



SM No. CMP5080510231

PROPOSAL AND CONTRACT DOCUMENTS

FOR THE CONSTRUCTION OF

13

Mill & Overlay approximately 10 miles of US 80 from Scott County Line to Scanlan Street, known as State Project No. MP-5080-51(023) / 306367301 in Newton County.

Project Completion: 120 Working Days

(STATE DELEGATED)

NOTICE

**BIDDERS MUST COMPLETE AN ONLINE REQUEST
FOR PERMISSION TO BID THIS PROJECT.**

Electronic addendum updates will be posted on www.gomdot.com

SECTION 900

OF THE CURRENT 2017 STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION JACKSON, MISSISSIPPI

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

05/30/2018 12:22 PM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 901 - ADVERTISEMENT

Electronic bids will be received by the Mississippi Transportation Commission at 10:00 o'clock A.M., Tuesday, June 26, 2018, from the Bid Express Service and shortly thereafter publicly read on the Sixth Floor for:

Mill & Overlay approximately 10 miles of US 80 from Scott County Line to Scanlan Street, known as State Project No. MP-5080-51(023) / 306367301 in Newton County.

The attention of bidders is directed to the predetermined minimum wage rate set by the U. S. Department of Labor under the Fair Labor Standards Act.

The Mississippi Department of Transportation hereby notifies all bidders that it will affirmatively insure that in any contract entered into pursuant to this advertisement, disadvantaged business enterprises will be afforded full opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of race, color, sex, age, disability, religion or national origin in consideration for an award.

The specifications are on file in the offices of the Mississippi Department of Transportation.

Contractors may request permission to bid online at <http://shopmdot.ms.gov> at no cost. Upon approval, Contractors shall be eligible to submit a bid using Bid Express at <http://bidx.com>. Specimen proposals may be viewed and downloaded online at no cost at <http://mdot.ms.gov> or purchased online at <http://shopmdot.ms.gov> at a cost of Ten Dollars (\$10.00) per proposal plus a small convenience fee. Cash or checks will not be accepted as payment.

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

MELINDA L. MCGRATH
EXECUTIVE DIRECTOR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Governing Specifications

The current (2017) Edition of the Standard Specifications for Road and Bridge Construction adopted by the Mississippi Transportation Commission is made a part hereof fully and completely as if it were attached hereto, except where superseded by special provisions, or amended by revisions of the Specifications contained within this proposal. Copies of the specification book may be purchased from the MDOT Construction Division, or online at shopmdot/default.aspx?StoreIndex=1.

A reference in any contract document to controlling requirements in another portion of the contract documents shall be understood to apply equally to any revision or amendment thereof included in the contract.

In the event the plans or proposal contain references to the 2004 Edition of the Standard Specifications for Road and Bridge Construction, it is to be understood that such references shall mean the comparable provisions of the 2017 Edition of the Standard Specifications.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 3

CODE: (SP)

DATE: 01/17/2017

SUBJECT: Final Clean-Up

Immediately prior to final inspection for release of maintenance, the Contractor shall pick up, load, transport and properly dispose of all litter from the entire highway right-of-way that is within the termini of the project.

Litter shall include, but not be limited to, solid wastes such as glass, paper products, tires, wood products, metal, synthetic materials and other miscellaneous debris.

Litter removal is considered incidental to other items of work and will not be measured for separate payment.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 9

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Federal Bridge Formula

Bidders are hereby advised that the latest revision of Federal Highway Administration Publication No. FHWA-HOP-06-105, **BRIDGE FORMULA WEIGHTS**, dated August 2006, is made a part of this contract when applicable.

Prior to the preconstruction conference, the Contractor shall advise the Engineer, in writing, what materials, if any, will be delivered to the jobsite via Interstate route(s).

Copies of the **BRIDGE FORMULA WEIGHTS** publication may be obtained by contacting:

Federal Highway Administration
400 7th Street, SW
Washington, DC 20590
(202) 366-2212

or

http://www.ops.fhwa.dot.gov/Freight/publications/brdg_frm_wghts/bridge_formula_all_rev.pdf

An on line **BRIDGE FORMULA WEIGHTS CALCULATOR** is available at

http://ops.fhwa.dot.gov/freight/sw/brdgcalc/calc_page.htm

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 12

CODE: (IS)

DATE: 03/01/2017

SUBJECT: MASH Compliant Devices

Bidders are hereby advised that the Standard Specifications may require certain traffic control and permanent safety hardware devices to meet the requirements of the Manual for Assessing Safety Hardware (MASH). However, devices meeting the requirements of NCHRP Report 350 will be allowed until the mandatory effective date for MASH compliance. The following table shows the effective dates for MASH compliant devices.

Device	Effective Date for MASH Compliance
W-beam barriers, cast-in-place concrete barriers	December 31, 2017
W-beam terminals	June 30, 2018
Cable barriers, cable barrier terminals, crash cushions	December 31, 2018
Bridge rails, transitions, all other longitudinal barriers including portable barriers installed permanently, all other terminals, sign supports, all other breakaway hardware	December 31, 2019

Temporary work zone devices, including portable barriers manufactured after December 31, 2019, must have been successfully tested to the 2016 Edition of MASH. Such devices manufactured on or before this date and successfully tested to NCHRP Report 350 or the 2009 Edition of MASH may continue to be used throughout their normal service lives.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

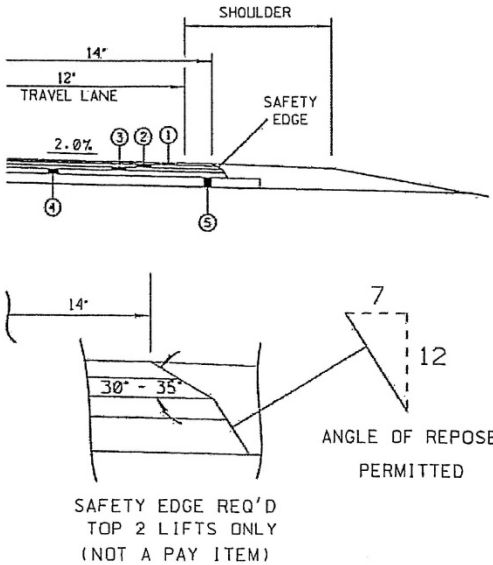
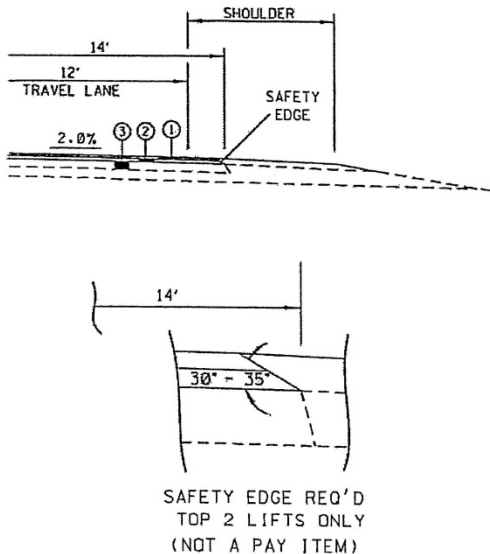
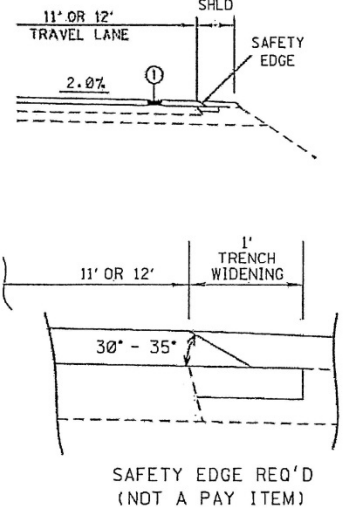
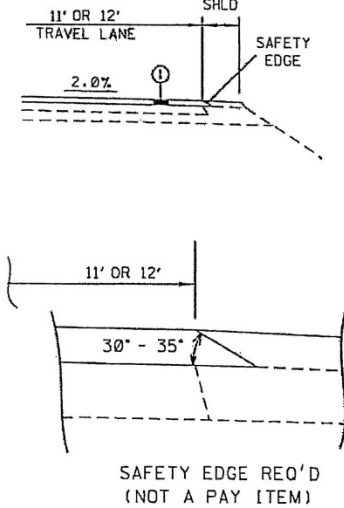
SECTION 904 - NOTICE TO BIDDERS NO. 13

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Safety Edge

Bidders are hereby advised that the Shoulder Wedge (Safety Edge) specified in Section 401, Asphalt Pavements, shall only apply to the top two (2) lifts of asphalt. Open Graded Friction Courses (OGFC) are not to be considered a lift as it pertains to safety edge. Attached is a drawing showing the safety edge. Note that the shoulder dimensions in the bottom two drawings will be less than three feet (3').

 <p>NEW CONSTRUCTION</p>	 <p>OVERLAY 14' PAVEMENT</p>
 <p>OVERLAY 11' OR 12' PAVEMENT WITH TRENCH WIDENING</p>	 <p>OVERLAY 11' OR 12' PAVEMENT WITHOUT TRENCH WIDENING</p>
<p>SAFETY EDGE DETAILS 4/8/2011</p>	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SUPPLEMENT TO NOTICE TO BIDDERS NO. 14

DATE: 5/29/2018

PROJECT: MP-5080-51(023) / 306367301 – Newton County

After the second paragraph on page 1, add the following:

Name Insured: *Meridian Speedway, LLC.*

Description and Designation: *Mill and Overlay of US 80 at the intersection of US 80 and SR 505*

Mile Post: *near Lawrence, Mississippi.*

After the fourth paragraph on page 1, add the following:

Sri Honnur, P.E.

Track and Bridge Construction Director

The Kansas City Southern Railway Co.

421 West 12th Street,

Kansas City, MO 64105

Ph: 816-983-1138

Shonnur@KCSouthern.com

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 14

CODE: (SP)

DATE: 01/17/2017

SUBJECT: Railway-Highway Provisions

Prior to bidding, the Contractor shall contact the Railroad concerning insurance coverage required for this project. In case the railroad requires coverage over and above that required by the Standard Specifications, the railroad requirements shall be met.

The name insured, description of the work and designation of the job site to be shown on the Policy are as follows:

Notice of starting to work, completion of any required forms, and correspondence pertaining to railroad liability insurance shall be directed to the person below.

The Contractor shall not commence, or carry on, any work for installation, maintenance, repair, changing or renewal of any FACILITY, under, over or on RAILROAD property at any location without giving at least ten (10) working days prior notice to the RAILROAD authorized representative at the RAILROAD's office(s) below.

If in the opinion of the RAILROAD, the presence of an authorized representative of the RAILROAD is required to supervise the same, the RAILROAD shall render bills to the Contractor for all expenses incurred by it for such supervision. This includes all labor costs for flagmen or cable locate supplied by the RAILROAD to protect RAILROAD operation, and for the full cost of furnishing, installation and later removal of any temporary supports for said tracks, as the RAILROAD's Chief Engineer's Office may deem necessary.

It will be the Contractor's responsibility to pay all bills associated with railroad flagging and cable locating. Generally, the flagging rate is \$700.00 per day (1 to 8 hours) plus overtime at \$125.00 per hour, however, the Contractor shall contact the RAILROAD to verify all rates.

A flagman is required anytime a Contractor does any work on or near RAILROAD property within twenty-five (25) feet horizontally of the centerline or any work over any railroad track. The RAILROAD, however, also reserves the right to require a flagman for work on RAILROAD property, which is more than twenty-five (25) feet from the centerline of a railroad track when there are other conditions or considerations that would dictate the need for a flagman to safeguard the RAILROAD's operations, property and safety of working personnel.

A cable locate of RAILROAD owned facilities may be required to identify and protect Signal & Communication cables that have been installed to provide power, signal control, wayside communications. These cables are vital to a safe and reliable railway operation. The cable locate will be performed by a qualified RAILROAD employee.

Outside Contractors are prohibited from driving on, along, or across any track that does not have a RAILROAD installed crossing. They may utilize an existing public crossing. The practice of allowing rubber tired equipment to operate over track with no crossing has been banned.

Exceptions to this rule will require the express approval from the RAILROAD Engineers.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 113

CODE: (SP)

DATE: 04/18/2017

SUBJECT: Tack Coat

Bidders are advised that in addition to the products listed on the Department's APL as referenced in Subsection 401.03.1.2 on page 256, the Contractor may use one of the following as a tack coat.

- CSS-1
- CSS-1h
- SS-1
- SS-1h

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 246

CODE: (SP)

DATE: 06/27/2017

SUBJECT: Kansas City Southern Construction Requirements

Bidders are hereby advised that provisions which are required as per the Notice to Bidders entitled “Railway-Highway Provisions” shall also include the following.

The Contractor shall submit to the Project Engineer and the Railroad detailed plans and design data for temporary construction clearances, stages of construction, erection plans, demolition plans, false-work plans, excavation plans, and temporary shoring plans and calculations, as required, and shall be sealed by a Mississippi Registered Professional Engineer. All submittals must be approved by the Railroad before excavation or construction can begin within Railroad Right-of-Way. All construction submittals for work performed within the Kansas City Southern Railway Company (KCS) right-of-way shall be made per the KCS “Guidelines for the Design and Construction of Railroad Overpasses and Underpasses” as updated in May 2008.

Prior to beginning any work on the KCS right-of-way, the Contractor shall obtain a Right of Entry Permit. To request a permit application, the Contractor should contact Sylvia Schmidt. Mrs. Schmidt’s contact information is as follows.

Sylvia Schmidt
Permit Manager
Jones Lang LaSalle Americas, Inc.
3017 Lou Menk Drive, Suite 100
Fort Worth, Texas 76131-2800
817-230-2688

The Contractor shall be responsible for payment of all application fees.

This project will require construction activities on the right-of-way of active railroad tracks which are currently owned and/or operated by KCS. When work requires that equipment or personnel be within the KCS right-of-way or the “foul zone” adjacent to the right-of-way, a qualified “Employee-in-Charge” (EIC) must be present for the purpose of providing on-track safety and flagging protection for the work crews. The EIC shall also be responsible for the coordination of the Contractor’s activities within the KCS right-of-way with the operation of the Railroad. The EIC must be certified under the KCS General Code of Operation Rules (GCOR) and must be approved by the local KCS Roadmaster prior to beginning work on the KCS right-of-way. The Contractor will be required to provide radios for the EIC, all equipment operators, supervisors, and foremen in charge of employees working within the KCS right-of-way. All personnel who must enter upon the KCS right-of-way must check in and out with the EIC and be logged in and out of the site.

All personnel who must work within the KCS right-of-way at any time shall be trained and certified as a KCS "Roadway Worker" and must at all times have their certification card with them and available for random inspection. The Contractor will be responsible for providing this training for Contractor employees or any subcontractor(s) employees. The Contractor shall contact Mr. Larry Slater of Track Sense Inc. at 330-847-8661 or 330-219-4721 (lslater@neo.rr.com) for approximate fees and scheduling the necessary training sessions. The Contractor shall also contact the MDOT Project Engineer to see if any MDOT employees need this training. If so, the Contractor shall include the MDOT employees in the list of participants for training. The Contractor shall bear the cost of training the MDOT employees. Costs for training the MDOT employees will be reimbursed to the Contractor by supplemental agreement.

Prior to commencing work, the Contractor shall provide to the Railroad Engineer or the Railroad Engineer's designated representative, [with copies to the Project Engineer](#), a detailed construction schedule for its work on Railroad's right-of-way, including the proposed temporary horizontal and vertical clearances and construction sequence for all work to be performed on Railroad right-of-way. This schedule shall also include the anticipated dates when the milestone events listed below will occur. The Contractor shall update the schedule for these milestone events as necessary, but at least monthly, and shall provide a copy of all updates to the Railroad so that site visits may be scheduled.

- Preconstruction meetings.
- Excavations, shoring placement/removal, pile driving, drilling of caissons or drilled shafts adjacent to tracks.
- Reinforcement and concrete placement for near track piers.
- Erection of precast concrete or steel overpass bridge superstructure.
- Reinforcement and concrete placement of overpass bridge decks.
- Completion of the bridge structure.

The Contractor shall so arrange and conduct construction operations in such a manner that there will be no interference with Railroad operations, including train, signal, telephone and telegraphic services, or damage to the property of the Railroad or to poles, cables or wires (whether overhead or underground) and other facilities or tenants on the rights-of-way of the Railroad. Before undertaking any work within Railroad right-of-way and before placing any obstruction over any track, the Contractor shall:

- Notify the Railroad's representative at least 72 hours in advance of the work.
- Provide assurance to the Railroad's representative that arrangements have been made for any required flagging service.
- Receive permission from the Railroad Engineer to proceed with the work.
- Ascertain that the Project Engineer has received copies of notice to the Railroad and the Railroad's response.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 296

CODE: (SP)

DATE: 07/25/2017

SUBJECT: Reduced Speed Limit Signs

Bidders are advised that when the plans or contract documents require the speed limit on a project to be reduced, the Contractor shall begin work within 48 hours of installing the reduced speed limit signs. Should the Contractor not start work or have no plans to start work within 48 hours of installing the signs, the reduced speed limit signs shall be covered and existing speed limit signs uncovered.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 401

CODE: (SP)

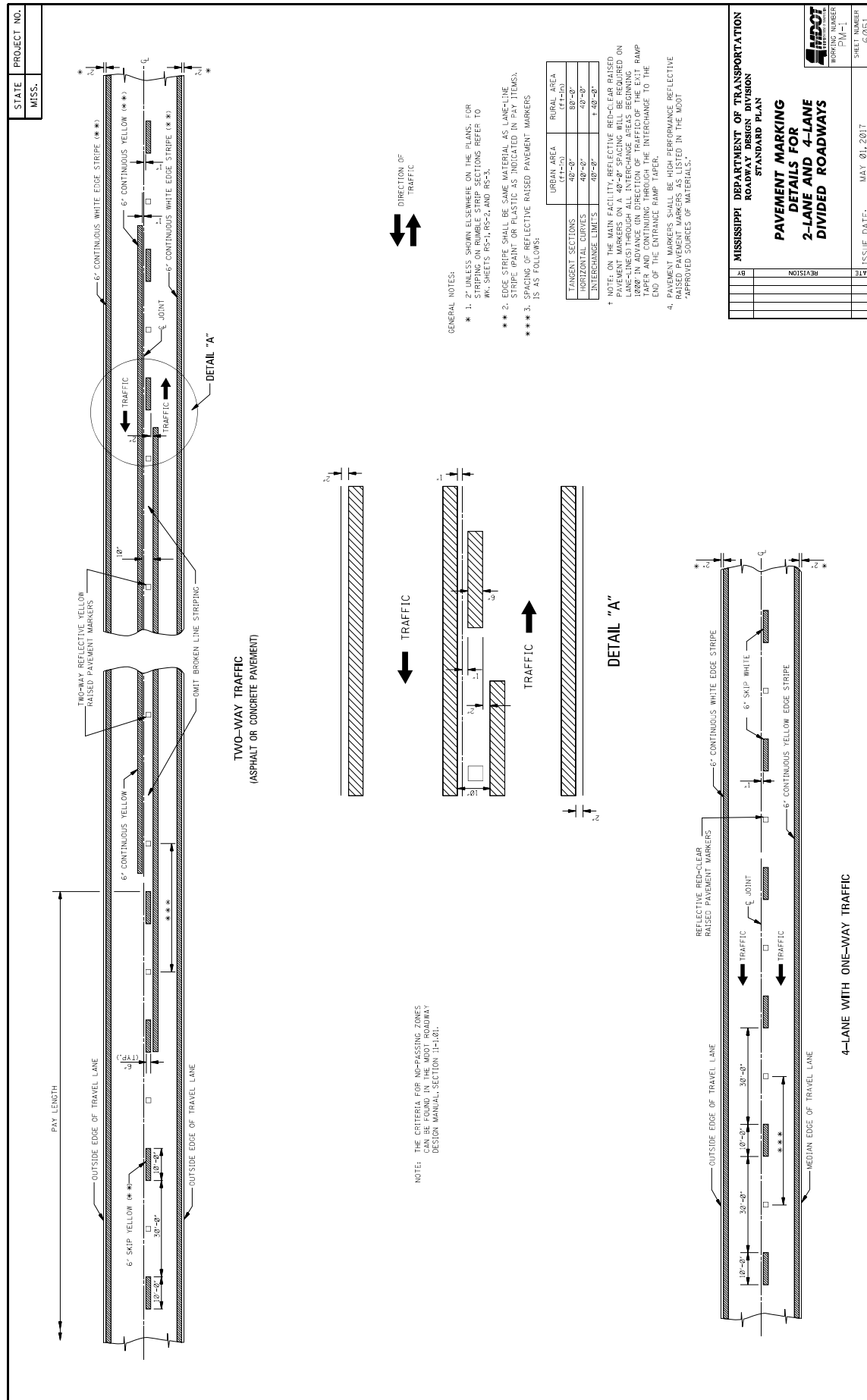
DATE: **09/12/2017**

SUBJECT: Standard Drawings

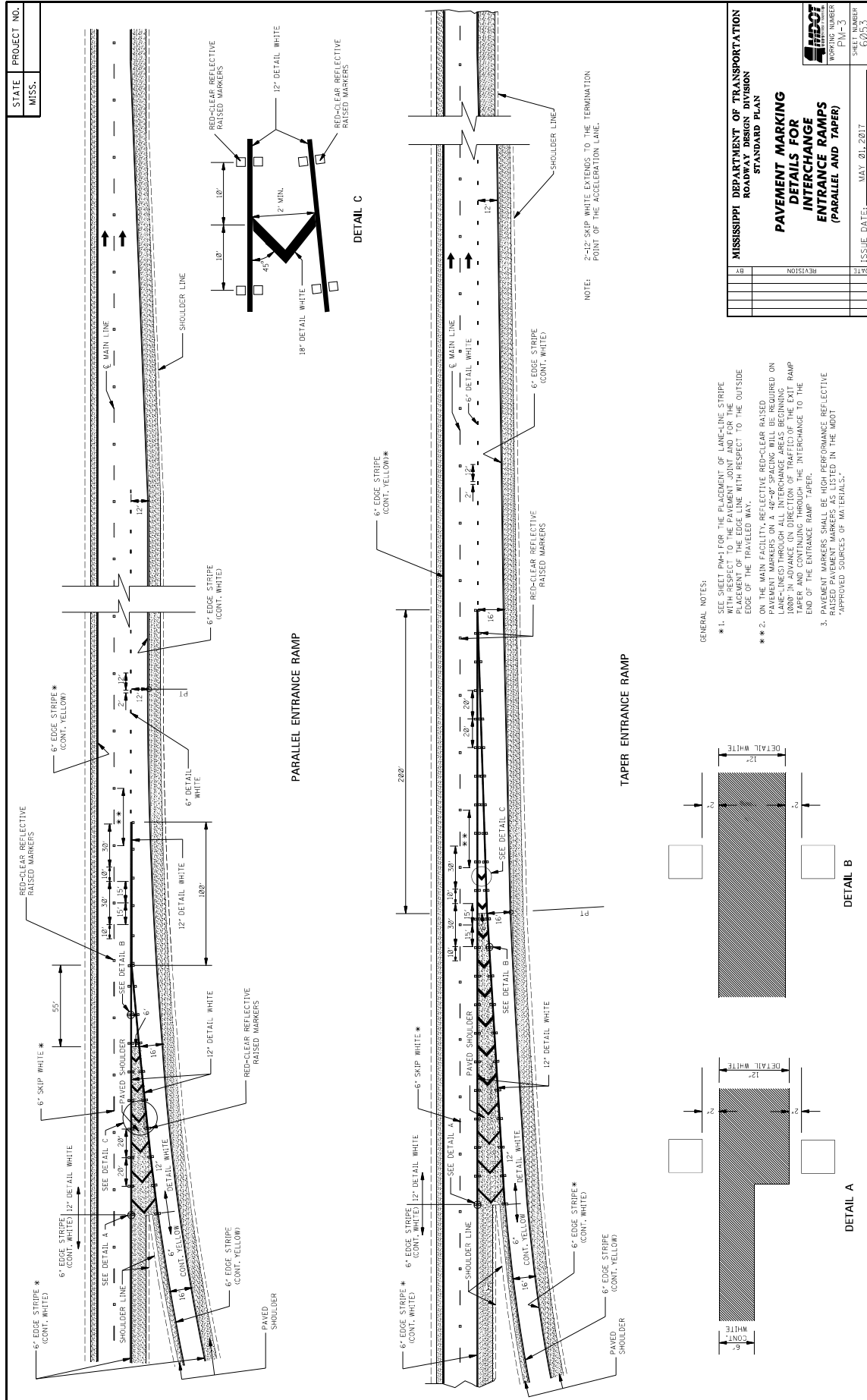
Standard Drawings attached hereto shall govern appropriate items of required work.

