

MDOT Use Only

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Keyed \_\_\_\_\_

11 -



SM No. CMP5000520931

# PROPOSAL AND CONTRACT DOCUMENTS

## FOR THE CONSTRUCTION OF

11

Mill & Overlay approximately 6 miles of SR 39 from SR 21 to SR 145, & approximately 2 miles of SR 21 from 1 mile West of SR 39 to SR 39, known as State Project Nos. MP-5000-52(093) / 305744301 & 302 in Noxubee County.

Project Completion: 34 Working Days

**(STATE DELEGATED)**

### NOTICE

**BIDDERS MUST PURCHASE A BID PROPOSAL FROM  
MDOT CONTRACT ADMINISTRATION DIVISION TO BID  
THIS PROJECT.**

Electronic addendum updates will be posted on [www.gomdot.com](http://www.gomdot.com)

# SECTION 900

## OF THE CURRENT 2017 STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION

JACKSON, MISSISSIPPI

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
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(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET  
OF SECTION 905 AS ADDENDA)

06/27/2017 02:56 PM

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

## SECTION 901 - ADVERTISEMENT

Electronic bids will be received by the Mississippi Transportation Commission at 10:00 o'clock A.M., Tuesday, July 25, 2017, from the Bid Express Service and shortly thereafter publicly read on the Sixth Floor for:

Mill & Overlay approximately 6 miles of SR 39 from SR 21 to SR 145, & approximately 2 miles of SR 21 from 1 mile West of SR 39 to SR 39, known as State Project Nos. MP-5000-52(093) / 305744301 & 302 in Noxubee County.

The attention of bidders is directed to the predetermined minimum wage rate set by the U. S. Department of Labor under the Fair Labor Standards Act.

The Mississippi Department of Transportation hereby notifies all bidders that it will affirmatively insure that in any contract entered into pursuant to this advertisement, disadvantaged business enterprises will be afforded full opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of race, color, sex, age, disability, religion or national origin in consideration for an award.

The specifications are on file in the offices of the Mississippi Department of Transportation.

Bid proposals must be purchased online at <https://shopmdot.ms.gov>. Specimen proposals may be viewed and downloaded online at no cost at <http://mdot.ms.gov> or purchased online. Proposals are available at a cost of Ten Dollars (\$10.00) per proposal plus a small convenience fee. Cash or checks will not be accepted as payment.

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

MELINDA L. MCGRATH  
EXECUTIVE DIRECTOR

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 - NOTICE TO BIDDERS NO. 1**

**CODE: (IS)**

**DATE: 03/01/2017**

**SUBJECT: Governing Specifications**

The current (2017) Edition of the Standard Specifications for Road and Bridge Construction adopted by the Mississippi Transportation Commission is made a part hereof fully and completely as if it were attached hereto, except where superseded by special provisions, or amended by revisions of the Specifications contained within this proposal. Copies of the specification book may be purchased from the MDOT Construction Division, or online at [shopmdot/default.aspx?StoreIndex=1](http://shopmdot/default.aspx?StoreIndex=1).

A reference in any contract document to controlling requirements in another portion of the contract documents shall be understood to apply equally to any revision or amendment thereof included in the contract.

In the event the plans or proposal contain references to the 2004 Edition of the Standard Specifications for Road and Bridge Construction, it is to be understood that such references shall mean the comparable provisions of the 2017 Edition of the Standard Specifications.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 3**

**CODE: (SP)**

**DATE: 01/17/2017**

**SUBJECT: Final Clean-Up**

Immediately prior to final inspection for release of maintenance, the Contractor shall pick up, load, transport and properly dispose of all litter from the entire highway right-of-way that is within the termini of the project.

Litter shall include, but not be limited to, solid wastes such a glass, paper products, tires, wood products, metal, synthetic materials and other miscellaneous debris.

Litter removal is considered incidental to other items of work and will not be measured for separate payment.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 9**

**CODE: (IS)**

**DATE: 03/01/2017**

**SUBJECT: Federal Bridge Formula**

Bidders are hereby advised that the latest revision of Federal Highway Administration Publication No. FHWA-HOP-06-105, **BRIDGE FORMULA WEIGHTS**, dated August 2006, is made a part of this contract when applicable.

Prior to the preconstruction conference, the Contractor shall advise the Engineer, in writing, what materials, if any, will be delivered to the jobsite via Interstate route(s).

Copies of the **BRIDGE FORMULA WEIGHTS** publication may be obtained by contacting:

Federal Highway Administration  
400 7<sup>th</sup> Street, SW  
Washington, DC 20590  
(202) 366-2212

or

[http://www.ops.fhwa.dot.gov/Freight/publications/brdg\\_frm\\_wgths/bridge\\_formula\\_all\\_rev.pdf](http://www.ops.fhwa.dot.gov/Freight/publications/brdg_frm_wgths/bridge_formula_all_rev.pdf)

An on line **BRIDGE FORMULA WEIGHTS CALCULATOR** is available at

[http://ops.fhwa.dot.gov/freight/sw/brdgcalc/calc\\_page.htm](http://ops.fhwa.dot.gov/freight/sw/brdgcalc/calc_page.htm)

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 12**

**CODE: (IS)**

**DATE: 03/01/2017**

**SUBJECT: MASH Compliant Devices**

Bidders are hereby advised that the Standard Specifications may require certain traffic control and permanent safety hardware devices to meet the requirements of the Manual for Assessing Safety Hardware (MASH). However, devices meeting the requirements of NCHRP Report 350 will be allowed until the mandatory effective date for MASH compliance. The following table shows the effective dates for MASH compliant devices.

<b>Device</b>	<b>Effective Date for MASH Compliance</b>
W-beam barriers, cast-in-place concrete barriers	December 31, 2017
W-beam terminals	June 30, 2018
Cable barriers, cable barrier terminals, crash cushions	December 31, 2018
Bridge rails, transitions, all other longitudinal barriers including portable barriers installed permanently, all other terminals, sign supports, all other breakaway hardware	December 31, 2019

Temporary work zone devices, including portable barriers manufactured after December 31, 2019, must have been successfully tested to the 2016 Edition of MASH. Such devices manufactured on or before this date and successfully tested to NCHRP Report 350 or the 2009 Edition of MASH may continue to be used throughout their normal service lives.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 13**

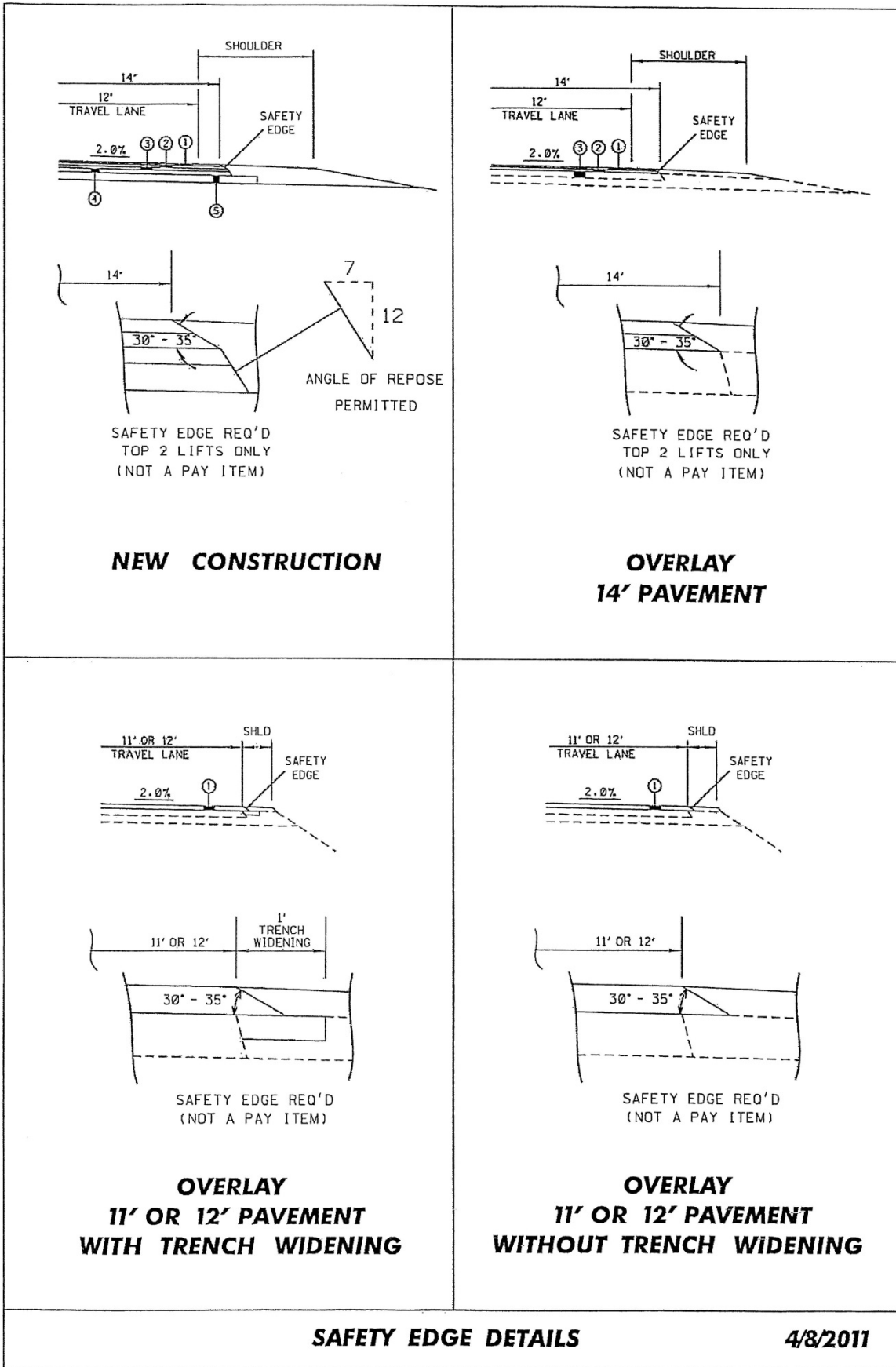
**CODE: (IS)**

**DATE: 03/01/2017**

**SUBJECT: Safety Edge**

Bidders are hereby advised that the Shoulder Wedge (Safety Edge) specified in Section 401, Asphalt Pavements, shall only apply to the top two (2) lifts of asphalt. Open Graded Friction Courses (OGFC) are not to be considered a lift as it pertains to safety edge. Attached is a drawing showing the safety edge. Note that the shoulder dimensions in the bottom two drawings will be less than three feet (3').





**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SUPPLEMENT TO NOTICE TO BIDDERS NO. 14**

**DATE:** 7/02/2017

**PROJECT:** MP-5000-52(093) / 305744301 & 302 - Noxubee County

After the second paragraph on page 1, add the following:

Name Insured: *KCS Railroad*

Description and Designation: *Mill and Overlay SR 39*

Mile Post: *Intersection of SR 39 and tracks, near Shuqualak, Mississippi.*

After the fourth paragraph on page 1, add the following:

Carmen Wallace  
Signal Project Engineer Kansas City Southern Railway  
4601 Hilry Huckaby III Ave  
Shreveport, Louisiana 71107  
Office: 318-676-6085  
Cell: 318-519-5083  
Fax: 318-676-6273  
[CWallace@KCSouthern.com](mailto:CWallace@KCSouthern.com)

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 14

CODE: (SP)

DATE: 01/17/2017

SUBJECT: Railway-Highway Provisions

Prior to bidding, the Contractor shall contact the Railroad concerning insurance coverage required for this project. In case the railroad requires coverage over and above that required by the Standard Specifications, the railroad requirements shall be met.

The name insured, description of the work and designation of the job site to be shown on the Policy are as follows:

Notice of starting to work, completion of any required forms, and correspondence pertaining to railroad liability insurance shall be directed to the person below.

The Contractor shall not commence, or carry on, any work for installation, maintenance, repair, changing or renewal of any FACILITY, under, over or on RAILROAD property at any location without giving at least ten (10) working days prior notice to the RAILROAD authorized representative at the RAILROAD's office(s) below.

If in the opinion of the RAILROAD, the presence of an authorized representative of the RAILROAD is required to supervise the same, the RAILROAD shall render bills to the Contractor for all expenses incurred by it for such supervision. This includes all labor costs for flagmen or cable locate supplied by the RAILROAD to protect RAILROAD operation, and for the full cost of furnishing, installation and later removal of any temporary supports for said tracks, as the RAILROAD's Chief Engineer's Office may deem necessary.

**It will be the Contractor's responsibility to pay all bills associated with railroad flagging and cable locating.** Generally, the flagging rate is \$700.00 per day (1 to 8 hours) plus overtime at \$125.00 per hour, however, the Contractor shall contact the RAILROAD to verify all rates.

A flagman is required anytime a Contractor does any work on or near RAILROAD property within twenty-five (25) feet horizontally of the centerline or any work over any railroad track. The RAILROAD, however, also reserves the right to require a flagman for work on RAILROAD property, which is more than twenty-five (25) feet from the centerline of a railroad track when there are other conditions or considerations that would dictate the need for a flagman to safeguard the RAILROAD's operations, property and safety of working personnel.

A cable locate of RAILROAD owned facilities may be required to identify and protect Signal & Communication cables that have been installed to provide power, signal control, wayside communications. These cables are vital to a safe and reliable railway operation. The cable locate will be performed by a qualified RAILROAD employee.

Outside Contractors are prohibited from driving on, along, or across any track that does not have a RAILROAD installed crossing. They may utilize an existing public crossing. The practice of allowing rubber tired equipment to operate over track with no crossing has been banned.

Exceptions to this rule will require the express approval from the RAILROAD Engineers.

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 - NOTICE TO BIDDERS NO. 18**

**CODE: (SP)**

**DATE: 01/17/2017**

**SUBJECT: Kansas City Southern Construction Requirements**

Bidders are hereby advised that provisions which are required as per the Notice to Bidders entitled "Railway-Highway Provisions" shall also include the following.

The Contractor shall submit to the Project Engineer and the Railroad detailed plans and design data for temporary construction clearances, stages of construction, erection plans, demolition plans, false-work plans, excavation plans, and temporary shoring plans and calculations, as required, and shall be sealed by a Mississippi Registered Professional Engineer. All submittals must be approved by the Railroad before excavation or construction can begin within Railroad Right-of-Way. All construction submittals for work performed within the Kansas City Southern Railway Company (KCS) right-of-way shall be made per the KCS "Guidelines for the Design and Construction of Railroad Overpasses and Underpasses" as updated in May 2008.

Prior to beginning any work on the KCS right-of-way, the Contractor shall obtain a Right of Entry Permit. To request a permit application, the Contractor should contact Sylvia Schmidt. Mrs. Schmidt's contact information is as follows.

Sylvia Schmidt  
Permit Manager  
Jones Lang LaSalle Americas, Inc.  
3017 Lou Menk Drive, Suite 100  
Fort Worth, Texas 76131-2800  
817-230-2688

The Contractor shall be responsible for payment of all application fees.

This project will require construction activities on the right-of-way of active railroad tracks which are currently owned and/or operated by KCS. When work requires that equipment or personnel be within the KCS right-of-way or the "foul zone" adjacent to the right-of-way, a qualified "Employee-in-Charge" (EIC) must be present for the purpose of providing on-track safety and flagging protection for the work crews. The EIC shall also be responsible for the coordination of the Contractor's activities within the KCS right-of-way with the operation of the Railroad. The EIC must be certified under the KCS General Code of Operation Rules (GCOR) and must be approved by the local KCS Roadmaster prior to beginning work on the KCS right-of-way. The Contractor will be required to provide radios for the EIC, all equipment operators, supervisors, and foremen in charge of employees working within the KCS right-of-way. All personnel who must enter upon the KCS right-of-way must check in and out with the EIC and be logged in and out of the site.

All personnel who must work within the KCS right-of-way at any time shall be trained and certified as a KCS "Roadway Worker" and must at all times have their certification card with them and available for random inspection. The Contractor will be responsible for providing this training for Contractor employees or any subcontractor(s) employees. The Contractor shall contact Mr. Larry Slater of Track Sense Inc. at 330-847-8661 or 330-219-4721 ([lslater@neo.rr.com](mailto:lslater@neo.rr.com)) for approximate fees and scheduling the necessary training sessions. The Contractor shall also contact the MDOT Project Engineer to see if any MDOT employees need this training. If so, the Contractor shall include the MDOT employees in the list of participants for training. The Contractor shall bear the cost of training the MDOT employees. Costs for training the MDOT employees will be reimbursed to the Contractor by supplemental agreement.

Prior to commencing work, the Contractor shall provide to the Railroad Engineer, or the Railroad Engineer's designated representative, a detailed construction schedule for its work on Railroad's right-of-way, including the proposed temporary horizontal and vertical clearances and construction sequence for all work to be performed on Railroad right-of-way. This schedule shall also include the anticipated dates when the milestone events listed below will occur. The Contractor shall update the schedule for these milestone events as necessary, but at least monthly, and shall provide a copy of all updates to the Railroad so that site visits may be scheduled.

- Preconstruction meetings.
- Excavations, shoring placement/removal, pile driving, drilling of caissons or drilled shafts adjacent to tracks.
- Reinforcement and concrete placement for near track piers.
- Erection of precast concrete or steel overpass bridge superstructure.
- Reinforcement and concrete placement of overpass bridge decks.
- Completion of the bridge structure.

The Contractor shall so arrange and conduct construction operations in such a manner that there will be no interference with Railroad operations, including train, signal, telephone and telegraphic services, or damage to the property of the Railroad or to poles, cables or wires (whether overhead or underground) and other facilities or tenants on the rights-of-way of the Railroad. Before undertaking any work within Railroad right-of-way and before placing any obstruction over any track, the Contractor shall:

- Notify the Railroad's representative at least 72 hours in advance of the work.
- Provide assurance to the Railroad's representative that arrangements have been made for any required flagging service.
- Receive permission from the Railroad Engineer to proceed with the work.
- Ascertain that the Project Engineer has received copies of notice to the Railroad and the Railroad's response.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 113**

**CODE: (SP)**

**DATE: 04/18/2017**

**SUBJECT: Tack Coat**

Bidders are advised that in addition to the products listed on the Department's APL as referenced in Subsection 401.03.1.2 on page 256, the Contractor may use one of the following as a tack coat.

- CSS-1
- CSS-1h
- SS-1
- SS-1h

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 – NOTICE TO BIDDERS NO. 115**

**CODE: (SP)**

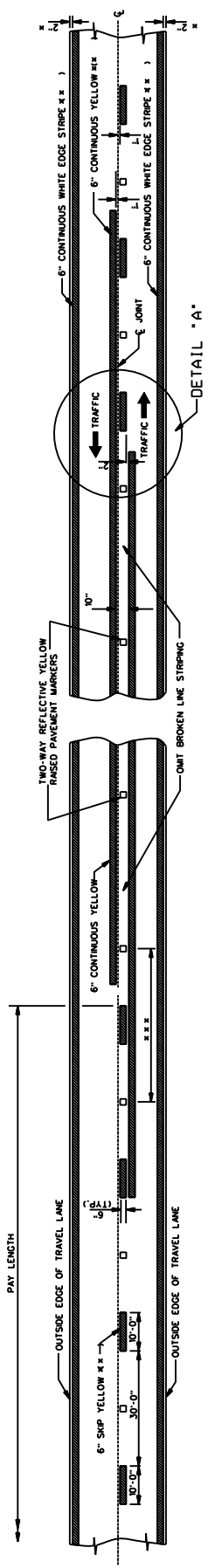
**DATE: 04/19/2017**

**SUBJECT: Standard Drawings**

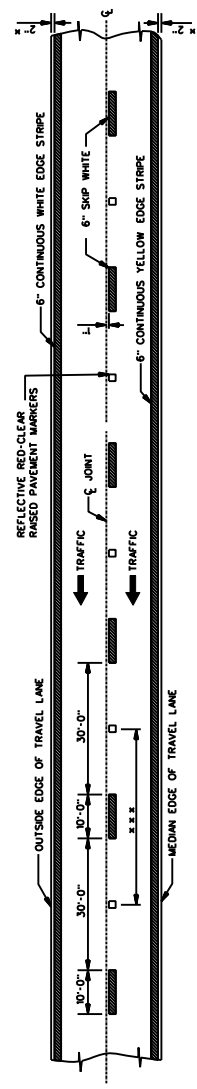
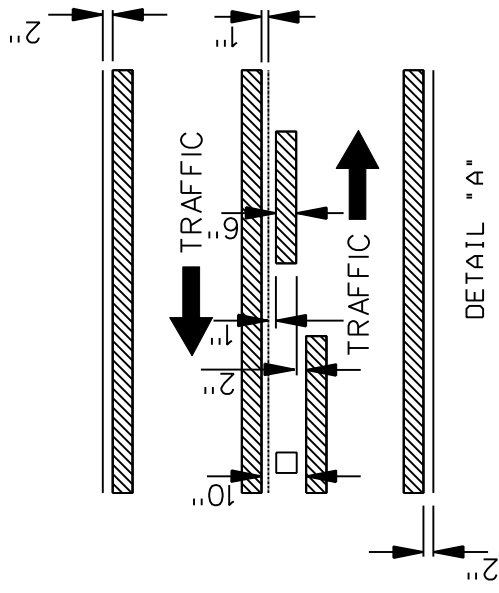
Standard Drawings attached hereto shall govern appropriate items of required work.

Larger copies of Standard Drawings may be purchased from:

MDOT Plans Print Shop  
MDOT Shop Complex, Building C, Room 114  
2567 North West Street  
P.O. Box 1850  
Jackson, MS 39215-1850  
Telephone: (601) 359-7460  
or FAX: (601) 359-7461  
or e-mail: [plans@mdot.state.ms.us](mailto:plans@mdot.state.ms.us)



TWO-WAY TRAFFIC  
(ASPHALT OR CONCRETE PAVEMENT)



4-LANE WITH ONE-WAY TRAFFIC



GENERAL NOTES:

- \* 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- \*\* 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
- \*\*\* 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	URBAN AREA (U/Lin)	RURAL AREA (U/Lin)
HORIZONTAL CURVES	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	40'-0"
	40'-0"	140'-0"

- 1. NOTES: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP AND ENDING 1000' FROM THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- 4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED SOURCES OF MATERIALS."

