

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.	<u>1</u>	DATED	<u>4/10/2018</u>	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	<u>2</u>	DATED	<u>4/19/2018</u>	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____

Number	Description
1	Revised Table of Contents; Added NTB No. 854; Revised Bid Items; Amendment EBS Download Required.
2	Revised NTB No. 739; Revised Bid Items; Amendment EBS Download Required.

TOTAL ADDENDA: 2
 (Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

 Contractor

BY _____
 Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

 President Address

 Secretary Address

 Treasurer Address

The following is my (our) itemized proposal.

MP-5019-50(013)/ 306368301000

Neshoba County(ies)

Revised 01/26/2016

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 739

CODE: (SP)

DATE: 4/19/2018

SUBJECT: Scope of Work

PROJECT: MP-5019-50(013)/306368301 -- Neshoba County

The contract documents do not include an official set of plans, but may by reference include some Standard Drawings or Special Drawings. All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on this project shall consist of the following:

Mill and overlay approximately 15 miles of existing asphalt pavement on SR 19 in Neshoba County. The BOP is at the Beginning of State Maintenance Limits at Old Highway 15 approximately 150 feet north of Saxon Airport Road in Philadelphia and the EOP is at the Winston/Neshoba County Line.

The existing pavement consists of 5½” to 15½” of asphalt over six inches (6”) of stabilizer or clay gravel with 12-foot to 14-foot lanes and 2-foot and variable shoulders.

Construction signage shall be installed as per the detail sheets included prior to the beginning of work.

Failed areas shall be repaired full depth with 12.5-mm, MT, asphalt in advance of milling and paving operations.

The existing curves shall be widened at the locations provided in a following table. The curve widening shall consist of a 2½” layer of 12.5-mm, MT, asphalt followed by a 2” layer of 12.5-mm, MT, asphalt. The curve widening shall be two feet (2’) wide.

Should the existing widened shoulder fail during the paving operations, said shoulder shall be removed and the underlying material excavated 2½’ wide by 3” depth and replaced with 12.5-mm, MT, asphalt.

The existing asphalt roadway shall be milled 1½” and overlaid with 1½” compacted 9.5 mm, MT, asphalt.

The SR 15/SR 19 Connector Road shall be milled 1½” and overlaid with 1½” compacted 9.5-mm, MT, asphalt according to the attached photo illustration.

A bituminous curb and island shall be constructed at station 303+73 across from Scotty's Country Store on the South side of SR 19. This bituminous curb and island shall be constructed according to the attached detail. The island shall be backfilled with four (4") of asphalt.

The bridge approach slabs shall be milled 1½" to remove the existing asphalt and overlaid with compacted 1½" of 9.5-mm, MT, asphalt.

Traffic on the milled surface shall be limited to seven (7) days.

Local public roads shall be milled and overlaid to the end of the existing asphalt pavement, end of MDOT maintenance or to right-of-way or as directed. Where a minimum of five feet (5') of shoulder width can be paved at the beginning of local road radii, a 100-foot asphalt pavement taper shall be constructed. Said taper shall be six inches (6") thick and shall conform to the detail drawings.

Temporary pavement markings shall be placed at the end of each day's paving operations and prior to opening the road to traffic. Permanent pavement markings shall be constructed after completion of all paving operations.

Permanent raised pavement markers shall be installed on mainline and local public roads after completion of all paving operations.

Any existing asphalt curbs that are deemed unsuitable by the Engineer shall be removed by the Contractor. The cost associated with this shall be included in other items bid.

The face of all existing undisturbed curbs shall be painted with at least two (2) coats of white traffic paint with glass beads being required in the top coat. The cost associated with the painting of new or existing curb is to be included in other items bid.

The face of all existing concrete islands shall be painted with at least two coats of white traffic paint with glass beads being required in the top coat. The cost associated with the painting of new or existing curb is to be included in other items bid.

Crushed Limestone material shall be used to bring roadway shoulders to grade. It is not anticipated that the granular material will be required throughout the length of the project but only in areas deficient of shoulder material and as directed. Crushed/Recycled concrete shall not be used for shoulder material.

Existing guardrail, terminal end sections, and bridge end sections shall be removed and replaced with new guardrail, terminal end sections, and bridge end sections. The Contractor shall deliver all existing guardrail, terminal end sections, and bridge end sections to the MDOT Neshoba County Maintenance Facility located at 1115 Highway 15 Bypass Philadelphia, MS. The Contractor shall give sufficient advance notice to ensure that MDOT Maintenance personnel will be on hand to direct the delivery. All guardrail removed is to be replaced the same day and prior to reopening the adjacent lane of traffic. This work shall consist of the following sequence of operations: removal of the existing guardrail and posts, milling the entire guardrail pad and

repaving the guardrail pad, and installation of the new guardrail. Voids created by removal of posts, concrete anchors, footings, etc. shall be backfilled and tamped in accordance with Section 203 of the Standard Specifications. The cost of the guardrail delineator removal and the object marker signs shall be included in other items bid.

The following recommendations from Bridge Division will pertain to Bridge #70.7 and will only be modified by the approval of the Director of Structures, State Bridge Engineer.

1. A milling machine with a milling head no larger than four feet (4') wide may be used to remove the asphalt overlay to a depth such that the milling head does not come in contact with the bridge deck. However, the asphalt thickness remaining on the bridge after milling shall not be less than one-half inch (½"). Once this depth is reached, the Engineer shall examine the remaining asphalt to determine whether it requires removal. If the remaining asphalt appears to have a sufficient bond to the bridge deck, then the ½-inch lift may be left in place upon approval by the Director of Structures, State Bridge Engineer.
2. If the remaining asphalt is not sufficiently bonded to the bridge deck, the Contractor must utilize another approach to remove the rest of the asphalt in a fashion that does no harm to the existing structure. Once this process is complete, the bare bridge deck shall be inspected before work continues. Upon inspection, if the deck has no signs of deterioration and the riding surface is acceptable for traffic, the deck shall be left bare upon approval by the Director of Structures, State Bridge Engineer.
3. In addition to milling the bridge deck, the Contractor shall also mill the existing roadway in order to provide a smooth transition between the existing alignment and the new asphalt overlay. The amount of milling necessary for this transition shall be determined in the field and approved by the Engineer.
4. If the remaining ½-inch lift of asphalt is properly bonded or if the bridge deck shows signs of deterioration, then a new lift of asphalt shall be placed upon approval by the Director of Structures, State Bridge Engineer. The new overlay course shall be 1½" thick.
5. The use of dynamic rollers is prohibited for compaction of the asphalt lifts on the bridge deck. Only static compaction will be allowed.
6. **Any damages that occur to the existing structure during the construction process shall be repaired as directed by the Engineer at no additional cost to the state.**

GENERAL NOTES

Milling and paving operations shall be performed such that a negative two percent (-2%) slope from centerline is provided in normal crown roadway sections. Superelevation through curves shall be maintained as it currently exists or improved as directed.

Temporary asphalt joints (paper joints) shall be constructed at the end of each day's milling operations where the milled surface joins the existing asphalt pavement surface. Paper joints

shall be a minimum of nine feet (9') in length and for the full width of the milled surface. Paper joints shall be adequately maintained.

The Contractor is responsible for providing shoulder drainage outlets as applicable in milled areas. Payment for these outlets shall be included in the bid price for the milling of bituminous pavement.

The reclaimed asphalt pavement (RAP) material removed by the milling operation shall become the property of the Contractor with the exception of 10,000 tons or 50% of the total anticipated quantity, whichever is less, and shall be delivered to the Philadelphia Maintenance Headquarters stockpile. Sufficient advance notice shall be given to ensure that MDOT Maintenance personnel will be on hand to direct the delivery. The Contractor shall also provide MDOT with an operator and the necessary equipment to stockpile the delivery. The cost of which shall be absorbed.

Existing asphalt/concrete driveway connections shall be milled or removed and replaced with new asphalt connections. Driveway pads are to be milled the same depth as the adjacent roadway milling and paved with the same depth and type of asphalt mixture as the adjacent roadway. Milling and paving of driveway pads shall be coordinated and conducted in a manner to prevent irregularities in the longitudinal and transverse slopes of the roadway. Payment will be made using the applicable milling and asphalt pay items.

Potholes that may exist or occur in the existing pavement are to be patched in a timely manner. Patching of potholes shall be considered an absorbed item.

Where applicable the existing shoulders are to be raised to match the new pavement elevation by placing variable depth crushed limestone on the existing shoulders. Placement of the granular material on the finished asphalt course shall not be permitted. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%). Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%). The cost of blading will be an absorbed item and is not to be included in the price of pay items bid.