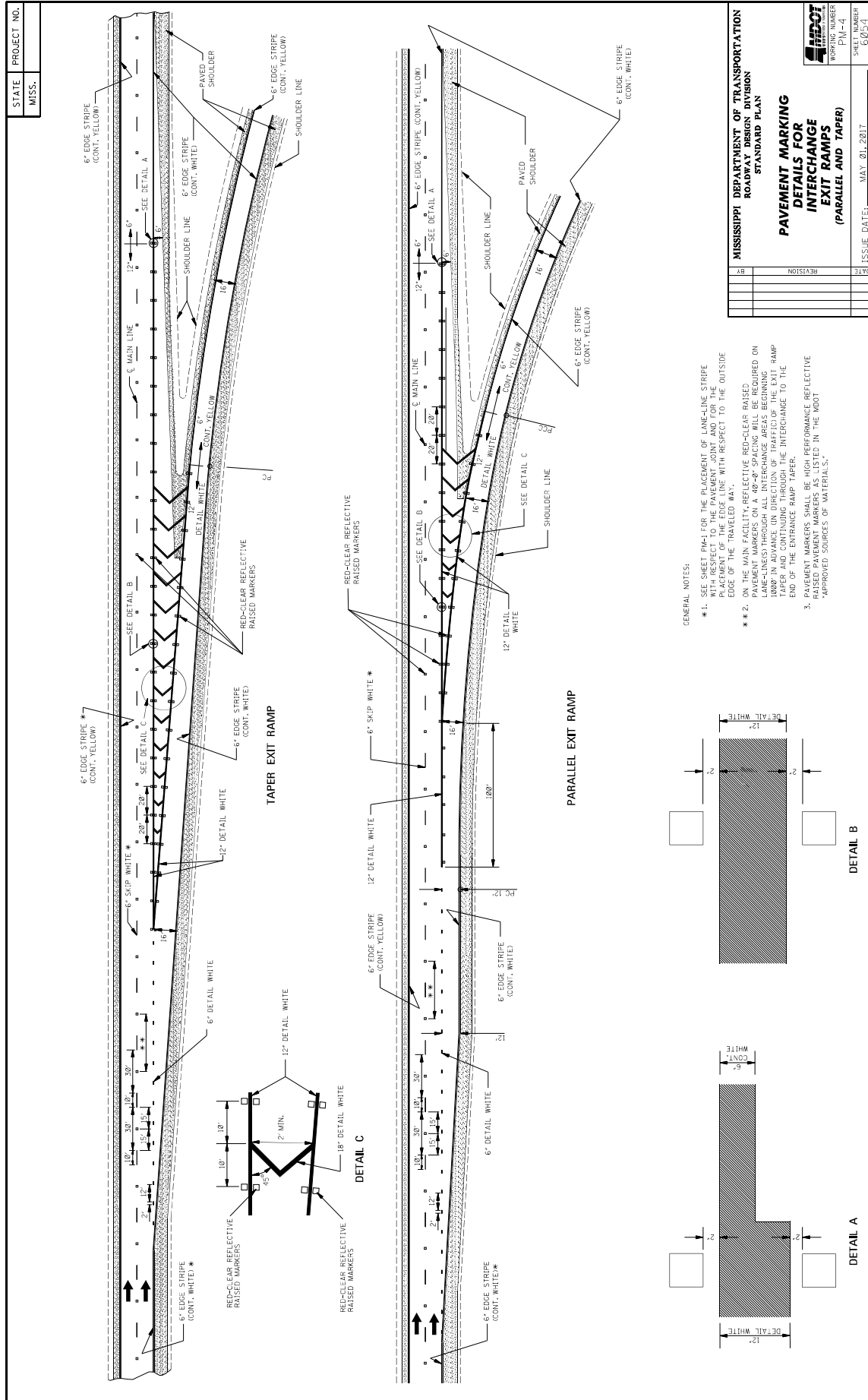
Larger copies of Standard Drawings may be purchased from:

MDOT Plans Print Shop
MDOT Shop Complex, Building C, Room 114
2567 North West Street
P.O. Box 1850
Jackson, MS 39215-1850
Telephone: (601) 359-7460
or FAX: (601) 359-7461
or e-mail: plans@mdot.state.ms.us

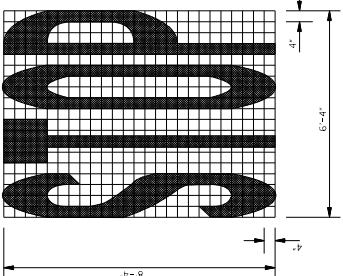




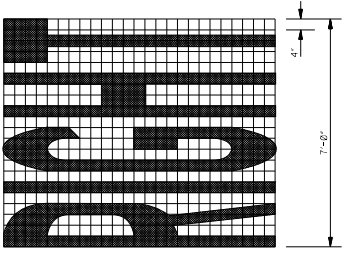




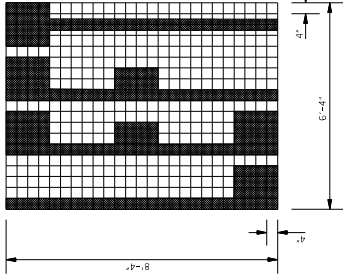
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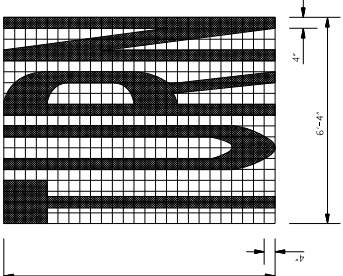
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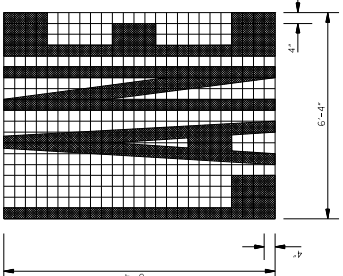
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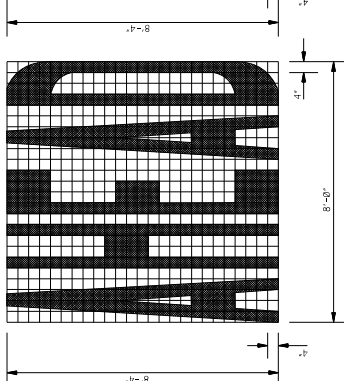
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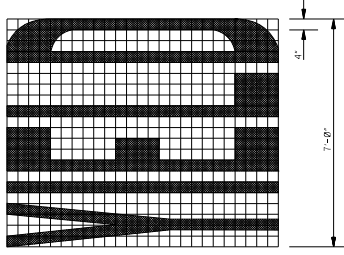
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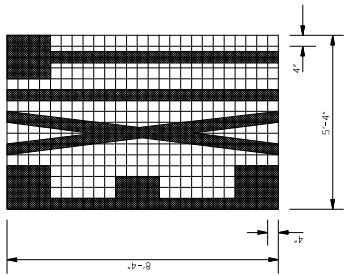
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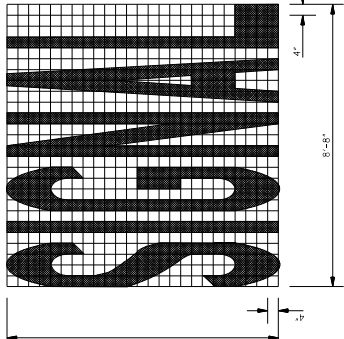
8'-4" x 9'-8"



8'-4" x 7'-8"



8'-4" x 5'-4"



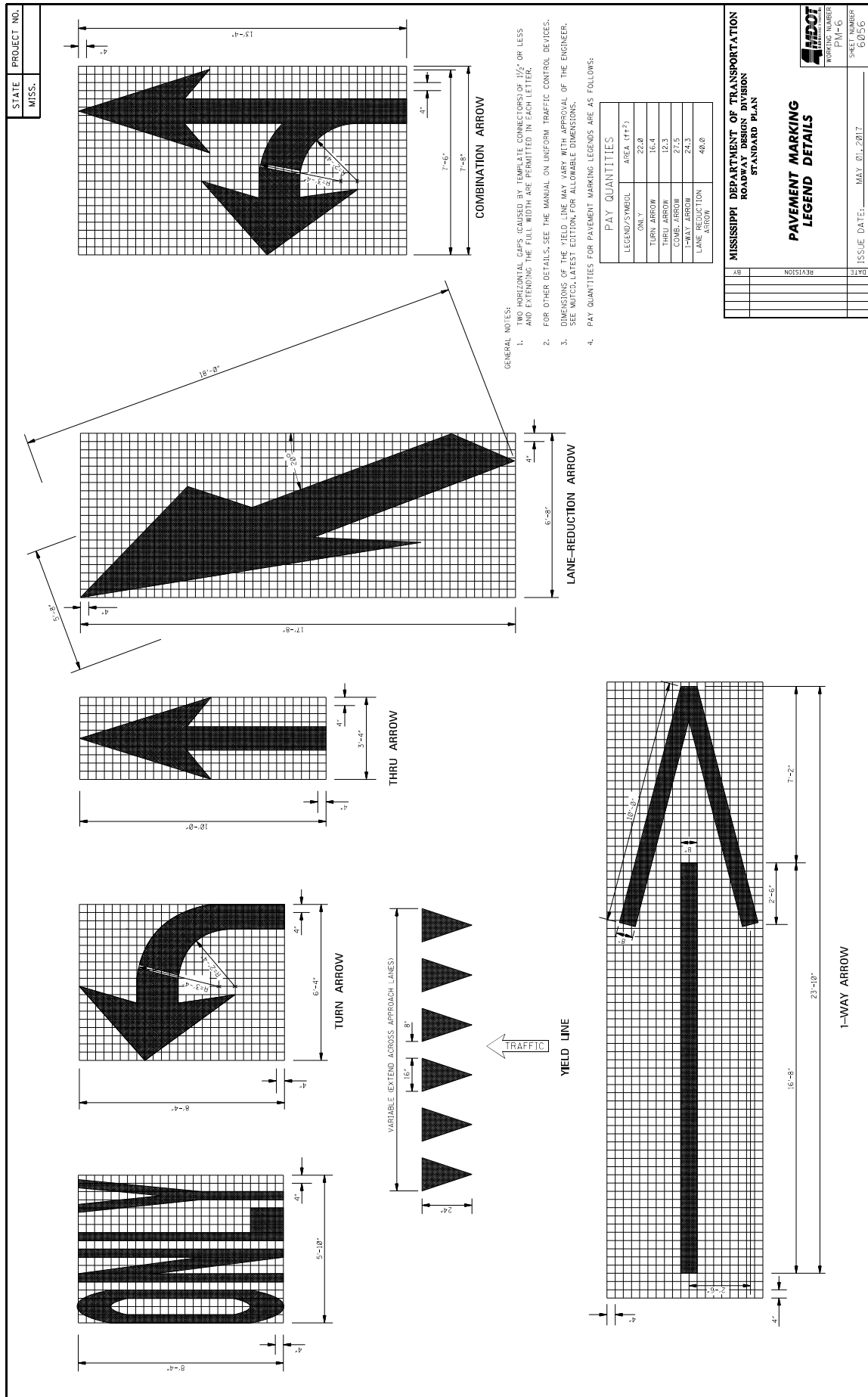
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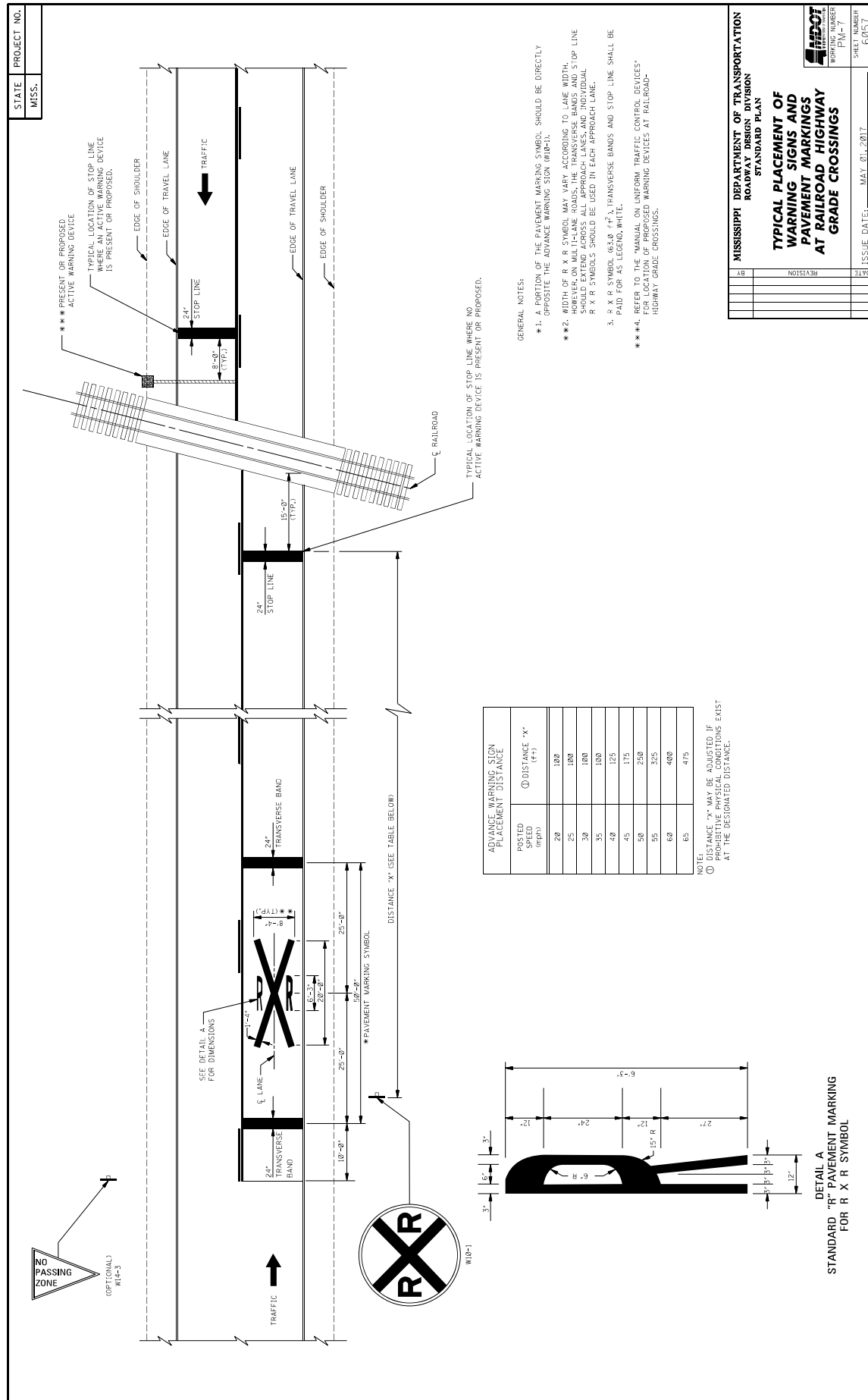
GENERAL NOTES:

- TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTIONS OF 1/2" LESS) ARE EXTENDING FROM ALL WIDTH OF SIGNING LETTERS.
- FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

LEGEND	AREA (sq ft)
STOP	24.6
RIGHT	28.6
LEFT	19.5
TRAFFIC	23.5
AHEAD	32.3
YIELD	26.8
EXIT	18.5
SIGNAL	32.5
SCHOOL	35.5

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
PAVEMENT MARKING LEGEND DETAILS	SHEET NUMBER PM-5 6055
DATE	ISSUE DATE: MAY 01, 2017
BY	REVISION





23









28

STATE PROJECT NO.
MISS.

MOBILE OPERATIONS ON TWO-LANE ROAD

MOBILE OPERATIONS ON MULTILANE ROAD

MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES FOR TWO-LANE OPERATION:

- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. SHADOW VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE WARNING CANNOT BE MAINTAINED, THE SHADOW VEHICLE SHOULD BE USED TO MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW VEHICLE SHALL BE EQUIPPED WITH BEACONS, FLASHING LIGHTS, AND WORK LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN, SHADOW, AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ARROW BOARD TO BE USED IN CAUTION MODE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MOBILE OPERATIONS ON MULTILANE ROAD

NOTES FOR MULTILANE LANE OPERATION:

- VEHICLES USED FOR THESE OPERATIONS SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT, SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLAGS, SIGNS, OR ARROW PANELS.
- SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK MOUNTED ATTENUATOR (TMA), AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON SHADOW VEHICLE 2 SO AS NOT TO OBSCURE THE ARROW PANEL.
- SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK-MOUNTED ATTENUATOR (TMA).
- SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
- WHEN ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, SHADOW VEHICLE 2 SHOULD BE ELIMINATED.
- ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE SHOULD BE USED (I.e., VEHICLE 3 ON THE SHOULDER (IF PRACTICAL), VEHICLE 2 IN THE CLOSED LANE, AND VEHICLE 1 IN THE CLOSED LANE).
- ARROW PANELS SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
- WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TRAFFIC CONTROL PLAN
MOBILE OPERATIONS
MULTILANE ROADS
TWO-LANE ROADS

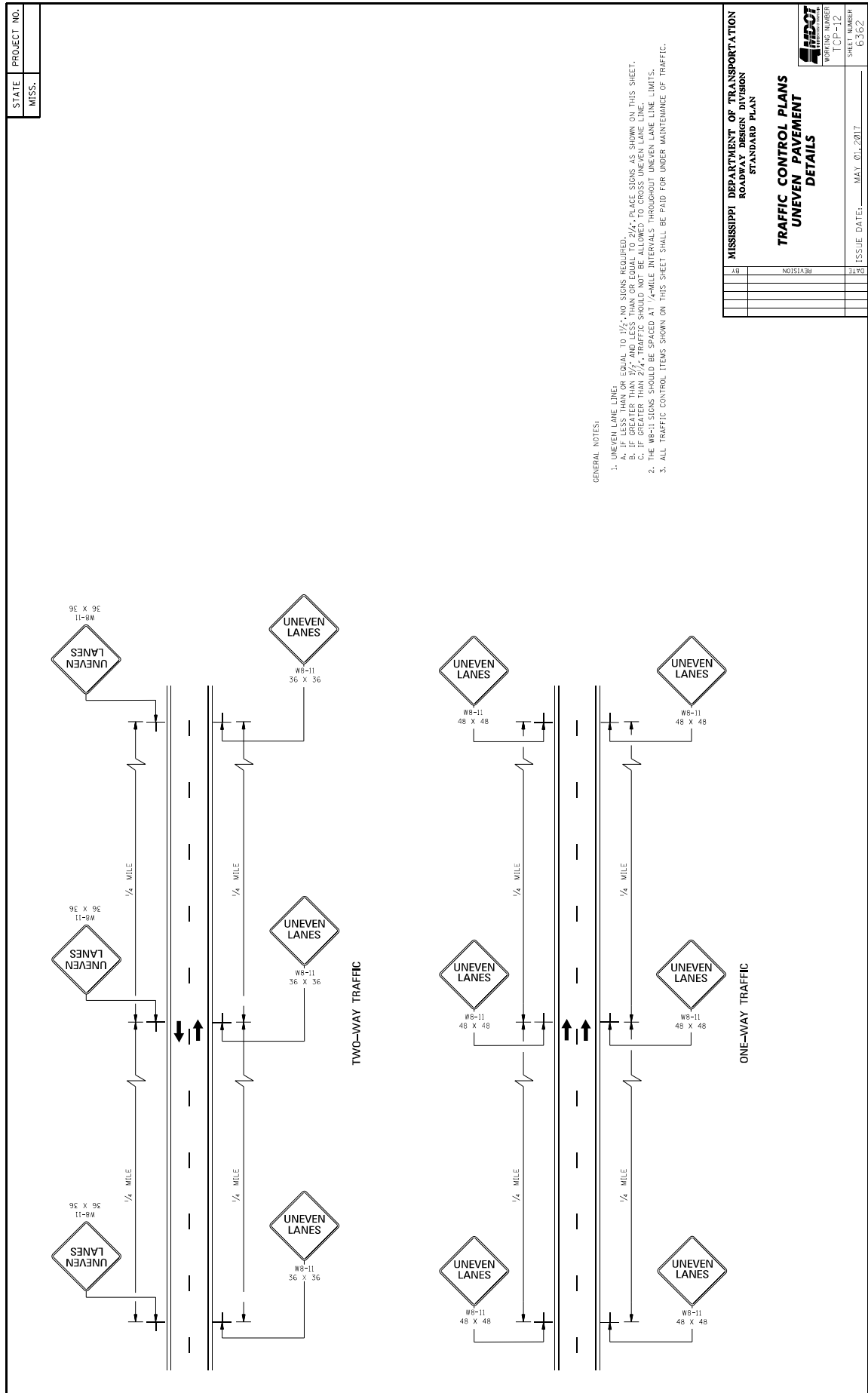
REVISION	DATE
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WORKING NUMBER	SHEET NUMBER
TCP-49	6339

ISSUE DATE	DATE
MAY 01, 2017	

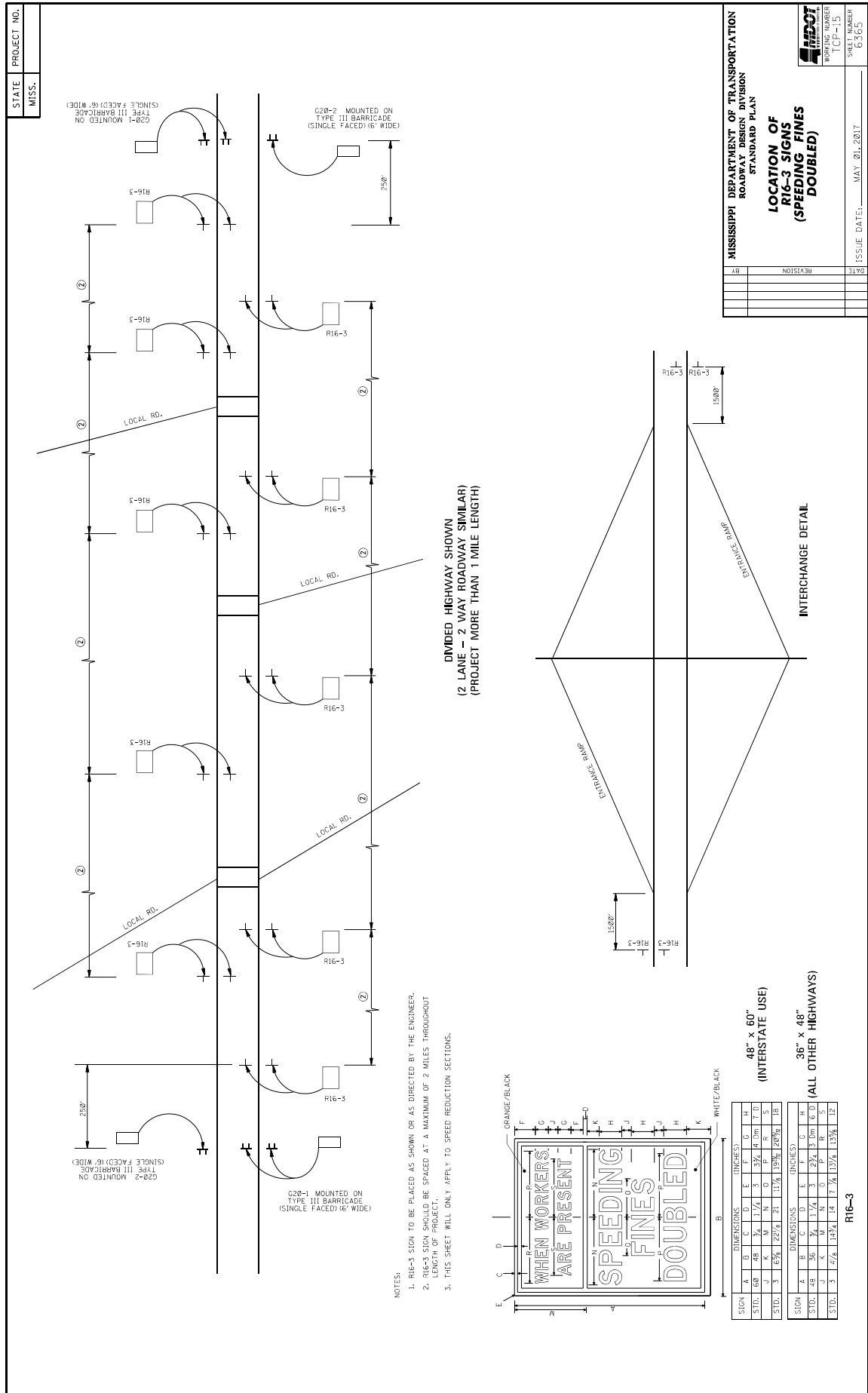












MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

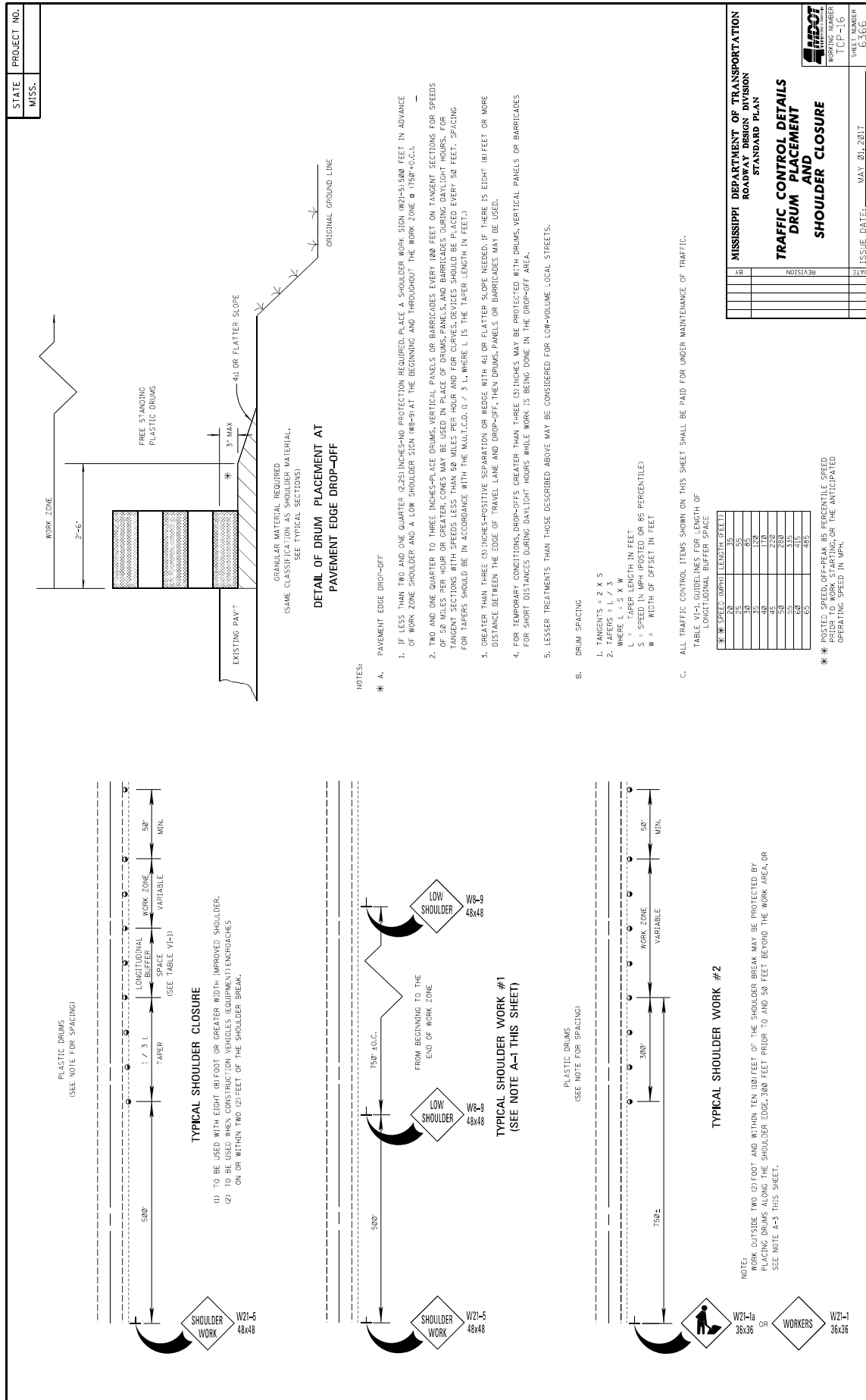
**LOCATION OF
R16-3 SIGNS
(SPEEDING FINES
DOUBLED)**