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

PAVEMENT MARKING  
DETAILS FOR  
2-LANE AND 4-LANE  
DIVIDED HIGHWAYS

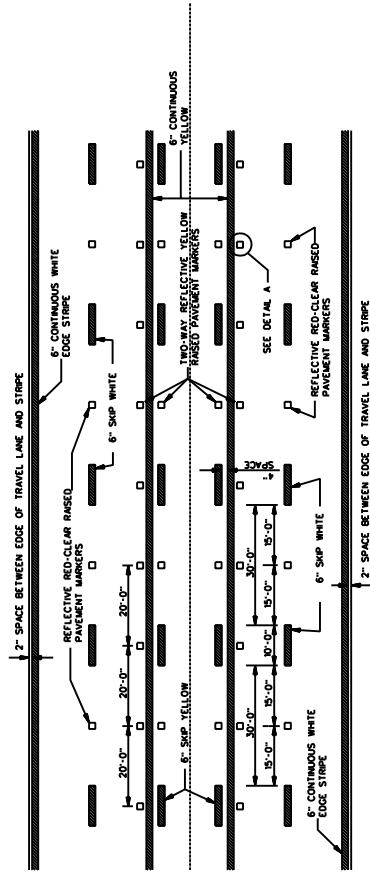
ISSUE DATE: OCTOBER 1, 1998

WORK NUMBER: PM-1

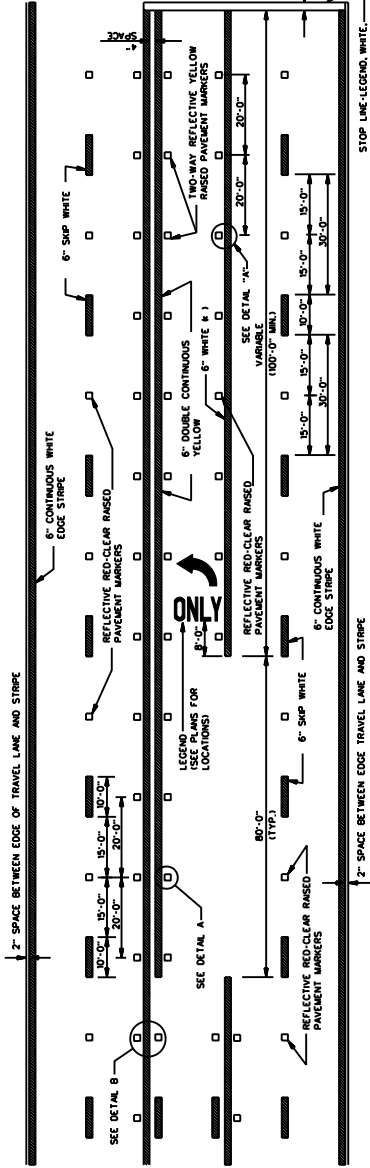
SHEET NUMBER: 100



STATE	PROJECT NO.
MISS.	



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 5-LANE SECTION



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION

TYPICAL STRIPING AND RAISED PAVEMENT MARKERS AT LEFT TURN LANES

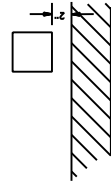
\*NOTE: USE DETAIL STRIPING IF LENGTH  $\leq$  150' AT THIS LOCATION, OTHERWISE USE CONTINUOUS STRIPING.



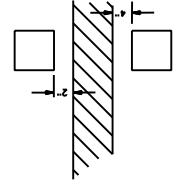
TYPICAL TWO-WAY ARROW INSTALLATION

NOTES: 1. CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY.  
 2. IF SEGMENT IS LESS THAN 300', PLACE ONE SET OF ARROWS IN CENTER OF SEGMENT.  
 3. IF SEGMENT IS GREATER THAN 300', PLACE FIRST SET OF ARROWS 50' TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS 1250' O.C.

GENERAL NOTE:  
 1. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT-APPROVED SOURCES OF MATERIALS.\*



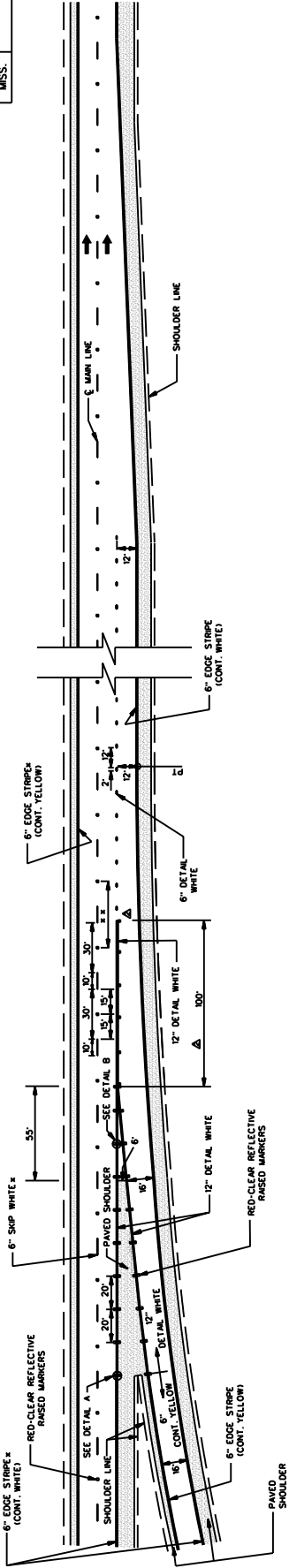
DETAIL A  
 LATERAL PLACEMENT OF PAVEMENT MARKERS



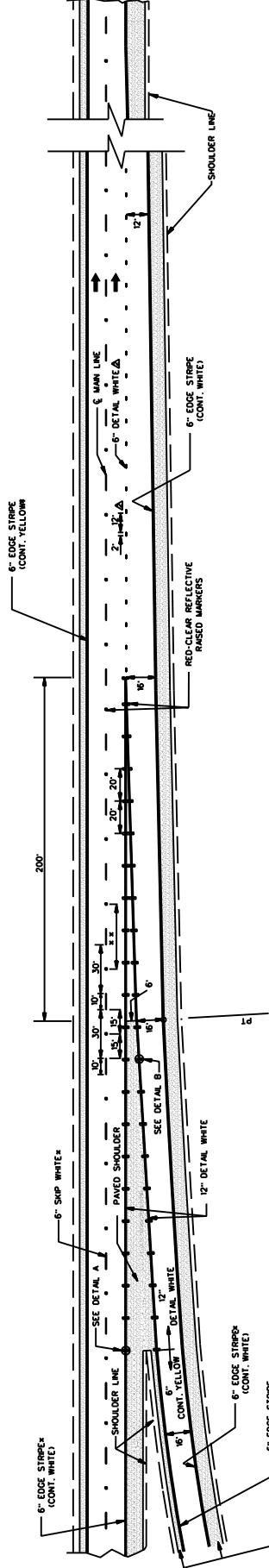
DETAIL B  
 LATERAL PLACEMENT OF PAVEMENT MARKERS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
PAVEMENT MARKING	
DETAILS FOR	
4-LANE AND 5-LANE	
UNDIVIDED ROADWAYS	
DATE	03/15/16
FILE NAME	SDPM-2.DGN
DESIGNER	CH2M
CHECKER	
DATE	03/15/16

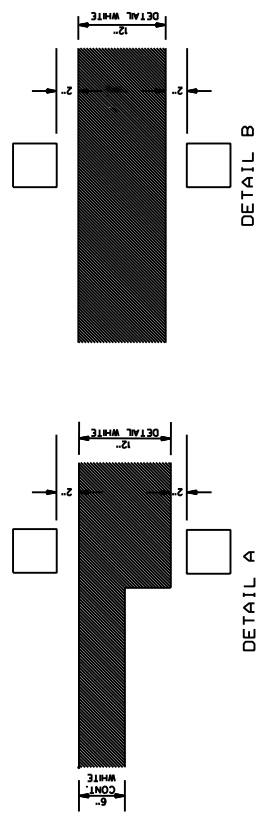
STATE	PROJECT NO.
MISS.	



PARALLEL ENTRANCE RAMP



TAPER ENTRANCE RAMP



- GENERAL NOTES:
- \*\* 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPES AND THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELED WAY.
  - \*\* 2. ON THE MAIN FACILITY, PLACE REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS AT A 40' SPACING ON ALL LANE-LINES THROUGHOUT THE INTERCHANGE AREA BEGINNING 300' IN THE MAIN FACILITY FROM THE END OF THE INTERCHANGE TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
  - 3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED SOURCES OF MATERIALS."

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION	
PAVEMENT MARKING DETAILS FOR INTERCHANGE RAMP (PARALLEL AND TAPER)	
ISSUE DATE:	OCTOBER 1, 1998
WORKING NUMBER	SDPM-3
SHEET NUMBER	





STATE PROJECT NO.					
MISS.					

THRU ARROW

TURN ARROW

COMBINATION ARROW

1-WAY ARROW

**GENERAL NOTES:**

1. UNLESS OTHERWISE SHOWN ON THE PLANS, ALL PAVEMENT MARKING SHALL BE APPLIED USING HIGH PERFORMANCE MATERIALS.
2. TWO HORIZONTAL GAPS CAUSED BY TEMPLATE CONNECTORS OF 1/2" OR LESS AND EXTENDING THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
3. FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM PAVEMENT MARKING LEGENDS.
4. PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

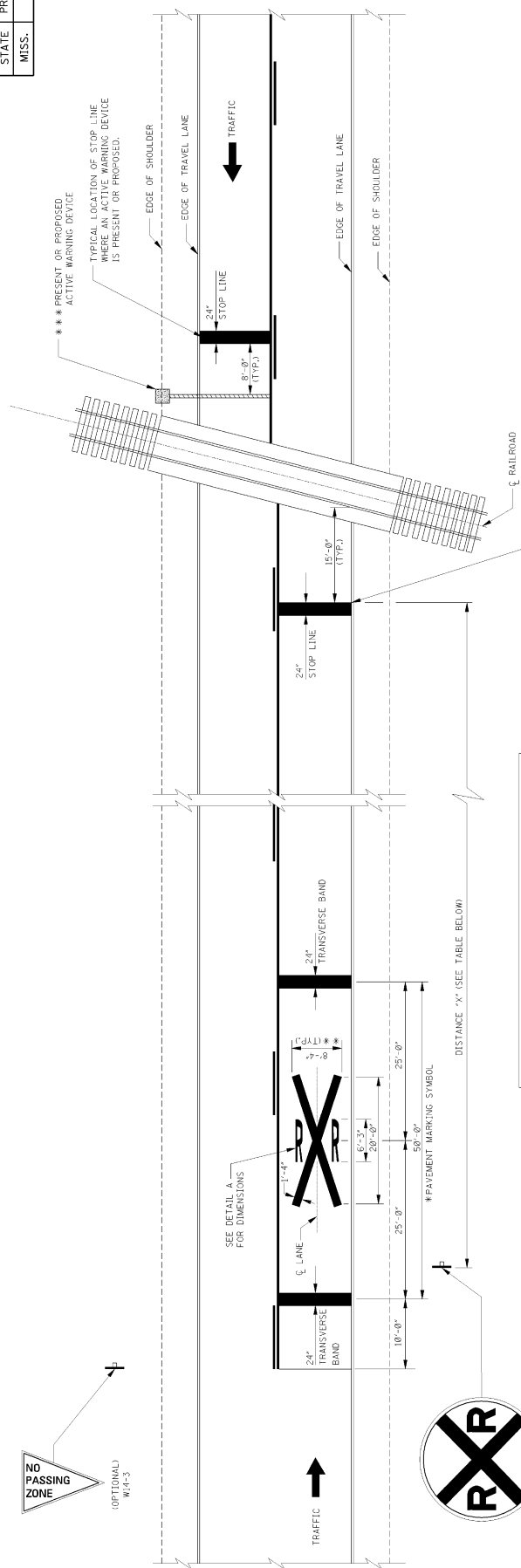
PAY QUANTITIES	
LEGEND/SYMBOL	AREA (ft <sup>2</sup> )
ONLY	22.0
TURN ARROW	18.4
THRU ARROW	21.2
COMBINATION ARROW	27.5
1-WAY ARROW	24.3

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

**PAVEMENT MARKING  
LEGEND DETAILS**

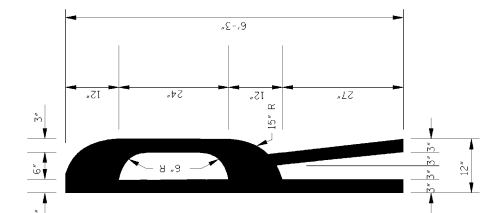
DATE	BY	REVISION	ISSUE DATE:	OCTOBER 1, 1998	SHEET NUMBER	125



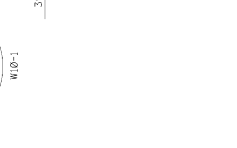
ADVANCE WARNING SIGN PLACEMENT DISTANCE

POSTED SPEED (mph)	DISTANCE "X" (FT)	
	RURAL	URBAN
20	175	100
25	250	100
30	325	100
35	400	150
40	475	225
45	550	300
50	625	375
55	700	450
60	775	550

NOTES:  
 ① DISTANCE "X" MAY BE ADJUSTED, IF PROHIBITIVE PHYSICAL CONDITIONS EXIST AT THE DESIGNATED DISTANCE.  
 ② THESE DISTANCES MAY BE ADJUSTED TO A MINIMUM OF 100 IN. FOR BUSINESS OR BUSINESS DISTRICTS WHERE LOW SPEEDS ARE PREVALENT.



DETAIL A  
STANDARD "R" PAVEMENT MARKING  
FOR R X R SYMBOL



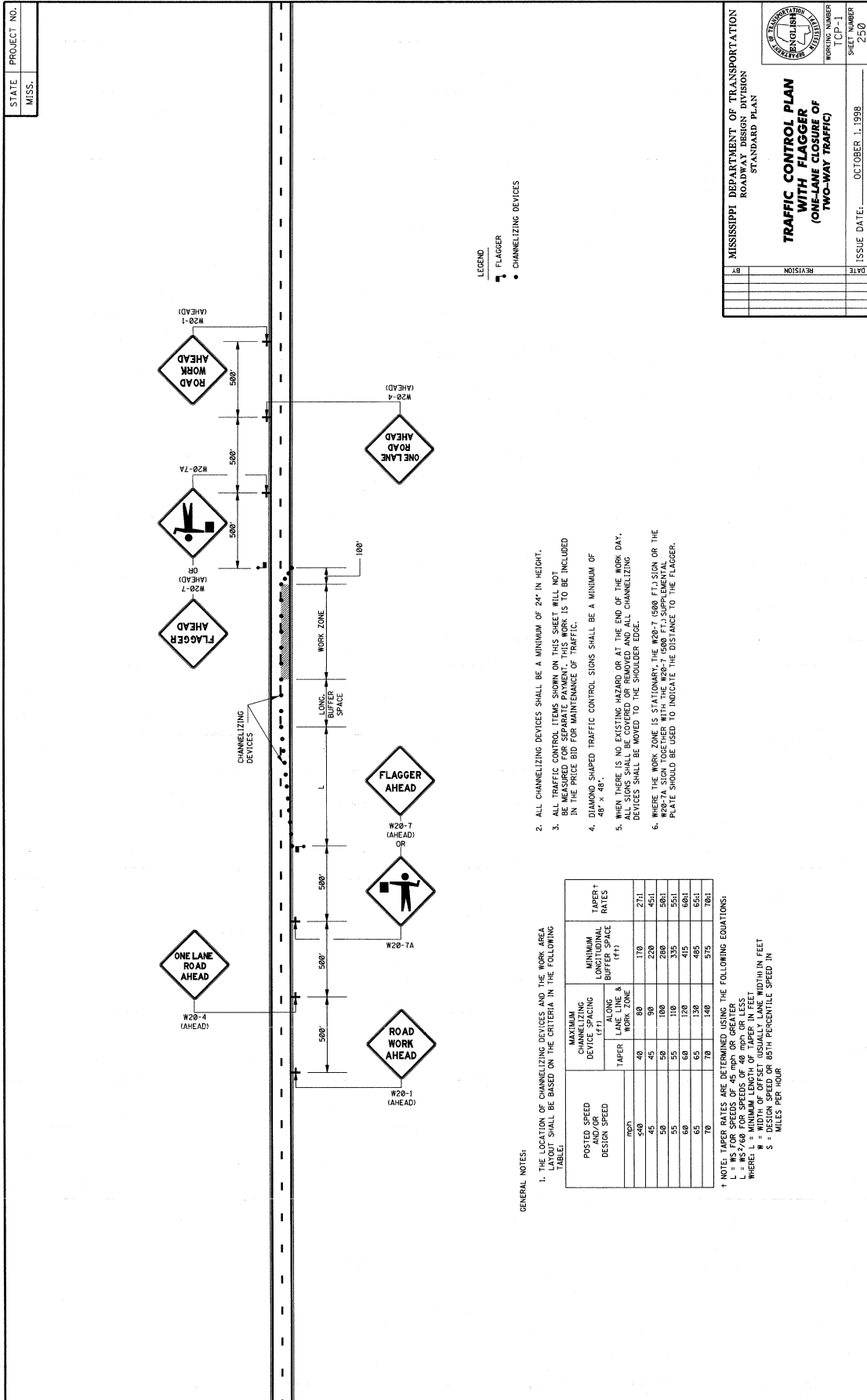
GENERAL NOTES:  
 \* 1. A PORTION OF THE PAVEMENT MARKING SYMBOL SHOULD BE DIRECTLY OPPOSITE THE ADVANCE WARNING SIGN (W10-1).  
 \* 2. WIDTH OF R X R SYMBOL MAY VARY ACCORDING TO LANE WIDTH. HOWEVER, ON MULTI-LANE ROADS, THE TRANSVERSE BANDS AND STOP LINE SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.  
 \* 3. R X R SYMBOL (63.0 #42), TRANSVERSE BANDS AND STOP LINE SHALL BE PAID FOR AS LEGEND, WHITE (PLASTIC), MATERIAL OPTIONAL FOR OTHER AGENCIES.  
 \* 4. REFER TO THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" FOR LOCATION OF PROPOSED WARNING DEVICES AT RAILROAD-HIGHWAY GRADE CROSSINGS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

**TYPICAL PLACEMENT OF WARNING SIGNS AND PAVEMENT MARKINGS AT RAILROAD HIGHWAY GRADE CROSSINGS**

WORKSHEET NO. 15  
SHEET NUMBER 12 OF 18  
ISSUE DATE: OCTOBER 11, 1998

DATE	REVISION
10-18-98	REVISE NOTE



GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:
2. ALL CHANNELIZING DEVICES SHALL BE A MINIMUM OF 24" IN HEIGHT.
3. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
4. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".
5. WHEN THERE IS NO EXISTING HAZARD OR AT THE END OF THE WORK DAY, DEVICES SHALL BE COVERED OR REMOVED AND ALL CHANNELIZING DEVICES SHALL BE MOVED TO THE SHOULDER EDGE.
6. WHERE THE WORK ZONE IS STATIONARY THE W20-7 (600 FT.) SIGN OR THE W20-7A SIGN TOGETHER WITH THE W20-7 (600 FT.) SUPPLEMENTAL PLATE SHOULD BE USED TO INDICATE THE DISTANCE TO THE FLAGGER.

