

GENERAL INDEX

INCLUDED THIS PROJECT	BEGIN WITH SHEET
<input checked="" type="checkbox"/> ROADWAY	1
<input type="checkbox"/> PERMANENT SIGNS	1001
<input type="checkbox"/> TRAFFIC SIGNALS	2001
<input type="checkbox"/> ITS COMPONENTS	3001
<input type="checkbox"/> LIGHTING	4001
<input type="checkbox"/> (RESERVED)	5001
<input checked="" type="checkbox"/> ROADWAY STANDARD DWGS	6001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (LRFD)	7001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (STD. SPEC.)	7501
<input type="checkbox"/> BRIDGE	8001
<input type="checkbox"/> CROSS SECTIONS	9001

BRIDGE STRUCTURES REQ'D.

NONE

BOX BRIDGES REQ'D.

NONE

CONVENTIONAL SYMBOLS

COUNTY LINE	---
TOWN CORPORATION LINE	---
SECTION LINE	---
EXISTING ROAD OR TRAVELED WAY	---
PROPOSED ROAD OR TRAVELED WAY	---
RAILROAD	---
SURVEY LINE	---
BRIDGES	---

LENGTH OF ROADWAY	34,493.572 FT.
LENGTH OF BRIDGES	0.000 FT.
LENGTH OF PROJECT (NET)	34,493.572
LENGTH OF EXCEPTIONS	0.000 FT.
LENGTH OF PROJECT (GROSS)	34,493.572

EQUATIONS

NB LANE	
STA. 41+94.321 BK =	STA. 43+08.520 AH (-114.199)
STA. 134+24.435 BK =	STA. 133+90.120 AH (+34.315)
STA. 191+19.203 BK =	STA. 190+88.920 AH (+30.283)
STA. 222+06.144 BK =	STA. 223+17.980 AH (-111.836)
STA. 265+87.299 BK =	STA. 265+43.660 AH (+43.639)
STA. 338+23.500 BK =	STA. 338+22.130 AH (+1.370)
TOTAL EQ. =	(-116.428)

LENGTH DATA

SB LANE	
STA. 41+34.609 BK =	STA. 41+34.780 AH (-0.171)
STA. 41+53.990 BK =	STA. 41+77.120 AH (-23.130)
STA. 221+41.657 BK =	STA. 221+67.400 AH (-25.743)
STA. 338+23.500 BK =	STA. 338+22.130 AH (+1.370)
TOTAL EQ. =	(-47.674)

