

## SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.	<u>1</u>	DATED	<u>9/10/2018</u>	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____
ADDENDUM NO.	_____	DATED	_____	ADDENDUM NO.	_____	DATED	_____

Number

Description

1 Added TSI-1 to NTB No. 1056; Amendment EBS Download Required.

TOTAL ADDENDA: 1

(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE \_\_\_\_\_

Contractor

BY \_\_\_\_\_

Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

President

Address

Secretary

Address

Treasurer

Address

The following is my (our) itemized proposal.

NH-0014-03(073)/ 107652301000

Perry County(ies)

Revised 01/26/2016

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

**SECTION 904 - NOTICE TO BIDDERS NO. 1056**

**CODE: (SP)**

**DATE: 07/17/2018**

**SUBJECT: Scope of Work**

**PROJECT: NH-0014-03(073) / 107652301 -- Perry County**

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings". All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following:

## **OVERLAY US 98** **FROM FORREST COUNTY LINE TO 1 MILE EAST OF SR 29**

- (A) Prior to the overlay, centerline alignment shall be determined by the Contractor by measuring the existing roadway at 500-foot intervals in tangent sections and 100-foot intervals in horizontal curves. The existing shoulders shall be clipped and surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Project Engineer, and will be an absorbed item.
- (B) Fine mill 1¼" and variable at all the transitions including B.O.P., E.O.P, bridges and all local road tie-ins. Fine mill the entire intersection of US 98 and SR 29. All fine milled areas shall be backfilled with asphalt within seven (7) days of milling. All milled material shall become property of the Contractor.
- (C) Overlay US 98 with 1¼" of 9.5-mm, HT, polymer modified asphalt from BOP to EOP (Approximately 19,000 Tons). Prior to the overlay, a leveling course of ¾" of 9.5-mm, HT, leveling asphalt will be required from BOP to west side of US 98/SR 29 intersection and from the east side of US 98/SR 29 intersection to EOP (Approximately 11,700 Tons). Smoothness tolerance will be governed by the Standard Specifications, Section 403, Category B. Existing paved outside shoulders; publicly maintained roads or streets shall be surfaced to the existing R.O.W.; privately owned entrances shall be surfaced a distance of 10 feet and variable from edge of pavement (Approximately 3,500 Tons 9.5-mm, ST, asphalt). Crossovers shall be widened and paved with 9.5-mm, ST, Asphalt Pavement as per Detail D6-1. Any site grading at local roads or drives will not be measured for separate payment but will be considered an absorbed item. Cross slopes shall be maintained to achieve a uniform cross slope of 2%. If water stands when project is complete, the contractor shall correct at no additional cost to the State.

- (D) Raise the existing shoulders to match the new pavement elevation by placing 2" & variable depth 825B crushed stone. Placement of the crushed stone on the finished surface course shall not be permitted. The material shall be bladed, rolled and compacted to a finished slope of 4% where practical. Shoulders with existing adequate shoulder material in place shall be bladed to a slope of 4%, the cost of which shall be included in the prices of other items bid.
- (E) Temporary striping shall conform to finished stripe specifications for alignment, neatness, reflectivity, and straightness. All permanent pavement markings are to be hot thermoplastic. Edge lines will be placed so as to maintain the original lane width. Special care should be taken for the placement of thermoplastic detail stripe along the edge of pavement at turn-outs on all local roads and along tapers where detail stripe is required. Any pavement management markings encountered shall be marked and replaced with four feet (4') of 6-inch white thermoplastic stripe placed at and perpendicular to centerline. The cost of placing pavement management markings shall not be measured for separate pay.
- (F) Raised pavement markers will be placed as per sheet PM-1 and crossover raised pavement markers shall be placed as per Sheet D6-1 and as directed by the Project Engineer. Any removal of existing raised pavement markers or rumble bars shall be considered an absorbed item. Only flexible adhesive shall be allowed for placement of raised pavement markers meeting the requirements of Subsection 720.03.7.7.
- (G) The installation of flared terminal end sections shall be used at each of the bridge locations on this project. The Contractor shall furnish the Project Engineer two (2) copies of the manufacture's installation instructions prior to beginning guardrail operations. Any site grading and all fill material necessary at the guardrail location will not be measured for separate payment but will be absorbed in other guardrail pay items. Seeding, fertilizer and vegetative mulch are required on all disturbed areas at the guardrail site and will be considered an absorbed item. Application rate for bermuda grass seed will be 25 pounds per acre. Application rate for combination fertilizer (13-13-13) will be 1,000 pounds per acre. Application rate for mulch shall be two (2) tons per acre.
- (H) A pedestrian crosswalk is to be added at the intersection of US 98 and SR 29 as per the attached Pedestrian Crosswalk Layout and Standard Drawings CR-1, CR-2, CR-3, and CR-4. Any saw cutting required for removal of concrete curb and islands is to be absorbed. Any necessary site grading for installation of the sidewalks is to be absorbed. If additional fill material is needed, it will be paid for as crushed stone. All sidewalks, including those running across existing median islands, shall be five feet (5') in width and four inches (4") in thickness.
- (I) An accessible pedestrian crossing system is to be added to the existing traffic signal at the intersection of US 98 and SR 29 as part of the added pedestrian crosswalk. Crossing from southeast island to northeast island shall be signalized and actuated. Crossings from islands to shoulder shall not be signalized or actuated. Cable shall be run through existing conduit and pull boxes. Existing malfunction management unit shall be reconfigured to monitor walk indication for conflicts. APS system shall be programmed with audible

messages and tones. Southeast pushbutton shall be mounted on Type VI, 8' Shaft Traffic Signal Equipment Pole. Northeast pushbutton shall be mounted on existing traffic signal pole. Payment shall be made under pay item 645-B: Accessible Pedestrian Detection Assembly. One Type 6 LED traffic signal heads shall be installed on the new southeast Type VI pole. One Type 6 LED signal head shall be installed on the existing northeast traffic signal pole. Pedestrian actuation signs shall be provided and installed as part of pay item 635-A. Pole foundations shall be constructed of Class "B" structural concrete.

