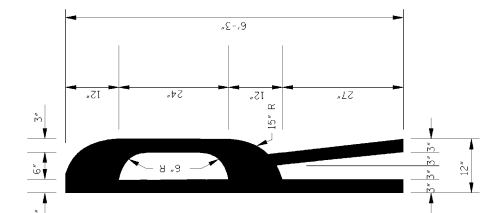
DATE	REVISION	BY	ISSUE DATE:	OCTOBER 1, 1998	WORKING NUMBER	PM-6	SHEET NUMBER	125
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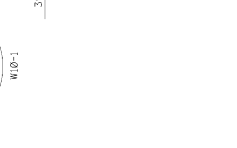
ADVANCE WARNING SIGN PLACEMENT DISTANCE

POSTED SPEED (mph)	DISTANCE "X" (FT)	
	RURAL	URBAN
20	175	100
25	250	100
30	325	100
35	400	150
40	475	225
45	550	300
50	625	375
55	700	450
60	775	550

NOTES:  
 ① DISTANCE "X" MAY BE ADJUSTED, IF PROHIBITIVE PHYSICAL CONDITIONS EXIST AT THE DESIGNATED DISTANCE.  
 ② THESE DISTANCES MAY BE ADJUSTED TO A MINIMUM OF 100' IN BUSINESS OR BUSINESS DISTRICTS WHERE LOW SPEEDS ARE PREVALENT.



DETAIL A  
STANDARD "R" PAVEMENT MARKING  
FOR R X R SYMBOL



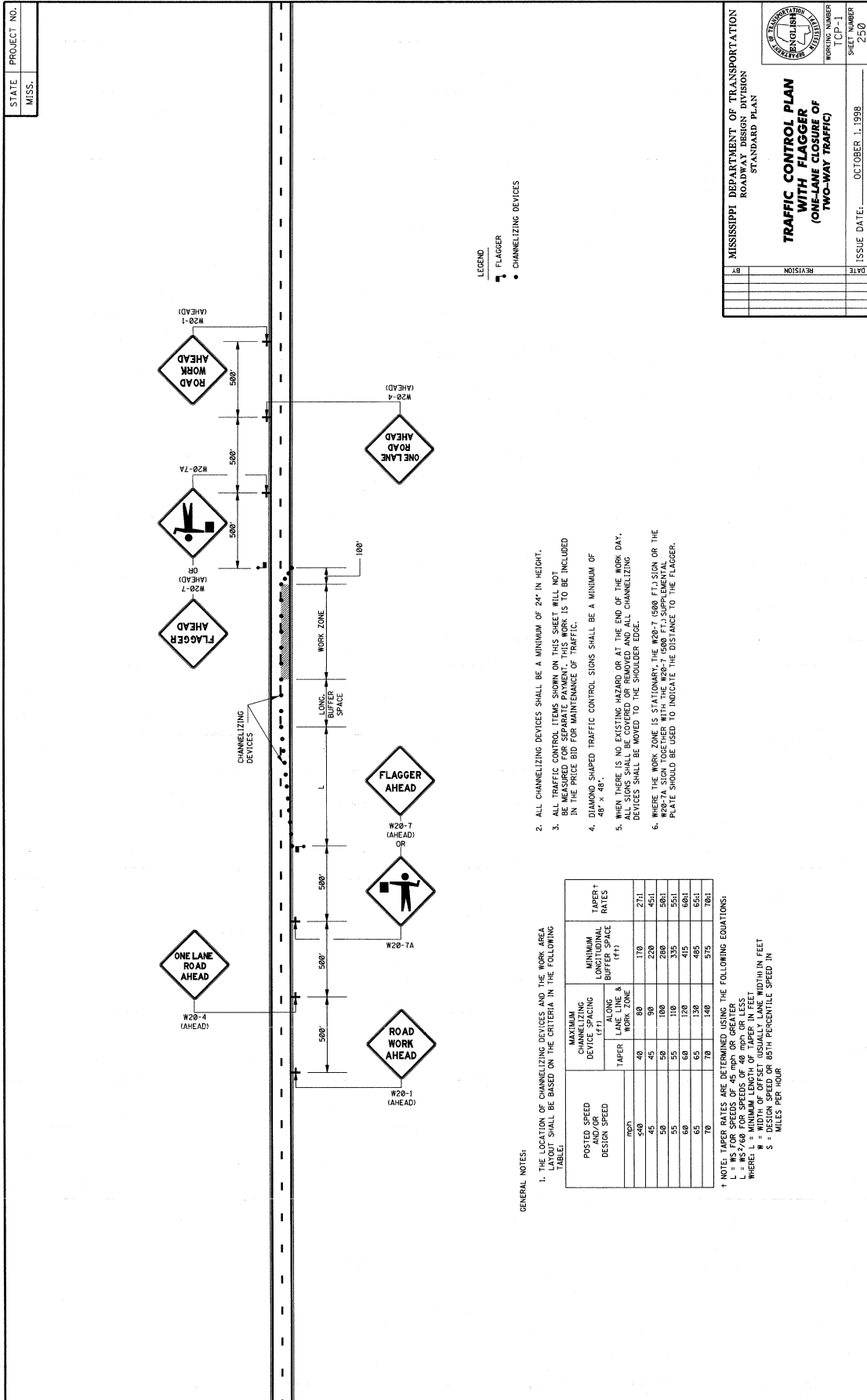
GENERAL NOTES:  
 \* 1. A PORTION OF THE PAVEMENT MARKING SYMBOL SHOULD BE DIRECTLY OPPOSITE THE ADVANCE WARNING SIGN (W10-1).  
 \* 2. WIDTH OF R X R SYMBOL MAY VARY ACCORDING TO LANE WIDTH. HOWEVER, ON MULTI-LANE ROADS, THE TRANSVERSE BANDS AND STOP LINE SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.  
 \* 3. R X R SYMBOL (63.0 #42), TRANSVERSE BANDS AND STOP LINE SHALL BE PAID FOR AS LEGEND, WHITE (PLASTIC), MATERIAL OPTIONAL FOR OTHER AGENCIES.  
 \* 4. REFER TO THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" FOR LOCATION OF PROPOSED WARNING DEVICES AT RAILROAD-HIGHWAY GRADE CROSSINGS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

**TYPICAL PLACEMENT OF WARNING SIGNS AND PAVEMENT MARKINGS AT RAILROAD HIGHWAY GRADE CROSSINGS**

WORKSHEET NO. 15  
SHEET NUMBER 12 OF 18  
ISSUE DATE: OCTOBER 11, 1998

DATE	REVISION
10-18-98	REVISE NOTE



STATE PROJECT NO.  
MISS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

**TRAFFIC CONTROL PLAN  
WITH FLAGGER  
(ONE-LANE CLOSURE OF  
TWO-WAY TRAFFIC)**

WORKING NUMBER  
TCP-1

SHEET NUMBER  
250

ISSUE DATE: OCTOBER 1, 1998

DATE	REVISION	BY

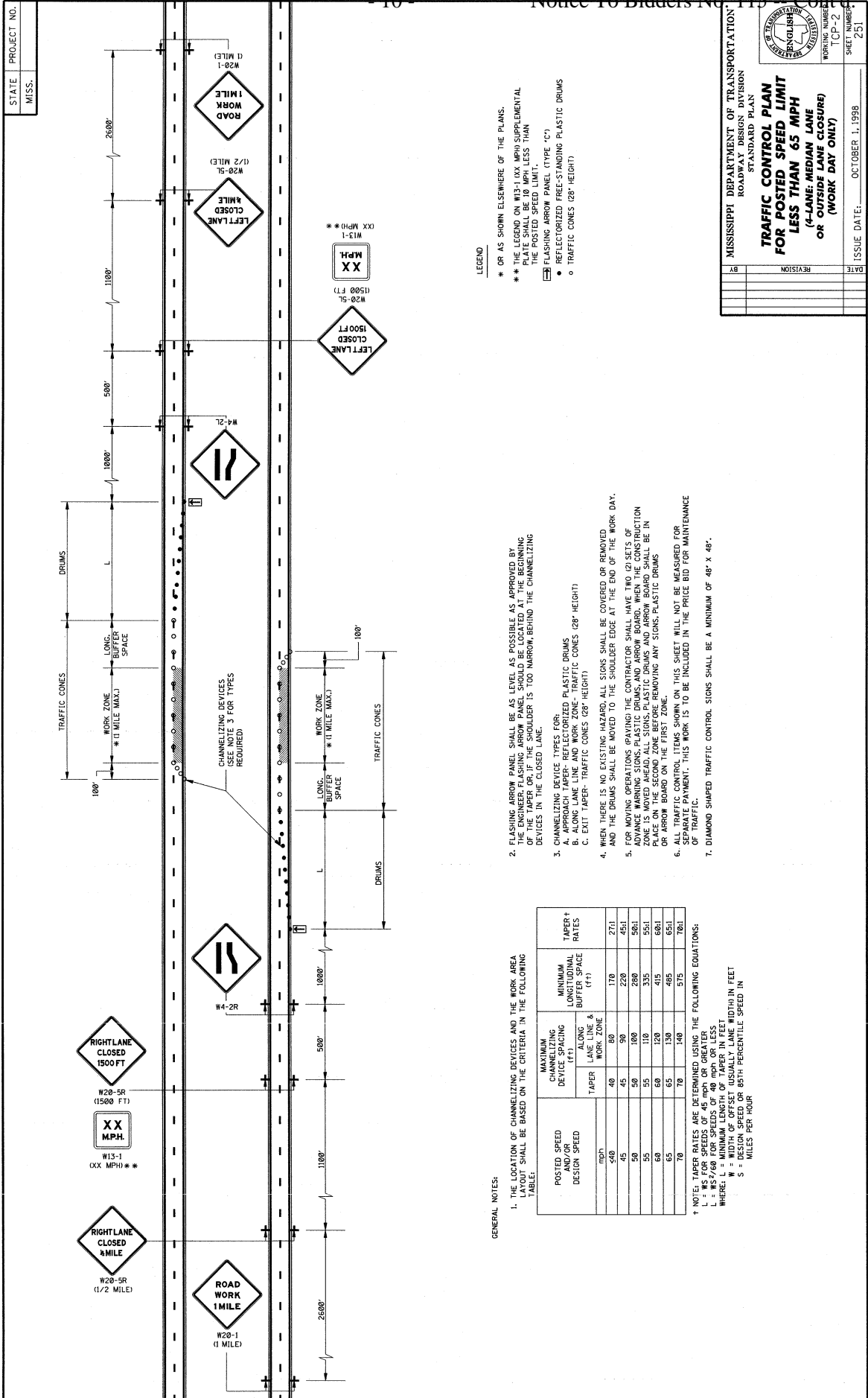
LEGEND  
 FLAGGER  
 CHANNELIZING DEVICES

2. ALL CHANNELIZING DEVICES SHALL BE A MINIMUM OF 24" IN HEIGHT.
3. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
4. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".
5. WHEN THERE IS NO EXISTING HAZARD OR AT THE END OF THE WORK DAY, CHANNELIZING DEVICES SHALL BE COVERED OR REMOVED AND ALL CHANNELIZING DEVICES SHALL BE MOVED TO THE SHOULDER EDGE.
6. WHERE THE WORK ZONE IS STATIONARY THE W20-7 (600 FT.) SIGN OR THE W20-7A SIGN TOGETHER WITH THE W20-7 (600 FT.) SUPPLEMENTAL PLATE SHOULD BE USED TO INDICATE THE DISTANCE TO THE FLAGGER.

GENERAL NOTES:  
 1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED (MPH)	MAXIMUM CHANNELIZING DEVICES ALONG WORK ZONE (FT.)		MINIMUM LONGITUDINAL BUFFER SPACE (FT.)	TAPER + BUFFER SPACE RATES
	ALONG WORK ZONE	ALONG BUFFER SPACE		
40	40	80	170	271
45	45	90	220	451
50	50	100	280	501
55	55	110	335	551
60	60	120	415	661
65	65	130	485	651
70	70	140	575	761

\* NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:  
 L = WS FOR SPEEDS OF 45 MPH OR GREATER  
 L = WS FOR SPEEDS OF 40 MPH OR GREATER  
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET  
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET  
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR



- LEGEND**
- \* OR AS SHOWN ELSEWHERE OF THE PLANS.
  - \*\* THE LEGEND ON W13-1 (XX MPH) SUPPLEMENTAL PLATE SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT.
  - REFLECTORIZED PRE-STANDING PLASTIC DRUMS
  - TRAFFIC CONES (28" HEIGHT)

2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICE TYPES FOR:
  - A. APPROACH TAPER- REFLECTORIZED PLASTIC DRUMS
  - B. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT)
  - C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT)
4. WHEN THERE IS NO EXISTING HAZARD, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE PREVIOUS ZONE.
6. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
7. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".

**GENERAL NOTES:**

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (FT)		MINIMUM LONGITUDINAL BUFFER SPACE (ft)	TAPER + TAPER RATES
	ALONG LANE LINE	WORK ZONE		
50	40	80	170	27:1
45	45	90	220	45:1
50	50	100	280	50:1
55	55	110	335	55:1
60	60	120	415	60:1
65	65	130	485	65:1
70	70	140	575	70:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:  
 $L = W^2$  FOR SPEEDS OF 45 MPH OR GREATER  
 $L = WS$  FOR SPEEDS OF 30 MPH OR LESS  
 WHERE L = MINIMUM LENGTH OF TAPER IN FEET  
 W = WIDTH OF OFFSET USUALLY LANE WIDTH IN FEET  
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

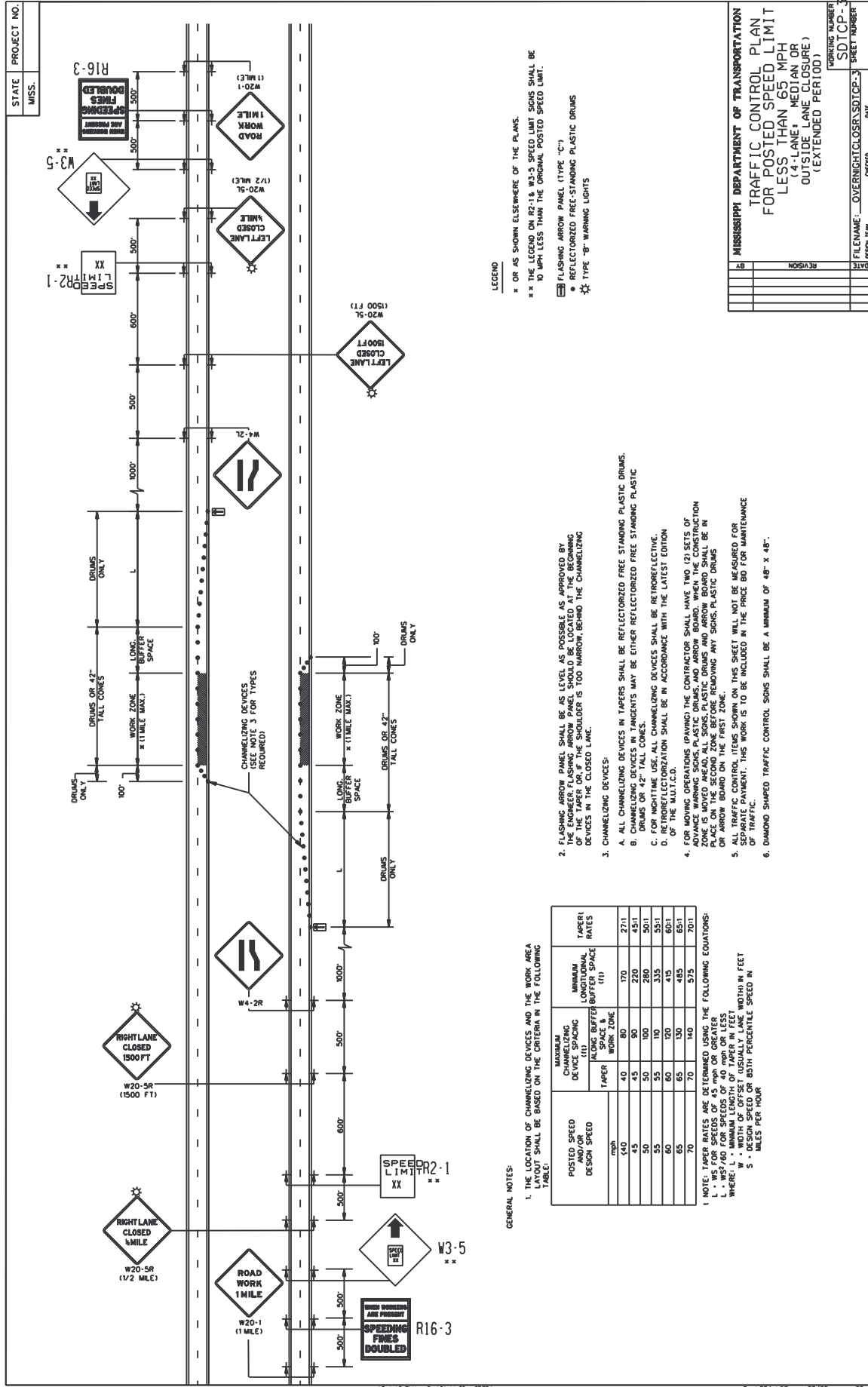
STATE PROJECT NO. MISS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
 ROADWAY DESIGN DIVISION  
 STANDARD PLAN

**TRAFFIC CONTROL PLAN  
 FOR POSTED SPEED LIMIT  
 LESS THAN 65 MPH  
 (4-LANE MEDIAN LANE  
 OR CLOSURE MEASURE)  
 (WORK DAY ONLY)**

WORKING NUMBER: TCP-2  
 SHEET NUMBER: 251  
 ISSUE DATE: OCTOBER 1, 1998





STATE MISS. PROJECT NO.

R16-3  
W3-5  
XX  
SPEED LIMIT 20-1

**LEGEND**

- x OR AS SHOWN ELSEWHERE OF THE PLANS.
- \*\* THE LEGEND ON R2-18 W3-5 SPEED LIMIT SIGNS SHALL BE 10 MPH LESS THAN THE ORIGINAL POSTED SPEED LIMIT.
- ◻ FLASHING ARROW PANEL (TYPE "C")
- ◊ REFLECTORIZED FREE-STANDING PLASTIC DRUMS
- ⊛ TYPE "B" WARNING LIGHTS

2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR AT THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.

**3. CHANNELIZING DEVICES:**

- A. ALL CHANNELIZING DEVICES IN TAPERS SHALL BE REFLECTORIZED FREE STANDING PLASTIC DRUMS.
  - B. CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER REFLECTORIZED FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.
  - C. FOR NIGHTTIME USE, ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE.
  - D. RETROREFLECTORIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE M.U.T.C.D.
4. FOR MOVING OPERATIONS (TAPERING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF CHANNELIZING DEVICES IN EACH TAPER. THE CHANNELIZING DEVICES IN THE FIRST ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
5. ALL CHANNELIZING DEVICES SHOWN ON THIS SHEET WILL NOT BE USED FOR SEPARATE PLACEMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
6. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" x 48".