POSTED SPEED AND/OR DESIGN SPEED (MPH)	MAXIMUM CHANNELIZING DEVICES ALONG WORK ZONE (FT.)		MINIMUM LONGITUDINAL BUFFER SPACE (FT.)	TAPER + BUFFER RATES
	ALONG WORK ZONE	ALONG BUFFER SPACE		
40	40	80	170	271
45	45	90	220	451
50	50	100	280	501
55	55	110	335	551
60	60	120	415	661
65	65	130	485	651
70	70	140	575	761

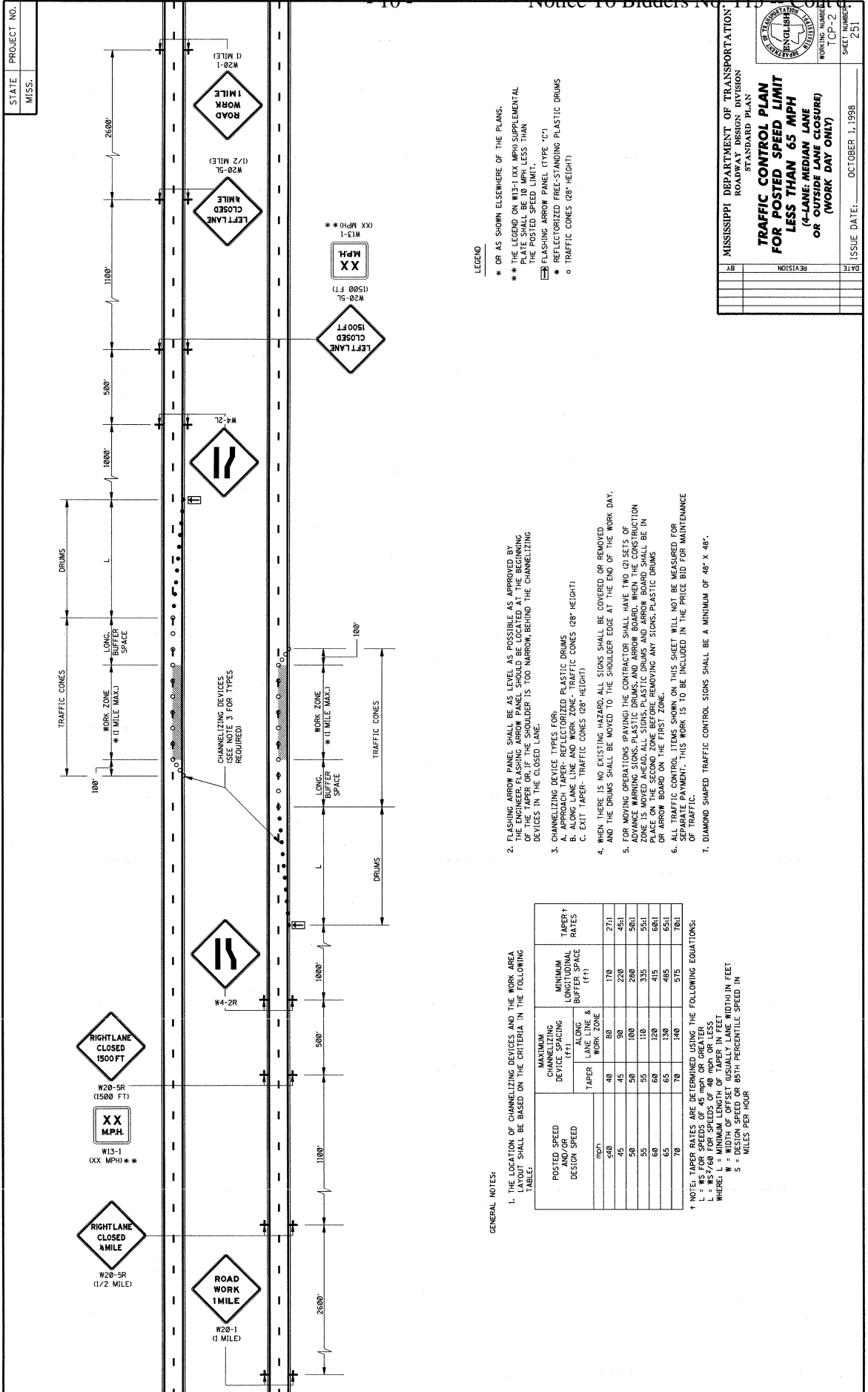
† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:  
 $L = WS$  FOR SPEEDS OF 45 MPH OR GREATER  
 $L = WS^2$  FOR SPEEDS OF 40 MPH OR LESS  
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET  
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET  
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
 ROADWAY DESIGN DIVISION  
 STANDARD PLAN

**TRAFFIC CONTROL PLAN  
 WITH FLAGGER  
 (ONE-LANE CLOSURE OF  
 TWO-WAY TRAFFIC)**

WORKING NUMBER: TCP-1  
 SHEET NUMBER: 250  
 ISSUE DATE: OCTOBER 1, 1998

DATE	REVISION



STATE PROJECT NO.  
MISS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
ROADWAY DESIGN DIVISION  
STANDARD PLAN

**TRAFFIC CONTROL PLAN  
FOR POSTED SPEED LIMIT  
LESS THAN 65 MPH  
(4-LANE MEDIAN LANE  
OR CLOSURE MEASURE)  
(WORK DAY ONLY)**

WORKING NUMBER: TCP-2  
SHEET NUMBER: 251  
ISSUE DATE: OCTOBER 1, 1998

- LEGEND**
- \* OR AS SHOWN ELSEWHERE OF THE PLANS.
  - \*\* THE LEGEND ON W13-1 (XX MPH) SUPPLEMENTAL PLATE SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT.
  - REFLECTORIZED PRE-STANDING PLASTIC DRUMS
  - TRAFFIC CONES (28" HEIGHT)

2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICE TYPES FOR:
  - A. APPROACH TAPER- REFLECTORIZED PLASTIC DRUMS
  - B. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT)
  - C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT)
4. WHEN THERE IS NO EXISTING HAZARD, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE PREVIOUS ZONE.
6. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
7. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".

**GENERAL NOTES:**

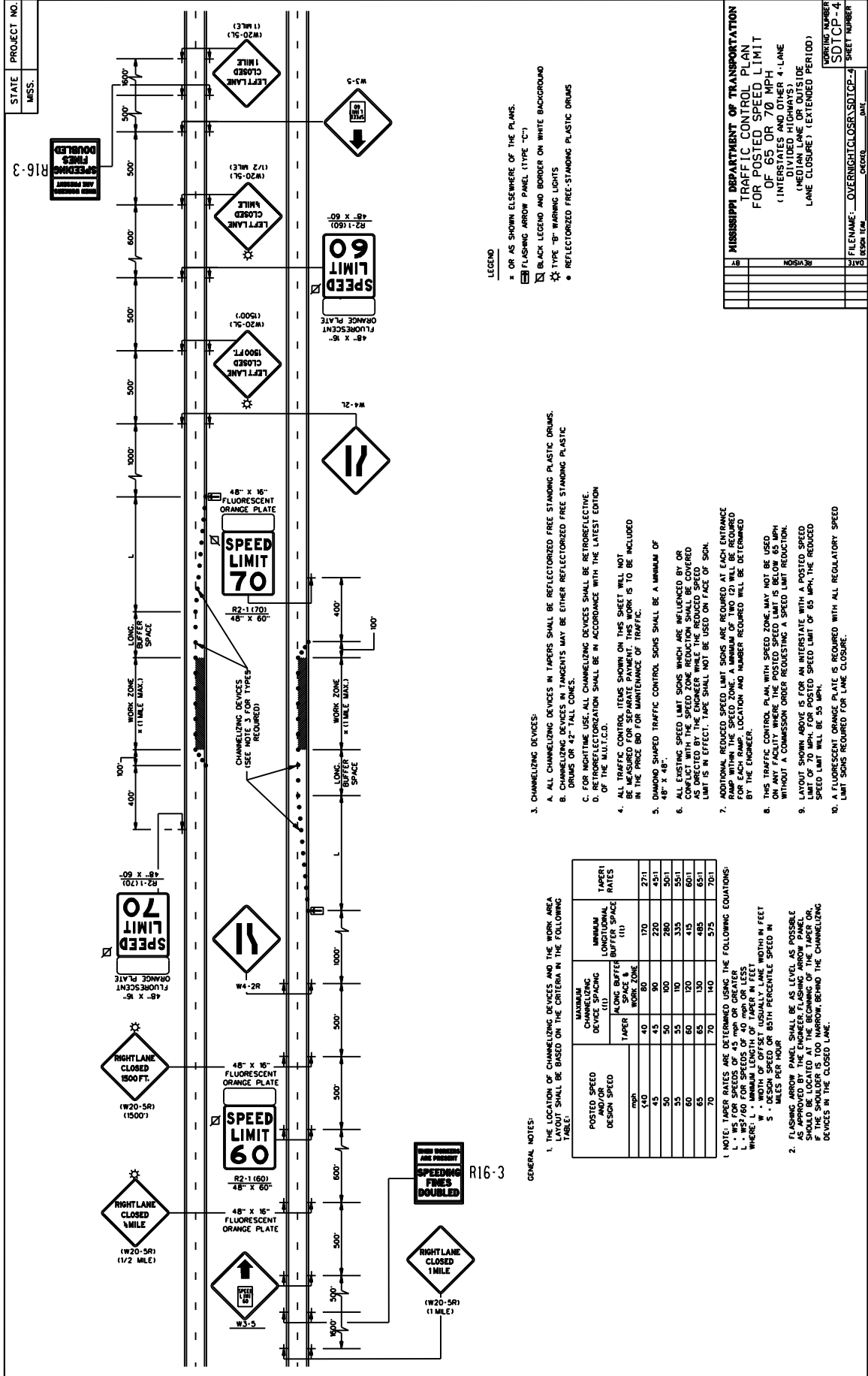
1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

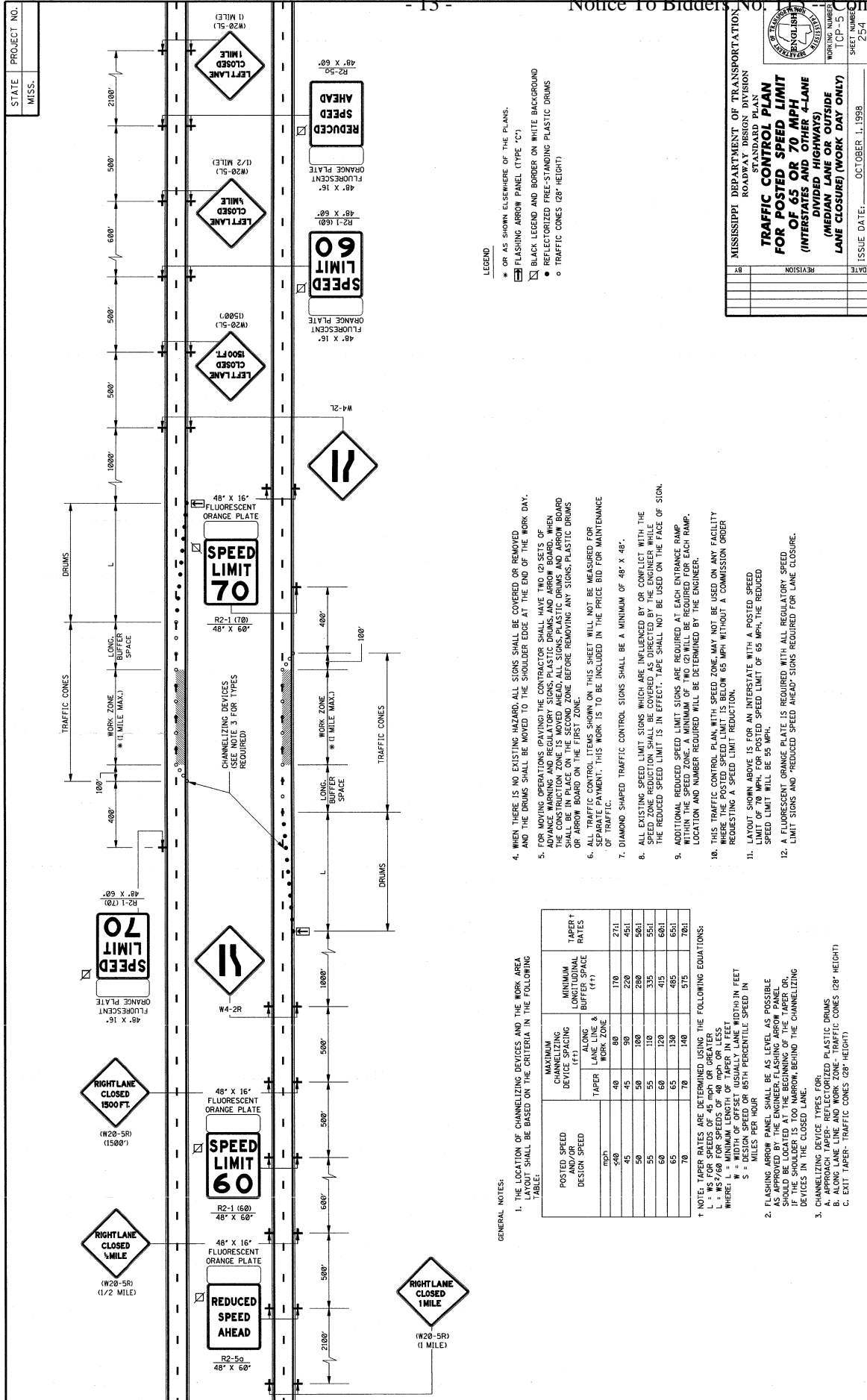
POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (FT)		MINIMUM LONGITUDINAL BUFFER SPACE (ft)	TAPER + TAPER RATES
	ALONG LANE LINE	WORK ZONE		
40	40	80	170	27:1
45	45	90	220	45:1
50	50	100	280	50:1
55	55	110	335	55:1
60	60	120	415	60:1
65	65	130	485	65:1
70	70	140	575	70:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:  
 $L = W^2$  FOR SPEEDS OF 45 MPH OR GREATER  
 $L = WS$  FOR SPEEDS OF 45 MPH OR GREATER  
 WHERE L = MINIMUM LENGTH OF TAPER IN FEET  
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET  
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR









**GENERAL NOTES:**

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:
2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE. APPROACH TAPER REFLECTORIZED PLASTIC DRUMS SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICE TYPES FOR:
  - A. APPROACH TAPER- REFLECTORIZED PLASTIC DRUMS
  - B. EXIT TAPER- REFLECTORIZED PLASTIC CONES (28" HEIGHT)
  - C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT)
4. WHEN THERE IS NO EXISTING HAZARD, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS PAYING THE CONTRACTOR SHALL HAVE TWO (2) SETS OF CHANNELIZING DEVICES LOCATED AT THE BEGINNING AND END OF THE WORK ZONE. THE CONSTRUCTION ZONE IS MOVED AHEAD ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD OR ARROW BOARD ON THE FIRST ZONE.
6. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
7. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".
8. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON THE FACE OF SIGN.
9. ADDITIONAL REDUCED SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. A MINIMUM OF TWO (2) WILL BE REQUIRED FOR EACH RAMP. LOCATION AND NUMBER REQUIRED WILL BE DETERMINED BY THE ENGINEER.
10. THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
11. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.
12. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS AND "REDUCED SPEED AHEAD" SIGNS REQUIRED FOR LANE CLOSURE.

POSTED SPEED DESIGN SPEED mph	CHANNELIZING DEVICE SPACING (FT)		MINIMUM LONGITUDINAL BUFFER SPACE (FT)	TAPER RATES
	LANE LINE & WORK ZONE	LANE LINE & WORK ZONE		
50	40	80	170	27:1
55	45	90	220	45:1
60	50	100	280	50:1
65	55	110	335	55:1
70	60	120	415	60:1
75	65	130	485	65:1
80	70	140	575	70:1

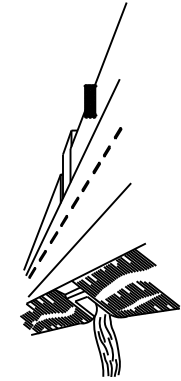
T NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:  
 L = WS FOR SPEEDS OF 45 mph OR GREATER  
 L = WS/60 FOR SPEEDS OF 40 mph OR LESS  
 WHERE:  
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET  
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

**LEGEND**

- \* OR AS SHOWN ELSEWHERE OF THE PLANS.
- ◻ FLASHING ARROW PANEL (TYPE "C")
- ◻ BLACK LEGEND AND BORDER ON WHITE BACKGROUND
- REFLECTORIZED FREE-STANDING PLASTIC DRUMS
- TRAFFIC CONES (28" HEIGHT)

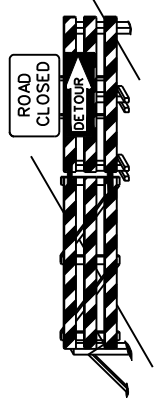
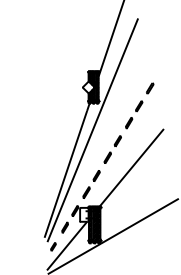
MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
 STANDARD PLAN  
**TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (WORK DAY ONLY)**

WORKING NUMBER: TCP-5  
 SHEET NUMBER: 254  
 ISSUE DATE: OCTOBER 1, 1988



**WING BARRICADES**

- WING BARRICADES ARE TYPE BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A WARNING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
- WING BARRICADES SHOULD BE USED:
  - IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
  - IN ADVANCE OF ALL BRIDGE OR CULVERT MOVING OPERATIONS.

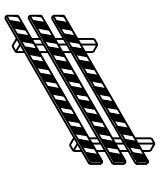


**BARRICADE CLOSING A ROAD**

**BARRICADE CHARACTERISTICS**

	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL*	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE*	6"	6"	6"
HEIGHT	35" MIN.	35" MIN.	60" MIN.
NUMBER OF REFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS
TYPE OF FRAME	LIGHT	LIGHT "X" FRAME	POST OR SHD

- \* 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- \*\* 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED FACINGS TRAFFIC.



**TYPE I**



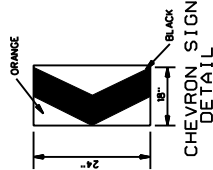
**TYPE II**



**TYPE III**

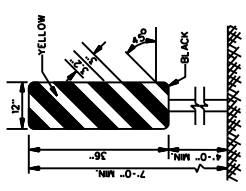
**STANDARD BARRICADES**

- A TYPE BARRICADE CONSISTS OF ONE (1) HORIZONTAL RAIL SUPPORTED BY A DEMOUNTABLE FRAME OR URBAN STREETS AND ARTERIALS.
- A TYPE BARRICADE CONSISTS OF TWO (2) HORIZONTAL RAILS ON A LIGHT "X" FRAME. TYPE BARRICADES ARE INTENDED FOR USE WHERE THE HAZARD IS LESS SEVERE THAN THAT OF TYPE I AND TYPE II BARRICADES.
- TYPE I AND TYPE BARRICADES ARE INTENDED FOR USE WHERE THE HAZARD IS LESS SEVERE THAN THAT OF TYPE I AND TYPE II BARRICADES. CONTINUOUS DELIMITING OF A RESTRICTED ROADWAY OR FOR TEMPORARY DAYTIME USE.
- A TYPE BARRICADE CONSISTS OF THREE (3) HORIZONTAL RAILS SUPPORTED BY FIXED POSTS, A ROAD SHD, A HEAVY DEMOUNTABLE FRAME OR A HEAVY WINGED "X" FRAME.
- TYPE BARRICADES ARE INTENDED FOR USE ON CONSTRUCTION AND MAINTENANCE PROJECTS AS BARRICADES AND AT ROAD CLOSURES, WHERE THEY MUST REMAIN IN PLACE FOR EXTENDED PERIODS.
- THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).
- DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
- FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.



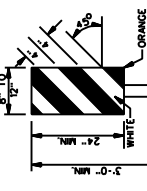
**CHEVRON SIGN  
DETAIL**

- A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
- THE CHEVRON SIGN SHALL BE MOUNTED ON FIXED POST OR ROAD SHD.
- CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHALL BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.



**TYPE 3 OBJECT MARKER  
(OM-3R)**

- TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
- THE OM-3R IS SHOWN. THE OM-3R IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
- THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



**VERTICAL PANEL**

- VERTICAL PANELS CONSIST OF AT LEAST ONE PANEL 8" TO 12" IN WIDTH AND A MINIMUM OF 24" IN HEIGHT.
- THE DIAGONAL STRIPES SHALL SLOPE DOWNWARD IN THE DIRECTION THAT TRAFFIC IS TO PASS THE PANEL. THE MINIMUM OF 35" ABOVE THE ROADWAY ON A SINGLE LIGHTMASS POST.
- VERTICAL PANELS USED ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH-SPEED ROADWAYS SHALL HAVE A MINIMUM OF 70 IN OF RETROREFLECTIVE AREA FACING TRAFFIC.
- FOR TWO-WAY TRAFFIC OPERATIONS, BACK-TO-BACK PANELS SHALL BE USED.

- GENERAL NOTES:**
- MARKINGS ON ALL DEVICES SHOWN ON THIS SHEET SHALL BE HIGH INTENSITY REFLECTIVE SHEETING.
  - THE TRAFFIC CONTROL PLAN WILL LIST THE VARIOUS TRAFFIC CONTROL DEVICES REQUIRED FOR EACH PROJECT.

**Notice To Bidder**

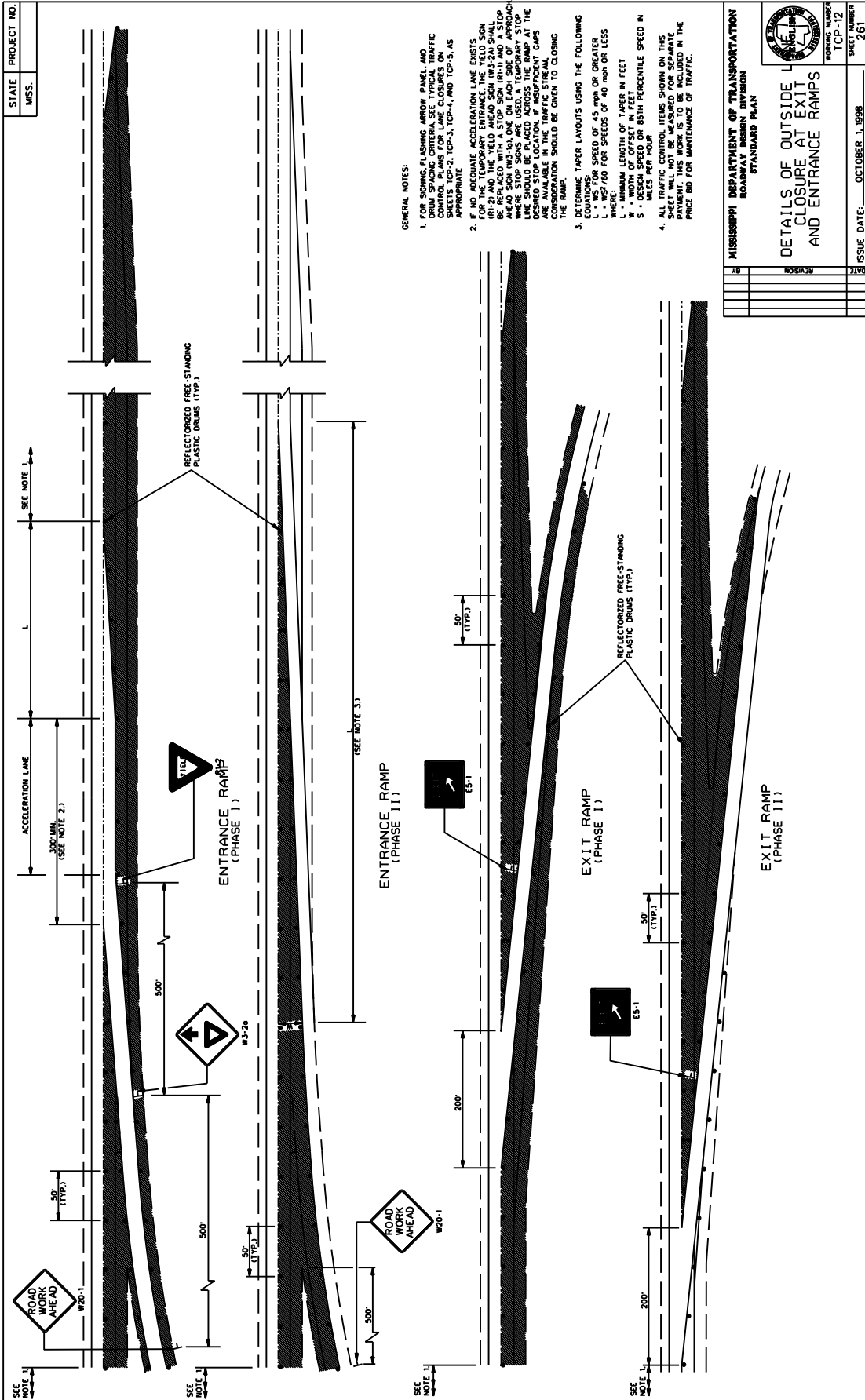
- PLASTIC DRUM STRIPING DETAIL**
- PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH MUTCD STANDARDS FOR BARRICADES. THE PREDOMINANT COLOR ON DRUMS SHALL BE ORANGE. STRIPES SHALL BE GREEN, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
  - DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
  - WHERE PRACTICAL PLASTIC DRUMS SHALL BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**HIGHWAY SIGN AND BARRICADE DETAIL FOR CONSTRUCTS**