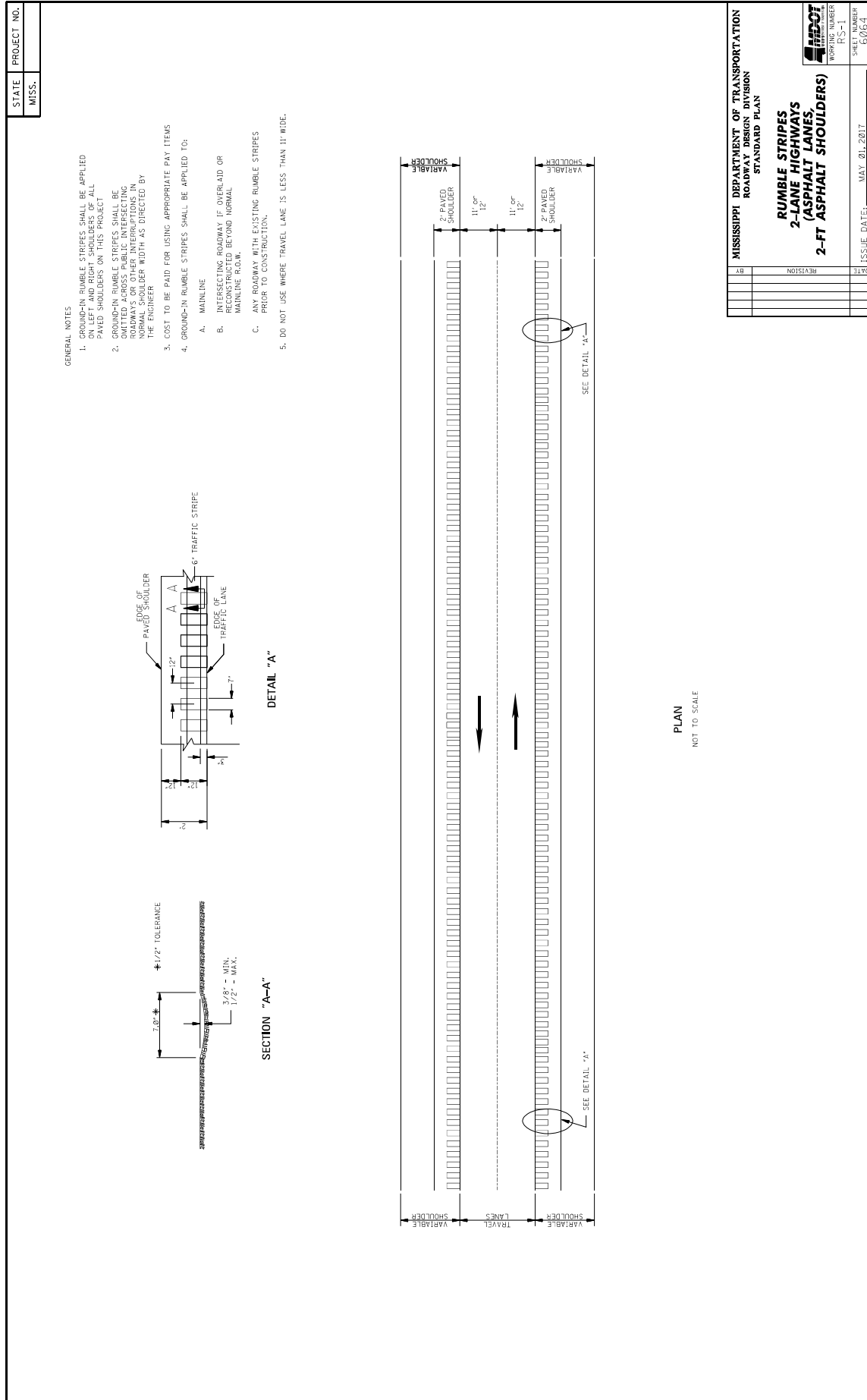
WORKING NUMBER
TCP-15

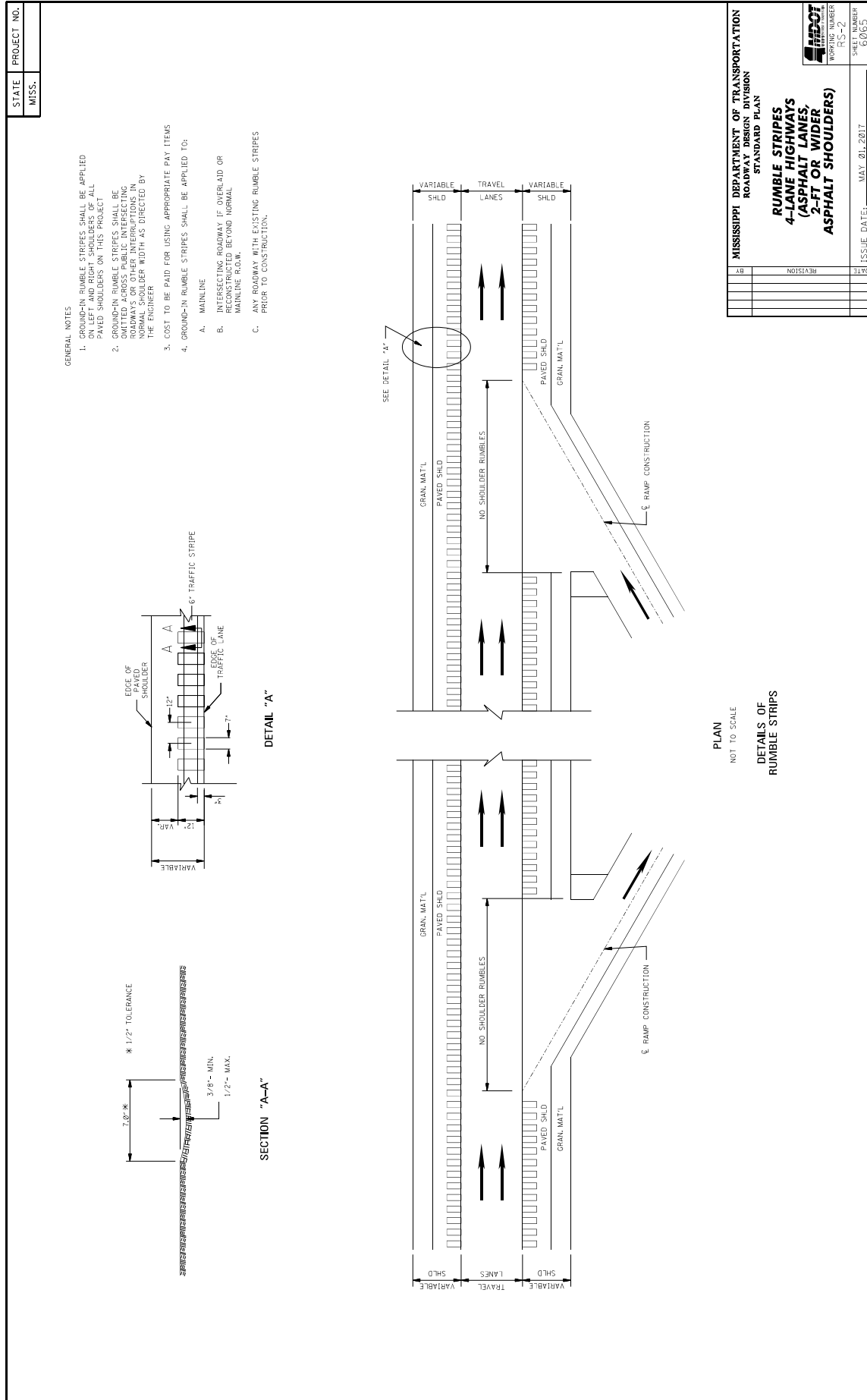
SHEET NUMBER
6365

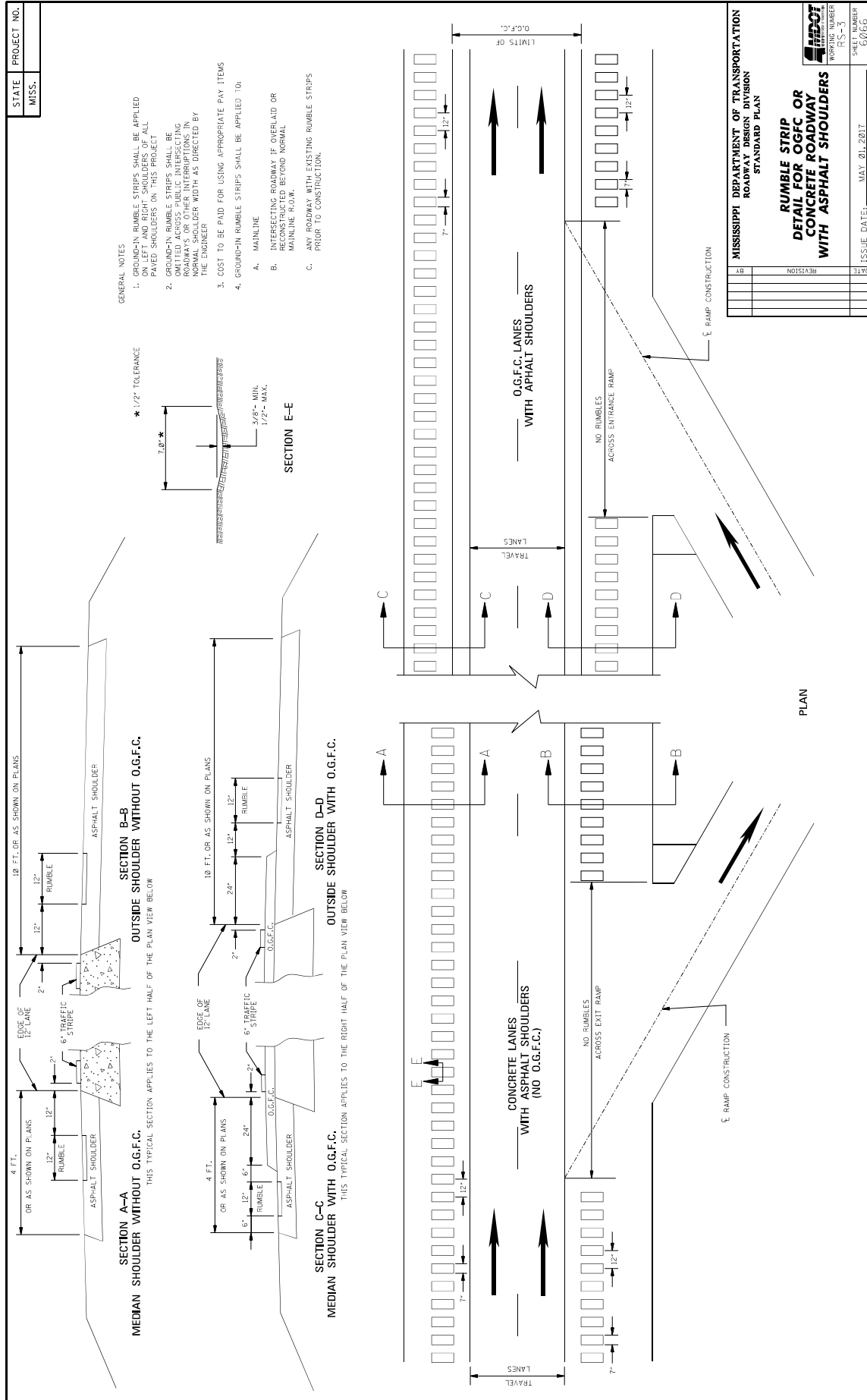
ISSUE DATE: MAY 01, 2017

DATE	REVISION	BY









MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 445

CODE: (SP)

DATE: 10/10/2017

SUBJECT: Mississippi Agent or Qualified Nonresident Agent

Bidders are hereby advised of the requirements of Subsections 102.08, 103.05.2, and 107.14.2.1 of the *2017 Standard Specifications for Road and Bridge Construction* as it refers to bonding agents. Proposal guaranties, bonds, and liability insurance policies must be signed by a **Mississippi Agent or Qualified Nonresident Agent**.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 447

CODE: (SP)

DATE: 10/18/2017

SUBJECT: Traffic on Milled Surface in Rural Areas

Bidders are hereby advised that when the main lanes of a roadway are fine milled, traffic will be allowed to run on a milled surfaces for up to seven (7) calendar days. The Contractor will be assessed a penalty of \$5,000 per calendar day afterwards until the milled surfaces are covered with the next lift of asphalt. It shall be the Contractor's responsibility to ensure that the milling operations do not commence until such time as forecasted weather conditions are suitable enough to allow the placement of the asphalt pavement after the milling operations.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 516

CODE: (IS)

DATE: 11/28/2017

SUBJECT: Errata and Modifications to the 2017 Standard Specifications

<u>Page</u>	<u>Subsection</u>	<u>Change</u>
16	102.06	In the seventh full paragraph, change “Engineer” to “Director.”
33	105.05.1	In the sixth sentence, change “Contract Administration Engineer” to “Contract Administration Director.”
34	105.05.2.1	In subparagraph 2, change “SWPPP, ECP” to “SWPPP and the ECP”
35	105.05.2.2	In subparagraphs 2, add “ and” to the end of the sentence. In subparagraph 3, remove “, and” and add “.”.
90	109.04.2	In the last paragraph of subparagraph (a), place a period “.” at the end of the sentence.
93	109.04.2	In the last paragraph of subparagraph (g), place a period “.” at the end of the sentence. Also, in the first paragraph of subparagraph (h), place a period “.” at the end of the sentence.
97	109.07	Under ADJUSTMENT CODE, subparagraph (A1), change “HMA mixture” to “Asphalt mixtures.”
98	109.11	In the third sentence, change “Engineer” to “Director.”
219	308.04	In the last sentence of the last paragraph, change “Contractor’s decision” to “Engineer’s decision.”
300	405.02.5.9	In the first sentence of the second paragraph, change “Hot Mix Asphalt” to “Asphalt Mixtures.”
502	630.01.1	In the first paragraph, change “ <u>AASHTO</u> ” to “AASHTO’s <u>LRFD</u> ”.
636	646.05	Change “each” to “per each” for the pay item units of payment.
640	656.02.6.2	In item 7), change “down stream” to “downstream”.
688	630.03.2	Change the subsection number from “630.03.2” to “680.03.2.”

- | | | |
|-----|---------------|---|
| 725 | 702.08.3 | In the second sentence of the first paragraph, change “hot-mix” to “asphalt.” |
| 954 | 804.02.13.1.6 | In the definition for “M” in the % Reduction formulas, change “paragraph 7.3” to “paragraph 5.3.” |

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 900

CODE: (SP)

DATE: 05/18/2018

SUBJECT: Contract Time

PROJECT: MP-5080-51(023) / 306367301 – Newton County

The completion of work to be performed by the Contractor for this project will not be a specified date but shall be when all allowable working days are assessed, or any extension thereto as provided in Subsection 108.06. It is anticipated that the Notice of Award will be issued no later than **July 10, 2018** and the date for Notice to Proceed / Beginning of Contract Time will be **August 9, 2018**.

Should the Contractor request a Notice to Proceed earlier than **August 9, 2018** and it is agreeable with the Department for an early Notice to Proceed, the requested date will become the new Notice to Proceed date.

120 Working Days have been allowed for the completion of work on this project.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 901

CODE: (SP)

DATE: 05/08/2018

SUBJECT: Scope of Work

PROJECT: MP-5080-51(023) / 306367301 – Newton County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings”. All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following:

US Highway 80 Milling and Overlaying US 80 from Scott County Line to 1000 Feet East of Scanlan Street in Newton County.

Mill & overlay approximately 10 miles of existing asphalt/composite pavement, repairing failed areas, 2-foot trench widening where applicable, leveling, shoulder erosion repair, placing rumble strips and replacing guard rail on US 80 in Newton County beginning at BOP Station 10+00 and ending at 1,000 feet east of Scanlan Street at the EOP Station 530+00.

Station 10+00 (BOP) to 30+00.

Work in this area shall consist of repairing failed areas full depth with 12.5-mm, MT, Leveling asphalt, 2-foot trench widening with 19-mm, MT, asphalt where applicable, milling the existing pavement to a depth of 1.5” on the existing 2% slope (if the slope is not 2%, the slope will be corrected by a combination of milling and paving), and placing 2” of 12.5-mm, MT, asphalt, as per the attached details and typical sections. Shoulder erosion repair will be made as directed by the Engineer using rip rap and geotextile. The locations and quantities of rip rap and geotextile can be found in attached tables. The area will be site graded to produce a uniform slope and shape prior to the installation of the geotextile and riprap. Site grading will not be measured for separate payment and should be included in other items bid. All local roads shall be milled to a depth of 1.5” and inlayed with 12.5-mm, MT, asphalt.

Station 30+00 to 530+00(EOP).

Work in this area shall consist of repairing failed areas full depth and leveling with 12.5-mm, MT, Leveling asphalt, repairing any failed JRCP joints full depth to three feet (3’) wide on either side of joint, sawing and sealing asphalt joints, milling the existing pavement to a depth of 1.5” on the existing 2% slope (if the slope is not 2%, the slope will be corrected by a combination of milling and paving), placing 2” of 12.5-mm, MT, asphalt, as per the typical sections, and replacing guard rail as per the attached details and typical sections. Asphalt located at Station 525+09 to 525+17 and 528+32 to 528+40 shall be removed from concrete approach slab by

milling. Mainline paving will be tied in to bridge approach slab at Stations 525+09 and 528+40. Guardrail post attached to bridge and curb at BR 107.8 can remain in place. All other guardrail will be replaced using appropriate pay items.

Shoulder erosion repair will be made as directed by the Engineer using rip rap and geotextile, locations and quantities can be found in attached tables. The area will be site graded to produce a uniform slope and shape prior to the installation of the geotextile and riprap. Site grading will not be measured for separate payment and should be included in other items bid.

West Church Street: Station 10+00 to 25+17.

Work in this area shall consist of milling the existing pavement to a depth of 1.5" and placing 2" of 12.5-mm, MT, asphalt, and replacing guard rail as per the attached details and typical sections.

General Notes:

Prior to the milling and paving operations, place a 2-foot trench widening with 19-mm, MT, asphalt where applicable. Failed areas in the existing pavement shall be repaired as per the attached details and typical sections. Any granular base deemed unsuitable by the Engineer shall be removed as directed and backfilled with 12.5-mm, MT, asphalt. Payment for the excavation of the granular base will be made using pay item 203-G: Excess Excavation. A list of the failed areas is shown in the attached table. Pavement repairs shall be completed as a continuous operation in order to minimize the traffic impact. Lane closures shall be in place until the failed area has been completely repaired.

Milling operations shall be in accordance with the contract documents and the Standard Specifications. Variable and length transitions will be used for local roads. Milling of driveway pads shall not be done simultaneously with main line milling. Milling of driveway pads shall be conducted in a manner to prevent gouging or otherwise affecting the roadway pavement structure and slope.

The following recommendations from Bridge Division will pertain to Bridge #106.2 on Old US Highway 80 and will only be modified by the approval of the Director of Structures, State Bridge Engineer.

1. A milling machine with a milling head no larger than four feet (4') wide shall be used to completely remove the asphalt overlay. The milling head shall be of the same type as to produce "fine milling" as defined by Subsection 406.3.1 of the Standard Specifications. The milling will be accomplished with individual passes not to exceed one inch (1"). The estimated depth of asphalt overlay on the bridge is three inches (3").
2. The intent of the work at this bridge is to completely remove the asphalt overlay. Other removal methods may be necessary if the milling machine does not completely remove the asphalt.
3. In addition to milling the bridge deck, the Contractor shall also mill the existing roadway in order to provide a smooth transition between the existing alignment and the bridge deck. The amount of milling necessary for this transition shall be determined in the field and approved by the Engineer.

4. Any damages that occur to the existing structure during the construction process shall be repaired as directed by the Engineer at no additional cost to the Department.
5. Maintenance of traffic during this operation shall consist of standard 2-lane, 2-way traffic control methods. Flaggers and signage shall be employed as necessary to control traffic. The cost of maintenance of traffic for this operation shall be included in pay item 618-A: Maintenance of Traffic.

Where slope correction is required correction will be made either by milling, paving, or combination thereof as directed by the Engineer. Milling correction: Mill outside edge of pavement to a depth of 1½" on a 2% slope towards the centerline. Paving Correction: Mill to depth of 1½" on existing slope, and place asphalt at 2¼" and variable on centerline and 1½" on outside edge. Combination Method: Combination of both methods as directed by the Engineer to achieve the desired slope. In super elevated areas where correct super-elevation exists, milling will transition to thickness through curves. Where correct super-elevation does not exist, milling will transition at curves to correct super-elevation as directed by the Engineer.

Bituminous curbs shall be removed and replaced where damaged curbs exist as directed by the engineer. Various locations are on the attached sheet. Bituminous curbs (existing and replaced) on illuminated streets shall be painted; the cost of paint shall be absorbed. Bituminous curbs on non-illuminated streets shall not be painted, but have raised pavement markers placed alongside of said curb.

Following the overlaying operation the transverse joints in the pavement shall be sawed and sealed within seven (7) days. The details for sawing and sealing transverse joints for this section are as shown in the attached drawings. The width of the sawing and sealing operation will be 11 feet on each side of centerline, unless otherwise directed, to prevent "sympathy cracking." It's the responsibility of the contractor to locate and mark all existing joints that are to be sawed and sealed prior to the milling operation.

Guardrails shall be replaced. The locations and quantities are listed in the attached table. Pay item 202-B: Removal of Guard Rail, Including Rail, Posts & Terminal Ends, shall consist of the removal of bridge end section, w-beam, terminal end section, posts, and all other appurtenances. All guardrail removed is to be replaced the same day and prior to reopening the adjacent lane of traffic. Guardrail post attached to curb at Bridge 107.8 can remain in place, but the w-beam and block-outs must be changed. Voids created by the removal of posts, concrete anchors, footings, etc. shall be backfilled and tamped in accordance with Section 203 of the Standard Specifications. All existing guardrail will be retained by the Department. The retained material shall be delivered to the MDOT Newton Maintenance Yard, Highway 80 Newton, MS 39345. The Contractor is responsible for coordinating the delivery of the retained guard rail with MDOT maintenance personnel. Sufficient advance notice shall be given to ensure the MDOT maintenance is equipped to handle the delivery. Object markers at the bridge approaches are also to be replaced. Asphalt is to be extended under the guard rail and two feet (2') behind guard rail post as per the attached detail. The area to be paved shall be bladed to accommodate 3" of 12.5-mm, MT, asphalt. The excavated material shall be retained and used to raise the existing shoulder to match the new pavement elevation. The cost of blading will be an absorbed item and is not to be included in the price of pay items bid. Material which cannot be placed and blended

in adjacent areas and deemed to be excess excavation by the Engineer shall be removed under Pay Item No. 203-G: Excess Excavation.

The Reclaimed Asphalt Pavement (RAP) material removed by the milling operations shall become the property of the Contractor with the exception of 10,000 tons or 50% of the total anticipated quantity, whichever is less. The reclaimed material shall be delivered to the MDOT Newton Maintenance Yard, Highway 80 Newton, MS 39345. The Contractor is responsible for coordinating the delivery of the RAP material with MDOT maintenance personnel. Sufficient advance notice shall be given to ensure the MDOT maintenance is equipped to handle the delivery. The Contractor shall be responsible for providing the equipment and operator to neatly stockpile the milled asphalt.

Temporary striping shall be placed as required prior to opening the roadway to traffic. Overnight lane closures will not be allowed for this operation. Milling shall be performed in accordance with the attached drawings at all city streets, mainline tie-ins, crossovers, etc. Transitions at local roads may be milled and left open if properly maintained. Temporary pavement joints (paper joints) shall be at least three (3) paper-widths long shall be used at all milled tie-ins and shall be adequately maintained.

Publicly maintained roads and streets shall be milled and paved to the existing right-of-way or as directed by the Engineer. Privately owned entrances shall be paved to the shoulder line per the included typical drawing. Pads shall be shaped horizontally and vertically to prevent excessive drop-offs. Granular Material (Crushed Stone) shall be placed around the pads to prevent shoulder drop-offs as directed and shall be placed in a timely manner. Drop-offs exceeding 2.5" shall be corrected within two (2) calendar days of the placement of the pad. Granular Material (Crushed Stone) will be used as directed by the Engineer to fill in existing low shoulders. Stabilizer aggregate shall be used as directed by the Engineer. Crushed concrete will not be allowed.

It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged during the life of the contract. No payment will be made for replacement or repair of damaged items.

Any signs that are in conflict with construction of this project shall be removed and relocated by the Contractor as directed by the Engineer, the cost of which is to be absorbed in other items bid.

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost is to be included in the price bid for pay item 618-A, Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background. Standard roadside construction signs and barricades will be paid for using the appropriate pay items.