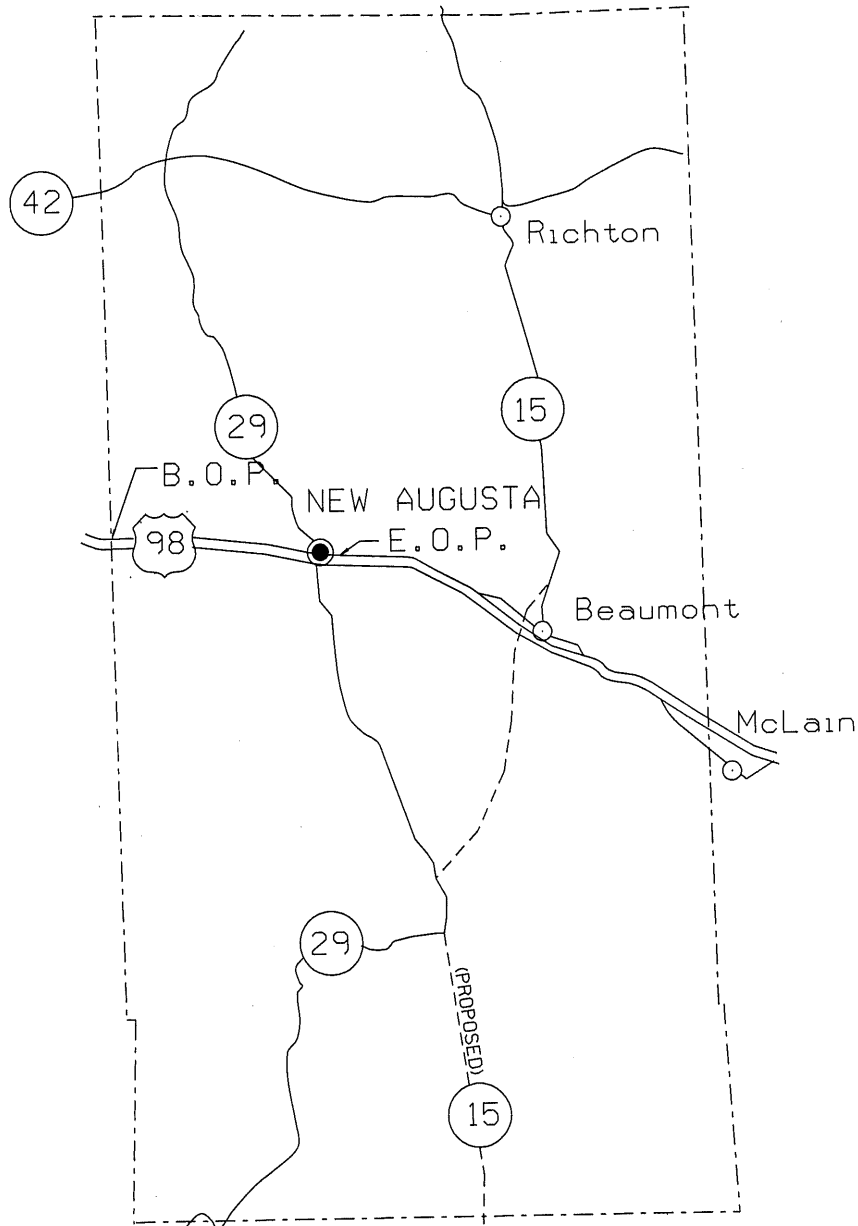
- (J) Any traffic signal vehicle detection devices not in proper operation prior to, or damaged as a result of construction operations, shall be replaced as directed by the Engineer. The type of vehicle detection device shall be the same as its predecessor. Vehicle loop assemblies shall be installed and paid as per pay item 640-A: Vehicle Loop Assemblies. Loop detectors shall be placed subsurface; they shall not be installed in the final lift of asphalt.

	# loops	lf/loop	loop (lf)	lead in (lf)	Total
6x6 loops	4	24	96	60	156
6x50 loops	4	112	448	120	568
	LF total 640-A: Vehicle Loop Assembly				724

- (K) Existing traffic signal cabinet assembly at the intersection of US 98 and SR 29 shall be removed and replaced on the existing cabinet foundation. Cabinet shall have a 30-amp main breaker, 5-position detector rack, laptop pull out tray, and 175-watt power supply. An advanced flasher system shall be required. No pre-emption shall be required. Contractor shall field verify dimensions of existing bolt pattern prior to ordering cabinet. Existing traffic signal equipment shall be delivered to MDOT District Headquarters office in Hattiesburg, MS for salvage to MDOT. All costs associated with this work are to be included in the price for pay item No. 907-632-B: Remove and Replace Existing Traffic Signal Cabinet Assembly, Type III Cabinet, Type 1 Controller.
- (L) The Contractor shall erect and maintain construction signing, provide all signs and traffic handling devices, and shall provide two portable R16-3 signs per work zone or lane closure in addition to signs required by standard drawings in accordance with the Manual on Uniform Traffic Control Devices (MUTCD), latest edition. Fluorescent orange sheeting shall be used on all construction and traffic control signs except for those designated to be black legend and border on white background. All plastic drums shall have a ballasting collar made from recycled truck tires or other suitable material. The cost for this work is to be included in the price bid for Pay Item No. 618-A: Maintenance of Traffic.
- (M) Incidental work such as removing vegetation, shaping and compaction of shoulder, removing excess asphalt material, project clean-up, and other incidental work necessary to complete the project will not be measured for separate payment, but will be included in other bid items, and must be performed during the operating hours for this project.
- (N) It shall be the responsibility of the Contractor to protect the roadway and all existing structures, such as bridges, culverts, signs, and curb, from damage occurring as a result of

the Contractor's operations. Damages to existing structures caused by the Contractor's operations shall be repaired or replaced at no cost to the Department.

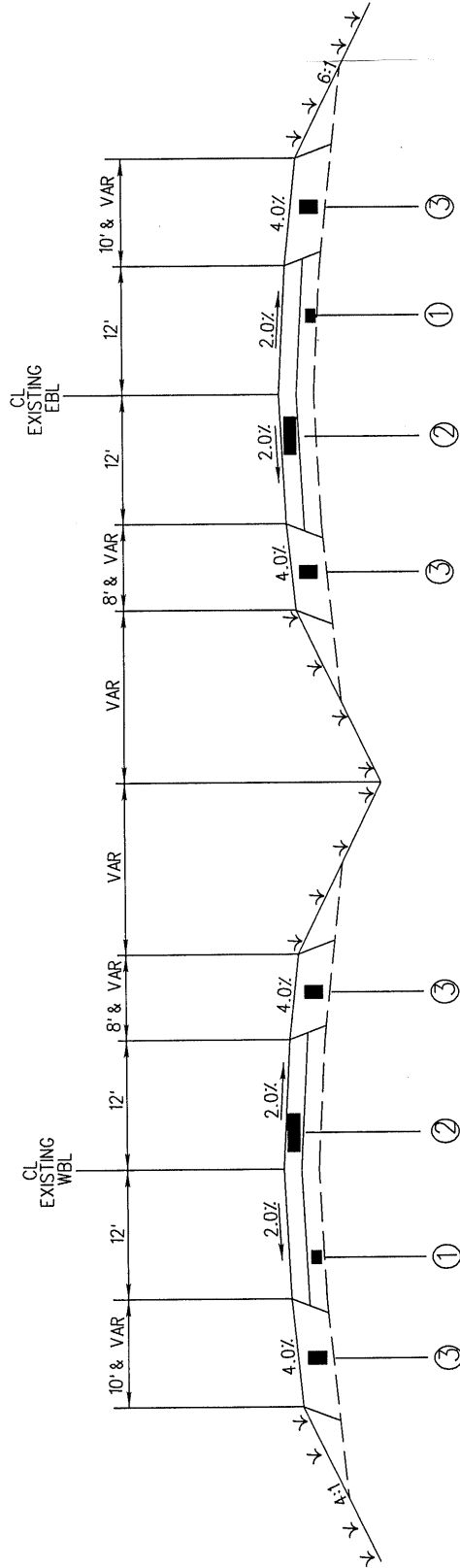
NH-001 4-03( 073) / 107652301  
PERRY COUNTY