EXCEPTIONS

NONE

STATE OF MISSISSIPPI

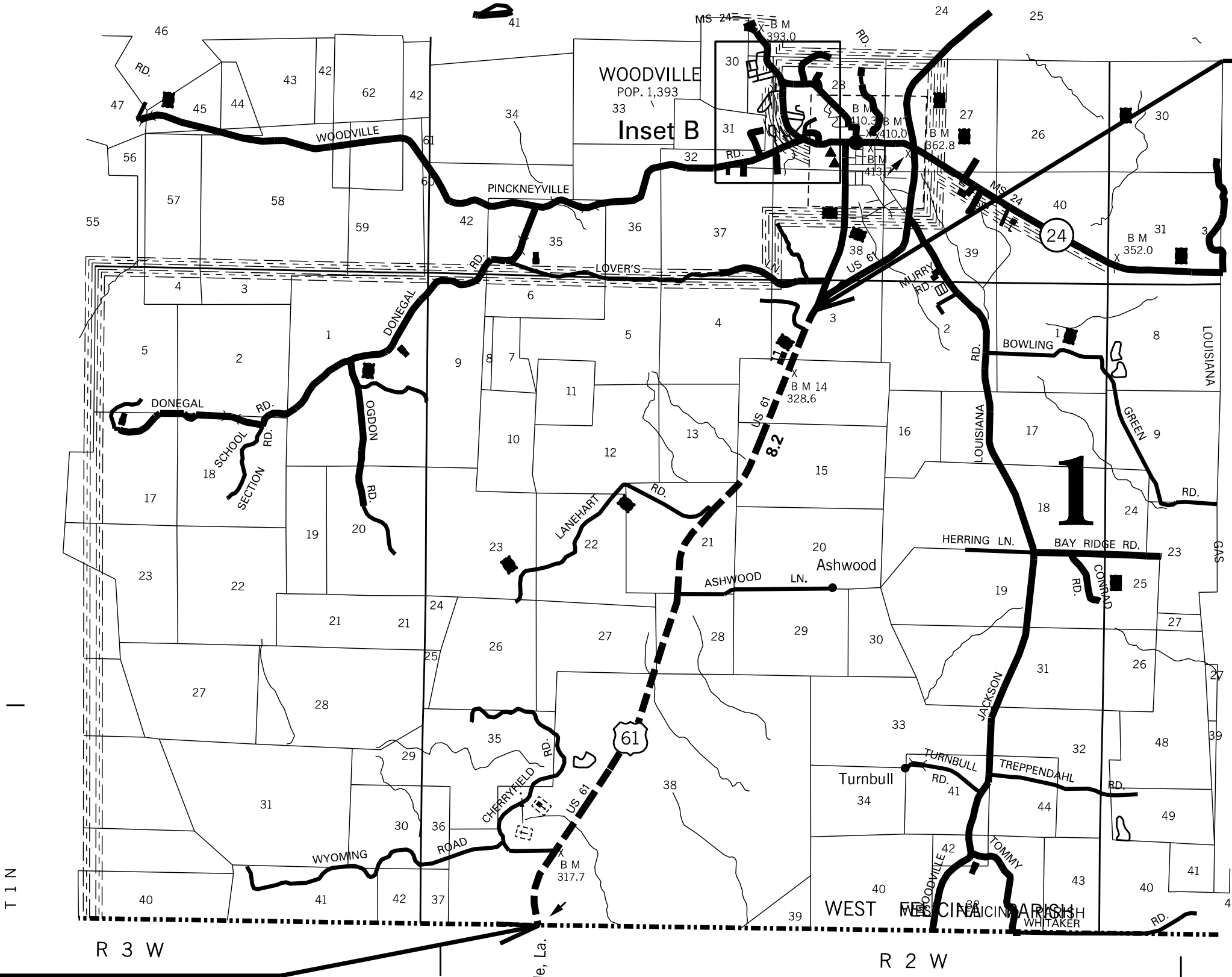
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE OF PROPOSED
STATE HIGHWAY
FEDERAL AID PROJECT NO. NH-0009-01(137)