Removal of the existing shoulder material shall be coincident with the milling/overlaying operation to prevent the possible ponding of water. No payment will be made for blading or removal of the existing shoulder material. Any material excavated from the existing shoulder shall be used to raise the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer and will be an absorbed item. Material which cannot be placed in adjacent areas and deemed to be excess excavations by the Engineer will be an absorbed item.

Temporary stripe will be required immediately after milling and overlaying and prior to opening the area to traffic. Temporary stripe is to be placed in the same location and layout as permanent stripe.

All permanent striping will be thermoplastic. The width of the permanent stripe will be six inches (6").

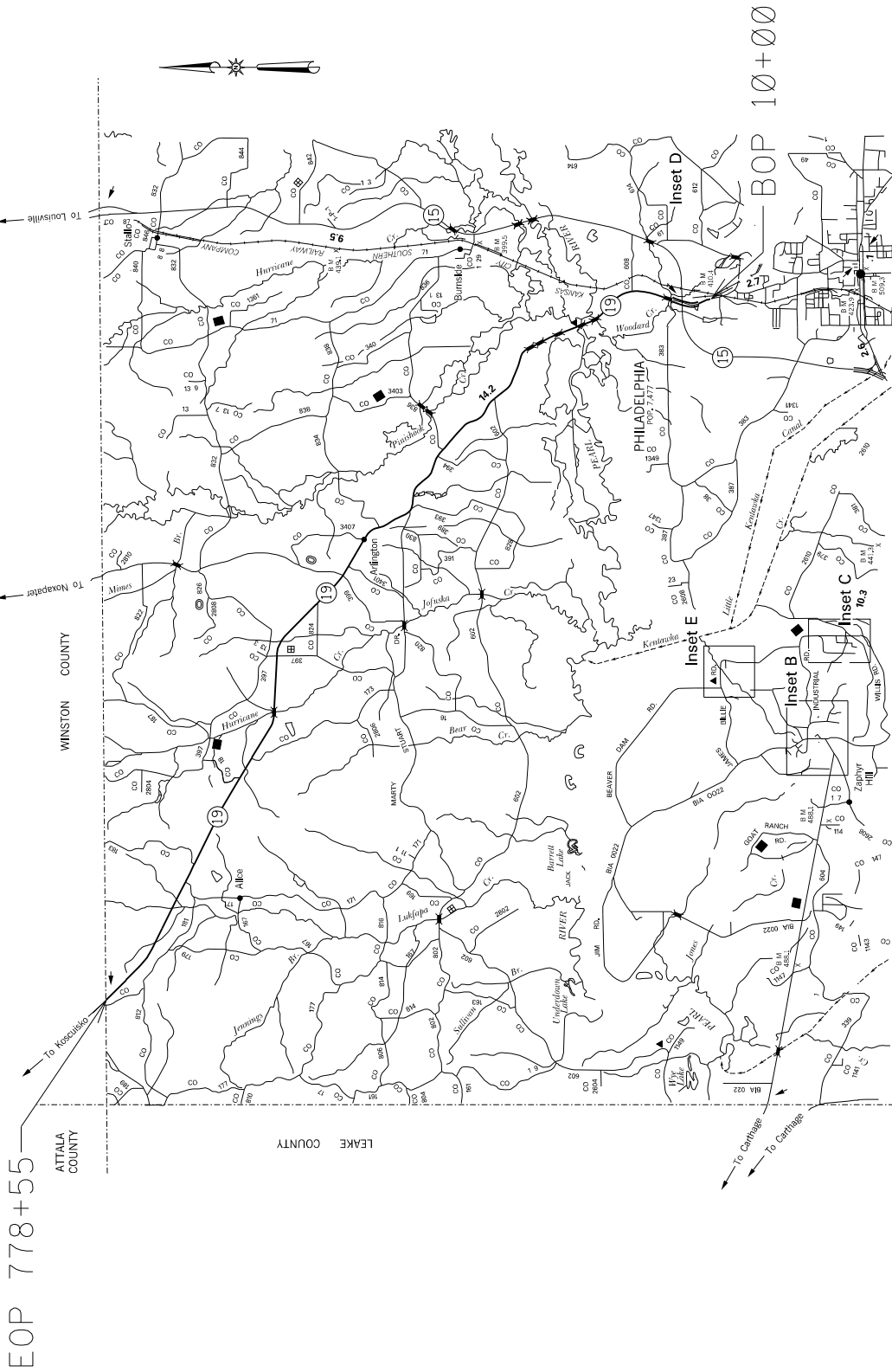
The Contractor shall erect and maintain construction signing, and provide and maintain all temporary signs and traffic control devices necessary to safely conduct traffic through the work area in accordance with the Traffic Control Plan and the MUTCD.

All traffic control devices shall meet current MDOT and MUTCD requirements.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal. The cost is to be included in the prices of items bid. Failure of the Contractor to remove debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor. As described in the applicable Notice-To-Bidders, final project cleanup is required and will be completed prior to the scheduling of the final inspection.

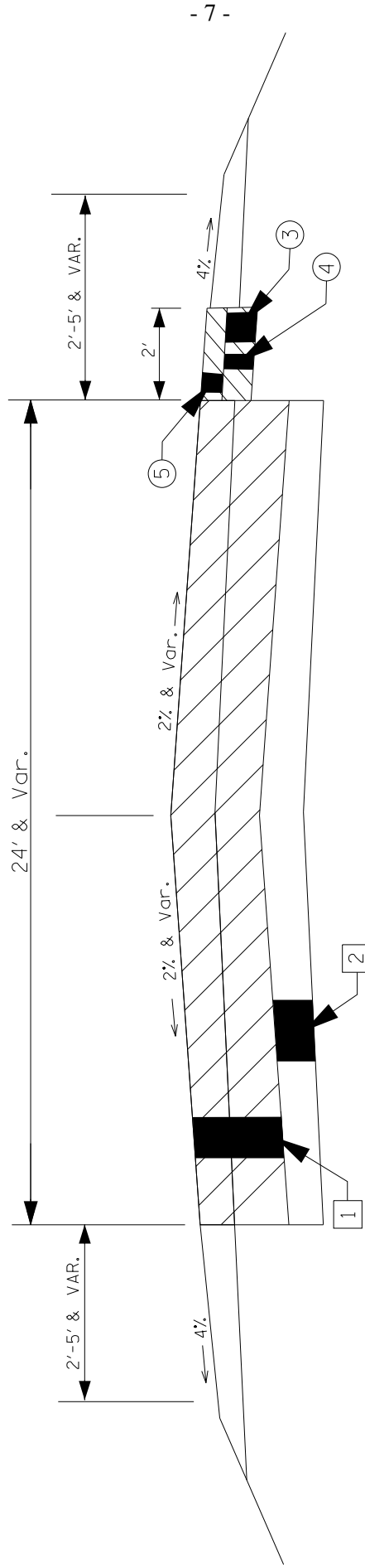
It shall be the responsibility of the Contractor to protect existing structures such as pipes, aprons, signs, utilities, etc. from damage occurring as a result of construction activities. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged during the life of the contract. No payment will be made for replacements and or repairs resulting from such damages.

MP-5019-50(013)
 306368-31000
 SR 19 - NESHOBA COUNTY
 PROJECT LOCATION MAP



SR 19 - NESHOBIA COUNTY
 TYPICAL SECTION - TRENCH WIDENING

STATIONS: 39+50 - 778+55



EXISTING

- 1 5 1/2" - 15 1/2" Existing Thickness based on Core Evaluation
- 2 6" Granular Material

PROPOSED

- 3 Blade Existing Shoulder Material Back 2' and to a Depth of 4 1/2" and use Bladed Shoulder Material to Bring Shoulders to Grade after Widening
- 4 2 1/2" 12.5mm, MT, Asphalt Pavement
- 5 2" 12.5mm, MT, Asphalt Pavement

NOTE 1: This Typical Represents Areas to Trench Widened with 12.5mm Mix, MT, Asphalt Pavement