WORKING NUMBER: SDTCP-10  
SHEET NUMBER: 10  
DATE: 06/21/2012  
FILE NAME: SDTCP-10\_06012012.DGN  
PERSON: [blank]  
CNCRCL: [blank]



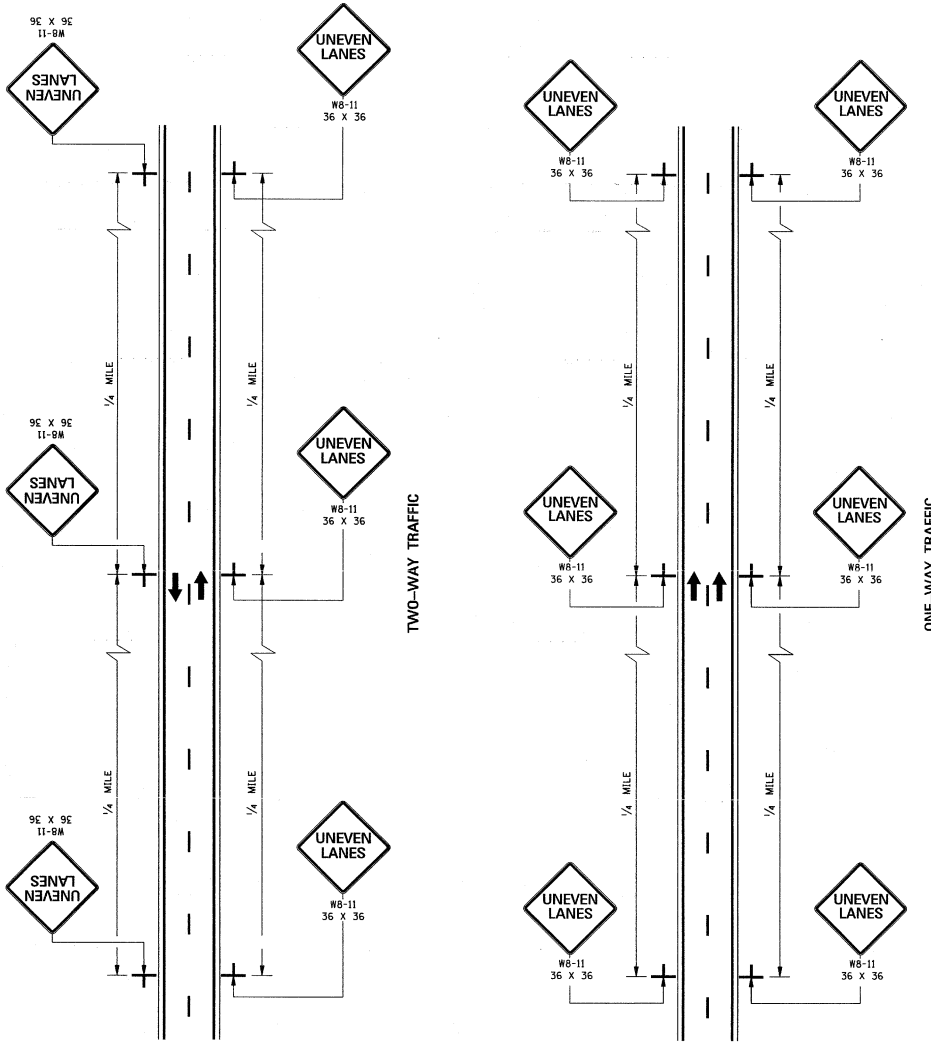


GENERAL NOTES:

- FOR SIGNING, FLASHING ARROW PANEL, AND TRAFFIC CONTROL PLANS FOR LANE CLOSURES, ON SHEETS TCP-2, TCP-3, TCP-4, AND TCP-5, AS APPROPRIATE.
- IF NO ADEQUATE ACCELERATION LANE EXISTS BEHIND THE RAMP, THE RAMP SHOULD BE REPLACED WITH A STOP SIGN (R11) AND A STOP AHEAD SIGN (W3-26) ON EACH SIDE OF APPROACH. THE STOP SIGN SHOULD BE PLACED AT THE DESIRED STOP LOCATION, IF INSUFFICIENT GAPS EXIST. THE STOP SIGN SHOULD BE PLACED AT THE DESIRED STOP LOCATION, IF INSUFFICIENT GAPS EXIST. THE STOP SIGN SHOULD BE PLACED AT THE DESIRED STOP LOCATION, IF INSUFFICIENT GAPS EXIST.
- DETERMINE TAPER LAYOUTS USING THE FOLLOWING EQUATIONS:
  - $L = 1.47 \times S \times T$  FOR SPEEDS OF 40 MPH OR LESS
  - $L = 1.47 \times S \times T^2$  FOR SPEEDS OF 40 MPH OR LESS
 WHERE:
  - L = MINIMUM LENGTH OF TAPER IN FEET
  - S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
  - T = TRAFFIC CONTROL ITEMS SHOWN ON THIS PLANET, IN FEET
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS PLANET, IN FEET, SHOULD BE USED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.


MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY MAINTENANCE DIVISION	
STANDARD PLAN	
DETAILS OF OUTSIDE LANE CLOSURE AT EXIT AND ENTRANCE RAMP	
WORKING NUMBER	TCP-12
SHEET NUMBER	261
ISSUE DATE:	OCTOBER 1, 1998

STATE	PROJECT NO.
MISS.	

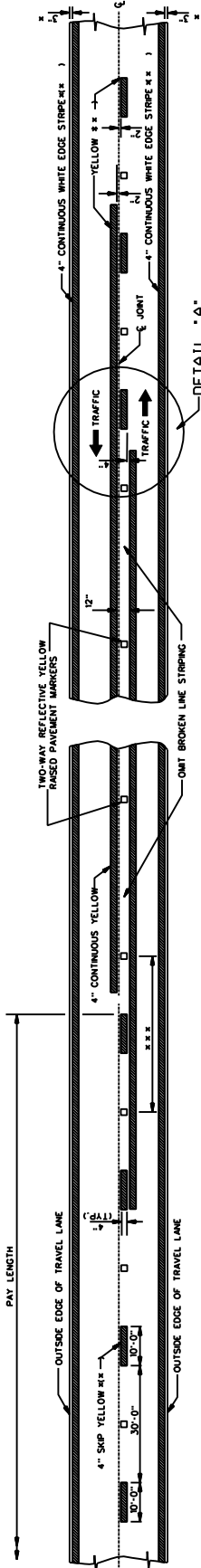


GENERAL NOTES:

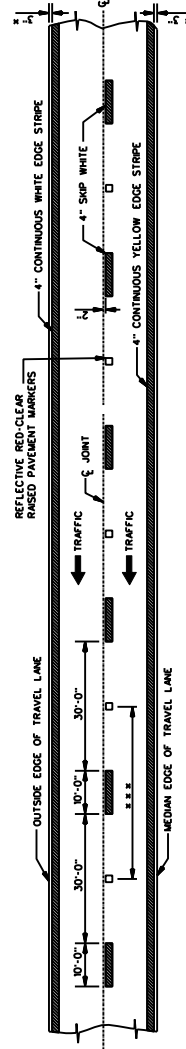
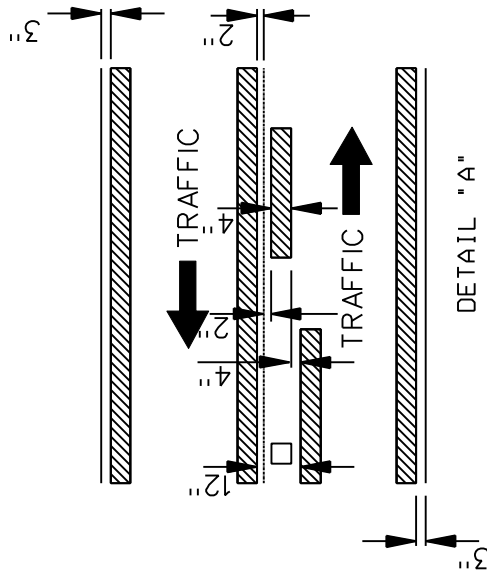
1. UNEVEN LANE LINE SPACING SHALL BE EQUAL TO 1/2" NO SIGNS REQUIRED.
  2. IF GREATER THAN 1/2" AND LESS THAN OR EQUAL TO 2/4" PLACE SIGNS AS SHOWN ON THIS SHEET.
  3. IF GREATER THAN 2/4" TRAFFIC SHOULD NOT BE ALLOWED TO CROSS UNEVEN LANE LINE.
2. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.
  3. THE W8-11 SIGNS SHALL BE SPACED AT 1/4-MILE INTERVALS THROUGHOUT UNEVEN LANE LINE LIMITS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION			WORKING NUMBER	263
ROADWAY DESIGN DIVISION			PROJECT NUMBER	115
STANDARD PLAN		<b>TRAFFIC CONTROL PLANS</b> <b>UNEVEN PAVEMENT</b> <b>DETAILS</b>		
DATE	ISSUE DATE:			
BY	REVISION			

STATE	PROJECT NO.
MISS.	



TWO-WAY TRAFFIC  
(ASPHALT OR CONCRETE PAVEMENT)



4-LANE WITH ONE-WAY TRAFFIC



GENERAL NOTES:

- \* 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- \*\* 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
- \*\*\* 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	URBAN AREA (U-L-40)	RURAL AREA (U-L-40)
LANE LENGTH	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	140'-0"

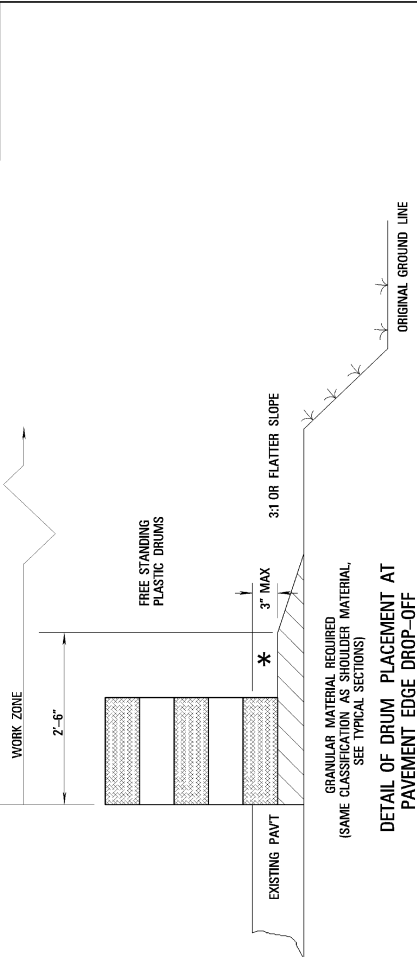
- 1. NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS SHALL BE USED THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- 2. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE MATERIALS AS SPECIFIED IN THE MOOT.
- 3. APPROVED SOURCES OF MATERIALS.

- 5. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKERS ARE TO REMAIN IN PLACE OVER 3 MONTHS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS	
WORKING NUMBER	TCP-15
SHEET NUMBER	264
ISSUE DATE:	DECEMBER 1, 1999







**NOTES**

- \* A. PAVEMENT EDGE DROP-OFF
  1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-3) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750 ± 0.0).
  2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. COMES MAY BE USED IN PLACE OF DRUMS, PANELS AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE M.U.T.C.D. (1/3 L, WHERE L IS THE TAPER LENGTH IN FEET)
  3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 3:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS/PANELS OR BARRICADES MAY BE USED. IF CONCRETE BARRIERS ARE USED, SPECIAL REFLECTIVE DEVICES OR STEADY BURN LIGHTS SHOULD BE USED FOR OVERNIGHT INSTALLATIONS.
  4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
  5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

**B. DRUM SPACING**

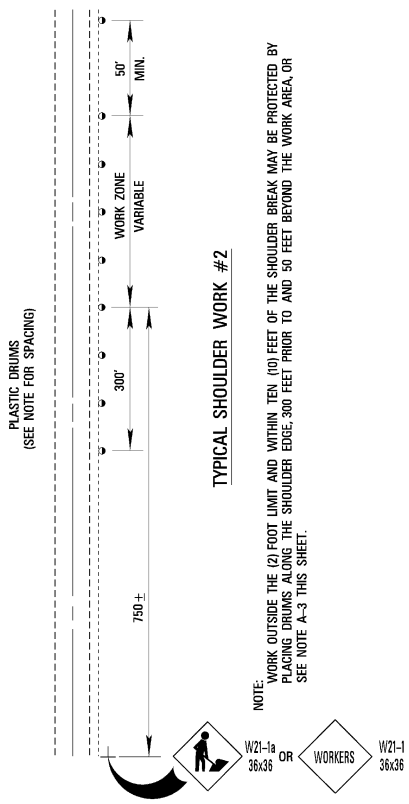
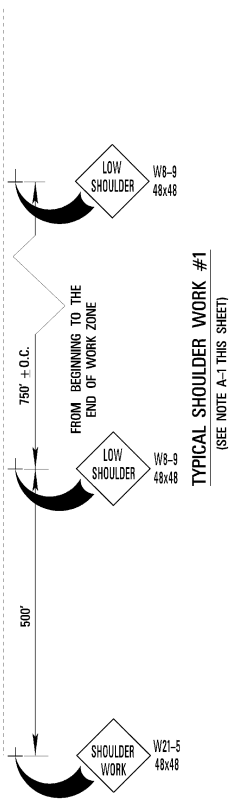
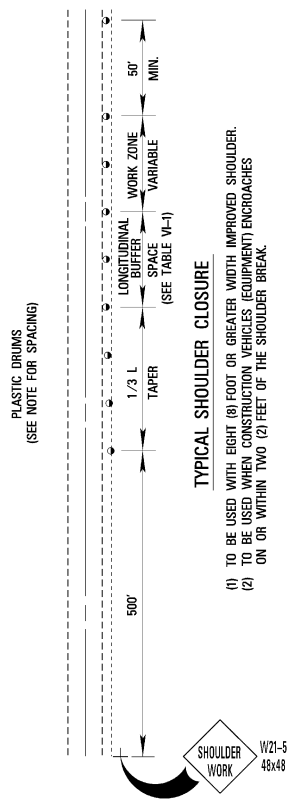
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- 2.  $2 \times W$
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C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

TABLE V-1 GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

SPEED (MPH)	MINIMUM BUFFER SPACE (FEET)
20	35
25	45
30	55
35	65
40	75
45	85
50	95
55	105
60	115
65	125
70	135
75	145
80	155
85	165
90	175
95	185
100	195

\* POSTED SPEED OFF PEAK 85 PERCENTILE SPEED PRIOR TO WORK STARTING OR THE ANTICIPATED GRINDING SPEED IN MPH



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS

DRUM PLACEMENT AND SHOULDER CLOSURE

PROJECT NO.: \_\_\_\_\_

COUNTY: \_\_\_\_\_

FILE NAME: \_\_\_\_\_

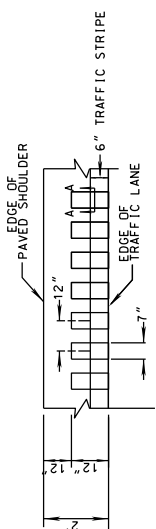
PERSON: \_\_\_\_\_

DATE: 12-28-93

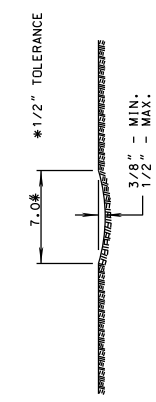
WORKING NUMBER: \_\_\_\_\_

TCP: \_\_\_\_\_

SHEET NUMBER: \_\_\_\_\_

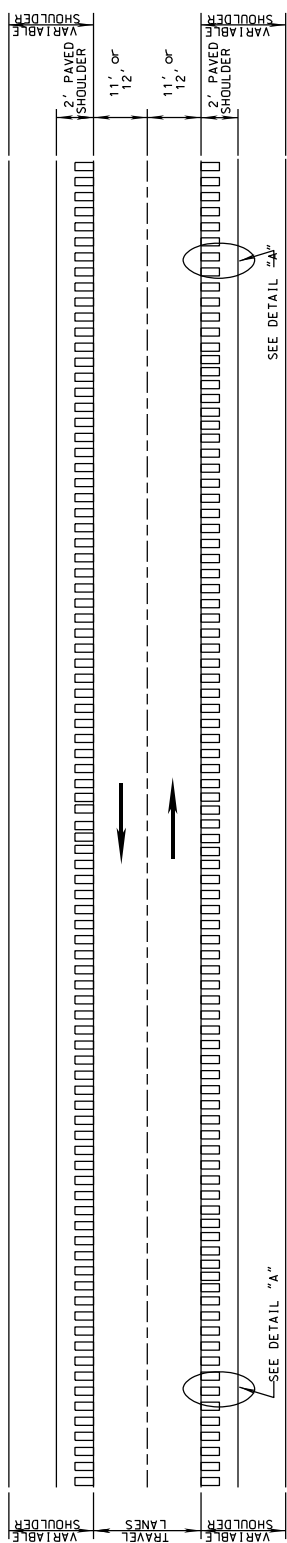


DETAIL "A"



SECTION "A-A"

- GENERAL NOTES
1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
  2. GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
  3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
  4. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.
  5. DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.



PLAN  
NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
RUMBLE STRIPES	
2-LANE HIGHWAYS	
(ASPH. LANES,	
2-FT ASPH. SHLD)	
PROJECT NO:	
COUNTY:	
ISSUING NUMBER	KS-1
SHEET NUMBER	
DATE	05/02/14
BY	
REVISION	
DATE	
FILE NAME	
DESIGN TEAM	SERVICES CHECKED

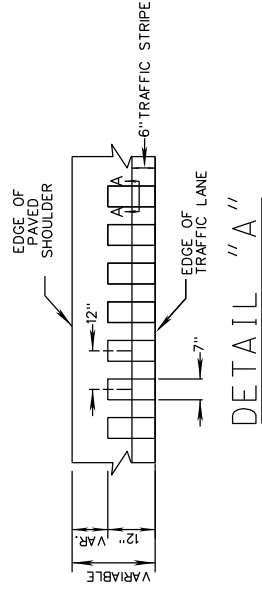
MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
RUMBLE STRIPES  
4-LANE HIGHWAYS  
(ASPH. LANES, 2-FT ASPH. SHLD)

PROJECT NO: R302  
COUNTY: COCA  
WORKING NUMBER: R302  
SHEET NUMBER: 22

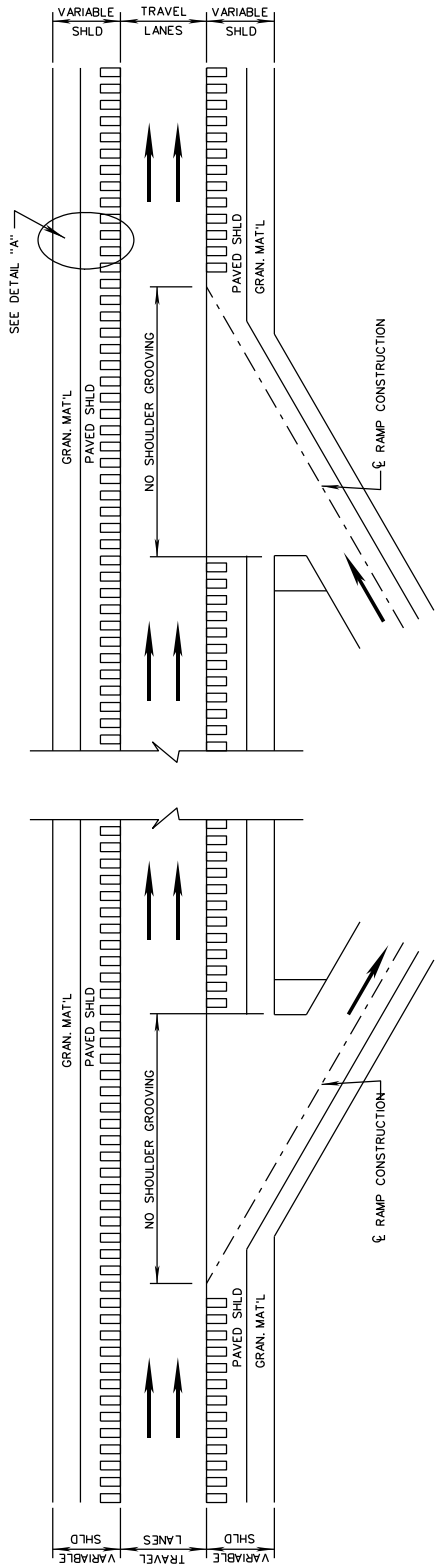
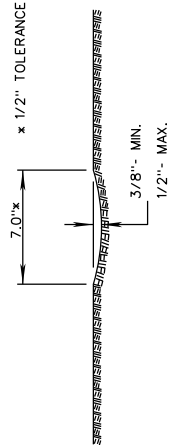
DATE: 05/01/14  
CHECKED: \_\_\_\_\_  
DESIGN TEAM: \_\_\_\_\_

REVISION	DATE

- GENERAL NOTES
1. ALL RUMBLE STRIPES SHALL BE GROUND-IN. RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
  2. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.



