



SM No. CSP0047010161

PROPOSAL AND CONTRACT DOCUMENTS

FOR THE CONSTRUCTION OF

01

Emergency Bridge Repair on SR 14, Bridge Nos. 11693 & 14407, known as State Project No. SP-0047-01(016) / 107693301 in Sharkey County.

Project Completion: 11/17/2017

(STATE DELEGATED)

NOTICE

**BIDDERS MUST COMPLETE AN ONLINE REQUEST
FOR PERMISSION TO BID TO BE ELIGIBLE TO BID
THIS PROJECT.**

Electronic addendum updates will be posted on www.gomdot.com

SECTION 900

OF THE CURRENT

2017 STANDARD SPECIFICATIONS

FOR ROAD AND BRIDGE CONSTRUCTION

JACKSON, MISSISSIPPI

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

08/30/2017 07:50 AM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 901 - ADVERTISEMENT

Electronic bids will be received by the Mississippi Transportation Commission at 10:00 o'clock A.M., Thursday, September 7, 2017, from Bid Express Service and shortly thereafter publicly opened in the Construction Division for:

Emergency Bridge Repair on SR 14, Bridge Nos. 11693 & 14407, known as State Project No. SP-0047-01(016) / 107693301 in Sharkey County.

The attention of bidders is directed to the predetermined minimum wage rate set by the U.S. Department of Labor under the Fair Labor Standards Act. The Mississippi Department of Transportation hereby notifies all bidders that it will affirmatively insure that in any contract entered into pursuant to this advertisement, disadvantaged business enterprises will be afforded full opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of race, color, sex, age, disability, religion or national origin in consideration for an award.

The specifications are on file in the offices of the Mississippi Department of Transportation.

Plans and bid proposals may be viewed and downloaded online at no cost at the <http://mdot.ms.gov> website. All Contractors may complete the online request to be added to the Eligible Bidders List and upon approval shall be eligible to submit a bid using Bid Express at <http://bidx.com>.

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

MELINDA L. MCGRATH
EXECUTIVE DIRECTOR

(EMG ST)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Governing Specifications

The current (2017) Edition of the Standard Specifications for Road and Bridge Construction adopted by the Mississippi Transportation Commission is made a part hereof fully and completely as if it were attached hereto, except where superseded by special provisions, or amended by revisions of the Specifications contained within this proposal. Copies of the specification book may be purchased from the MDOT Construction Division, or online at shopmdot/default.aspx?StoreIndex=1.

A reference in any contract document to controlling requirements in another portion of the contract documents shall be understood to apply equally to any revision or amendment thereof included in the contract.

In the event the plans or proposal contain references to the 2004 Edition of the Standard Specifications for Road and Bridge Construction, it is to be understood that such references shall mean the comparable provisions of the 2017 Edition of the Standard Specifications.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Status of Right-of-Way

Although it is desirable to have acquired all rights-of-way and completed all railroad agreements, utility adjustments and work to be performed by others prior to receiving bids, sometimes it is not considered to be in the public interest to wait until each and every such clearance has been obtained. The bidder is hereby advised of possible unacquired rights-of-way, relocations, railroad agreements and utilities adjustments which have not been completed.

The status of right-of-way acquisition, utility adjustments, encroachments, potentially contaminated sites, railroad facilities, improvements, and asbestos contamination are set forth in the following attachments.

In the event right of entry is not available to ALL parcels of right-of-way and/or all work that is to be accomplished by others on the date set forth in the contract for the Notice to Proceed is not complete, the Department will issue a restricted Notice to Proceed.

STATUS OF RIGHT-OF-WAY

SP-0047-01(016)

107693-301000

Sharkey County

All rights of way and legal rights of entry have been acquired **except:**

None.

Dick Hall
Central District Commissioner
J. Kevin Magee
District 3 Engineer

P. O. Box 630
Yazoo City, MS 39194-0630
Telephone (662) 746-2513
FAX (662) 746-3672



Melinda L. McGrath
Executive Director

James A. Williams, III
Deputy Executive Director/Chief Engineer

Lisa M. Hancock
Deputy Executive Director/Administration

GoMDOT.com

Memorandum

Date: August 29th, 2017
To: Right of Way Division
Trudi Loflin
From: Pre-Construction Engineer District Three
Valentino DeVellis ✓
Re: Project # 107693/301000
SP-0047-01(016)
SR 14 BR# 11693 and 14407
Sharkey County

ENCROACHMENT CERTIFICATION

This is to certify that the above captioned project has been inspected, and no encroachments were found.

VD:vd

pc: Charlie Milner (District Three)
Leroy Crisco (Construction Division)
Katherine Lamey (Construction Division)
Ann Russell (Right of Way)
File

Dick Hall
Central District Commissioner
J. Kevin Magee
District 3 Engineer

P. O. Box 630
Yazoo City, MS 39194-0630
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Re: Project # 107693/301000
SP-0047-01(016)
SR 14 BR# 11693 and 14407
Sharkey County

UTILITY STATUS REPORT

This is to certify that the above captioned project has been inspected, and there are no known utilities in conflict with the project.

VD:vd

pc: Charlie Milner (District Three)
Ann Russell (Right of Way)
Leroy Crisco (Construction Division)
Katherine Lamey (Construction Division)
File

ASBESTOS CONTAMINATION STATUS OF BUILDINGS
TO BE REMOVED BY THE CONTRACTOR

SP-0047-01(016)
107693 - 301000
Sharkey County
August 29, 2017

Reference is made to notices to bidders entitled "Asbestos National Emission Standards for Hazardous Air Pollutants (NESHAP)" and "Removal of Obstructions".

The following pertinent information is furnished concerning asbestos containing materials (ACMs), if any, found in buildings to be removed by the Contractor.

There is no Right of Way required for this project. There are no buildings to be removed by the contractor.

STATUS OF POTENTIALLY CONTAMINATED SITES

SP-0047-01(016)

107693-301000

Sharkey County

August 29, 2017

THERE IS NO RIGHT OF WAY REQUIRED FOR THIS PROJECT. NO INITIAL SITE ASSESSMENT WILL BE PERFORMED. IF CONTAMINATION ON EXISTING RIGHT OF WAY IS DISCOVERED, IT WILL BE HANDLED BY THE DEPARTMENT.

Dick Hall
Central District Commissioner
J. Kevin Magee
District 3 Engineer

P. O. Box 630
Yazoo City, MS 39194-0630
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Re: Project # 107693/301000
SP-0047-01(016)
SR 14 BR# 11693 and 14407
Sharkey County

ROW STATUS REPORT OF AFFECTED RAILROAD FACILITIES

There are no railroad facilities affected by the above referenced project.

VD:vd

pc: Charlie Milner (District Three)
Ann Russell (Right of Way)
Leroy Crisco (Construction Division)
Katherine Lamey (Construction Division)
File

Improvements to be included in Notice to Bidders to be removed by the Construction Contractor
FMS Construction Project No: 107693-301000
External ROW No: SP-0047-01(016)

Parcel No:
Station No:
Property Owner:
Description/Pictures:

NA

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 3

CODE: (SP)

DATE: 01/17/2017

SUBJECT: Final Clean-Up

Immediately prior to final inspection for release of maintenance, the Contractor shall pick up, load, transport and properly dispose of all litter from the entire highway right-of-way that is within the termini of the project.

Litter shall include, but not be limited to, solid wastes such a glass, paper products, tires, wood products, metal, synthetic materials and other miscellaneous debris.

Litter removal is considered incidental to other items of work and will not be measured for separate payment.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 8

CODE: (SP)

DATE: 07/19/2017

SUBJECT: Errata and Modifications to the 2017 Standard Specifications

<u>Page</u>	<u>Subsection</u>	<u>Change</u>
16	102.06	In the seventh full paragraph, change “Engineer” to “Director.”
33	105.05.1	In the sixth sentence, change “Contract Administration Engineer” to “Contract Administration Director.”
34	105.05.2.1	In subparagraph 2, change “SWPPP, ECP” to “SWPPP and the ECP”
35	105.05.2.2	In subparagraphs 2, add “ and” to the end of the sentence. In subparagraph 3, remove “, and” and add “.”.
90	109.04.2	In the last paragraph of subparagraph (a), place a period “.” at the end of the sentence.
93	109.04.2	In the last paragraph of subparagraph (g), place a period “.” at the end of the sentence. Also, in the first paragraph of subparagraph (h), place a period “.” at the end of the sentence.
98	109.11	In the third sentence, change “Engineer” to “Director.”
219	308.04	In the last sentence of the last paragraph, change “Contractor’s decision” to “Engineer’s decision.”
300	405.02.5.9	In the first sentence of the second paragraph, change “Hot Mix Asphalt” to “Asphalt Mixtures.”
502	630.01.1	In the first paragraph, change “ <u>AASHTO</u> ” to “ <u>AASHTO’s LRFD</u> ”.
532	642.02.6.6.2	Change the subsection number from “642.02.6.6.2” to “632.02.6.6.2”
532	642.02.6.6.2	Change “Section 661” to “Section 907-661.”
532	632.02.6.6.4	Change “Subsection 663.02.2” to “Subsection 907-663.02.2.”
554	634.05	In the description for 634-A, change “___’ Pole” to “___’ Shaft.”

- | | | |
|-----|---------------|---|
| 688 | 630.03.2 | Change the subsection number from “630.03.2” to “680.03.2.” |
| 725 | 702.08.3 | In the second sentence of the first paragraph, change “hot-mix” to “asphalt.” |
| 954 | 804.02.13.1.6 | In the definition for “M” in the % Reduction formulas, change “paragraph 7.3” to “paragraph 5.3.” |

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 9

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Federal Bridge Formula

Bidders are hereby advised that the latest revision of Federal Highway Administration Publication No. FHWA-HOP-06-105, **BRIDGE FORMULA WEIGHTS**, dated August 2006, is made a part of this contract when applicable.

Prior to the preconstruction conference, the Contractor shall advise the Engineer, in writing, what materials, if any, will be delivered to the jobsite via Interstate route(s).

Copies of the **BRIDGE FORMULA WEIGHTS** publication may be obtained by contacting:

Federal Highway Administration
400 7th Street, SW
Washington, DC 20590
(202) 366-2212

or

http://www.ops.fhwa.dot.gov/Freight/publications/brdg_frm_wgths/bridge_formula_all_rev.pdf

An on line **BRIDGE FORMULA WEIGHTS CALCULATOR** is available at

http://ops.fhwa.dot.gov/freight/sw/brdgcalc/calc_page.htm

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 12

CODE: (IS)

DATE: 03/01/2017

SUBJECT: MASH Compliant Devices

Bidders are hereby advised that the Standard Specifications may require certain traffic control and permanent safety hardware devices to meet the requirements of the Manual for Assessing Safety Hardware (MASH). However, devices meeting the requirements of NCHRP Report 350 will be allowed until the mandatory effective date for MASH compliance. The following table shows the effective dates for MASH compliant devices.

Device	Effective Date for MASH Compliance
W-beam barriers, cast-in-place concrete barriers	December 31, 2017
W-beam terminals	June 30, 2018
Cable barriers, cable barrier terminals, crash cushions	December 31, 2018
Bridge rails, transitions, all other longitudinal barriers including portable barriers installed permanently, all other terminals, sign supports, all other breakaway hardware	December 31, 2019

Temporary work zone devices, including portable barriers manufactured after December 31, 2019, must have been successfully tested to the 2016 Edition of MASH. Such devices manufactured on or before this date and successfully tested to NCHRP Report 350 or the 2009 Edition of MASH may continue to be used throughout their normal service lives.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 115

CODE: (SP)

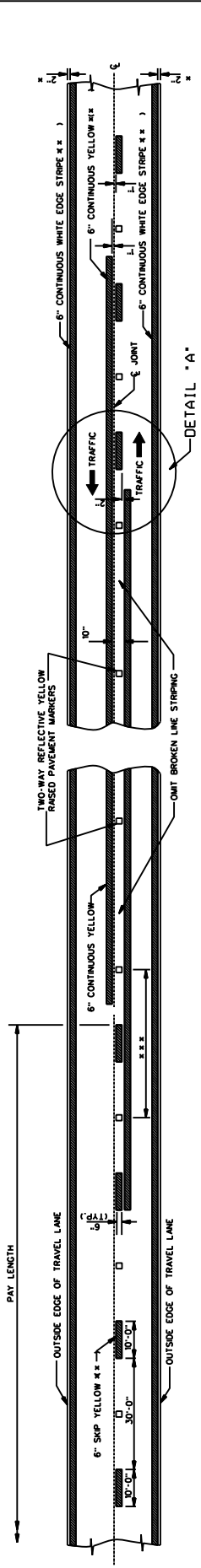
DATE: 04/19/2017

SUBJECT: Standard Drawings

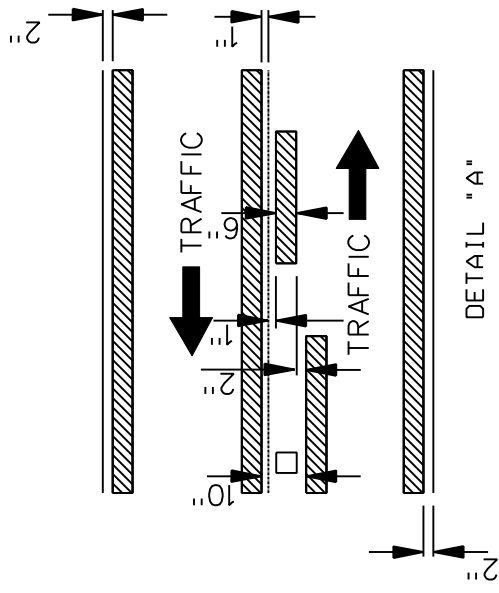
Standard Drawings attached hereto shall govern appropriate items of required work.

Larger copies of Standard Drawings may be purchased from:

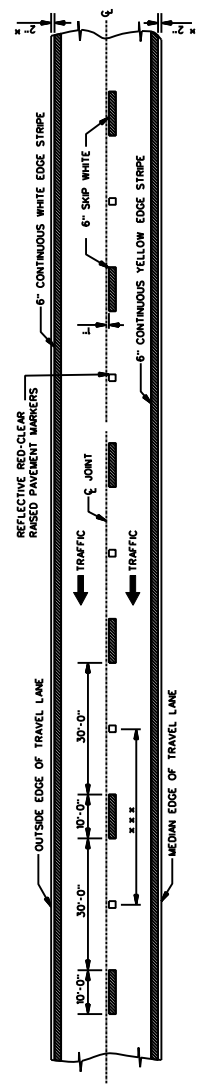
MDOT Plans Print Shop
MDOT Shop Complex, Building C, Room 114
2567 North West Street
P.O. Box 1850
Jackson, MS 39215-1850
Telephone: (601) 359-7460
or FAX: (601) 359-7461
or e-mail: plans@mdot.state.ms.us



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



DETAIL "A"



4-LANE WITH ONE-WAY TRAFFIC



GENERAL NOTES:

- * 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS.
 - ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
 - *** 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:
- | TANGENT SECTIONS | URBAN AREA (U/Lin) | RURAL AREA (U/Lin) |
|--------------------|--------------------|--------------------|
| HORIZONTAL CURVES | 40'-0" | 80'-0" |
| INTERCHANGE LIMITS | 40'-0" | 40'-0" |
| | 40'-0" | 140'-0" |
- 1. NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR BASED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP AND ENDING 1000' FROM THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - 4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED SOURCES OF MATERIALS."

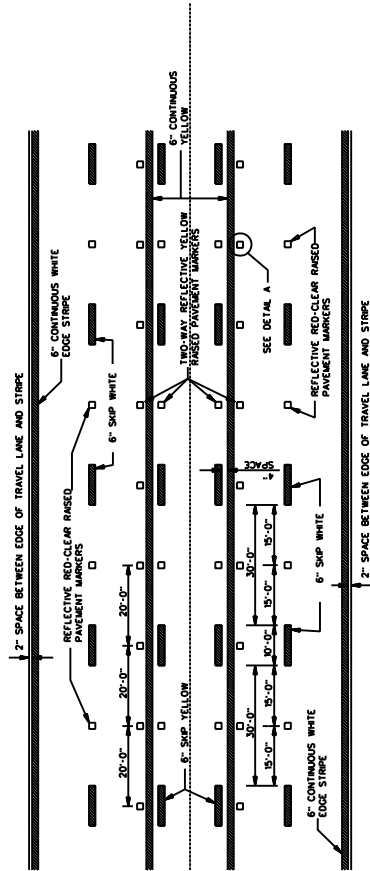
Notice To Bidders

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

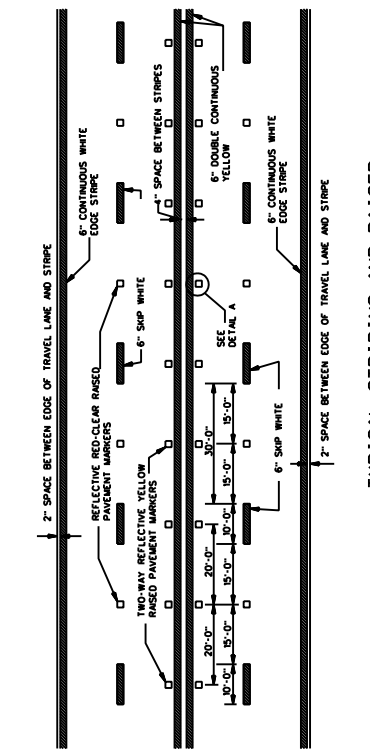
PAVEMENT MARKING
DETAILS FOR
2-LANE AND 4-LANE
DIVIDED HIGHWAYS

WORK NUMBER: PM-1
SHEET NUMBER: 100
ISSUE DATE: OCTOBER 1, 1998

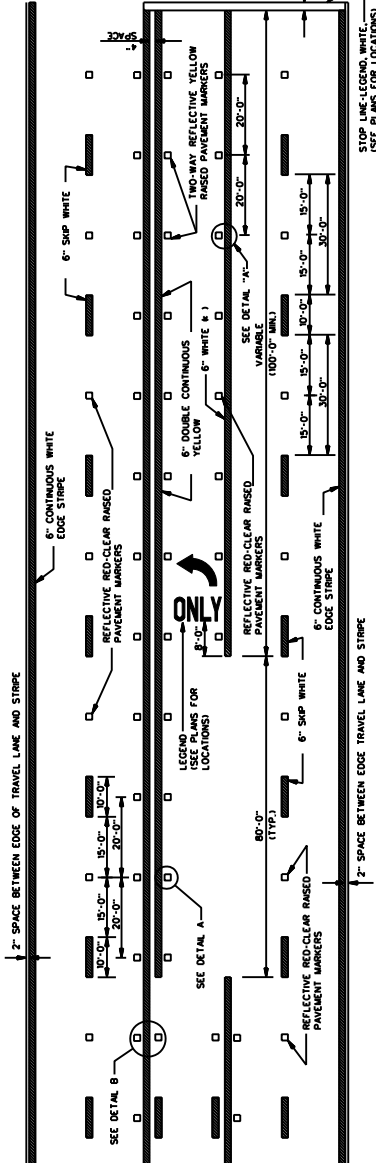
STATE	PROJECT NO.
MISS.	



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 5-LANE SECTION

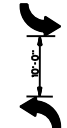


TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS AT LEFT TURN LANES

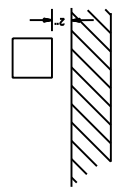
*NOTE: USE DETAIL STRIPING IF LENGTH \leq 150' AT THIS LOCATION, OTHERWISE USE CONTINUOUS STRIPING.