NOTE 2: Table in Proposal will List Station Locations

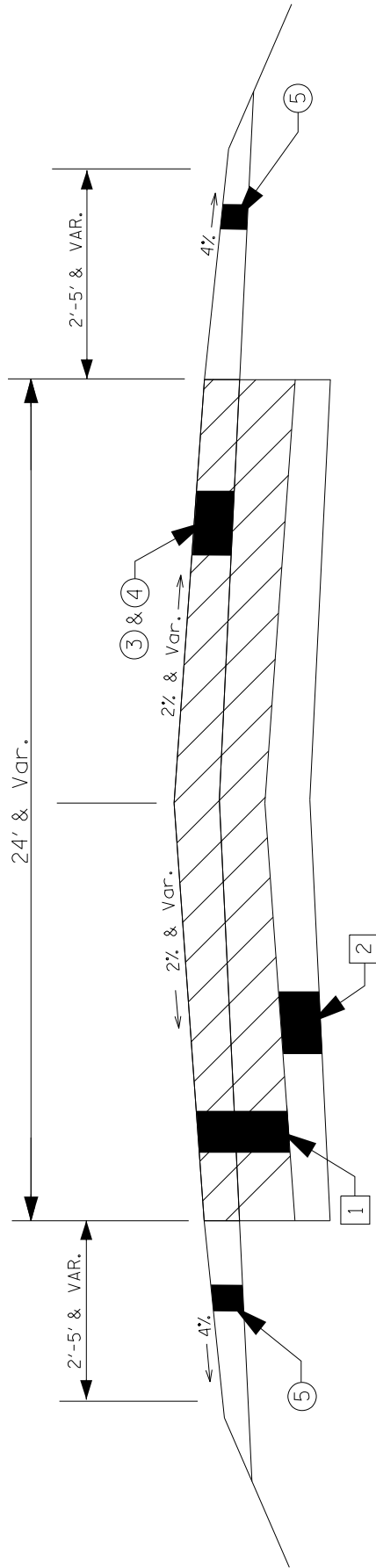
SR 19 - NESHOPA COUNTY

TYPICAL SECTION - MILL & OVERLAY

STATIONS: 10+00 - 12+00

39+50 - 98+00

163+00 - 778+55



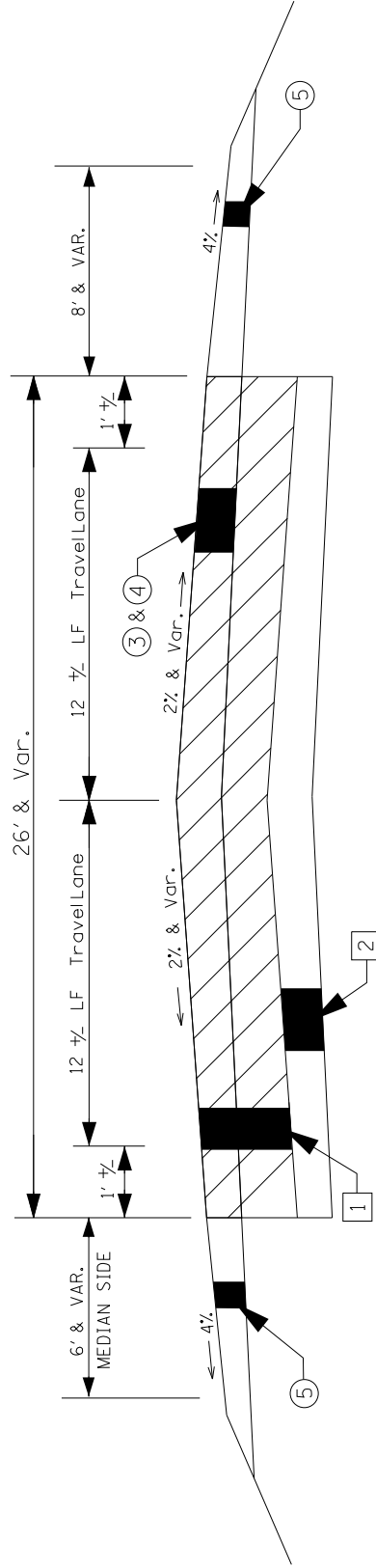
EXISTING

PROPOSED

- 1 5 1/2" - 15 1/2" Existing Thickness based on Core Evaluation
- 2 6" Granular Material
- 3 1 1/2" Milling (Correct to 2% Normal Crown or SE Where Needed)
- 4 1 1/2" 9.5mm, MT, Asphalt Pavement
- 5 Crushed Limestone 3" and Var.

NOTE 1: Prior to milling, Repair any failed areas full depth with 12.5mm Mix, MT, Asphalt Pavement

SR 19 - NESHOPA COUNTY
 TYPICAL SECTION - MILL & OVERLAY
 STATIONS: 12+00 - 39+50
 4 LANE, NORTH BOUND



EXISTING

- 1 5 1/2"-15 1/2" Existing Thickness based on Core Evaluation
- 2 6" Granular Material

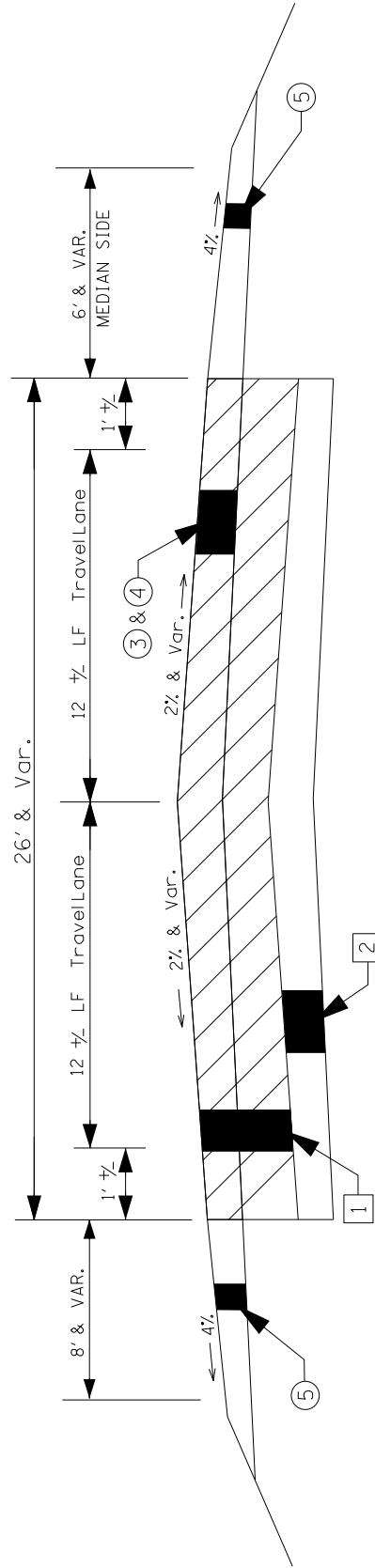
PROPOSED

- 3 1 1/2" Milling (Correct to 2% Normal Crown or SE Where Needed)
- 4 1 1/2" 9.5mm, MT, Asphalt Pavement
- 5 Crushed Limestone 3" and Var.

NOTE 1: Prior to milling, Repair any failed areas fulldepth with 12.5mm Mix, MT, Asphalt Pavement

NOTE 2: On existing widened shoulders that fail during milling operation, remove HMA in-place and excavate underlying shoulder 2' wide by 3" depth, inlaying with 12.5 mm Mix, MT.

SR 19 - NESHOBIA COUNTY
 TYPICAL SECTION - MILL & OVERLAY
 STATIONS: 12+00 - 39+50
 4 LANE, SOUTH BOUND



EXISTING

- 1 5 1/2"-15 1/2" Existing Thickness based on Core Evaluation
- 2 6" Granular Material

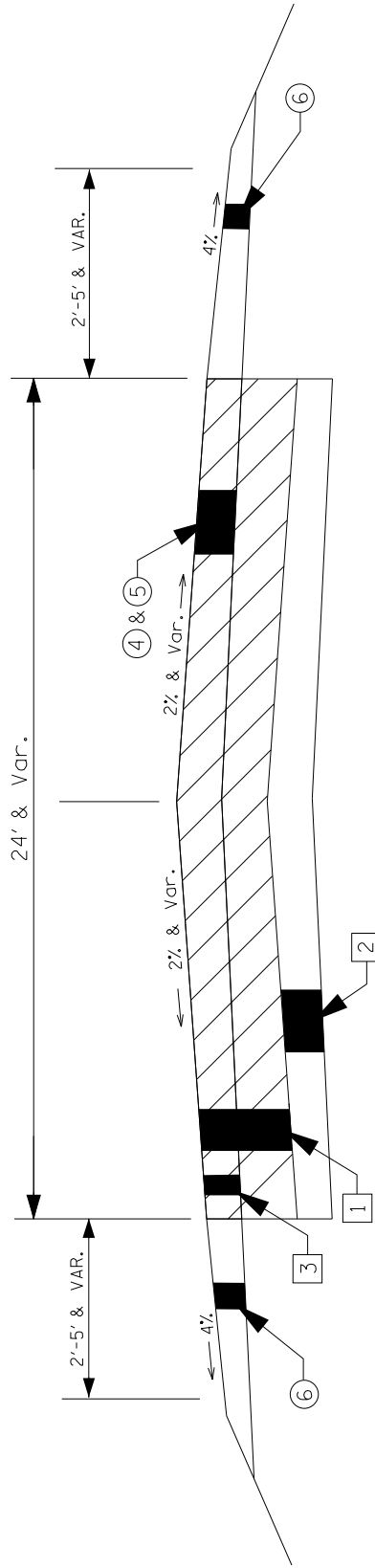
PROPOSED

- 3 1 1/2" Milling (Correct to 2% Normal Crown or SE Where Needed)
- 4 1 1/2" 9.5mm, MT, Asphalt Pavement
- 5 Crushed Limestone 3" and Var.