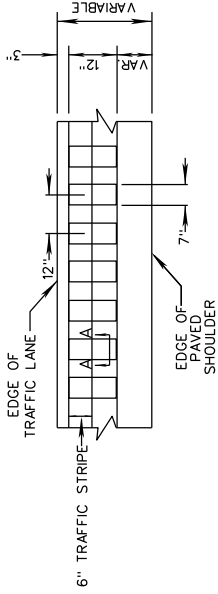
SECTION "A-A"



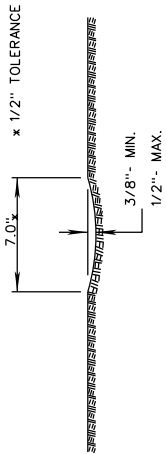
PLAN  
NOT TO SCALE  
DETAILS OF  
RUMBLE STRIPS

STATE	PROJECT NO.
MISS.	

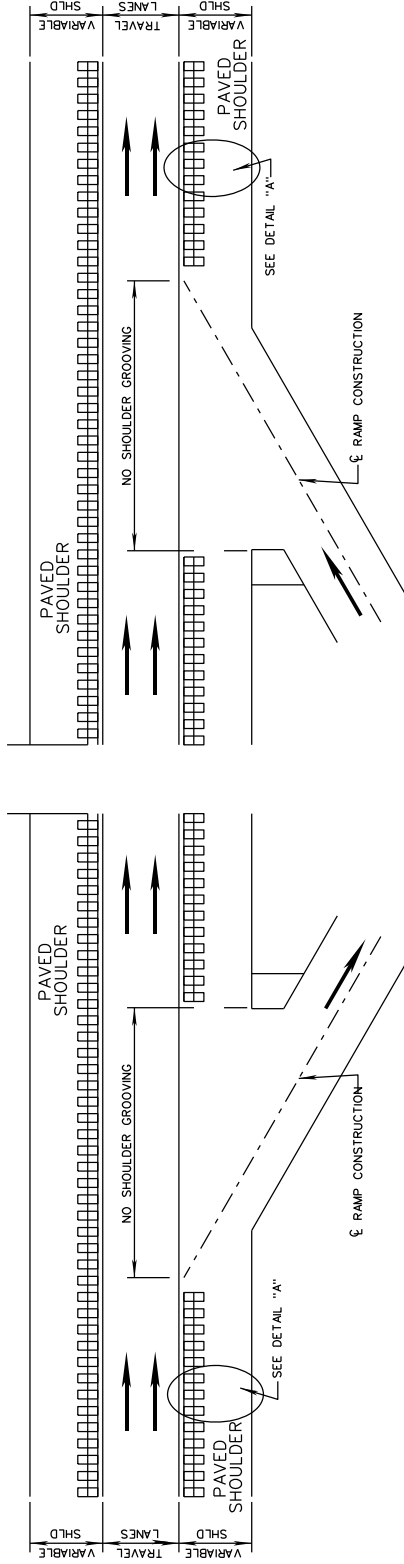
- GENERAL NOTES**
1. ALL RUMBLE STRIPES SHALL BE GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
  2. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.



DETAIL "A"



SECTION "A-A"



PLAN

NOT TO SCALE  
 DETAILS OF  
 RUMBLE STRIPS  
 (GROUND-IN)

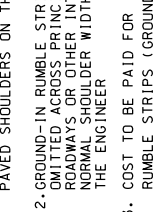
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
RUMBLE STRIPES	
4-LANE WITH FULL-WIDTH	
PAVED SHOULDERS	
(CONC. LANES, ASPH. SHLD)	
WORKING NUMBER	PROJECT NO:
RS-3	COUNTY:
SHEET NUMBER	FILE NAME:
3	DCS011E.W
DATE	CHECKED:
	DATE 05/07/14



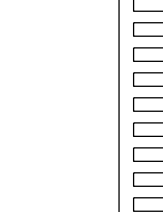
10 FT. OR AS SHOWN ON PLANS



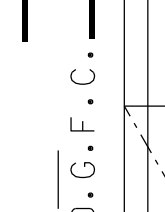
SECTION A-A  
MEDIAN SHOULDER WITHOUT O.G.F.C.



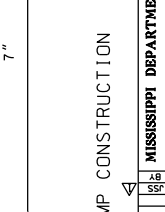
SECTION B-B  
OUTSIDE SHOULDER WITHOUT O.G.F.C.



SECTION C-C  
MEDIAN SHOULDER WITH O.G.F.C.



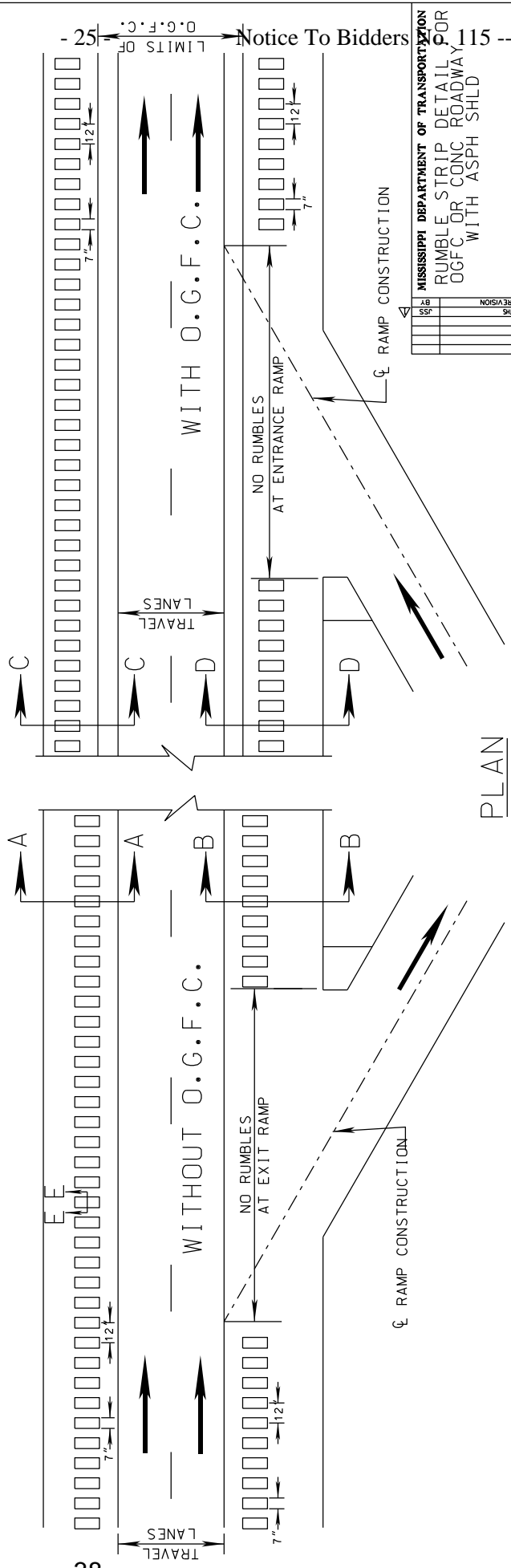
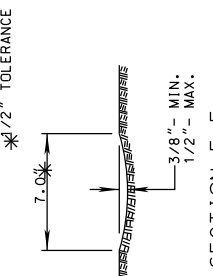
SECTION D-D  
OUTSIDE SHOULDER WITH O.G.F.C.



SECTION E-E  
MEDIAN SHOULDER WITH O.G.F.C.

GENERAL NOTES

- GROUND-IN RUMBLE STRIPS SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
- GROUND-IN RUMBLE STRIPS SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTIONS, ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
- COST TO BE PAID FOR IN PAY ITEM 907-423-A RUMBLE STRIPS (GROUND-IN).
- GROUND-IN RUMBLE STRIPS SHALL BE APPLIED TO MAINLINE ONLY.



PLAN

MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
RUMBLE STRIP DETAIL FOR  
OGFC OR CONC ROADWAY  
WITH ASPH SHLD

DATE	REVISION

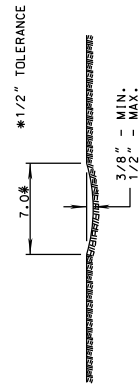
WORKING NUMBER: **RS05**  
SHEET NUMBER: **115** of **118**

PROJECT NO. :  
COUNTY:

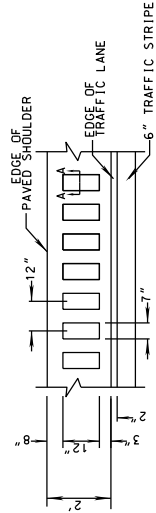
FILE NAME: RumbleStripOGFC.dgn  
DESIGN TEAM:   
CHECKED:   
DATE: 05/02/14

Notice To Bidders

STATE	PROJECT NO.
MSS.	



SECTION "A-A"



DETAIL "A-A"

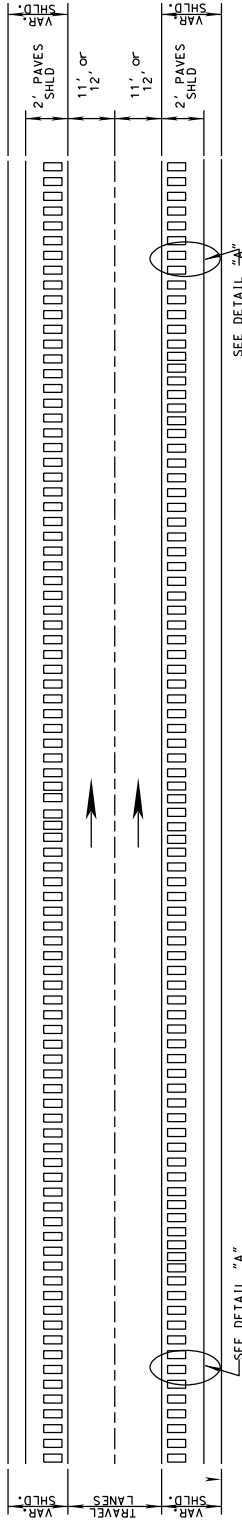
GENERAL NOTES  
 1. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT

2. GROUND-IN RUMBLE STRIPS SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER

3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS

4. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED TO MAINLINE ONLY.

5. DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.



SEE DETAIL "A"

SEE DETAIL "A"

PLAN  
 NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
RUMBLE STRIPS FOR 4-LANE DIVIDED ROADWAYS WITH 2-FOOT PAVED SHOULDERS CONCRETE LANES, ASPH. SHOULDER	
PROJECT NO. : 115	
COUNTY : SHELBY	
WORKING NUMBER	RS-6
SHEET NUMBER	1
FILE NAME	115\Cummins\115.DWG
DESIGN TEAM	S. LEWIS
DATE	05/01/14



**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 193**

**CODE: (SP)**

**DATE: 6/15/2017**

**SUBJECT: Contract Time**

**PROJECT: MP-5000-52(093) / 305744301 & 302 - Noxubee County**

The completion of work to be performed by the Contractor for this project will not be a specified date but shall be when all allowable working days are assessed, or any extension thereto as provided in Subsection 108.06. It is anticipated that the Notice of Award will be issued no later than August 8, 2017 and the date for Notice to Proceed / Beginning of Contract Time will be September 7, 2017.

Should the Contractor request a Notice to Proceed earlier than September 7, 2017 and it is agreeable with the Department for an early Notice to Proceed, the requested date will become the new Notice to Proceed date.

34 Working Days have been allowed for the completion of work on this project.

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION - 904 – NOTICE TO BIDDERS NO. 194**

**CODE: (SP)**

**DATE: 06/09/2017**

**SUBJECT: Scope of Work**

**PROJECT: MP-5000-52(093) / 305744301 & 302 -- Noxubee County**

The contract documents do not include an official set of plans, but may by reference include some Standard Drawings or Special Drawings. All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on this project shall consist of the following:

Mill and overlay approximately 5 miles of existing asphalt pavement on SR 21 and SR 39 in Noxubee County beginning at station 229+16 (approximately 1 mile south/west of the SR 21 & SR 39 intersection) and ending at station 495+11 (SR 145 in Shuqualak).

Existing pavement management data shows the existing asphalt pavement structure to be 3" to 5¼" over 3" to 6" granular material. The existing asphalt pavement has 11½' to 12' wide lanes with 2' to 5' gravel shoulders. Pavement cores showed the existing asphalt pavement to be ¾" to 5" on SR 21 south/west of SR 21 & SR 39 intersection and to be 5" to 9" on SR 21 from SR 21 & SR 39 intersection to SR 145 in Shuqualak.

Construction signage shall be installed as per the detail sheets included prior to the beginning of work.

The existing asphalt roadway shall be cold milled 1½" & variable and overlaid with 2" & variable compacted 12.5-mm, MT, asphalt. SR 39 shall be milled for approximately 800 feet south of the SR 21 & SR 39 intersection.

The failed area between stations 324+00 to 328+00 will be milled 3"± and filled in with 1½"± of 12.5-mm, MT, Leveling asphalt and 1½" of 12.5-mm, MT, asphalt. Increased milling depth of this area shall be paid as 406-A, Cold Milling of Bituminous Pavement, All Depths.

Pay Item 202-B, Removal of Asphalt Paved Shoulders, All Depths, is intended to be used for payment of removal of asphalt shoulders around bridge which cannot be cold milled.

Shoulders shall be paved beneath existing guardrail.

Local public roads shall be milled and overlaid to the end of the existing asphalt pavement, end of MDOT maintenance or to right-of-way or as directed. Where a minimum of five feet (5') of shoulder width can be paved at the beginning of local road radii, a 100-foot asphalt pavement

taper shall be constructed. Said taper shall be six inches (6") thick and shall conform to the detail drawings.

Temporary pavement markings shall be constructed at the end of each day's paving operations. Permanent pavement markings shall be constructed after completion of all paving operations. Railroad pavement markings shall be placed at the railroad in Shuqualak as per the detail sheet shown.

Raised pavement markers shall be installed on SR 21 and SR 39 and local public roads (Jones Road, John Hale Road, Shuqualak Road) outside the urban limits of the town of Shuqualak.

### **GENERAL NOTES**

Milling and paving operations shall be performed such that a -2% slope from centerline is provided in normal crown roadway sections. Superelevation through curves shall be maintained as it currently exists or improved as directed.

Traffic will not be allowed on milled main line surfaces.

Temporary asphalt joints (paper joints) shall be constructed at the end of each day's milling operations where the milled surface joins the existing asphalt pavement surface. Paper joints shall be a minimum of nine feet (9') in length and for the full width of the milled surface. Paper joints shall be adequately maintained.

The Contractor is responsible for providing shoulder drainage outlets as applicable in milled areas. Payment for these outlets shall be included in the bid price for the milling of bituminous pavement.

The reclaimed asphalt pavement (RAP) material removed by the milling operation shall become the property of the Contractor with the exception of 10,000 tons or 50% of the total anticipated quantity, whichever is less, and shall be delivered to the MDOT's milling stockpile located on the west side of US 45 north of the SR145 / US 45 intersection on the north side of Shuqualak. Sufficient advance notice shall be given to ensure that MDOT Maintenance personnel will be on hand to direct the delivery. The Contractor shall also provide MDOT with an Operator and the necessary equipment to stockpile the delivery. The cost of which shall be absorbed.

Existing asphalt/concrete driveway connections shall be milled or removed and replaced with new asphalt connections.

Potholes that may exist or occur in the existing pavement are to be patched in a timely manner. Patching of potholes shall be considered an absorbed item.

Where applicable the existing shoulders are to be raised to match the new pavement elevation by placing variable depth Granular Material (Class 5, Group E) on the existing shoulders. Placement of the granular material on the finished asphalt course shall not be permitted. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%).

Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%). The cost of blading will be an absorbed item and is not to be included in the price of pay items bid.

Removal of the existing shoulder material shall be coincident with the milling/overlaying operation to prevent the possible ponding of water. No payment will be made for blading or removal of the existing shoulder material. Any material excavated from the existing shoulder shall be used to raise the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer and will be an absorbed item. Material which cannot be placed in adjacent areas and deemed to be excess excavations by the Engineer will be an absorbed item.

Temporary stripe will be required immediately after milling and overlaying and prior to opening the area to traffic. Temporary stripe is to be placed in the same location and layout as permanent stripe.

All permanent striping will be thermoplastic. The width of the permanent stripe will be six inches (6").

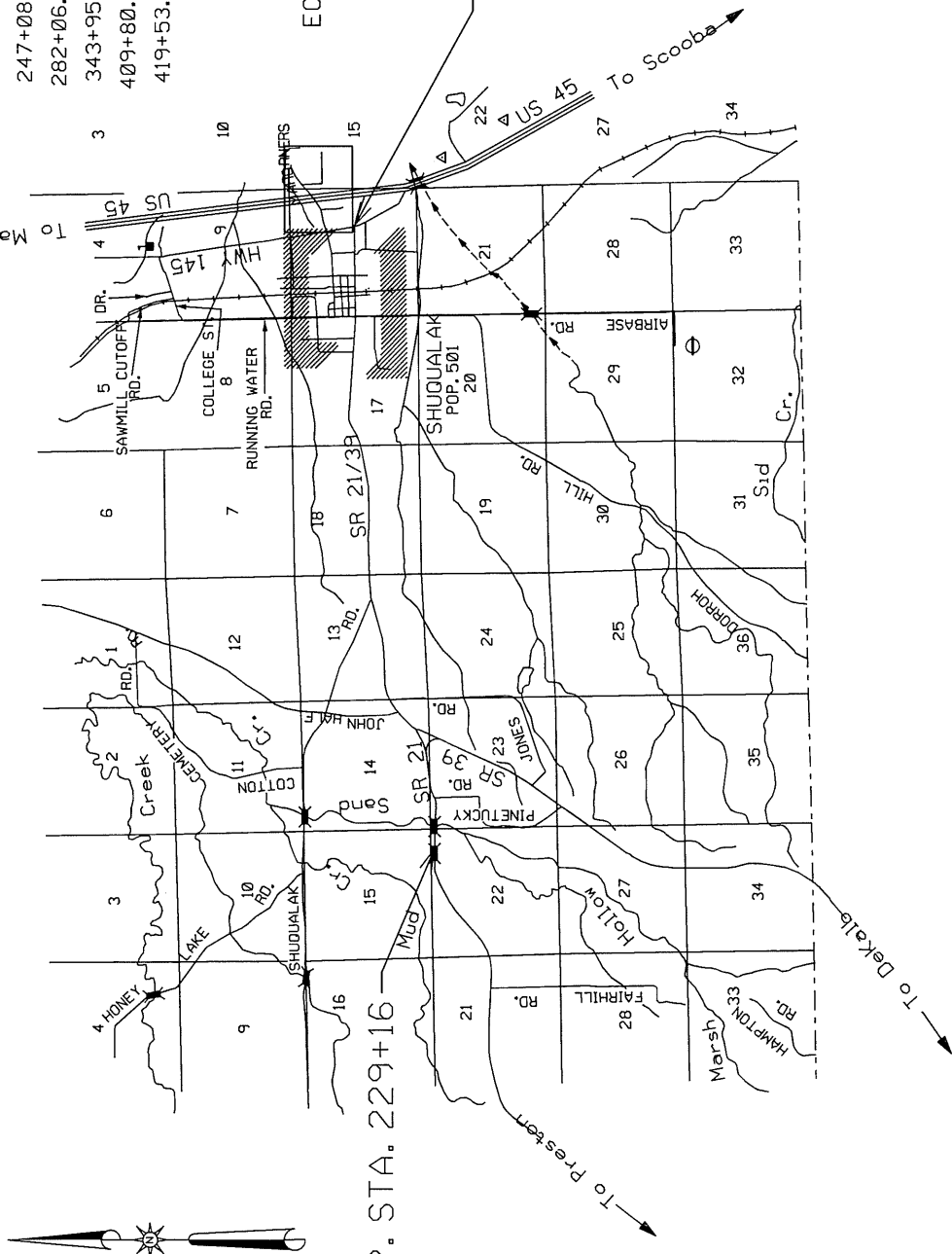
The Contractor shall erect and maintain construction signing, and provide and maintain all temporary signs and traffic control devices necessary to safely conduct traffic through the work area in accordance with the Traffic Control Plan and the MUTCD.

All traffic control devices shall meet current MDOT and MUTCD requirements.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal. The cost is to be included in the prices of items bid. Failure of the Contractor to remove debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor. As described in the applicable Notice-To-Bidders, final project cleanup is required and will be completed prior to the scheduling of the final inspection.

It shall be the responsibility of the Contractor to protect existing structures such as pipes, aprons, signs, utilities, etc. from damage occurring as a result of construction activities. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged during the life of the contract. No payment will be made for replacements and or repairs resulting from such damages.