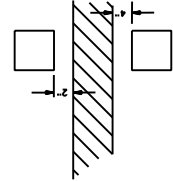
TYPICAL TWO-WAY ARROW INSTALLATION

NOTES: 1. CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY.
 2. IF SEGMENT IS LESS THAN 300', PLACE ONE SET OF ARROWS IN CENTER OF SEGMENT.
 3. IF SEGMENT IS GREATER THAN 300', PLACE FIRST SET OF ARROWS 50' TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS 1250' O.C.

GENERAL NOTE:
 1. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT -APPROVED SOURCES OF MATERIALS.*



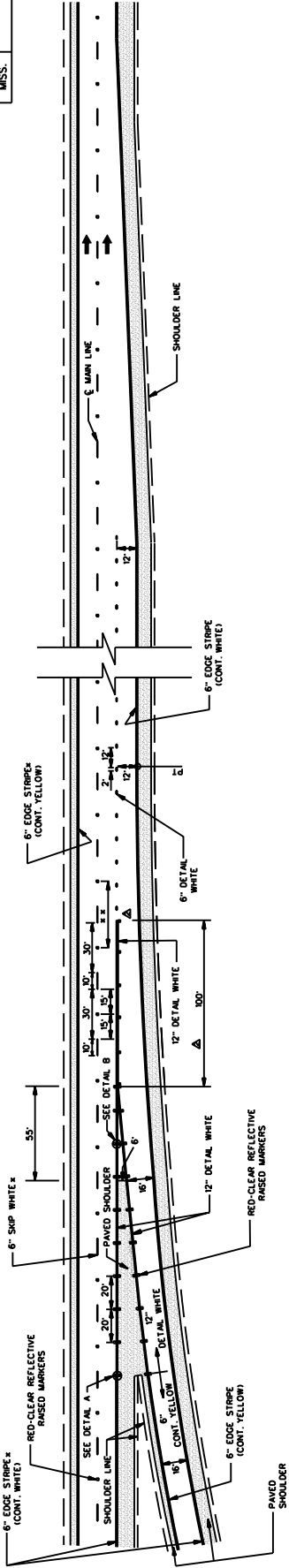
DETAIL A
 LATERAL PLACEMENT OF PAVEMENT MARKERS



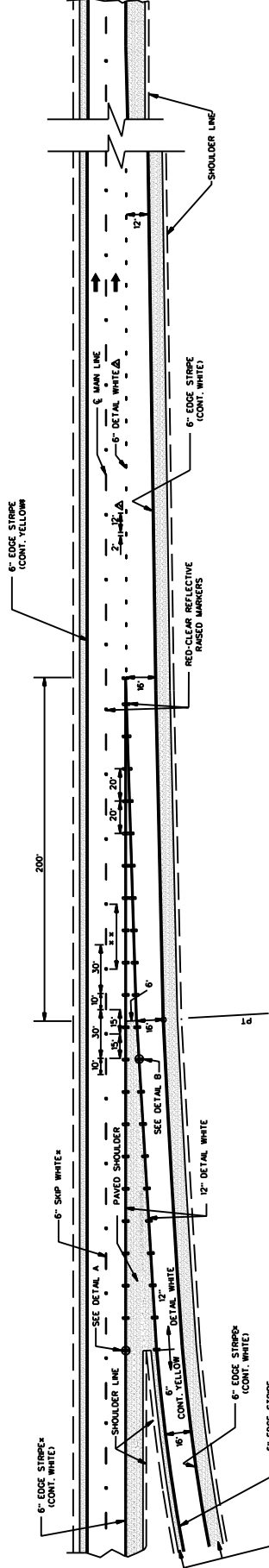
DETAIL B
 LATERAL PLACEMENT OF PAVEMENT MARKERS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
PAVEMENT MARKING	
DETAILS FOR	
4-LANE AND 5-LANE	
UNDIVIDED ROADWAYS	
DATE	03/20/12
FILE NAME	SDPM-2.DGN
DESIGNER	CH2M
CHECKER	
DATE	
PROJECT NO.	
SHEET NUMBER	
TOTAL SHEETS	

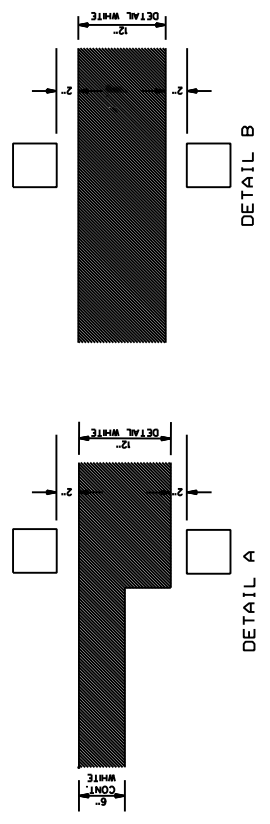
STATE	PROJECT NO.
MISS.	



PARALLEL ENTRANCE RAMP



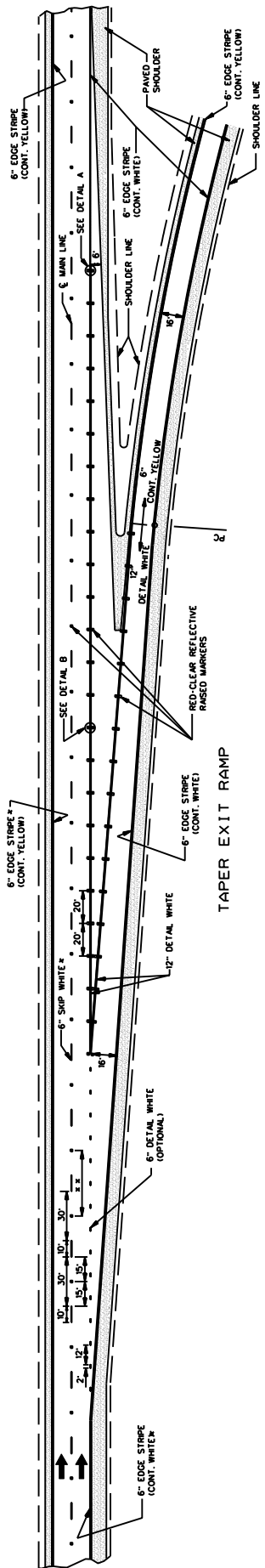
TAPER ENTRANCE RAMP



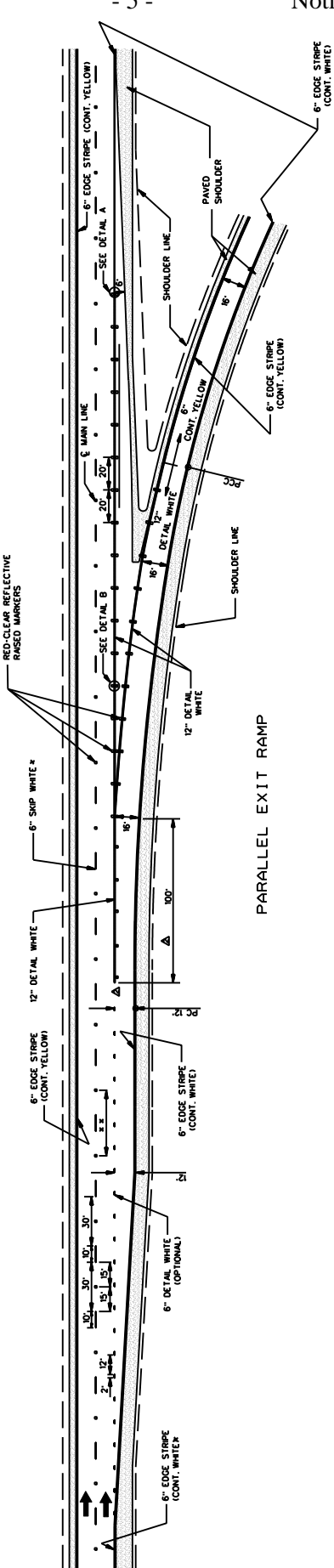
- GENERAL NOTES:
- ** 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPES AND RED-CLEAR REFLECTIVE RAISED MARKERS. THE PLACEMENT OF THE EDGE STRIPES WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELED WAY.
 - ** 2. ON THE MAIN FACILITY, PLACE REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS AT A 40' SPACING ON ALL LANE-LINES THROUGHOUT THE INTERCHANGE AREA BEGINNING 300' IN THE MAIN FACILITY FROM THE INTERCHANGE AND CONTAINING TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - 3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED SOURCES OF MATERIALS."

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION			WORKING NUMBER SDPM-3 SHEET NUMBER
PAVEMENT MARKING DETAILS FOR INTERCHANGE RAMP (PARALLEL AND TAPER)			

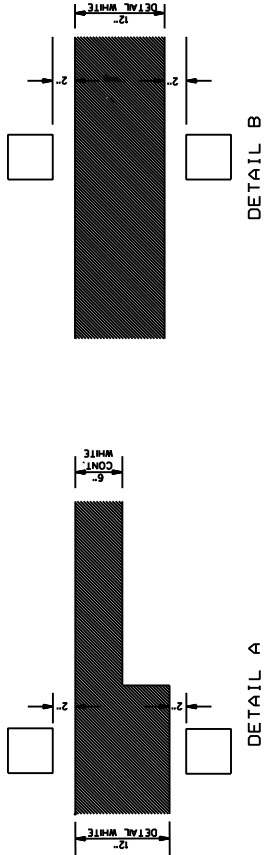
STATE	PROJECT NO.
MISS.	



TAPER EXIT RAMP



PARALLEL EXIT RAMP



- GENERAL NOTES:
- SEE SHEET PM-1 FOR THE PLACEMENT OF LINE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELLED WAY.
 - ON THE MAINLINE, PLACE REFLECTIVE RED-CLEAR RAISED MARKERS AT THE INTERCHANGE AREA BEGINNING 1000' IN ADVANCE IN DIRECTION OF TRAFFIC OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED MARKERS AS SHOWN IN THE MOOT "APPROVED SOURCES OF MATERIALS."

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION

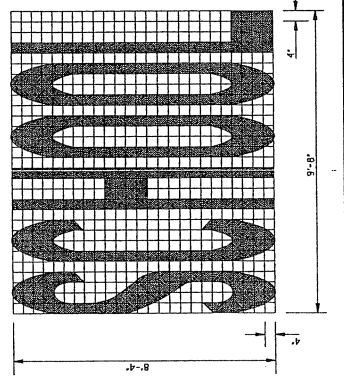
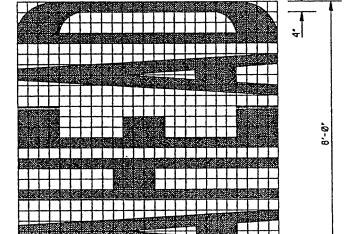
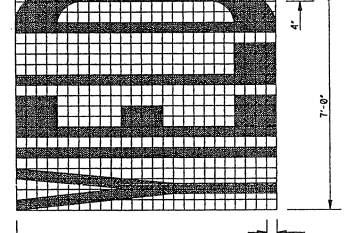
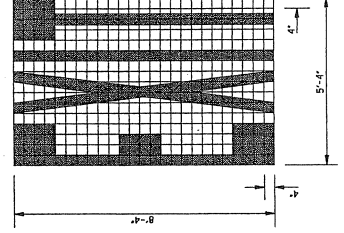
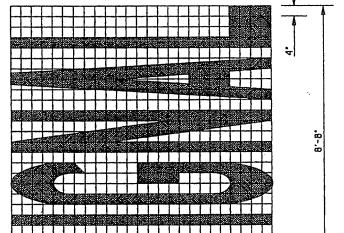
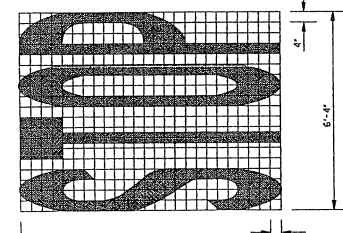
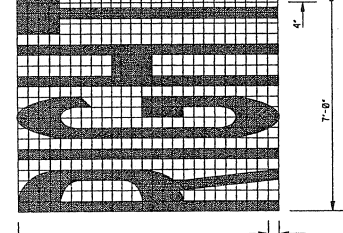
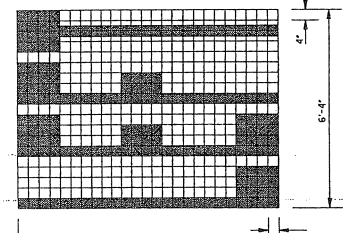
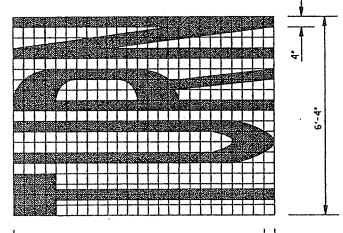
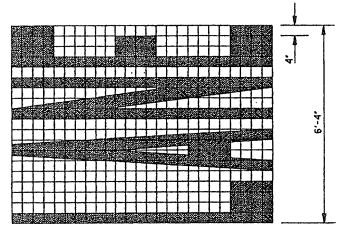
PAVEMENT MARKING
DETAILS FOR
INTERCHANGE
EXIT RAMP
(PARALLEL AND TAPER)

ISSUE DATE: OCTOBER 1, 1998

DATE	BY	DESCRIPTION
10/1/98	SMR	REVISION
10/1/98	SMR	UPDATE TO 2008 M/C/D
10/1/98	SMR	REVISION
10/1/98	SMR	REVISION

WORKING NUMBER: SDPM-41
SHEET NUMBER: 151


STATE PROJECT NO.
MISS.



- GENERAL NOTES:
1. UNLESS OTHERWISE SHOWN ON THE PLANS, ALL PAVEMENT MARKING LEGENDS SHALL BE APPLIED USING HIGH PERFORMANCE POLYURETHANE PAINT.
 2. TWO HORIZONTAL GAPS (MINIMUM BY TEMPLATE CONNECTORS OF 1/4" OR LESS AND EXTENDING 1/2" FROM EACH END) SHALL BE MAINTAINED THROUGHOUT THE LENGTH OF THE MARKING.
 3. FOR OTHER DETAILS SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 4. PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

LEGEND	AREA (FT ²)
STOP	24.6
RIGHT	35.8
LEFT	35.8
TRAFFIC	27.3
LANE	22.7
AHEAD	32.3
EXIT	18.5
SIGNAL	32.5
SCHOOL	35.5

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN



PAVEMENT MARKING LEGEND DETAILS

REVISION	
DATE	
ISSUE DATE:	OCTOBER 1, 1998
PROJECT NUMBER	PN-5
SHEET NUMBER	124

STATE PROJECT NO.					
MISS.					

TURN ARROW

THRU ARROW

COMBINATION ARROW

1-WAY ARROW

GENERAL NOTES:

1. UNLESS OTHERWISE SHOWN ON THE PLANS, ALL PAVEMENT MARKING SHALL BE APPLIED USING HIGH PERFORMANCE MATERIALS.
2. TWO HORIZONTAL GAPS CAUSED BY TEMPLATE CONNECTORS OF 1/2" OR LESS AND EXTENDING THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
3. FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM PAVEMENT MARKINGS FOR PAYMENT MARKING LEGENDS ARE AS FOLLOWS:

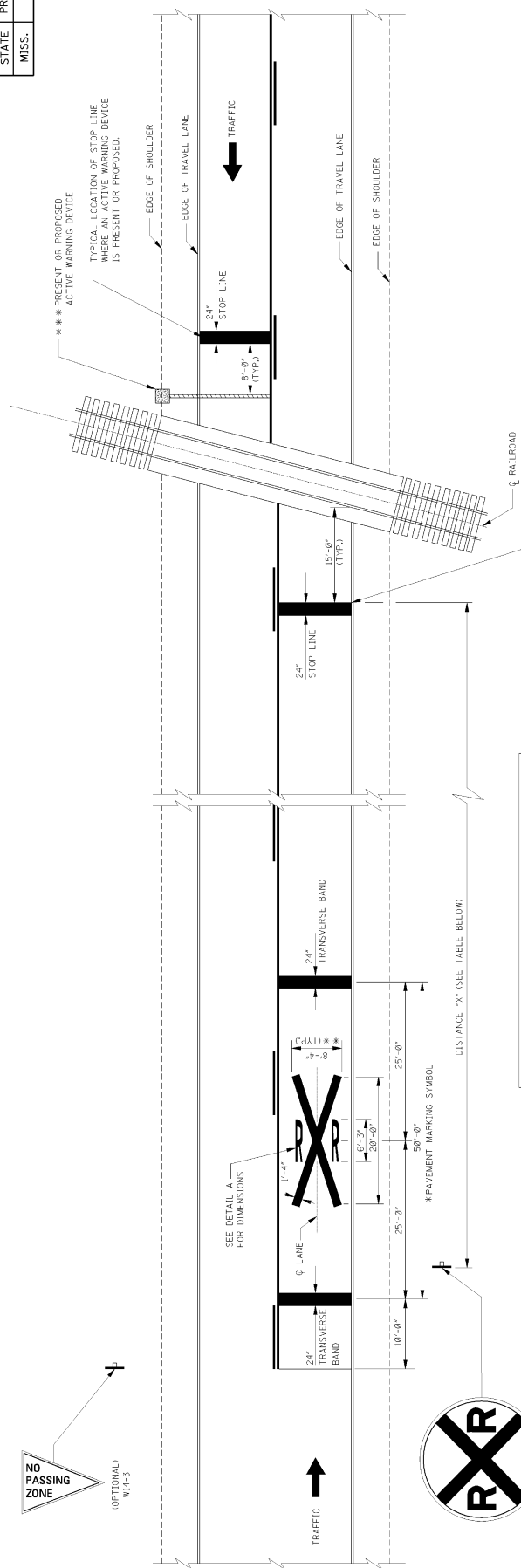
PAY QUANTITIES	
LEGEND/SYMBOL	AREA (ft ²)
ONLY	22.0
TURN ARROW	18.4
THRU ARROW	21.2
COMBINATION ARROW	27.5
1-WAY ARROW	24.3

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
LEGEND DETAILS**

	ISSUE DATE: OCTOBER 1, 1998
DATE	BY
REVISION	BY
REVISION	BY

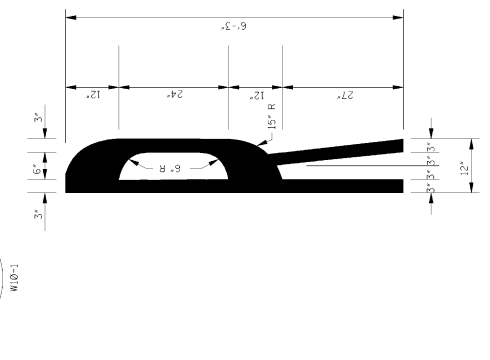
WORKING NUMBER: PM-6
SHEET NUMBER: 125



ADVANCE WARNING SIGN PLACEMENT DISTANCE

POSTED SPEED (mph)	DISTANCE "X" (FT)	
	RURAL	URBAN
20	175	100
25	250	100
30	325	100
35	400	150
40	475	225
45	550	300
50	625	375
55	700	450
60	775	525

NOTES:
 ① DISTANCE "X" MAY BE ADJUSTED, IF PROHIBITIVE PHYSICAL CONDITIONS EXIST AT THE DESIGNATED DISTANCE.
 ② THESE DISTANCES MAY BE ADJUSTED TO A MINIMUM OF 100 IN. FOR BUSINESS OR BUSINESS DISTRICTS WHERE LOW SPEEDS ARE PREVALENT.