NOTE 1: Prior to milling, Repair any failed areas full depth with 12.5mm Mix, MT, Asphalt Pavement

NOTE 2: On existing widened shoulders that fail during milling operation, remove HMA in-place and excavate underlying shoulder 2' wide by 3" depth, inlaying with 12.5 mm Mix, MT.

SR 19 - NESHOPA COUNTY
 TYPICAL SECTION - MILL & OVERLAY
 STATIONS: 98+00 - 163+00
 PEARL RIVER SECTION



EXISTING

- 1 5 1/2"-15 1/2" Existing Thickness based on Core Evaluation
- 2 6" Granular Material
- 3 2' Widening by 3" Depth

PROPOSED

- 4 1 1/2" Milling (Correct to 2% Normal Crown or SE Where Needed)
- 5 1 1/2" 9.5mm, MT, Asphalt Pavement
- 6 Crushed Limestone 3" and Var.

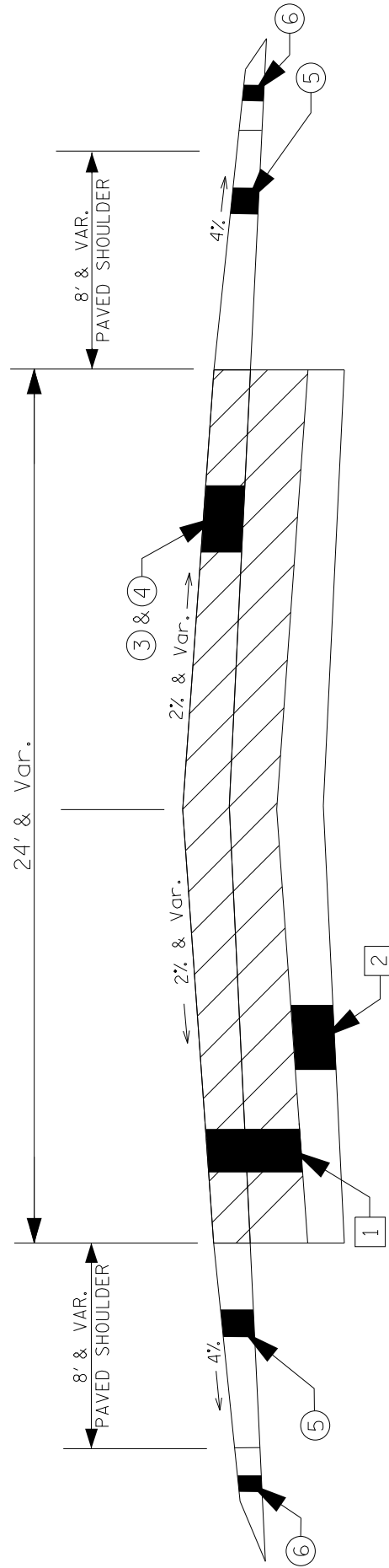
NOTE 1: Prior to milling, Repair any failed areas full depth with 12.5mm Mix, MT, Asphalt Pavement

NOTE 2: On existing widened shoulders that fail during milling operation, remove HMA in-place and excavate underlying shoulder 2' wide by 3" depth, inlaying with 12.5 mm Mix, MT.

SR 19 - NESHOBBA COUNTY

TYPICAL SECTION - MILL & OVERLAY

HWY 15 & HWY 19 CONNECTION ROAD



PROPOSED

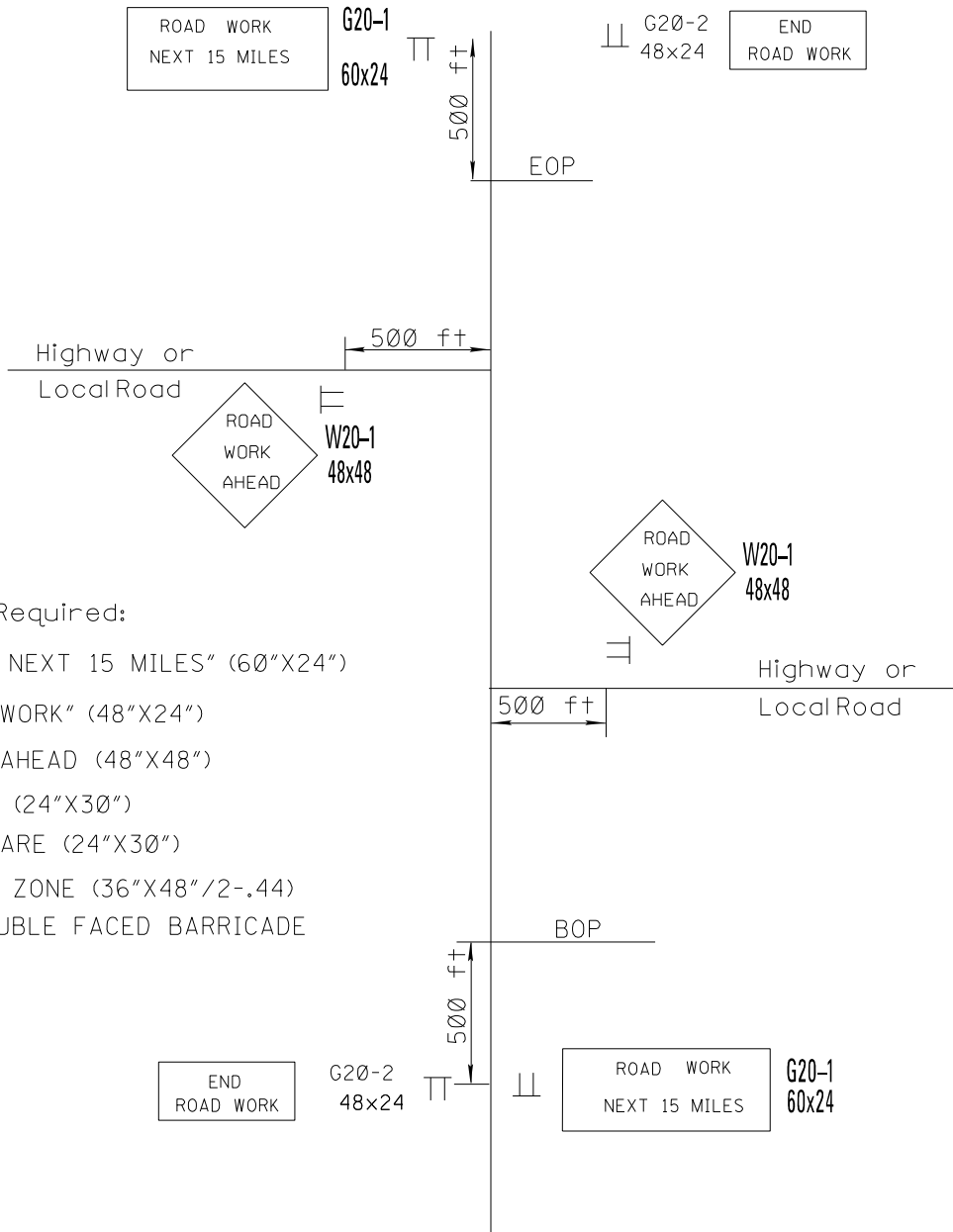
- ③ 1 1/2" Milling (Correct to 2% Normal Crown or SE Where Needed)
- ④ 1 1/2" 9.5mm, MT, Asphalt Pavement
- ⑤ Existing Paved Shoulder to Remain in Place
- ⑥ Crushed Limestone 3" and Var.

EXISTING

- ① 5 1/2" - 15 1/2" Existing Thickness based on Core Evaluation
- ② 6" Granular Material

NOTE 1: Prior to milling, Repair any failed areas full depth with 12.5mm Mix, MT, Asphalt Pavement

SR 19 - NESHOPA COUNTY CONSTRUCTION SIGNING



Traffic Control Signs Required:

- 2 - G20-1 "ROAD WORK NEXT 15 MILES" (60"x24")
- 3 - G20-2 "END ROAD WORK" (48"x24")
- 24 - W20-1 ROAD WORK AHEAD (48"x48")
- 143 - R4-1 DO NOT PASS (24"x30")
- 27 - R4-2 PASS WITH CARE (24"x30")
- 25 - W14-3 NO PASSING ZONE (36"x48"/2-.44)
- 5 - 619-G TYPE III DOUBLE FACED BARRICADE

- NOTES: ① One (1) W20-1 "ROAD WORK AHEAD" Sign is Required at each Local Road, Street or Highway Entering the Project.
- ② G20-1 and G20-2 signs mounted on Type III Double Faced Barricade.
- ③ R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE", and W14-3 "NO PASSING ZONE" signs are required in accordance with Subsection 618.03.3 and as specified in the MUTCD. If No Passing zones are 1000 ft or more, install additional "DO NOT PASS" signs on maximum spacing of 750 ft.
- ④ Placement of W20-1 signs on intersecting roads may vary from typical shown as conditions warrant.