Roadside construction signs, barrels, etc. shall be placed in accordance with the attached drawings or as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed.

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal; the cost is to be included in the prices of items bid. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor.

Where applicable, the existing shoulders are to be raised to match the new pavement elevation by placing variable depth Granular Material (Crushed Stone) on the existing shoulders. Crushed concrete will not be allowed.

Placement of the granular material (crushed stone) on the finished asphalt course shall not be permitted. The existing shoulder shall be scarified to allow incorporation of the new shoulder material. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%). Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%). The cost of blading will be an absorbed item and is to be included in the price of other items bid. Crushed concrete will not be allowed.

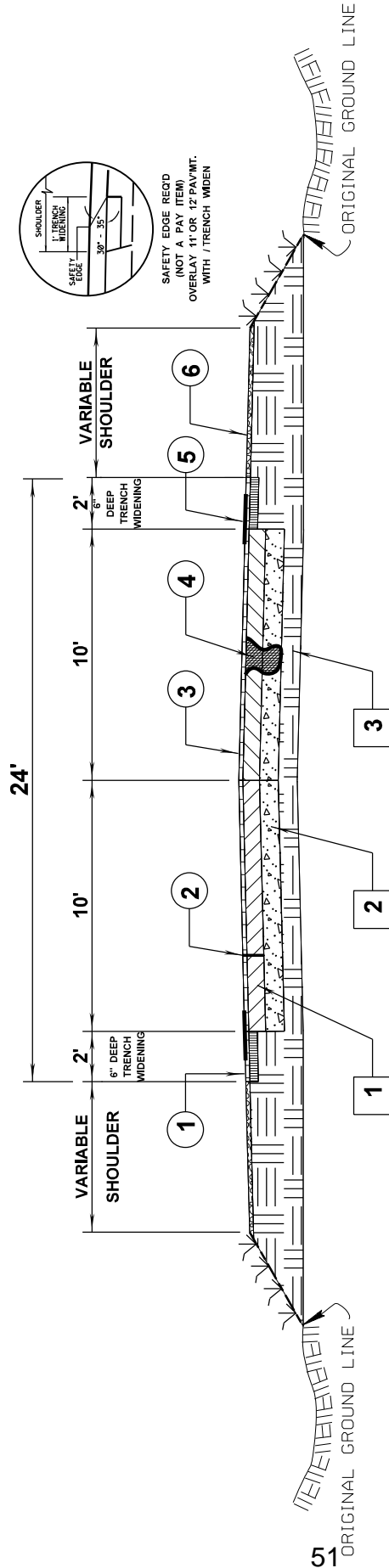
Removal of the existing shoulder material shall be coincidental with the milling/overlaying operation to prevent the possible ponding of water. No payment will be made for blading or removal of the existing shoulder material. Any material excavated from the existing shoulder shall be used to raise the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, foreslopes, or other adjacent areas as directed by the Engineer and will be an absorbed item. Material which cannot be placed in adjacent areas and deemed to be excess excavation by the Engineer shall be removed under pay item 203-G Excess Excavation. Shoulders will be pulled up at the end of each days paving operation.

Temporary traffic stripe will be required immediately after the required overlay and prior to opening area to traffic. Temporary stripe is to be placed in the same location and configuration as the permanent stripe.

All permanent striping will be thermoplastic, 90-mil double drop. Rumble stripe will be placed throughout the project limits in accordance with the attached details. Edge lines will be placed to

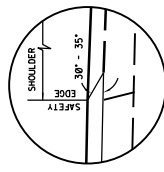
accommodate the lane widths shown on the attached applicable typical sections unless prevented by field conditions.

NEWTON COUNTY
 306367/301000
 TYPICAL SECTION - 1
 WIDEN AND OVERLAY
 STA. 10+00 - STA. 30+00

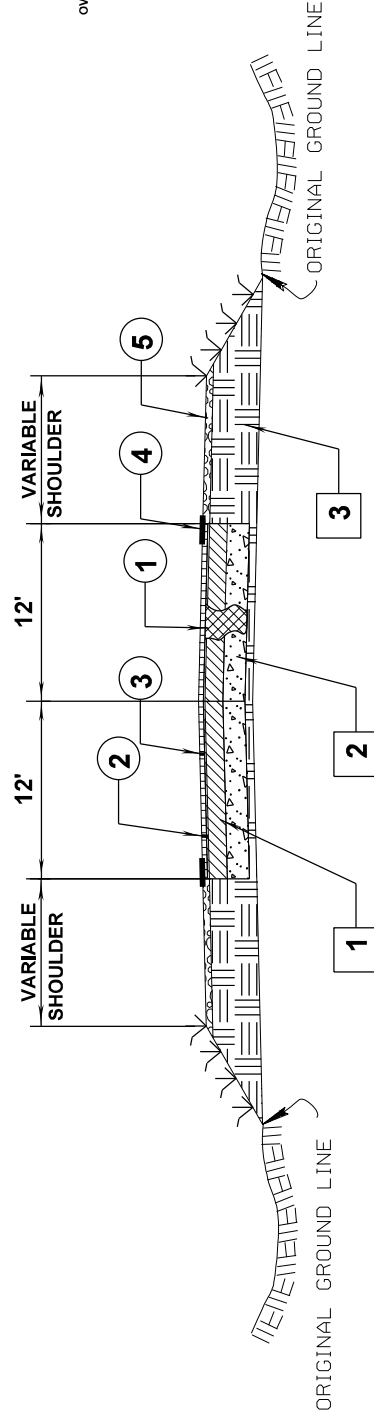


- 1 Existing 6 $\frac{1}{2}$ " to 8" asphalt pavement
 - 2 Existing 4 $\frac{1}{4}$ " to 9" JRCP
 - 3 6" and variable granular material
 - 4 Proposed ASPHALT 12.5mm Mix, MT
 - 5 EXISTING ASPHALT PAVEMENT
 - 6 CONCRETE
 - 7 GRANULAR MATERIAL
 - 8 CRUSHED STONE BASE
- 1 Excavate 6" deep X 2' wide place 6" asphalt pavement, 19mm Mix, MT. where applicable
 - 2 Mill 1 $\frac{1}{2}$ " and variable of existing asphalt
 - 3 Place 2" of asphalt pavement 12.5mm Mix, MT
 - 4 Repair all failed areas full depth with 12.5MM, MT Leveling
 - 5 Place rumble strips from B.O.P. to Newton City Limits
 - 6 Place granular (crushed stone base) material to bring shoulders to grade after overlay.

NEWTON COUNTY
306367/301000
TYPICAL SECTION - 2
STA. 30+00 - STA. 432+14



SAFETY EDGE REQ'D
 (NOT A PAY ITEM)
 OVERLAY 11" OR 12" PAV'T.
 WITHOUT TRENCH



PROPOSED

- ① - Repair all failed areas full depth with 12.5mm MIX, MT Leveling
- ② - Mill 1½" of asphalt
- ③ - Place 2" Hot Mix Asphalt, 12.5mm Mix, MT
- ④ - Place 2' of rumble strips from BOP to Newton City Limits
- ⑤ - Place granular (crushed stone base) material to bring shoulders to grade after overlay

1 Existing 6½" to 8" asphalt pavement

2 Existing 4¼" to 9" JRCP

3 6" and variable granular material

PROPOSED ASPHALT 12.5mm Mix, MT

EXISTING ASPHALT PAVEMENT

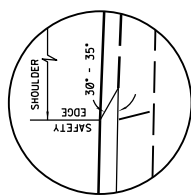
CONCRETE

GRANULAR MATERIAL

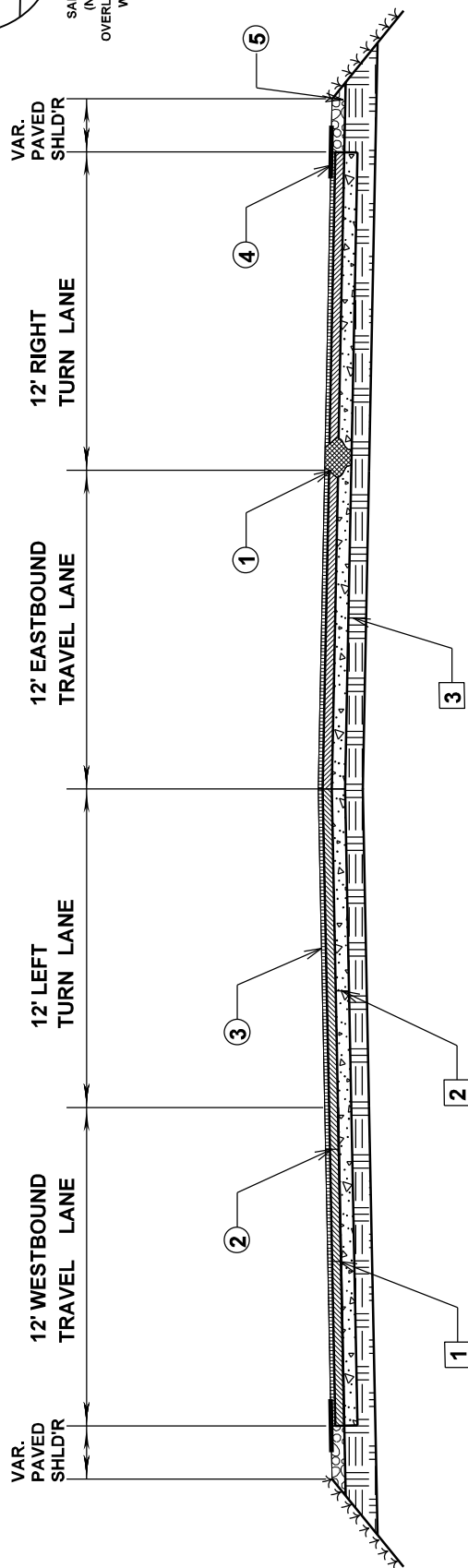
GRANULAR MATERIAL

NEWTON COUNTY
306367/301000

TYPICAL SECTION 3
LOCATION: LAWRENCE FEED MILL
AT PONDEROSA ROAD



SAFETY EDGE REQ'D
(NOT A PAY ITEM)
OVERLAY 11' OR 12' PAV'T.
WITHOUT TRENCH



- 8 -

EXISTING

- 1 - Existing 6½" to 8" asphalt pavement
- 2 - Existing 4½" to 9" OF JRCP
- 3 - 6" and variable granular material



PROPOSED ASPHALT 12.5mm Mix, MT



EXISTING ASPHALT PAVEMENT



CONCRETE



GRANULAR MATERIAL



CRUSHED STONE BASE

PROPOSED

- 1 - Repair all failed areas full depth with 12.5mm MIX, MT Leveling
- 2 - Mill 1½" of asphalt
- 3 - Place 2" hot mix asphalt, 12.5mm MIX, MT
- 4 - Place 2' of rumble strips from BOP to Newton City Limits
- 5 - Place granular (crushed stone base) material to bring shoulders to grade after overlay

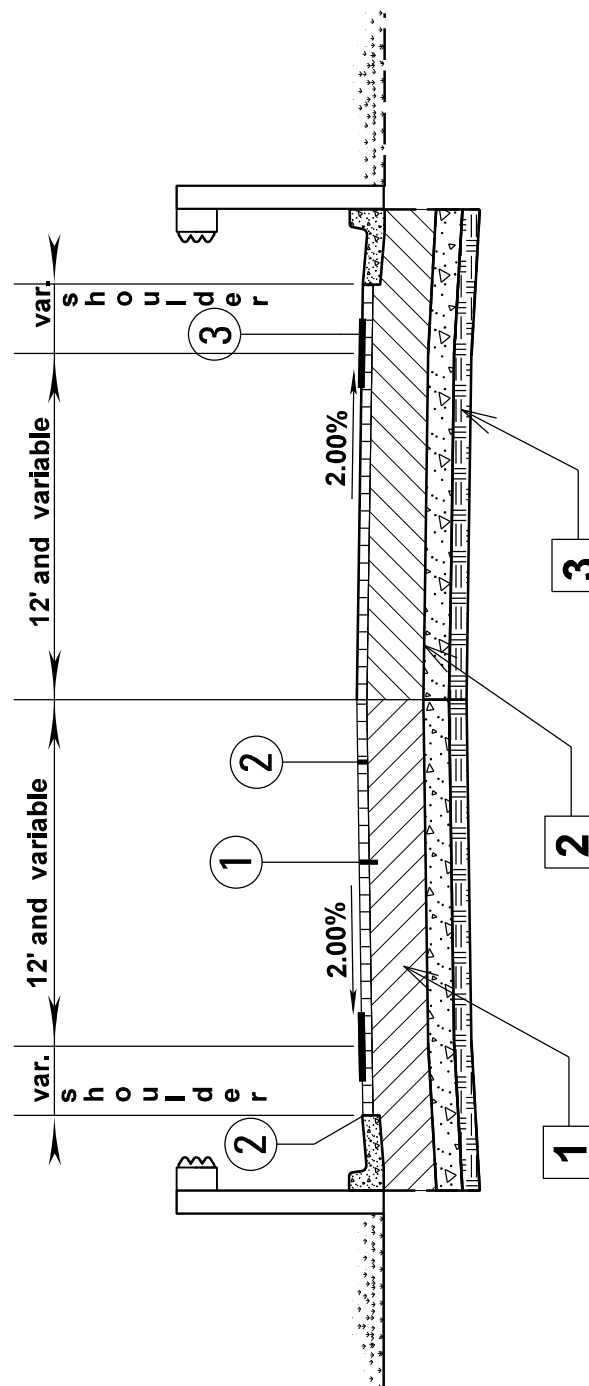
NEWTON COUNTY

306367/301000

TYPICAL SECTION - 4

AT EXIT 100

UNDERNEATH THE BRIDGE AT INTERSTATE 20



1 Existing 6 $\frac{1}{2}$ " to 8" asphalt pavement

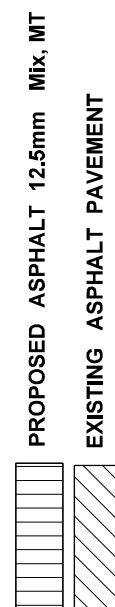
2 Existing 4 $\frac{1}{2}$ " to 9" JRCP

3 6" and variable granular material

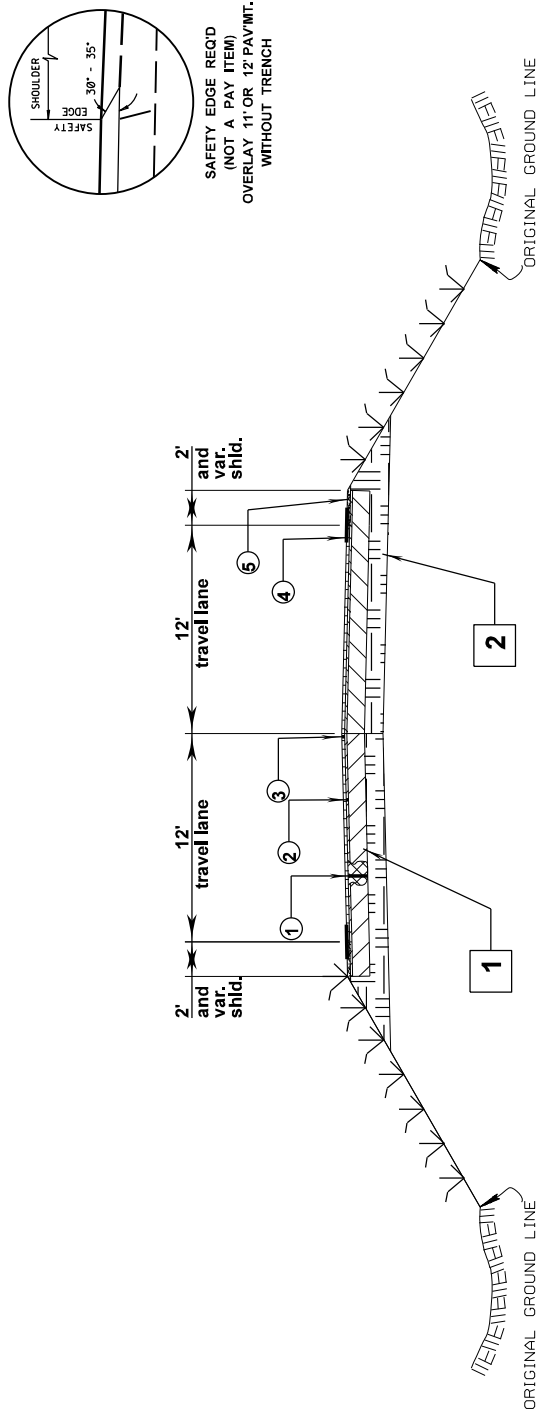
1 Mill 1 $\frac{1}{2}$ " depth of existing asphalt

2 Place 2" asphalt pavement 12.5mm Mix, MT. and tie back to existing concrete curb

3 Place rumble strips from BOP to Newton City Limits.



NEWTON COUNTY
 306367/301000
 TYPICAL SECTION - 5
 STA. 432+14 STA. 530+00

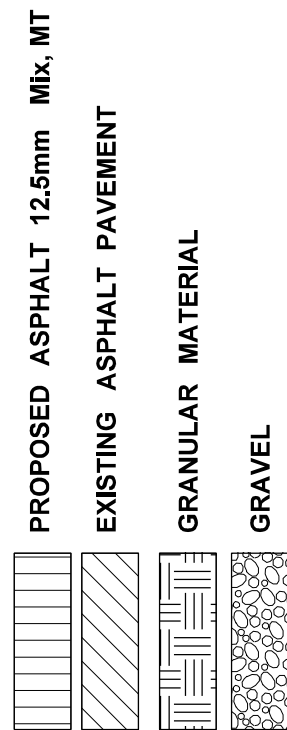


PROPOSED

- ① - Repair all failed areas full depth with 12.5mm MIX, MT Levelling
- ② - Mill 1½ of asphalt
- ③ - Place 2" hot mix asphalt, 12.5mm Mix, MT
- ④ - Place 2' OF rumble strips from BOP to Newton City Limits
- ⑤ - Place granular (crushed stone base) material to bring shoulders to grade after overlay

1 Existing 11" to 12" asphalt pavement

2 variable granular material



Removal of Bituminous Curb				
Station	Lane	Location	Removal (LF)	Remarks
355+50	LT	Lawrence Bethal Road	4	broken off
470+00		Newton Conehatta	145	
470+00		Ford Avenue	138	
423+75	RT	MDOT	5	broken off
TOTAL			292	

Bituminous Curb				
Station	Lane	Location	Removal (LF)	Remarks
470+00		Newton Conehatta	145	
470+00		Ford Avenue	138	
TOTAL			283	

LEVELING MAINLINE: 403-B002 12.5MM, MT Asphalt Pavement, Leveling								
Station to Station		Location	Width (FT)	Length (FT)	Total (SF)	Depth (FT)	Total (TONS)	Remarks
12+90	14+50	LT & RT	22	160	3520.000	0.167	44	
150+00	152+00	LT & RT	28	200	5600.000	0.167	70	
							TOTAL	
							114	

NEWTON COUNTY - 306367/301000

Guardrail Quantities																	
GUARDRAIL			THRIE BEAM		TERMINAL		Cable		BRIDGE END SECTION				DELINEATORS		REMOVAL ITEMS		REMARKS
STATION	(W-BEAM) (LF)	TRANS. SECT. (LF)	THRIE BEAM (LF)	SECTION	ANCHOR TYPE I	TYPE "A" (EA)	TYPE "C" (EA)	TYPE "F" (EA)	SPECIAL DESIGN BRIDGE END CONN.	WHITE	YELLOW	Object Markers	Type 3	GUARDRAIL			
115+40	75	12.5	225	2						13				387.5		LEFT	
117+20	75	12.5	225	2						13				387.5		RIGHT	
525+42	137.5			1		1			1	7		1		237.5		RIGHT	
525+55	25			1		1			1	3		1		100		LEFT	
527+90	25			1		1			1	3		1		100		Right	
528+06	87.5			1		1			1	7		1		162.5		LEFT	
23+50	100			1			1		1	5		1		162.5		LEFT	
24+38	37.5			1				1		3		1		100		RIGHT	
23+18	37.5			1				1		3		1		100		LEFT	
24+16	100			1			1		1	5		1		162.5		RIGHT	
TOTAL =	700	25	450	12	0	4	2	2	6	62	0	8		1900			
	L.F.	EA.	LF.	EA.	EA.	EA.	EA.	EA.	LF.	EA.	EA.	EA.		LF.			
* REMOVAL OF ALL GUARDRAIL (BRIDGE END SECTIONS, W-BEAM, TYPE-I CABLE ANCHORAGE, TERMINAL END SECTIONS, ETC.) WILL BE PAID UNDER PAY ITEM 202-B REMOVAL OF GUARD RAIL.																	
* REMOVAL OF GUARDRAIL DELINEATORS ARE CONSIDERED INCIDENTAL TO THE REMOVAL OF GUARDRAIL AND WILL NOT BE MEASURED AS A SEPARATE PAY ITEM.																	
* ALL GUARDRAIL WILL BE RETAINED BY MDOT.																	
GUARD RAIL POSTS ATTACHED TO CURB AT BRIDGE 107.8 CAN REMAIN IN PLACE, BUT THE W-BEAM AND BLOCK-OUTS MUST BE CHANGED.																	

FAILED AREAS

STATION	STATION	LANE	WIDTH (FT)	LENGTH (FT)	AREA (SF)*	Removal of Concrete w/variable depth overlay (SY)	12.5MM. MT ASPHALT PAVEMENT LEVELING (Tons)	Excess Excavation, LVM, AH (CY)
12+90	14+50	RT	12	160	1920.0	213.333	144.00	35.56
13+60	13+90	RT	12	30	360.0	40.000	27.00	6.67
28+00	34+00	RT	6	600	3600.0	400.000	270.00	66.67
34+90	38+00	RT	6	310	1860.0	206.667	139.50	34.44
43+10	43+20	RT	12	10	120.0	13.333	9.00	2.22
138+60	139+63	RT	6	103	618.0	68.667	46.35	11.44
140+40	143+15	RT	12	275	3300.0	366.667	247.50	61.11
145+00	146+15	RT	12	115	1380.0	153.333	103.50	25.56
147+28	148+33	RT	12	105	1260.0	140.000	94.50	23.33
155+48	157+48	RT	6	200	1200.0	133.333	90.00	22.22
175+95	176+50	RT	6	55	330.0	36.667	24.75	6.11
179+27	179+78	RT	6	51	306.0	34.000	22.95	5.67
188+54	190+53	RT	6	199	1194.0	132.667	89.55	22.11
192+27	192+90	RT	6	63	378.0	42.000	28.35	7.00
202+72	202+90	RT	6	18	108.0	12.000	8.10	2.00
253+60	254+80	RT	6	120	720.0	80.000	54.00	13.33
260+10	260+20	RT	12	10	120.0	13.333	9.00	2.22
263+90	264+04	RT	12	14	168.0	18.667	12.60	3.11
317+16	319+00	RT	6	184	1104.0	122.667	82.80	20.44
330+00	331+00	RT	12	100	1200.0	133.333	90.00	22.22
330+10	331+35	RT	6	125	750.0	83.333	56.25	13.89
332+54	334+05	RT	6	151	906.0	100.667	67.95	16.78
332+85	333+28	RT	12	43	516.0	57.333	38.70	9.56
				SUBTOTAL	23418.0	2602.000	1756.35	433.667

STATION	STATION	LANE	WIDTH (FT)	LENGTH (FT)	AREA (SF)*	Removal of Concrete w/variable depth overlay (SY)	12.5MM, MT ASPHALT PAVEMENT LEVELING (Tons)	Excess Excavation, LVM, AH (CY)
36+27	38+00	LT	12	173	2076.0	230.667	155.70	38.44
40+50	41+78	LT	6	128	768.0	85.333	57.60	14.22
147+28	148+33	LT	12	105	1260.0	140.000	94.50	23.33
175+67	176+62	LT	6	95	570.0	63.333	42.75	10.56
196+65	199+00	LT	6	235	1410.0	156.667	105.75	26.11
203+22	204+81	LT	6	159	954.0	106.000	71.55	17.67
329+90	333+13	LT	6	323	1938.0	215.333	145.35	35.89
330+00	331+00	LT	12	100	1200.0	133.333	90.00	22.22
367+40	369+10	LT	6	170	1020.0	113.333	76.50	18.89
SUBTOTAL					11196.0	1244.000	839.70	207.333
PAGE TOTALS					270	34614.000	2596.050	641.000
SAW CUT =					540	150	PCF =	150
4529						1	DEPTH=	0.5

FAILED AREAS

STATION	STATION	LANE	WIDTH (FT)	LENGTH (FT)	AREA (SF)*	Removal of Asphalt (SY)	12.5MM, MT ASPHALT PAVEMENT LEVELING (Tons)	Excess Excavation, LVM, AH (CY)
450+60	451+80	LT	12	120	1440.0	160,000	108.00	28.67
465+77	466+20	LT	6	43	160,000	17,778	12.00	2.96
				SUBTOTAL	1600.0	177,778	120.00	29,630
PAGE TOTALS					1600.0	177,778	120,000	29,630

FAILED AREAS (trench widening)