THE ABOVE REFERENCED PROJECT IS FOR OVERLAYING  
U.S 98 FROM FORREST COUNTY LINE EAST 7.5 MILES  
TO 1 MILE EAST OF SR 29 IN PERRY COUNTY

NH-0014-03(073) 107652/301000  
PERRY COUNTY

FROM PERRY COUNTY LINE TO 1 MILE EAST OF HWY 29



### TYPICAL SECTION No. 1

MILL AND OVERLAY

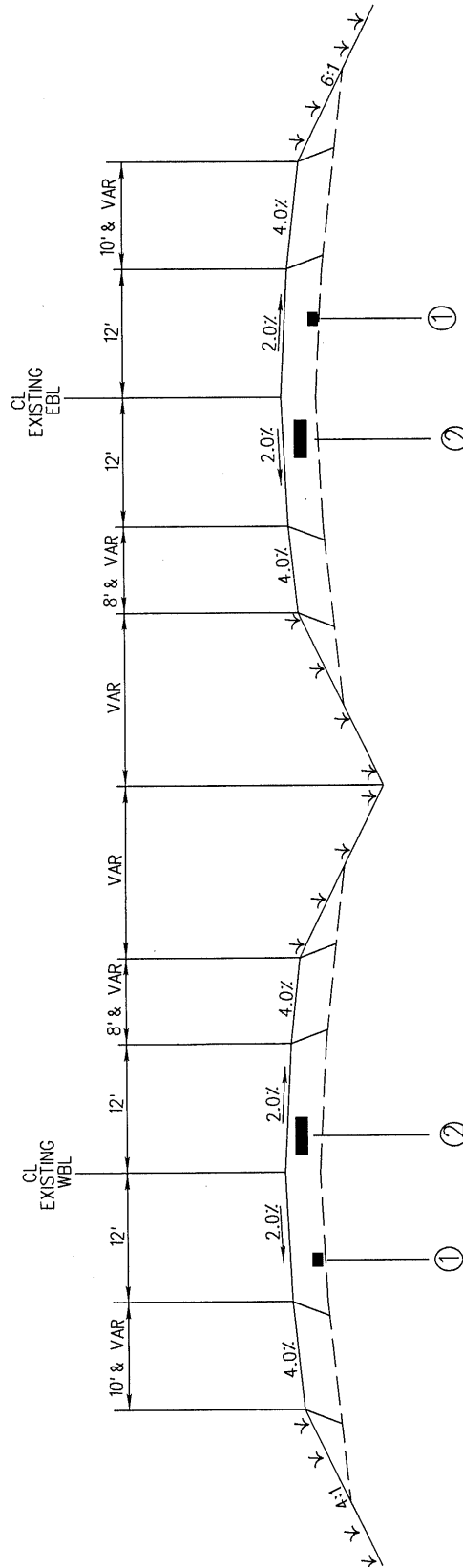
LIMITS

PERRY COUNTY LINE TO CROSSOVER WEST OF HWY 29

- ① 3/4" 9.5mm HT ASPHALT PAVEMENT, LEVEL 1
- ② 1 1/4" 9.5MM HT-POLYMOD
- ③ 2" SIZE 825B CRUSHED STONE BASE

NH-0014-03(073) 107652/301000  
PERRY COUNTY

FROM PERRY COUNTY LINE TO 1 MILE EAST OF HWY 29



TYPICAL SECTION No. 2  
MILL AND OVERLAY

LIMITS

FROM CROSSOVER WEST OF HWY 29 TO MAIN STREET

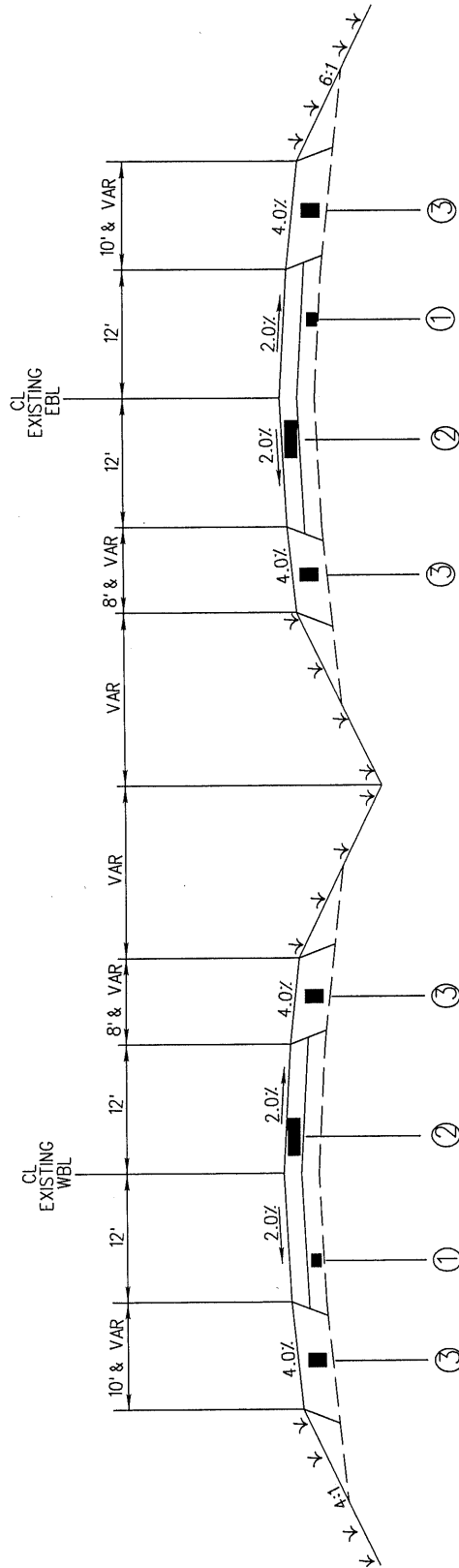
- ① 1 1/2" FINE MILLING
- ② 1 1/2" 9.5MM HT-POLYMOD