FAILED AREA LOCATIONS

LOCATION	STA.	TO	STA.	LENGTH (ft)	WIDTH (ft)	AREA (SF)	AREA (SY)
LT LANE	112+70	--	113+07	37	14	518	58
LT LANE	118+30	--	120+85	255	14	3,570	397
LT LANE	409+80	--	410+40	60	14	840	93
RT LANE	112+79	--	113+07	28	14	392	44
RT LANE	118+36	--	119+88	152	14	2,128	236
RT LANE	471+20		472+48	128	14	1,792	199
Total = 1027 SY							

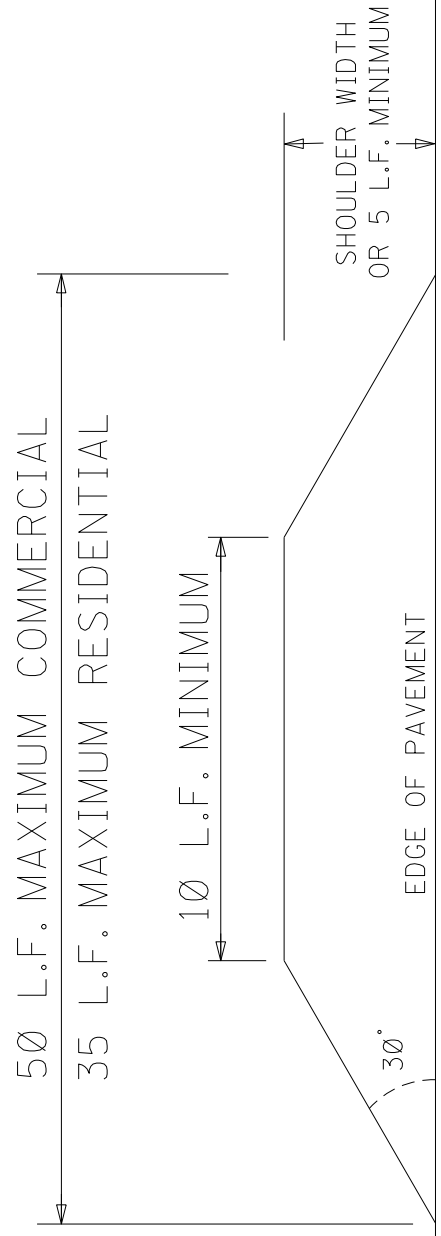
DEPTH = Variable

CURVE WIDENING LOCATIONS

LOCATION	STA.	TO	STA.	LENGTH (ft)	WIDTH (ft)	AREA (SF)	ASPHALT TONNAGE
LT LANE	162+66	--	162+83	17	2	34	1
LT & RT LANE	299+00	--	305+00	600	2	1,200	34
RT LANE	316+00	--	325+00	900	2	1,800	51
RT LANE	400+52	--	406+44	592	2	1,184	33
RT LANE	728+12	--	730+48	236	2	472	13
Total = 132 TONS							

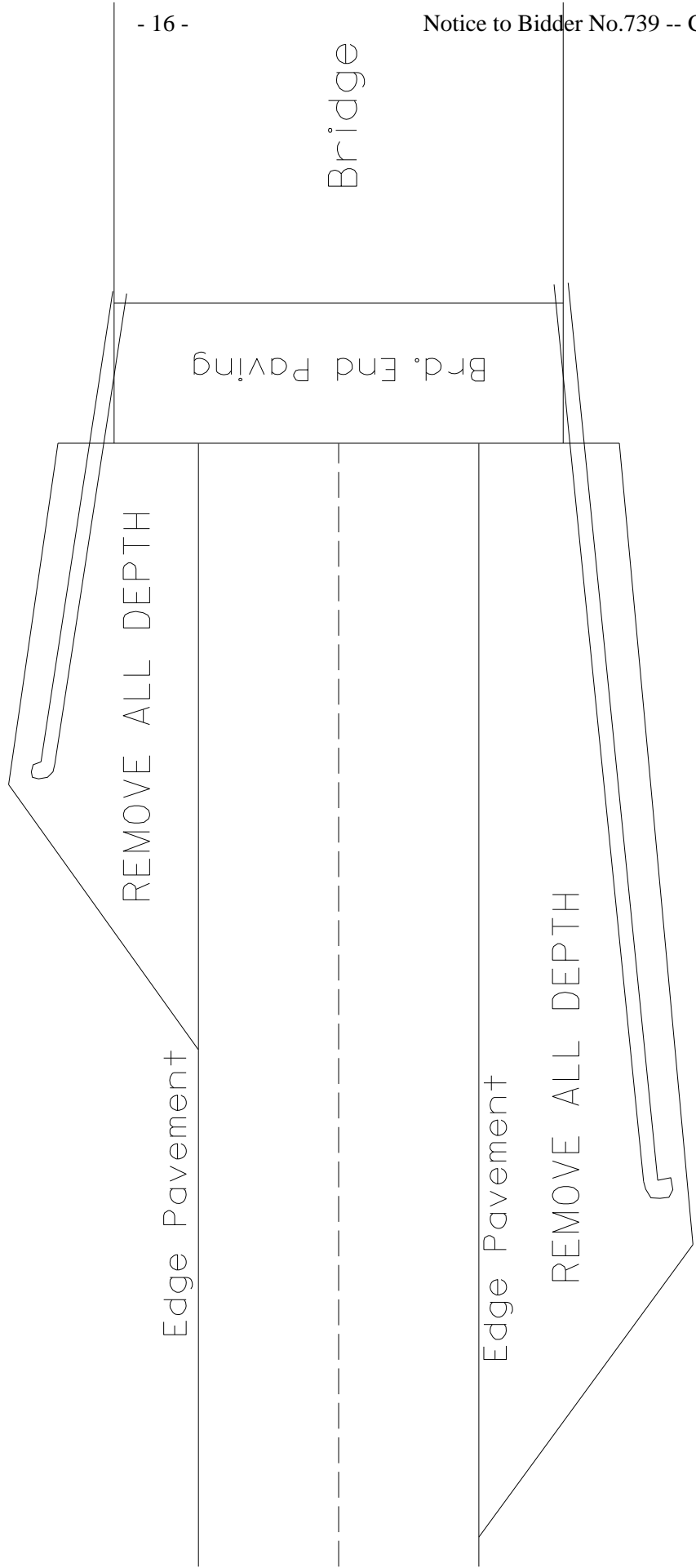
DEPTH = 4.5"