STATION	STATION	LANE	WIDTH (FT)	LENGTH (FT)	AREA (SF)*	Removal of Asphalt (SY)	12.5MM. MT ASPHALT PAVEMENT LEVELING (Tons)	Excess Excavation, LVM, AH (CY)
13+06	13+20	RT	2	14	28.0	3.111	2.10	0.52
14+70	15+60	RT	1	90	90.0	10.000	6.75	1.67
18+32	19+37	RT	1	105	105.0	11.667	7.88	1.94
21+59	23+51	RT	3	192	576.0	64.000	43.20	10.67
25+19	25+67	RT	3	48	144.0	16.000	10.80	2.67
27+56	27+82	RT	3	26	78.0	8.667	5.85	1.44
28+00	34+00	RT	4	600	2400.0	266.667	180.00	44.44
34+90	38+00	RT	4	310	1240.0	137.778	93.00	22.96
40+38	41+80	RT	3	142	426.0	47.333	31.95	7.89
42+00	42+25	RT	2	25	50.0	5.556	3.75	0.93
290+88	291+50	RT	3	62	186.0	20.667	13.95	3.44
292+43	305+10	RT	3	1267	3801.0	422.333	285.08	70.39
409+95	413+60	RT	3	365	1095.0	121.667	82.13	20.28
12+90	14+22	LT	3	132	396.0	44.000	29.70	7.33
24+72	25+62	LT	2	90	180.0	20.000	13.50	3.33
34+50	38+75	LT	3	425	1275.0	141.667	95.63	23.61
141+50	144+00	LT	3	250	750.0	83.333	56.25	13.89
284+75	284+88	LT	3	13	39.0	4.333	2.93	0.72
287+00	287+50	LT	2	50	100.0	11.111	7.50	1.85
PAGE TOTALS					12959.0	1439.889	971.93	239.981

JOINT REPAIR AREAS

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Notice to Bidder No.901 -- Cont'd

STATION	WIDTH (FT)	LENGTH (FT)	AREA (SF)*	Removal of Concrete w/variable depth overlay (SY)	12.5MM, MT ASPHALT PAVEMENT LEVELING (Tons)	Excess Excavation, LVM, AH (CY)
43+72	22	6	132.0	14.667	9.90	2.44
44+33	22	6	132.0	14.667	9.90	2.44
45+50	22	6	132.0	14.667	9.90	2.44
46+07	22	6	132.0	14.667	9.90	2.44
47+90	22	6	132.0	14.667	9.90	2.44
75+50	22	6	132.0	14.667	9.90	2.44
76+40	22	6	132.0	14.667	9.90	2.44
77+12	22	6	132.0	14.667	9.90	2.44
77+74	22	6	132.0	14.667	9.90	2.44
78+35	22	6	132.0	14.667	9.90	2.44
78+95	22	6	132.0	14.667	9.90	2.44
79+25	22	6	132.0	14.667	9.90	2.44
79+65	22	6	132.0	14.667	9.90	2.44
80+20	22	6	132.0	14.667	9.90	2.44
80+78	22	6	132.0	14.667	9.90	2.44
81+40	22	6	132.0	14.667	9.90	2.44
82+00	22	6	132.0	14.667	9.90	2.44
82+30	22	6	132.0	14.667	9.90	2.44
82+62	22	6	132.0	14.667	9.90	2.44
83+85	22	6	132.0	14.667	9.90	2.44
84+75	22	6	132.0	14.667	9.90	2.44
86+90	22	6	132.0	14.667	9.90	2.44
87+08	22	6	132.0	14.667	9.90	2.44
87+50	22	6	132.0	14.667	9.90	2.44
90+00	22	6	132.0	14.667	9.90	2.44
90+60	22	6	132.0	14.667	9.90	2.44
91+86	22	6	132.0	14.667	9.90	2.44
92+47	22	6	132.0	14.667	9.90	2.44
95+50	22	6	132.0	14.667	9.90	2.44
97+83	22	6	132.0	14.667	9.90	2.44
132+20	22	6	132.0	14.667	9.90	2.44
152+13	22	6	132.0	14.667	9.90	2.44
153+35	22	6	132.0	14.667	9.90	2.44
153+95	22	6	132.0	14.667	9.90	2.44
154+57	22	6	132.0	14.667	9.90	2.44
158+86	22	6	132.0	14.667	9.90	2.44
160+71	22	6	132.0	14.667	9.90	2.44
165+95	22	6	132.0	14.667	9.90	2.44
SUBTOTAL			5016.0	557.333	376.20	92.889

JOINT REPAIR AREAS

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Notice to Bidder No.901 -- Cont'd

STATION	WIDTH (FT)	LENGTH (FT)	AREA (SF)*	Removal of Concrete w/variable depth overlay (SY)	12.5MM, MT ASPHALT PAVEMENT LEVELING (Tons)	Excess Excavation, LVM, AH (CY)
165+53	22	6	132.0	14.667	9.90	2.44
168+40	22	6	132.0	14.667	9.90	2.44
169+70	22	6	132.0	14.667	9.90	2.44
176+43	22	6	132.0	14.667	9.90	2.44
180+36	22	6	132.0	14.667	9.90	2.44
183+70	22	6	132.0	14.667	9.90	2.44
213+90	22	6	132.0	14.667	9.90	2.44
214+50	22	6	132.0	14.667	9.90	2.44
253+36	22	6	132.0	14.667	9.90	2.44
253+95	22	6	132.0	14.667	9.90	2.44
259+95	22	6	132.0	14.667	9.90	2.44
260+58	22	6	132.0	14.667	9.90	2.44
264+72	22	6	132.0	14.667	9.90	2.44
278+22	22	6	132.0	14.667	9.90	2.44
284+36	22	6	132.0	14.667	9.90	2.44
295+42	22	6	132.0	14.667	9.90	2.44
295+72	22	6	132.0	14.667	9.90	2.44
300+35	22	6	132.0	14.667	9.90	2.44
303+12	22	6	132.0	14.667	9.90	2.44
309+22	22	6	132.0	14.667	9.90	2.44
316+22	22	6	132.0	14.667	9.90	2.44
322+50	22	6	132.0	14.667	9.90	2.44
330+43	22	6	132.0	14.667	9.90	2.44
337+45	22	6	132.0	14.667	9.90	2.44
340+27	22	6	132.0	14.667	9.90	2.44
346+12	22	6	132.0	14.667	9.90	2.44
355+16	22	6	132.0	14.667	9.90	2.44
367+92	22	6	132.0	14.667	9.90	2.44
371+02	22	6	132.0	14.667	9.90	2.44
371+64	22	6	132.0	14.667	9.90	2.44
376+32	22	6	132.0	14.667	9.90	2.44
386+93	22	6	132.0	14.667	9.90	2.44
391+85	22	6	132.0	14.667	9.90	2.44
395+10	22	6	132.0	14.667	9.90	2.44
400+62	22	6	132.0	14.667	9.90	2.44
405+20	22	6	132.0	14.667	9.90	2.44
405+51	22	6	132.0	14.667	9.90	2.44
SUBTOTAL			4884.0	542.667	366.30	90.444
PAGE TOTALS			1650	450	9900.000	1100.000
					742.500	183.333

SAW CUT = 3300.000 450

PCF = 150
DEPTH = 1

PCF = 150
DEPTH = 0.5

REPAIR AREA WITH RIPRAP							
STATION	STATION	LANE	WIDTH (FT)	LENGTH (FT)	GEOTEXTILE (SY)	RIPRAP (TON)	
21+90	22+00	RT	20	10	22.222	16.280	
524+75	524+80	RT	15	5	8.333	6.105	
528+80	528+90	RT	10	10	11.111	8.140	
528+50	529+10	RT	12	70	93.333	68.376	
TOTAL					135	99	

107377301000

619-D2001 Standard Roadside Construction Signs (10 Sq. Ft. or More)					
STATION	LOCATION	DESCRIPTION	QUANTITY	UNIT	REMARKS
	RT	W20-1	16	SF	500' West of BOP
	RT	W20-1	16	SF	500' PONDEROSA RD
	LT	W20-1	16	SF	500' PONDEROSA RD
	RT	W20-1	16	SF	500' WHITEHEAD RD
	LT	W20-1	16	SF	500' VELMA MOORE RD
	RT	W20-1	16	SF	500' HIGHWAY 505
	LT	W20-1	16	SF	500' LAWRENCE HAZEL RD
	LT	W20-1	16	SF	500' L C RD
	LT	W20-1	16	SF	500' GREENIE MORROW RD
	LT	W20-1	16	SF	500' LANCASTER RD
	RT	W20-1	16	SF	500' LAWRENCE BETHEL RD
	LT	W20-1	16	SF	500' RUBY LANCASTER RD
	LT	W20-1	16	SF	500' HORSESHOE LOOP RD
	LT	W20-1	16	SF	500' HORSESHOE LOOP RD
	RT	W20-1	16	SF	500' HILLMAN WILLIAMS RD
	LT	W20-1	16	SF	500' MORGAN FIELD RD
	RT	W20-1	16	SF	500' W CHURCH ST
	LT	W20-1	16	SF	500' NEWTON CONEHATTA RD
	RT	W20-1	16	SF	500' FORD AVE
	LT	W20-1	16	SF	500' NEW IRELAND RD
	RT	W20-1	16	SF	500' SCANLAN ST
	LT	W20-1	16	SF	500' East of EOP
		Total	352	SF	

107377301000

619-D1001 Standard Roadside Construction Signs (Less than 10 Sq. Ft.)					
Station	Location	Description	Quantity	Unit	Remarks
10+00	LT	G20-2	8	SF	500' West of BOP
10+00	RT	W14-3	5.56	SF	
17+50	LT	R4-1	5	SF	
17+50	RT	R4-1	5	SF	
25+00	LT	R4-1	5	SF	
25+00	RT	R4-1	5	SF	
32+50	LT	R4-1	5	SF	
32+50	RT	R4-1	5	SF	
40+00	LT	R4-1	5	SF	
40+00	RT	R4-1	5	SF	
47+50	LT	R4-1	5	SF	
47+50	RT	R4-1	5	SF	
55+00	LT	R4-1	5	SF	
55+00	RT	R4-1	5	SF	
62+50	LT	R4-1	5	SF	
62+50	RT	R4-1	5	SF	
70+00	LT	R4-1	5	SF	
77+50	RT	R4-1	5	SF	
77+50	LT	R4-1	5	SF	
85+00	RT	R4-1	5	SF	
85+00	LT	R4-1	5	SF	
92+50	RT	R4-1	5	SF	
92+50	LT	R4-1	5	SF	
100+00	RT	R4-1	5	SF	
100+00	LT	R4-1	5	SF	

107377301000

619-D1001 Standard Roadside Construction Signs (Less than 10 Sq. Ft.)					
Station	Location	Description	Quantity	Unit	Remarks
107+50	RT	R4-1	5	SF	
107+50	LT	R4-1	5	SF	
115+00	RT	R4-1	5	SF	
115+00	LT	R4-1	5	SF	
122+50	RT	R4-1	5	SF	
122+50	LT	R4-1	5	SF	
130+00	RT	R4-1	5	SF	
130+00	LT	R4-1	5	SF	
137+50	RT	R4-1	5	SF	
137+50	LT	R4-1	5	SF	
145+00	RT	R4-1	5	SF	
145+00	LT	R4-1	5	SF	
152+50	RT	R4-1	5	SF	
152+50	LT	R4-1	5	SF	
160+00	RT	R4-1	5	SF	
160+00	LT	R4-1	5	SF	
167+50	RT	R4-1	5	SF	
167+50	LT	R4-1	5	SF	
175+00	RT	R4-1	5	SF	
175+00	LT	R4-1	5	SF	
182+50	RT	R4-1	5	SF	
182+50	LT	R4-1	5	SF	
190+00	RT	R4-1	5	SF	
190+00	LT	R4-1	5	SF	
197+50	RT	R4-1	5	SF	
197+50	LT	R4-1	5	SF	

107377301000

619-D1001 Standard Roadside Construction Signs (Less than 10 Sq. Ft.)					
Station	Location	Description	Quantity	Unit	Remarks
205+00	RT	R4-1	5	SF	
205+00	LT	R4-1	5	SF	
212+50	LT	R4-1	5	SF	
212+50	RT	R4-1	5	SF	
220+00	LT	R4-1	5	SF	
220+00	RT	R4-1	5	SF	
227+50	LT	R4-1	5	SF	
227+50	RT	R4-1	5	SF	
235+00	LT	R4-1	5	SF	
235+00	RT	R4-1	5	SF	
242+50	LT	R4-1	5	SF	
242+50	RT	R4-1	5	SF	
250+00	LT	R4-1	5	SF	
250+00	RT	R4-1	5	SF	
257+50	LT	R4-1	5	SF	
257+50	RT	R4-1	5	SF	
265+00	LT	R4-1	5	SF	
265+00	RT	R4-1	5	SF	
272+50	LT	R4-1	5	SF	
272+50	RT	R4-1	5	SF	
280+00	LT	R4-1	5	SF	
280+00	RT	R4-1	5	SF	
287+50	LT	R4-1	5	SF	
287+50	RT	R4-1	5	SF	
295+00	LT	R4-1	5	SF	
295+00	RT	R4-1	5	SF	

107377301000

619-D1001 Standard Roadside Construction Signs (Less than 10 Sq. Ft.)					
Station	Location	Description	Quantity	Unit	Remarks
302+50	LT	R4-1	5	SF	
302+50	RT	R4-1	5	SF	
310+00	LT	R4-1	5	SF	
310+00	RT	R4-1	5	SF	
317+50	LT	R4-1	5	SF	
317+50	RT	R4-1	5	SF	
325+00	LT	R4-1	5	SF	
325+00	RT	R4-1	5	SF	
332+50	LT	R4-1	5	SF	
332+50	RT	R4-1	5	SF	
340+00	LT	R4-1	5	SF	
340+00	RT	R4-1	5	SF	
347+50	LT	R4-1	5	SF	
347+50	RT	R4-1	5	SF	
355+00	LT	R4-1	5	SF	
355+00	RT	R4-1	5	SF	
362+50	LT	R4-1	5	SF	
362+50	RT	R4-1	5	SF	
370+00	LT	R4-1	5	SF	
370+00	RT	R4-1	5	SF	
377+50	LT	R4-1	5	SF	
377+50	RT	R4-1	5	SF	
385+00	LT	R4-1	5	SF	
385+00	RT	R4-1	5	SF	
392+50	LT	R4-1	5	SF	
392+50	RT	R4-1	5	SF	

107377301000

619-D1001 Standard Roadside Construction Signs (Less than 10 Sq. Ft.)					
Station	Location	Description	Quantity	Unit	Remarks
400+00	LT	R4-1	5	SF	
400+00	RT	R4-1	5	SF	
407+50	LT	R4-1	5	SF	
407+50	RT	R4-1	5	SF	
415+00	LT	R4-1	5	SF	
415+00	RT	R4-1	5	SF	
422+50	LT	R4-1	5	SF	
422+50	RT	R4-1	5	SF	
430+00	LT	R4-1	5	SF	
430+00	RT	R4-1	5	SF	
437+50	LT	R4-1	5	SF	
437+50	RT	R4-1	5	SF	
445+00	LT	R4-1	5	SF	
445+00	RT	R4-1	5	SF	
452+50	LT	R4-1	5	SF	
452+50	RT	R4-1	5	SF	
460+00	LT	R4-1	5	SF	
460+00	RT	R4-1	5	SF	
467+50	LT	R4-1	5	SF	
467+50	RT	R4-1	5	SF	
475+00	LT	R4-1	5	SF	
475+00	RT	R4-1	5	SF	
482+50	LT	R4-1	5	SF	

107377301000

619-D1001 Standard Roadside Construction Signs (Less than 10 Sq. Ft.)					
Station	Location	Description	Quantity	Unit	Remarks
482+50	RT	R4-1	5	SF	
490+00	LT	R4-1	5	SF	
490+00	RT	R4-1	5	SF	
497+50	LT	R4-1	5	SF	
497+50	RT	R4-1	5	SF	
505+00	LT	R4-1	5	SF	
505+00	RT	R4-1	5	SF	
512+50	LT	R4-1	5	SF	
512+50	RT	R4-1	5	SF	
520+00	LT	R4-1	5	SF	
520+00	RT	R4-1	5	SF	
525+16	LT	G20-2	8	SF	500' East of EOP
525+16	RT	W14-3	5.56	SF	
TOTAL			702.12	SF	

107377301000

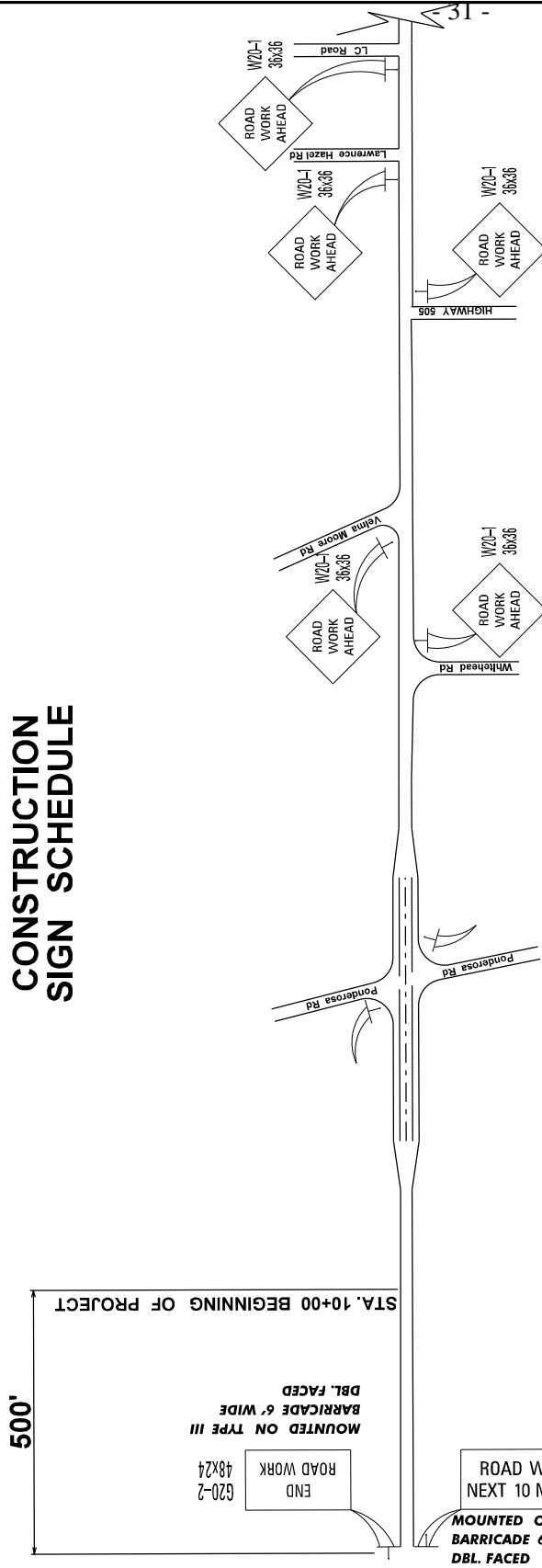
619-D1001 Standard Roadside Construction Signs (Less than 10 Sq. Ft.)					
Station	Location	Description	Quantity	Unit	Remarks
Location	Station	Quantity	Unit	Description	
LT	500' West of BOP	6	LF	Mounted on G20-1	
RT	500' West of BOP	6	LF	Mounted on G20-2	
RT	500' East of BOP	6	LF	Mounted on G20-1	
LT	500' East of BOP	6	LF	Mounted on G20-2	
		TOTAL	24	LF	

TRENCH WIDENING

Location	Beginning Station	Ending Station	LF	Width	Total (SF)	Total (Tons)	Remarks
LEFT	10+00.00	40+00.00	3000.00	2	6000.000	225.000	
RIGHT	10+00.00	40+00.00	3000.00	2	6000.000	225.000	
					TOTAL	450.000	

NEWTON COUNTY
 306367/301000
 HIGHWAY 80

CONSTRUCTION
 SIGN SCHEDULE



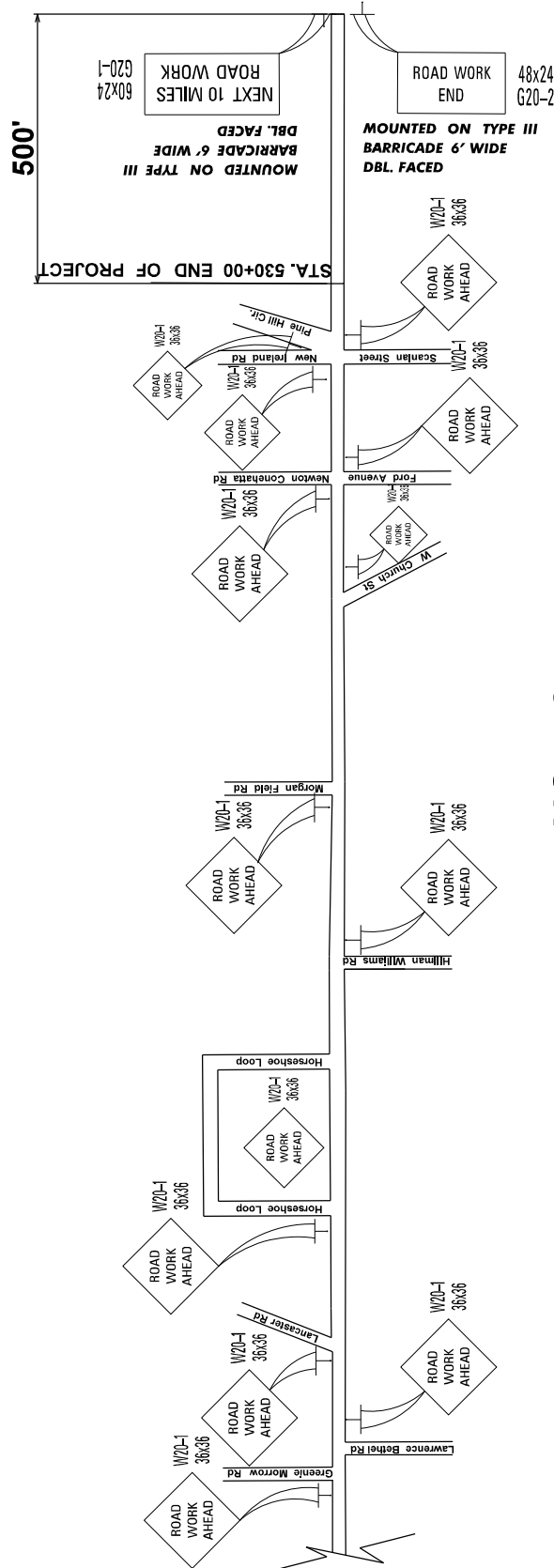
SIGN LEGEND		
NUMBER	DESCRIPTION	
①	ROAD WORK NEXT 10 MILE G20-1 60x24	④ DO NOT PASS R4-1 24x30
②	END ROAD WORK G20-2 48x24	⑤ PASS WITH CARE R4-2 48x60
③	ROAD WORK AHEAD W20-1 36x48	⑥ NO PASSING ZONE W14-3 36x48x48

TRAFFIC CONTROL SIGNS REQUIRED

- 2 - G20-1 "ROAD WORK NEXT 10 MILES"
- 2 - G20-2 "END ROAD WORK"
- 22 - W20-1 "ROAD WORK AHEAD"
- 135 - R4-1 "DO NOT PASS"
- 0 - R4-2 "PASS WITH CARE"
- 2 - W14-3 "NO PASSING ZONE"
- 2 - G20-2 "BARRICADES"
- 2 - G20-1 "BARRICADES"

**NEWTON COUNTY
306367/301000
HIGHWAY 80**

**CONSTRUCTION
SIGN SCHEDULE**



NOTES:

ONE (1) W20-1 "ROAD WORK AHEAD SIGN IS REQUIRED AT EACH LOCAL ROAD, STREET, OR HIGHWAY ENTERING THE PROJECT.

G20-1 AND G20-2 SIGNS MOUNTED ON TYPE III DOUBLE FACED BARRICADE.

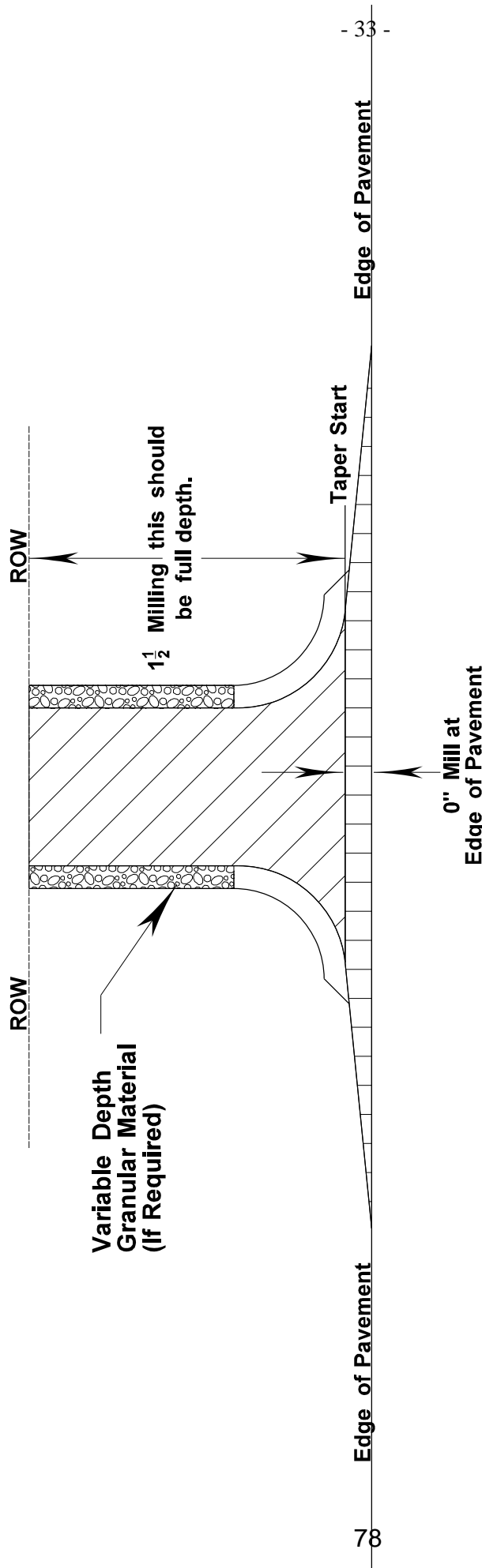
R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE", AND W14-3 "NO PASSING ZONE" signs are required in accordance with Subsection 618.03.3 and as specified in the MUTCD. If No Passing zones are 1000 ft or more, install additional "DO NOT PASS" signs on maximum spacing of 750 ft.

FIELD CONDITIONS MAY REQUIRE SOME SIGNS ON THIS DETAIL TO BE ADJUSTED.