DETAIL A
STANDARD "R" PAVEMENT MARKING
FOR R X R SYMBOL

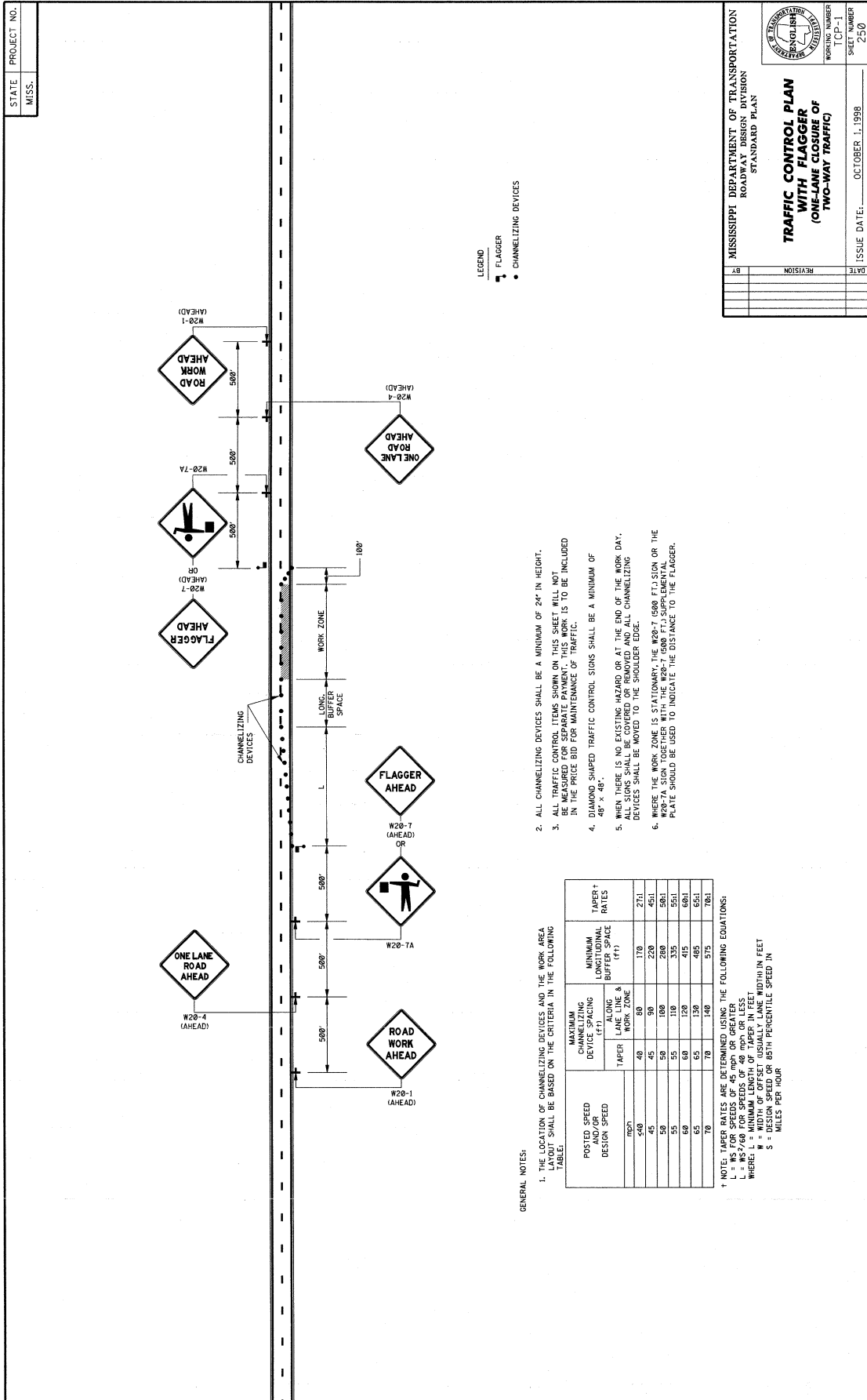
GENERAL NOTES:
 * 1. A PORTION OF THE PAVEMENT MARKING SYMBOL SHOULD BE DIRECTLY OPPOSITE THE ADVANCE WARNING SIGN (W10-1).
 * 2. WIDTH OF R X R SYMBOL MAY VARY ACCORDING TO LANE WIDTH. HOWEVER, ON MULTI-LANE ROADS, THE TRANSVERSE BANDS AND STOP LINE SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.
 * 3. R X R SYMBOL (63.0 #4²), TRANSVERSE BANDS AND STOP LINE SHALL BE PAID FOR AS LEGEND, WHITE (PLASTIC), MATERIAL OPTIONAL FOR OTHER AGENCIES.
 * 4. REFER TO THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" FOR LOCATION OF PROPOSED WARNING DEVICES AT RAILROAD-HIGHWAY GRADE CROSSINGS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TYPICAL PLACEMENT OF WARNING SIGNS AND PAVEMENT MARKINGS AT RAILROAD HIGHWAY GRADE CROSSINGS

WORKSHEET NO. 15
SHEET NUMBER 12 OF 18
ISSUE DATE: OCTOBER 11, 1998

DATE	REVISION
10-20-88	REVISE NOTE



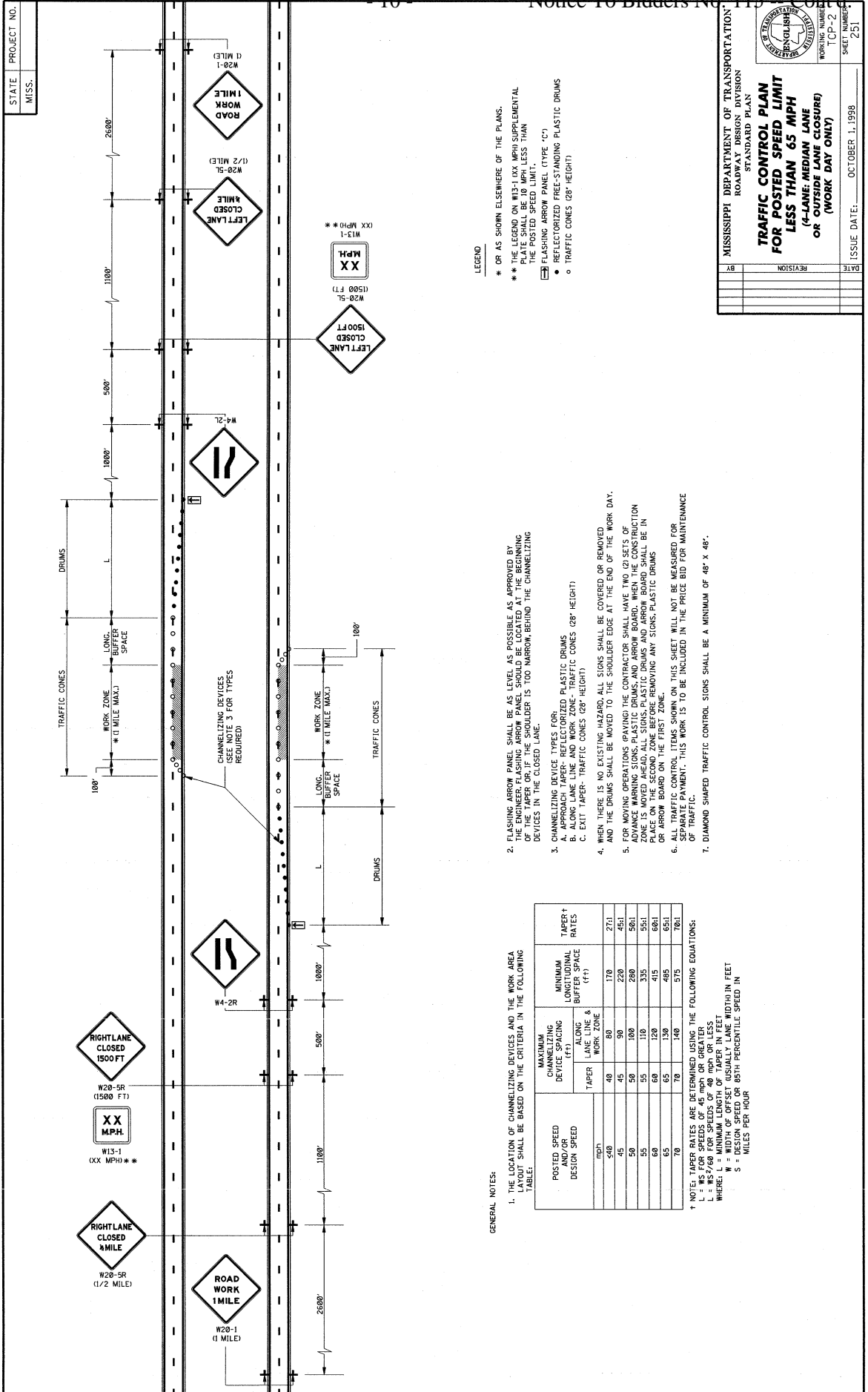
GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICES ALONG WORK ZONE		MINIMUM LONGITUDINAL BUFFER SPACE (FT)	TAPER RATES
	ALONG WORK ZONE	ALONG BUFFER SPACE		
60	40	80	170	27:1
45	45	90	220	45:1
50	50	100	280	50:1
55	55	110	335	55:1
60	60	120	415	60:1
65	65	130	485	65:1
70	70	140	575	70:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 L = WS FOR SPEEDS OF 45 MPH OR GREATER
 L = WS FOR SPEEDS OF 30 MPH OR GREATER
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

- ALL CHANNELIZING DEVICES SHALL BE A MINIMUM OF 24" IN HEIGHT.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
- DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".
- WHEN THERE IS NO EXISTING HAZARD AS AT THE END OF THE WORK DAY, DEVICES SHALL BE COVERED OR REMOVED AND ALL CHANNELIZING DEVICES SHALL BE MOVED TO THE SHOULDER EDGE.
- WHERE THE WORK ZONE IS STATIONARY THE W20-7 (600 FT.) SIGN OR THE W20-7A SIGN TOGETHER WITH THE W20-7 (600 FT.) SUPPLEMENTAL PLATE SHOULD BE USED TO INDICATE THE DISTANCE TO THE FLAGGER.



- LEGEND**
- * OR AS SHOWN ELSEWHERE OF THE PLANS.
 - ** THE LEGEND ON W13-1 (XX MPH) SUPPLEMENTAL PLATE SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT.
 - REFLECTORIZED PRE-STANDING PLASTIC DRUMS
 - TRAFFIC CONES (28" HEIGHT)

2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICE TYPES FOR:
 - A. APPROACH TAPER- REFLECTORIZED PLASTIC DRUMS
 - B. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT)
 - C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT)
4. WHEN THERE IS NO EXISTING HAZARD, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING SIGNS- PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS- PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE PREVIOUS ZONE.
6. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
7. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".

GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (FT)		MINIMUM LONGITUDINAL BUFFER SPACE (ft)	TAPER + TAPER RATES
	ALONG LANE LINE	WORK ZONE		
40	40	80	170	27:1
45	45	90	220	45:1
50	50	100	280	50:1
55	55	110	335	55:1
60	60	120	415	60:1
65	65	130	485	65:1
70	70	140	575	70:1

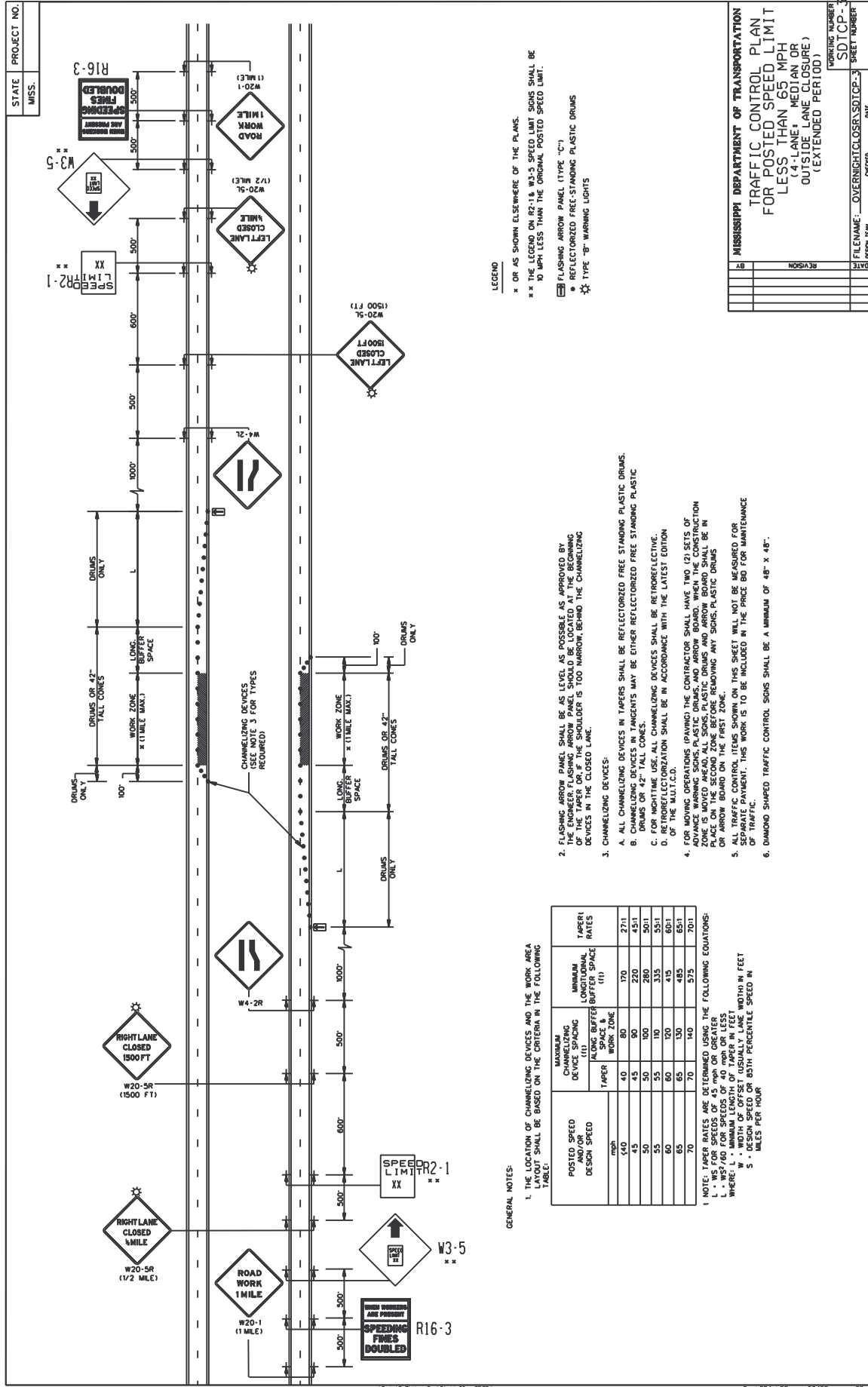
† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 $L = W^2$ FOR SPEEDS OF 45 mph OR GREATER
 $L = WS$ FOR SPEEDS OF 45 mph OR GREATER
 WHERE L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET USUALLY LANE WIDTH IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

STATE PROJECT NO. MISS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN

**TRAFFIC CONTROL PLAN
 FOR POSTED SPEED LIMIT
 LESS THAN 65 MPH
 (4-LANE MEDIAN LANE
 OR CLOSURE MEASURE)
 (WORK DAY ONLY)**

WORKING NUMBER: TCP-2
 SHEET NUMBER: 251
 ISSUE DATE: OCTOBER 1, 1998



LEGEND

- x OR AS SHOWN ELSEWHERE OF THE PLANS.
- ** THE LEGEND ON R2-1B W3-5 SPEED LIMIT SIGNS SHALL BE 40 MPH LESS THAN THE ORIGINAL POSTED SPEED LIMIT.
- ◻ FLASHING ARROW PANEL (TYPE "C")
- ◉ REFLECTORIZED FREE-STANDING PLASTIC DRUMS
- ⊛ TYPE "B" WARNING LIGHTS

2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR AT THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICES:
 - A. ALL CHANNELIZING DEVICES IN TAPERS SHALL BE REFLECTORIZED FREE STANDING PLASTIC DRUMS.
 - B. CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER REFLECTORIZED FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.
 - C. FOR NIGHTTIME USE, ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE.
 - D. RETROREFLECTORIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE M.U.T.C.D.
4. FOR MOVING OPERATIONS (TAPERING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF CHANNELIZING DEVICES IN EACH TAPER. THE CHANNELIZING DEVICES IN THE FIRST ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
5. ALL CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
6. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" x 48".

GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICES SPACING ALONG BUFFER SPACE IN TAPER		MINIMUM LONGITUDINAL BUFFER SPACE (L)	TAPER RATES
	(1)	(2)		
40	40	80	170	27:1
45	45	90	220	45:1
50	50	100	280	50:1
55	55	110	335	55:1
60	60	120	415	60:1
65	65	130	485	65:1
70	70	140	575	70:1

1. NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 L = WS FOR SPEEDS OF 45 MPH OR GREATER
 L = WS/40 FOR SPEEDS OF 40 MPH OR LESS
 WHERE:
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

STATE MISS. PROJECT NO. _____

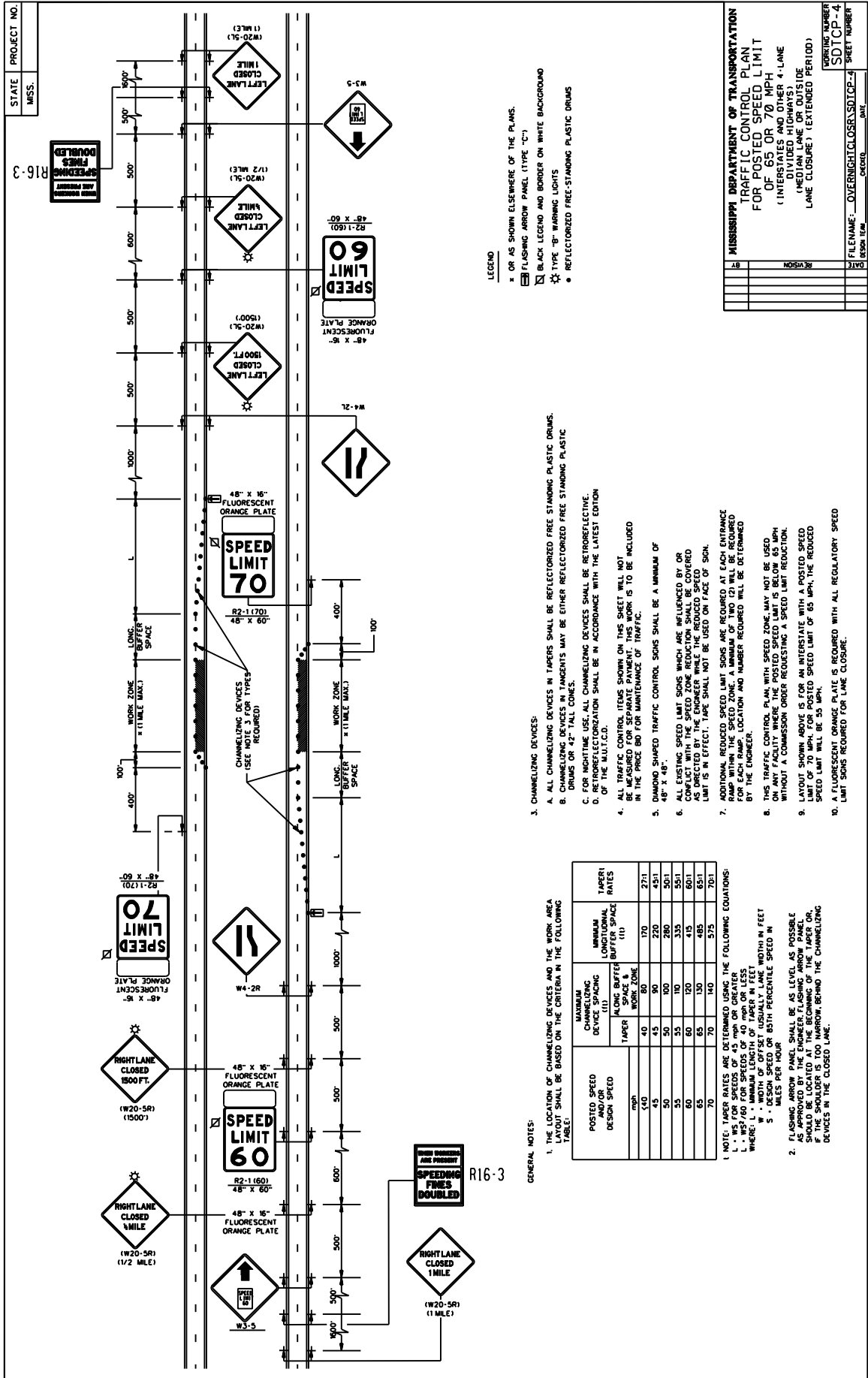
DESIGN NO. _____

DATE _____

REVISION _____

FILE NAME: OVERNIGHT/CONTRACT/CP-3

PROJECT NUMBER: SDT/CP-3



STATE PROJECT NO.
MISS. R16-3

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL PLAN
FOR POSTED SPEED LIMIT
OF 65 OR 70 MPH
(INTERSTATES AND OTHER 4-LANE
DIVIDED HIGHWAYS)
(LANE CLOSURE PERIODS)
(LANE CLOSURE EXTENDED PERIOD)

REPORTING NUMBER
SDTCP-4
DATE
DESIGN FIRM
CHECKED
DATE
FILE NAME: OVERNIGHTCLOSURESDTCP-4

- LEGEND
- OR AS SHOWN ELSEWHERE OF THE PLANS.
 - FLASHING ARROW PANEL (TYPE "C")
 - BLACK LEGEND AND BORDER ON WHITE BACKGROUND
 - TYPE "B" WARNING LIGHTS
 - REFLECTORIZED FREE-STANDING PLASTIC DRUMS

3. CHANNELIZING DEVICES:

- A. ALL CHANNELIZING DEVICES IN TAPERS SHALL BE REFLECTORIZED FREE STANDING PLASTIC DRUMS.
- B. CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER REFLECTORIZED FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.
- C. FOR NIGHTTIME USE, ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE.
- D. REFLECTORIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL.