SR 19 - NESHOBIA COUNTY
DRIVEWAY APRON



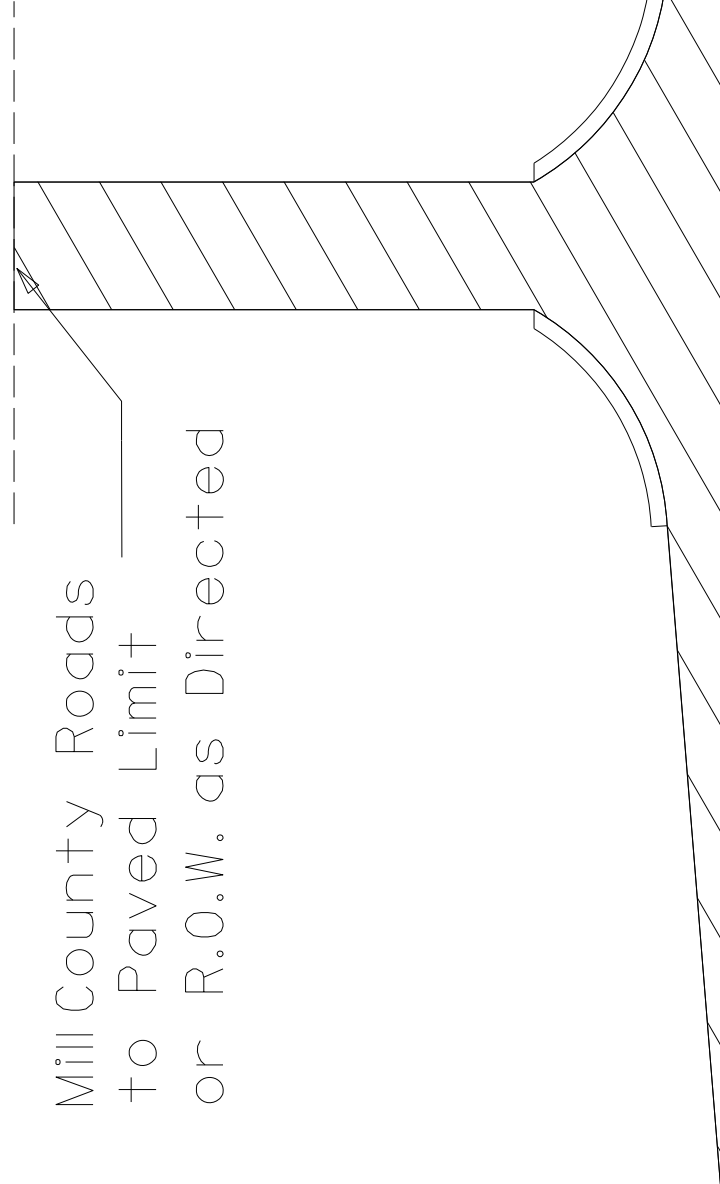
_____ MAINLINE PAVEMENT _____

SR 19 - NESHOBIA COUNTY
REMOVE & REPAVE BRIDGE GUARDRAIL PAD

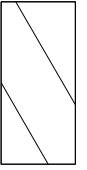


* Asphalt depth shall be 4" for the guardrail pads.

SR 19 - NESHOBIA COUNTY
MILLING COUNTY ROADS

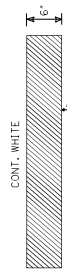
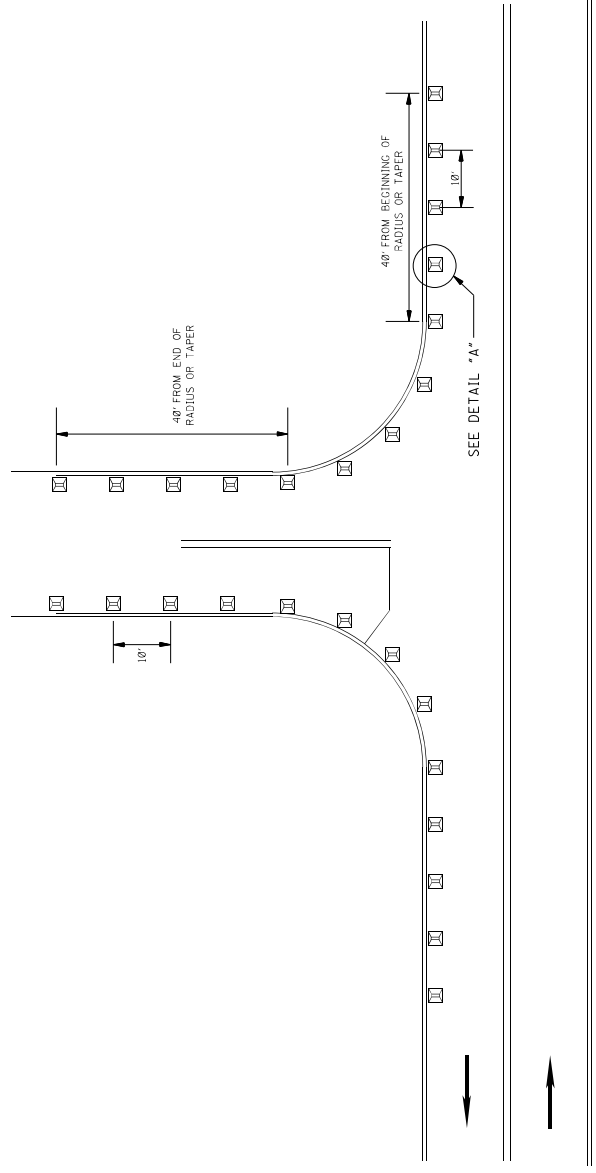


Mill County Roads
to Paved Limit
or R.O.W. as Directed



Mill Area

TYPICAL PLACEMENT OF RAISED PAVEMENT
MARKERS ON SIDE ROAD RADIUS
2-LANE, TWO WAY TRAFFIC



DETAIL A

GENERAL NOTES:

1. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
2. MARKERS SHALL BE HIGH PERFORMANCE TWO-WAY CLEAR.
3. MARKERS SHALL NOT BE ROTATED WHEN BEING PLACED ALONG RADIUS AND TANGENT SECTION OF LOCAL ROAD.
4. MARKERS SHALL BE INSTALLED AT SIMPLE AND CHANNELIZED INTERSECTIONS TO THE LIMITS SHOWN ABOVE.