THE ABOVE SHOWN ITEMS WILL BE PAID UNDER THE APPROPRIATE PAY ITEMS.

NEWTON COUNTY -306367/301000
HIGHWAY 80

MILLING AND PAVING DETAIL
COUNTY ROADS



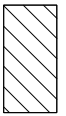
- 33 -

Notice to Bidder No.901 -- Cont'd

NOTES:

- Place 2" 12.5mm, MT, Mixture to tie to mainline overlay.
- Start at End of Maintenance of County Road and Mill 1 1/2" to 0" at Edge of Pavement to tie into mainline overlay.

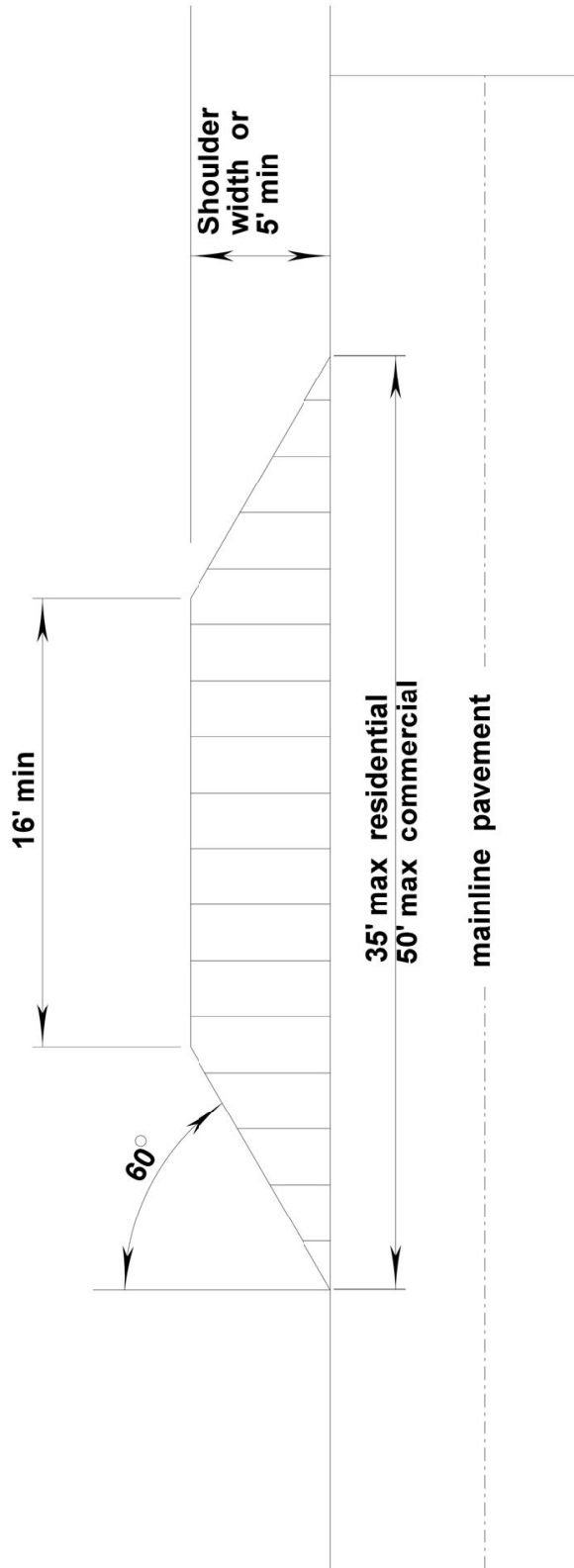
Milling & Paving area -



Locations

Ponderosa Rd (North)	Ruby Lancaster Rd
Ponderosa Rd (South)	Horseshoe Loop Rd
Whitehead Rd	Hillman Williams Rd
Velma Moore Rd	Morgan Field Rd
HWY 505	W Church ST
Lawrence Hazel Rd	Newton Conehatta Rd
LC Rd	Ford Avenue
Greenie Morrow Rd	New Ireland Rd
Lawrence Bethel Rd	Scanlan St
Lancaster Rd	

NEWTON COUNTY
306367/301000
HIGHWAY 80
DRIVEWAY PAD DETAIL

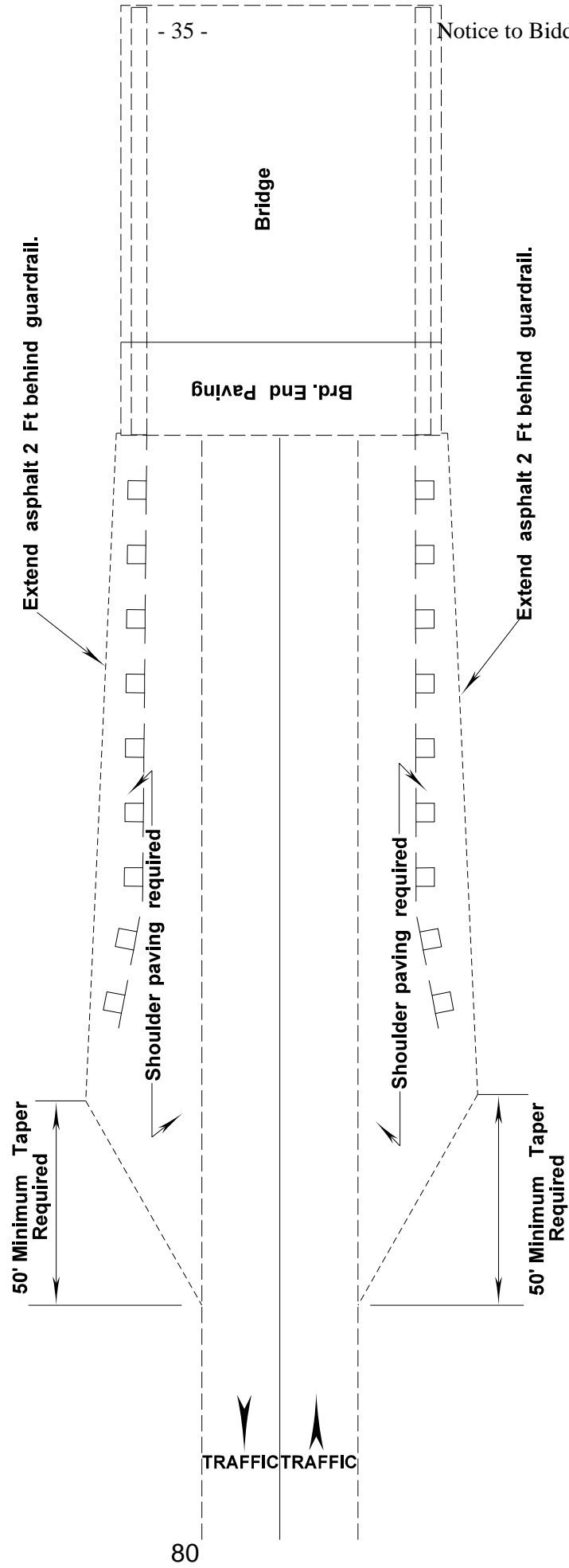


Note:

The asphalt on the existing driveway/ramp pads are to remain in their current size and location and milled/overlaid. If, in the opinion of the engineer, a pad should be modified or replaced, payment will be made for the work using the appropriate pay items. Granular material and/or stabilizer aggregate should be placed around the pads as required.

NEWTON COUNTY
306367/301000
HIGHWAY 80

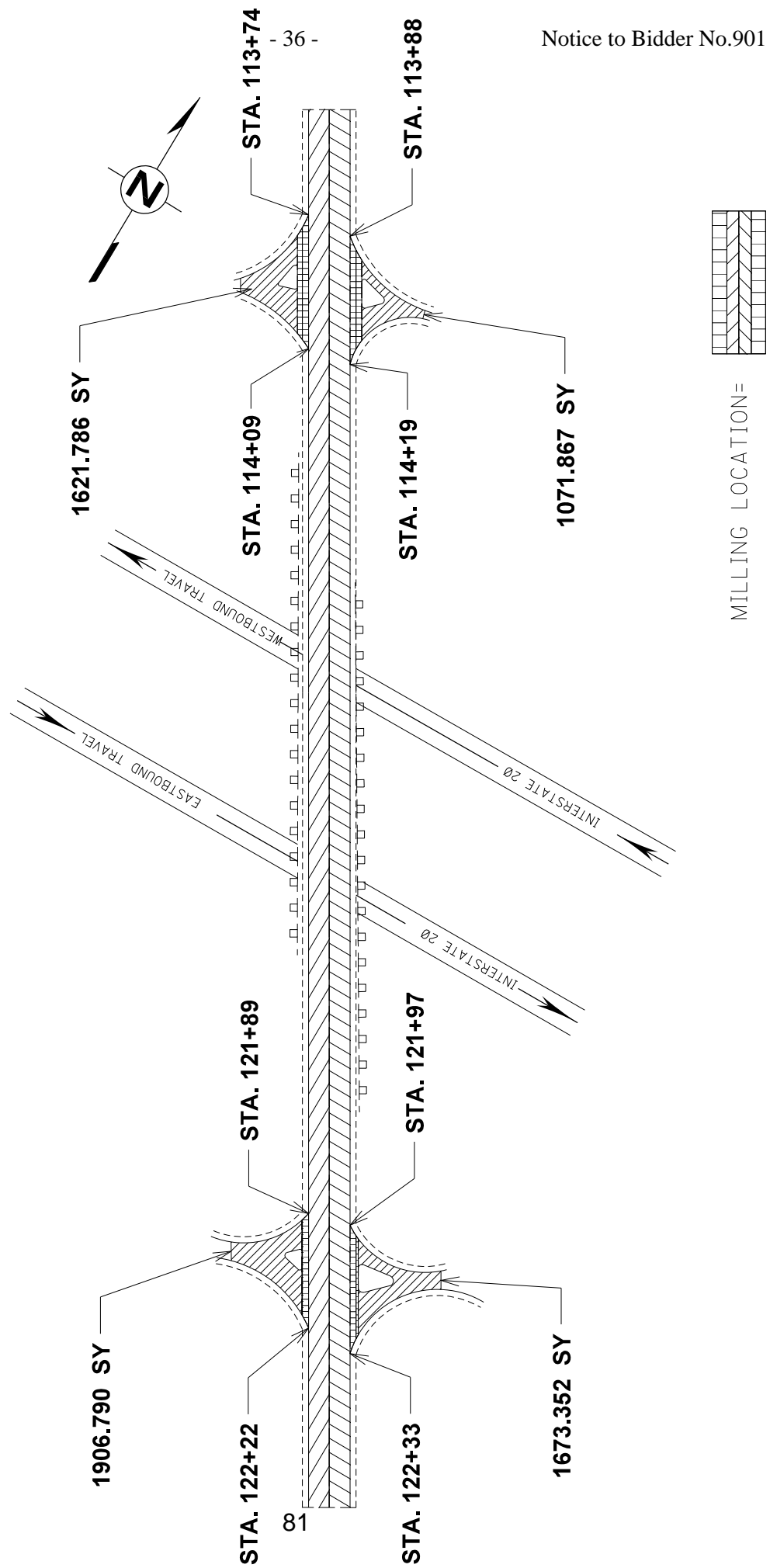
TYPICAL DETAIL OF ADDITIONAL SHOULDER PAVING
REQUIRED AT GUARDRAIL LOCATIONS



*Asphalt Thickness
See scope of work for additional details

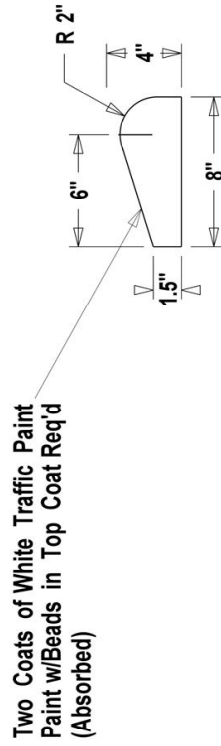
**NEWTON COUNTY
306365/301000
HIGHWAY 80
MILLING DETAIL**

Notice to Bidder No.901 -- Cont'd

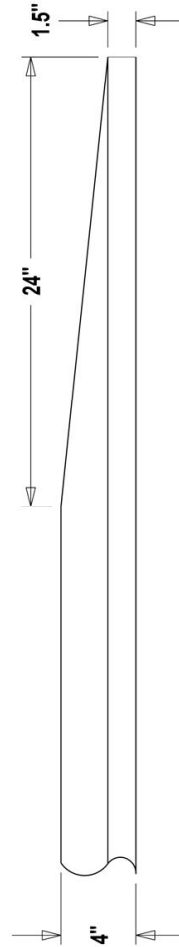


NEWTON COUNTY
306367/301000
HIGHWAY 80

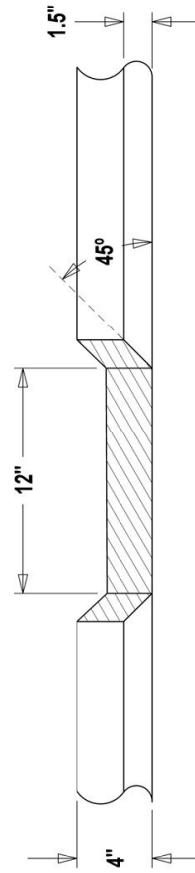
DETAIL OF BITUMINOUS CURB



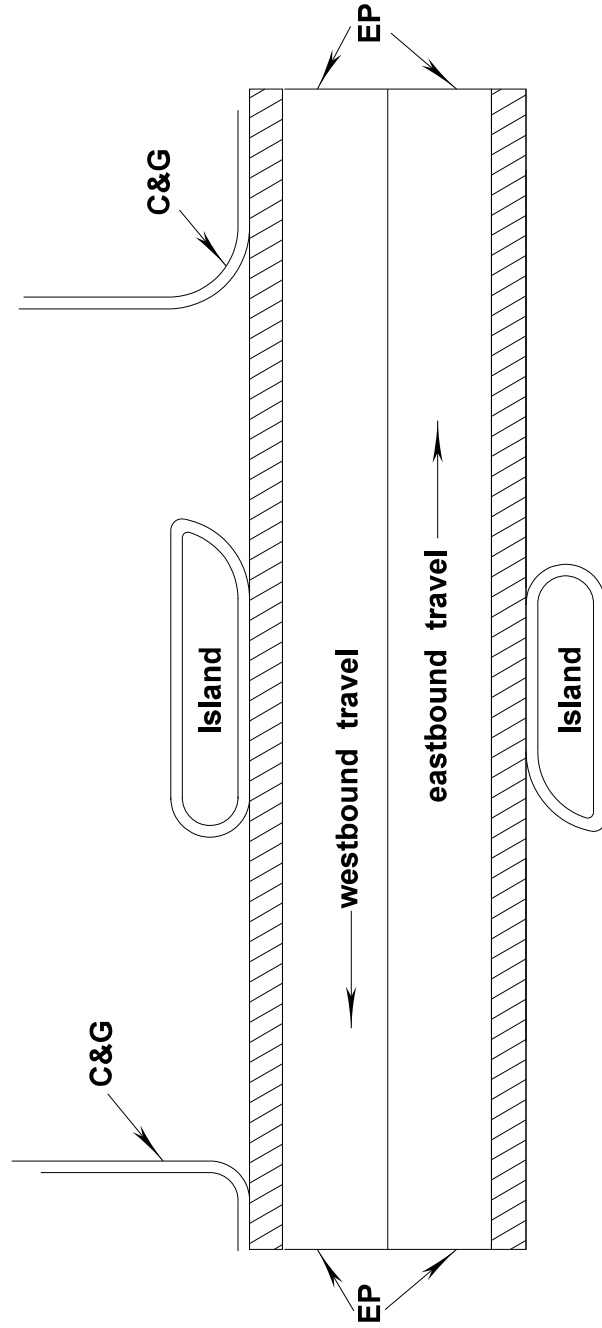
Detail of Curb Terminus



Detail of Spill Thru



NEWTON COUNTY
306367/301000
HIGHWAY 80
AT THE LAWRENCE STORE IN LAWRENCE
MILLING & OVERLAY AT ISLAND AREAS



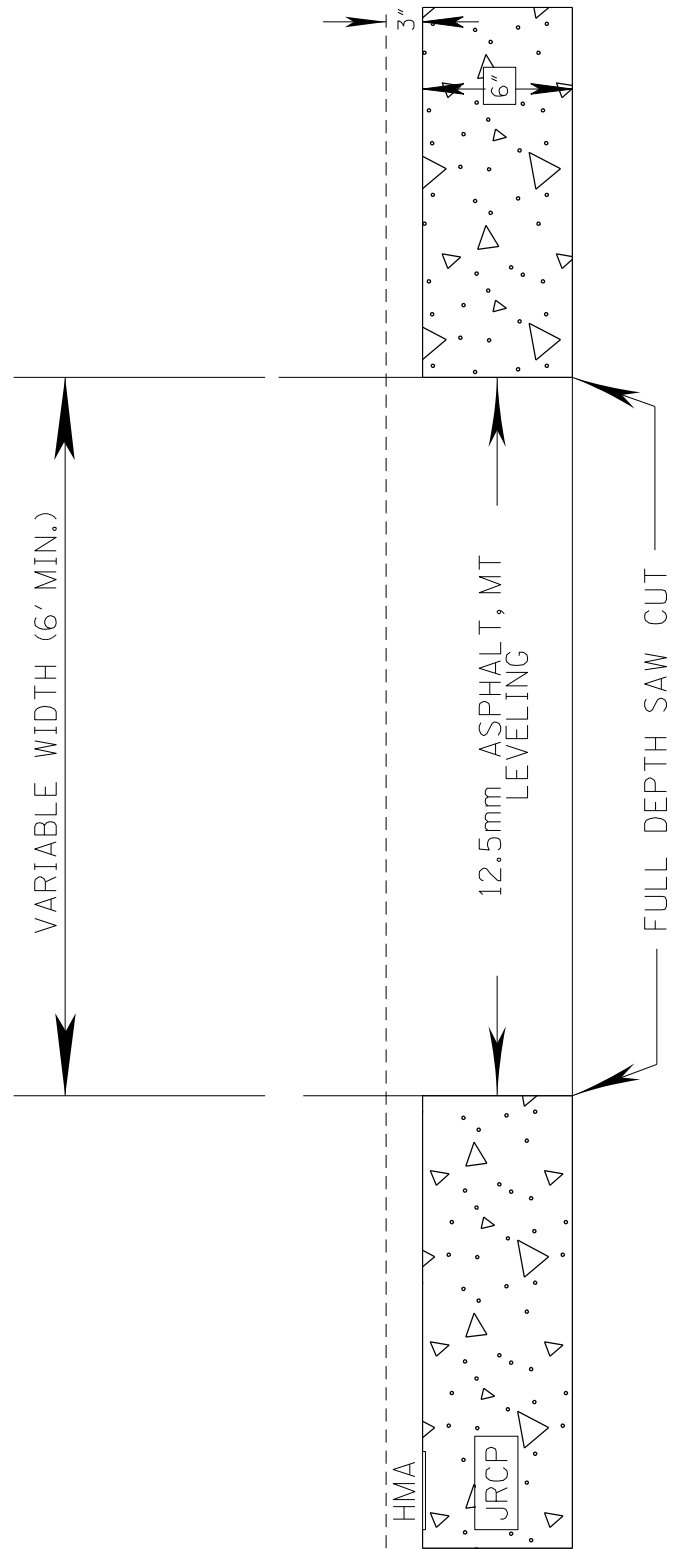
*Islands may vary in their locations.



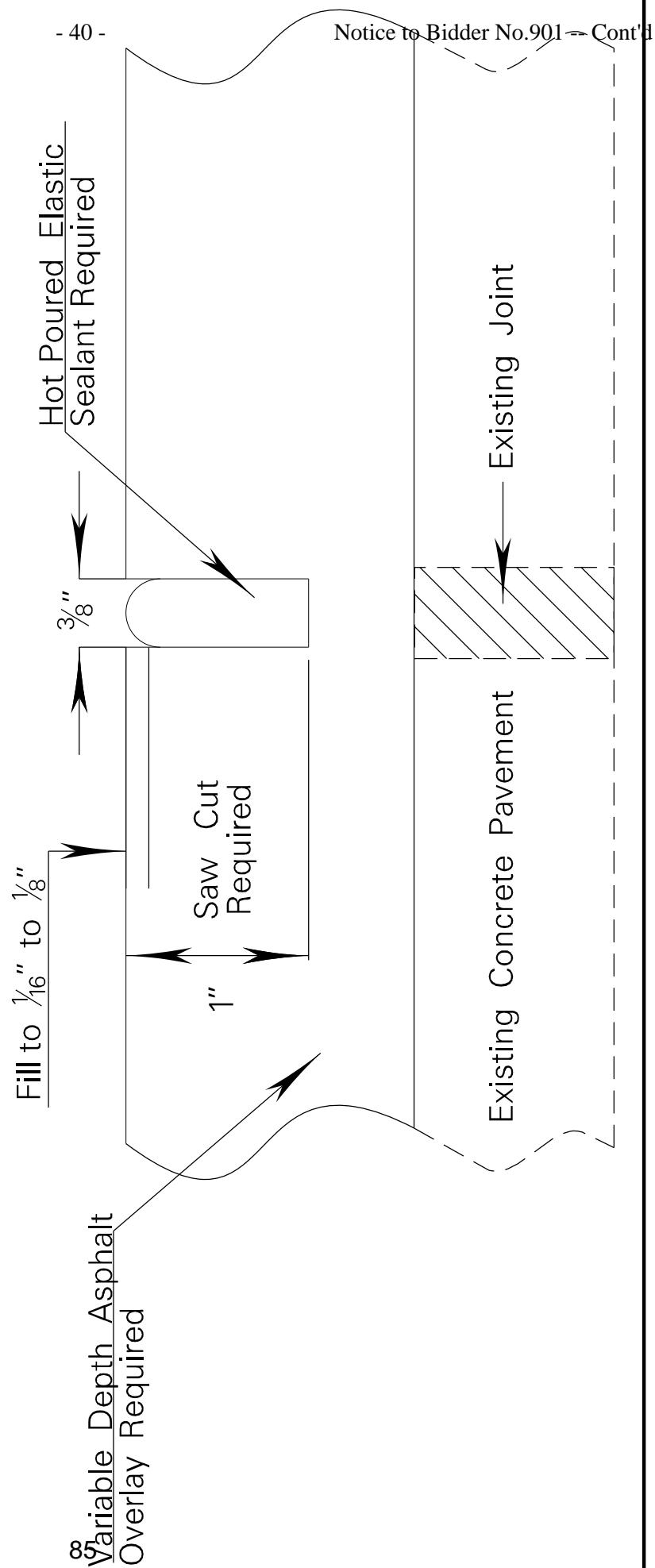
MILL 1.5" at C&G TO 0" at EP.
PLACE 2" HMA 12.5 MM, MIX TO TIE
TO MAINLINE OVERLAY (WIDTH
6' & VAR EP TO C&G)

MP-5080-51(023) 306367/301000
 NEWTON COUNTY

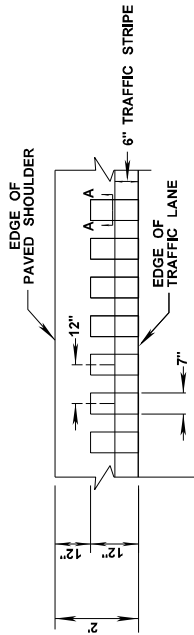
CONCRETE PAVEMENT REPAIR DETAIL



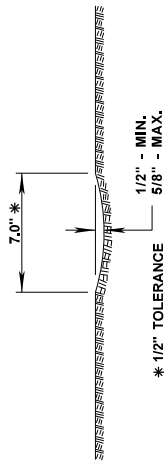
NEWTON COUNTY -306367/301000
HIGHWAY 80



NEWTON COUNTY -306367/301000
HIGHWAY 80
RUMBLE STRIP ON 2-LANE ROADWAY

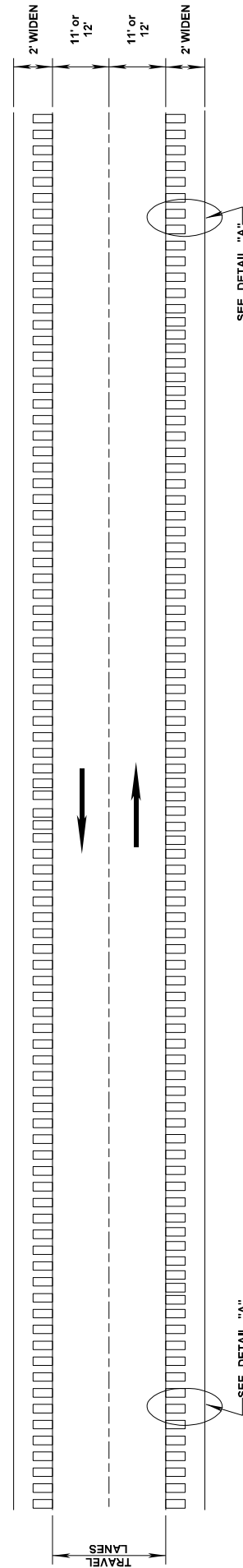


DETAIL "A"



SECTION "A-A"

- GENERAL NOTES
1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
 2. GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
 4. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.
 5. DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.