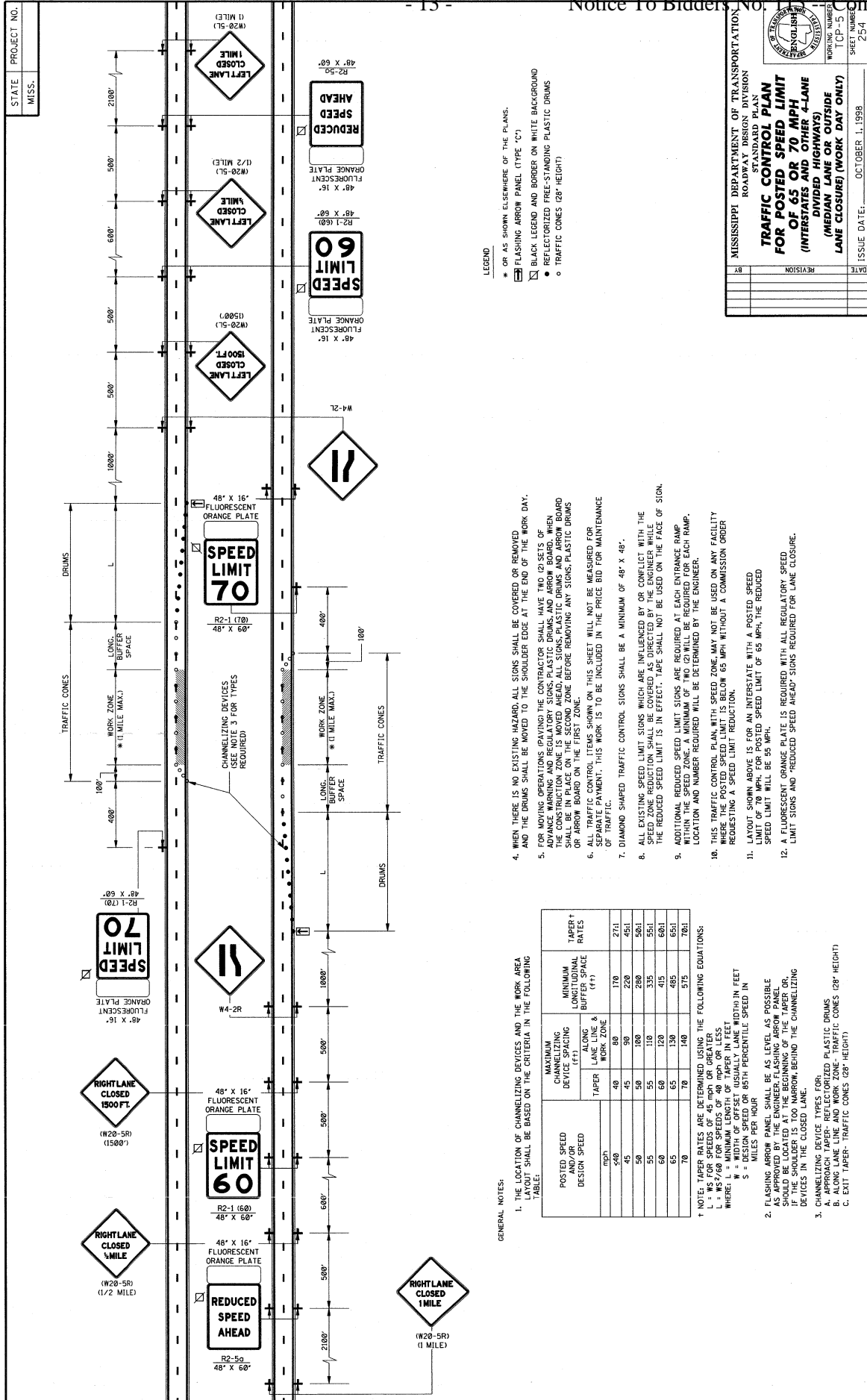
4. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE INCLUDED IN THE PRICE BIDD FOR MAINTENANCE OF TRAFFIC.

5. DYNAMIC SHARED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".
6. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON FACE OF SIGN.
7. ADDITIONAL REDUCED SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. A MINIMUM OF TWO (2) WILL BE REQUIRED FOR EACH RAMP. LOCATION AND NUMBER REQUIRED WILL BE DETERMINED BY THE ENGINEER.
8. THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE MAY NOT BE USED WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. THE REDUCED SPEED LIMIT WILL BE 55 MPH.
9. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS REQUIRED FOR LANE CLOSURE.

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICES ALONG WORK SPACE & BUFFER SPACE	MINIMUM LONGITUDINAL BUFFER SPACE RATES	
		TAPER	TANGENT
40	40	80	170
45	45	90	220
50	50	100	270
55	55	110	320
60	60	120	370
65	65	130	420
70	70	140	470

- NOTES: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 L = WS FOR SPEEDS OF 45 MPH OR GREATER
 L = WS/60 FOR SPEEDS OF 40 MPH OR LESS
 WHERE: L = MINIMUM REQUIRED BUFFER SPACE IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE AND SHOULD BE PLACED AT THE END OF THE TAPER OR SHOULD BE PLACED AT THE END OF THE TAPER OR IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.



STATE PROJECT NO.
MISS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN PLAN
STANDARD PLAN
**TRAFFIC CONTROL PLAN
FOR POSTED SPEED LIMIT
OF 65 OR 70 MPH
(INTERSTATES AND OTHER 4-LANE
DIVIDED HIGHWAYS)
(MEDIAN LANE OR OUTSIDE
LANE CLOSURE) (WORK DAY ONLY)**

WORKING NUMBER: TCP-5
SHEET NUMBER: 254
ISSUE DATE: OCTOBER 1, 1988

- LEGEND**
- OR AS SHOWN ELSEWHERE OF THE PLANS.
 - FLASHING ARROW PANEL (TYPE "C")
 - BLACK LEGEND AND BORDER ON WHITE BACKGROUND
 - REFLECTORIZED FREE-STANDING PLASTIC DRUMS
 - TRAFFIC CONES (28" HEIGHT)

4. WHEN THERE IS NO EXISTING HAZARD, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS PAYING THE CONTRACTOR SHALL HAVE TWO (2) SETS OF CHANNELIZING DEVICES (SEE NOTE 3 FOR TYPES) LOCATED AT THE BEGINNING AND END OF THE CONSTRUCTION ZONE. SIGNS, PLASTIC DRUMS, AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
6. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
7. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 48" X 48".
8. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON THE FACE OF SIGN.
9. ADDITIONAL REDUCED SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. A MINIMUM OF TWO (2) WILL BE REQUIRED FOR EACH RAMP. LOCATION AND NUMBER REQUIRED WILL BE DETERMINED BY THE ENGINEER.
10. THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
11. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.
12. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS AND "REDUCED SPEED AHEAD" SIGNS REQUIRED FOR LANE CLOSURE.

GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

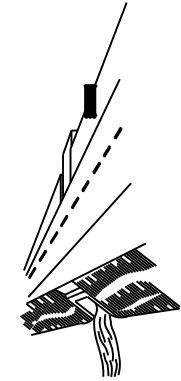
POSTED SPEED DESIGN SPEED mph	CHANNELIZING DEVICE SPACING (FT)		MINIMUM LONGITUDINAL BUFFER SPACE (FT)	TAPER + RATES
	LANE WORK ZONE	LANE & WORK ZONE		
50	40	80	170	27:1
55	45	90	220	45:1
60	50	100	280	50:1
65	55	110	335	55:1
70	60	120	415	60:1
75	65	130	485	65:1
80	70	140	575	70:1

NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:

$L = WS$ FOR SPEEDS OF 45 mph OR GREATER
 $L = WS^2/60$ FOR SPEEDS OF 40 mph OR LESS
 WHERE:
 L = LENGTH OF TAPER (USUALLY LANE WIDTH) IN FEET
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

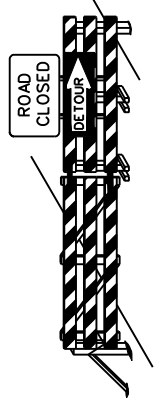
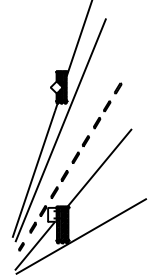
2. FLASHING ARROW PANEL SHALL BE AS LEVEL AS POSSIBLE. THE SIGN SHALL BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.

3. CHANNELIZING DEVICE TYPES FOR:
 A. APPROACH TAPER- REFLECTORIZED PLASTIC DRUMS
 B. EXIT TAPER- REFLECTORIZED PLASTIC DRUMS (28" HEIGHT)
 C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT)



WING BARRICADES

- WING BARRICADES ARE TYPE BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A WARNING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
- WING BARRICADES SHOULD BE USED:
 - IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - IN ADVANCE OF ALL BRIDGE OR CULVERT MOVING OPERATIONS.



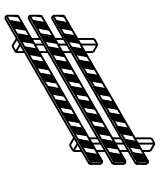
BARRICADE CLOSING A ROAD

BARRICADE CHARACTERISTICS

	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL *	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	35" MIN.	35" MIN.	60" MIN.
NUMBER OF REFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS
TYPE OF FRAME	LIGHT	LIGHT "X" FRAME	POST OR SHD

* 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.

** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED FACINGS TRAFFIC SHALL HAVE A MINIMUM OF 270 IN OF REFLECTIVE AREA.



TYPE III



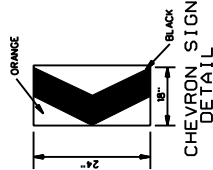
TYPE II



TYPE I

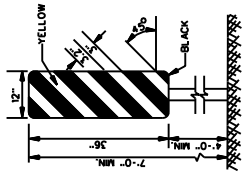
STANDARD BARRICADES

- A TYPE I BARRICADE CONSISTS OF ONE (1) HORIZONTAL RAIL SUPPORTED BY A DEMOUNTABLE FRAME. BARRICADES NORMALLY WOULD BE USED ON CONVENTIONAL ROADS OR URBAN STREETS AND ARTERIALS.
- A TYPE II BARRICADE CONSISTS OF TWO (2) HORIZONTAL RAILS ON A LIGHT "X" FRAME. TYPE II BARRICADES ARE INTENDED FOR USE WHERE THE HAZARD IS LESS SEVERE THAN THAT OF TYPE I BARRICADES. TYPE II BARRICADES ARE USED FOR CONTINUOUS DELIMITING OF A RESTRICTED ROADWAY OR FOR TEMPORARY DAYTIME USE.
- A TYPE III BARRICADE CONSISTS OF THREE (3) HORIZONTAL RAILS SUPPORTED BY FIXED POSTS, A ROAD SHD, A HEAVY DEMOUNTABLE FRAME OR A HEAVY WINGED "X" FRAME.
- TYPE I BARRICADES ARE INTENDED FOR USE ON CONSTRUCTION AND MAINTENANCE PROJECTS AS BARRICADES AND AT ROAD CLOSURES, WHERE THEY MUST REMAIN IN PLACE FOR EXTENDED PERIODS.
- THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).
- DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
- FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.



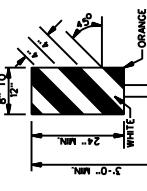
**CHEVRON SIGN
DETAIL**

- A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
- THE CHEVRON SIGN SHALL BE MOUNTED ON FIXED POST OR ROAD SHD.
- CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHALL BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.



**TYPE 3 OBJECT MARKER
(OM-3R)**

- TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
- THE OM-3R IS SHOWN. THE OM-3R IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
- THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



VERTICAL PANEL

- VERTICAL PANELS CONSIST OF AT LEAST ONE PANEL 8" TO 12" IN WIDTH AND A MINIMUM OF 24" IN HEIGHT.
- THE DIAGONAL STRIPES SHALL SLOPE DOWNWARD IN THE DIRECTION THAT TRAFFIC IS TO PASS THE PANEL. THE MINIMUM OF 35" ABOVE THE ROADWAY ON A SINGLE LIGHTMASS POST.
- VERTICAL PANELS USED ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH-SPEED ROADWAYS SHALL HAVE A MINIMUM OF 70 IN OF RETROREFLECTIVE AREA FACING TRAFFIC.
- FOR TWO-WAY TRAFFIC OPERATIONS, BACK-TO-BACK PANELS SHALL BE USED.

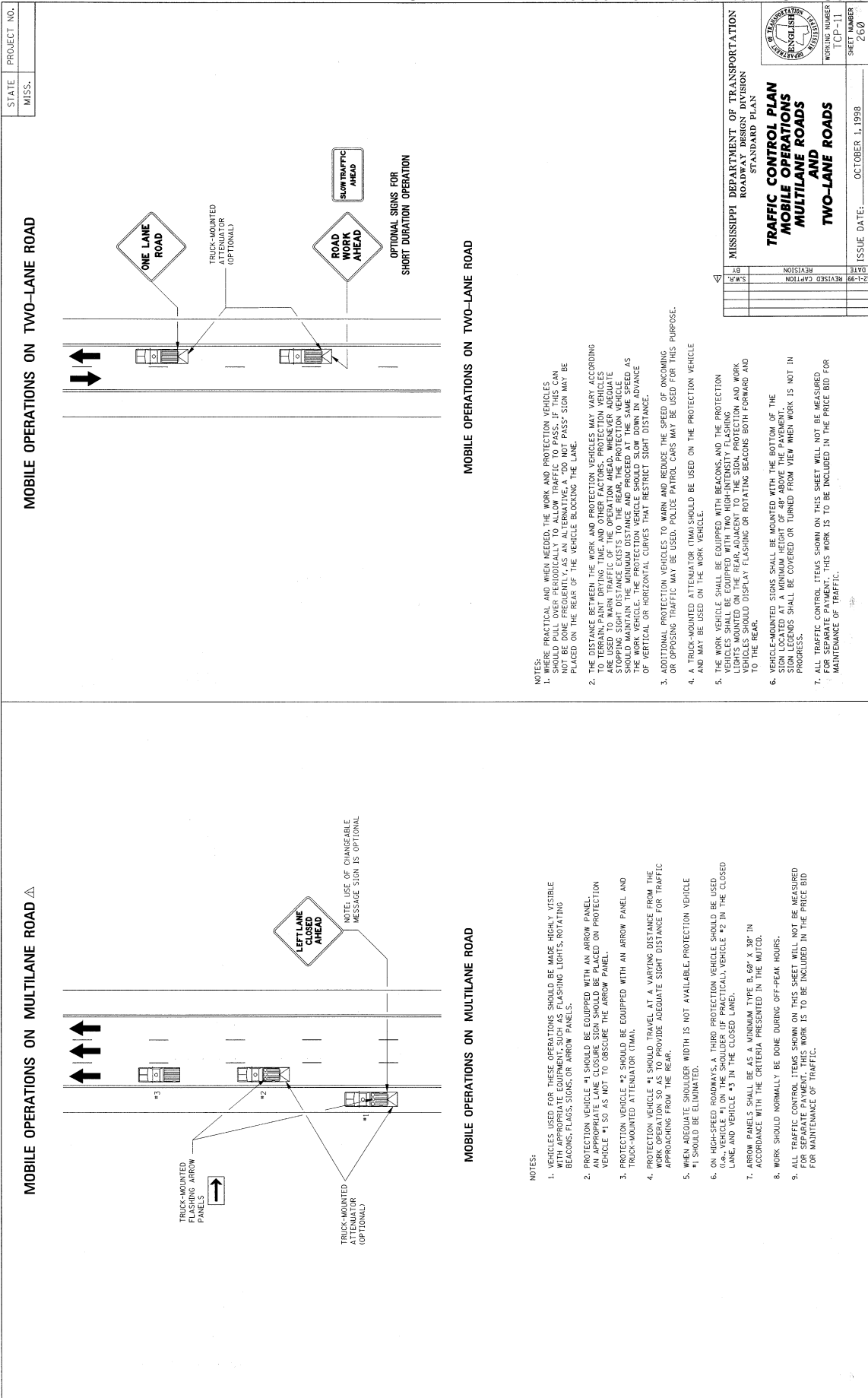
GENERAL NOTES:
1. MARKINGS ON ALL DEVICES SHOWN ON THIS SHEET SHALL BE HIGH INTENSITY REFLECTIVE SHEETING.
2. THE TRAFFIC CONTROL PLAN WILL LIST THE VARIOUS TRAFFIC CONTROL DEVICES REQUIRED FOR EACH PROJECT.