**GENERAL NOTES:**

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

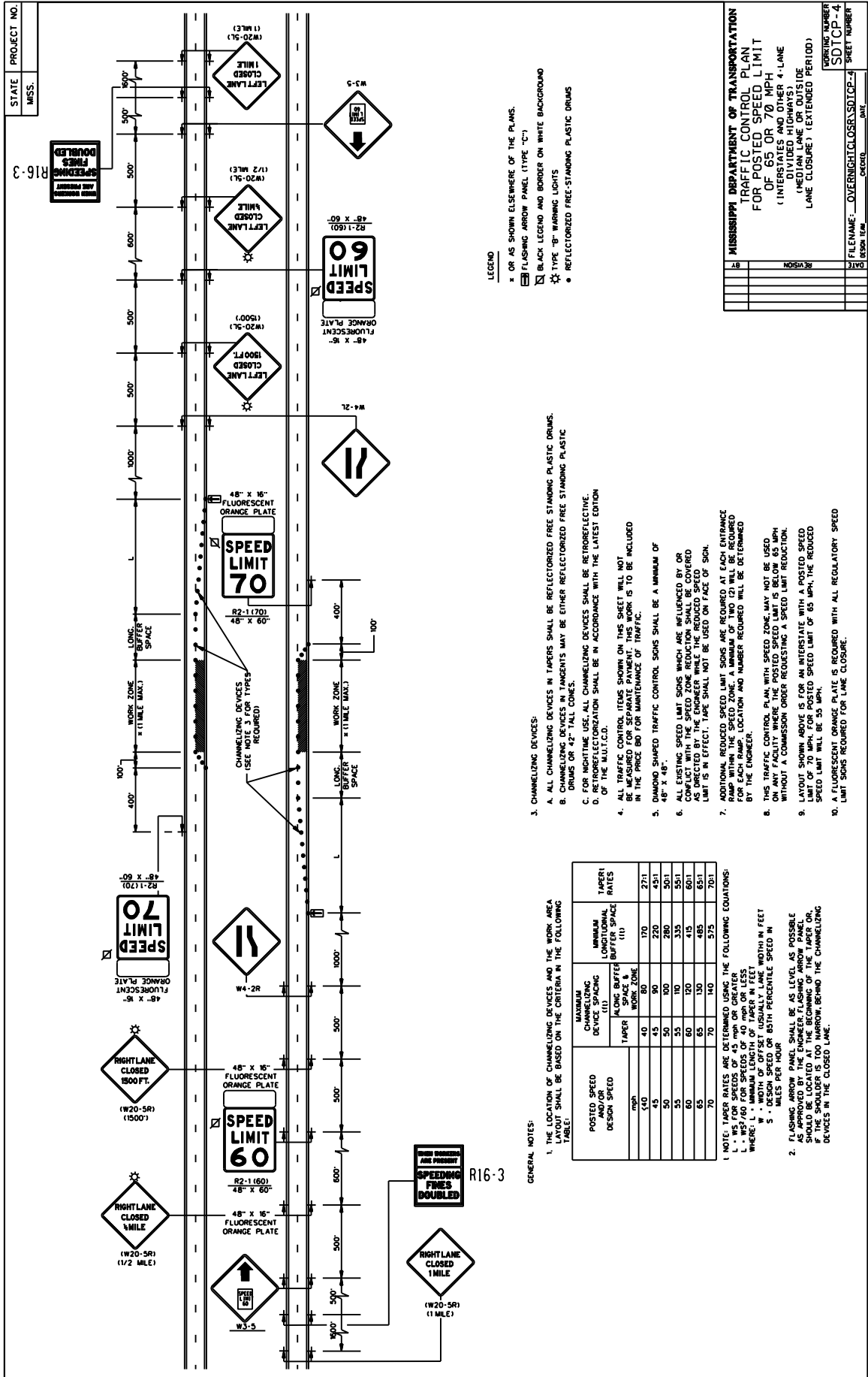
POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICES SPACING ALONG BUFFER SPACE IN TAPER		MINIMUM LONGITUDINAL BUFFER SPACE (L)	TAPER RATES
	(L)	(T)		
40	40	80	170	27:1
45	45	90	220	45:1
50	50	100	280	50:1
55	55	110	335	55:1
60	60	120	415	60:1
65	65	130	485	65:1
70	70	140	575	70:1

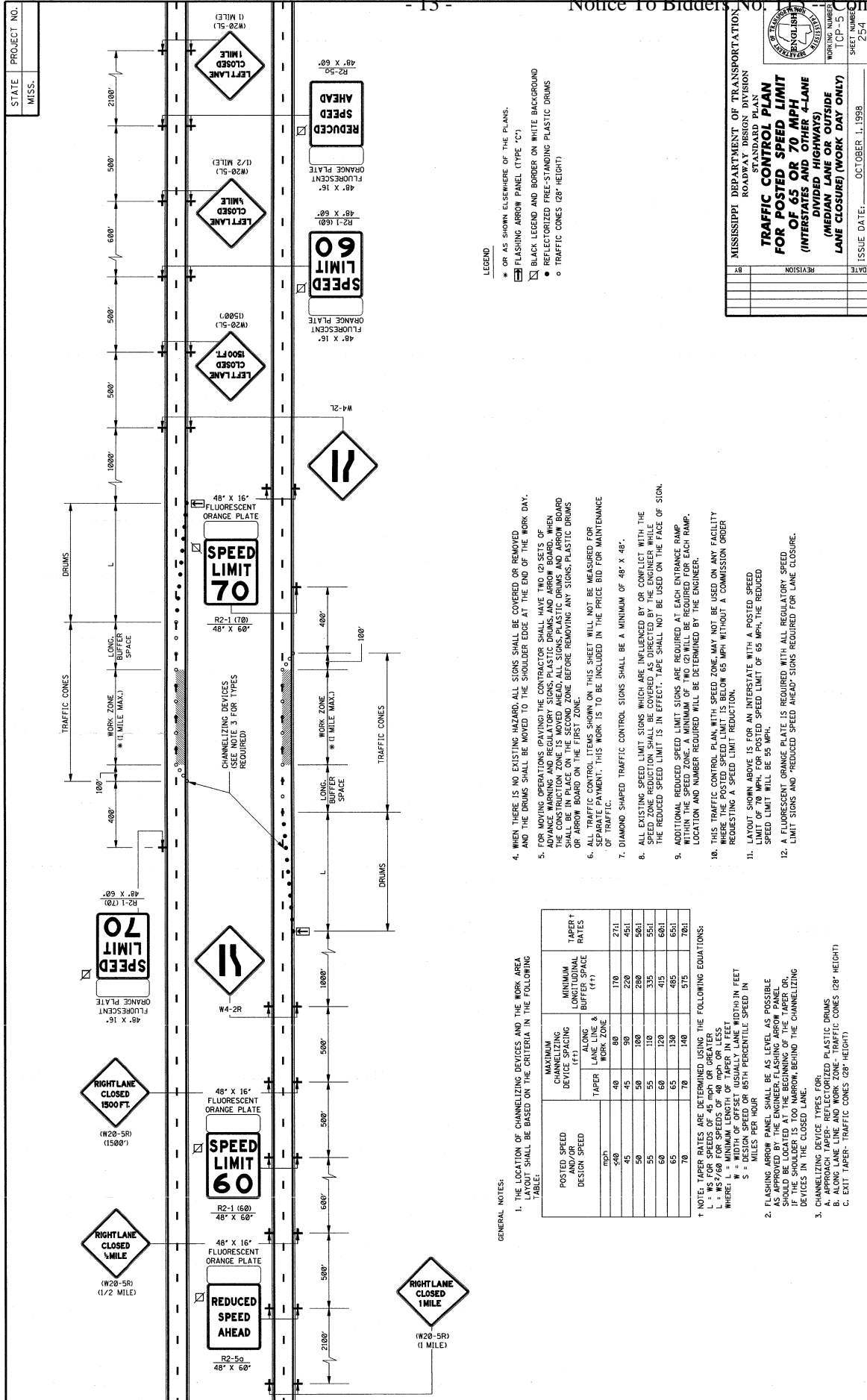
1. NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:  
 L = WS FOR SPEEDS OF 45 MPH OR GREATER  
 L = WS/40 FOR SPEEDS OF 40 MPH OR LESS  
 WHERE:  
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET  
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
 TRAFFIC CONTROL PLAN  
 FOR POSTED SPEED LIMIT  
 LESS THAN 65 MPH  
 (4' LANE WIDTH OR  
 DUST LANE CLOSURE)  
 (EXTENDED PERIOD)

PROJECT NUMBER: SDT CP-3  
 SHEET NUMBER: 11

DATE: \_\_\_\_\_  
 REVISION: \_\_\_\_\_  
 DESIGN ENGINEER: \_\_\_\_\_  
 CHECKED: \_\_\_\_\_





GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:
2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE. THE SIGN SHALL BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICE TYPES FOR:
  - A. APPROACH TAPER- REFLECTORIZED PLASTIC DRUMS
  - B. EXIT TAPER- REFLECTORIZED PLASTIC CONES (28" HEIGHT)
  - C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT)
4. WHEN THERE IS NO EXISTING HAZARD, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS PAYING THE CONTRACTOR SHALL HAVE TWO (2) SETS OF CHANNELIZING DEVICES. THE FIRST SET SHALL BE MOVED AHEAD OF THE WORK ZONE. THE SECOND SET SHALL BE MOVED AHEAD OF THE WORK ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
6. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
7. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".
8. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON THE FACE OF SIGN.
9. ADDITIONAL REDUCED SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. A MINIMUM OF TWO (2) WILL BE REQUIRED FOR EACH RAMP. LOCATION AND NUMBER REQUIRED WILL BE DETERMINED BY THE ENGINEER.
10. THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
11. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.
12. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS AND "REDUCED SPEED AHEAD" SIGNS REQUIRED FOR LANE CLOSURE.

POSTED SPEED DESIGN SPEED mph	CHANNELIZING DEVICE SPACING (FT)		MINIMUM LONGITUDINAL BUFFER SPACE (FT)	TAPER RATES
	LANE LINE & WORK ZONE	LANE LINE & WORK ZONE		
50	40	80	170	27:1
55	45	90	220	45:1
60	50	100	280	50:1
65	55	110	335	55:1
70	60	120	415	60:1
75	65	130	485	65:1
80	70	140	575	70:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:  
 L = WS FOR SPEEDS OF 45 mph OR GREATER  
 L = WS/60 FOR SPEEDS OF 40 mph OR LESS  
 WHERE:  
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET  
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

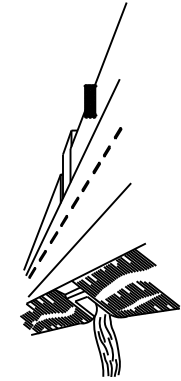
1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:
2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE. THE SIGN SHALL BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICE TYPES FOR:
  - A. APPROACH TAPER- REFLECTORIZED PLASTIC DRUMS
  - B. EXIT TAPER- REFLECTORIZED PLASTIC CONES (28" HEIGHT)
  - C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT)

- LEGEND
- \* OR AS SHOWN ELSEWHERE OF THE PLANS.
  - FLASHING ARROW PANEL (TYPE "C")
  - BLACK LEGEND AND BORDER ON WHITE BACKGROUND
  - REFLECTORIZED FREE-STANDING PLASTIC DRUMS
  - TRAFFIC CONES (28" HEIGHT)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
 STANDARD PLAN  
**TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (LANE CLOSURE) (WORK DAY ONLY)**

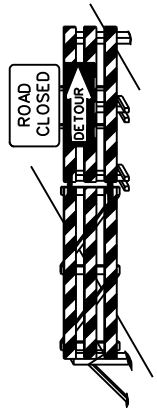
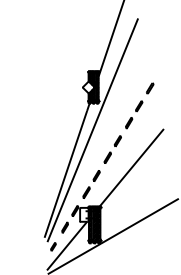
WORKING NUMBER: TCP-5  
 SHEET NUMBER: 254  
 ISSUE DATE: OCTOBER 1, 1988

STATE PROJECT NO.  
 MISS.



**WING BARRICADES**

- WING BARRICADES ARE TYPE BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
- WING BARRICADES SHOULD BE USED:
  - IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
  - IN ADVANCE OF ALL BRIDGE OR CULVERT MOVING OPERATIONS.

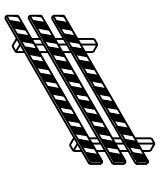


**BARRICADE CLOSING A ROAD**

**BARRICADE CHARACTERISTICS**

	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL *	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	35" MIN.	35" MIN.	60" MIN.
NUMBER OF REFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS
TYPE OF FRAME	LIGHT	LIGHT "X" FRAME	POST OR SHD

- \* 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- \*\* 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED FACINGS TRAFFIC.



**TYPE I**



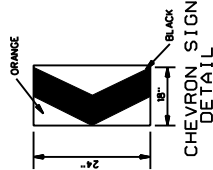
**TYPE II**



**TYPE III**

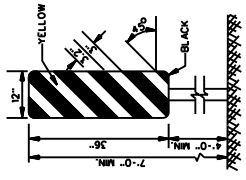
**STANDARD BARRICADES**

- A TYPE BARRICADE CONSISTS OF ONE (1) HORIZONTAL RAIL SUPPORTED BY A DEMOUNTABLE FRAME OR URBAN STREETS AND ARTERIALS.
- A TYPE BARRICADE CONSISTS OF TWO (2) HORIZONTAL RAILS ON A LIGHT "X" FRAME. TYPE BARRICADES ARE INTENDED FOR USE WHERE THE HAZARD IS LESS SEVERE THAN THAT OF TYPE I AND TYPE II BARRICADES. TYPE BARRICADES ARE USED FOR TEMPORARY DAYTIME USE, CONTINUOUS DELIMITING OF A RESTRICTED ROADWAY, OR FOR TEMPORARY DAYTIME USE.
- A TYPE BARRICADE CONSISTS OF THREE (3) HORIZONTAL RAILS SUPPORTED BY FIXED POSTS, A ROAD SHD, A HEAVY DEMOUNTABLE FRAME OR A HEAVY WINGED "X" FRAME.
- TYPE BARRICADES ARE INTENDED FOR USE ON CONSTRUCTION AND MAINTENANCE PROJECTS AS BARRICADES AND AT ROAD CLOSURES, WHERE THEY MUST REMAIN IN PLACE FOR EXTENDED PERIODS.
- THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).
- DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
- FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.



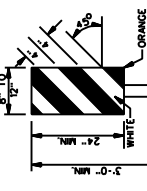
**CHEVRON SIGN  
DETAIL**

- A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
- THE CHEVRON SIGN SHALL BE MOUNTED ON FIXED POST OR ROAD SHD.
- CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHALL BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.



**TYPE 3 OBJECT MARKER  
(OM-3R)**

- TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
- THE OM-3R IS SHOWN. THE OM-3R IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
- THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



**VERTICAL PANEL**

- VERTICAL PANELS CONSIST OF AT LEAST ONE PANEL 8" TO 12" IN WIDTH AND A MINIMUM OF 24" IN HEIGHT.
- THE DIAGONAL STRIPES SHALL SLOPE DOWNWARD IN THE DIRECTION THAT TRAFFIC IS TO PASS THE PANEL. THE MINIMUM OF 36" ABOVE THE ROADWAY ON A SINGLE LIGHTMASS POST.
- VERTICAL PANELS USED ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH-SPEED ROADWAYS SHALL HAVE A MINIMUM OF 70 IN OF RETROREFLECTIVE AREA FACING TRAFFIC.
- FOR TWO-WAY TRAFFIC OPERATIONS, BACK-TO-BACK PANELS SHALL BE USED.

GENERAL NOTES:  
1. MARKINGS ON ALL DEVICES SHOWN ON THIS SHEET SHALL BE HIGH INTENSITY REFLECTIVE SHEETING.  
2. THE TRAFFIC CONTROL PLAN WILL LIST THE VARIOUS TRAFFIC CONTROL DEVICES REQUIRED FOR EACH PROJECT.

**Notice To Bidder**

- PLASTIC DRUM STRIPING DETAIL**
- PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH MUTCD STANDARDS FOR BARRICADES. THE PREDOMINANT COLOR ON DRUMS SHALL BE ORANGE. STRIPES SHALL BE GREEN, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
  - DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
  - WHERE PRACTICAL PLASTIC DRUMS SHALL BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**HIGHWAY SIGN AND BARRICADE DETAIL FOR CONSTRUCTS**

WORKING NUMBER: SDTCP-10  
SHEET NUMBER: 10  
FILE NAME: SDTCP-10\_06012012.DGN  
PERSON: [blank]  
DATE: [blank]

STATE MISS.	PROJECT NO.	
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### MOBILE OPERATIONS ON MULTILANE ROAD

**MOBILE OPERATIONS ON MULTILANE ROAD**

**NOTES:**

- VEHICLES USED FOR THESE OPERATIONS SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLAOS, SIGNS, OR ARROW PANELS.
- PROTECTION VEHICLE #1 SHOULD BE EQUIPPED WITH AN ARROW PANEL. AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON PROTECTION VEHICLE #1 SO AS NOT TO OBSCURE THE ARROW PANEL.
- PROTECTION VEHICLE #2 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK-MOUNTED ATTENUATOR (TMA).
- PROTECTION VEHICLE #1 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
- WHEN ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, PROTECTION VEHICLE #1 SHOULD BE ELIMINATED.
- ON HIGH-SPEED ROADWAYS, A THIRD PROTECTION VEHICLE SHOULD BE USED (I.E., VEHICLE #1 ON THE SHOULDER (IF PRACTICAL), VEHICLE #2 IN THE CLOSED LANE, AND VEHICLE #3 IN THE CLOSED LANE).
- ARROW PANELS SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
- WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