PROJECT LOCATION MAP  
 SR 21 / 39 - NOXUBEE COUNTY  
 MP-5000-52(093)



PROJECT EQUATIONS

(PER BEST FIT CL/NOT PER PLANS)

- 247+08.23 BK = 247+09.82 AH (CL TOPO) -1.59'
  - 282+06.77 BK = 282+85.92 AH (CL TOPO) -79.15'
  - 343+95.06 BK = 343+97.00 AH (CL TOPO) -1.94'
  - 409+80.45 BK = 409+40.00 AH (CL TOPO) +40.45'
  - 419+53.84 BK = 420+00.00 AH (CL TOPO) -46.16'
- TOTAL = -88.39'

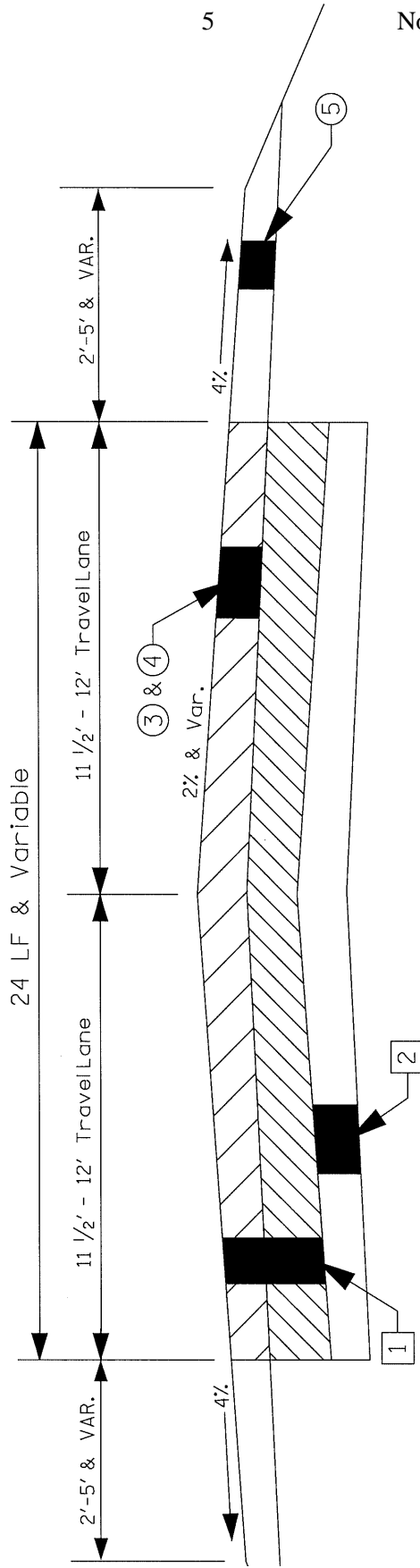
LENGTH OF PROJECT

EOP 495+11 - BOP 229+16 = 26,595'  
 -88.39'  
 TOTAL LENGTH = 26,506.61'  
 (5.02 MILES)

44B.O.P. STA. 229+16

E.O.P. STA. 495+11

SR 21 - NOXUBEE COUNTY  
 MP-5000-52(093)  
 TYPICAL SECTION - MILL & OVERLAY  
 (B.O.P.) 229+16 - 284+00 (SR 21/39 Intersection)



EXISTING

- 1 3" HMA (3/4"-5" Core Evaluation)
- 2 6" Granular Material

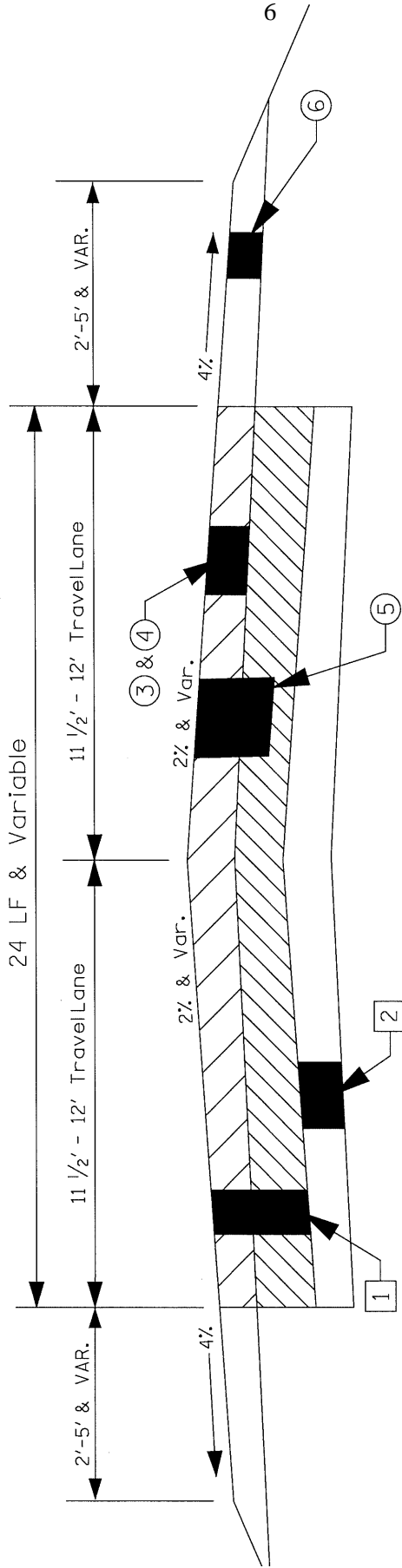
PROPOSED

- 3 1 1/2" Cold Milling (Correct to 2% Normal Crown or SE)
- 4 2" HMA/WMA 12.5mm MT
- 5 Class 5 Group E Granular Material (As Directed)

SR 21/39 - NOXUBEE COUNTY  
MP-5000-52(093)

TYPICAL SECTION - MILL & OVERLAY

(SR 21/39 Intersection) 284+00 - E.O.P. 495+11 (SR 145 Intersection)



EXISTING

- 1 5 1/4" HMA (5'-9 1/2" Core Evaluation)
- 2 3" Granular Material

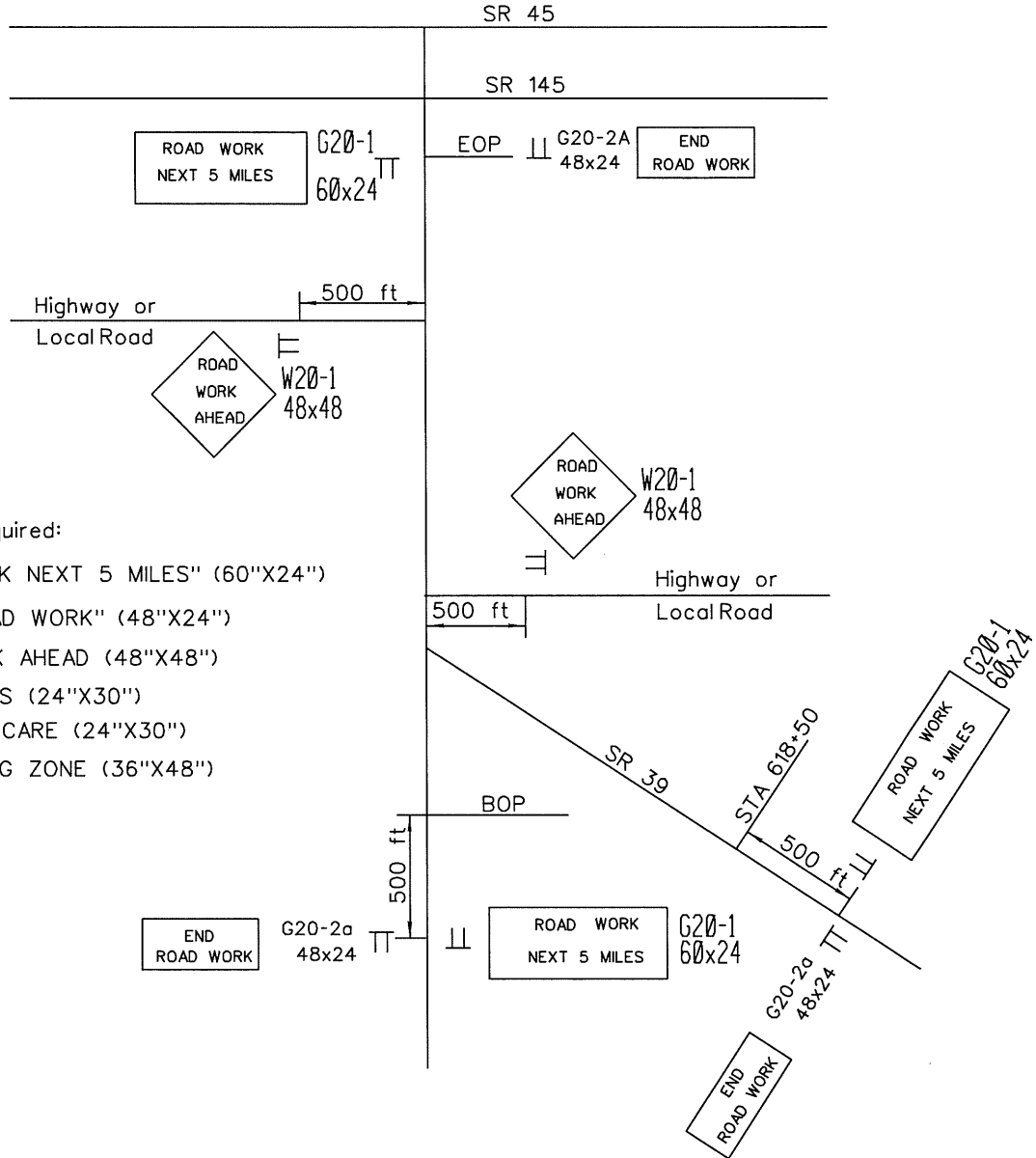
PROPOSED

- 3 1 1/2" Cold Milling (Correct to 2% Normal Crown or SE)
- 4 2" HMA/WMA 12.5mm MT
- 5 Failed Area HMA/WMA 12.5mm MT Leveling
- 6 Class 5 Group E Granular Material (As Directed)

Note: Failed area repair to be milled 3"±.

Note: Failed area asphalt repair (1 1/2" 12.5mm MT + 1 1/2" 12.5mm MT Leveling).

# SR 21/39 - NOXUBEE COUNTY MP-5000-52(093) CONSTRUCTION SIGNING



**Traffic Control Signs Required:**

- 3 - G20-1 "ROAD WORK NEXT 5 MILES" (60"x24")
- 3 - G20-2a "END ROAD WORK" (48"x24")
- 15 - W20-1 ROAD WORK AHEAD (48"x48")
- 55 - R4-1 DO NOT PASS (24"x30")
- 8 - R4-2 PASS WITH CARE (24"x30")
- 11 - W14-3 NO PASSING ZONE (36"x48")

- NOTES:
- ① One (1) W20-1 "ROAD WORK AHEAD" Sign is Required at each Local Road, Street or Highway Entering the Project.
  - ② G20-1 signs mounted on Type III Double Faced Barricade (6' Wide).
  - ③ R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE", and W14-3 "NO PASSING ZONE" signs are required in accordance with Subsection 618.03.3 and as specified in the MUTCD. If No Passing zones are 1000 ft or more, install additional "DO NOT PASS" signs on maximum spacing of 750 ft.
  - ④ Placement of W20-1 signs on intersecting roads may vary from typical shown as conditions warrant.



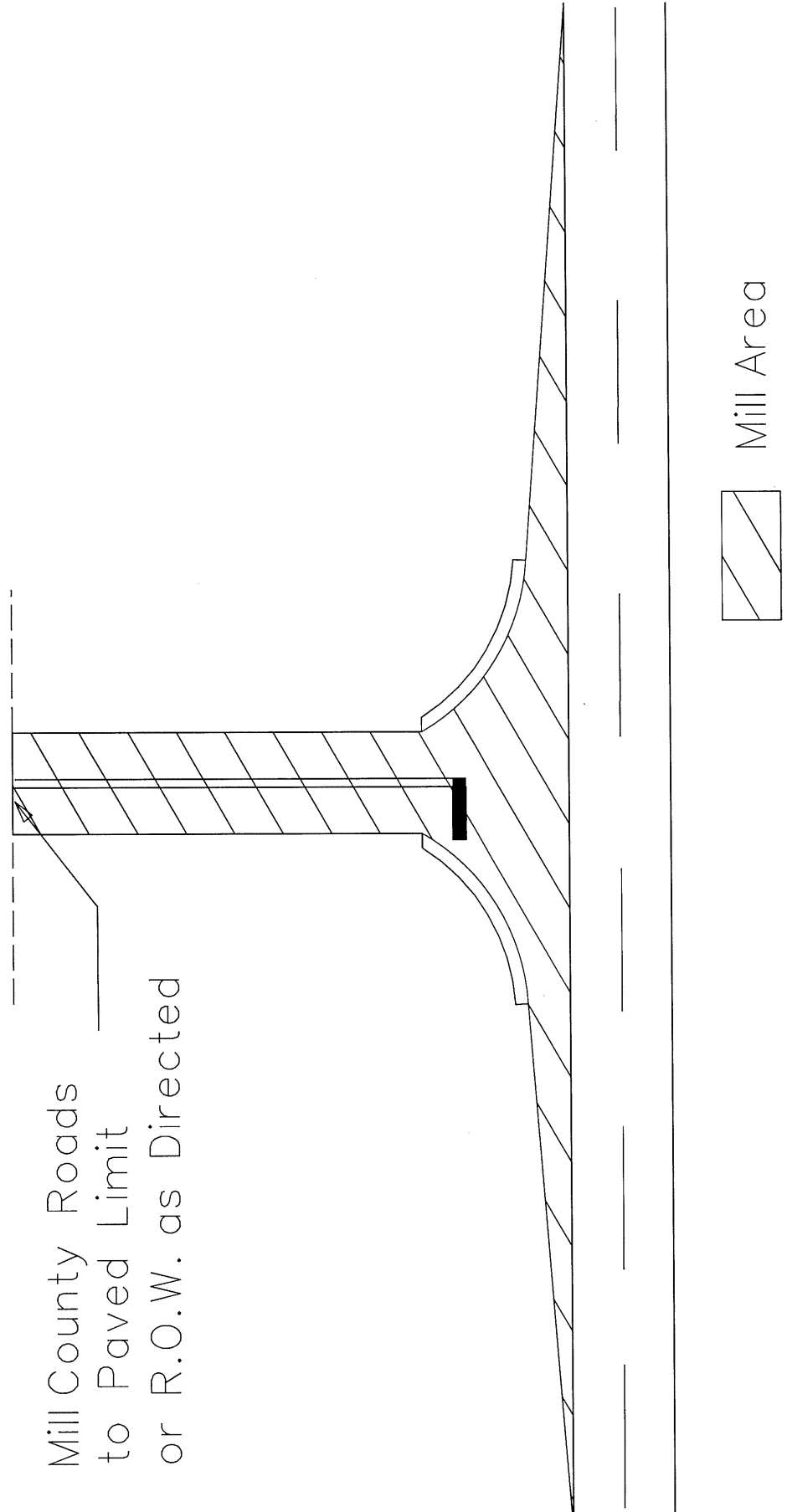
MP-5000-52(093)					
619-D Standard Roadside Construction Signs (Less than 10 Sq. Ft.) - 1 of 2					
Station	Location	Description	Quantity	Unit	Remarks
	LT	G20-2A	8	SF	500' SOUTH OF BOP
229+16	RT	R4-1	5	SF	NORTHBOUND
242+40	LT	W14-3	5.56	SF	NORTHBOUND
242+40	RT	R4-1	5	SF	NORTHBOUND
249+88	RT	R4-1	5	SF	NORTHBOUND
257+38	RT	R4-1	5	SF	NORTHBOUND
264+88	RT	R4-1	5	SF	NORTHBOUND
272+38	RT	R4-1	5	SF	NORTHBOUND
279+88	RT	R4-1	5	SF	NORTHBOUND
286+59	RT	R4-1	5	SF	NORTHBOUND
294+09	RT	R4-1	5	SF	NORTHBOUND
301+59	RT	R4-1	5	SF	NORTHBOUND
309+09	RT	R4-1	5	SF	NORTHBOUND
316+59	RT	R4-1	5	SF	NORTHBOUND
323+30	RT	R4-2	5	SF	NORTHBOUND
332+78	LT	W14-3	5.56	SF	NORTHBOUND
332+78	RT	R4-1	5	SF	NORTHBOUND
340+28	RT	R4-1	5	SF	NORTHBOUND
347+76	RT	R4-1	5	SF	NORTHBOUND
355+26	RT	R4-1	5	SF	NORTHBOUND
362+76	RT	R4-1	5	SF	NORTHBOUND
370+26	RT	R4-1	5	SF	NORTHBOUND
377+76	RT	R4-1	5	SF	NORTHBOUND
385+26	RT	R4-1	5	SF	NORTHBOUND
392+76	RT	R4-1	5	SF	NORTHBOUND
400+26	RT	R4-1	5	SF	NORTHBOUND
407+76	RT	R4-1	5	SF	NORTHBOUND
415+66	RT	R4-1	5	SF	NORTHBOUND
417+60	RT	R4-2	5	SF	NORTHBOUND
454+30	LT	W14-3	5.56	SF	NORTHBOUND
454+30	RT	R4-1	5	SF	NORTHBOUND
461+80	RT	R4-1	5	SF	NORTHBOUND
469+92	LT	W14-3	5.56	SF	NORTHBOUND
469+92	RT	R4-1	5	SF	NORTHBOUND
471+86	RT	R4-2	5	SF	NORTHBOUND
488+73	LT	W14-3	5.56	SF	NORTHBOUND
488+73	RT	R4-1	5	SF	NORTHBOUND
	RT	G20-2A	8	SF	500' NORTH OF EOP
495+11	RT	W14-3	5.56	SF	SOUTHBOUND
495+11	LT	R4-1	5	SF	SOUTHBOUND
494+30	LT	R4-2	5	SF	SOUTHBOUND
475+36	RT	W14-3	5.56	SF	SOUTHBOUND
475+36	LT	R4-1	5	SF	SOUTHBOUND
468+82	RT	W14-3	5.56	SF	SOUTHBOUND
468+82	LT	R4-1	5	SF	SOUTHBOUND
461+32	LT	R4-1	5	SF	SOUTHBOUND
459+90	LT	R4-2	5	SF	SOUTHBOUND
428+30	RT	W14-3	5.56	SF	SOUTHBOUND
428+30	LT	R4-1	5	SF	SOUTHBOUND
420+80	LT	R4-1	5	SF	SOUTHBOUND
412+84	LT	R4-1	5	SF	SOUTHBOUND
405+74	LT	R4-1	5	SF	SOUTHBOUND
398+24	LT	R4-1	5	SF	SOUTHBOUND
390+74	LT	R4-1	5	SF	SOUTHBOUND
PAGE TOTAL			281.040		

<b>MP-5000-52(093)</b>					
<b>619-D Standard Roadside Construction Signs (Less than 10 Sq. Ft.) - 2 of 2</b>					
<b>Station</b>	<b>Location</b>	<b>Description</b>	<b>Quantity</b>	<b>Unit</b>	<b>Remarks</b>
383+24	LT	R4-1	5	SF	SOUTHBOUND
375+74	LT	R4-1	5	SF	SOUTHBOUND
368+24	LT	R4-1	5	SF	SOUTHBOUND
862+37	LT	R4-1	5	SF	SOUTHBOUND
360+74	LT	R4-1	5	SF	SOUTHBOUND
353+24	LT	R4-1	5	SF	SOUTHBOUND
345+74	LT	R4-1	5	SF	SOUTHBOUND
342+90	LT	R4-2	5	SF	SOUTHBOUND
333+40	RT	W14-3	5.56	SF	SOUTHBOUND
333+40	LT	R4-1	5	SF	SOUTHBOUND
325+90	LT	R4-1	5	SF	SOUTHBOUND
318+40	LT	R4-1	5	SF	SOUTHBOUND
310+90	LT	R4-1	5	SF	SOUTHBOUND
303+40	LT	R4-1	5	SF	SOUTHBOUND
295+90	LT	R4-1	5	SF	SOUTHBOUND
288+40	LT	R4-1	5	SF	SOUTHBOUND
280+11	LT	R4-1	5	SF	SOUTHBOUND
272+61	LT	R4-1	5	SF	SOUTHBOUND
265+11	LT	R4-1	5	SF	SOUTHBOUND
257+61	LT	R4-1	5	SF	SOUTHBOUND
252+55	LT	R4-2	5	SF	SOUTHBOUND
231+97	RT	W14-3	5.56	SF	SOUTHBOUND
231+97	LT	R4-1	5	SF	SOUTHBOUND
	LT	G20-2A	8	SF	500' SOUTH OF BOP
<b>PAGE TOTAL</b>			<b>124.120</b>		
<b>TOTAL</b>			<b>405.160</b>		

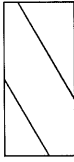


<b>MP-5000-52(093)</b>			
<b>FAILED AREA REPAIR</b>			
<b>Station</b>	<b>Location</b>	<b>Description</b>	<b>Remarks</b>
324+00± - 328+00±	LT LN	MILL 3"± - REPLACE WITH 1½"± 12.5mm MT LEVELING & 1½"± 12.5mm MT	ASPHALT PAVEMENT REMOVAL TO BE PAID AS 406-A002 COLD MILLING OF BITUMINOUS PAVEMENT, ALL DEPTHS

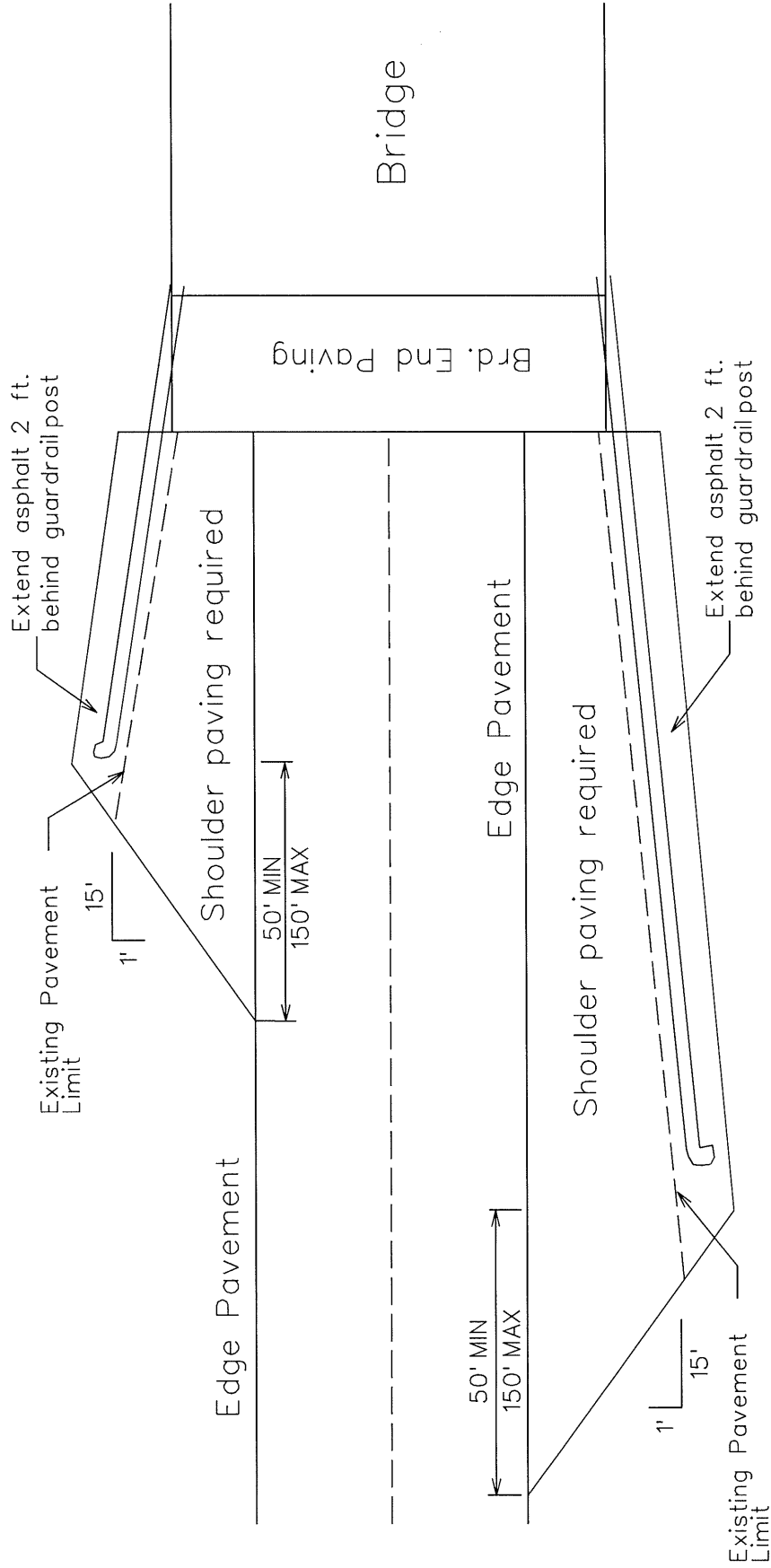
SR 21 - NOXUBEE COUNTY  
MP-5000-52(093)  
MILLING COUNTY ROADS