Notice To Bidder

- PLASTIC DRUM STRIPING DETAIL**
- PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH MUTCD STANDARDS FOR BARRICADES. THE PREDOMINANT COLOR ON DRUMS SHALL BE ORANGE. STRIPES SHALL BE GREEN, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
 - DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
 - WHERE PRACTICAL PLASTIC DRUMS SHALL BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
HIGHWAY SIGN AND BARRICADE DETAIL FOR CONSTRUCTS

WORKING NUMBER: SDTCP-10
SHEET NUMBER: 10
DATE: 06/21/2012
FILE NAME: SDTCP-10_06012012.DGN
PERSON: [blank]
CROSSL: [blank]



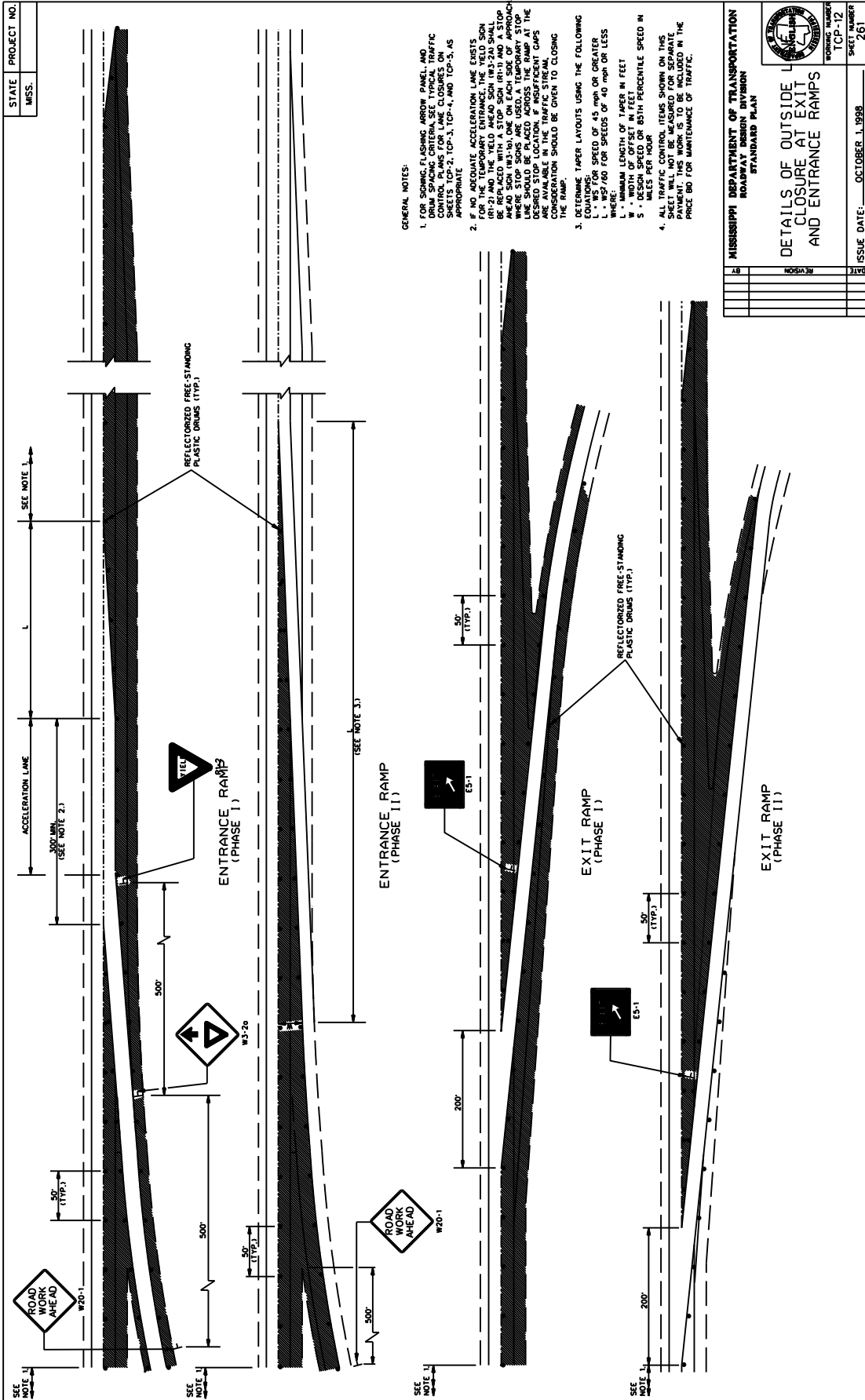
STATE PROJECT NO.
MISS.

DATE	BY	REVISION
12-99	S.M.H.	REVISED CAPTION

ISSUE DATE: OCTOBER 1, 1998
SHEET NUMBER: 260
WORKING NUMBER: TCF-11

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TRAFFIC CONTROL PLAN
MOBILE OPERATIONS
MULTILANE ROADS
AND
TWO-LANE ROADS



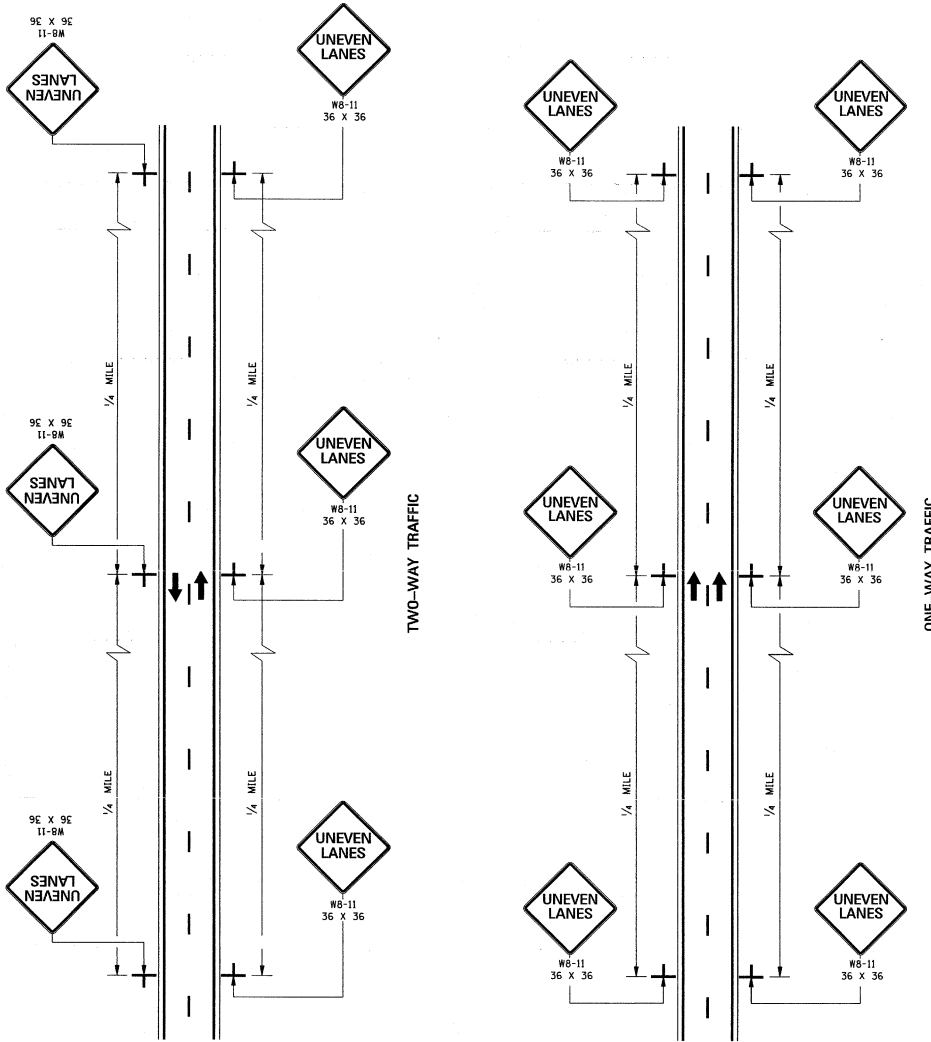
GENERAL NOTES:

- FOR SIGNING, FLASHING ARROW PANEL, AND TRAFFIC CONTROL PLANS FOR LANE CLOSURES, ON SHEETS TCC-2, TCC-3, TCC-4, AND TCC-5, AS APPROPRIATE.
- IF NO ADEQUATE ACCELERATION LANE EXISTS BEHIND THE RAMP, THE RAMP SHOULD BE REPLACED WITH A STOP SIGN (R1-1) AND A STOP AHEAD SIGN (W3-26), ONE ON EACH SIDE OF APPROACH. THE STOP SIGN SHOULD BE PLACED AT THE DESIRED STOP LOCATION, IF INSUFFICIENT GAPS EXIST. THE STOP SIGN SHOULD BE PLACED AT THE DESIRED STOP LOCATION, IF INSUFFICIENT GAPS EXIST. THE STOP SIGN SHOULD BE PLACED AT THE DESIRED STOP LOCATION, IF INSUFFICIENT GAPS EXIST.
- DETERMINE TAPER LAYOUTS USING THE FOLLOWING EQUATIONS:
 - $L = 1.47 \cdot S \cdot T$ FOR SPEEDS OF 40 MPH OR GREATER
 - $L = W \cdot S^2 / 100$ FOR SPEEDS OF 40 MPH OR LESS
 WHERE:
 - L = MINIMUM LENGTH OF TAPER IN FEET
 - S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
 - T = TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET
 - W = NUMBER OF TAPER LAYOUTS FOR EACH PHASE
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET ARE TO BE USED FOR MAINTENANCE OF TRAFFIC. THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY MAINTENANCE DIVISION	
STANDARD PLAN	
WORKING NUMBER	TCP-12
SHEET NUMBER	261
ISSUE DATE:	OCTOBER 1, 1998


DETAILS OF OUTSIDE LANE CLOSURE AT EXIT AND ENTRANCE RAMP

STATE	PROJECT NO.
MISS.	

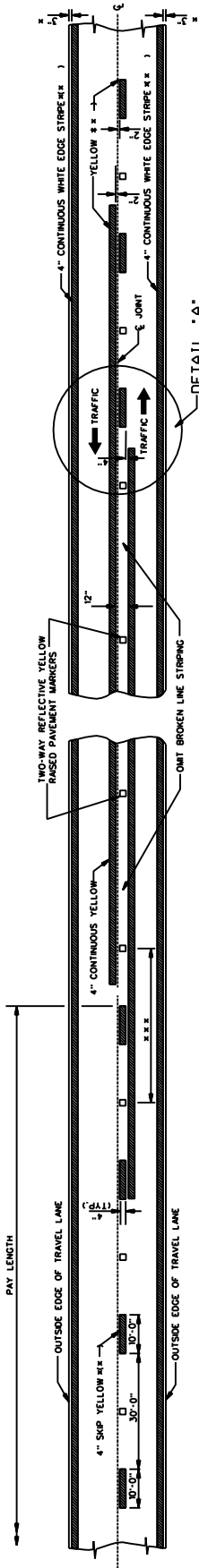


GENERAL NOTES:

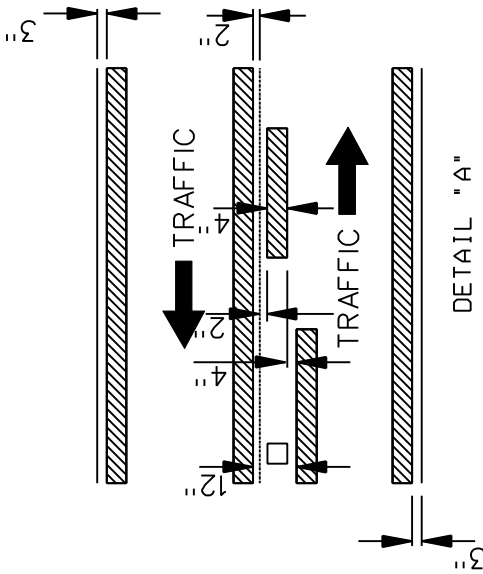
1. UNEVEN LANE LINE SPACING SHALL BE EQUAL TO 1/2" NO SIGNS REQUIRED.
 2. IF GREATER THAN 1/2" AND LESS THAN OR EQUAL TO 2/4" PLACE SIGNS AS SHOWN ON THIS SHEET.
 3. IF GREATER THAN 2/4" TRAFFIC SHOULD NOT BE ALLOWED TO CROSS UNEVEN LANE LINE.
2. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.
 3. THE W8-11 SIGNS SHALL BE SPACED AT 1/4-MILE INTERVALS THROUGHOUT UNEVEN LANE LINE LIMITS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN			WORKING NUMBER TCP-14	ISSUE DATE: OCTOBER 1, 1998
TRAFFIC CONTROL PLANS UNEVEN PAVEMENT DETAILS			SHEET NUMBER 263	
DATE	BY	REVISION		

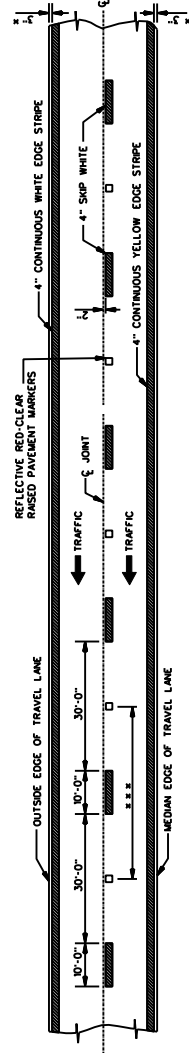
STATE	PROJECT NO.
MISS.	



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



DETAIL "A"



4-LANE WITH ONE-WAY TRAFFIC



GENERAL NOTES:

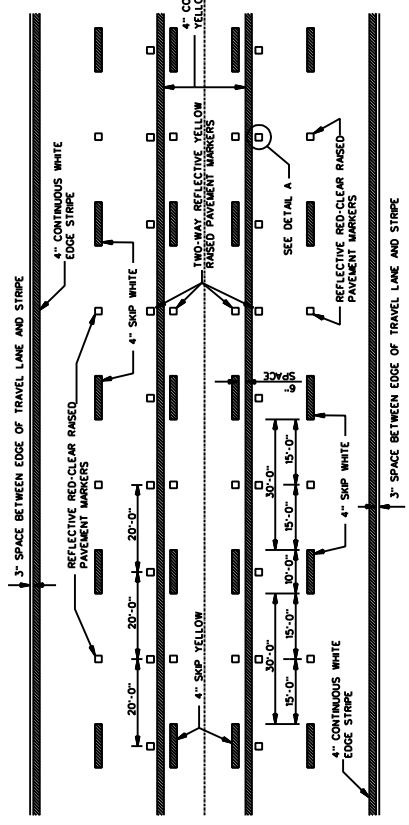
- * 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
- *** 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	URBAN AREA (U-U)	RURAL AREA (U-L)
LANE LINES	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	140'-0"

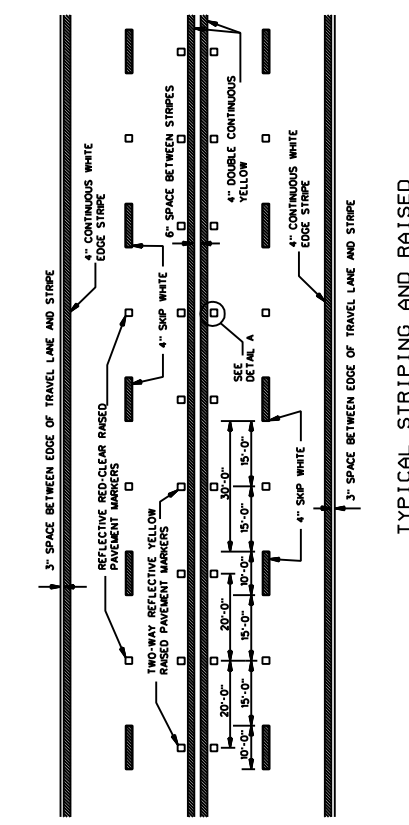
- 1. NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS SHALL BE USED THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- 2. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE MATERIALS AS SPECIFIED IN THE MOOT.
- 3. APPROVED SOURCES OF MATERIALS.
- 4. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKERS ARE TO REMAIN IN PLACE OVER 3 MONTHS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS	
WORKING NUMBER	TCP-15
SHEET NUMBER	264
ISSUE DATE:	DECEMBER 1, 1999

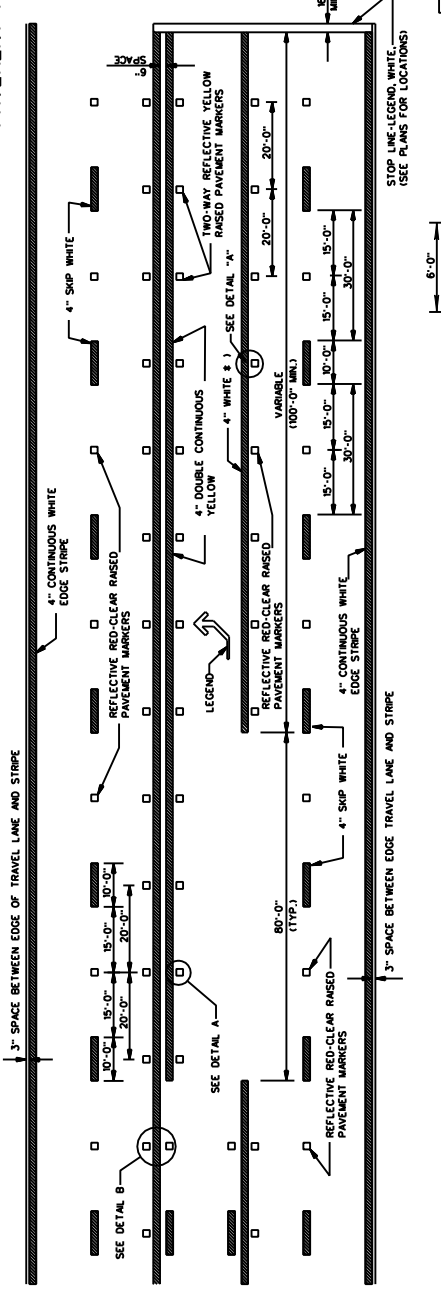
STATE	PROJECT NO.
MISS.	



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 5-LANE SECTION



TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION



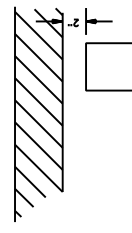
TYPICAL STRIPING AND RAISED PAVEMENT MARKERS AT LEFT TURN LANES

* NOTE: USE DETAIL STRIPING IF LENGTH $\leq 150'$ AT THIS LOCATION. OTHERWISE USE CONTINUOUS STRIPING.

TYPICAL TWO-WAY ARROW INSTALLATION

1. CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY.
2. IF SEGMENT IS LESS THAN 300', PLACE ONE SET OF ARROWS IN CENTER OF SEGMENT.
3. IF SEGMENT IS GREATER THAN 300', PLACE FIRST SET OF ARROWS 50 TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS (250 O.C.).

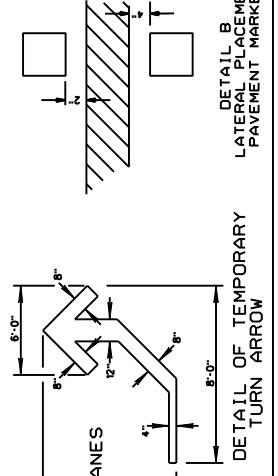
DETAIL A LATERAL PLACEMENT OF PAVEMENT MARKERS



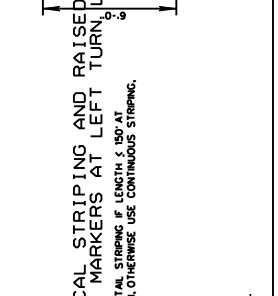
GENERAL NOTE:

1. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MOST APPROVED SOURCE OF MATERIALS.
2. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKERS ARE TO REMAIN IN PLACE OVER 3 MONTHS.
3. TEMPORARY TURN ARROW TO BE PAID FOR AS TEMPORARY TRAFFIC CONTROL STRIPING LEGEND, ESTIMATED AT 10.9 SQ. FT. PER ARROW.