NH-0014-03(073) 107652/301000

PERRY COUNTY

FROM PERRY COUNTY LINE TO 1 MILE EAST OF HWY 29



TYPICAL SECTION No. 3

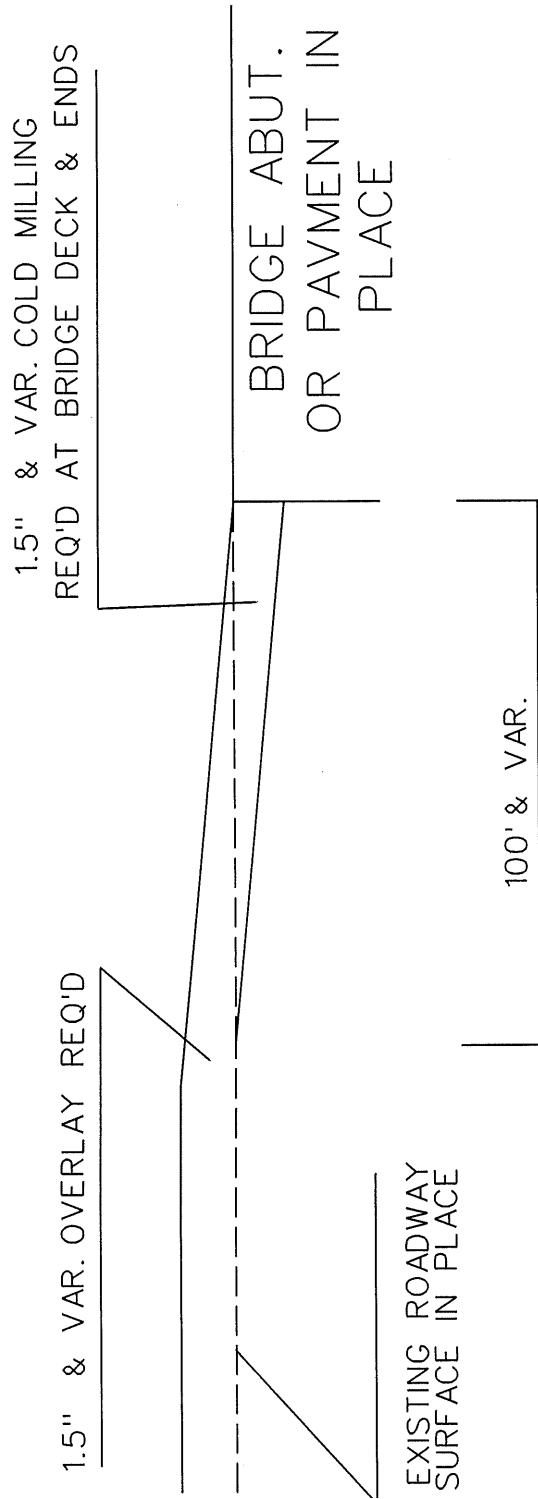
MILL AND OVERLAY

LIMITS

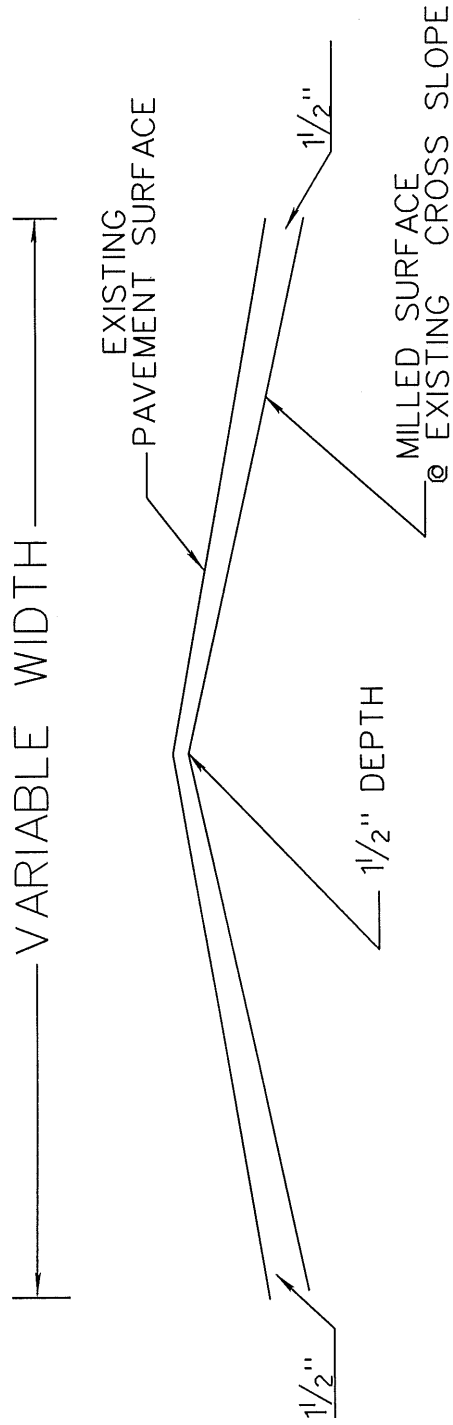
MAIN STREET TO 1 MILE EAST OF HWY 29

- ① 3/4" 9.5mm HT ASPHALT PAVEMENT, LEVELENG