Mill County Roads  
to Paved Limit  
or R.O.W. as Directed

 Mill Area

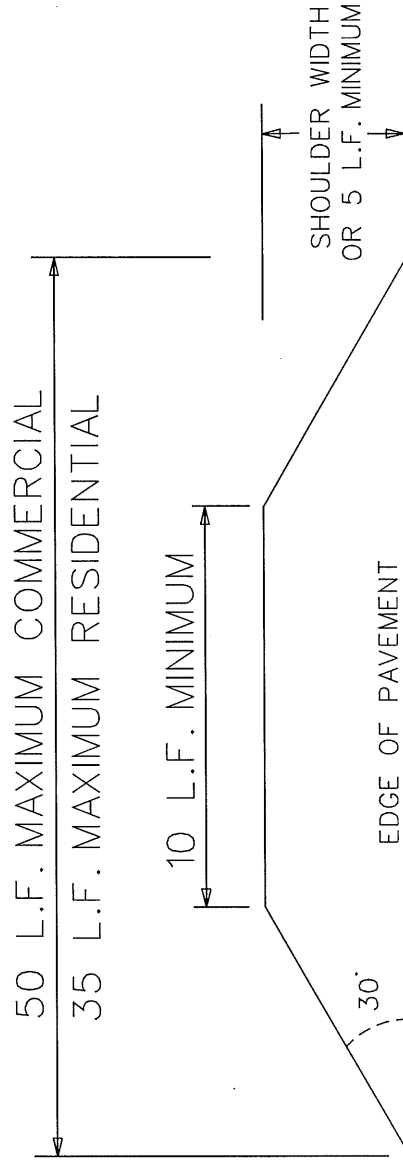
SR 21 - NOXUBEE COUNTY  
 MP-5000-52(093)  
 SHOULDER PAVING AT BRIDGE GUARDRAIL



NOTE: MILL EXISTING SHOULDER PAVEMENT & PAVE UNDER EXISTING GUARD RAIL ON SLOPE NECESSARY TO ACHIEVE MINIMUM 27<sup>3</sup>/<sub>4</sub>" TOP OF RAIL HEIGHT ABOVE PAVED SHOULDER.

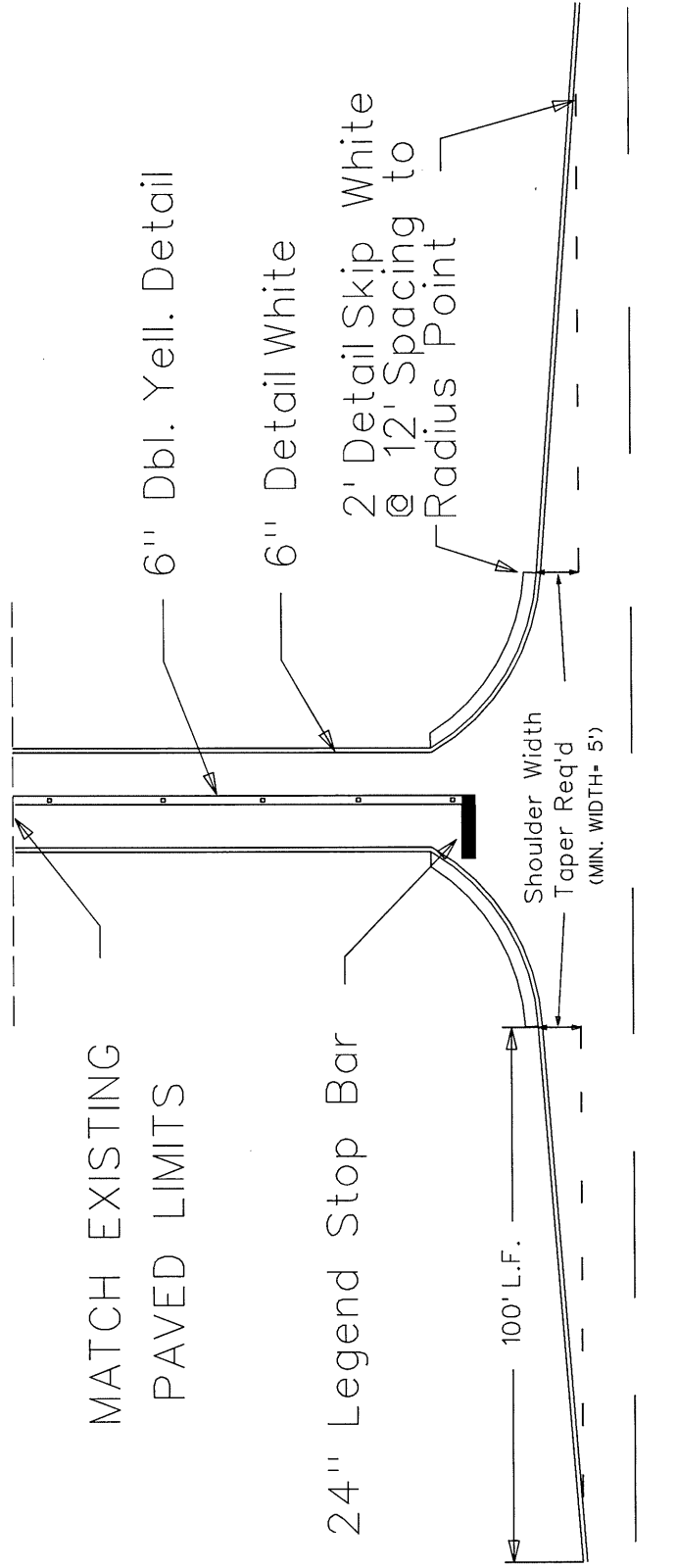
NOTE: MINIMUM 2" COMPACTED ASPHALT SHOULDER

SR 21 - NOXUBEE COUNTY  
 MP-5000-52(093)  
 DRIVEWAY APRON



\_\_\_\_\_ MAINLINE PAVEMENT \_\_\_\_\_

SR 21 - NOXUBEE COUNTY  
 MP-5000-52(093)  
 COUNTY ROAD PAVING/STRIPING



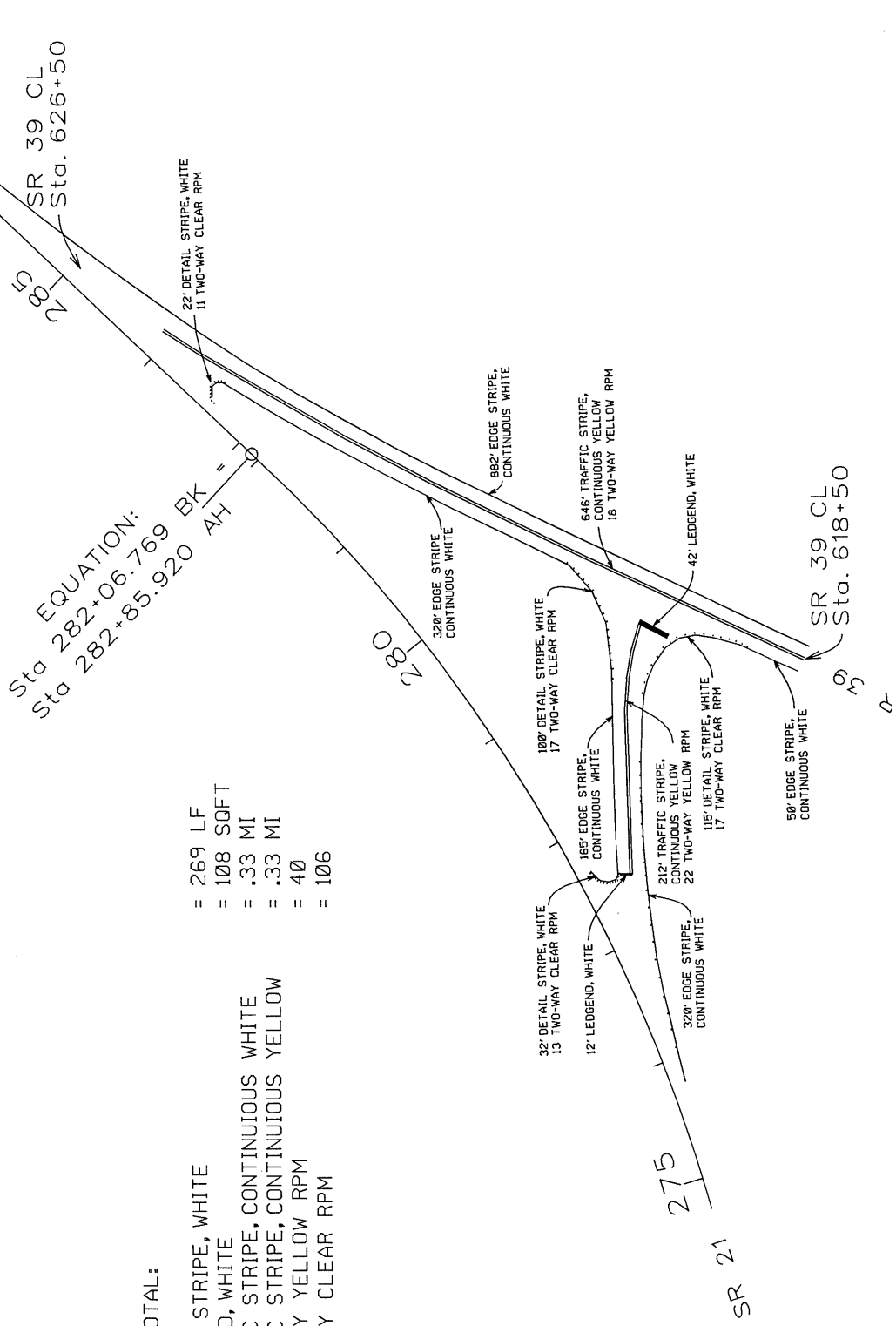
NOTE: 100' TAPERS TO BE CONSTRUCTED WHERE 5' SHOULDER WIDTH IS AVAILABLE AT THE BEGINNING OF LOCAL ROAD RADIUS.

NOTE: ASPHALT PAVEMENT THICKNESS IN TAPER SHALL BE 6".

NOTE: DETAIL SKIP SHALL BE PLACED ON LOCAL ROADS WITH TAPERS.

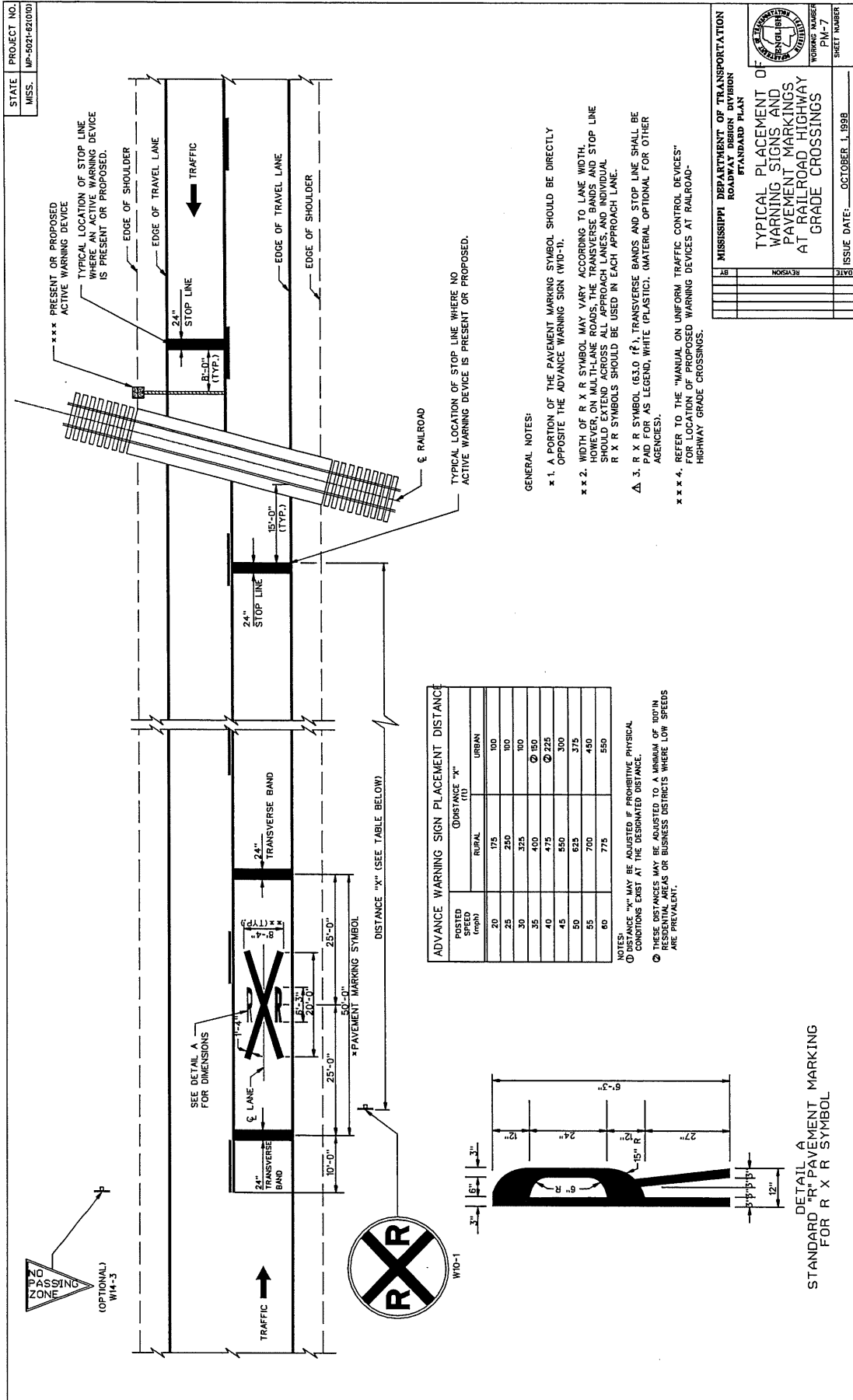


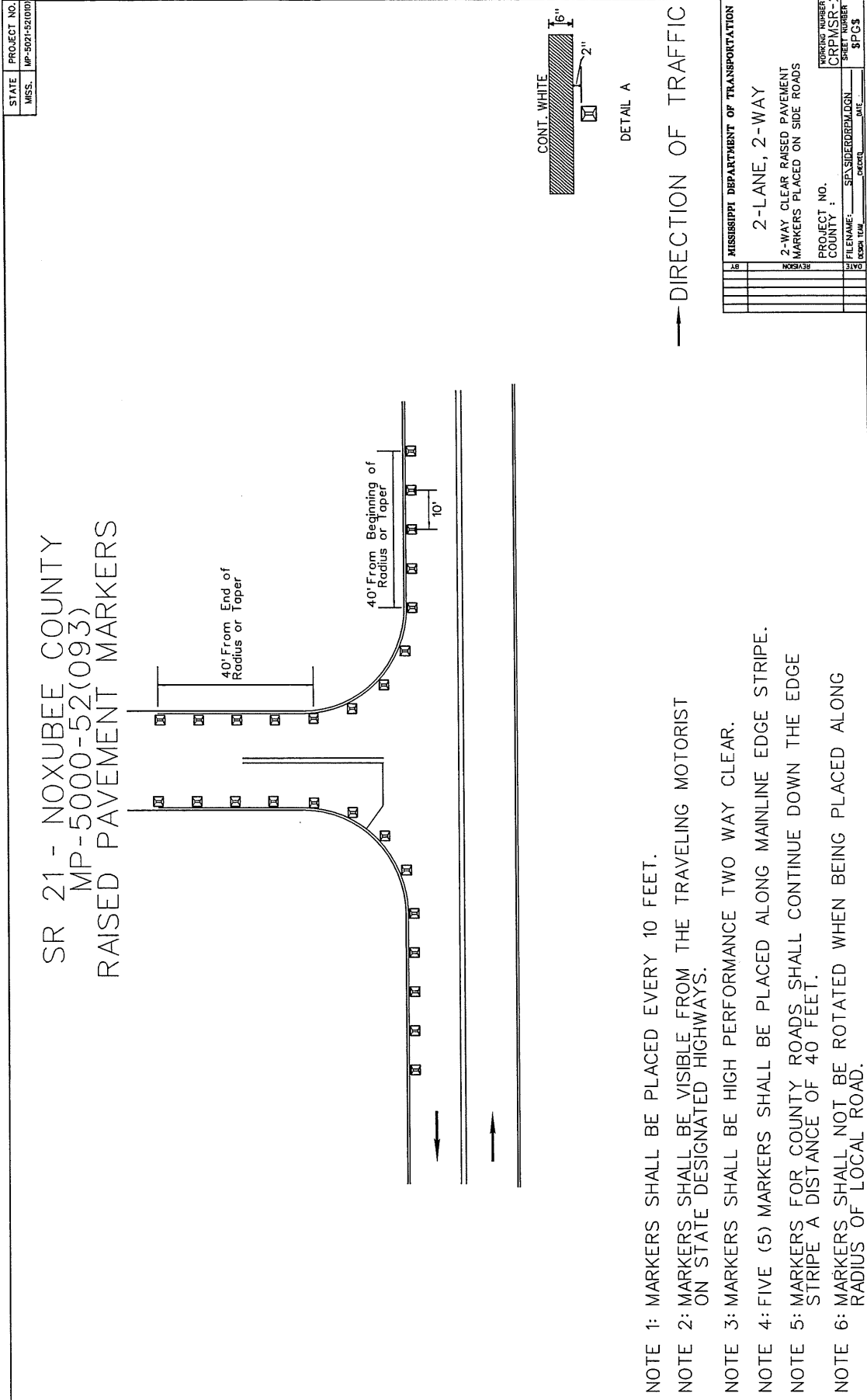
SR 21 - NOXUBEE COUNTY  
 MP-5000-52(093)  
 21-39 INTERSECTION PAVEMENT MARKINGS



PAGE TOTAL:

- DETAIL STRIPE, WHITE = 269 LF
- LEDGEND, WHITE = 108 SQFT
- TRAFFIC STRIPE, CONTINUOUS WHITE = .33 MI
- TRAFFIC STRIPE, CONTINUOUS YELLOW = .33 MI
- TWO-WAY YELLOW RPM = 40
- TWO-WAY CLEAR RPM = 106





**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SECTION 904 - NOTICE TO BIDDERS NO. 195**

**CODE: (SP)**

**DATE: 6/02/2017**

**SUBJECT: Temporary Construction Signs**

**PROJECT: MP-5000-52(093) / 305744301, & 302 – Noxubee County**

Bidders are hereby advised of the following regarding the installation and removal of Temporary Construction Signs:

Should the Contractor elect to install Temporary Construction Signs by first driving short u-channel sections and then bolting longer u-channel sections to them to achieve the correct height, the short sections shall extend a minimum of four (4) feet from the ground level after they are driven. Furthermore, these short sections **shall be removed** at the completion of the project.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**SPECIAL PROVISION NO. 907-103-1**

**CODE: (SP)**

**DATE: 05/02/2017**

**SUBJECT: Award and Execution of Contract**

Section 103, Award and Execution of Contract, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

**907-103.01--Consideration of Proposal.**

Delete the fourth and fifth sentences of the third paragraph of Subsection 103.01 on page 19, and substitute the following.

Should the attachment not accompany the bid when submitted, the Contractor shall have 10 days following the opening of the bids to furnish the required information to the Contract Administration Director for attachment to the bid. Failure to provide the attachment within 10 days will result in the nonresident Contractor's bid being rejected and not considered for award. As used herein, the term "resident Contractor" includes a nonresident person, firm or corporation that has been qualified to do business in this State and has maintained a permanent full-time office in the State of Mississippi for two years prior to the submission of the bid, and the subsidiaries and affiliates of such a person, firm or corporation.

## MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-899-1

CODE: (SP)

DATE: 01/17/2017

SUBJECT: Railway-Highway Provisions

Section 907-899, Railway-Highway Provisions, is hereby added to and made part of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction as follows:

### SECTION 907-899--RAILWAY-HIGHWAY PROVISIONS

**907-899.01--Description.** This special provision addresses the Contractor's involvement with railroad flagging, Contractor Safety Orientation, Contractor Background Investigation, Contractor Roadway Worker on Track Safety Program and Safety Action Plan, and any other requirements set forth by the Railroad and any attached Exhibits.