### MOBILE OPERATIONS ON TWO-LANE ROAD

**MOBILE OPERATIONS ON TWO-LANE ROAD**

**NOTES:**

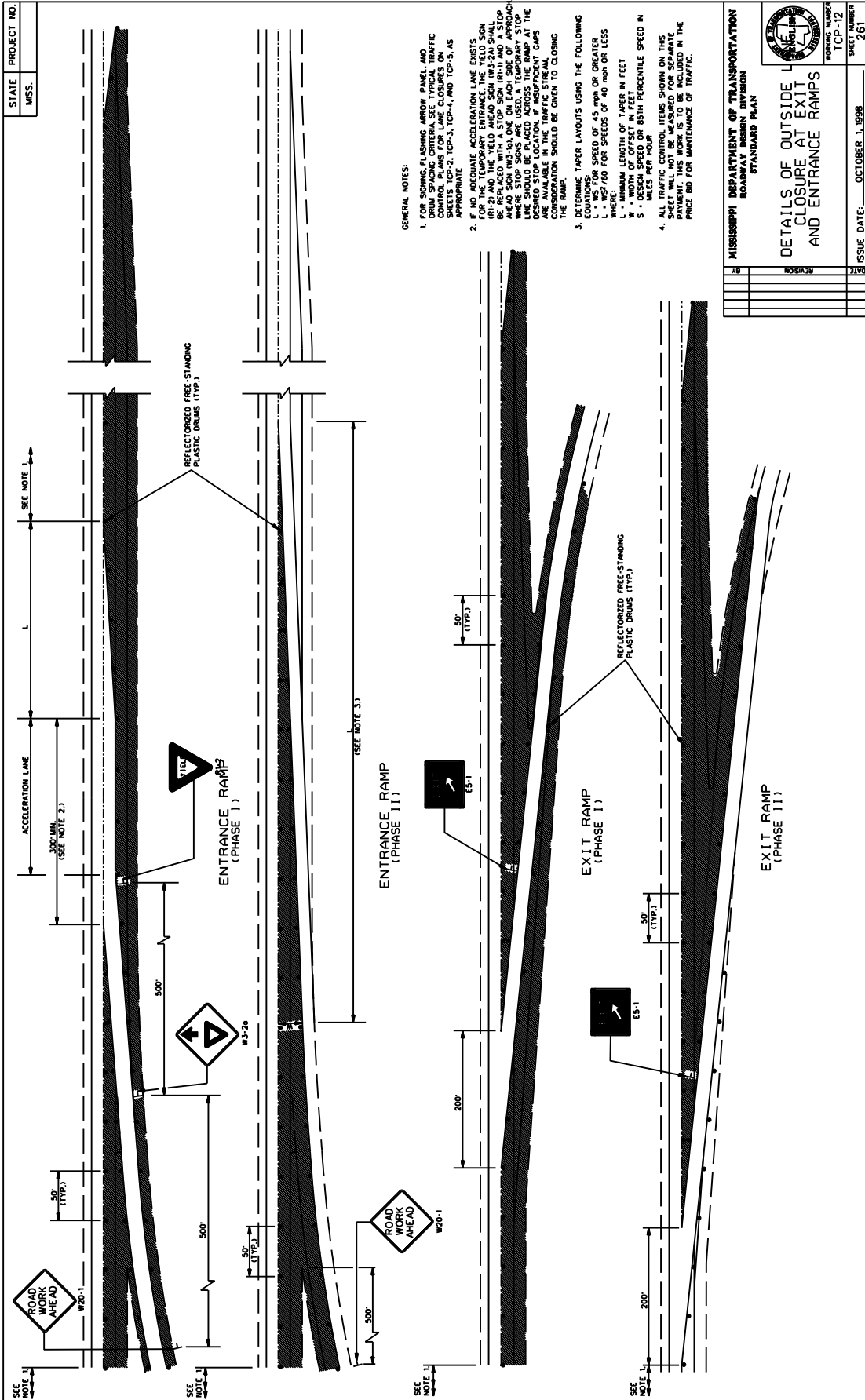
- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND PROTECTION VEHICLES SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLAOS, SIGNS, OR ARROW PANELS. THIS EQUIPMENT SHOULD NOT BE DONE FREQUENTLY, AS AN ALTERNATIVE A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND PROTECTION VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. PROTECTION VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE WORK, PROTECTION VEHICLES SHOULD BE ELIMINATED. THE PROTECTION VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL PROTECTION VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE PROTECTION VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE PROTECTION VEHICLE SHALL BE EQUIPPED WITH BEACONS. THE PROTECTION AND WORK VEHICLES SHOULD DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

**TRAFFIC CONTROL PLAN**  
**MOBILE OPERATIONS**  
**MULTILANE ROADS**  
**AND**  
**TWO-LANE ROADS**

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

DATE	BY	REVISION	S.M.R.	REVISED	CAPTION

ISSUE DATE: OCTOBER 1, 1998  
SHEET NUMBER: 260  
WORKING NUMBER: T-CP-11

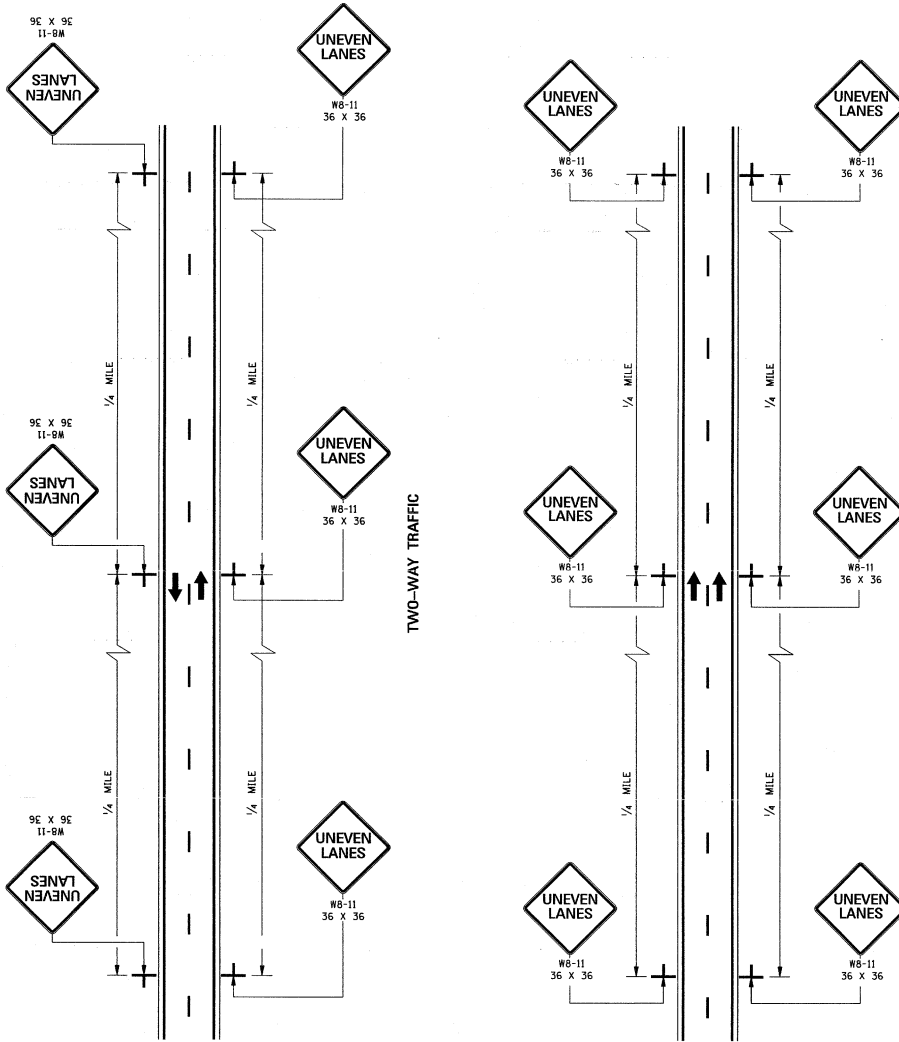


GENERAL NOTES:

- FOR SIGNING, FLASHING ARROW PANEL, AND CONTROL PANELS, REFER TO THE SIGNING AND CONTROL PLANS FOR LANE CLOSURES ON SHEETS TCP-2, TCP-3, TCP-4, AND TCP-5, AS APPROPRIATE.
- IF NO ADEQUATE ACCELERATION LANE EXISTS AT THE ENTRANCE TO THE RAMP, THE STOP SIGN (R1-1) AND THE YIELD AHEAD SIGN (W3-26) SHALL BE REPLACED WITH A STOP SIGN (R1-1) AND A STOP AHEAD SIGN (W3-26), ONE ON EACH SIDE OF APPROACH TO THE RAMP. THE STOP SIGN AND STOP AHEAD SIGN SHOULD BE PLACED ACROSS THE RAMP AT THE DESIRED STOP LOCATION, IF INSUFFICIENT GAPS EXIST. THE STOP SIGN AND STOP AHEAD SIGN CONSIDERATION SHOULD BE GIVEN TO CLOSING THE RAMP.
- DETERMINE TAPER LAYOUTS USING THE FOLLOWING EQUATIONS:
  - $L = 1.47 S T$  FOR SPEEDS OF 45 MPH OR GREATER
  - $L = WS/100$  FOR SPEEDS OF 40 MPH OR LESS
 WHERE:
  - L = MINIMUM LENGTH OF TAPER IN FEET
  - W = WIDTH OF TAPER IN FEET
  - S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS PLAN SHALL BE INSTALLED AND MAINTAINED FOR THE DURATION OF THE WORK. WORK SHALL BE DONE IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.


MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY MAINTENANCE DIVISION	
STANDARD PLAN	
DETAILS OF OUTSIDE LANE CLOSURE AT EXIT AND ENTRANCE RAMP	
WORKING NUMBER	TCP-12
SHEET NUMBER	261
ISSUE DATE:	OCTOBER 1, 1998

STATE	PROJECT NO.
MISS.	

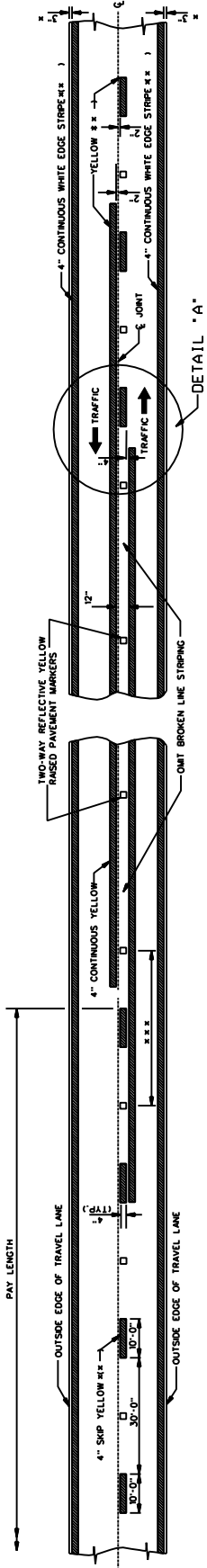


GENERAL NOTES:

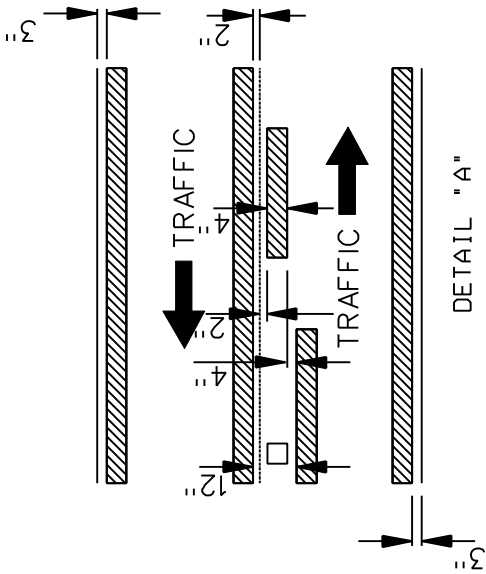
1. UNEVEN LANE LINE SPACING SHALL BE EQUAL TO 1/2' NO SIGNS REQUIRED.
  2. IF GREATER THAN 1/2' AND LESS THAN OR EQUAL TO 2/4' PLACE SIGNS AS SHOWN ON THIS SHEET.
  3. IF GREATER THAN 2/4' TRAFFIC SHOULD NOT BE ALLOWED TO CROSS UNEVEN LANE LINE.
2. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.
  3. THE W8-11 SIGNS SHALL BE SPACED AT 1/4-MILE INTERVALS THROUGHOUT UNEVEN LANE LINE LIMITS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN			WORKING NUMBER TCP-14	DATE OCTOBER 1, 1998
<b>TRAFFIC CONTROL PLANS</b> <b>UNEVEN PAVEMENT</b> <b>DETAILS</b>			SHEET NUMBER 263	ISSUE DATE: OCTOBER 1, 1998
BY	REVISION	DATE		

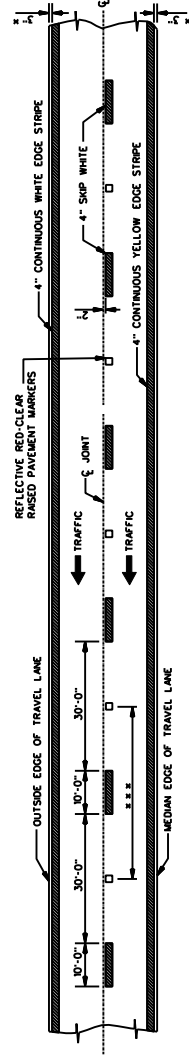
STATE	PROJECT NO.
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TWO-WAY TRAFFIC  
(ASPHALT OR CONCRETE PAVEMENT)



DETAIL "A"



4-LANE WITH ONE-WAY TRAFFIC



GENERAL NOTES:  
 \* 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.  
 \*\* 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).  
 \*\*\* 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

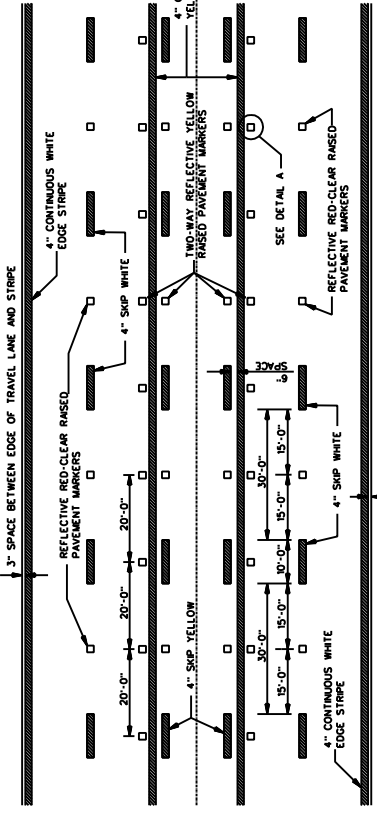
TARGET SECTIONS	URBAN AREA (U-L-0)	RURAL AREA (U-L-0)
LANE LINES	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	140'-0"

1. NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS SHALL BE USED THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.  
 2. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE MATERIALS AS SPECIFIED IN THE MOOT.  
 3. APPROVED SOURCES OF MATERIALS.  
 4. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKERS ARE TO REMAIN IN PLACE OVER 3 MONTHS.

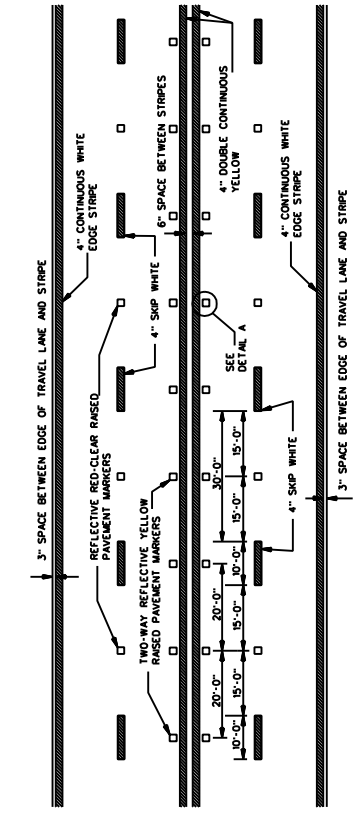
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS	
WORKING NUMBER	TCP-15
SHEET NUMBER	264
ISSUE DATE:	DECEMBER 1, 1999



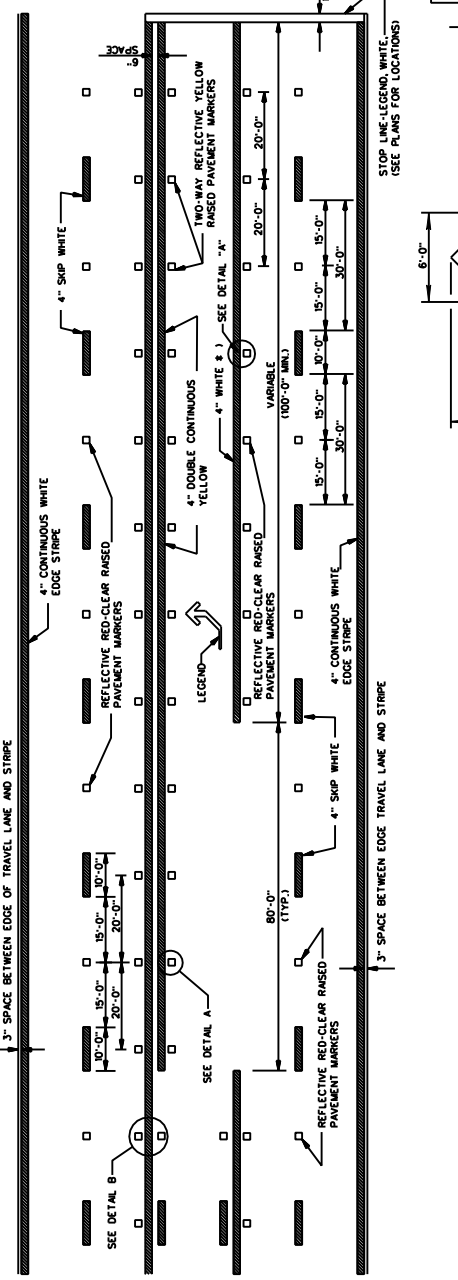
STATE	PROJECT NO.
MISS.	



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 5-LANE SECTION



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION

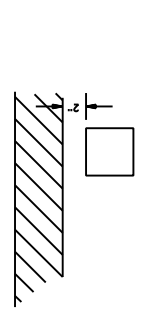


TYPICAL STRIPING AND RAISED PAVEMENT MARKERS AT LEFT TURN LANES

\* NOTE: USE DETAIL STRIPING IF LENGTH  $\leq 150'$  AT THIS LOCATION, OTHERWISE USE CONTINUOUS STRIPING.

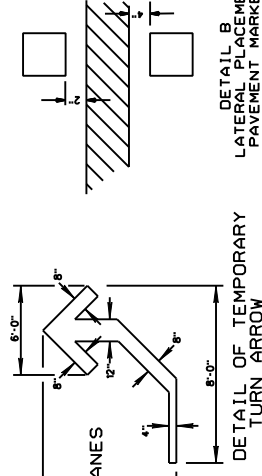
TYPICAL TWO-WAY ARROW INSTALLATION

- NOTES: 1. CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY.
- 2. IF SEGMENT IS LESS THAN 350', PLACE ONE SET OF ARROWS IN CENTER OF SEGMENT.
- 3. IF SEGMENT IS GREATER THAN 350', PLACE FIRST SET OF ARROWS 50 TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS (250 O.C.).