- ② 1 1/4" 9.5MM HT-POLYMOD
- ③ 2" SIZE 825B CRUSHED STONE BASE

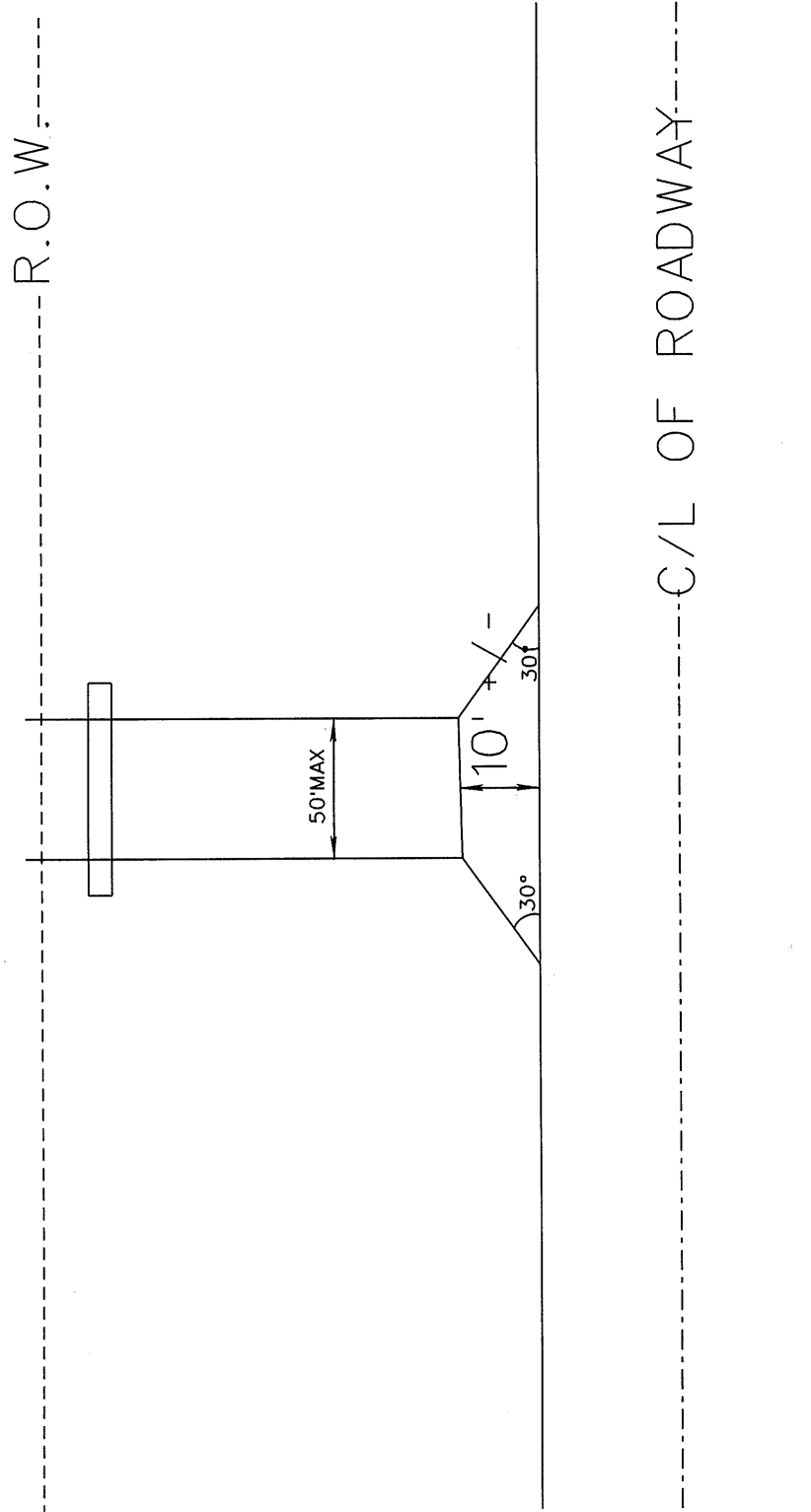
# TYPICAL MILLED TRANSITION AT BRIDGE ABUT. OR PAVEMENT IN PLACE

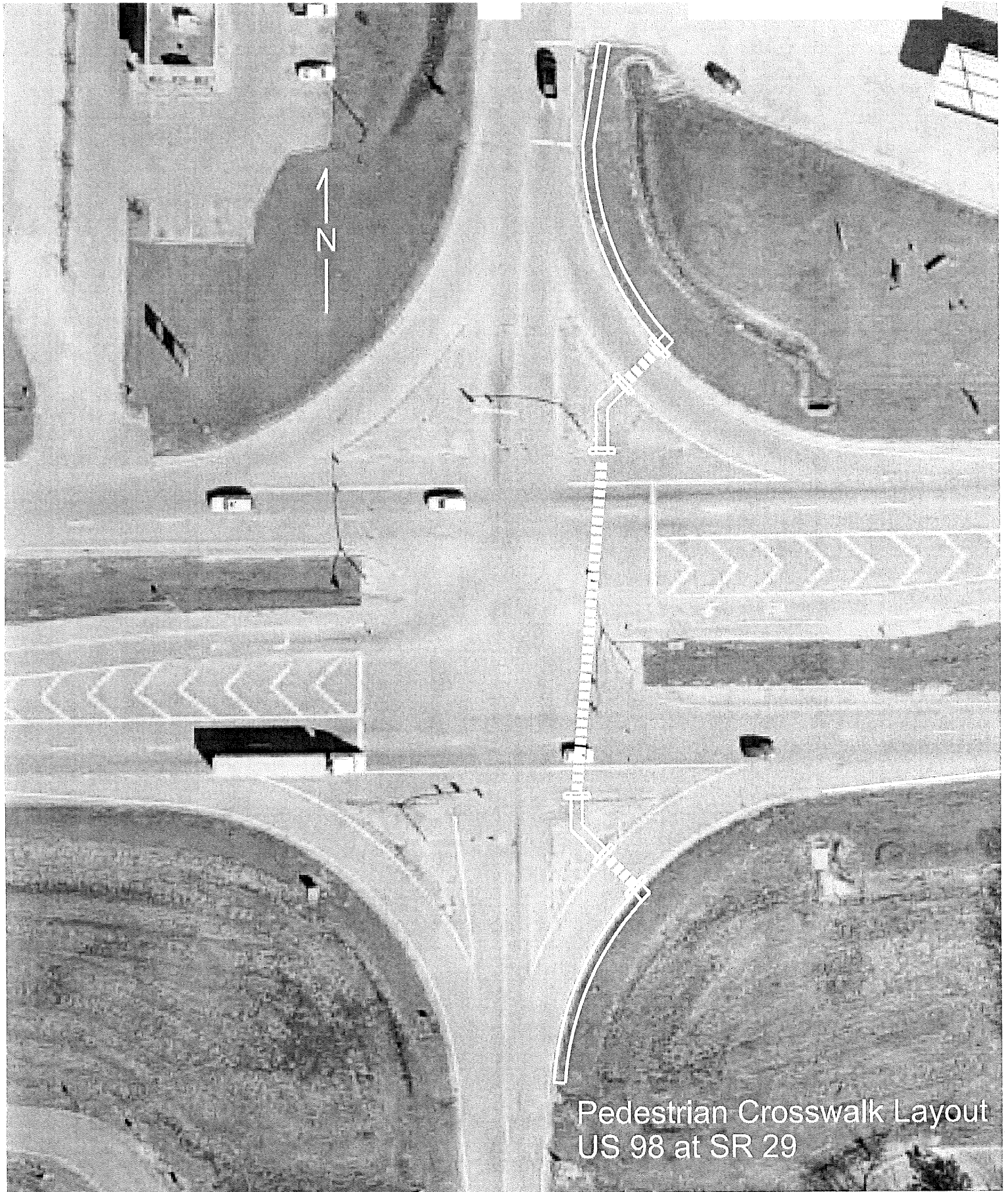


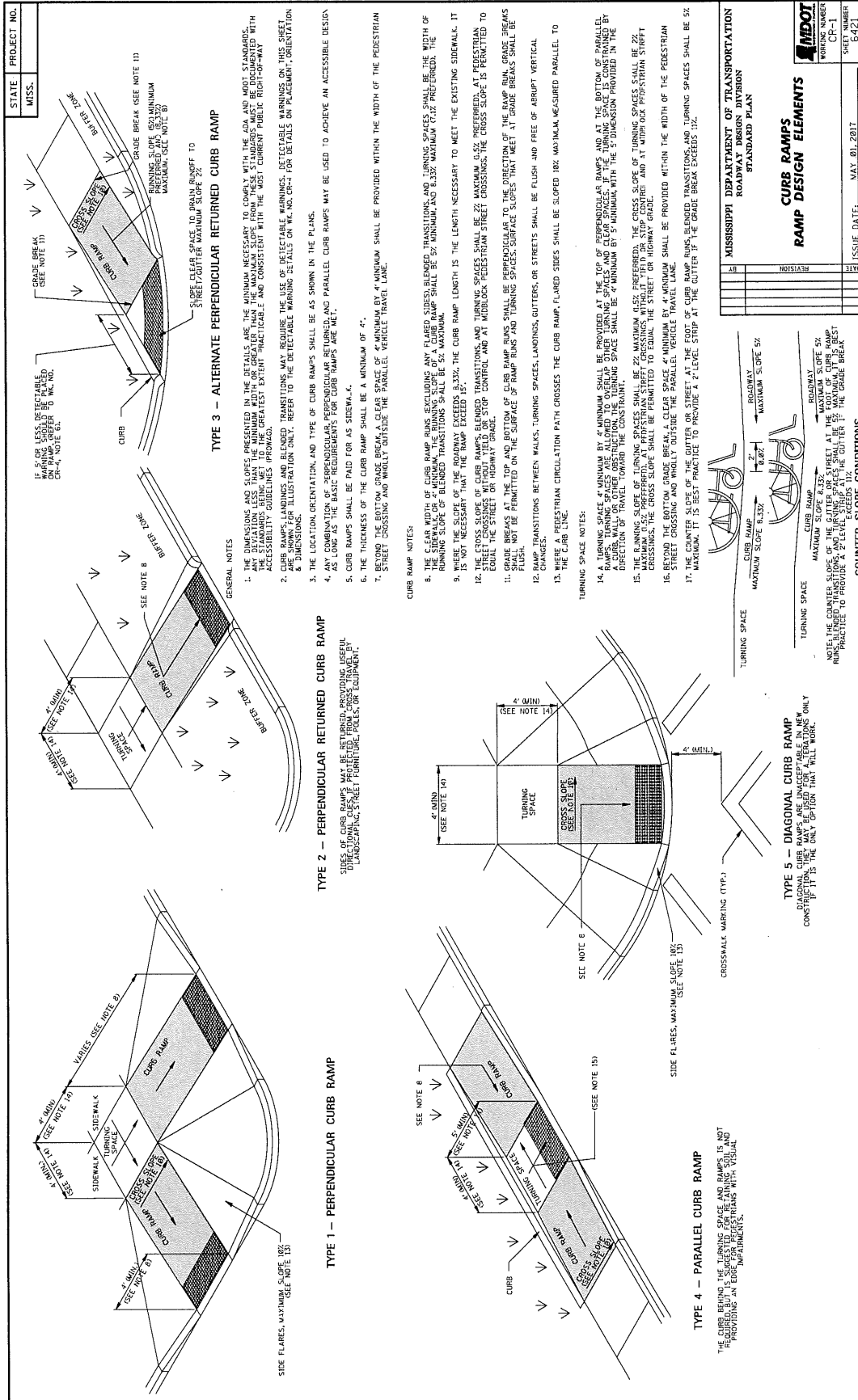