U.S. 61
LA STATE LINE NORTH
TO WOODVILLE
WILKINSON COUNTY

FMS. CONST. NO. 107604 /301000

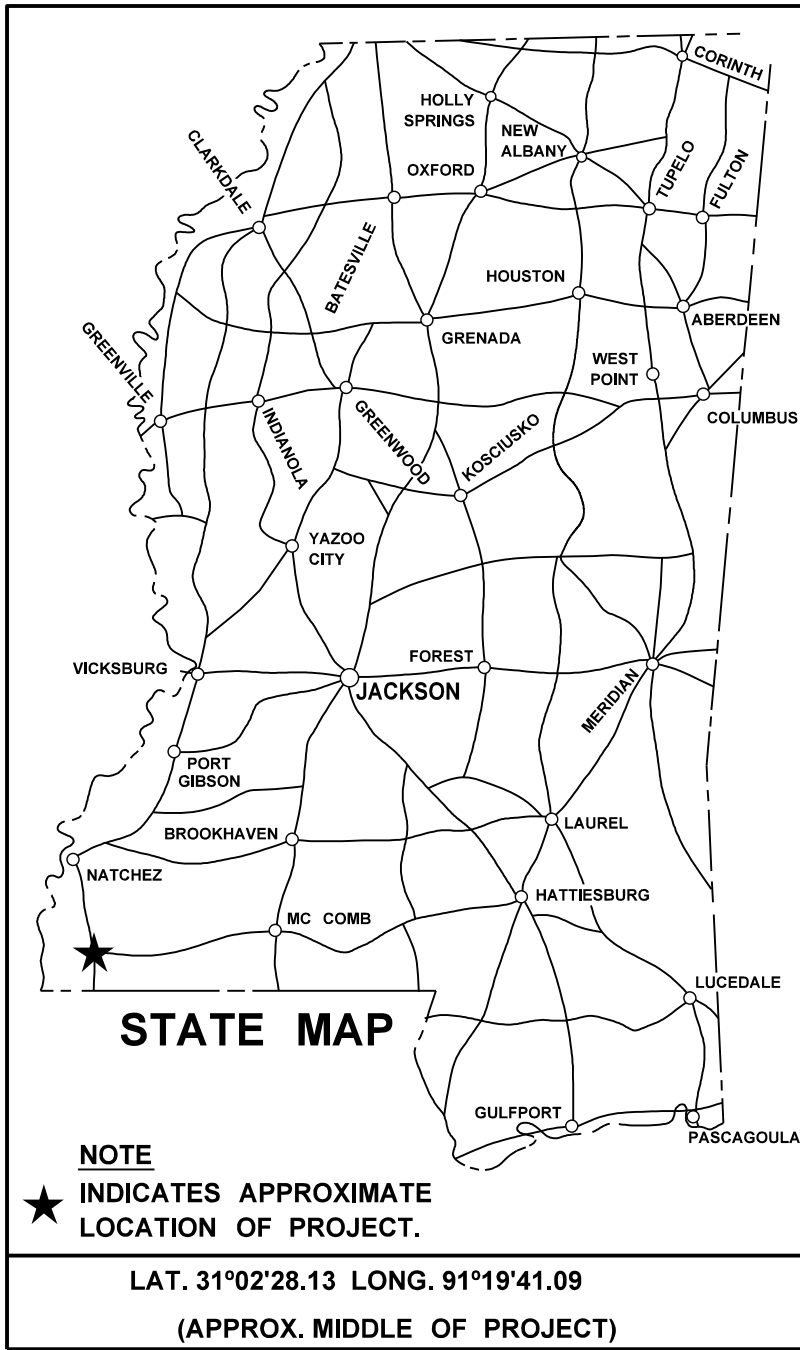
PLAN		1 IN. = 100 FT.
PROFILE {		1 IN. = 100 FT.
HORIZ. VERT.		1 IN. = 10 FT.
LAYOUT		1 IN. = 5,523 FT.



B.O.P STA. 10 + 00

E.O.P STA. 356 + 10

STATE	PROJECT NUMBER	SHEET NO.
MISSISSIPPI	NH-0009-01(137)	1



DESIGN CONTROL	
MPH =	V (SPEED DESIGN)
ADT () =	: ADT () =
DHV =	: D = % T = %

PERMITS ACQUIRED BY MDOT	
WETLANDS AND WATERS PERMITS (NECESSARY FOR ULTIMATE IMPROVEMENTS ONLY):	
NATIONWIDE #14	<input checked="" type="checkbox"/>
NATIONWIDE (OTHER)*	<input checked="" type="checkbox"/>
GENERAL*	<input checked="" type="checkbox"/>
INDIVIDUAL (404)*	<input checked="" type="checkbox"/>
* ACQUISITION OF PERMITS FOR TEMPORARY IMPACTS DURING CONSTRUCTION ARE THE RESPONSIBILITY OF THE CONTRACTOR	
STORMWATER PERMIT <input checked="" type="checkbox"/>	
Y	REQUIRED, CNOI SUBMITTED BY MDOT (DISTURBED AREA=5 ACRES)
S	REQUIRED, CNOI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES)
N	NO STORMWATER PERMIT REQUIRED (<1 ACRE)
APPROVED BY: _____	

P S & E DATE: 08/03/2017

APPROVED:	DEPUTY EXECUTIVE DIRECTOR / CHIEF ENGINEER
EXECUTIVE DIRECTOR	



STATE	PROJECT NO.
MISS.	NH-0009-01(137)