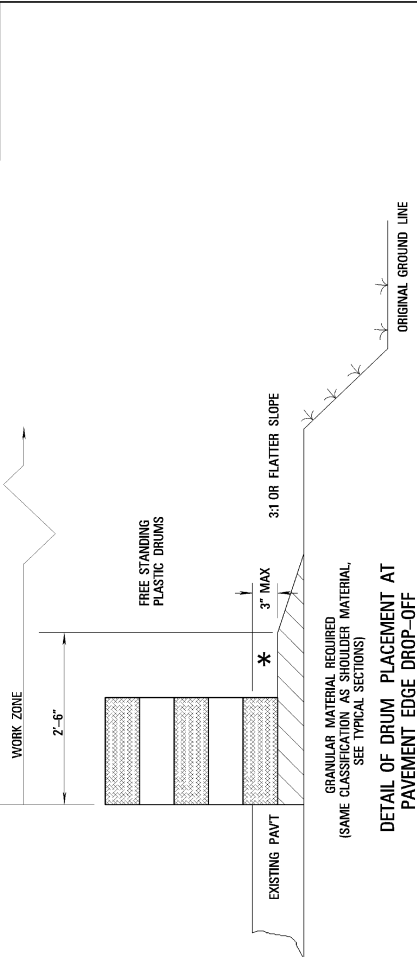
DETAIL A  
LATERAL PLACEMENT OF PAVEMENT MARKERS

- GENERAL NOTE:
- 1. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MOST APPROVED SOURCE OF MATERIALS.
  - 2. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKERS ARE TO REMAIN IN PLACE OVER 3 MONTHS.
  - 3. TEMPORARY TURN ARROW TO BE PAID FOR AS TEMPORARY TRAFFIC CONTROL LEGEND, ESTIMATED AT 10.9 SQ. FT. PER ARROW.



DETAIL B  
LATERAL PLACEMENT OF PAVEMENT MARKERS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN			TEMPORARY STRIPING FOR TRAFFIC CONTROL 4-LANE AND 5-LANE UNDIVIDED ROADWAYS
DRAWING NUMBER TCP-16	SHEET NUMBER 265		
ISSUE DATE: DECEMBER 1, 1999			



NOTES

- \* A. PAVEMENT EDGE DROP-OFF
  - IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-3) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750 ± 0.0).
  - TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. COMES MAY BE USED IN PLACE OF DRUMS, PANELS AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE M.U.T.C.D. (1/3 L, WHERE L IS THE TAPER LENGTH IN FEET)
  - GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 3:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS/PANELS OR BARRICADES MAY BE USED. IF CONCRETE BARRIERS ARE USED, SPECIAL REFLECTIVE DEVICES OR STEADY BURN LIGHTS SHOULD BE USED FOR OVERNIGHT INSTALLATIONS.
  - FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
  - LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

B. DRUM SPACING

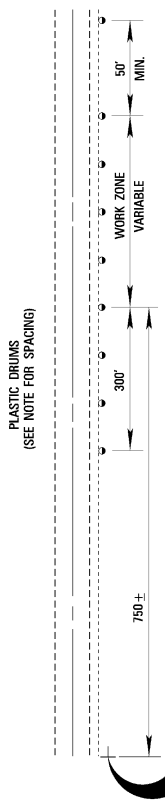
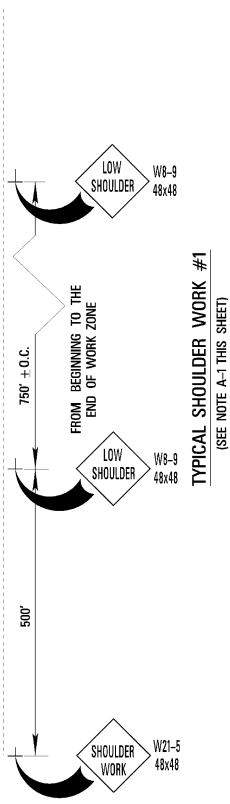
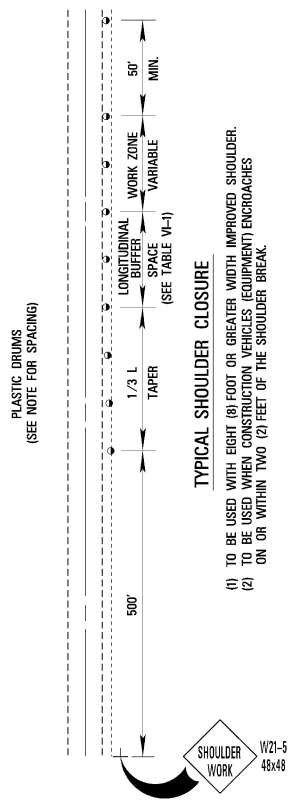
- 1. TANGENTS = 2 X S
- 2. WHERE L = 1/3 X W
- 3. L = TAPER LENGTH IN FEET
- 4. S = SPEED IN MPH (POSTED OR 85 PERCENTILE)
- 5. W = WIDTH OF OFFSET IN FEET

C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

TABLE V1-1 GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

SPEED (MPH)	LENGTH (FEET)
20	35
25	45
30	55
35	65
40	75
45	85
50	95
55	105
60	115
65	125
70	135
75	145
80	155
85	165
90	175
95	185
100	195
110	215
120	235
130	255
140	275
150	295
160	315
170	335
180	355
190	375
200	395
210	415
220	435
230	455
240	475
250	495

\* POSTED SPEED OFF-PEAK 85 PERCENTILE SPEED PRIOR TO WORK STARTING OR THE ANTICIPATED GRINDING SPEED IN MPH



NOTE: WORK OUTSIDE THE (2) FOOT LIMIT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA. OR SEE NOTE A-3 THIS SHEET.

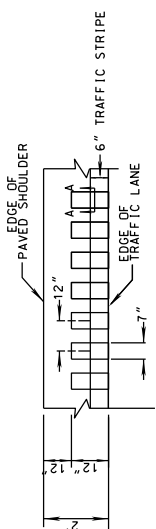
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS  
DRUM PLACEMENT  
AND  
SHOULDER CLOSURE

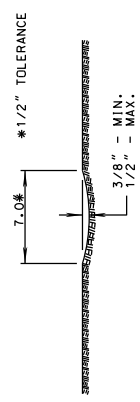
PROJECT NO.: \_\_\_\_\_  
COUNTY: \_\_\_\_\_

FILE NAME: \_\_\_\_\_  
PERSON: \_\_\_\_\_  
DATE: 12-28-2012

WORKING NUMBER: \_\_\_\_\_  
TCP: \_\_\_\_\_  
SHEET NUMBER: \_\_\_\_\_

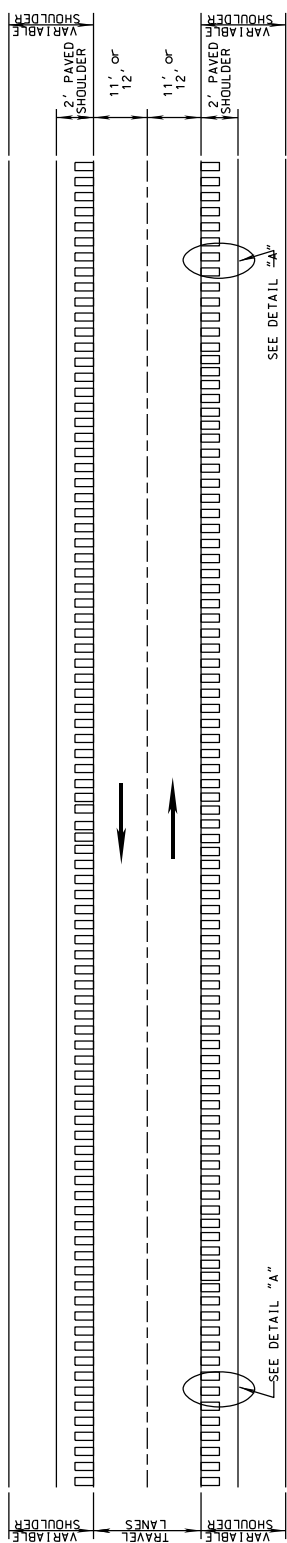


DETAIL "A"



SECTION "A-A"

- GENERAL NOTES
1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
  2. GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
  3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
  4. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.
  5. DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.

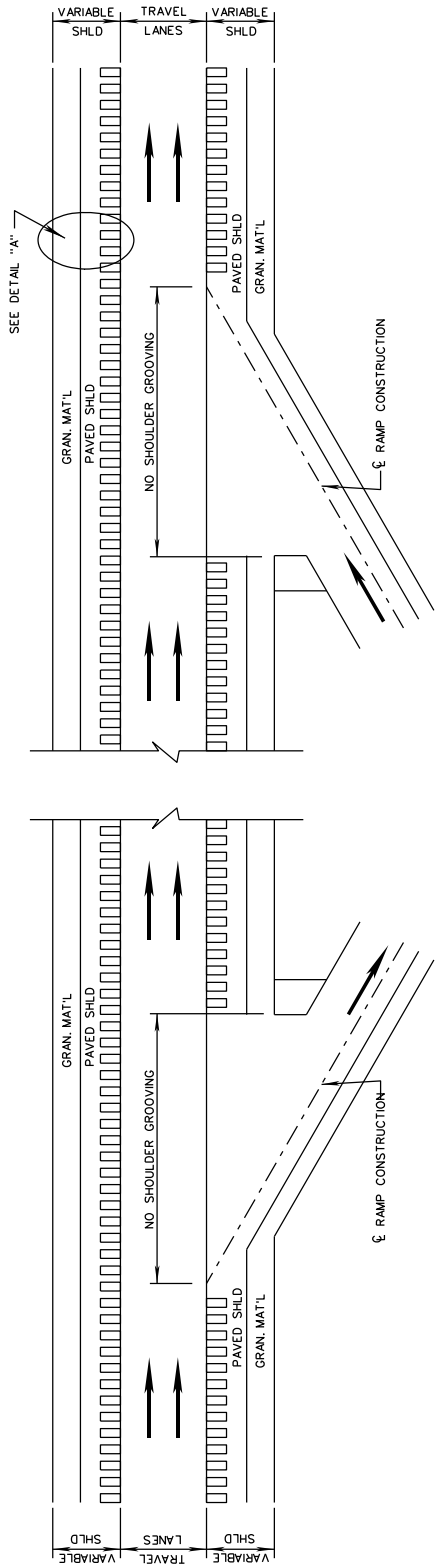
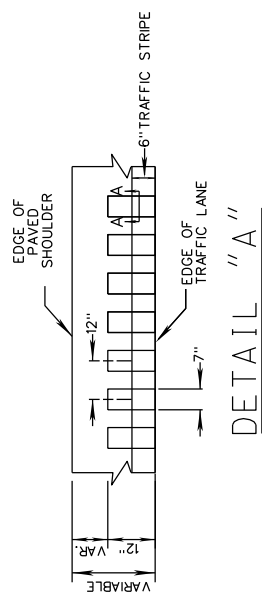


PLAN  
NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
RUMBLE STRIPES	
2-LANE HIGHWAYS	
(ASPH. LANES,	
2-FT ASPH. SHLD)	
PROJECT NO:	
COUNTY:	
ISSUING NUMBER	RS-1
SHEET NUMBER	
DATE	05/02/14
BY	
REVISION	
DATE	
FILE NAME	
DESIGN TEAM	SERVICES
CHECKED	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
RUMBLE STRIPES	
4-LANE HIGHWAYS	
(ASPH. LANES, 2-FT ASPH. SHLD)	
PROJECT NO:	002
COUNTY:	
WORKING NUMBER	RS-2
SHEET NUMBER	002
DATE	05/01/14
CHECKED	
DESIGN TEAM	

- GENERAL NOTES
1. ALL RUMBLE STRIPES SHALL BE GROUND-IN. RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
  2. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.



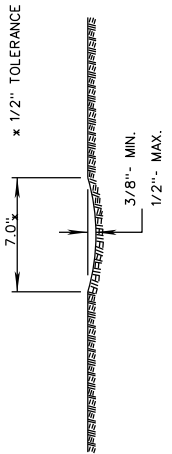
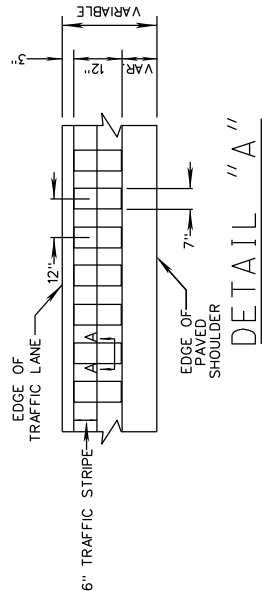
PLAN

NOT TO SCALE

DETAILS OF RUMBLE STRIPS

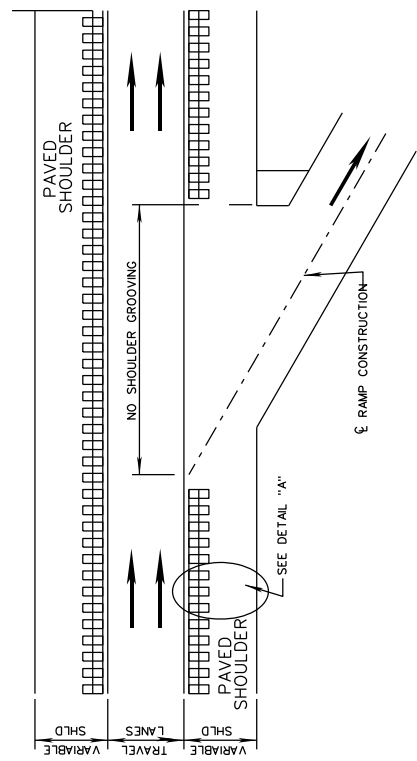
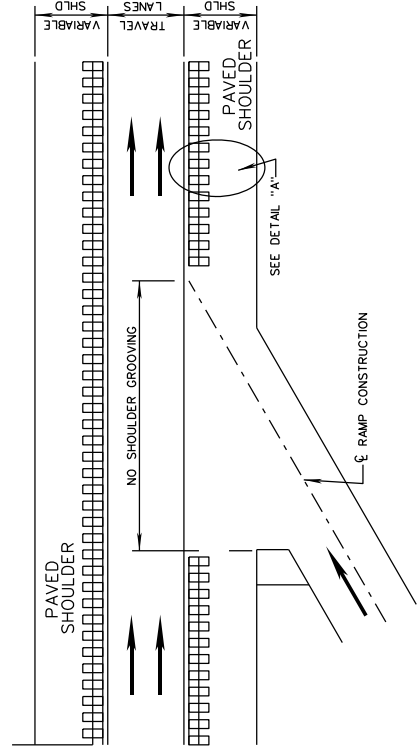
STATE	PROJECT NO.
MISS.	

- GENERAL NOTES**
1. ALL RUMBLE STRIPES SHALL BE GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
  2. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.



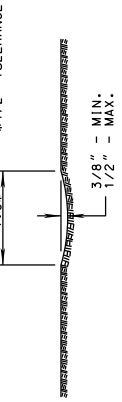
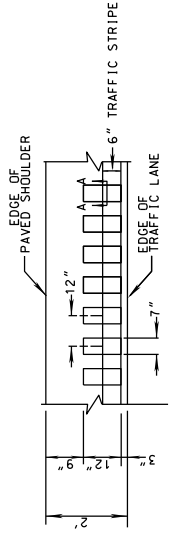
SECTION "A-A"

DETAIL "A"



PLAN  
NOT TO SCALE  
DETAILS OF  
RUMBLE STRIPS  
(GROUND-IN)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
RUMBLE STRIPES	
4-LANE WITH FULL-WIDTH	
PAVED SHOULDERS	
(CONC. LANES, ASPH. SHLD)	
PROJECT NO:	01111
COUNTY:	CLAY
FILE NAME:	RS-3
DESIGN TEAM:	DATE: 05/07/14
DATE:	CHECKED:
BY:	REVISION:
DATE:	WORKING NUMBER:
DATE:	SHEET NUMBER:

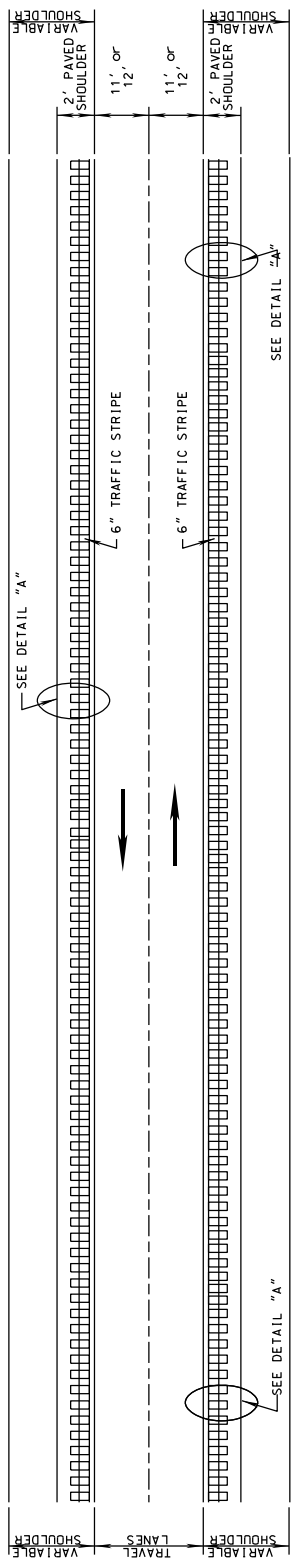


SECTION "A-A"

DETAIL "A"

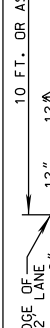
GENERAL NOTES  
 1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT  
 2. GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAY OR OTHER INTERRUPTIONS IN TRAVEL LANE WIDTH AS DIRECTED BY THE ENGINEER

- COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.
- DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.