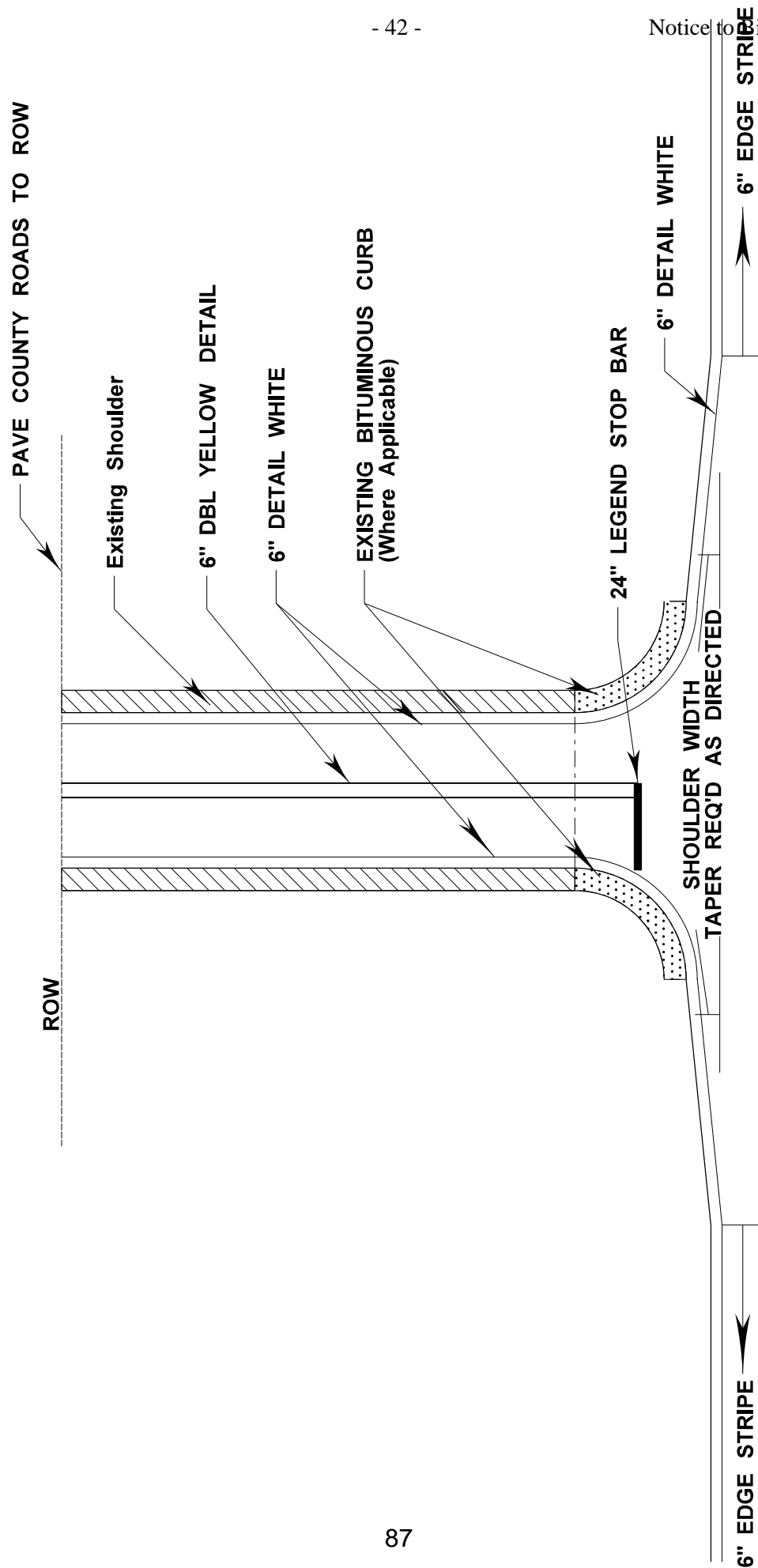


PLAN

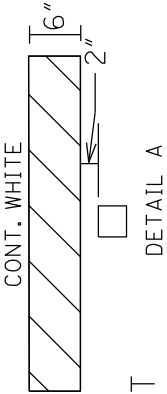
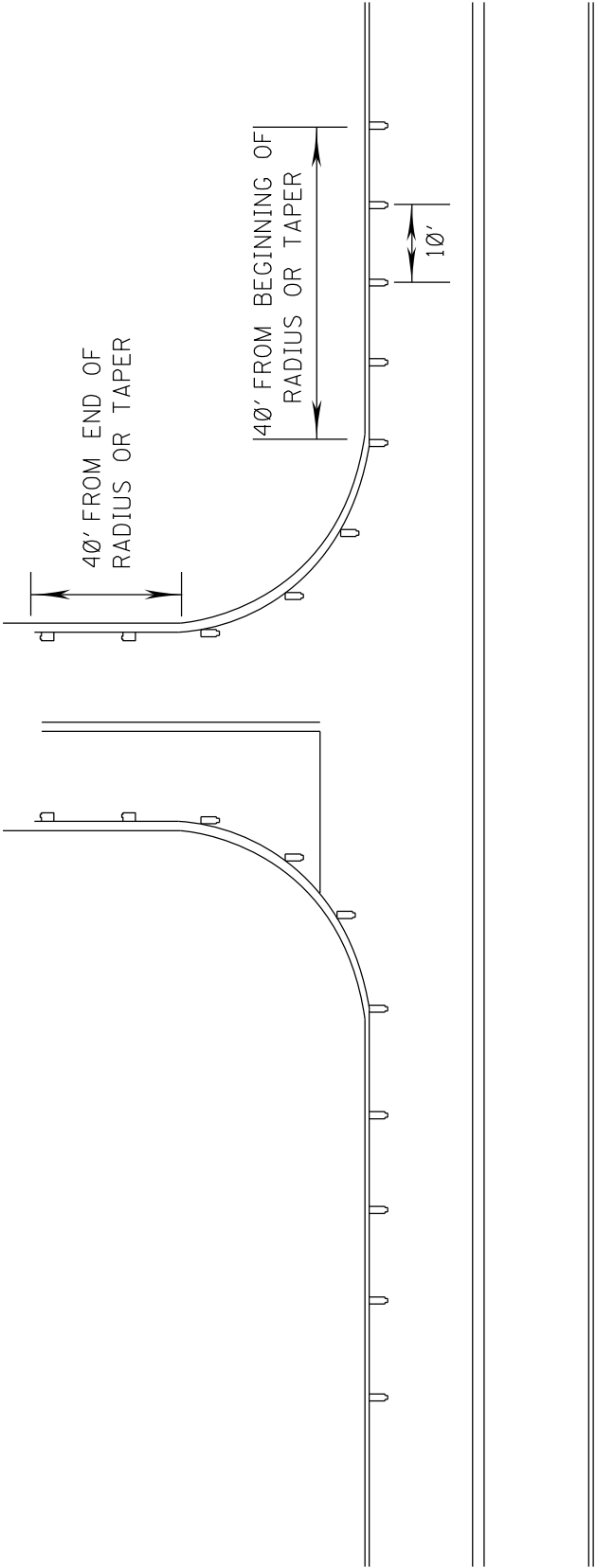
NOT TO SCALE

NEWTON COUNTY - 306367/301000
HIGHWAY 80

STRIPE DETAIL - COUNTY ROADS



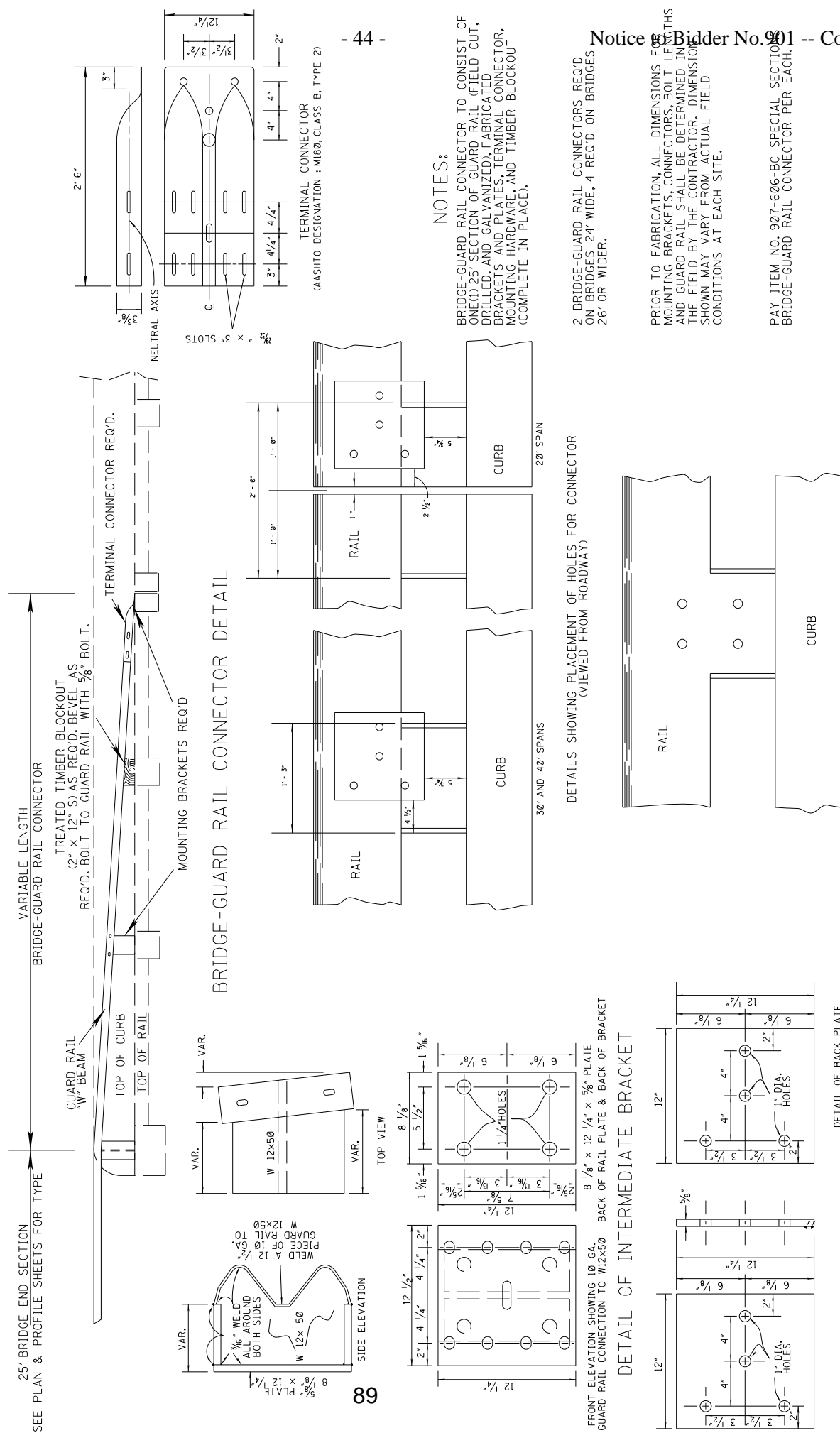
NEWTON COUNTY -306367/301000 HIGHWAY 80 TYPICAL FOR RAISED PAVEMENT MARKERS PLACED ON SIDE ROAD RADIUS



- NOTE 1. MARKERS SHALL BE PLACED EVERY 10 FEET.
- NOTE 2. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
- NOTE 3. MARKERS SHALL BE HIGH PERFORMANCE TWO WAY CLEAR.
- NOTE 4. FIVE (5) MARKERS SHALL BE PLACED ALONG MAINLINE EDGE STRIPE.
- NOTE 5. MARKERS FOR COUNTY ROADS SHALL CONTINUE DOWN THE EDGE STRIPE A DISTANCE OF 40 FEET.

**NEWTON COUNTY - 306367/301000
HIGHWAY 80**

SPECIAL DESIGN BRIDGE-GUARD RAIL CONNECTOR



DETAILS SHOWING PLACEMENT OF HOLES FOR INTERMEDIATE BRACKET
(VIEWED FROM ROADWAY)

Notice to Bidder No. 201 -- Cont'd

PRIOR TO FABRICATION, ALL DIMENSIONS FOR THE MOUNTING BRACKETS, CONNECTORS, BOLT LENGTHS AND GUARD RAIL SHALL BE DETERMINED IN THE FIELD BY THE CONTRACTOR. DIMENSIONS SHOWN MAY VARY FROM ACTUAL FIELD CONDITIONS AT EACH SITE.

PAY ITEM NO. 907-606-BC SPECIAL SECTION
BRIDGE-GUARD RAIL CONNECTOR PER EACH.

2 BRIDGE-GUARD RAIL CONNECTORS REQ'D
ON BRIDGES 24' WIDE, 4 REQ'D ON BRIDGES
26' OR WIDER.

NOTES:

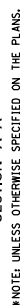
BRIDGE-GUARD RAIL CONNECTOR TO CONSIST OF ONE(1) 25' SECTION OF GUARD RAIL (FIELD CUT, DRILLED, AND GALVANIZED), FABRICATED BRACKETS AND PLATES, TERMINAL CONNECTOR, MOUNTING HARDWARE, AND TIMBER BLOCKOUT (COMPLETE IN PLACE).

TERMINAL CONNECTOR
(AASHTO DESIGNATION : M180, CLASS B, TYPE 2)

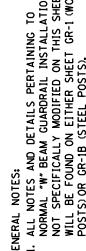
- 44 -

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**GUARDRAIL:
BRIDGE END SECTIONS
TYPE "A" & "C"**



PLAN OF BRIDGE END SECTION



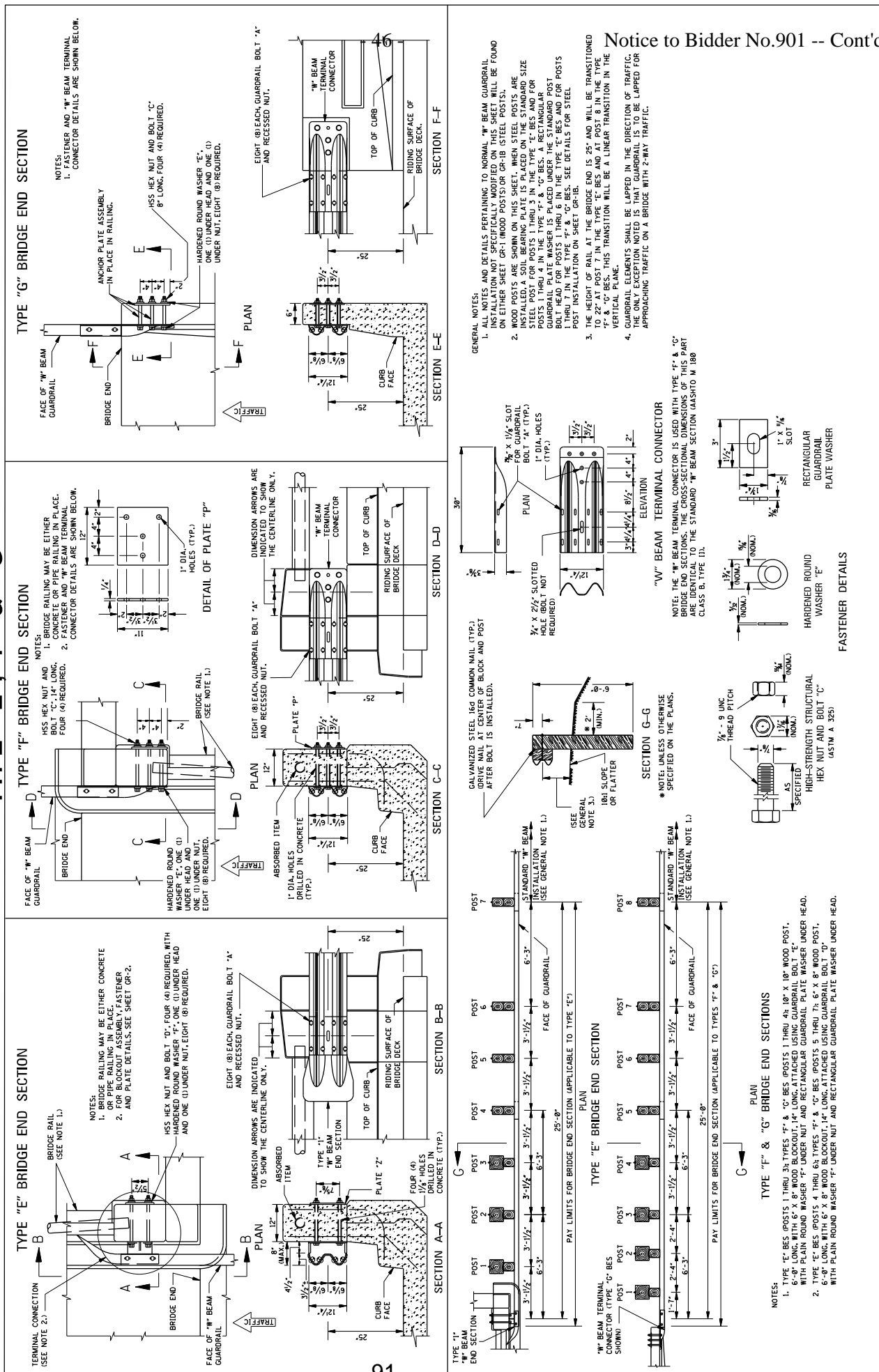
GENERAL NOTES:

- ALL NOTES AND DETAILS PERTAINING TO NORMAL "W" BEAM GUARDRAIL INSTALLATION NOT SPECIFICALLY MODIFIED ON THIS SHEET WILL BE FOUND ON EITHER SHEET GR-1 (WOOD POST/STUB GR-1B STEEL POSTS).
- TYPE "A"
2. WOOD POSTS ARE SHOWN ON THIS SHEET. WHEN STEEL POSTS ARE INSTALLED, A SOLID BEARING PLATE IS PLACED ON TOP OF THE POST AND THE GUARDRAIL IS PLACED ON THE RECTANGULAR GUARDRAIL PLATE WASHER IS PLACED UNDER THE STANDARD POST BOLT HEAD FOR POSTS 1 THRU 6. SEE DETAILS FOR STEEL POST INSTALLATIONS ON SHEET GR-1B.
- TYPE "E" BRIDGE END SECTION. SEE SHEET GR-2A.
1. THE HEIGHT OF RAIL AT THE BRIDGE END IS 20% AND WILL BE TRANSITIONED TO 22" AT POST 7. THIS TRANSITION WILL BE A LINEAR TRANSITION IN THE VERTICAL PLANE.
 2. ALL GUARDRAIL ELEMENTS SHALL BE LAPPED IN THE DIRECTION OF TRAFFIC.
 3. THE APPLICATION OF THIS STANDARD IS FOR ONE-WAY TRAFFIC ONLY.
 4. POINT "G", WHICH IS AT THE CENTERLINE BETWEEN THE ANCHOR BOLTS, WILL BE CENTERED OVER THE CENTERLINE OF THE BRIDGE DECK. THE MINIMUM DISTANCE FROM THE ANCHOR BOLT TO THE JOINT WILL BE A MINIMUM OF 3' FROM THE JOINT.

GUARDRAIL CONNECTION DETAILS

NOTE: UNLESS OTHERWISE SPECIFIED, THE BLOCKOUT ASSEMBLY, FASTENER AND PLATE DETAILS SHOWN ABOVE ARE ALSO APPLICABLE TO THE TYPE "E" BRIDGE END SECTION. SEE GENERAL NOTE 3.

**GUARDRAIL:
BRIDGE END SECTIONS
TYPE "E", "F" & "G"**



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 902

CODE: (SP)

DATE: 03/22/2018

SUBJECT: Temporary Construction Signs

PROJECT: MP-5080-51(023) / 306367301 – Newton County

Bidders are hereby advised of the following regarding the Temporary Construction Signs required:

Should the Bidders elect to install Temporary Construction Signs by first driving short u-channel sections and then bolting the longer, correct height u-channel sections to them, the Bidders are advised that these short sections shall be a minimum of five (5) feet from the ground level when driven and the splice must consist of a minimum of eighteen (18) inches of overlap with a total of four (4) bolts. Bidders are also advised that it is mandatory that these short sections be removed at the completion of the project.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904- NOTICE TO BIDDERS NO. 903

CODE: (SP)

DATE: 05/18/2018

SUBJECT: Smoothness Tolerances

Bidders are hereby advised that the smoothness tolerances for this project shall meet the requirements of a Category C project according to Subsection 403.03.2.1. Bidders are responsible for the collection of a preliminary smoothness profile prior to any work being performed.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-102-2

CODE: (IS)

DATE: 11/22/2017

SUBJECT: Bidding Requirements and Conditions

Section 102, Bidding Requirements and Conditions, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-102.01--Prequalification of Bidders. Delete the last sentence of the third paragraph of Subsection 102.01 on page 13, and substitute the following.

The Bidder's Certificate of Responsibility number must be on file with the Department's Contract Administration Division prior to request for permission to bid.

907-102.02--Contents of Proposal Forms. Delete the fourth paragraph in Subsection 102.02 on page 13, and substitute the following.

Prospective bidders must complete an online request for permission to be eligible to bid a project. Upon approval, the bidder will be authorized to submit a bid electronically using Bid Express at <http://bidx.com>.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-103-2

CODE: (SP)

DATE: 06/22/2017

SUBJECT: Award and Execution of Contract

Section 103, Award and Execution of Contract, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-103.01--Consideration of Proposal. Delete the second and third paragraphs of Subsection 103.01 on page 19, and substitute the following.

907-103.01.1--For Projects Constructed Without Federal Funds. Resident Contractors actually domiciled in Mississippi are to be granted preference over nonresidents in awarding of Contracts financed 100% with State funds.

In consideration of proposals that are equal to or in excess of \$50,000 and financed 100% with State funds, a nonresident bidder domiciled in a state having laws granting preference to local Contractors will be considered for such contracts on the same basis as the nonresident bidder's state awards contracts to Mississippi Contractors bidding under similar circumstances. When a nonresident Contractor submits a bid equal to or in excess of \$50,000 on a contract financed 100% with State funds, a copy of the current laws from the state of domicile and an explanation thereof pertaining to treatment of nonresident Contractors shall be attached. If no preferential treatment is provided for Contractors in the state of domicile and contracts are awarded to the lowest responsible bidder, a statement to this effect shall be attached. Should the attachment not accompany the bid when submitted, the Contractor shall have 10 days following the opening of the bids to furnish the required information to the Contract Administration Director for attachment to the bid. Failure to provide the attachment within 10 days will result in the nonresident Contractor's bid being rejected and not considered for award. As used herein, the term "resident Contractor" includes a nonresident person, firm or corporation that has been qualified to do business in this State and has maintained a permanent full-time office in the State of Mississippi for two years prior to the submission of the bid, and the subsidiaries and affiliates of such a person, firm or corporation.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-619-6

CODE: (SP)

DATE: 03/21/2018

SUBJECT: Temporary Portable Rumble Strips

Section 619, Traffic Control for Construction Zones, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-619.02--Materials. After Subsection 619.02.15 on page 472, add the following.

907-619.02.16--Temporary Portable Rumble Strips. Temporary portable rumble strips shall be RoadQuake manufactured by PSS and meet the following requirements:

- capable of being installed without adhesives or bolts,
- have a minimum weight of 100 pounds,
- have a minimum overall length of 11 feet,
- have a minimum width of 12 inches, and
- have a maximum height of 3/4 inch.

Temporary portable rumble strips shall be installed in accordance with the attached details, or as directed by the Engineer.

907-619.03--Construction Requirements. After Subsection 619.03.11 on page 476, add the following.

907-619.03.16--Temporary Portable Rumble Strips. Temporary portable rumble strips shall be placed at locations shown on the traffic control plans, attached drawing, or as directed by the Engineer. The rumble strips shall be removed when lane closures are removed, relocated when lane closures are relocated, or as directed by the Engineer.

Prior to placement of the rumble strips, the roadway shall be cleaned to be free of dust, sand, and other materials that may cause slippage. The minimum roadway temperature at the time of installation shall be in accordance with manufacturer recommendations.

A minimum of three (3) temporary portable rumble strips shall be arranged in an array. The spacing of temporary portable rumble strips in each array shall be on 15-foot centers. One array of three (3) strips shall be used in each lane. The rumble strips shall be regularly monitored and maintained to ensure they stay in place under traffic.

907-619.04--Method of Measurement. At the end of Subsection 619.04 on page 478, add the following.

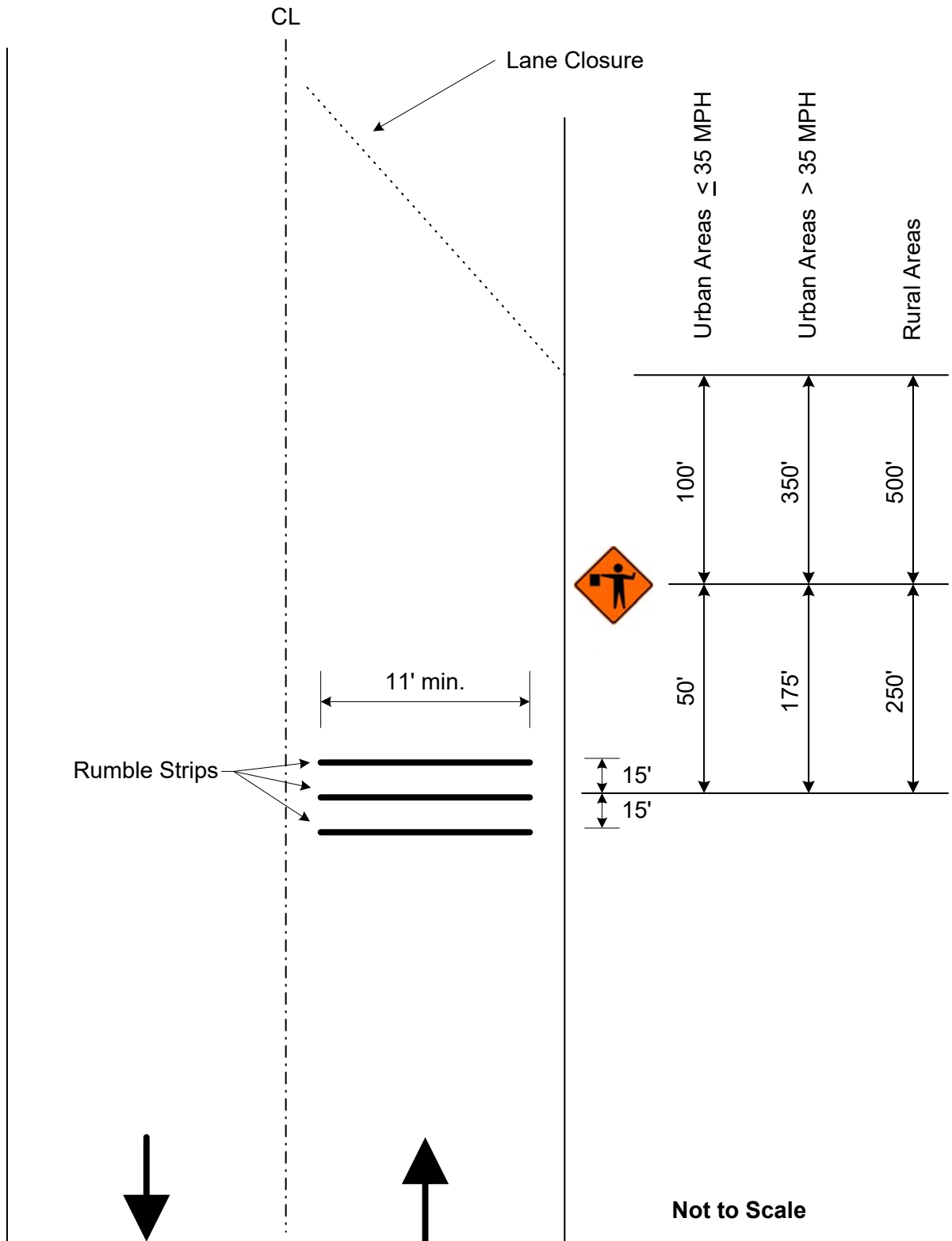
Temporary Portable Rumble Strips will be measured for payment by the linear foot only when a pay item for temporary portable rumble strips is included in the contract. Otherwise, temporary portable rumble strips will be included in the cost of pay item 618-A, Maintenance of Traffic. The quantity of temporary portable rumble strips will be the length of rumble strips approved by the Engineer to be in-place on the project at any one time.