# TYPICAL MILLING DIAGRAM

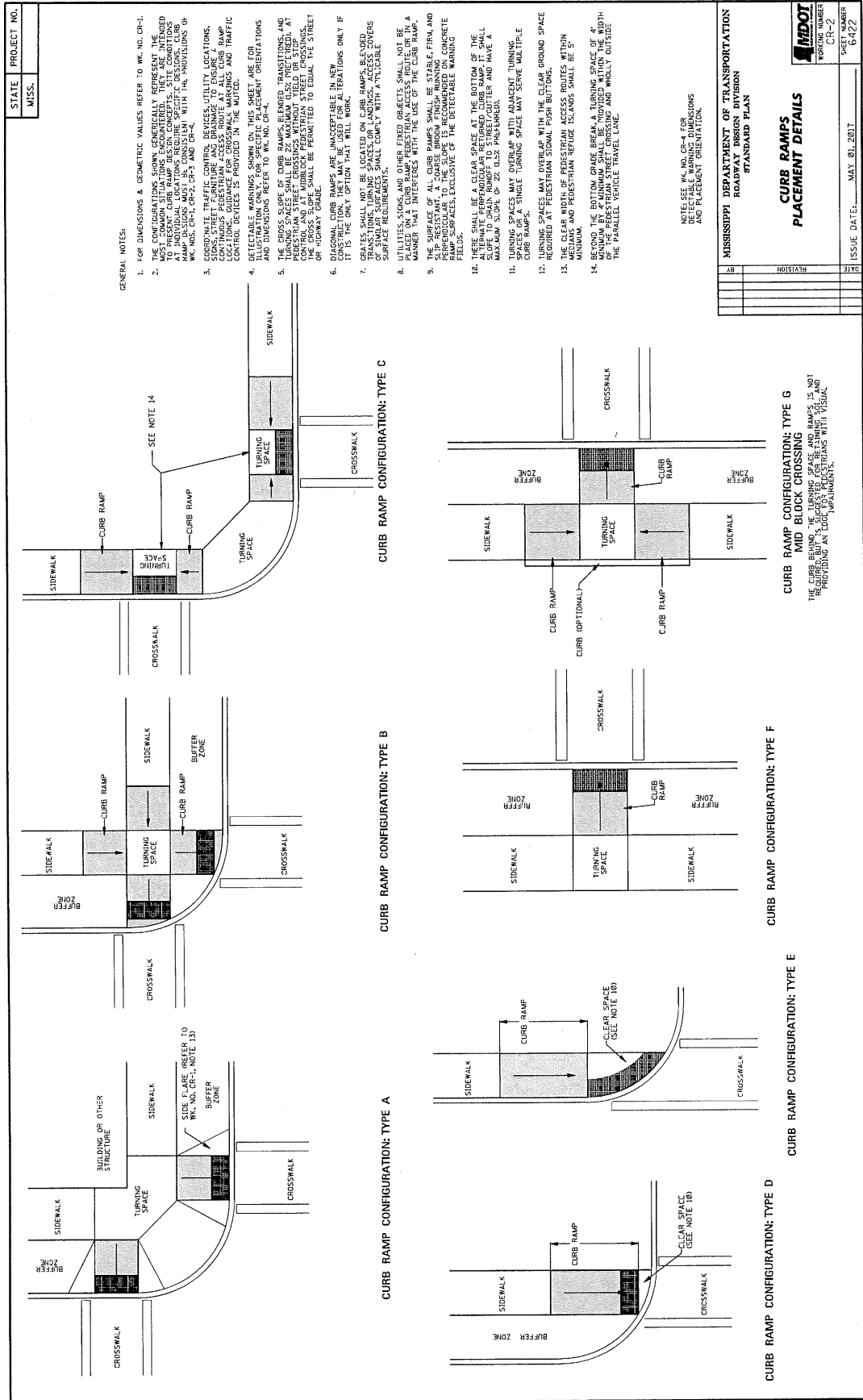


PRIVATE DRIVEWAY DETAIL









**CURB RAMP CONFIGURATION: TYPE G**  
**MID BLOCK CROSSING**  
THE CURB BEHIND THE TURNING SPACE AND RAMP IS NOT REQUIRED TO BE SET BACK FROM THE STREET AND PROVIDING AN ADEQUATE FIELD OF VISION.