PLAN  
 NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
RUMBLE STRIPES	
2-LANE HIGHWAY	
(CONCRETE LANES, 2-FT. ASPH. SHLD.)	
BY	REVISION
PROJECT NO:	
COUNTY:	
DATE	DATE REVISION
FILE NAME:	DESCRIPTION
WORKING NUMBER	SHEET NUMBER
URS-4	



**GENERAL NOTES**

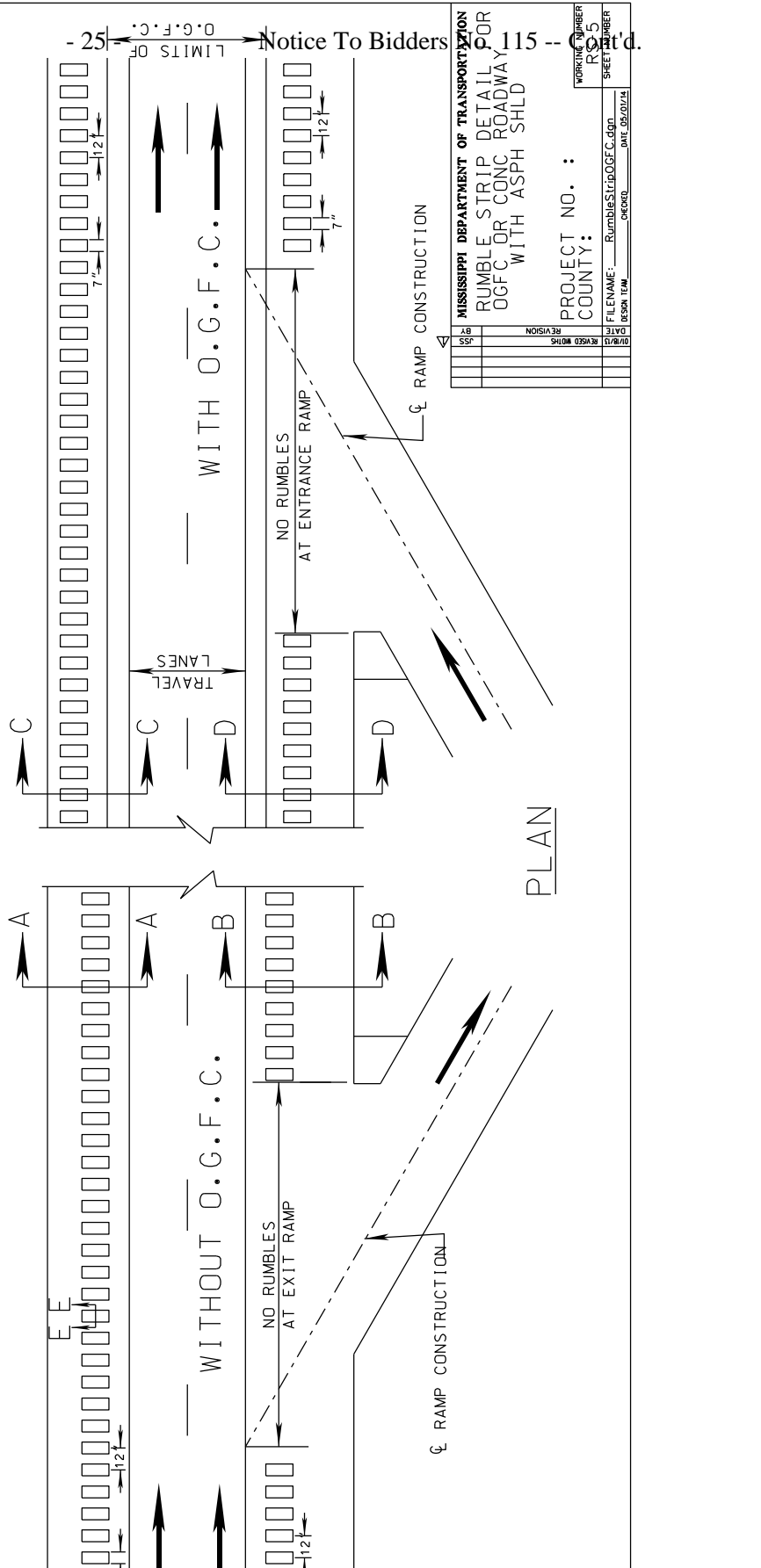
1. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
2. GROUND-IN RUMBLE STRIPS SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTIONS, ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
3. COST TO BE PAID FOR IN PAY ITEM 907-423-A RUMBLE STRIPS (GROUND-IN).
4. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED TO MAINLINE ONLY.

**SECTION E-E**

7'-0" TOLERANCE

7'-0"

3/8" - MIN.  
1/2" - MAX.



DATE	BY	REVISION

DATE: 05/02/14

FILENAME: RumbleStripOGFC.dgn

CHECKED: \_\_\_\_\_

DESIGN TEAM: \_\_\_\_\_

PROJECT NO. : \_\_\_\_\_

COUNTY: \_\_\_\_\_

WORKING NUMBER: **RS05**

SHEET NUMBER: **115** of **118**

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
RUMBLE STRIP DETAIL FOR  
OGFC OR CONC ROADWAY  
WITH ASPH SHLD

**Notice To Bidders**

LIMITS OF O.G.F.C.

WITH O.G.F.C.

WITHOUT O.G.F.C.

NO RUMBLES AT ENTRANCE RAMP

NO RUMBLES AT EXIT RAMP

RAMP CONSTRUCTION

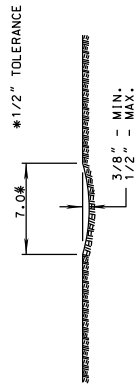
CL RAMP CONSTRUCTION

TRAVEL LANS

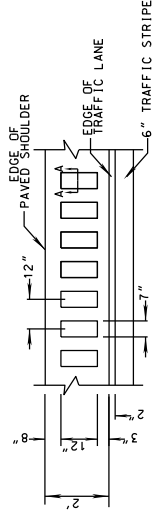
PLAN

STATE PROJECT NO.  
MISS.

STATE	PROJECT NO.
MSS.	



SECTION "A-A"



DETAIL "A"

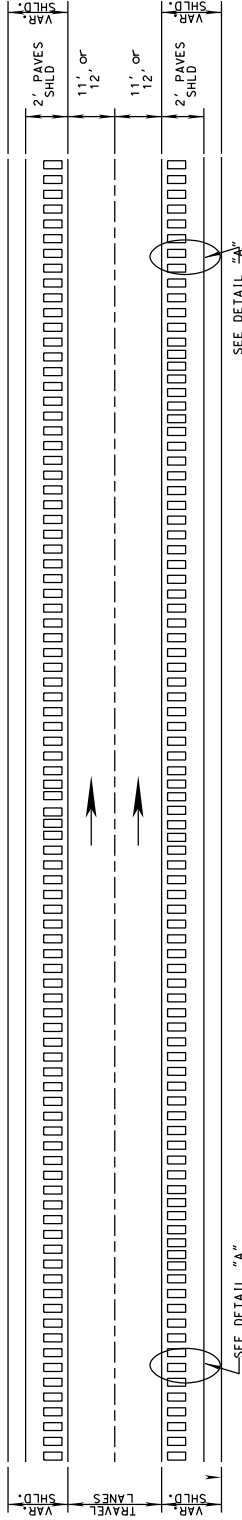
GENERAL NOTES  
 1. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT

2. GROUND-IN RUMBLE STRIPS SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER

3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS

4. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED TO MAINLINE ONLY.

5. DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.



SEE DETAIL "A"

SEE DETAIL "A"

PLAN  
 NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
RUMBLE STRIPS FOR 4-LANE DIVIDED ROADWAYS WITH 2-FOOT PAVED SHOULDERS CONCRETE LANES, ASPH. SHOULDER	
PROJECT NO. : 115	
COUNTY :	
WORKING NUMBER	PROJECT NUMBER
FILE NAME: \\s:\cumberland\tds.dwg	RS-6
DESIGN TEAM: S. LEWIS	SHEET NUMBER
DATE: 05/01/14	



**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 145**

**CODE: (SP)**

**DATE: 05/3/2017**

**SUBJECT: Contract Time**

**PROJECT: MP-2000-00(084) / 306163301- Districtwide (2)**

The calendar date for completion of work to be performed by the Contractor for this project shall be **March 30, 2018** which date or extended date as provided in Subsection 108.06 shall be the end of contract time. It is anticipated that the Notice of Award will be issued no later than **July 11, 2017** and the effective date of the Notice to Proceed / Beginning of Contract Time will be **August 10, 2017**.

Should the Contractor request a Notice to Proceed earlier than **August 10, 2017** and it is agreeable with the Department for an early Notice to Proceed, the requested date will become the new Notice to Proceed date.

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 - NOTICE TO BIDDERS NO. 146**

**CODE: (SP)**

**DATE: 05/03/2017**

**SUBJECT: Scope of Work**

**PROJECT: MP-2000-00(084) / 306163301 – District Wide**

The contract documents do not include an official set of construction plans but may, by reference; include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings”. All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following:

The Contractor is advised to follow the written description in the tables below for the placement of raised pavement markers rather than the county maps. The Contractor shall be responsible for furnishing and placing High Performance Raised Pavement Markers (HPRPMs) in accordance with Section 627 of the Standard Specifications and in accordance with the enclosed Standard Drawings PM-1, PM-2, PM-3, PM-4 and PM-8 as applicable. Also enclosed are maps of each county within the District showing the routes or the segments of routes on which HPRPMs are to be placed. Estimated quantities are shown by County.

The Contractor shall be responsible for removal of all existing raised pavement markers (RPMs) on any routes where they are currently located prior to placement of the new HPRPMs. The Contractor shall take all necessary precautions to insure that the underlying pavement is not damaged during removal of the existing RPMs and shall be responsible for any pavement damage resulting from the removal process. At the discretion of the Engineer, existing RPMs that are severely embedded into the pavement may be allowed to remain in place. In no case shall existing RPMs be removed more than one week prior to the placement of the new HPRPMs. This work will not be measured for separate payment, the cost shall be included in the unit prices of other items bid.

The Contractor will be further responsible for establishing all the necessary controls to insure that the HPRPMs are installed in a uniform line in accordance with the above mentioned detail sheets. The Contractor is also responsible for all pavement surface cleaning or other preparation necessary prior to placement of the HPRPMs to insure a proper bond. This work will not be measured for separate payment, the cost shall be included in the unit prices of other items bid.

The Contractor shall provide all signs and traffic handling devices necessary to safely maintain traffic around and through the work area. This shall include both vehicular and pedestrian traffic.

The Contractor is further advised that at the final inspection, all HPRPMs that are missing or damaged shall be replaced and will not be measured for separate payment.

Incidental work that is necessary to complete the work will not be measured for separate payment and the cost will be included in the unit prices of other items bid.

County	Length	HPRPMs	
		Yellow	Red-Clear
Attala	39.224	4,849	641
Calhoun	74.155	7,087	
Carroll	59.882	3,463	3,318
Grenada	66.301	9,815	2,503
Lafayette	72.728	4,772	5,023
Leflore	60.931	5,895	3,396
Montgomery	45.911	4,367	1,981
Quitman	63.301	5,570	
Tallahatchie	87.618	8,584	237
Yalobusha	108.654	9,077	4,658
<b>Total</b>	<b>679.365</b>	<b>64,261</b>	<b>21,757</b>

Attala County					
Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
14	SR 43	SR19/SR 14	10.119	911	
19	Winston Cnty Line	SR 14	10.705	963	
35	Kosciusko CL	SR 12	2.465	1,282	641
429	SR 14	SR 12	6.540	654	
731	SR 35 Bypass	Gilliland Street	1.319	132	
	Begin State Main.	SR 12	0.089	26	
735	SR 12	Urban Limit	0.614	61	
	Urban Limit	End State Main.	1.441	241	
	Begin State Main.	SR 12	1.482	148	
736			2.020	202	
739			1.800	166	
741			0.630	63	
<b>Total</b>			<b>39.224</b>	<b>4,849</b>	<b>641</b>

<b>Calhoun County</b>					
Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
8	Grenada Cnty Line	SR 9	9.747	975	
8	SR 9	SR 8	4.016	402	
9	Webster Cnty Line	SR 8	6.406	538	
330	Yalobusha Cnty Line	SR 32	7.180	718	
32	Yalobusha Cnty Line	SR330	11.146	1,003	
341	Chickasaw Cnty Line	SR 8	4.683	468	
	SR 8	SR 32	13.291	1,329	
	SR 32	Pontotoc Cnty Line	1.376	138	
9W	SR 9	Lafayette Cnty Line	11.520	1,037	
331	SR 9	Lafayette Cnty Line	4.790	479	
<b>Total</b>			<b>74.155</b>	<b>7,087</b>	

<b>Carroll County</b>					
Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
35	Attala Cnty Line	US 51	4.357	436	
	US 51	I-55	2.234	223	
	Begin State Main.	Grenada Cnty Line	13.800	1,380	
51	Holmes Cnty Line	Montgomery Cnty Line	14.480	1,448	
82	SR 17	Montgomery Cnty Line	10.710		1,842
7	Leflore Cnty Line	Grenada Cnty Line	5.660	566	
17	US 82	End State Main.	1.390	139	
404	I-55	Montgomery Cnty Line	0.530	53	
I-55	0.5 mi S/O SR 35	Montgomery Cnty Line	5.984		1,197
	Montgomery Cnty Line	SR 404	1.397		279
<b>Total</b>			<b>60.542</b>	<b>4,245</b>	<b>3,318</b>

<b>Grenada County</b>					
Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
7	Carroll Cnty Line	SR 8	8.380	804	
8	Tallahatchie Cnty Line	SR 35	6.017	542	
	SR 35 (North)	SR 35 (South)	3.765	339	
	SR 35 (South)	I-55	8.210	1,807	589
	US 51	SR 333	1.209	431	314
	SR 333	Calhoun Cnty Line	16.863	2,050	315
51	Montgomery Cnty Line	1 m/so SR 8	6.318	1,410	674
	1 m/so SR 8	SR 8	1.063	553	276
	SR 8	SR 332	1.759	730	335
	SR 332	SR 7	5.172	465	
	SR 7	Yalobusha Cnty Line	2.829	240	
35	SR 8	Tallahatchie Cnty Line	1.906	191	
332	US 51	End State Main.	2.810	253	
<b>Total</b>			<b>66.301</b>	<b>9,815</b>	<b>2,503</b>

<b>Lafayette County</b>					
Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
7	Yalobusha Cnty Line	SR 9W	12.058	1,206	
278	Panola Cnty Line	West Oxford CL	8.654		1,731
	East Oxford CL	Pontotoc Cnty Line	15.493		3,099
30	Begin State Main.	SR 7	0.200		40
	SR 7	Union Cnty Line	16.250	1,592	66
331	Calhoun Cnty Line	SR 334	8.630	863	
334	Begin State Main.	1 mile E/O CR# 418	5.479	548	
738			0.536	20	87
315	Yalobusha Cnty Line	Panola Cnty Line	4.158	416	
733	SR 328	End State Main.	0.890	89	
812			0.380	38	
<b>Total</b>			<b>72.728</b>	<b>4,772</b>	<b>5,023</b>

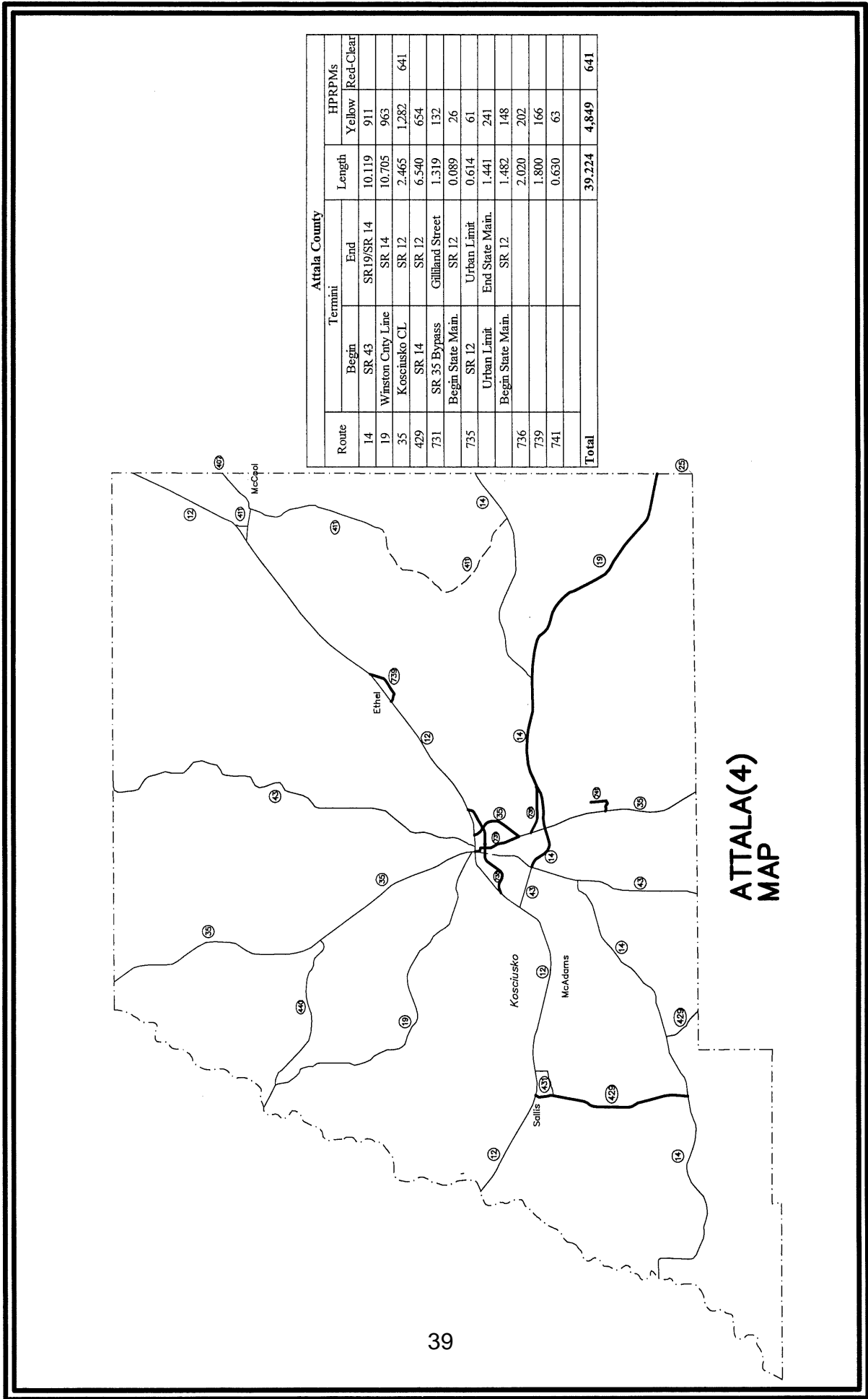
<b>Leflore County</b>					
Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
49E	SR 442	Tallahatchie Cnty Line	4.608	461	
82	Sunflower Cnty Line	US 49E	13.799		2,760
7	Humphreys Cnty Line	US 82	21.485	2,149	
	US 82	Greenwood CL	1.150	598	299
	Greenwood CL	Carroll Cnty Line	6.035	815	16
8	Sunflower Cnty Line	Tallahatchie Cnty Line	8.600	860	
706	US 49E	RR Crossing	0.370	56	
430	US 82	Carroll Cnty Line	4.244	933	239
744			0.230	23	
743			0.410		82
<b>Total</b>			<b>60.931</b>	<b>5,895</b>	<b>3,396</b>

<b>Montgomery County</b>					
Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
51	Winona CL (South)	US 82	2.762	2,034	139
	Winona CL (North)	Grenada Cnty Line	11.543	1,304	
82	Carroll Cnty Line	End Conc. Section	1.498		389
	End Conc. Section	Webster Cnty Line	22.294	222	1,453
407	Attala Cnty Line	Winston Cnty Line	3.800	380	
	Winston Cnty Line	US 51	3.374	337	
745	Vaiden-Kilmicrael Rd	SR 413	0.640	90	
<b>Total</b>			<b>45.911</b>	<b>4,367</b>	<b>1,981</b>

<b>Quitman County</b>					
Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
3	SR 278	Panola Cnty Line	14.770	1,182	
278	Coahoma Cnty Line	SR 3	10.701	803	
322	Coahoma Cnty Line	SR 3	8.574	857	
	SR 3	Panola Cnty Line	9.803	980	
316	Coahoma Cnty Line	SR 6	6.043	604	
315	Coahoma Cnty Line	SR 3	12.320	1,047	
	SR 3	Panola Cnty Line	0.800	68	
714	RR (Sledge)	SR 3	0.290	29	
<b>Total</b>			<b>63.301</b>	<b>5,570</b>	