DESCRIPTION OF SHEET	REV. DATE	WKG. NO.	SH. NO.	DESCRIPTION OF SHEET	REV. DATE	WKG. NO.	SH. NO.
TITLE AND LAYOUT SHEET (1)			1	SPECIAL DESIGN SHEETS (11)			
DETAILED INDEX (1)		DI-1	2	DETAIL OF CONSTRUCTION SIGNING		DCS-1	36
GENERAL NOTES (1)		GN-1	3	△ RUMBLE STRIP FOR 4-LANE HIGHWAYS		RS-4L	37
TYPICAL SECTION SHEETS (3)				TRAFFIC CONTROL DETAILS (DRUM PLACEMENT & SHOULDER CLOSURE)		TCP-SC	38
Typical Section-Mainline 4 Lane Section		TS-1	4	TYPICAL PAVEMENT MARKING DETAIL FOR MEDIAN CROSSEOVERS		SDXO-1	39
Typical Section-Mainline 4 Lane Section (MILL & INLAY AREAS)		TS-2	5	2-WAY CLEAR RAISED PAVEMENT MARKERS PLACED ON SIDE ROADS (4-LANE)		CRPMSR-4	40
Typical Section-Mainline 4 Lane Section (FAILED AREAS)		TS-3	6	DETAIL FOR CURB AND ISLAND STRIPING		CI-1	41
QUANTITY SHEETS (5)				△ TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATE AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSER) (EXTENDED PERIOD)		SDTCP-4	42
Summary of Estimated Quantities		SQ-1	7	△ TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATE AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSER) (WORK DAY ONLY)		SDTCP-5	43
Estimated Quantities for Traffic Control Signing		TCQ-1	8				
Estimated Quantities for Milling AND Leveling		EQ-1	9				
Estimated Quantities for Asphalt and Granular Materials		EQ-2	10				
Estimated Base Failure Repair Quantities		EQ-3	11				
PLAN & PROFILE SHEETS (24)				HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS		SDTCP-10	44
STA. 10+00 TO STA. 40+00		WK3 LT.	12				
STA. 10+00 TO STA. 40+00		WK3 RT.	13	TRAFFIC RECORDER WIM KISTLER SYSTEM ON 4-LANE DIVIDED HIGHWAYS		TRS-1	45
STA. 40+00 TO STA. 70+00		WK4 LT.	14	PAVEMENT MARKING DETAIL @ GORE AREA		PMD-1	46
STA. 40+00 TO STA. 70+00		WK4 RT.	15				
STA. 70+00 TO STA. 100+00		WK5 LT.	16				
STA. 70+00 TO STA. 100+00		WK5 RT.	17	STANDARD DRAWINGS (8)			
STA. 100+00 TO STA. 130+00		WK6 LT.	18	PAVEMENT MARKING DETAILS FOR 2-LANE AND 4-LANE DIVIDED HIGHWAYS		PM-1	6120
STA. 100+00 TO STA. 130+00		WK6 RT.	19	PAVEMENT MARKING DETAILS FOR 4-LANE AND 5-LANE UNDIVIDED HIGHWAYS		PM-2	6121
STA. 130+00 TO STA. 160+00		WK7 LT.	20	PAVEMENT MARKING LEGEND DETAILS		PM-5	6124
STA. 130+00 TO STA. 160+00		WK7 RT.	21	PAVEMENT MARKING LEGEND DETAILS		PM-6	6125
STA. 160+00 TO STA. 190+00		WK8 LT.	22	TYPICAL CROSSOVER DELINEATION		SN-8B	6235
STA. 160+00 TO STA. 190+00		WK8 RT.	23				
STA. 190+00 TO STA. 220+00		WK9 LT.	24	TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO LANE ROADS		TCP-11	6260
STA. 190+00 TO STA. 220+00		WK9 RT.	25	TRAFFIC CONTROL PLANS UNEVEN PAVEMENT DETAILS		TCP-14	6263
STA. 220+00 TO STA. 250+00		WK10 LT.	26	TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LN. AND 4-LN DIVIDED HIGHWAYS.		TCP-15	6264
STA. 220+00 TO STA. 250+00		WK10 RT.	27				
STA. 250+00 TO STA. 280+00		WK11 LT.	28				
STA. 250+00 TO STA. 280+00		WK11 RT.	29				
STA. 280+00 TO STA. 310+00		WK12 LT.	30				
STA. 280+00 TO STA. 310+00		WK12 RT.	31				
STA. 310+00 TO STA. 338+22.13(AH)		WK13 LT.	32				
STA. 310+00 TO STA. 338+22.13(AH)		WK13 RT.	33				
STA. 338+22.13(AH) TO STA. 356+10		WK14 LT.	34				
STA. 338+22.13(AH) TO STA. 356+10		WK14 RT.	35				
				TOTAL SHEETS (54)			