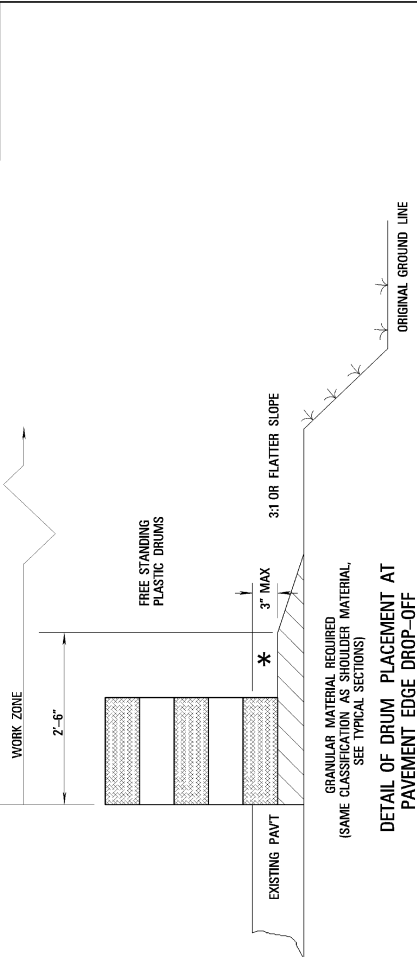
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TEMPORARY STRIPING FOR TRAFFIC CONTROL 4-LANE AND 5-LANE UNDIVIDED ROADWAYS	
WORKING NUMBER	TCP-16
SHEET NUMBER	265
ISSUE DATE:	DECEMBER 1, 1999



DETAIL B LATERAL PLACEMENT OF PAVEMENT MARKERS



DETAIL OF TEMPORARY TURN ARROW



NOTES

- * A. PAVEMENT EDGE DROP-OFF
 1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-3) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750 ± 0.0).
 2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. COMES MAY BE USED IN PLACE OF DRUMS, PANELS AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE M.U.T.C.D. (1/3 L, WHERE L IS THE TAPER LENGTH IN FEET)
 3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 3:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS/PANELS OR BARRICADES MAY BE USED. IF CONCRETE BARRIERS ARE USED, SPECIAL REFLECTIVE DEVICES OR STEADY BURN LIGHTS SHOULD BE USED FOR OVERNIGHT INSTALLATIONS.
 4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
 5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

B. DRUM SPACING

- 1. TANGENTS = 2 X S
- 2. WHERE L = 1/3 X W
- 3. L = TAPER LENGTH IN FEET
- 4. S = SPEED IN MPH (POSTED OR 85 PERCENTILE)
- 5. W = WIDTH OF OFFSET IN FEET

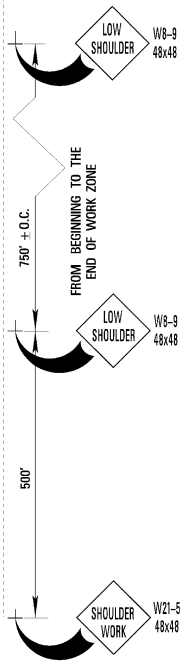
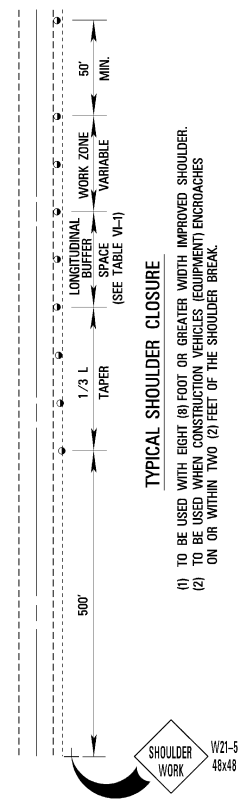
C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

TABLE V-1 GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

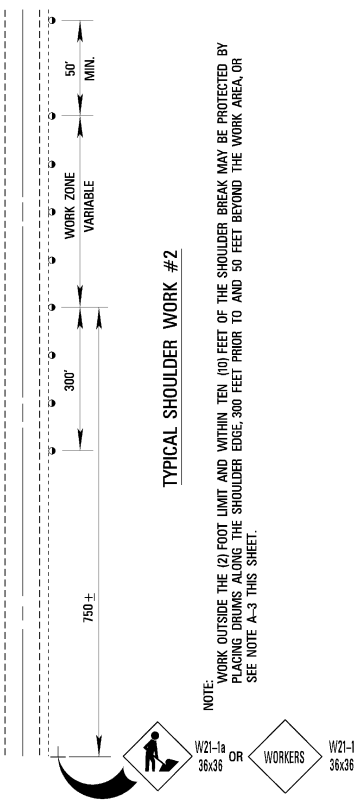
SPEED (MPH)	MIN	MAX	LENGTH (FEET)
20		35	35
25		45	45
30		55	55
35		65	65
40		75	75
45		85	85
50		95	95
55		105	105
60		115	115
65		125	125
70		135	135
75		145	145
80		155	155
85		165	165
90		175	175
95		185	185
100		195	195
105		205	205
110		215	215
115		225	225
120		235	235
125		245	245
130		255	255
135		265	265
140		275	275
145		285	285
150		295	295
155		305	305
160		315	315
165		325	325
170		335	335
175		345	345
180		355	355
185		365	365
190		375	375
195		385	385
200		395	395
205		405	405
210		415	415
215		425	425
220		435	435
225		445	445
230		455	455
235		465	465
240		475	475
245		485	485
250		495	495
255		505	505
260		515	515
265		525	525
270		535	535
275		545	545
280		555	555
285		565	565
290		575	575
295		585	585
300		595	595
305		605	605
310		615	615
315		625	625
320		635	635
325		645	645
330		655	655
335		665	665
340		675	675
345		685	685
350		695	695
355		705	705
360		715	715
365		725	725
370		735	735
375		745	745
380		755	755
385		765	765
390		775	775
395		785	785
400		795	795
405		805	805
410		815	815
415		825	825
420		835	835
425		845	845
430		855	855
435		865	865
440		875	875
445		885	885
450		895	895
455		905	905
460		915	915
465		925	925
470		935	935
475		945	945
480		955	955
485		965	965
490		975	975
495		985	985
500		995	995

* POSTED SPEED OFF PEAK 85 PERCENTILE SPEED PRIOR TO WORK STARTING OR THE ANTICIPATED GRINDING SPEED IN MPH

TYPICAL SHOULDER CLOSURE



TYPICAL SHOULDER WORK #2



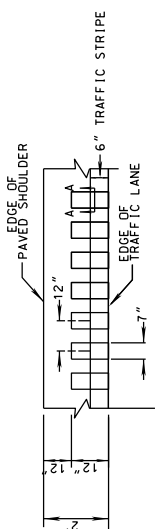
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS
DRUM PLACEMENT
AND
SHOULDER CLOSURE

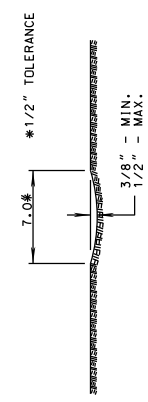
PROJECT NO.: _____
COUNTY: _____

FILE NAME: _____
PERSON: _____
DATE: 12-28-20

WORKING NUMBER: _____
TCP: _____
SHEET NUMBER: _____

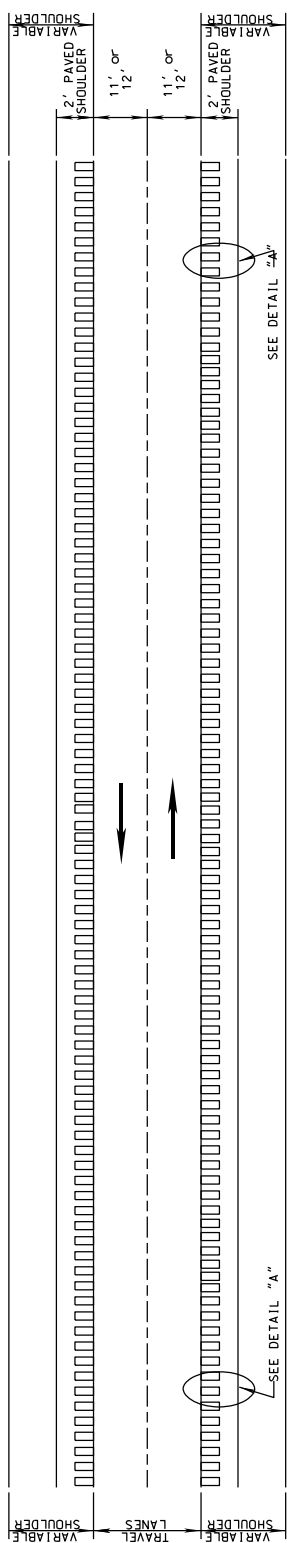


DETAIL "A"



SECTION "A-A"

- GENERAL NOTES
1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
 2. GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
 4. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.
 5. DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.

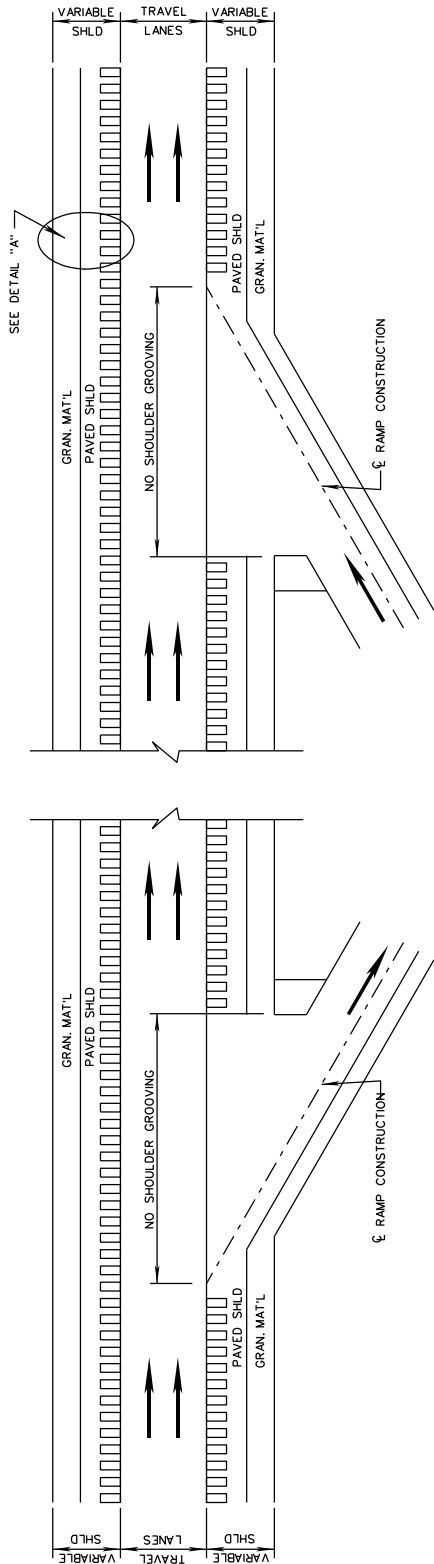
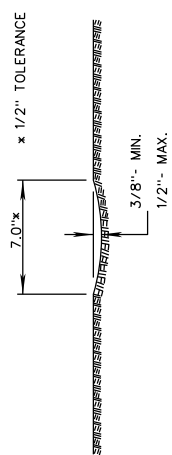
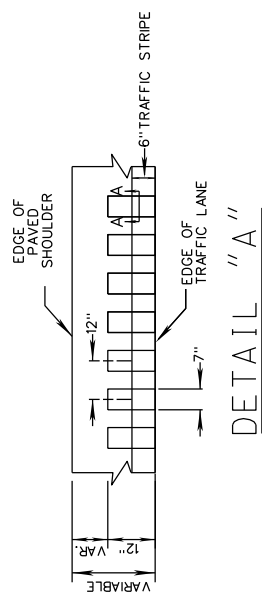


PLAN
NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
RUMBLE STRIPES	
2-LANE HIGHWAYS	
(ASPH. LANES,	
2-FT ASPH. SHLD)	
PROJECT NO:	
COUNTY:	
ISSUING NUMBER	RS-1
SHEET NUMBER	
DATE	05/02/14
BY	
REVISION	
DATE	
DESIGN TEAM	SERVICES CHECKED

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
RUMBLE STRIPES	
4-LANE HIGHWAYS	
(ASPH. LANES, 2-FT ASPH. SHLD)	
PROJECT NO:	002
COUNTY:	
WORKING NUMBER	RS-2
SHEET NUMBER	002
DATE	05/01/14
CHECKED	
DESIGN TEAM	

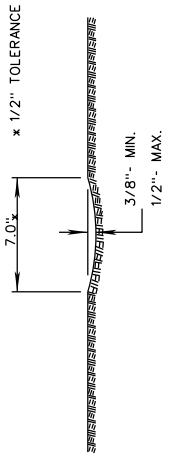
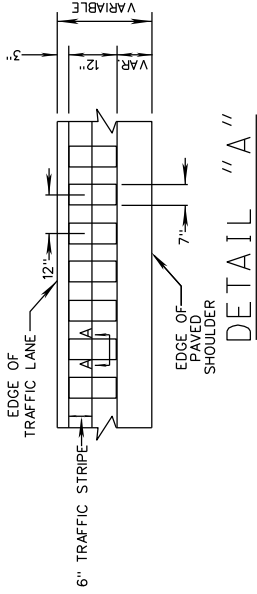
- GENERAL NOTES
1. ALL RUMBLE STRIPES SHALL BE GROUND-IN. RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 2. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.



NOT TO SCALE
DETAILS OF
RUMBLE STRIPS

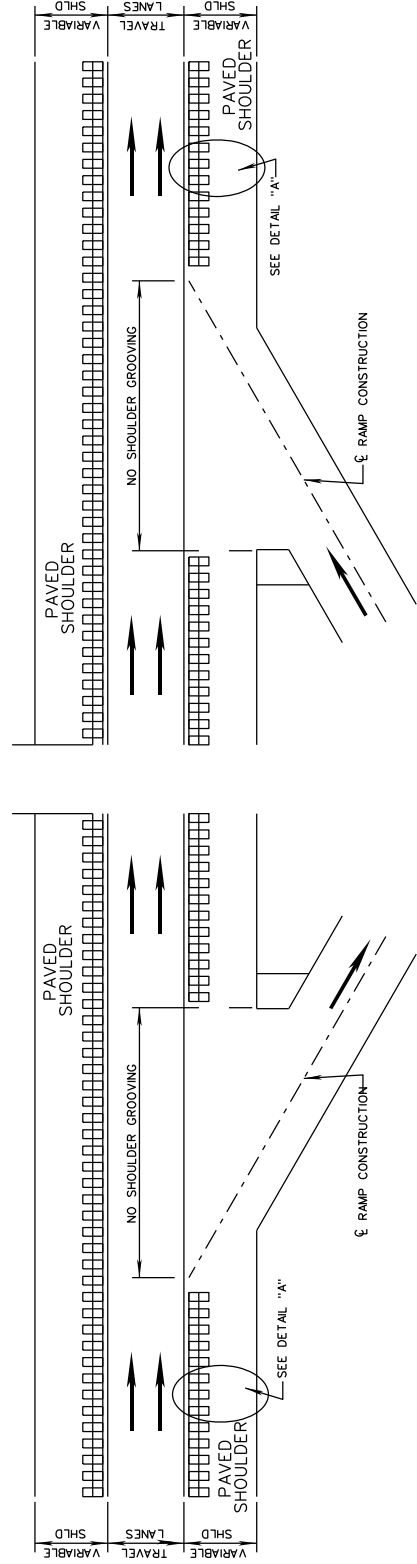
STATE	PROJECT NO.
MISS.	

- GENERAL NOTES**
1. ALL RUMBLE STRIPES SHALL BE GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 2. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.



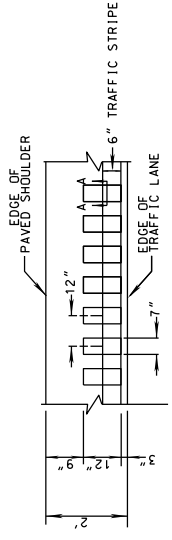
SECTION "A-A"

DETAIL "A"

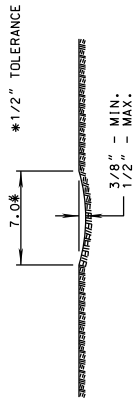


MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
RUMBLE STRIPES	
4-LANE WITH FULL-WIDTH	
PAVED SHOULDERS	
(CONC. LANES, ASPH. SHLD)	
WORKING NUMBER	PROJECT NO:
FILE NUMBER	COUNTY:
SHEET NUMBER	DESIGN TEAM
DATE	CHECKED
REVISION	DATE

PLAN
NOT TO SCALE
DETAILS OF
RUMBLE STRIPS
(GROUND-IN)

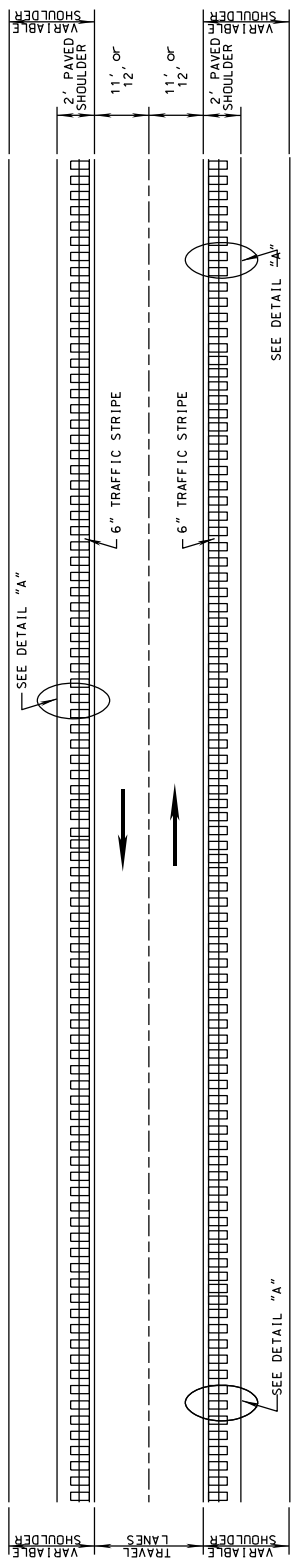


DETAIL "A"



SECTION "A-A"

GENERAL NOTES
 1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
 2. GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAY OR OTHER INTERRUPTIONS IN ALL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
 4. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.
 5. DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.



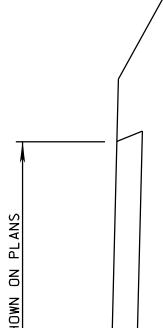
PLAN
 NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
RUMBLE STRIPES	
2-LANE HIGHWAY	
(CONCRETE LANES,	
2-FT. ASPH. SHLD.)	
PROJECT NO:	
COUNTY:	
DATE:	DATE REVISION:
BY:	REVISION:
FILE NAME:	WORKING NUMBER:
DESIGN TEAM:	DRAWING NUMBER:
DATE 05/01/24:	SHEET NUMBER:

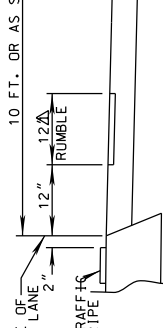
10 FT. OR AS SHOWN ON PLANS



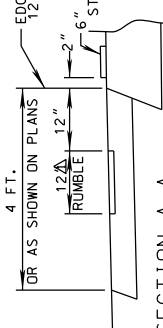
SECTION A-A
MEDIAN SHOULDER WITHOUT O.G.F.C.



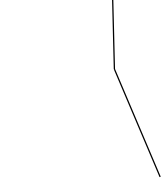
SECTION B-B
OUTSIDE SHOULDER WITHOUT O.G.F.C.



SECTION C-C
MEDIAN SHOULDER WITH O.G.F.C.

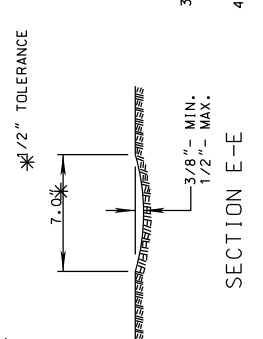


SECTION D-D
OUTSIDE SHOULDER WITH O.G.F.C.