Prior to bidding, the Contractor shall read and comply with the requirements of the Railroad and any attached Exhibits. The Contractor shall contact the Railroad concerning insurance coverage requirements, Railroad flagging costs, Contractor Safety Orientation, Contractor Background Investigation, Contractor Roadway Worker on Track Safety Program and Safety Action Plan, and any other requirements set forth by the Railroad and any attached Exhibits. In case the railroad requires coverage over and above that required by the Standard Specifications, the railroad requirements shall be met.

If in the opinion of the RAILROAD, the presence of an authorized representative of the RAILROAD is required to supervise the same, the RAILROAD shall render bills to the Contractor for all expenses incurred by it for such supervision. This includes all labor costs for flagmen or cable locate supplied by the RAILROAD to protect RAILROAD operation, and for the full cost of furnishing, installation and later removal of any temporary supports for said tracks, as the RAILROAD's Chief Engineer's Office may deem necessary.

**It will be the Contractor's responsibility to pay all bills associated with the Railroad requirements and any attached Exhibits.**

A cable locate of RAILROAD owned facilities may be required to identify and protect Signal & Communication cables that have been installed to provide power, signal control, wayside communications. These cables are vital to a safe and reliable railway operation. The cable locate will be performed by a qualified RAILROAD employee.

Outside Contractors are prohibited from driving on, along, or across any track that does not have a RAILROAD installed crossing. They may utilize an existing public crossing. The practice of allowing rubber tired equipment to operate over track with no crossing has been banned.

The Contractor shall complete and process any required forms addressed by the Railroad or any attached Exhibits. The Contractor shall not commence or carry on any form of work on, under, above or within the designated distance from the Railroad track prior to getting approval from the Railroad.

**907-899.02--Blank.**

**907-899.03--Construction Requirements.** The Contractor shall read and comply with the requirements of the Railroad and any attached Exhibits.

**907-899.04--Method of Measurement.** Railway-highway provisions will be measured as a unit lump sum quantity. Measurement for payment will be in accordance with the following schedule:

- a) On the first estimate, twenty five percent (25%) of the amount bid for Railway Highway Provision will be paid.
- b) When twenty five percent (25%) of the original contract amount is earned from all direct pay items, fifty percent (50%) of the amount bid for Railway Highway Provision will be paid.
- c) When fifty percent (50%) of the original contract amount is earned from all direct pay items, one hundred percent (100%) of the amount bid for Railway Highway Provision will be paid.

**907-899.05--Basis of Payment.** Railway-highway provisions, measured as prescribed above, will be paid for at the contract lump sum price, which price shall be payment in full for all insurance coverage requirements, railroad flagging costs, Contractor safety orientation, Contractor background investigation, Contractor safety programs and plans, and any other requirements set forth by the Railroad and any attached Exhibits, and other incidentals necessary to complete the requirements of this work.

Payment will be made under:

907-899-A: Railway-Highway Provisions

- lump sum

# SECTION 905 - PROPOSAL

Date \_\_\_\_\_

Mississippi Transportation Commission  
Jackson, Mississippi

Sirs: The following proposal is made on behalf of \_\_\_\_\_  
\_\_\_\_\_ of \_\_\_\_\_

for constructing the following designated project(s) within the time(s) hereinafter specified.

The plans are composed of drawings and blue prints on file in the offices of the Mississippi Department of Transportation, Jackson, Mississippi.

The Specifications are the current Standard Specifications of the Mississippi Department of Transportation approved by the Federal Highway Administration, except where superseded or amended by the plans, Special Provisions and Notice(s) to Bidders attached hereto and made a part thereof.

I (We) certify that I (we) possess a copy of said Standard and any Supplemental Specifications.

Evidence of my (our) authority to submit the Proposal is hereby furnished. The proposal is made without collusion on the part of any person, firm or corporation. I (We) certify that I (we) have carefully examined the Plans, the Specifications, including the Special Provisions and Notice(s) to Bidders, herein, and have personally examined the site of the work. On the basis of the Specifications, Special Provisions, Notice(s) to Bidders, and Plans, I (we) propose to furnish all necessary machinery, tools, apparatus and other means of construction and do all the work and furnish all the materials in the manner specified. I (We) understand that the quantities mentioned herein are approximate only and are subject to either increase or decrease, and hereby propose to perform any increased or decreased quantities of work at the unit prices bid, in accordance with the above.

I (We) acknowledge that this proposal will be found irregular and/or non-responsive unless a certified check, cashier's check, or Proposal Guaranty Bond in the amount as required in the Advertisement (or, by law) is submitted electronically with the proposal or is delivered to the Contract Administration Engineer prior to the bid opening time specified in the advertisement.

**INSTRUCTION TO BIDDERS: Alternate and Optional Items on Bid Schedule.**

1. Two or more items entered opposite a single unit quantity WITHOUT DEFINITE DESIGNATION AS "ALTERNATE ITEMS" are considered as "OPTIONAL ITEMS". Bidders may or may not indicate on bids the Optional Item proposed to be furnished or performed WITHOUT PREJUDICE IN REGARD TO IRREGULARITY OF BIDS.
2. Items classified on the bid schedule as "ALTERNATE ITEMS" and/or "ALTERNATE TYPES OF CONSTRUCTION" must be preselected and indicated on bids. However, "Alternate Types of Construction" may include Optional Items to be treated as set out in Paragraph 1, above.
3. Optional items not preselected and indicated on the bid schedule MUST be designated in accordance with Subsection 102.06 prior to or at the time of execution of the contract.
4. Optional and Alternate items designated must be used throughout the project.

I (We) further propose to perform all "force account or extra work" that may be required of me (us) on the basis provided in the Specifications and to give such work my (our) personal attention in order to see that it is economically performed.

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) shall submit electronically with our proposal or deliver prior to the bid opening time a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.



**SECTION 905 -- PROPOSAL (CONTINUED)**

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Respectfully Submitted,

DATE \_\_\_\_\_

\_\_\_\_\_  
Contractor

BY \_\_\_\_\_  
Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

\_\_\_\_\_  
President Address

\_\_\_\_\_  
Secretary Address

\_\_\_\_\_  
Treasurer Address

The following is my (our) itemized proposal.

Mill & Overlay approximately 6 miles of SR 39 from SR 21 to SR 145, & approximately 2 miles of SR 21 from 1 mile West of SR 39 to SR 39, known as State Project Nos. MP-5000-52(093) / 305744301 & 302 in Noxubee County.

Line no.	Item Code	Adj Code	Quantity	Units	Description[Fixed Unit Price]
<b>Roadway Items</b>					
0010	202-B004		1,467	Square Yard	Removal of Asphalt Driveways, All Depths
0020	202-B006		150	Square Yard	Removal of Asphalt Paved Shoulders, All Depths
0030	202-B240		300	Linear Feet	Removal of Traffic Stripe
0040	304-A006	(GY)	400	Cubic Yard	Granular Material, LVM, Class 5, Group E
0050	403-A002	(BA1)	9,150	Ton	12.5-mm, MT, Asphalt Pavement
0060	403-B002	(BA1)	100	Ton	12.5-mm, MT, Asphalt Pavement, Leveling
0070	406-A002		80,000	Square Yard	Cold Milling of Bituminous Pavement, All Depths
0080	407-A001	(A2)	8,000	Gallon	Asphalt for Tack Coat
0090	618-A001		1	Lump Sum	Maintenance of Traffic
0100	619-A1001		22	Mile	Temporary Traffic Stripe, Continuous White
0110	619-A2001		17	Mile	Temporary Traffic Stripe, Continuous Yellow
0120	619-A4002		5	Mile	Temporary Traffic Stripe, Skip Yellow
0130	619-A5001		4,072	Linear Feet	Temporary Traffic Stripe, Detail
0140	619-A6001		944	Square Feet	Temporary Traffic Stripe, Legend
0150	619-D1001		407	Square Feet	Standard Roadside Construction Signs, Less than 10 Square Feet
0160	619-D2001		336	Square Feet	Standard Roadside Construction Signs, 10 Square Feet or More
0170	619-G4001		20	Linear Feet	Barricades, Type III, Double Faced
0180	620-A001		1	Lump Sum	Mobilization
0190	626-C002		11	Mile	6" Thermoplastic Double Drop Edge Stripe, Continuous White
0200	626-D001		3	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip Yellow
0210	626-E001		9	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow
0220	626-G004		3,051	Linear Feet	Thermoplastic Double Drop Detail Stripe, White
0230	626-G005		994	Linear Feet	Thermoplastic Double Drop Detail Stripe, Yellow
0240	626-H001		940	Square Feet	Thermoplastic Double Drop Legend, White
0250	627-J001		550	Each	Two-Way Clear Reflective High Performance Raised Markers
0260	627-L001		550	Each	Two-Way Yellow Reflective High Performance Raised Markers
0270	630-F006		16	Each	Delineators, Guard Rail, White
0280	907-899-A001		1	Lump Sum	Railway-Highway Provisions

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

**CONDITIONS FOR COMBINATION BID**

If a bidder elects to submit a combined bid for two or more of the contracts listed for this month's letting, the bidder must complete and execute these sheets of the proposal in each of the individual proposals to constitute a combination bid. In addition to this requirement, each individual contract shall be completed, executed and submitted in the usual specified manner.

Failure to execute this Combination Bid Proposal in each of the contracts combined will be just cause for each proposal to be received and evaluated as a separate bid.

It is understood that the Mississippi Transportation Commission not only reserves the right to reject any and all proposals, but also the right to award contracts upon the basis of lowest separate bids or combination bids most advantageous to the State.

It is further understood and agreed that the Combination Bid Proposal is for comparison of bids only and that each contract shall operate in every respect as a separate contract in accordance with its proposal and contract documents.

I (We) agree to complete each contract on or before its specified completion date.

\*\*\*\*\*

**COMBINATION BID PROPOSAL**

This proposal is tendered as one part of a Combination Bid Proposal utilizing option \_\_\_\* of Subsection 102.11 on the following contracts:

\* Option to be shown as either (a), (b), or (c).

	<u>Project No.</u>	<u>County</u>	<u>Project No.</u>	<u>County</u>
1.	_____	_____	6.	_____
2.	_____	_____	7.	_____
3.	_____	_____	8.	_____
4.	_____	_____	9.	_____
5.	_____	_____	10.	_____

(a) If Combination A has been selected, your Combination Bid is complete.

(b) If Combination B has been selected, then complete the following page.

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
1.					
2.					
3.					
4.					
5.					
6.					
7.					
8.					

For Informational Purposes Only

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
9.	_____	_____	_____	_____	_____
10.	_____	_____	_____	_____	_____

(c) If Combination C has been selected, then initial and complete ONE of the following.

\_\_\_\_\_ I (We) desire to be awarded work not to exceed a total monetary value of \$ \_\_\_\_\_.

\_\_\_\_\_ I (We) desire to be awarded work not to exceed \_\_\_\_\_ number of contracts.



TO: EXECUTIVE DIRECTOR, MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
JACKSON, MISSISSIPPI

**CERTIFICATE**

If awarded this contract, I (we) contemplate that portions of the contract will be sublet. I (we) certify that those subcontracts which are equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

I (we) agree that this notification of intent DOES NOT constitute APPROVAL of the subcontracts.

_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)

NOTE: Failure to complete the above DOES NOT preclude subsequent subcontracts. Subsequent subcontracts, if any, equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

Contractor \_\_\_\_\_

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**  
**CERTIFICATION**

I, \_\_\_\_\_,  
(Name of person signing bid)

individually, and in my capacity as \_\_\_\_\_ of  
(Title of person signing bid)

\_\_\_\_\_  
(Name of Firm, partnership, or Corporation)

do hereby certify under penalty of perjury under the laws of the United States and the State of Mississippi

that \_\_\_\_\_, Bidder  
(Name of Firm, Partnership, or Corporation)

on Project No. **MP-5000-52(093)/ 305744301000 & MP-5000-52(093)/ 305744302000**

in **Noxubee** \_\_\_\_\_ County(ies), Mississippi, has not either directly or indirectly entered into any agreement, participated in any collusion; or otherwise taken any action in restraint of free competitive bidding in connection with this contract; nor have any of its corporate officers or principal owners.

Except as noted hereafter, it is further certified that said legal entity and its corporate officers, principal owners, managers, auditors and others in a position of administering federal funds are not currently under suspension, debarment, voluntary exclusion or determination of ineligibility; nor have a debarment pending; nor been suspended, debarred, voluntarily excluded or determined ineligible within the past three years by the Mississippi Transportation Commission, the State of Mississippi, any other State or a federal agency; nor been indicted, convicted or had a civil judgment rendered by a court of competent jurisdiction in any matter involving fraud or official misconduct within the past three years.

Do exceptions exist and are made a part thereof?            Yes / No

Any exceptions shall address to whom it applies, initiating agency and dates of such action.

Note: Exceptions will not necessarily result in denial of award but will be considered in determining bidder responsibility. Providing false information may result in criminal prosecution or administrative sanctions.

All of the foregoing is true and correct.

(1/2016 S)

SECTION 902

CONTRACT FOR MP-5000-52(093)/ 305744301000 & MP-5000-52(093)/ 305744302000

LOCATED IN THE COUNTY(IES) OF Noxubee

STATE OF MISSISSIPPI,  
COUNTY OF HINDS

This contract entered into by and between the Mississippi Transportation Commission on one hand, and the undersigned contractor, on the other witnesseth;

That, in consideration of the payment by the Mississippi Transportation Commission of the prices set out in the proposal hereto attached, to the undersigned contractor, such payment to be made in the manner and at the time of times specified in the specifications and the special provisions, if any, the undersigned contractor hereby agrees to accept the prices stated in the proposal in full compensation for the furnishing of all materials and equipment and the executing of all the work contemplated in this contract.

It is understood and agreed that the advertising according to law, the Advertisement, the instructions to bidders, the proposal for the contract, the specifications, the revisions of the specifications, the special provisions, and also the plans for the work herein contemplated, said plans showing more particularly the details of the work to be done, shall be held to be, and are hereby made a part of this contract by specific reference thereto and with like effect as if each and all of said instruments had been set out fully herein in words and figures.

It is further agreed that for the same consideration the undersigned contractor shall be responsible for all loss or damage arising out of the nature of the work aforesaid; or from the action of the elements and unforeseen obstructions or difficulties which may be encountered in the prosecution of the same and for all risks of every description connected with the work, exceptions being those specifically set out in the contract; and for faithfully completing the whole work in good and workmanlike manner according to the approved Plans, Specifications, Special Provisions, Notice(s) to Bidders and requirements of the Mississippi Department of Transportation.

It is further agreed that the work shall be done under the direct supervision and to the complete satisfaction of the Executive Director of the Mississippi Department of Transportation, or his authorized representatives, and when Federal Funds are involved subject to inspection at all times and approval by the Federal Highway Administration, or its agents as the case may be, or the agents of any other Agency whose funds are involved in accordance with those Acts of the Legislature of the State of Mississippi approved by the Governor and such rules and regulations issued pursuant thereto by the Mississippi Transportation Commission and the authorized Federal Agencies.

The Contractor agrees that all labor as outlined in the Special Provisions may be secured from list furnished by

It is agreed and understood that each and every provision of law and clause required by law to be inserted in this contract shall be deemed to be inserted herein and this contract shall be read and enforced as though it were included herein, and, if through mere mistake or otherwise any such provision is not inserted, then upon the application of either party hereto, the contract shall forthwith be physically amended to make such insertion.

The Contractor agrees that he has read each and every clause of this Contract, and fully understands the meaning of same and that he will comply with all the terms, covenants and agreements therein set forth.

Witness our signatures this the \_\_\_ day of \_\_\_\_\_, \_\_\_\_\_.

\_\_\_\_\_  
Contractor(s)

By \_\_\_\_\_

MISSISSIPPI TRANSPORTATION COMMISSION

Title \_\_\_\_\_

By \_\_\_\_\_

Signed and sealed in the presence of:  
(names and addresses of witnesses)

Executive Director

\_\_\_\_\_

\_\_\_\_\_

Secretary to the Commission

Award authorized by the Mississippi Transportation Commission in session on the \_\_\_ day of \_\_\_\_\_, \_\_\_\_\_, Minute Book No. \_\_\_\_\_, Page No. \_\_\_\_\_.

Revised 8/06/2003



**SECTION 903**  
**PERFORMANCE AND PAYMENT BOND**

CONTRACT BOND FOR: MP-5000-52(093)/ 305744301000 & MP-5000-52(093)/ 305744302000

LOCATED IN THE COUNTY(IES) OF: Noxubee

STATE OF MISSISSIPPI,  
COUNTY OF HINDS

Know all men by these presents: that we, \_\_\_\_\_  
\_\_\_\_\_  
(Contractor)

Principal, a \_\_\_\_\_

residing at \_\_\_\_\_ in the State of \_\_\_\_\_

and \_\_\_\_\_

(Surety)  
residing at \_\_\_\_\_ in the State of \_\_\_\_\_,

authorized to do business in the State of Mississippi, under the laws thereof, as surety, effective as of the contract date

shown below, are held and firmly bound unto the State of Mississippi in the sum of \_\_\_\_\_

\_\_\_\_\_ Dollars, lawful money of the United States of America, to be paid to it for which  
payment well and truly to be made, we bind ourselves, our heirs, administrators, successors, or assigns jointly and  
severally by these presents.

The conditions of this bond are such, that whereas the said \_\_\_\_\_

principal, has (have) entered into a contract with the Mississippi Transportation Commission, bearing the date of

\_\_\_\_\_ day of \_\_\_\_\_ A.D. \_\_\_\_\_ hereto annexed, for the construction of certain projects(s) in

the State of Mississippi as mentioned in said contract in accordance with the Contract Documents therefor, on file in the

offices of the Mississippi Department of Transportation, Jackson, Mississippi.

Now therefore, if the above bounden \_\_\_\_\_

in all things shall stand to and abide by and well and truly observe, do keep and perform all and singular the terms, covenants, conditions, guarantees and agreements in said contract, contained on his (their) part to be observed, done, kept and performed and each of them, at the time and in the manner and form and furnish all of the material and equipment specified in said contract in strict accordance with the terms of said contract which said plans, specifications and special provisions are included in and form a part of said contract and shall maintain the said work contemplated until its final completion and acceptance as specified in Subsection 109.11 of the approved specifications, and save harmless said Mississippi Transportation Commission from any loss or damage arising out of or occasioned by the negligence, wrongful or criminal act, overcharge, fraud, or any other loss or damage whatsoever, on the part of said principal (s), his (their) agents, servants, or employees in the performance of said work or in any manner connected therewith, and shall be liable and responsible in a civil action instituted by the State at the instance of the Mississippi Transportation Commission or any officer of the State authorized in such cases, for double any amount in money or property, the State may lose or be overcharged or otherwise defrauded of, by reason of wrongful or criminal act, if any, of the Contractor(s), his (their) agents or employees, and shall promptly pay the said agents, servants and employees and all persons furnishing labor, material, equipment or supplies therefor, including premiums incurred, for Surety Bonds, Liability Insurance, and Workmen's Compensation Insurance; with the additional obligation that such Contractor shall promptly make payment of all taxes, licenses, assessments, contributions, damages,

any liquidated damages which may arise prior to any termination of said principal's contract, any liquidated damages which may arise after termination of the said principal's contract due to default on the part of said principal, penalties and interest thereon, when and as the same may be due this state, or any county, municipality, board, department, commission or political subdivision: in the course of the performance of said work and in accordance with Sections 31-5-51 et seq. Mississippi Code of 1972, and other State statutes applicable thereto, and shall carry out to the letter and to the satisfaction of the Executive Director of the Mississippi Department of Transportation, all, each and every one of the stipulations, obligations, conditions, covenants and agreements and terms of said contract in accordance with the terms thereof and all of the expense and cost and attorney's fee that may be incurred in the enforcement of the performance of said contract, or in the enforcement of the conditions and obligations of this bond, then this obligation shall be null and void, otherwise to be and remain in full force and virtue.

_____	_____
(Contractors) Principal	Surety
By _____	By _____
	(Signature) Attorney in Fact
	Address _____
	_____
Title _____	_____
(Contractor's Seal)	(Printed) MS Agent
	_____
	(Signature) MS Agent
	Address _____
	_____
	_____
	(Surety Seal)
	_____
	Mississippi Insurance ID Number



# BID BOND

KNOW ALL MEN BY THESE PRESENTS, that we \_\_\_\_\_  
Contractor

\_\_\_\_\_  
Address

\_\_\_\_\_  
City, State ZIP

As principal, hereinafter called the Principal, and \_\_\_\_\_  
Surety

a corporation duly organized under the laws of the state of \_\_\_\_\_

as Surety, hereinafter called the Surety, are held and firmly bound unto **State of Mississippi, Jackson, Mississippi**

As Obligee, hereinafter called Obligee, in the sum of **Five Per Cent (5%) of Amount Bid**

Dollars(\$ \_\_\_\_\_ )

for the payment of which sum will and truly to be made, the said Principal and said Surety, bind ourselves, our heirs, executors, administrators, successors and assigns, jointly and severally, firmly by these presents.

WHEREAS, the Principal has submitted a bid for **Mill & Overlay approximately 6 miles of SR 39 from SR 21 to SR 145, & approximately 2 miles of SR 21 from 1 mile West of SR 39 to SR 39, known as State Project Nos. MP-5000 -52(093) / 305744301 & 302 in Noxubee County.**

NOW THEREFORE, the condition of this obligation is such that if the aforesaid Principal shall be awarded the contract, the said Principal will, within the time required, enter into a formal contract and give a good and sufficient bond to secure the performance of the terms and conditions of the contract, then this obligation to be void; otherwise the Principal and Surety will pay unto the Obligee the difference in money between the amount of the bid of the said Principal and the amount for which the Obligee legally contracts with another party to perform the work if the latter amount be in excess of the former, but in no event shall liability hereunder exceed the penal sum hereof.

Signed and sealed this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_

\_\_\_\_\_  
(Witness)

\_\_\_\_\_  
(Principal) (Seal)  
By: \_\_\_\_\_  
(Name) (Title)

\_\_\_\_\_  
(Witness)

\_\_\_\_\_  
(Surety) (Seal)  
By: \_\_\_\_\_  
(Attorney-in-Fact)

\_\_\_\_\_  
(MS Agent)

\_\_\_\_\_  
Mississippi Insurance ID Number