<b>Tallahatchie County</b>					
Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
49E	Leflore Cnty Line	SR 32 E	10.175	1,018	
	SR 32 E	US 49 W	6.429	643	
49	US 49 W	Coahoma Cnty Line	1.886	161	21
49 W	Coahoma Cnty Line	US 49	1.930	193	
321	SR 32	Brazil	5.908	591	
32	Sunflower Cnty Line	US 49 E	7.761	776	
	Cassidy Bayou	SR 35	15.250	1,525	
	SR 35	Yalobusha Cnty Line	11.089	1,053	216
734			0.250	40	
35	Grenada Cnty Line	SR 32 E	12.480	1,148	
	SR 32	Panola Cnty Line	10.980	1,098	
728			0.660	66	
724			0.820	98	
726			0.660	40	
732			1.140	114	
727			0.200	20	
<b>Total</b>			<b>87.618</b>	<b>8,584</b>	<b>237</b>

<b>Yalobusha County</b>					
Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
51	SR 727	Panola Cnty Line	7.600	760	
330	US 51	I-55	2.391	239	
	I-55	SR 7	11.900	1,190	
	SR 7	SR 32	11.800	1,180	
32	Tallahatchie Cnty Line	US 51	1.183	106	
	US 51	SR 315	17.457	1,746	
	SR 315	Calhoun Cnty Line	10.169	1,017	
315	Blackmur Library	SR 32	8.125	813	
	SR 7	Lafayette Cnty Line	0.810	100	
727			0.990	89	
7	Grenada Cnty Line	Lafayette Cnty Line	23.828	1,837	1,433
55	0.4 mi N/O 330	Panola Cnty Line	12.401		3,225
<b>Total</b>			<b>108.654</b>	<b>9,077</b>	<b>4,658</b>

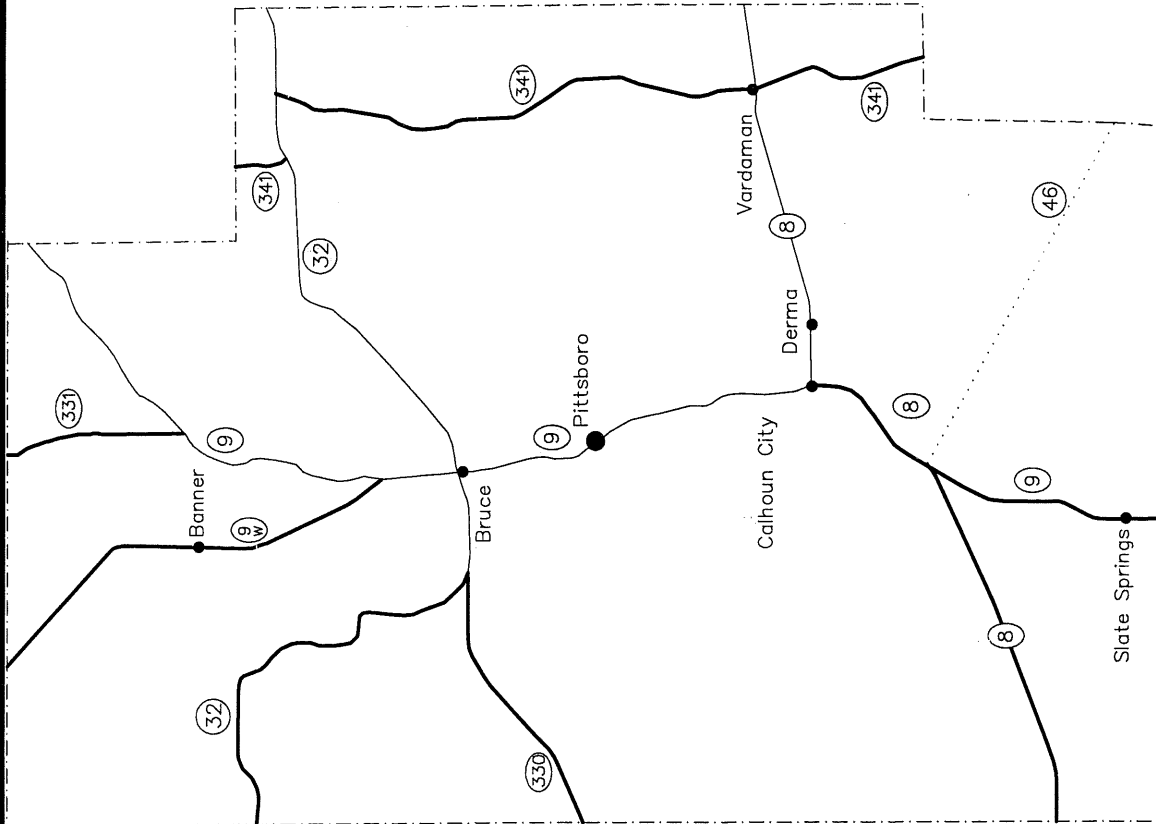


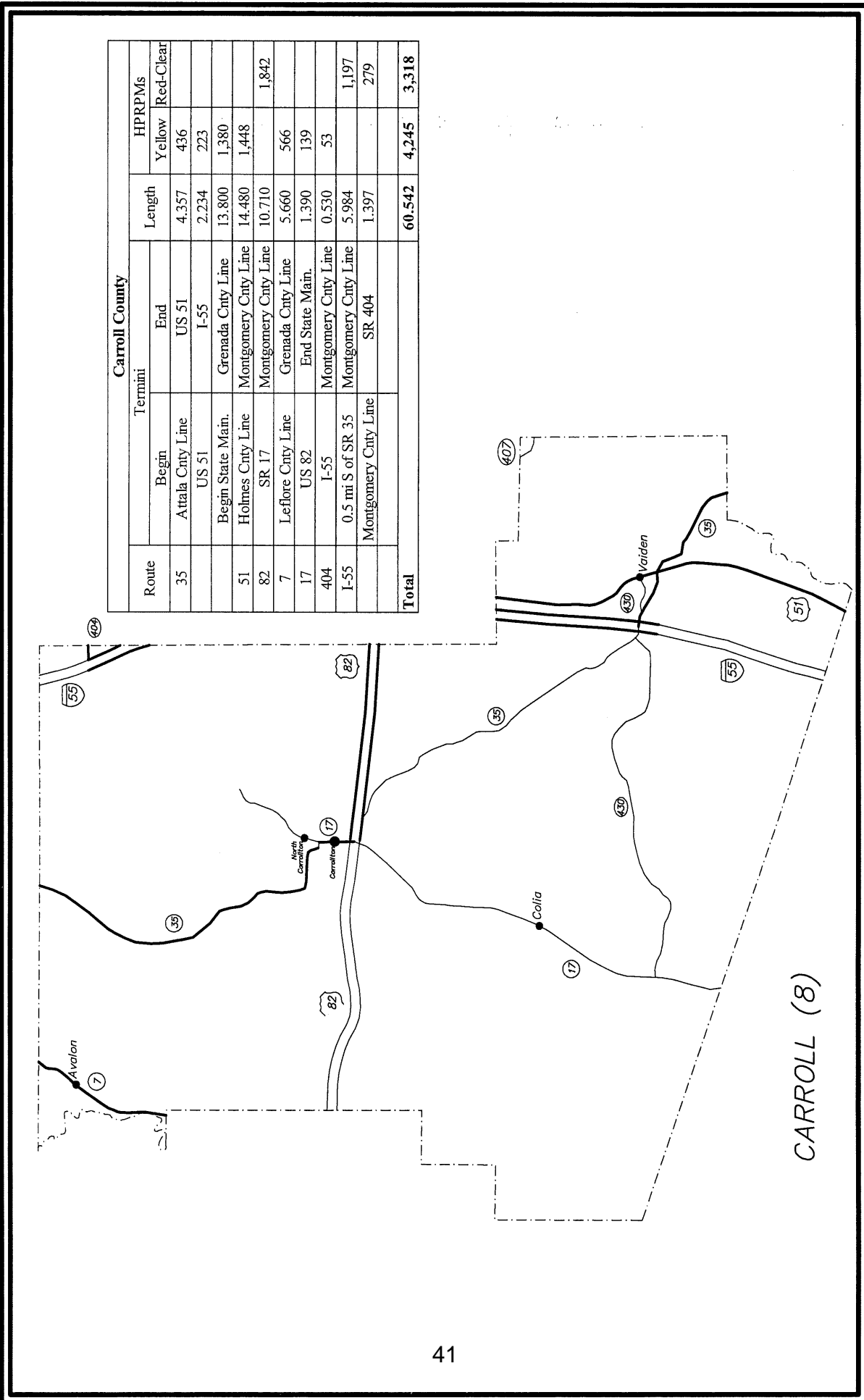
ATTALA(4)  
MAP



Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
8	Grenada Cnty Line	SR 9	9.747	975	
9	SR 9	SR 8	4.016	402	
330	Webster Cnty Line	SR 8	6.406	538	
32	Yalobusha Cnty Line	SR 32	7.180	718	
341	Yalobusha Cnty Line	SR330	11.146	1,003	
	Chickasaw Cnty Line	SR 8	4.683	468	
	SR 8	SR 32	13.291	1,329	
	SR 32	Ponotoc Cnty Line	1.376	138	
9W	SR 9	Lafayette Cnty Line	11.520	1,037	
331	SR 9	Lafayette Cnty Line	4.790	479	
<b>Total</b>			<b>74.155</b>	<b>7,087</b>	<b>0</b>

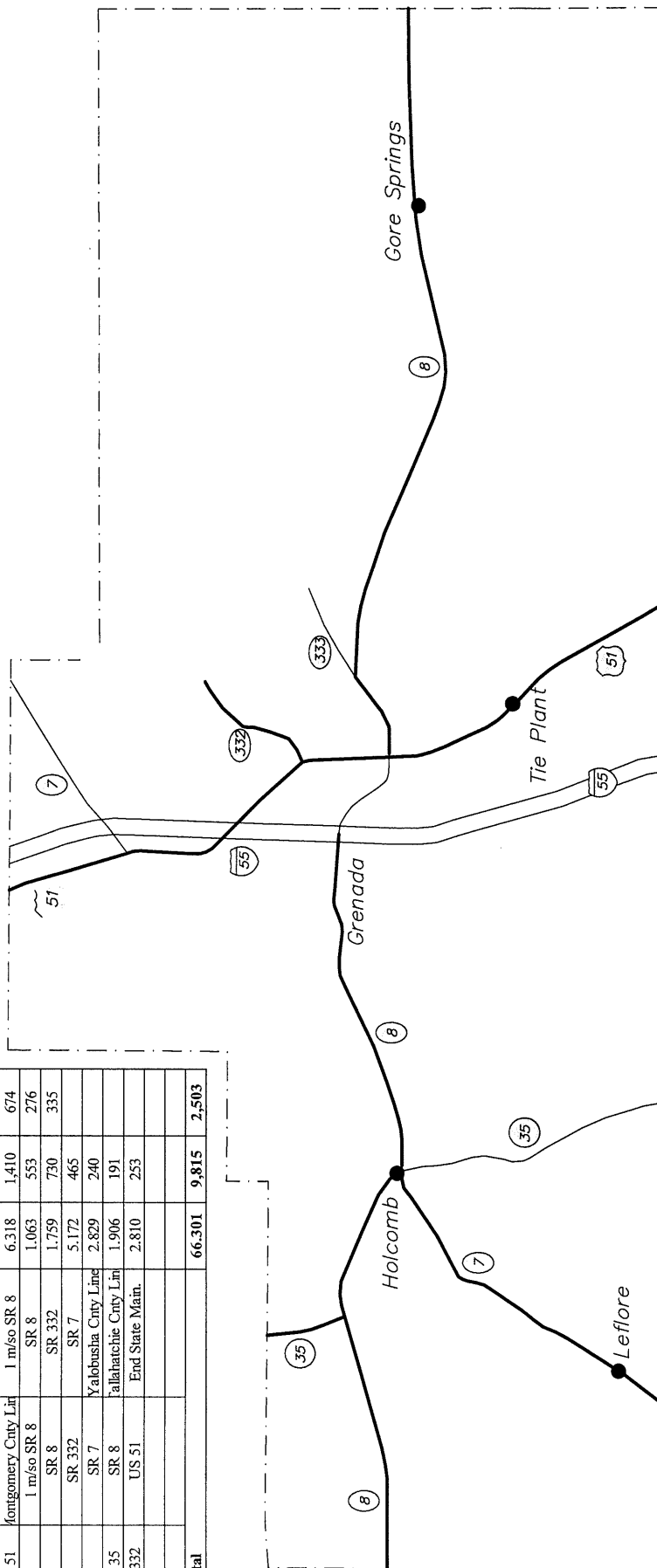
CALHOUN (7)





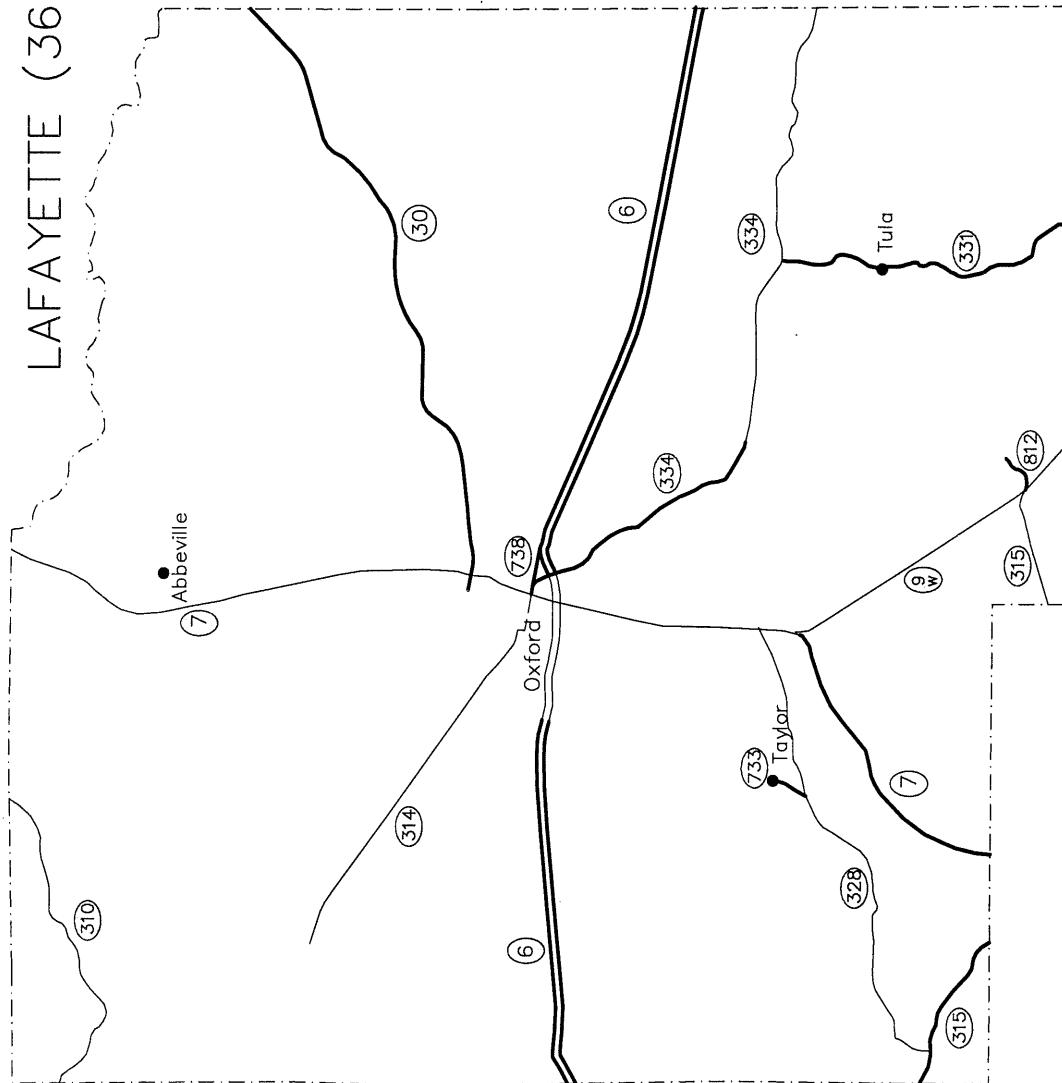
Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
35	Atala Cnty Line	US 51	4.357	436	
	US 51	I-55	2.234	223	
	Begin State Main.	Grenada Cnty Line	13.800	1,380	
51	Holmes Cnty Line	Montgomery Cnty Line	14.480	1,448	
82	SR 17	Montgomery Cnty Line	10.710		1,842
7	Leflore Cnty Line	Grenada Cnty Line	5.660	566	
17	US 82	End State Main.	1.390	139	
404	I-55	Montgomery Cnty Line	0.530	53	
I-55	0.5 mi S of SR 35	Montgomery Cnty Line	5.984	1,197	
	Montgomery Cnty Line	SR 404	1.397	279	
<b>Total</b>			<b>60.542</b>	<b>4,245</b>	<b>3,318</b>

Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
7	Carroll Cnty Line	SR 8	8.380	804	
8	Tallahatchie Cnty Lin	SR 35	6.017	542	
	SR 35 (North)	SR 35 (South)	3.765	339	
	SR 35 (South)	I-55	8.210	1,807	589
	US 51	SR 333	1.209	431	314
	SR 333	Calhoun Cnty Line	16.863	2,050	315
51	Montgomery Cnty Lin	1 m/so SR 8	6.318	1,410	674
	1 m/so SR 8	SR 8	1.063	553	276
	SR 8	SR 332	1.759	730	335
	SR 332	SR 7	5.172	465	
	SR 7	Yalobusha Cnty Line	2.829	240	
35	SR 8	Tallahatchie Cnty Lin	1.906	191	
332	US 51	End State Main.	2.810	253	
<b>Total</b>			<b>66.501</b>	<b>9,815</b>	<b>2,503</b>