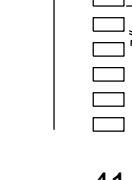
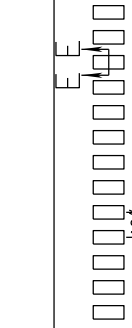
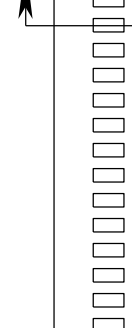
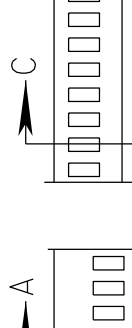
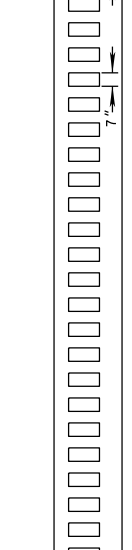


SECTION E-E

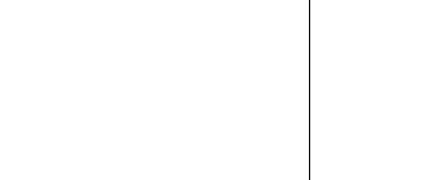
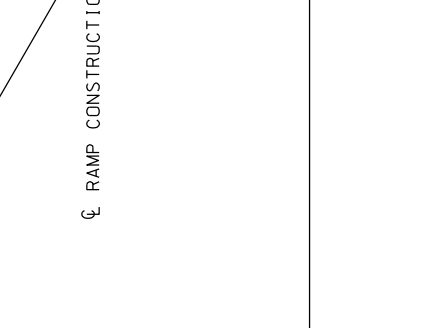
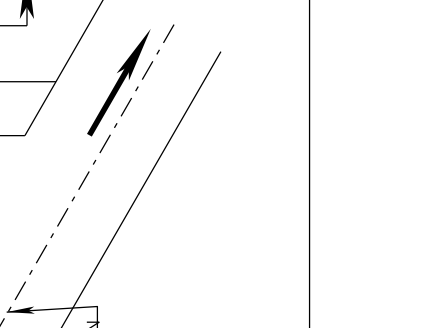
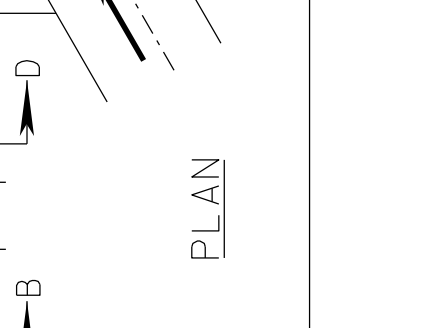
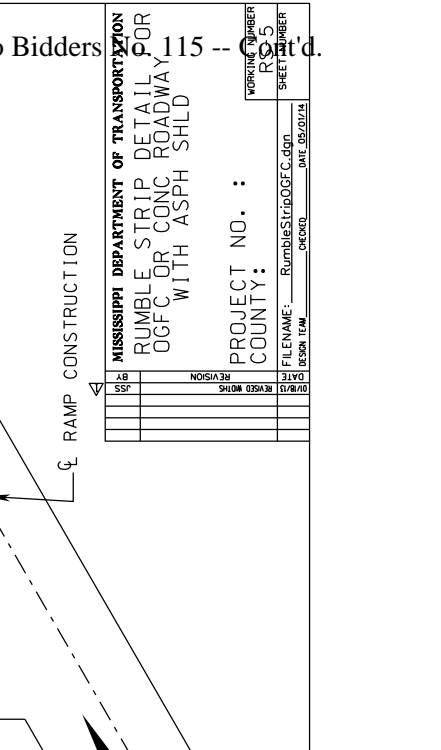
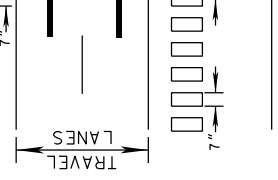
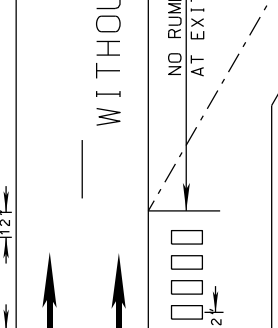
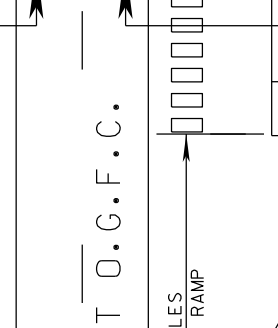
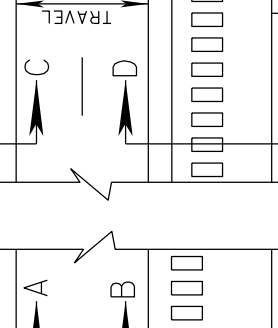
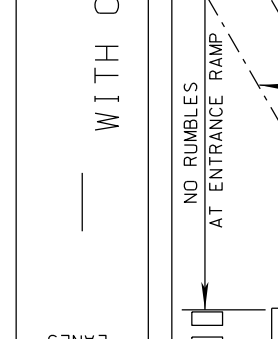
GENERAL NOTES
 1. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
 2. GROUND-IN RUMBLE STRIPS SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTIONS, ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 3. COST TO BE PAID FOR IN PAY ITEM 907-423-A RUMBLE STRIPS (GROUND-IN).
 4. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED TO MAINLINE ONLY.



4 FT. OR AS SHOWN ON PLANS



GENERAL NOTES
 1. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
 2. GROUND-IN RUMBLE STRIPS SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTIONS, ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 3. COST TO BE PAID FOR IN PAY ITEM 907-423-A RUMBLE STRIPS (GROUND-IN).
 4. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED TO MAINLINE ONLY.



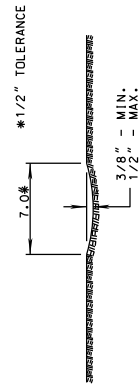
Notice To Bidders
 MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 RUMBLE STRIP DETAIL FOR
 O.G.F.C. OR CONC ROADWAY
 WITH ASPH SHLD

PROJECT NO. :
 COUNTY:

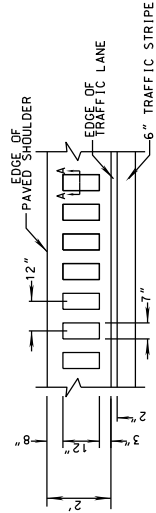
DATE	BY	REVISION

WORKING NUMBER	SHEET NUMBER
RS-5	115

FILENAME: RumbleStripOGFC.dgn
 DATE: 05/01/14
 CHECKED: []
 DESIGN TEAM: []



SECTION "A-A"



DETAIL "A"

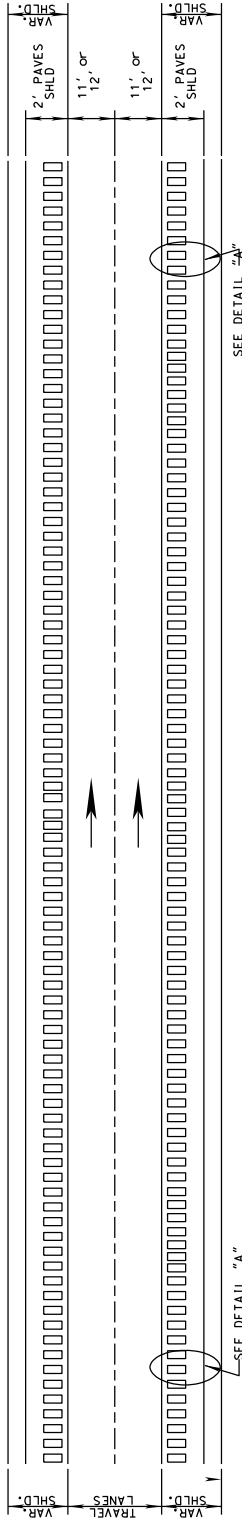
GENERAL NOTES
1. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT

2. GROUND-IN RUMBLE STRIPS SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER

3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS

4. GROUND-IN RUMBLE STRIPS SHALL BE APPLIED TO MAINLINE ONLY.

5. DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.



PLAN
NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
RUMBLE STRIPS FOR 4-LANE DIVIDED ROADWAYS WITH 2-FOOT PAVED SHOULDERS CONCRETE LANES, ASPH. SHOULDER	
PROJECT NO. : 115	
COUNTY : 115	
WORKING NUMBER	RS-6
SHEET NUMBER	115-6
FILE NAME: \\s:\cumberland\115.dwg	DATE: 05/01/14
DESIGN: T.M. S. LEWIS	DRAWN: J.C. LEWIS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 296

CODE: (SP)

DATE: 07/25/2017

SUBJECT: Reduced Speed Limit Signs

Bidders are advised that when the plans or contract documents require the speed limit on a project to be reduced, the Contractor shall begin work within 48 hours of installing the reduced speed limit signs. Should the Contractor not start work or have no plans to start work within 48 hours of installing the signs, the reduced speed limit signs shall be covered and existing speed limit signs uncovered.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 385

CODE: (SP)

DATE: 08/29/2017

SUBJECT: Contract Time

PROJECT: SP-0047-01(016) / 107693301 – Sharkey County

The calendar date for completion of work to be performed by the Contractor for this project shall be **November 17, 2017** which date or extended date as provided in Subsection 907-108.06 shall be the end of contract time. It is anticipated that the Notice of Award will be issued no later than **September 12, 2017** and the effective date of the Notice to Proceed / Beginning of Contract Time will be simultaneous with the execution of the contract.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 386

CODE: (SP)

DATE: 8/28/2017

SUBJECT: Scope of Work

PROJECT: SP-0047-01(016) / 107693301 – Sharkey County

Bridge Repairs on SR 14 Bridge Numbers: 11693 and 14407

The contract documents do not include an official set of construction plans, but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings”. All other references to plans in the contract documents and Standard Specification for Road and Bridge Construction are to be disregarded.

In general, the work to be accomplished using the pay items and corresponding specifications set forth in this contract is to repair bridge beams and mill excess asphalt pavement on the bridge surfaces.

Work on the project shall consist of the following:

1. The Contractor shall erect and maintain construction signing, and provide all signs and traffic handling devices needed for any other traffic control in accordance with the Traffic Control Plan. The cost is to be included in the price bid for pay item No. 618-A, Maintenance of Traffic. All traffic control devices on this project should comply with the MUTCD (Latest Edition). Fluorescent orange sheeting shall be used on all construction and traffic control signs except for those designated in plans to be black legend and border on white background. Cones shall be narrow profile with a minimum height of 28 inches and a minimum weight of ten (10) pounds. Cones used in speed zones equal to or greater than 45 miles per hour shall be narrow profile with a minimum height of 28 inches and a minimum weight of fifteen (15) pounds. All cones shall be approved by the Engineer prior to use.
2. Repair bridge beams by installing Saddles as per the provided drawing at the following locations:

State Route 14 - Bridge #14407 Bents 2 and 3

State Route 14 - Bridge #11693 Bents 2 and 3

NOTE: The pay item for this work is 907-824-PP006, Bridge Repair, Saddle Installation. All work to repair the bridge shall be absorbed in this pay item. Items such as but not limited to shim plates, bearing pads, bolts, washers and connectors, W-Beams, welding, etc. shall be absorbed in the pay item.

3. After the Saddles have been installed the milling operation will commence. No milling will be allowed before Saddle installation is complete on the bridges. The milling operation will start 500 feet on either side of both bridges. Mill the roadway at a rate of 1 inch per 100 feet so that when the milling machine reaches the bridge end a total of five inches (5") of asphalt has been removed. This is the estimated thickness of asphalt pavement on the bridge. At this point the milling operation will traverse the bridge deck removing all of the asphalt down to the original concrete deck.

During the work day while the milling operation is on the project, drop offs will be limited to three inches (3") with the appropriate lane closures. At no time will a drop off be greater than three inches (3").

At the end of the work day all lane closures will be removed and the drop off will be no greater than 1½ inches.

4. The Contractor shall remove any failed areas uncovered by the milling operation as directed by the Project Engineer using the following construction sequence:
 - A) Saw cut full depth through the asphalt. There will be no pay item for this saw cut, and the price of the work should be absorbed in the pay item 202-B188 - Removal of Pavement, All Types, All Depths.
 - B) Remove the failed asphalt.
 - C) Remove any unsuitable material in the subgrade as directed by the Engineer. Removal of this material will be paid for as excess excavation.
 - D) Backfill and stabilize the failed area with crushed stone in lifts to the current roadway elevation. No lift of crushed stone shall be greater than six inches (6") in thickness or in a thickness as designated by the Project Engineer.
5. All shoulders in the milled areas shall be graded to the new asphalt elevation on the appropriate slope. This will be as an absorbed item of work. If excess granular material is present it will be removed from MDOT right of way and paid for as excess excavation.

GENERAL NOTES:

1. Fabricator shall verify all dimensions of the existing structure shall be field verified by the Contractor. The Contractor shall be responsible for adjusting the elements Shop drawings of saddles, including an erection plan, to match the existing structure. The Director of Structures shall be notified in duplicate of the fabrication. Structures shall conform to A.S.T.M. A449. All steel plates, bars, and W-shapes shall conform to A.S.T.M. A449. All thread/anchor bolts shall conform to A.S.T.M. A449. All steel plates, bars, and W-shapes shall be new. Washers shall conform to F436, 3/4", Grade E7 and nuts shall be heavy hex. The minimum amount required for proper assembly shall be provided. All bolts and anchor bolts shall have washers and jam nuts on all threads. After fabrication, all other saddle assembly parts shall be galvanized in accordance with A.S.T.M. A123 and a procedure for storage and handling of welded steel-to-steel joints shall be provided. The Project Engineer through the State Bridge Engineer shall be notified of the progress and shall be kept advised of the progress and shall be notified of any delays. The latest edition of the AASHTO Guide Specification for Highway Bridge Fabrication with High Performance Steel, when applicable, shall be used. The fabricator shall have a Certified Welding Inspector (CWI) on each work shift where welding or other significant work is performed.

NOTE:
All bolts shall be uniformly tightened. At completion of the saddle assembly, all bolts shall be in full contact and the neoprene pads tight and compressed.

NOTE:
1" Steel neoprene pads can be field cut.

EPOXY MORTAR REPAIR NOTES:

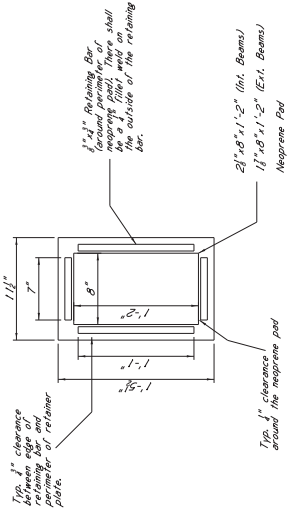
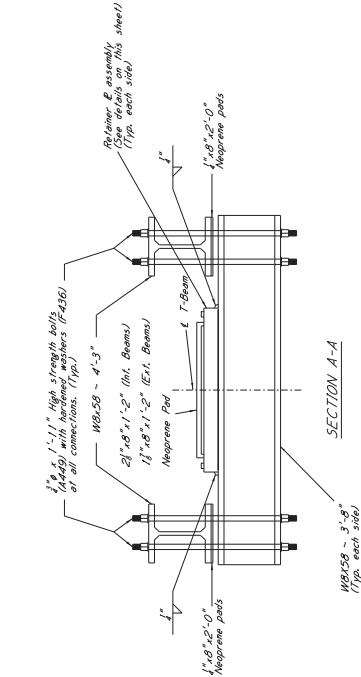
1. Repair concrete spalled areas on the bridges as directed by the Project Engineer using epoxy mortar. Epoxy mortar shall be applied to the spalled areas and shall be finished to match the existing concrete. All areas of the bridge repaired with epoxy mortar shall be restored to the original dimensions and details as shown in the information plans, unless otherwise indicated. Epoxy Mortar Mix: Epoxy mortar mix shall consist of part liquid epoxy and part cementitious sand mixed in the ratio recommended by the manufacturer. Application: The epoxy mortar must be prepared for sufficient time to allow the epoxy to cure. The epoxy mortar shall be applied to the surface with wet epoxy. Prior to placement of the mortar mix, the prepared surface shall be lightly primed with wet epoxy. Curing time shall be in accordance with manufacturer's recommendations.

CONSTRUCTION SEQUENCE:

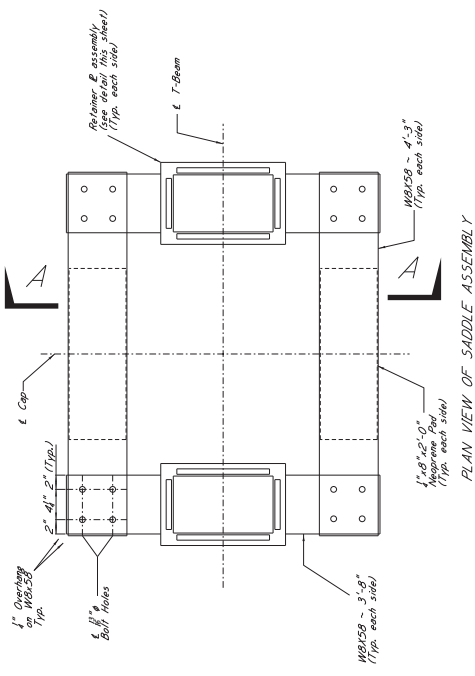
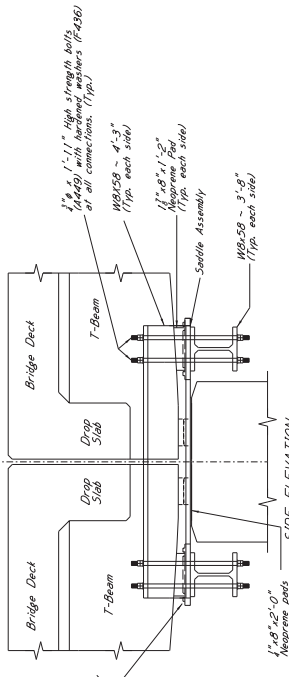
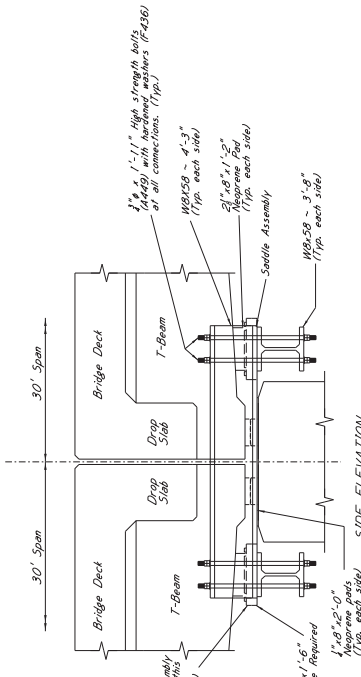
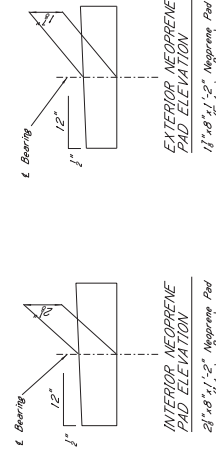
1. Clean all bearings and steel caps of the repair locations.
2. Prime the contact surface of old concrete with an epoxy primer and restore grout ends to original slope with an epoxy mortar.
3. Paint the contact surface of old concrete with an epoxy primer and restore grout ends to original slope with an epoxy mortar.
4. Clean and paint all exposed surfaces of existing bearings and steel caps of the repair locations with an epoxy primer and restore grout ends to original slope with an epoxy mortar.
5. Install saddle assemblies in accordance with the details on this sheet.