Notice to Bidder No. 739

DIRECTION OF TRAFFIC

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**2-WAY RAISED
PAVEMENT MARKERS
AT INTERSECTING
ROADS (2-LANE)**

NO.	DATE	REVISION

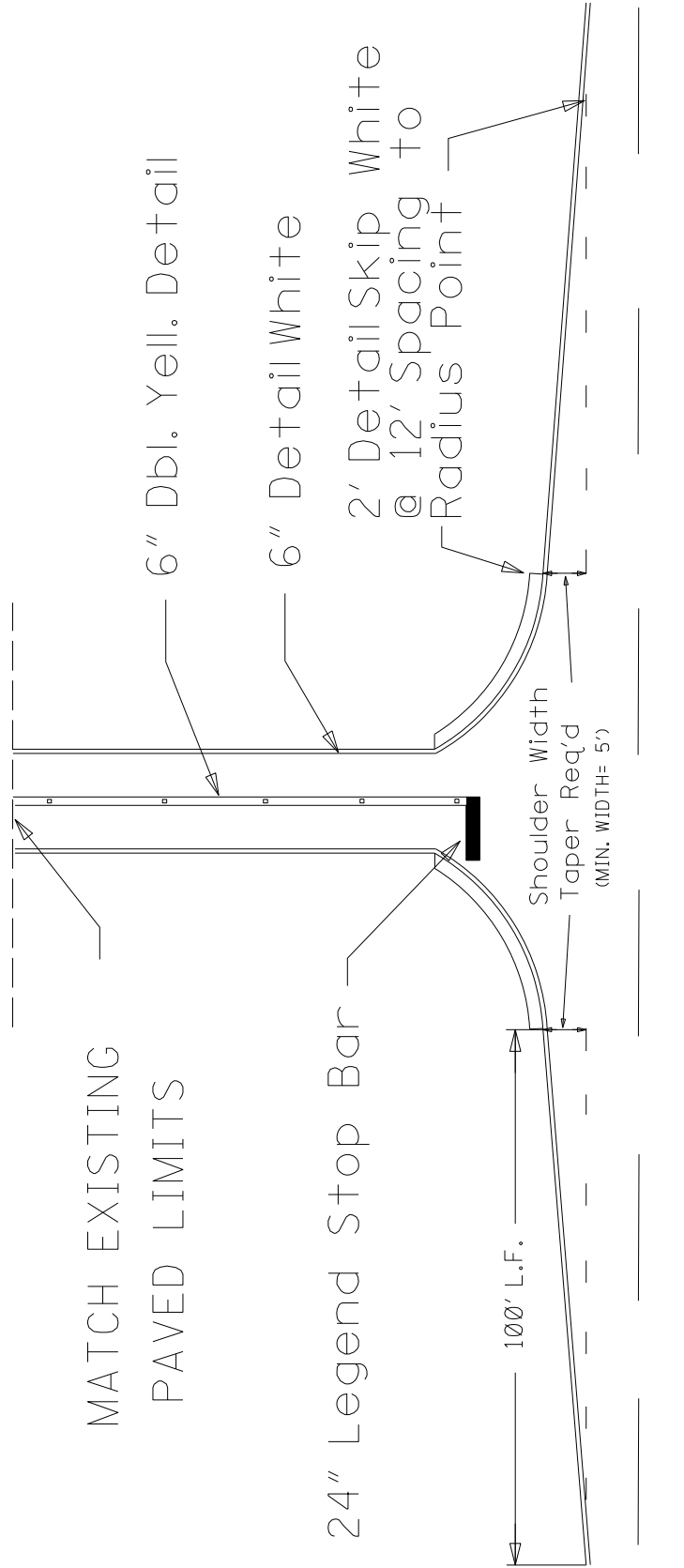
ISSUE DATE: AUGUST 01, 2017

REPORT NUMBER: PM-11

SHEET NUMBER: 6061

Cont'd

SR 19 - NESHOBIA COUNTY
 COUNTY ROAD PAVING/STRIPING

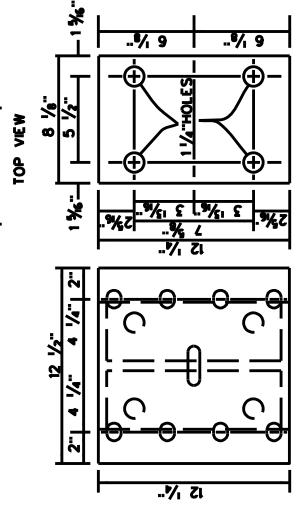
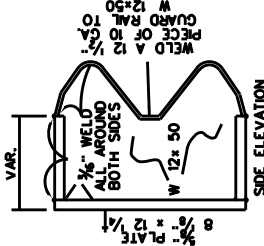
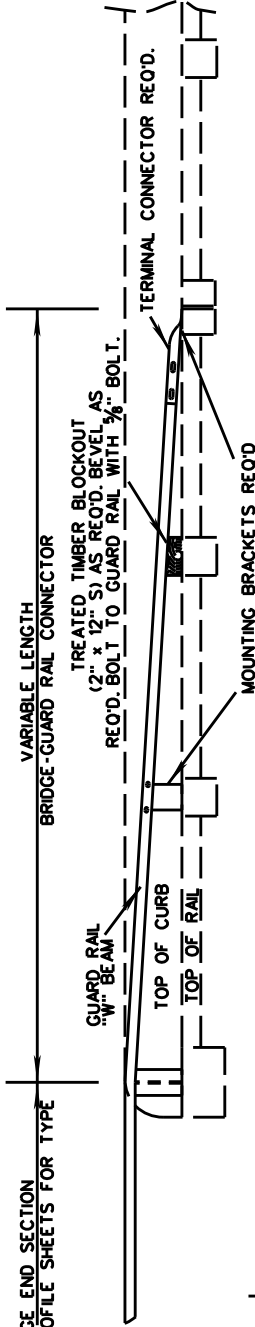


NOTE: 100' TAPERS TO BE CONSTRUCTED WHERE 5' SHOULDER WIDTH IS AVAILABLE AT THE BEGINNING OF LOCAL ROAD RADIUS.

NOTE: ASPHALT PAVEMENT THICKNESS IN TAPER SHALL BE 6".

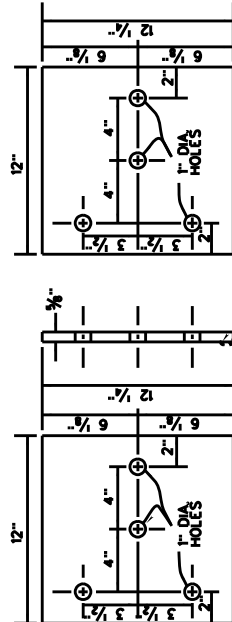
NOTE: DETAIL SKIP SHALL BE PLACED ON LOCAL ROADS WITH TAPERS.

25' BRIDGE END SECTION
SEE PLAN & PROFILE SHEETS FOR TYPE



FRONT ELEVATION SHOWING 10 GA. GUARD RAIL CONNECTION TO W12x50 BACK OF RAIL PLATE & BACK OF BRACKET

DETAIL OF INTERMEDIATE BRACKET

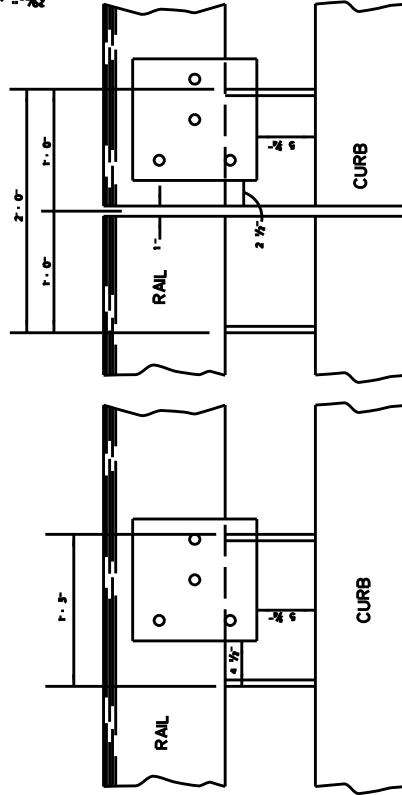


DETAIL OF BACK PLATE

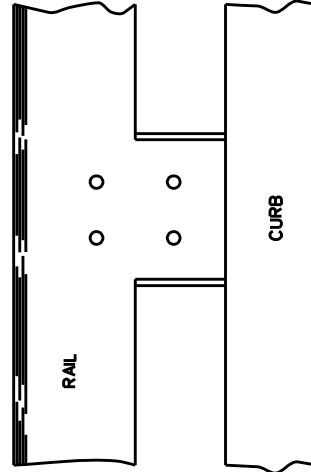
DETAIL OF FRONT PLATE

DETAIL OF TERMINAL CONNECTOR PLATE

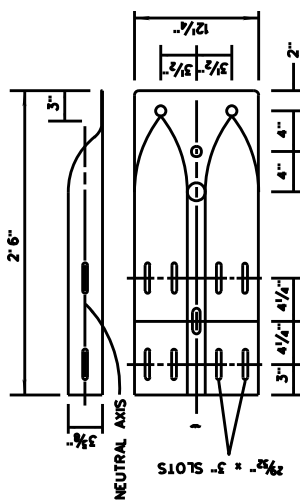
BRIDGE-GUARD RAIL CONNECTOR DETAIL



DETAILS SHOWING PLACEMENT OF HOLES FOR CONNECTOR (VIEWED FROM ROADWAY)



DETAILS SHOWING PLACEMENT OF HOLES FOR INTERMEDIATE BRACKET (VIEWED FROM ROADWAY)



TERMINAL CONNECTOR
(AASHTO DESIGNATION : M180, CLASS B, TYPE 2)

NOTES:

1. BRIDGE-GUARD RAIL CONNECTOR TO CONSIST OF ONE (1) 25' SECTION OF GUARD RAIL (FIELD CUT, DRILLED, AND GALVANIZED), FABRICATED BRACKETS AND PLATES, TERMINAL CONNECTOR, MOUNTING HARDWARE, AND TIMBER BLOCKOUT (COMPLETE IN PLACE).

2. BRIDGE-GUARD RAIL CONNECTORS REQ'D ON BRIDGES 24' WIDE, 4' REQ'D ON BRIDGES 26' OR WIDER.

PRIOR TO FABRICATION, ALL DIMENSIONS FOR MOUNTING BRACKETS, CONNECTORS, BOLT LENGTHS AND GUARD RAIL SHALL BE DETERMINED IN THE FIELD BY THE CONTRACTOR. DIMENSION SHOWN MAY VARY FROM ACTUAL FIELD CONDITIONS AT EACH SITE.

PAY ITEM NO. 606-0003 GUARDRAIL, BRIDGE CONNECTOR, PER PLANS.