**DETECTABLE WARNING PLACEMENT DETAIL 1**

NOTE: IF THE DISTANCE FROM THE GRADE BREAK TO THE CURB OR RAMP IS LESS THAN 5', DETECTABLE WARNING SURFACES SHALL BE PLACED ALONG THE RADIUS OF THE CURVE AS SHOWN IN THE ABOVE DETAIL.

**DETECTABLE WARNING PLACEMENT DETAIL 2**

NOTE: IF THE DISTANCE FROM THE GRADE BREAK TO THE CURB OR RAMP IS LESS THAN 5', DETECTABLE WARNING SURFACES SHALL BE PLACED ON THE CURB 5' TO 10' FROM THE FRONT OF THE CURB OR EDGE OF THE ROADWAY.

**DETECTABLE WARNING AT RAILROAD CROSSING**

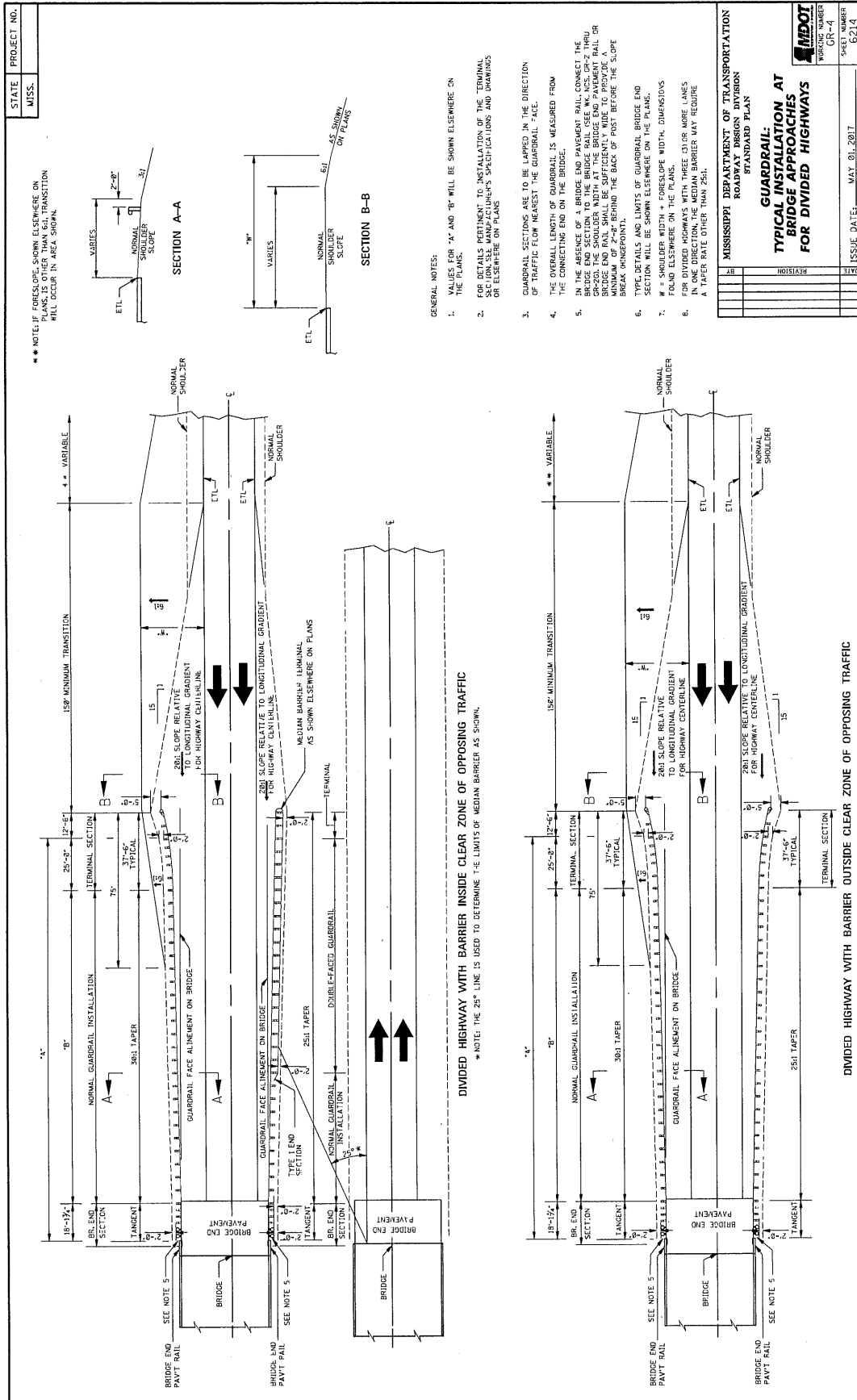
**DETECTABLE WARNING AT BLENDED TRANSITION**

**DETECTABLE WARNING AT CURB RAMP**

**DETECTABLE WARNING LAYOUT**

NO.	DESCRIPTION	QUANTITY	UNIT	PRICE	TOTAL
1	DETECTABLE WARNING SURFACES	100	SQ. YD.	1.50	150.00
2	DETECTABLE WARNING SURFACES	50	SQ. YD.	1.50	75.00
3	DETECTABLE WARNING SURFACES	25	SQ. YD.	1.50	37.50
4	DETECTABLE WARNING SURFACES	10	SQ. YD.	1.50	15.00
5	DETECTABLE WARNING SURFACES	5	SQ. YD.	1.50	7.50
6	DETECTABLE WARNING SURFACES	2	SQ. YD.	1.50	3.00
7	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
8	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
9	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
10	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
11	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
12	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
13	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
14	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
15	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
16	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
17	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
18	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
19	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
20	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
21	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
22	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
23	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
24	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
25	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
26	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
27	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
28	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
29	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
30	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
31	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
32	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
33	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
34	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
35	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
36	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
37	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
38	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
39	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
40	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
41	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
42	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
43	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
44	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
45	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
46	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
47	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
48	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
49	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
50	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
51	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
52	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
53	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
54	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
55	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
56	DETECTABLE WARNING SURFACES	1	SQ. YD.	1.50	1.50
57	DETECTABLE WARNING SURFACES				