DISTRICT 7		
PS & E PLANS-DATE 08/03/2017		
FMS CON. # 107604/301000		
REVISIONS		
DATE	SHEET NO.	BY
9/18/17	1,2,5,7,8	MA
9/20/17	7	MA

9/20/2017 08:40:36 (2) INDEX#1.DGN

PLAN DIVISION

ROADWAY

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

REVISION

DATE

DESIGN TEAM

CHECKED

DATE

CB

BY

REVISION

DATE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

COUNTY: WILKINSON

PROJ. NUM.: NH-0009-01(137)

FILENAME: (2) INDEX#1.DGN

DETAILED INDEX

WORKING NUMBER

DI-1

SHEET NUMBER

2

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

STATE	PROJECT NO.
MISS.	NH-0009-01(137)

GENERAL NOTES

- 1

THE LOCATION AND SPACING OF SIGNS AS SHOWN ON THE TRAFFIC CONTROL PLANS ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.
- 2

ALL TRAFFIC CONTROL DEVICES ON THIS PROJECT SHALL COMPLY WITH THE MUTCD (LATEST EDITION).
- 3

FLUORESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.
- 4

ALL PLASTIC DRUMS SHALL HAVE A BALLASTING COLLAR MADE FROM RECYCLED TRUCK TIRES OR SUITABLE MATERIAL.
- 5

ALL SIGNS AND DELINEATORS THAT CONFLICT WITH THE CONSTRUCTION OF THIS PROJECT SHALL BE REMOVED AND RESET BY THE CONTRACTOR; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- 6

THE GRASS ON EXISTING SHOULDERS SHALL BE REMOVED PRIOR TO THE PLACEMENT OF SHOULDER MATERIAL BY LIGHTLY BLADING OR CLOSELY MOWING; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- 7

ALL LOCAL ROADS TO BE PAVED TO THE R.O.W. LIMITS, OR AS DIRECTED BY THE ENGINEER.
- 8

ALL RAMPS AND DRIVEWAYS TO BE PAVED TO THE SHOULDER LINE, MINIMUM, OR FURTHER, AS DIRECTED WITHIN THE LIMITS OF THE R.O.W.
- 9

WHERE MILLING OF THE ROADWAY IS REQUIRED, THE CONTRACTOR SHALL PROVIDE OUTLETS IN THE EXISTING SHOULDER AT SUFFICIENT INTERVALS TO PREVENT POOLING OR STANDING WATER ON MILLED SURFACE; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- 10

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT EXISTING STRUCTURES SUCH AS SIGNS, PIPES, INLETS, APRONS, BRIDGES, ETC., FROM DAMAGE WHICH MIGHT OCCUR DURING CONSTRUCTION. THE CONTRACTOR SHALL REPLACE OR REPAIR, AS DIRECTED BY THE ENGINEER, ANY STRUCTURES DAMAGED DURING THE LIFE OF THE CONTRACT. NO PAYMENT WILL BE MADE FOR REPLACEMENT OR REPAIR OF DAMAGED ITEMS.
- 11

THE CONTRACTOR SHALL REMOVE EXISTING PAVEMENT MARKERS PRIOR TO PLACING ASPHALT; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- 12

TEMPORARY STRIPING IS REQUIRED IMMEDIATELY AFTER MILLING, PRELIMINARY LEVELING AND OVERLAYING OPERATION; WEATHER PERMITTED OR PRIOR TO OPENING THE AREA TO TRAFFIC; TEMPORARY STRIPING SHALL BE PLACED IN THE SAME LOCATION AND LAYOUT AS PERMANENT STRIPE.
- 13

ALL ASPHALT AND CONCRETE CURBS ALONG THE LOCAL ROADS FROM B.O.P. TO E.O.P. SHALL BE PAINTED (TWO APPLICATIONS) WITH WHITE TRAFFIC PAINT AND TRAFFIC BEADS; COST SHALL BE ABSORBED IN OTHER PAY ITEMS.