907-619.05--Basis of Payment. After the fifth paragraph of Subsection 619.05 on page 478, add the following.

Temporary Portable Rumble Strips measured as prescribed above, will be paid for at the contract unit price per linear foot, which price shall be full compensation for cleaning the roadway surface, installing the rumble strips, maintenance and repair of the strips, cleaning and resetting of the strips, removal and replacement, and for all labor, equipment, tools, and incidentals necessary to complete the work.

After the last pay item listed on page 480, add the following.

907-619-B: Temporary Portable Rumble Strips - per linear foot



Detail of Temporary Portable Rumble Strips

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-899-1

CODE: (SP)

DATE: 01/17/2017

SUBJECT: Railway-Highway Provisions

Section 907-899, Railway-Highway Provisions, is hereby added to and made part of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction as follows:

SECTION 907-899--RAILWAY-HIGHWAY PROVISIONS

907-899.01--Description. This special provision addresses the Contractor's involvement with railroad flagging, Contractor Safety Orientation, Contractor Background Investigation, Contractor Roadway Worker on Track Safety Program and Safety Action Plan, and any other requirements set forth by the Railroad and any attached Exhibits.

Prior to bidding, the Contractor shall read and comply with the requirements of the Railroad and any attached Exhibits. The Contractor shall contact the Railroad concerning insurance coverage requirements, Railroad flagging costs, Contractor Safety Orientation, Contractor Background Investigation, Contractor Roadway Worker on Track Safety Program and Safety Action Plan, and any other requirements set forth by the Railroad and any attached Exhibits. In case the railroad requires coverage over and above that required by the Standard Specifications, the railroad requirements shall be met.

If in the opinion of the RAILROAD, the presence of an authorized representative of the RAILROAD is required to supervise the same, the RAILROAD shall render bills to the Contractor for all expenses incurred by it for such supervision. This includes all labor costs for flagmen or cable locate supplied by the RAILROAD to protect RAILROAD operation, and for the full cost of furnishing, installation and later removal of any temporary supports for said tracks, as the RAILROAD's Chief Engineer's Office may deem necessary.

It will be the Contractor's responsibility to pay all bills associated with the Railroad requirements and any attached Exhibits.

A cable locate of RAILROAD owned facilities may be required to identify and protect Signal & Communication cables that have been installed to provide power, signal control, wayside communications. These cables are vital to a safe and reliable railway operation. The cable locate will be performed by a qualified RAILROAD employee.

Outside Contractors are prohibited from driving on, along, or across any track that does not have a RAILROAD installed crossing. They may utilize an existing public crossing. The practice of allowing rubber tired equipment to operate over track with no crossing has been banned.

The Contractor shall complete and process any required forms addressed by the Railroad or any attached Exhibits. The Contractor shall not commence or carry on any form of work on, under, above or within the designated distance from the Railroad track prior to getting approval from the Railroad.

907-899.02--Blank.

907-899.03--Construction Requirements. The Contractor shall read and comply with the requirements of the Railroad and any attached Exhibits.

907-899.04--Method of Measurement. Railway-highway provisions will be measured as a unit lump sum quantity. Measurement for payment will be in accordance with the following schedule:

- a) On the first estimate, twenty five percent (25%) of the amount bid for Railway Highway Provision will be paid.
- b) When twenty five percent (25%) of the original contract amount is earned from all direct pay items, fifty percent (50%) of the amount bid for Railway Highway Provision will be paid.
- c) When fifty percent (50%) of the original contract amount is earned from all direct pay items, one hundred percent (100%) of the amount bid for Railway Highway Provision will be paid.

907-899.05--Basis of Payment. Railway-highway provisions, measured as prescribed above, will be paid for at the contract lump sum price, which price shall be payment in full for all insurance coverage requirements, railroad flagging costs, Contractor safety orientation, Contractor background investigation, Contractor safety programs and plans, and any other requirements set forth by the Railroad and any attached Exhibits, and other incidentals necessary to complete the requirements of this work.

Payment will be made under:

907-899-A: Railway-Highway Provisions

- lump sum

SECTION 905 - PROPOSAL

Date _____

Mississippi Transportation Commission
Jackson, Mississippi

Sirs: The following proposal is made on behalf of _____
_____ of _____

for constructing the following designated project(s) within the time(s) hereinafter specified.

The plans are composed of drawings and blue prints on file in the offices of the Mississippi Department of Transportation, Jackson, Mississippi.

The Specifications are the current Standard Specifications of the Mississippi Department of Transportation approved by the Federal Highway Administration, except where superseded or amended by the plans, Special Provisions and Notice(s) to Bidders attached hereto and made a part thereof.

I (We) certify that I (we) possess a copy of said Standard and any Supplemental Specifications.

Evidence of my (our) authority to submit the Proposal is hereby furnished. The proposal is made without collusion on the part of any person, firm or corporation. I (We) certify that I (we) have carefully examined the Plans, the Specifications, including the Special Provisions and Notice(s) to Bidders, herein, and have personally examined the site of the work. On the basis of the Specifications, Special Provisions, Notice(s) to Bidders, and Plans, I (we) propose to furnish all necessary machinery, tools, apparatus and other means of construction and do all the work and furnish all the materials in the manner specified. I (We) understand that the quantities mentioned herein are approximate only and are subject to either increase or decrease, and hereby propose to perform any increased or decreased quantities of work at the unit prices bid, in accordance with the above.

I (We) acknowledge that this proposal will be found irregular and/or non-responsive unless a certified check, cashier's check, or Proposal Guaranty Bond in the amount as required in the Advertisement (or, by law) is submitted electronically with the proposal or is delivered to the Contract Administration Engineer prior to the bid opening time specified in the advertisement.

INSTRUCTION TO BIDDERS: Alternate and Optional Items on Bid Schedule.

1. Two or more items entered opposite a single unit quantity WITHOUT DEFINITE DESIGNATION AS "ALTERNATE ITEMS" are considered as "OPTIONAL ITEMS". Bidders may or may not indicate on bids the Optional Item proposed to be furnished or performed WITHOUT PREJUDICE IN REGARD TO IRREGULARITY OF BIDS.
2. Items classified on the bid schedule as "ALTERNATE ITEMS" and/or "ALTERNATE TYPES OF CONSTRUCTION" must be preselected and indicated on bids. However, "Alternate Types of Construction" may include Optional Items to be treated as set out in Paragraph 1, above.
3. Optional items not preselected and indicated on the bid schedule MUST be designated in accordance with Subsection 102.06 prior to or at the time of execution of the contract.
4. Optional and Alternate items designated must be used throughout the project.

I (We) further propose to perform all "force account or extra work" that may be required of me (us) on the basis provided in the Specifications and to give such work my (our) personal attention in order to see that it is economically performed.

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) shall submit electronically with our proposal or deliver prior to the bid opening time a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

President

Address

Secretary

Address

Treasurer

Address

The following is my (our) itemized proposal.

Mill & Overlay approximately 10 miles of US 80 from Scott County Line to Scanlan Street, known as State Project No. MP-5080-51(023) / 306367301 in Newton County.

Line no.	Item Code	Adj Code	Quantity	Units	Description[Fixed Unit Price]
Roadway Items					
0010	202-B009		1,618	Square Yard	Removal of Asphalt Pavement, Failed Areas
0020	202-B014		292	Linear Feet	Removal of Bituminous Curb
0030	202-B069		4,946	Square Yard	Removal of Concrete Pavement w/ Variable Depth Overlay
0040	202-B158		1,900	Linear Feet	Removal of Guard Rail, Including Rails, Posts and Terminal Ends
0050	202-B240		1,324	Linear Feet	Removal of Traffic Stripe
0060	203-G002	(E)	1,094	Cubic Yard	Excess Excavation, LVM, AH
0070	305-B002	(GY)	50	Cubic Yard	Size I Stabilizer Aggregate, Coarse
0080	403-A002	(BA1)	25,346	Ton	12.5-mm, MT, Asphalt Pavement
0090	403-B002	(BA1)	4,430	Ton	12.5-mm, MT, Asphalt Pavement, Leveling
0100	403-C002	(BA1)	450	Ton	19-mm, MT, Asphalt Pavement, Trench Widening
0110	406-D001		187,641	Square Yard	Fine Milling of Bituminous Pavement, All Depths
0120	407-A001	(A2)	26,898	Gallon	Asphalt for Tack Coat
0130	413-E001		29,062	Linear Feet	Sawing and Sealing Transverse Joints in Asphalt Pavement
0140	423-A001		9	Mile	Rumble Strips, Ground In
0150	501-E001		92	Linear Feet	Expansion Joints, Without Dowels
0160	503-C010		8,819	Linear Feet	Saw Cut, Full Depth
0170	606-B003		700	Linear Feet	Guard Rail, Class A, Type 1, 'W' Beam, Metal Post
0180	606-B011		450	Linear Feet	Guard Rail, Class A, Type 1, Thrie Beam, Metal Post
0190	606-B013		25	Linear Feet	Guard Rail, Class A, Type 1, Thrie Beam, Transition Section
0200	606-D005		4	Each	Guard Rail, Bridge End Section, Type A
0210	606-D009		2	Each	Guard Rail, Bridge End Section, Type C
0220	606-D015		2	Each	Guard Rail, Bridge End Section, Type F
0230	606-E007		12	Each	Guard Rail, Terminal End Section, Non-Flared
0240	606-G002		6	Each	Special Sections, Guard Rail Bridge End Connector
0250	609-E001		283	Linear Feet	Bituminous Curb
0260	618-A001		1	Lump Sum	Maintenance of Traffic
0270	619-A1001		57	Mile	Temporary Traffic Stripe, Continuous White
0280	619-A2001		27	Mile	Temporary Traffic Stripe, Continuous Yellow
0290	619-A4002		21	Mile	Temporary Traffic Stripe, Skip Yellow
0300	619-A5001		83,700	Linear Feet	Temporary Traffic Stripe, Detail
0310	619-A6002		2,780	Linear Feet	Temporary Traffic Stripe, Legend
0320	619-D1001		716	Square Feet	Standard Roadside Construction Signs, Less than 10 Square Feet
0330	619-D2001		336	Square Feet	Standard Roadside Construction Signs, 10 Square Feet or More
0340	619-G4001		24	Linear Feet	Barricades, Type III, Double Faced

Line no.	Item Code	Adj Code	Quantity	Units	Description[Fixed Unit Price]
0350	620-A001		1	Lump Sum	Mobilization
0360	626-C002		19	Mile	6" Thermoplastic Double Drop Edge Stripe, Continuous White
0370	626-D001		7	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip Yellow
0380	626-E001		9	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow
0390	626-G004		12,400	Linear Feet	Thermoplastic Double Drop Detail Stripe, White
0400	626-G005		15,500	Linear Feet	Thermoplastic Double Drop Detail Stripe, Yellow
0410	626-H001		538	Square Feet	Thermoplastic Double Drop Legend, White
0420	626-H002		2,780	Linear Feet	Thermoplastic Double Drop Legend, White
0430	627-J001		574	Each	Two-Way Clear Reflective High Performance Raised Markers
0440	627-K001		1,158	Each	Red-Clear Reflective High Performance Raised Markers
0450	627-L001		83	Each	Two-Way Yellow Reflective High Performance Raised Markers
0460	630-F006		62	Each	Delineators, Guard Rail, White
0470	630-G005		8	Each	Type 3 Object Markers, OM-3R or OM-3L, Post Mounted
0480	815-A007	(S)	99	Ton	Loose Riprap, Size 300
0490	815-E001	(S)	135	Square Yard	Geotextile under Riprap
0500	907-619-B001		66	Linear Feet	Temporary Portable Rumble Strips
0510	907-899-A001		1	Lump Sum	Railway-Highway Provisions
ALTERNATE GROUP AA NUMBER 1					
0520	304-F001	(GT)	3,867	Ton	3/4" and Down Crushed Stone Base
ALTERNATE GROUP AA NUMBER 2					
0530	304-F002	(GT)	3,867	Ton	Size 610 Crushed Stone Base
ALTERNATE GROUP AA NUMBER 3					
0540	304-F003	(GT)	3,867	Ton	Size 825B Crushed Stone Base

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

CONDITIONS FOR COMBINATION BID

If a bidder elects to submit a combined bid for two or more of the contracts listed for this month's letting, the bidder must complete and execute these sheets of the proposal in each of the individual proposals to constitute a combination bid. In addition to this requirement, each individual contract shall be completed, executed and submitted in the usual specified manner.

Failure to execute this Combination Bid Proposal in each of the contracts combined will be just cause for each proposal to be received and evaluated as a separate bid.

It is understood that the Mississippi Transportation Commission not only reserves the right to reject any and all proposals, but also the right to award contracts upon the basis of lowest separate bids or combination bids most advantageous to the State.

It is further understood and agreed that the Combination Bid Proposal is for comparison of bids only and that each contract shall operate in every respect as a separate contract in accordance with its proposal and contract documents.

I (We) agree to complete each contract on or before its specified completion date.

COMBINATION BID PROPOSAL

This proposal is tendered as one part of a Combination Bid Proposal utilizing option ___* of Subsection 102.11 on the following contracts:

* Option to be shown as either (a), (b), or (c).

	<u>Project No.</u>	<u>County</u>	<u>Project No.</u>	<u>County</u>
1.	_____	_____	6.	_____
2.	_____	_____	7.	_____
3.	_____	_____	8.	_____
4.	_____	_____	9.	_____
5.	_____	_____	10.	_____

(a) If Combination A has been selected, your Combination Bid is complete.

(b) If Combination B has been selected, then complete the following page.

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
1. _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____
2. _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____
3. _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____
4. _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____
5. _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____
6. _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____
7. _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____
8. _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
9. _____ _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____
10. _____ _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____ _____ _____	_____

(c) If Combination C has been selected, then initial and complete ONE of the following.

- _____ I (We) desire to be awarded work not to exceed a total monetary value of \$ _____.
- _____ I (We) desire to be awarded work not to exceed _____ number of contracts.

TO: EXECUTIVE DIRECTOR, MISSISSIPPI DEPARTMENT OF TRANSPORTATION
JACKSON, MISSISSIPPI

CERTIFICATE

If awarded this contract, I (we) contemplate that portions of the contract will be sublet. I (we) certify that those subcontracts which are equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

I (we) agree that this notification of intent DOES NOT constitute APPROVAL of the subcontracts.

_____ (Individual or Firm)	_____ (Address)
_____ (Individual or Firm)	_____ (Address)
_____ (Individual or Firm)	_____ (Address)
_____ (Individual or Firm)	_____ (Address)

NOTE: Failure to complete the above DOES NOT preclude subsequent subcontracts. Subsequent subcontracts, if any, equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

Contractor _____

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
CERTIFICATION

I, _____,
(Name of person signing bid)

individually, and in my capacity as _____ of
(Title of person signing bid)

(Name of Firm, partnership, or Corporation)

do hereby certify under penalty of perjury under the laws of the United States and the State of Mississippi

that _____, Bidder
(Name of Firm, Partnership, or Corporation)

on Project No. **MP-5080-51(023)/ 306367301000**

in **Newton** _____ County(ies), Mississippi, has not either directly or indirectly entered into any agreement, participated in any collusion; or otherwise taken any action in restraint of free competitive bidding in connection with this contract; nor have any of its corporate officers or principal owners.

Except as noted hereafter, it is further certified that said legal entity and its corporate officers, principal owners, managers, auditors and others in a position of administering federal funds are not currently under suspension, debarment, voluntary exclusion or determination of ineligibility; nor have a debarment pending; nor been suspended, debarred, voluntarily excluded or determined ineligible within the past three years by the Mississippi Transportation Commission, the State of Mississippi, any other State or a federal agency; nor been indicted, convicted or had a civil judgment rendered by a court of competent jurisdiction in any matter involving fraud or official misconduct within the past three years.

Do exceptions exist and are made a part thereof? Yes / No

Any exceptions shall address to whom it applies, initiating agency and dates of such action.

Note: Exceptions will not necessarily result in denial of award but will be considered in determining bidder responsibility. Providing false information may result in criminal prosecution or administrative sanctions.

All of the foregoing is true and correct.

(1/2016 S)

SECTION 902

CONTRACT FOR MP-5080-51(023)/ 306367301000

LOCATED IN THE COUNTY(IES) OF Newton

STATE OF MISSISSIPPI,
COUNTY OF HINDS

This contract entered into by and between the Mississippi Transportation Commission on one hand, and the undersigned contractor, on the other witnesseth;

That, in consideration of the payment by the Mississippi Transportation Commission of the prices set out in the proposal hereto attached, to the undersigned contractor, such payment to be made in the manner and at the time of times specified in the specifications and the special provisions, if any, the undersigned contractor hereby agrees to accept the prices stated in the proposal in full compensation for the furnishing of all materials and equipment and the executing of all the work contemplated in this contract.

It is understood and agreed that the advertising according to law, the Advertisement, the instructions to bidders, the proposal for the contract, the specifications, the revisions of the specifications, the special provisions, and also the plans for the work herein contemplated, said plans showing more particularly the details of the work to be done, shall be held to be, and are hereby made a part of this contract by specific reference thereto and with like effect as if each and all of said instruments had been set out fully herein in words and figures.

It is further agreed that for the same consideration the undersigned contractor shall be responsible for all loss or damage arising out of the nature of the work aforesaid; or from the action of the elements and unforeseen obstructions or difficulties which may be encountered in the prosecution of the same and for all risks of every description connected with the work, exceptions being those specifically set out in the contract; and for faithfully completing the whole work in good and workmanlike manner according to the approved Plans, Specifications, Special Provisions, Notice(s) to Bidders and requirements of the Mississippi Department of Transportation.

It is further agreed that the work shall be done under the direct supervision and to the complete satisfaction of the Executive Director of the Mississippi Department of Transportation, or his authorized representatives, and when Federal Funds are involved subject to inspection at all times and approval by the Federal Highway Administration, or its agents as the case may be, or the agents of any other Agency whose funds are involved in accordance with those Acts of the Legislature of the State of Mississippi approved by the Governor and such rules and regulations issued pursuant thereto by the Mississippi Transportation Commission and the authorized Federal Agencies.

The Contractor agrees that all labor as outlined in the Special Provisions may be secured from list furnished by

It is agreed and understood that each and every provision of law and clause required by law to be inserted in this contract shall be deemed to be inserted herein and this contract shall be read and enforced as though it were included herein, and, if through mere mistake or otherwise any such provision is not inserted, then upon the application of either party hereto, the contract shall forthwith be physically amended to make such insertion.

The Contractor agrees that he has read each and every clause of this Contract, and fully understands the meaning of same and that he will comply with all the terms, covenants and agreements therein set forth.

Witness our signatures this the ____ day of _____, _____.

Contractor(s)

By _____

Title _____

Signed and sealed in the presence of:
(names and addresses of witnesses)

MISSISSIPPI TRANSPORTATION COMMISSION

By _____

Executive Director

Secretary to the Commission

Award authorized by the Mississippi Transportation Commission in session on the ____ day of _____, _____, Minute Book No. _____, Page No. _____.

Revised 8/06/2003

SECTION 903
PERFORMANCE AND PAYMENT BOND

CONTRACT BOND FOR: MP-5080-51(023)/ 306367301000

LOCATED IN THE COUNTY(IES) OF: Newton

STATE OF MISSISSIPPI,
COUNTY OF HINDS

Know all men by these presents: that we, _____
_____, (Contractor)
_____, Principal, a _____

residing at _____ in the State of _____

and _____
_____, (Surety)

residing at _____ in the State of _____,

authorized to do business in the State of Mississippi, under the laws thereof, as surety, effective as of the contract date

shown below, are held and firmly bound unto the State of Mississippi in the sum of _____

(\$ _____) Dollars, lawful money of the United States of America, to be paid to it for which
payment well and truly to be made, we bind ourselves, our heirs, administrators, successors, or assigns jointly and
severally by these presents.

The conditions of this bond are such, that whereas the said _____

principal, has (have) entered into a contract with the Mississippi Transportation Commission, bearing the date of
_____ day of _____ A.D. _____ hereto annexed, for the construction of certain projects(s) in
the State of Mississippi as mentioned in said contract in accordance with the Contract Documents therefor, on file in the
offices of the Mississippi Department of Transportation, Jackson, Mississippi.

Now therefore, if the above bounden _____
_____ in all things shall stand to and abide by and well and truly observe, do keep and perform all and
singular the terms, covenants, conditions, guarantees and agreements in said contract, contained on his (their) part to be
observed, done, kept and performed and each of them, at the time and in the manner and form and furnish all of the
material and equipment specified in said contract in strict accordance with the terms of said contract which said plans,
specifications and special provisions are included in and form a part of said contract and shall maintain the said work
contemplated until its final completion and acceptance as specified in Subsection 109.11 of the approved specifications,
and save harmless said Mississippi Transportation Commission from any loss or damage arising out of or occasioned by
the negligence, wrongful or criminal act, overcharge, fraud, or any other loss or damage whatsoever, on the part of said
principal (s), his (their) agents, servants, or employees in the performance of said work or in any manner connected
therewith, and shall be liable and responsible in a civil action instituted by the State at the instance of the Mississippi
Transportation Commission or any officer of the State authorized in such cases, for double any amount in money or
property, the State may lose or be overcharged or otherwise defrauded of, by reason of wrongful or criminal act, if any, of
the Contractor(s), his (their) agents or employees, and shall promptly pay the said agents, servants and employees and all
persons furnishing labor, material, equipment or supplies therefor, including premiums incurred, for Surety Bonds,
Liability Insurance, and Workmen's Compensation Insurance; with the additional obligation that such Contractor shall
promptly make payment of all taxes, licenses, assessments, contributions, damages,

any liquidated damages which may arise prior to any termination of said principal's contract, any liquidated damages which may arise after termination of the said principal's contract due to default on the part of said principal, penalties and interest thereon, when and as the same may be due this state, or any county, municipality, board, department, commission or political subdivision: in the course of the performance of said work and in accordance with Sections 31-5-51 et seq. Mississippi Code of 1972, and other State statutes applicable thereto, and shall carry out to the letter and to the satisfaction of the Executive Director of the Mississippi Department of Transportation, all, each and every one of the stipulations, obligations, conditions, covenants and agreements and terms of said contract in accordance with the terms thereof and all of the expense and cost and attorney's fee that may be incurred in the enforcement of the performance of said contract, or in the enforcement of the conditions and obligations of this bond, then this obligation shall be null and void, otherwise to be and remain in full force and virtue.

_____ (Contractors) Principal	_____ Surety
By _____	By _____ (Signature) Attorney in Fact
	Address _____ _____ _____
Title _____ (Contractor's Seal)	_____ (Printed) MS Agent
	_____ (Signature) MS Agent
	Address _____ _____ _____
	_____ (Surety Seal)
	_____ Mississippi Insurance ID Number



BID BOND

KNOW ALL MEN BY THESE PRESENTS, that we _____
Contractor

Address

City, State ZIP

As principal, hereinafter called the Principal, and _____
Surety

a corporation duly organized under the laws of the state of _____

as Surety, hereinafter called the Surety, are held and firmly bound unto **State of Mississippi, Jackson, Mississippi**

As Obligee, hereinafter called Obligee, in the sum of **Five Per Cent (5%) of Amount Bid**

Dollars(\$ _____)

for the payment of which sum will and truly to be made, the said Principal and said Surety, bind ourselves, our heirs, executors, administrators, successors and assigns, jointly and severally, firmly by these presents.

WHEREAS, the Principal has submitted a bid for **Mill & Overlay approximately 10 miles of US 80 from Scott County Line to Scanlan Street, known as State Project No. MP-5080-51(023) / 306367301 in Newton County.**

NOW THEREFORE, the condition of this obligation is such that if the aforesaid Principal shall be awarded the contract, the said Principal will, within the time required, enter into a formal contract and give a good and sufficient bond to secure the performance of the terms and conditions of the contract, then this obligation to be void; otherwise the Principal and Surety will pay unto the Obligee the difference in money between the amount of the bid of the said Principal and the amount for which the Obligee legally contracts with another party to perform the work if the latter amount be in excess of the former, but in no event shall liability hereunder exceed the penal sum hereof.

Signed and sealed this _____ day of _____, 20__

(Witness)

(Witness)

(Principal) (Seal)

By: _____
(Name) (Title)

(Surety) (Seal)

By: _____
(Attorney-in-Fact)

(MS Agent)

Mississippi Insurance ID Number