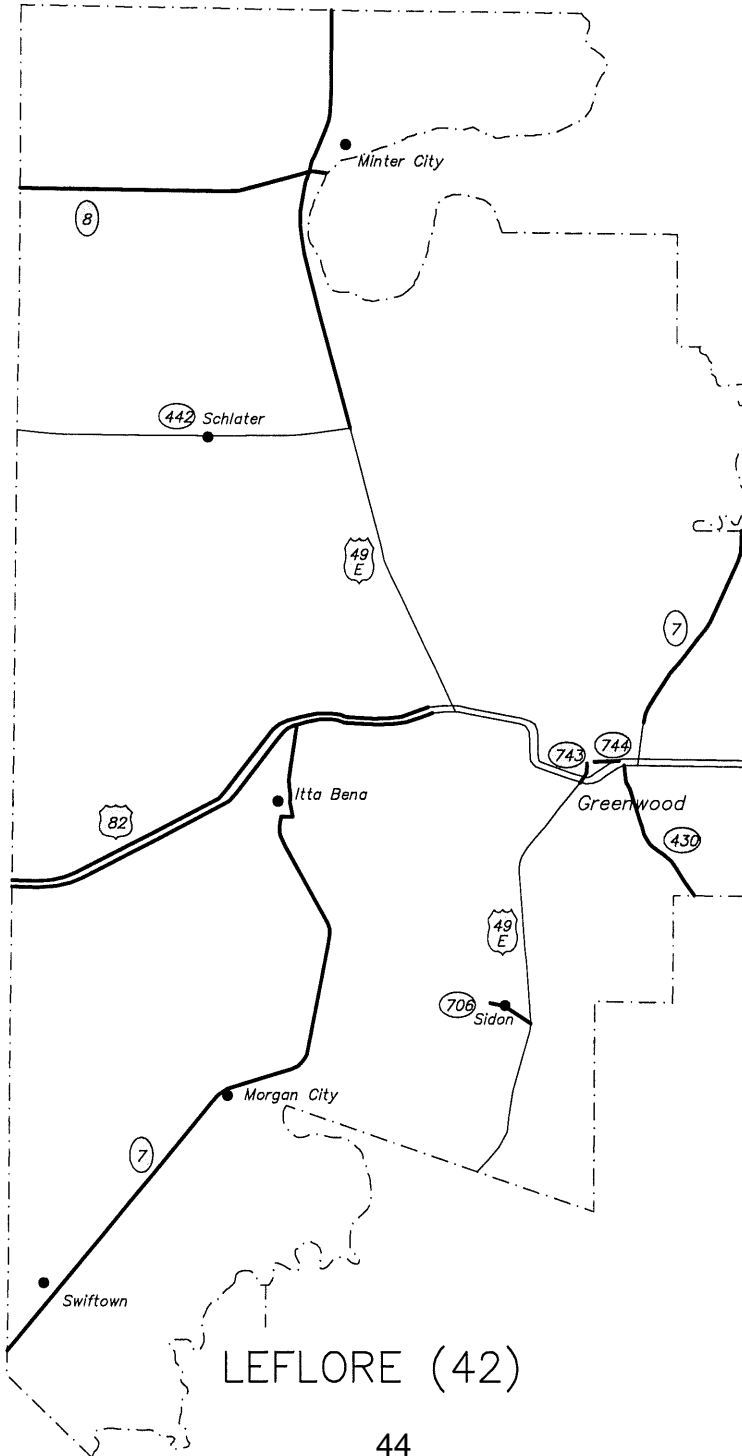
# GRENADA (22)

# LAFAYETTE (36)

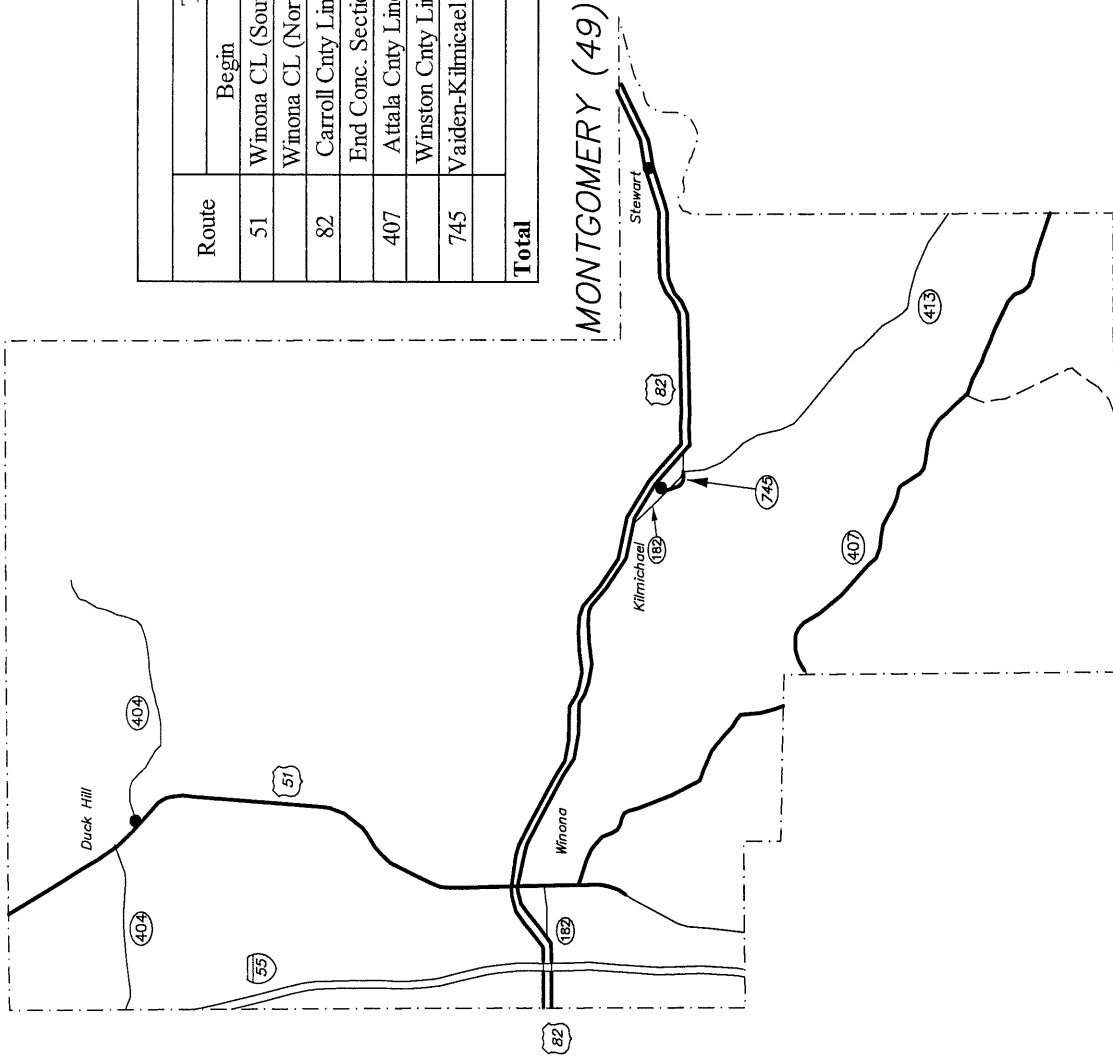


Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
7	Yalobusha Cnty Line	SR 9W	12.058	1,206	
278	Panola Cnty Line	West Oxford CL	8.654		1,731
	East Oxford CL	Ponotoc Cnty Line	15.493		3,099
30	Begin State Main.	SR 7	0.200		40
	SR 7	Union Cnty Line	16.250	1,592	66
331	Calhoun Cnty Line	Hwy 334	8.630	863	
334	Begin State Main.	1 mile E/O CR# 418	5.479	548	
738			0.536	20	87
315	Yalobusha Cnty Line	Panola Cnty Line	4.158	416	
733	SR 328	End State Main.	0.890	89	
812			0.380	38	
<b>Total</b>			<b>72.728</b>	<b>4,772</b>	<b>5,023</b>

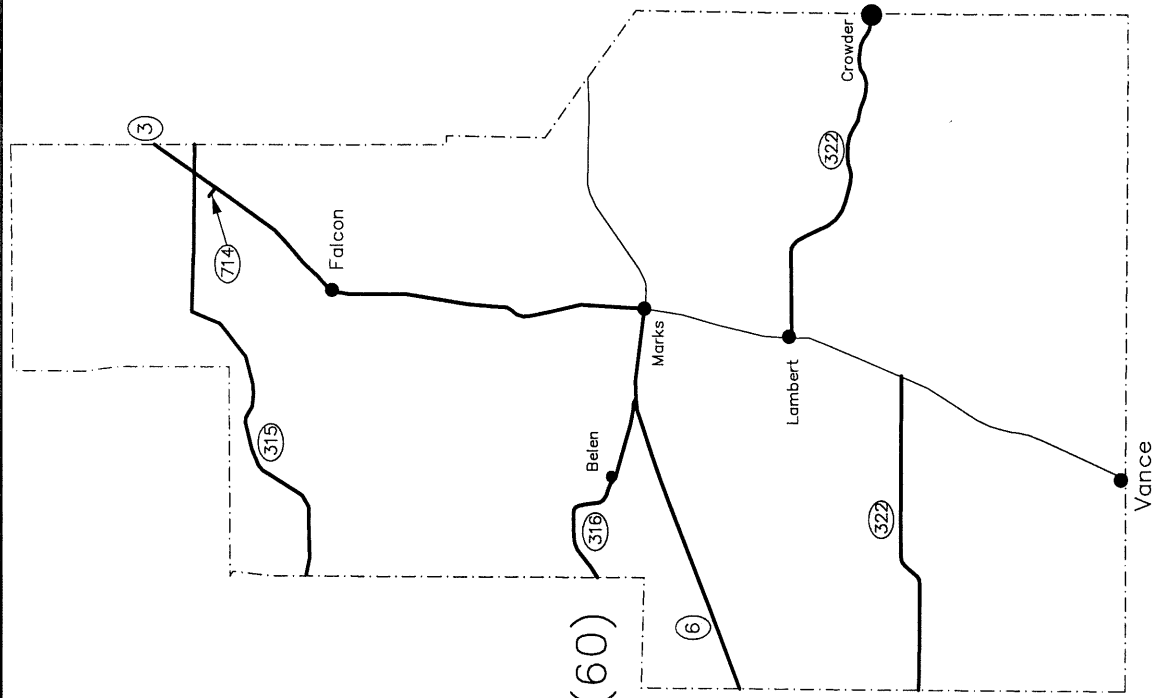
Leflore County					
Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
49E	SR 442	Tallahatchie Cnty Lin	4.608	461	
82	Sunflower Cnty Line	US 49E	13.799		2,760
7	Humphreys Cnty Line	US 82	21.485	2,149	
	US 82	Greenwood CL	1.150	598	299
	Greenwood CL	Carroll Cnty Line	6.035	815	16
8	Sunflower Cnty Line	Tallahatchie Cnty Lin	8.600	860	
706	US 49E	RR Crossing	0.370	56	
430	US 82	Carroll Cnty Line	4.244	933	239
744			0.230	23	
743			0.410		82
<b>Total</b>			<b>60.931</b>	<b>5,895</b>	<b>3,396</b>



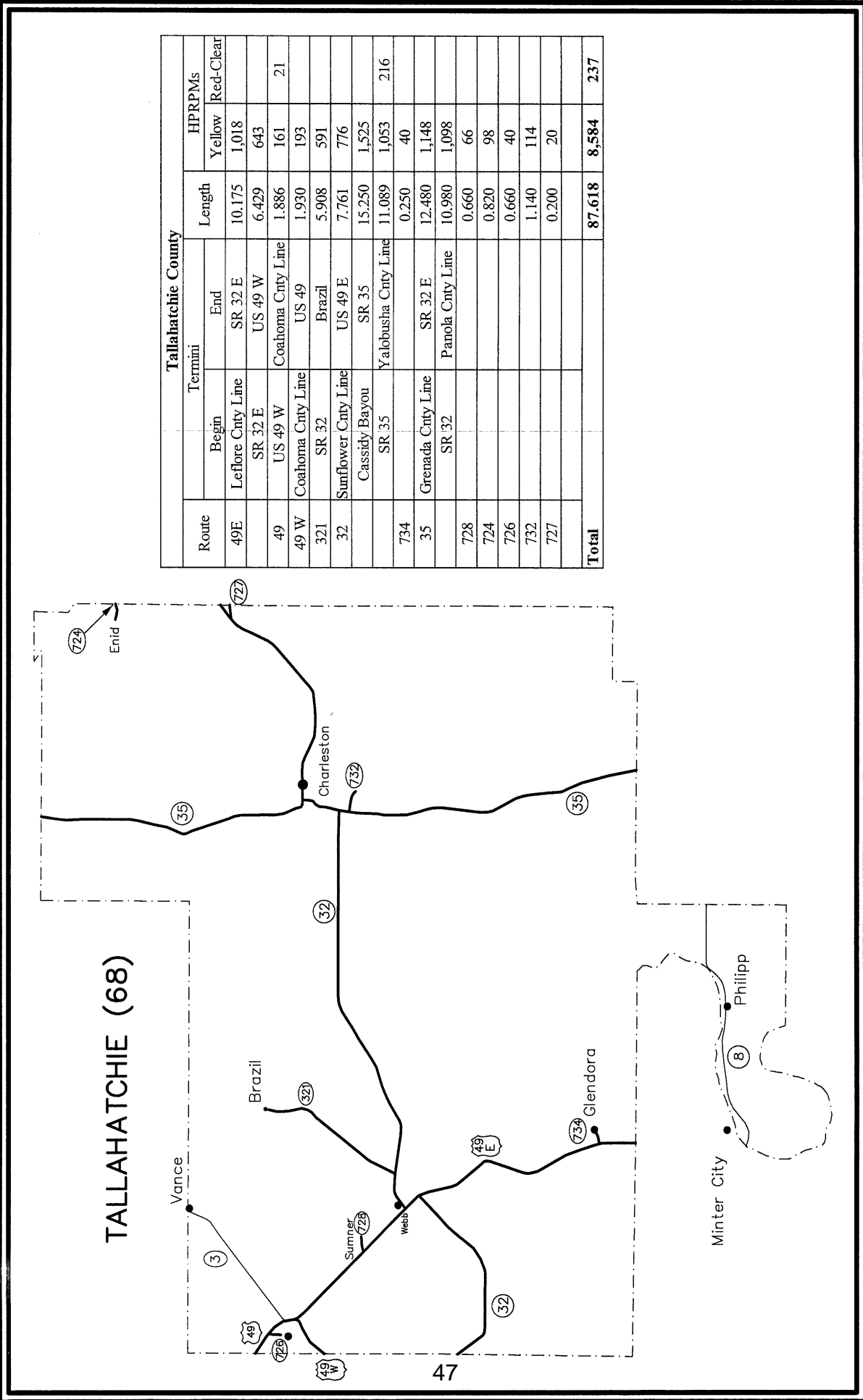
Montgomery County						
Route	Termini		Length	HPRPMs		
	Begin	End		Yellow	Red-Clear	
51	Winona CL (South)	US 82	2.762	2,034	139	
82	Winona CL (North)	Grenand Cnty Line	11.543	1,304		389
	Carroll Cnty Line	End Conc. Section	1.498			1,453
	End Conc. Section	Webster Cnty Line	22.294	222		380
407	Attala Cnty Line	Winston Cnty Line	3.800	337		90
	Winston Cnty Line	US 51	3.374			
745	Vaiden-Kilmichael Rd	SR 413	0.640			
<b>Total</b>			<b>45.911</b>	<b>4,367</b>		<b>1,981</b>



Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
3	SR 278	Panola Cnty Line	14.770	1,182	
278	Coahoma Cnty Line	SR 3	10.701	803	
322	Coahoma Cnty Line	SR 3	8.574	857	
	SR 3	Panola Cnty Line	9.803	980	
316	Coahoma Cnty Line	SR 6	6.043	604	
315	Coahoma Cnty Line	SR 3	12.320	1,047	
	SR 3	Panola Cnty Line	0.800	68	
714	RR (Sledge)	SR 3	0.290	29	
<b>Total</b>			<b>63.301</b>	<b>5,570</b>	<b>0</b>

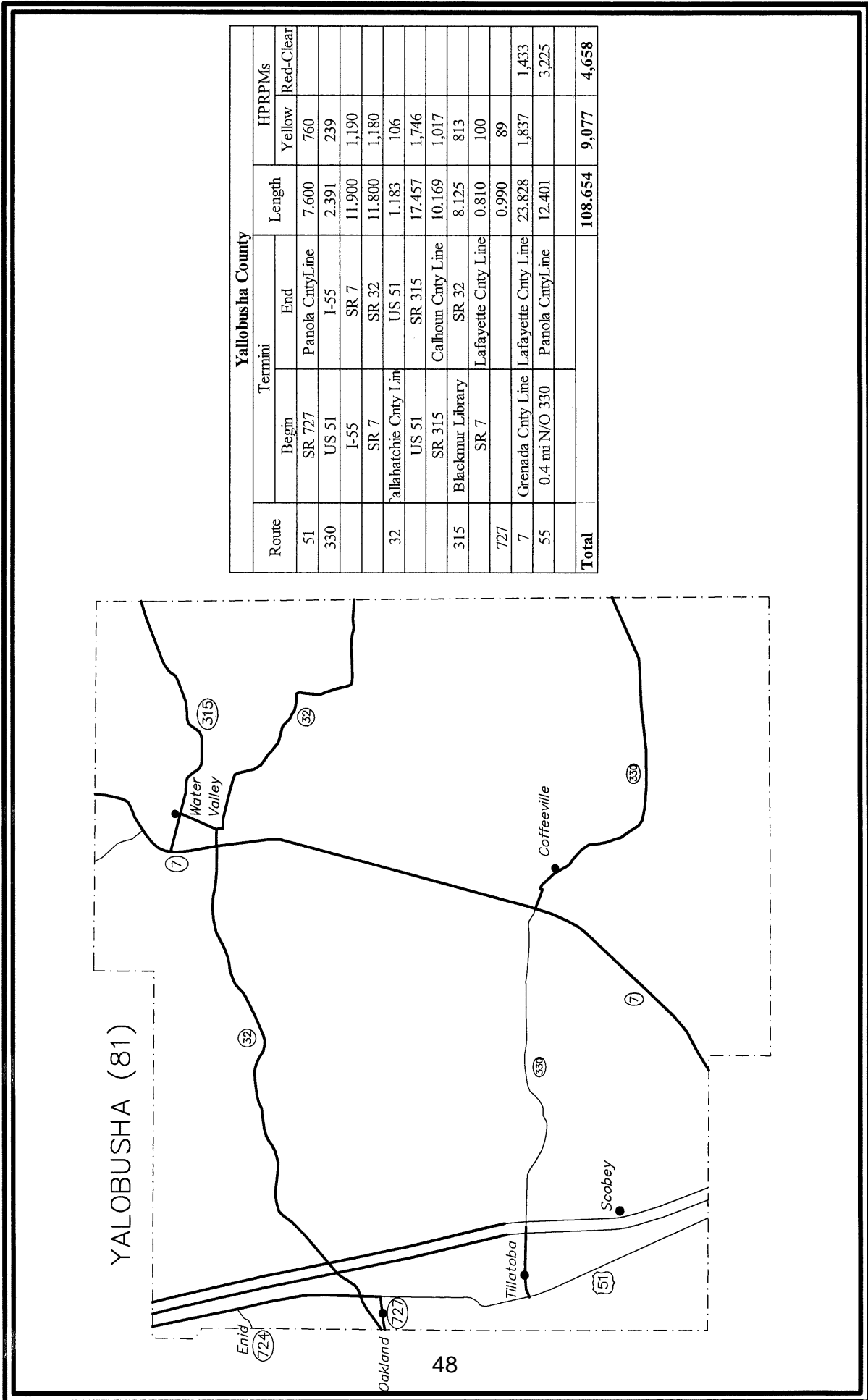


QUITMAN (60)



Route	Termini		Length	HPRPMs	
	Begin	End		Yellow	Red-Clear
49E	Leflore Cnty Line	SR 32 E	10.175	1,018	
	SR 32 E	US 49 W	6.429	643	
49	US 49 W	Coahoma Cnty Line	1.886	161	21
49 W	Coahoma Cnty Line	US 49	1.930	193	
321	SR 32	Brazil	5.908	591	
32	Sunflower Cnty Line	US 49 E	7.761	776	
	Cassidy Bayou	SR 35	15.250	1,525	
	SR 35	Yalobusha Cnty Line	11.089	1,053	216
734			0.250	40	
35	Grenada Cnty Line	SR 32 E	12.480	1,148	
	SR 32	Panola Cnty Line	10.980	1,098	
728			0.660	66	
724			0.820	98	
726			0.660	40	
732			1.140	114	
727			0.200	20	
<b>Total</b>			<b>87.618</b>	<b>8,584</b>	<b>237</b>





Route	Yalobusha County				HPRPMs	
	Termimi		Length	Yellow	Red-Clear	
	Begin	End				
51	SR 727	Panola CntyLine	7.600	760		
330	US 51	I-55	2.391	239		
	I-55	SR 7	11.900	1,190		
	SR 7	SR 32	11.800	1,180		
32	allahatchie Cnty Lin	US 51	1.183	106		
	US 51	SR 315	17.457	1,746		
	SR 315	Calhoun Cnty Line	10.169	1,017		
315	Blackmur Library	SR 32	8.125	813		
	SR 7	Lafayette Cnty Line	0.810	100		
727			0.990	89		
7	Grenada Cnty Line	Lafayette Cnty Line	23.828	1,837	1,433	
55	0.4 mi N/O 330	Panola CntyLine	12.401		3,225	
<b>Total</b>			<b>108.654</b>	<b>9,077</b>	<b>4,658</b>	

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-103-1

CODE: (SP)

DATE: 05/02/2017

SUBJECT: Award and Execution of Contract

Section 103, Award and Execution of Contract, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

### **907-103.01--Consideration of Proposal.**

Delete the fourth and fifth sentences of the third paragraph of Subsection 103.01 on page 19, and substitute the following.