- 14

THE BOP AND EOP WILL REQUIRE 1 1/2" OF MILLING. (SEE TYPICAL SECTION TS-1, TS-3)
- 15

BIDDERS ARE ADVISED THAT HARD COPIES OF ANY ADDENDA FOR THIS PROJECT WILL NO LONGER BE MAILED. ALL ADDENDA FOR THIS PROJECT WILL BE POSTED ON WWW.MDOT.MS.GOV UNDER THE PROPOSAL ADDENDA COLUMN. IT IS THE BIDDER'S RESPONSIBILITY TO CHECK AND SEE IF ANY ADDENDA HAVE BEEN POSTED FOR THIS PROJECT. PLEASE CONTACT CONTRACT ADMINISTRATION DIVISION AT 601-359-7700 FOR ANY QUESTIONS REGARDING ELECTRONIC ADDENDA.
- 16

FOR ASPHALT PAVING, 9.5 MM MT MIXTURE WILL BE USED FOR MAINLINE, CROSSTOVERS, LOCAL ROADS, AND DRIVEWAY PADS. 19MM MT AND 12.5MM MT MIXTURE, WILL BE USED FOR THE FAILED AREAS AS SHOWN IN TS-3.
- 17

GRANULAR MATERIAL WILL NOT BE ALLOWED TO BE PLACED DIRECTLY ON THE SURFACE LIFT OF ASPHALT, BUT MUST BE PLACED DIRECTLY ON THE SHOULDER OR A ROAD WIDENER MACHINE USED AND APPROVED BY THE PROJECT ENGINEER.
- 18

IN THE ASPHALT FAILED AREAS, BACKFILL AND STABILZIED FAILED AREAS WITH CRUSHED STONE IN LIFTS TO AN ELEVATIONS OF 7.5" BELOW THE ORIGINAL FINISHED PAVEMENT ELEVATION. NO LIFT OF CRUSHED STONE SHALL BE GREATER THAN 6" IN THICKNESS. DENSITY WILL BE REQUIRED ON THE ASPHALT PAVEMENT AND CRUSHED STONE PER STANDARD SPECIFICATIONS.
- 19

IN THE FAILED AREAS, THE FINAL GRADE OF THE ASPHALT SHALL MATCH THE EXISTING GRADE OF THE HIGHWAY. ALL REPAIRS MUST BE COMPLETED BY THE END OF THE WORK DAY AND THE LANE CLOSURES MUST BE REMOVED FROM THE HIGHWAY SO THAT ALL LANES ARE OPEN.
- 20

STORAGE OF FLAMMABLE MATERIALS WILL NOT BE ALLOWED UNDER ANY BRIDGE STRUCTURES.
- 21

THE EXISTING TRAFFIC RECORDER EQUIPMENT CABINET AND COMPONENTS @ STA. 145 +/- SHALL BE REMOVED AND PAYMENT WILL BE ABSORBED IN OTHER PAY ITEMS; WIM KISTLER SYSTEM CABINET AND COMPONENTS SHALL BECOME PROPERTY OF MDOT.

B:7/2017 9:07:49 AM 0310NOTES.DGN PLAN DIVISION ROADWAY MISSISSIPPI DEPARTMENT OF TRANSPORTATION

				BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
				REVISION	GENERAL NOTES	
				DATE	COUNTY: WILKINSON PROJ. NUM.: NH-0009-01(137)	
				DESIGN TEAM	FILENAME: (3)GNOTES.DGN	WORKING NUMBER GN-1
				CHECKED		SHEET NUMBER 3
				DATE		