Notice to Designer No. _____

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
SPECIAL DESIGN
BRIDGE-GUARD RAIL CONNECTOR

PROJECT NO. _____
SHEET NO. _____
DATE _____

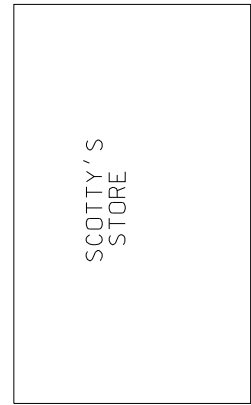
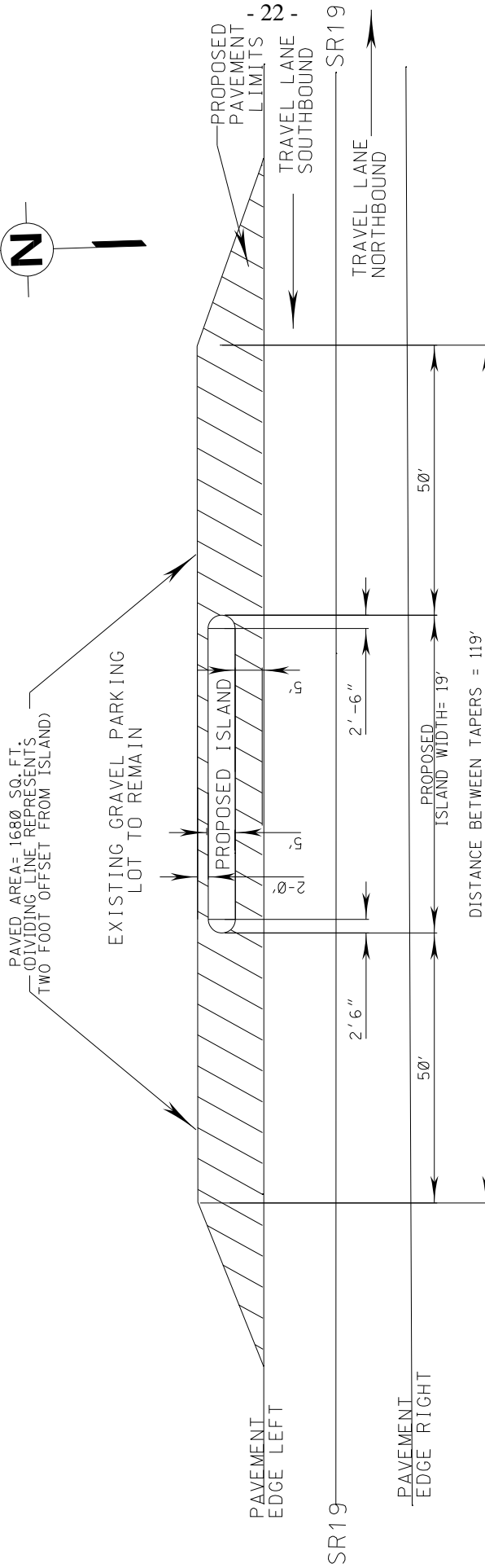
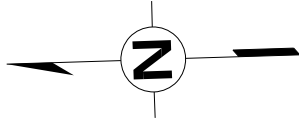
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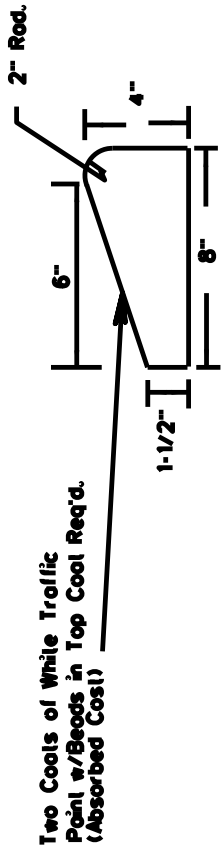
SR 15/SR 19 Connector Road Milling/Paving Limits
Outlined in Blue

NESHOBA COUNTY
MP-5019-50(013)/306368

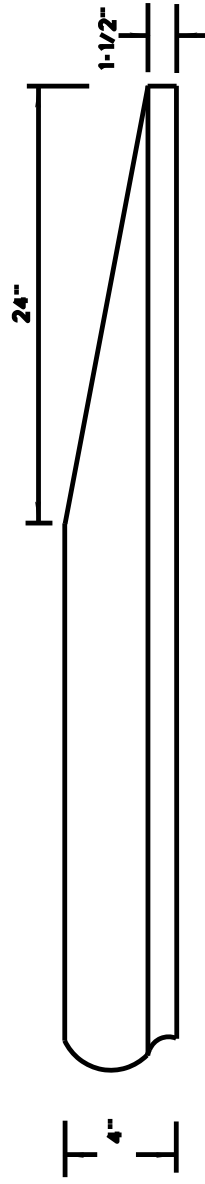
DETAIL OF ISLAND
AT SCOTTY'S STORE OFF SR 19
AT STA. 303+73



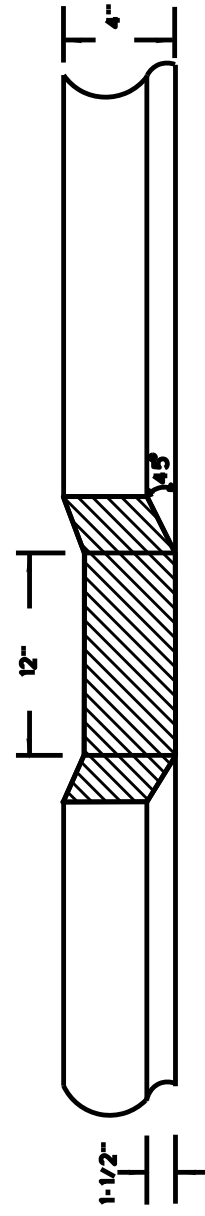
DETAIL OF BITUMINOUS CURB



DETAIL OF CURB TERMINUS



DETAIL OF SPILL THRU



Mill & Overlay approximately 15 miles of SR 19 from SR 15 to the Winston County Line, known as State Project No. MP-5019-50(013) / 306368301 in Neshoba County.

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
Roadway Items					
0010	202-B007		4,098	Square Yard	Removal of Asphalt Pavement, All Depths
0020	202-B117		180	Each	Removal of Delineator, All Types
0030	202-B138		16	Each	Removal of Guard Rail Bridge End Section
0040	202-B158		3,450	Linear Feet	Removal of Guard Rail, Including Rails, Posts and Terminal Ends
0050	202-B240		3,100	Linear Feet	Removal of Traffic Stripe
0060	203-G001	(E)	100	Cubic Yard	Excess Excavation, FM, AH
0070	403-A002	(BA1)	709	Ton	12.5-mm, MT, Asphalt Pavement
0080	403-A014	(BA1)	22,050	Ton	9.5-mm, MT, Asphalt Pavement
0090	406-D001		262,700	Square Yard	Fine Milling of Bituminous Pavement, All Depths
0100	407-A001	(A2)	26,270	Gallon	Asphalt for Tack Coat
0110	503-C010		50	Linear Feet	Saw Cut, Full Depth
0120	606-B003		2,100	Linear Feet	Guard Rail, Class A, Type 1, 'W' Beam, Metal Post
0130	606-D003		4	Each	Guard Rail, Bridge End Section, Special Design
0140	606-D005		4	Each	Guard Rail, Bridge End Section, Type A
0150	606-D020		12	Each	Guard Rail, Bridge End Section, Type H, Metal Post
0160	606-E005		24	Each	Guard Rail, Terminal End Section, Flared
0170	609-E001		44	Linear Feet	Bituminous Curb
0180	616-B002	(S)	8	Square Yard	Bituminous Median and/or Island Pavement,
0190	618-A001		1	Lump Sum	Maintenance of Traffic
0200	619-A1001		58	Mile	Temporary Traffic Stripe, Continuous White
0210	619-A2001		38	Mile	Temporary Traffic Stripe, Continuous Yellow
0220	619-A3001		2	Mile	Temporary Traffic Stripe, Skip White
0230	619-A4002		14	Mile	Temporary Traffic Stripe, Skip Yellow
0240	619-A5001		41,940	Linear Feet	Temporary Traffic Stripe, Detail
0250	619-A6001		2,000	Square Feet	Temporary Traffic Stripe, Legend
0260	619-D1001		1,005	Square Feet	Standard Roadside Construction Signs, Less than 10 Square Feet
0270	619-D2001		380	Square Feet	Standard Roadside Construction Signs, 10 Square Feet or More
0280	619-G4001		30	Linear Feet	Barricades, Type III, Double Faced
0290	620-A001		1	Lump Sum	Mobilization
0300	626-A001		1	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip White
0310	626-B002		29	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous White
0320	626-D001		7	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip Yellow

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
0330	626-E001		19	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow
0340	626-G004		9,400	Linear Feet	Thermoplastic Double Drop Detail Stripe, White
0350	626-G005		11,570	Linear Feet	Thermoplastic Double Drop Detail Stripe, Yellow
0360	626-H001		1,000	Square Feet	Thermoplastic Double Drop Legend, White
0370	627-J001		700	Each	Two-Way Clear Reflective High Performance Raised Markers
0380	627-K001		130	Each	Red-Clear Reflective High Performance Raised Markers
0390	627-L001		1,600	Each	Two-Way Yellow Reflective High Performance Raised Markers
0400	630-F002		10	Each	Delineators, Flexible Post Mounted, Crossover, Type I, Yellow
0410	630-F006		180	Each	Delineators, Guard Rail, White
0420	630-G003		8	Each	Type 3 Object Markers, OM-3L, Post Mounted
0430	630-G007		8	Each	Type 3 Object Markers, OM-3R, Post Mounted
0440	907-619-B001		66	Linear Feet	Temporary Portable Rumble Strips
0441	907-824-PP003		95	Square Feet	Bridge Repair, Epoxy Repair
ALTERNATE GROUP AA NUMBER 1					
0450	304-H001	(GY)	3,000	Cubic Yard	3/4" and Down Crushed Stone Base, LVM
ALTERNATE GROUP AA NUMBER 2					
0460	304-H002	(GY)	3,000	Cubic Yard	Size 610 Crushed Stone Base, LVM
ALTERNATE GROUP AA NUMBER 3					
0470	304-H003	(GY)	3,000	Cubic Yard	Size 825B Crushed Stone Base, LVM