FIELD PART NOTE:

All paint rust scale within the designated areas shall be removed by sandblasting to a minimum of 100 mesh. The steel surface shall be cleaned with a biodegradable solvent. Existing paint shall be roughened and removed from the existing structure. The steel surface shall be primed with an epoxy primer. After the steel has been thoroughly cleaned, all exposed bearing surfaces shall be primed with an epoxy primer. The contractor shall provide technical data for the proposed encapsulating paint to the Director of Structures, State Bridge Engineer, for approval. New paint shall be applied by hand, with either a brush or roller.



Showing dimensions and details for:
1" x 1 1/2" x 1-5/8" retainer plate (Int. Beams)
2" x 1 1/2" x 1-5/8" retainer plate (Ext. Beams)



PLAN VIEW OF SADDLE ASSEMBLY

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-103-2

CODE: (SP)

DATE: 06/22/2017

SUBJECT: Award and Execution of Contract

Section 103, Award and Execution of Contract, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-103.01--Consideration of Proposal. Delete the second and third paragraphs of Subsection 103.01 on page 19, and substitute the following.

907-103.01.1--For Projects Constructed Without Federal Funds. Resident Contractors actually domiciled in Mississippi are to be granted preference over nonresidents in awarding of Contracts financed 100% with State funds.

In consideration of proposals that are equal to or in excess of \$50,000 and financed 100% with State funds, a nonresident bidder domiciled in a state having laws granting preference to local Contractors will be considered for such contracts on the same basis as the nonresident bidder's state awards contracts to Mississippi Contractors bidding under similar circumstances. When a nonresident Contractor submits a bid equal to or in excess of \$50,000 on a contract financed 100% with State funds, a copy of the current laws from the state of domicile and an explanation thereof pertaining to treatment of nonresident Contractors shall be attached. If no preferential treatment is provided for Contractors in the state of domicile and contracts are awarded to the lowest responsible bidder, a statement to this effect shall be attached. Should the attachment not accompany the bid when submitted, the Contractor shall have 10 days following the opening of the bids to furnish the required information to the Contract Administration Director for attachment to the bid. Failure to provide the attachment within 10 days will result in the nonresident Contractor's bid being rejected and not considered for award. As used herein, the term "resident Contractor" includes a nonresident person, firm or corporation that has been qualified to do business in this State and has maintained a permanent full-time office in the State of Mississippi for two years prior to the submission of the bid, and the subsidiaries and affiliates of such a person, firm or corporation.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SUPPLEMENT TO SPECIAL PROVISION NO. 907-618-3

DATE: 8/29/2017

PROJECT: SP-0047-01(016) / 107693301 – Sharkey County

After the first paragraph of Subsection 907-618.01.2 on page 1, add the following.

Additional signs will be required as follows:

ADDITIONAL TRAFFIC CONTROL SIGNS REQUIRED:

- 2 - W20-1 “Road Work Ahead” signs required.
- 4 - W20-1 “Road Work Ahead ½ Mile” signs required.
- 4 - W20-1 “Road Work Ahead 1500 Feet” signs required.
- 4 - W20-1 “Road Work Ahead 1000 Feet” signs required.
- 4 - W20-1 “Road Work Ahead 500 Feet” signs required.*
- 4 - G20-2 “End Road Work” signs required.*

* Mounted on Type III Barricade, Double Faced (6’ Wide)

All W20-1 signs are to be 48” x 48” and all G20-2 signs are to be 48” x 24”.

All Construction signing is included in the bid for Pay Item 618-A, Maintenance of Traffic.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-618-3

CODE: (SP)

DATE: 01/17/2017

SUBJECT: Additional Signing Requirements

Section 618, Maintenance of Traffic and Traffic Control Plan, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-618.01.2--Traffic Control Plan. At the end of Subsection 618.01.2 on page 441, add the following:

For compliance with the traffic control plan, the Contractor will be required to install and maintain construction signs at various locations throughout the project. Payment for these signs will be included in the price bid for pay item no. 618-A, Maintenance of Traffic per lump sum.

SECTION 905 - PROPOSAL

Date _____

Mississippi Transportation Commission
Jackson, Mississippi

Sirs: The following proposal is made on behalf of _____
_____ of _____

for constructing the following designated project(s) within the time(s) hereinafter specified.

The plans are composed of drawings and blue prints on file in the offices of the Mississippi Department of Transportation, Jackson, Mississippi.

The Specifications are the current Standard Specifications of the Mississippi Department of Transportation approved by the Federal Highway Administration, except where superseded or amended by the plans, Special Provisions and Notice(s) to Bidders attached hereto and made a part thereof.

I (We) certify that I (we) possess a copy of said Standard and any Supplemental Specifications.

Evidence of my (our) authority to submit the Proposal is hereby furnished. The proposal is made without collusion on the part of any person, firm or corporation. I (We) certify that I (we) have carefully examined the Plans, the Specifications, including the Special Provisions and Notice(s) to Bidders, herein, and have personally examined the site of the work. On the basis of the Specifications, Special Provisions, Notice(s) to Bidders, and Plans, I (we) propose to furnish all necessary machinery, tools, apparatus and other means of construction and do all the work and furnish all the materials in the manner specified. I (We) understand that the quantities mentioned herein are approximate only and are subject to either increase or decrease, and hereby propose to perform any increased or decreased quantities of work at the unit prices bid, in accordance with the above.

I (We) acknowledge that this proposal will be found irregular and/or non-responsive unless a certified check, cashier's check, or Proposal Guaranty Bond in the amount as required in the Advertisement (or, by law) is submitted electronically with the proposal or is delivered to the Contract Administration Engineer prior to the bid opening time specified in the advertisement.

INSTRUCTION TO BIDDERS: Alternate and Optional Items on Bid Schedule.

1. Two or more items entered opposite a single unit quantity WITHOUT DEFINITE DESIGNATION AS "ALTERNATE ITEMS" are considered as "OPTIONAL ITEMS". Bidders may or may not indicate on bids the Optional Item proposed to be furnished or performed WITHOUT PREJUDICE IN REGARD TO IRREGULARITY OF BIDS.
2. Items classified on the bid schedule as "ALTERNATE ITEMS" and/or "ALTERNATE TYPES OF CONSTRUCTION" must be preselected and indicated on bids. However, "Alternate Types of Construction" may include Optional Items to be treated as set out in Paragraph 1, above.
3. Optional items not preselected and indicated on the bid schedule MUST be designated in accordance with Subsection 102.06 prior to or at the time of execution of the contract.
4. Optional and Alternate items designated must be used throughout the project.

I (We) further propose to perform all "force account or extra work" that may be required of me (us) on the basis provided in the Specifications and to give such work my (our) personal attention in order to see that it is economically performed.

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) shall submit electronically with our proposal or deliver prior to the bid opening time a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

President Address

Secretary Address

Treasurer Address

The following is my (our) itemized proposal.

Emergency Bridge Repair on SR 14, Bridge Nos. 11693 & 14407, known as State Project No. SP-0047-01(016) / 107693301 in Sharkey County.

Line no.	Item Code	Adj Code	Quantity	Units	Description[Fixed Unit Price]
Roadway Items					
0010	202-B188		100	Square Yard	Removal of Pavement, All Types and Depths
0020	203-G001	(E)	100	Cubic Yard	Excess Excavation, FM, AH
0030	406-D003		800	Ton	Fine Milling of Bituminous Pavement, All Depths
0040	618-A001		1	Lump Sum	Maintenance of Traffic
0050	618-B001		1	Square Feet	Additional Construction Signs (\$10.00)
0060	620-A001		1	Lump Sum	Mobilization
ALTERNATE GROUP AA NUMBER 1					
0070	304-F001	(GT)	250	Ton	3/4" and Down Crushed Stone Base
ALTERNATE GROUP AA NUMBER 2					
0080	304-F002	(GT)	250	Ton	Size 610 Crushed Stone Base
ALTERNATE GROUP AA NUMBER 3					
0090	304-F003	(GT)	250	Ton	Size 825B Crushed Stone Base
Bridge Items					
0100	907-824-PP006		16	Each	Bridge Repair, Saddle Installation

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

CONDITIONS FOR COMBINATION BID

If a bidder elects to submit a combined bid for two or more of the contracts listed for this month's letting, the bidder must complete and execute these sheets of the proposal in each of the individual proposals to constitute a combination bid. In addition to this requirement, each individual contract shall be completed, executed and submitted in the usual specified manner.

Failure to execute this Combination Bid Proposal in each of the contracts combined will be just cause for each proposal to be received and evaluated as a separate bid.

It is understood that the Mississippi Transportation Commission not only reserves the right to reject any and all proposals, but also the right to award contracts upon the basis of lowest separate bids or combination bids most advantageous to the State.

It is further understood and agreed that the Combination Bid Proposal is for comparison of bids only and that each contract shall operate in every respect as a separate contract in accordance with its proposal and contract documents.

I (We) agree to complete each contract on or before its specified completion date.

COMBINATION BID PROPOSAL

This proposal is tendered as one part of a Combination Bid Proposal utilizing option ___* of Subsection 102.11 on the following contracts:

* Option to be shown as either (a), (b), or (c).

	<u>Project No.</u>	<u>County</u>	<u>Project No.</u>	<u>County</u>
1.	_____	_____	6.	_____
2.	_____	_____	7.	_____
3.	_____	_____	8.	_____
4.	_____	_____	9.	_____
5.	_____	_____	10.	_____

(a) If Combination A has been selected, your Combination Bid is complete.

(b) If Combination B has been selected, then complete the following page.

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
1.					
2.					
3.					
4.					
5.					
6.					
7.					
8.					

For Informational Purposes Only

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
9.					
10.					

(c) If Combination C has been selected, then initial and complete ONE of the following.

_____ I (We) desire to be awarded work not to exceed a total monetary value of \$ _____.

_____ I (We) desire to be awarded work not to exceed _____ number of contracts.



TO: EXECUTIVE DIRECTOR, MISSISSIPPI DEPARTMENT OF TRANSPORTATION
JACKSON, MISSISSIPPI

CERTIFICATE

If awarded this contract, I (we) contemplate that portions of the contract will be sublet. I (we) certify that those subcontracts which are equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

I (we) agree that this notification of intent DOES NOT constitute APPROVAL of the subcontracts.

_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)

NOTE: Failure to complete the above DOES NOT preclude subsequent subcontracts. Subsequent subcontracts, if any, equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

Contractor _____

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
CERTIFICATION

I, _____,
(Name of person signing bid)

individually, and in my capacity as _____ of
(Title of person signing bid)

(Name of Firm, partnership, or Corporation)

do hereby certify under penalty of perjury under the laws of the United States and the State of Mississippi

that _____, Bidder
(Name of Firm, Partnership, or Corporation)

on Project No. **SP-0047-01(016)/ 107693301000**

in Sharkey County(ies), Mississippi, has not either directly or indirectly entered into any agreement, participated in any collusion; or otherwise taken any action in restraint of free competitive bidding in connection with this contract; nor have any of its corporate officers or principal owners.

Except as noted hereafter, it is further certified that said legal entity and its corporate officers, principal owners, managers, auditors and others in a position of administering federal funds are not currently under suspension, debarment, voluntary exclusion or determination of ineligibility; nor have a debarment pending; nor been suspended, debarred, voluntarily excluded or determined ineligible within the past three years by the Mississippi Transportation Commission, the State of Mississippi, any other State or a federal agency; nor been indicted, convicted or had a civil judgment rendered by a court of competent jurisdiction in any matter involving fraud or official misconduct within the past three years.

Do exceptions exist and are made a part thereof? Yes / No

Any exceptions shall address to whom it applies, initiating agency and dates of such action.

Note: Exceptions will not necessarily result in denial of award but will be considered in determining bidder responsibility. Providing false information may result in criminal prosecution or administrative sanctions.

All of the foregoing is true and correct.

(1/2016 S)

SECTION 902

CONTRACT FOR SP-0047-01(016)/ 107693301000

LOCATED IN THE COUNTY(IES) OF Sharkey

STATE OF MISSISSIPPI,
COUNTY OF HINDS

This contract entered into by and between the Mississippi Transportation Commission on one hand, and the undersigned contractor, on the other witnesseth;

That, in consideration of the payment by the Mississippi Transportation Commission of the prices set out in the proposal hereto attached, to the undersigned contractor, such payment to be made in the manner and at the time of times specified in the specifications and the special provisions, if any, the undersigned contractor hereby agrees to accept the prices stated in the proposal in full compensation for the furnishing of all materials and equipment and the executing of all the work contemplated in this contract.

It is understood and agreed that the advertising according to law, the Advertisement, the instructions to bidders, the proposal for the contract, the specifications, the revisions of the specifications, the special provisions, and also the plans for the work herein contemplated, said plans showing more particularly the details of the work to be done, shall be held to be, and are hereby made a part of this contract by specific reference thereto and with like effect as if each and all of said instruments had been set out fully herein in words and figures.

It is further agreed that for the same consideration the undersigned contractor shall be responsible for all loss or damage arising out of the nature of the work aforesaid; or from the action of the elements and unforeseen obstructions or difficulties which may be encountered in the prosecution of the same and for all risks of every description connected with the work, exceptions being those specifically set out in the contract; and for faithfully completing the whole work in good and workmanlike manner according to the approved Plans, Specifications, Special Provisions, Notice(s) to Bidders and requirements of the Mississippi Department of Transportation.

It is further agreed that the work shall be done under the direct supervision and to the complete satisfaction of the Executive Director of the Mississippi Department of Transportation, or his authorized representatives, and when Federal Funds are involved subject to inspection at all times and approval by the Federal Highway Administration, or its agents as the case may be, or the agents of any other Agency whose funds are involved in accordance with those Acts of the Legislature of the State of Mississippi approved by the Governor and such rules and regulations issued pursuant thereto by the Mississippi Transportation Commission and the authorized Federal Agencies.

The Contractor agrees that all labor as outlined in the Special Provisions may be secured from list furnished by

It is agreed and understood that each and every provision of law and clause required by law to be inserted in this contract shall be deemed to be inserted herein and this contract shall be read and enforced as though it were included herein, and, if through mere mistake or otherwise any such provision is not inserted, then upon the application of either party hereto, the contract shall forthwith be physically amended to make such insertion.

The Contractor agrees that he has read each and every clause of this Contract, and fully understands the meaning of same and that he will comply with all the terms, covenants and agreements therein set forth.

Witness our signatures this the ___ day of _____, _____.

Contractor(s)

By _____

MISSISSIPPI TRANSPORTATION COMMISSION

Title _____

By _____

Signed and sealed in the presence of:
(names and addresses of witnesses)

Executive Director

Secretary to the Commission

Award authorized by the Mississippi Transportation Commission in session on the ___ day of _____, _____, Minute Book No. _____, Page No. _____.

SECTION 903
PERFORMANCE AND PAYMENT BOND

CONTRACT BOND FOR: SP-0047-01(016)/ 107693301000

LOCATED IN THE COUNTY(IES) OF: Sharkey

STATE OF MISSISSIPPI,
COUNTY OF HINDS

Know all men by these presents: that we, _____

(Contractor)

Principal, a _____

residing at _____ in the State of _____

and _____

(Surety)
residing at _____ in the State of _____,

authorized to do business in the State of Mississippi, under the laws thereof, as surety, effective as of the contract date

shown below, are held and firmly bound unto the State of Mississippi in the sum of _____

_____ Dollars, lawful money of the United States of America, to be paid to it for which payment well and truly to be made, we bind ourselves, our heirs, administrators, successors, or assigns jointly and severally by these presents.

The conditions of this bond are such, that whereas the said _____

principal, has (have) entered into a contract with the Mississippi Transportation Commission, bearing the date of

_____ day of _____ A.D. _____ hereto annexed, for the construction of certain projects(s) in

the State of Mississippi as mentioned in said contract in accordance with the Contract Documents therefor, on file in the

offices of the Mississippi Department of Transportation, Jackson, Mississippi.

Now therefore, if the above bounden _____

in all things shall stand to and abide by and well and truly observe, do keep and perform all and singular the terms, covenants, conditions, guarantees and agreements in said contract, contained on his (their) part to be observed, done, kept and performed and each of them, at the time and in the manner and form and furnish all of the material and equipment specified in said contract in strict accordance with the terms of said contract which said plans, specifications and special provisions are included in and form a part of said contract and shall maintain the said work contemplated until its final completion and acceptance as specified in Subsection 109.11 of the approved specifications, and save harmless said Mississippi Transportation Commission from any loss or damage arising out of or occasioned by the negligence, wrongful or criminal act, overcharge, fraud, or any other loss or damage whatsoever, on the part of said principal (s), his (their) agents, servants, or employees in the performance of said work or in any manner connected therewith, and shall be liable and responsible in a civil action instituted by the State at the instance of the Mississippi Transportation Commission or any officer of the State authorized in such cases, for double any amount in money or property, the State may lose or be overcharged or otherwise defrauded of, by reason of wrongful or criminal act, if any, of the Contractor(s), his (their) agents or employees, and shall promptly pay the said agents, servants and employees and all persons furnishing labor, material, equipment or supplies therefor, including premiums incurred, for Surety Bonds, Liability Insurance, and Workmen's Compensation Insurance; with the additional obligation that such Contractor shall promptly make payment of all taxes, licenses, assessments, contributions, damages,

any liquidated damages which may arise prior to any termination of said principal's contract, any liquidated damages which may arise after termination of the said principal's contract due to default on the part of said principal, penalties and interest thereon, when and as the same may be due this state, or any county, municipality, board, department, commission or political subdivision: in the course of the performance of said work and in accordance with Sections 31-5-51 et seq. Mississippi Code of 1972, and other State statutes applicable thereto, and shall carry out to the letter and to the satisfaction of the Executive Director of the Mississippi Department of Transportation, all, each and every one of the stipulations, obligations, conditions, covenants and agreements and terms of said contract in accordance with the terms thereof and all of the expense and cost and attorney's fee that may be incurred in the enforcement of the performance of said contract, or in the enforcement of the conditions and obligations of this bond, then this obligation shall be null and void, otherwise to be and remain in full force and virtue.

_____	_____
(Contractors) Principal	Surety
By _____	By _____
	(Signature) Attorney in Fact
	Address _____

Title _____	_____
(Contractor's Seal)	(Printed) MS Agent

	(Signature) MS Agent
	Address _____

	(Surety Seal)

	Mississippi Insurance ID Number



BID BOND

KNOW ALL MEN BY THESE PRESENTS, that we _____
Contractor

Address

City, State ZIP

As principal, hereinafter called the Principal, and _____
Surety

a corporation duly organized under the laws of the state of _____

as Surety, hereinafter called the Surety, are held and firmly bound unto State of Mississippi, Jackson, Mississippi

As Obligee, hereinafter called Obligee, in the sum of **Five Per Cent (5%) of Amount Bid**

Dollars(\$ _____)

for the payment of which sum will and truly to be made, the said Principal and said Surety, bind ourselves, our heirs, executors, administrators, successors and assigns, jointly and severally, firmly by these presents.

WHEREAS, the Principal has submitted a bid for **Emergency Bridge Repair on SR 14, Bridge Nos. 11693 & 14407, known as State Project No. SP-0047-01(016) / 107693301 in Sharkey County.**

NOW THEREFORE, the condition of this obligation is such that if the aforesaid Principal shall be awarded the contract, the said Principal will, within the time required, enter into a formal contract and give a good and sufficient bond to secure the performance of the terms and conditions of the contract, then this obligation to be void; otherwise the Principal and Surety will pay unto the Obligee the difference in money between the amount of the bid of the said Principal and the amount for which the Obligee legally contracts with another party to perform the work if the latter amount be in excess of the former, but in no event shall liability hereunder exceed the penal sum hereof.

Signed and sealed this _____ day of _____, 20__

(Witness)

(Principal) (Seal)

By: _____
(Name) (Title)

(Surety) (Seal)

(Witness)

By: _____
(Attorney-in-Fact)

(MS Agent)

Mississippi Insurance ID Number