Should the attachment not accompany the bid when submitted, the Contractor shall have 10 days following the opening of the bids to furnish the required information to the Contract Administration Director for attachment to the bid. Failure to provide the attachment within 10 days will result in the nonresident Contractor's bid being rejected and not considered for award. As used herein, the term "resident Contractor" includes a nonresident person, firm or corporation that has been qualified to do business in this State and has maintained a permanent full-time office in the State of Mississippi for two years prior to the submission of the bid, and the subsidiaries and affiliates of such a person, firm or corporation.

# SECTION 905 - PROPOSAL

Date \_\_\_\_\_

Mississippi Transportation Commission  
Jackson, Mississippi

Sirs: The following proposal is made on behalf of \_\_\_\_\_  
\_\_\_\_\_ of \_\_\_\_\_  
\_\_\_\_\_

for constructing the following designated project(s) within the time(s) hereinafter specified.

The plans are composed of drawings and blue prints on file in the offices of the Mississippi Department of Transportation, Jackson, Mississippi.

The Specifications are the current Standard Specifications of the Mississippi Department of Transportation approved by the Federal Highway Administration, except where superseded or amended by the plans, Special Provisions and Notice(s) to Bidders attached hereto and made a part thereof.

I (We) certify that I (we) possess a copy of said Standard and any Supplemental Specifications.

Evidence of my (our) authority to submit the Proposal is hereby furnished. The proposal is made without collusion on the part of any person, firm or corporation. I (We) certify that I (we) have carefully examined the Plans, the Specifications, including the Special Provisions and Notice(s) to Bidders, herein, and have personally examined the site of the work. On the basis of the Specifications, Special Provisions, Notice(s) to Bidders, and Plans, I (we) propose to furnish all necessary machinery, tools, apparatus and other means of construction and do all the work and furnish all the materials in the manner specified. I (We) understand that the quantities mentioned herein are approximate only and are subject to either increase or decrease, and hereby propose to perform any increased or decreased quantities of work at the unit prices bid, in accordance with the above.

I (We) acknowledge that this proposal will be found irregular and/or non-responsive unless a certified check, cashier's check, or Proposal Guaranty Bond in the amount as required in the Advertisement (or, by law) is submitted electronically with the proposal or is delivered to the Contract Administration Engineer prior to the bid opening time specified in the advertisement.

**INSTRUCTION TO BIDDERS: Alternate and Optional Items on Bid Schedule.**

1. Two or more items entered opposite a single unit quantity WITHOUT DEFINITE DESIGNATION AS "ALTERNATE ITEMS" are considered as "OPTIONAL ITEMS". Bidders may or may not indicate on bids the Optional Item proposed to be furnished or performed WITHOUT PREJUDICE IN REGARD TO IRREGULARITY OF BIDS.
2. Items classified on the bid schedule as "ALTERNATE ITEMS" and/or "ALTERNATE TYPES OF CONSTRUCTION" must be preselected and indicated on bids. However, "Alternate Types of Construction" may include Optional Items to be treated as set out in Paragraph 1, above.
3. Optional items not preselected and indicated on the bid schedule MUST be designated in accordance with Subsection 102.06 prior to or at the time of execution of the contract.
4. Optional and Alternate items designated must be used throughout the project.

I (We) further propose to perform all "force account or extra work" that may be required of me (us) on the basis provided in the Specifications and to give such work my (our) personal attention in order to see that it is economically performed.

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) shall submit electronically with our proposal or deliver prior to the bid opening time a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

**SECTION 905 -- PROPOSAL (CONTINUED)**

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Respectfully Submitted,

DATE \_\_\_\_\_

\_\_\_\_\_  
Contractor

BY \_\_\_\_\_  
Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

\_\_\_\_\_  
President Address

\_\_\_\_\_  
Secretary Address

\_\_\_\_\_  
Treasurer Address

The following is my (our) itemized proposal.

Raised Pavement Markers on various routes in District 2, known as State Project No. MP-2000-00(084) / 306163301 Districtwide.

Line no.	Item Code	Adj Code	Quantity	Units	Description[Fixed Unit Price]
					<b>Roadway Items</b>
0010	618-A001		1	Lump Sum	Maintenance of Traffic
0020	627-K001		21,757	Each	Red-Clear Reflective High Performance Raised Markers
0030	627-L001		64,261	Each	Two-Way Yellow Reflective High Performance Raised Markers

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

**CONDITIONS FOR COMBINATION BID**

If a bidder elects to submit a combined bid for two or more of the contracts listed for this month's letting, the bidder must complete and execute these sheets of the proposal in each of the individual proposals to constitute a combination bid. In addition to this requirement, each individual contract shall be completed, executed and submitted in the usual specified manner.

Failure to execute this Combination Bid Proposal in each of the contracts combined will be just cause for each proposal to be received and evaluated as a separate bid.

It is understood that the Mississippi Transportation Commission not only reserves the right to reject any and all proposals, but also the right to award contracts upon the basis of lowest separate bids or combination bids most advantageous to the State.

It is further understood and agreed that the Combination Bid Proposal is for comparison of bids only and that each contract shall operate in every respect as a separate contract in accordance with its proposal and contract documents.

I (We) agree to complete each contract on or before its specified completion date.

\*\*\*\*\*

**COMBINATION BID PROPOSAL**

This proposal is tendered as one part of a Combination Bid Proposal utilizing option \_\_\_\* of Subsection 102.11 on the following contracts:

\* Option to be shown as either (a), (b), or (c).

	<u>Project No.</u>	<u>County</u>	<u>Project No.</u>	<u>County</u>
1.	_____	_____	6.	_____
2.	_____	_____	7.	_____
3.	_____	_____	8.	_____
4.	_____	_____	9.	_____
5.	_____	_____	10.	_____

(a) If Combination A has been selected, your Combination Bid is complete.

(b) If Combination B has been selected, then complete the following page.

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
1.					
2.					
3.					
4.					
5.					
6.					
7.					
8.					

For Informational Purposes Only

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
9.					
10.					

(c) If Combination C has been selected, then initial and complete ONE of the following.

\_\_\_\_\_ I (We) desire to be awarded work not to exceed a total monetary value of \$ \_\_\_\_\_.

\_\_\_\_\_ I (We) desire to be awarded work not to exceed \_\_\_\_\_ number of contracts.





TO: EXECUTIVE DIRECTOR, MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
JACKSON, MISSISSIPPI

**CERTIFICATE**

If awarded this contract, I (we) contemplate that portions of the contract will be sublet. I (we) certify that those subcontracts which are equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

I (we) agree that this notification of intent DOES NOT constitute APPROVAL of the subcontracts.

_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)

NOTE: Failure to complete the above DOES NOT preclude subsequent subcontracts. Subsequent subcontracts, if any, equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

Contractor \_\_\_\_\_

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**  
**CERTIFICATION**

I, \_\_\_\_\_,  
(Name of person signing bid)

individually, and in my capacity as \_\_\_\_\_ of  
(Title of person signing bid)

\_\_\_\_\_  
(Name of Firm, partnership, or Corporation)

do hereby certify under penalty of perjury under the laws of the United States and the State of Mississippi

that \_\_\_\_\_, Bidder  
(Name of Firm, Partnership, or Corporation)

on Project No. **MP-2000-00(084)/ 306163301000**

in **District 2** \_\_\_\_\_ County(ies), Mississippi, has not either directly or indirectly entered into any agreement, participated in any collusion; or otherwise taken any action in restraint of free competitive bidding in connection with this contract; nor have any of its corporate officers or principal owners.

Except as noted hereafter, it is further certified that said legal entity and its corporate officers, principal owners, managers, auditors and others in a position of administering federal funds are not currently under suspension, debarment, voluntary exclusion or determination of ineligibility; nor have a debarment pending; nor been suspended, debarred, voluntarily excluded or determined ineligible within the past three years by the Mississippi Transportation Commission, the State of Mississippi, any other State or a federal agency; nor been indicted, convicted or had a civil judgment rendered by a court of competent jurisdiction in any matter involving fraud or official misconduct within the past three years.

Do exceptions exist and are made a part thereof?      Yes / No

Any exceptions shall address to whom it applies, initiating agency and dates of such action.

Note: Exceptions will not necessarily result in denial of award but will be considered in determining bidder responsibility. Providing false information may result in criminal prosecution or administrative sanctions.

All of the foregoing is true and correct.

(1/2016 S)

SECTION 902

CONTRACT FOR MP-2000-00(084)/ 306163301000

LOCATED IN THE COUNTY(IES) OF District 2

STATE OF MISSISSIPPI,  
COUNTY OF HINDS

This contract entered into by and between the Mississippi Transportation Commission on one hand, and the undersigned contractor, on the other witnesseth;

That, in consideration of the payment by the Mississippi Transportation Commission of the prices set out in the proposal hereto attached, to the undersigned contractor, such payment to be made in the manner and at the time of times specified in the specifications and the special provisions, if any, the undersigned contractor hereby agrees to accept the prices stated in the proposal in full compensation for the furnishing of all materials and equipment and the executing of all the work contemplated in this contract.

It is understood and agreed that the advertising according to law, the Advertisement, the instructions to bidders, the proposal for the contract, the specifications, the revisions of the specifications, the special provisions, and also the plans for the work herein contemplated, said plans showing more particularly the details of the work to be done, shall be held to be, and are hereby made a part of this contract by specific reference thereto and with like effect as if each and all of said instruments had been set out fully herein in words and figures.

It is further agreed that for the same consideration the undersigned contractor shall be responsible for all loss or damage arising out of the nature of the work aforesaid; or from the action of the elements and unforeseen obstructions or difficulties which may be encountered in the prosecution of the same and for all risks of every description connected with the work, exceptions being those specifically set out in the contract; and for faithfully completing the whole work in good and workmanlike manner according to the approved Plans, Specifications, Special Provisions, Notice(s) to Bidders and requirements of the Mississippi Department of Transportation.

It is further agreed that the work shall be done under the direct supervision and to the complete satisfaction of the Executive Director of the Mississippi Department of Transportation, or his authorized representatives, and when Federal Funds are involved subject to inspection at all times and approval by the Federal Highway Administration, or its agents as the case may be, or the agents of any other Agency whose funds are involved in accordance with those Acts of the Legislature of the State of Mississippi approved by the Governor and such rules and regulations issued pursuant thereto by the Mississippi Transportation Commission and the authorized Federal Agencies.

The Contractor agrees that all labor as outlined in the Special Provisions may be secured from list furnished by

It is agreed and understood that each and every provision of law and clause required by law to be inserted in this contract shall be deemed to be inserted herein and this contract shall be read and enforced as though it were included herein, and, if through mere mistake or otherwise any such provision is not inserted, then upon the application of either party hereto, the contract shall forthwith be physically amended to make such insertion.

The Contractor agrees that he has read each and every clause of this Contract, and fully understands the meaning of same and that he will comply with all the terms, covenants and agreements therein set forth.

Witness our signatures this the \_\_\_ day of \_\_\_\_\_, \_\_\_\_\_.

\_\_\_\_\_  
Contractor(s)

By \_\_\_\_\_

MISSISSIPPI TRANSPORTATION COMMISSION

Title \_\_\_\_\_

By \_\_\_\_\_

Signed and sealed in the presence of:  
(names and addresses of witnesses)

Executive Director

\_\_\_\_\_

\_\_\_\_\_

Secretary to the Commission

Award authorized by the Mississippi Transportation Commission in session on the \_\_\_ day of \_\_\_\_\_, \_\_\_\_\_, Minute Book No. \_\_\_\_\_, Page No. \_\_\_\_\_.

**SECTION 903**  
**PERFORMANCE AND PAYMENT BOND**

CONTRACT BOND FOR: MP-2000-00(084)/ 306163301000

LOCATED IN THE COUNTY(IES) OF: District 2

STATE OF MISSISSIPPI,  
COUNTY OF HINDS

Know all men by these presents: that we, \_\_\_\_\_  
\_\_\_\_\_  
Principal, a \_\_\_\_\_ (Contractor)

residing at \_\_\_\_\_ in the State of \_\_\_\_\_

and \_\_\_\_\_  
\_\_\_\_\_  
(Surety)

residing at \_\_\_\_\_ in the State of \_\_\_\_\_,

authorized to do business in the State of Mississippi, under the laws thereof, as surety, effective as of the contract date shown below, are held and firmly bound unto the State of Mississippi in the sum of \_\_\_\_\_

\_\_\_\_\_ (\$ \_\_\_\_\_) Dollars, lawful money of the United States of America, to be paid to it for which payment well and truly to be made, we bind ourselves, our heirs, administrators, successors, or assigns jointly and severally by these presents.

The conditions of this bond are such, that whereas the said \_\_\_\_\_

\_\_\_\_\_  
principal, has (have) entered into a contract with the Mississippi Transportation Commission, bearing the date of \_\_\_\_\_ day of \_\_\_\_\_ A.D. \_\_\_\_\_ hereto annexed, for the construction of certain projects(s) in the State of Mississippi as mentioned in said contract in accordance with the Contract Documents therefor, on file in the offices of the Mississippi Department of Transportation, Jackson, Mississippi.

Now therefore, if the above bounden \_\_\_\_\_  
\_\_\_\_\_ in all things shall stand to and abide by and well and truly observe, do keep and perform all and singular the terms, covenants, conditions, guarantees and agreements in said contract, contained on his (their) part to be observed, done, kept and performed and each of them, at the time and in the manner and form and furnish all of the material and equipment specified in said contract in strict accordance with the terms of said contract which said plans, specifications and special provisions are included in and form a part of said contract and shall maintain the said work contemplated until its final completion and acceptance as specified in Subsection 109.11 of the approved specifications, and save harmless said Mississippi Transportation Commission from any loss or damage arising out of or occasioned by the negligence, wrongful or criminal act, overcharge, fraud, or any other loss or damage whatsoever, on the part of said principal (s), his (their) agents, servants, or employees in the performance of said work or in any manner connected therewith, and shall be liable and responsible in a civil action instituted by the State at the instance of the Mississippi Transportation Commission or any officer of the State authorized in such cases, for double any amount in money or property, the State may lose or be overcharged or otherwise defrauded of, by reason of wrongful or criminal act, if any, of the Contractor(s), his (their) agents or employees, and shall promptly pay the said agents, servants and employees and all persons furnishing labor, material, equipment or supplies therefor, including premiums incurred, for Surety Bonds, Liability Insurance, and Workmen's Compensation Insurance; with the additional obligation that such Contractor shall promptly make payment of all taxes, licenses, assessments, contributions, damages,

any liquidated damages which may arise prior to any termination of said principal's contract, any liquidated damages which may arise after termination of the said principal's contract due to default on the part of said principal, penalties and interest thereon, when and as the same may be due this state, or any county, municipality, board, department, commission or political subdivision: in the course of the performance of said work and in accordance with Sections 31-5-51 et seq. Mississippi Code of 1972, and other State statutes applicable thereto, and shall carry out to the letter and to the satisfaction of the Executive Director of the Mississippi Department of Transportation, all, each and every one of the stipulations, obligations, conditions, covenants and agreements and terms of said contract in accordance with the terms thereof and all of the expense and cost and attorney's fee that may be incurred in the enforcement of the performance of said contract, or in the enforcement of the conditions and obligations of this bond, then this obligation shall be null and void, otherwise to be and remain in full force and virtue.

_____	_____
(Contractors) Principal	Surety
By _____	By _____
	(Signature) Attorney in Fact
	Address _____
	_____
Title _____	_____
(Contractor's Seal)	(Printed) MS Agent
	_____
	(Signature) MS Agent
	Address _____
	_____
	_____
	(Surety Seal)
	_____
	Mississippi Insurance ID Number



# BID BOND

KNOW ALL MEN BY THESE PRESENTS, that we \_\_\_\_\_  
Contractor

\_\_\_\_\_  
Address

\_\_\_\_\_  
City, State ZIP

As principal, hereinafter called the Principal, and \_\_\_\_\_  
Surety

a corporation duly organized under the laws of the state of \_\_\_\_\_

as Surety, hereinafter called the Surety, are held and firmly bound unto State of Mississippi, Jackson, Mississippi

As Obligee, hereinafter called Obligee, in the sum of **Five Per Cent (5%) of Amount Bid**

Dollars(\$ \_\_\_\_\_ )

for the payment of which sum will and truly to be made, the said Principal and said Surety, bind ourselves, our heirs, executors, administrators, successors and assigns, jointly and severally, firmly by these presents.

WHEREAS, the Principal has submitted a bid for **Raised Pavement Markers on various routes in District 2, known as State Project No. MP-2000-00(084) / 306163301 Districtwide.**

NOW THEREFORE, the condition of this obligation is such that if the aforesaid Principal shall be awarded the contract, the said Principal will, within the time required, enter into a formal contract and give a good and sufficient bond to secure the performance of the terms and conditions of the contract, then this obligation to be void; otherwise the Principal and Surety will pay unto the Obligee the difference in money between the amount of the bid of the said Principal and the amount for which the Obligee legally contracts with another party to perform the work if the latter amount be in excess of the former, but in no event shall liability hereunder exceed the penal sum hereof.

Signed and sealed this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_

\_\_\_\_\_  
(Witness)

\_\_\_\_\_  
(Principal) (Seal)

By: \_\_\_\_\_  
(Name) (Title)

\_\_\_\_\_  
(Surety) (Seal)

\_\_\_\_\_  
(Witness)

By: \_\_\_\_\_  
(Attorney-in-Fact)

\_\_\_\_\_  
(MS Agent)

\_\_\_\_\_  
Mississippi Insurance ID Number