STATE		PROJECT NO.	
MISS.		UPDATE	
<p>1. POLES, SIGNAL HEADS, EQUIPMENT BOXES, PULLBOXES AND CONDUIT LOCATIONS MAY BE VARIED SLIGHTLY TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. HOWEVER, SIGNAL HEAD OR POLE LOCATIONS SHALL BE WITHIN REQUIREMENTS OUTLINED IN THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND HIGHWAY DESIGN AND OPERATIONAL PRACTICES RELATED TO HIGHWAY SAFETY.</p> <p>2. THE CONTRACTOR SHALL PROVIDE MAST ARM POLE DESIGN CERTIFICATION AND CALCULATIONS AS OUTLINED IN SECTION 722.02 OF STANDARD SPECIFICATIONS. DESIGN STANDARD FOR MAST ARMS POLES SHALL BE 2013 AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS. USE FATIGUE CATEGORY II, USE 50 YEAR DESIGN SERVICE LIFE AND DO NOT CONSIDER GALLOPING OR TRUCK-INDUCED GUSTS, WIND AND ICE LOADS VARIABLE BASED UPON MAPS IN THE 2013 AASHTO STANDARD SPECIFICATIONS. USE UPSWEEP MAST ARMS UNLESS OTHERWISE NOTED ON PLANS. SEE TSD 3.</p> <p>3. DETERMINATION OF REQUIRED SIZES, LENGTHS AND GAUGES OF TYPE 1 - XI STEEL POLES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR IN ACCORDANCE WITH THE PLANS AND SECTION 722.02 OF THE STANDARD SPECIFICATIONS, UNLESS OTHERWISE SPECIFIED IN PLANS OR SPECIFICATIONS.</p> <p>4. TRAFFIC SIGNAL MAST ARM POLES SHALL BE HOT DIPPED GALVANIZED WITH FINISH APPROVED BY THE ENGINEER.</p> <p>5. TRAFFIC SIGNAL MAST ARM POLES REQUIRING LUMINAIRES ARE DESIGNATED BY (L). ALL LUMINAIRES SHALL BE LED UNLESS OTHERWISE NOTED ON PLANS.</p> <p>6. STAINLESS STEEL TAG ATTACHED TO THE POLE SHAFT USING 3/16 INCH STAINLESS STEEL POP RIVETS WITH PROPERTIES AND INFORMATION AS FOLLOWS: - MINIMUM 1/16 INCH THICKNESS - MINIMUM 1/4 INCH SLOTTED LEGEND WITH FOLLOWING INFORMATION: - MONTH / YEAR OF MANUFACTURE - UNIQUE IDENTIFYING NUMBER FOR FUTURE MANUFACTURER REFERENCE - EXTERNAL PROJECT NUMBER FROM THE PLANS COVER SHEET (EXAMPLE: STP-XXXX-XX.) TAG TO BE INSTALLED ON SHAFT SIDE OPPOSITE THE MAINLINE HIGHWAY AND LOCATED APPROXIMATELY 48 INCHES ABOVE THE TOP OF BASE PLATE.</p> <p>7. THE TOP OF THE STRAIN POLE FOUNDATION SHALL BE 6" ABOVE THE GROUND. THE CONTRACTOR SHALL PROVIDE POLES OF SUFFICIENT LENGTH PLUS 2 FEET TO PROVIDE REQUIRED VERTICAL CLEARANCE OF THE TRAFFIC SIGNAL HEADS WITHOUT EXTENDING THE FOUNDATION ABOVE THE GROUND LINE OF THE POINT WHERE THE POLE IS LOCATED. EVEN THOUGH THIS MAY BE BELOW THE FINISHED GRADE OF THE ROADWAY.</p> <p>8. ALL STRAIN POLES AT AN INTERSECTION SHALL BE THE SAME DIAMETER AND UTILIZE THE SAME BOLT ORCLE SPACING.</p> <p>9. POLE FOUNDATIONS AND BASE MOUNTED CABINET FOUNDATIONS, GRADE SHALL BE ESTABLISHED TO .33" OF EDGE OF PAVEMENT ELEVATION UNLESS APPROVED BY SIGNAL PROJECT ENGINEER.</p> <p>10. TRAFFIC SIGNAL HEADS SHALL BE BLACK IN COLOR UNLESS OTHERWISE NOTED ON PLANS WITH BLACK BACK PLATES</p> <p>11. PEDESTRIAN HEADS SHALL BE BLACK IN COLOR UNLESS OTHERWISE NOTED ON PLANS.</p> <p>12. PEDESTRIAN PUSHBUTTONS SHALL BE EITHER STANDARD PUSHBUTTONS OR APS (ACCESSIBLE PEDESTRIAN SYSTEM) STYLE AS NOTED ON PLANS. SIGNS TO BE INCLUDED IN PAY ITEM FOR PEDESTRIAN PUSHBUTTONS AT NO ADDITIONAL COST. SIDE OF POLE LOCATIONS OF PUSHBUTTONS MAY BE HELD ADJUSTED. PUSHBUTTON HARDWARE SHALL BE BLACK IN COLOR UNLESS OTHERWISE NOTED ON PLANS.</p> <p>13. FIELD DRILL AND TAP EXISTING POLES WHERE PEDESTRIAN SIGNALS AND PUSHBUTTONS ARE REQUIRED ON PLANS. (ABSORBED ITEM).</p> <p>14. TRAFFIC SIGNAL CABINETS AND CONTROLLERS SHALL BE WIRED TO PROVIDE FOR ALL PHASES INCLUDING FUTURE PHASES IN ACCORDANCE WITH THE PHASE SEQUENCE DIAGRAM.</p> <p>15. ALL TRAFFIC SIGNAL CONTROLLERS SHALL BE THE CURRENT VERSION OF SIEMENS EAGLE TRAFFIC SIGNAL CONTROLLER WITH SEPCAC SOFTWARE, ETHERNET READY, AND COMPATIBLE WITH MDOOT'S EXISTING TRAFFIC SIGNAL MANAGEMENT SOFTWARE. ALL TRAFFIC SIGNAL CONTROLLER FIRMWARE SHALL BE CAPABLE OF DELAYING THE ONSET OF THE FLASHING YELLOW ARROW. ALL MMTS SHALL BE ETHERNET READY, 16 CHANNEL, AND CAPABLE OF RUNNING 12 DIFFERENT MODES OF FLASHING YELLOW ARROW OPERATION. THE CONTRACTOR SHALL COORDINATE WITH MDOOT FOR IP ADDRESSES ON ALL NETWORKABLE DEVICES. DEVICES INCLUDING THE MMTS, DETECTOR UNIT, AND TRAFFIC SIGNAL CONTROLLER CABINET SHALL HAVE A 16 LOAD BAY FACILITY. REAR ACCESS DOOR, LAPTOP TRAY, AND DUAL POSITION INTERNAL LED LIGHTING. AND ONE 175 WATT MINIMUM POWER SUPPLY AND 4 AVAILABLE SLOTS UNLESS OTHERWISE NOTED ON PLANS.</p> <p>16. FOR PROTECTED/PERMITTED LEFT TURN PHASING USING TYPE 2 PVA TRAFFIC SIGNAL HEADS, OPERATION SHALL BE AS FOLLOWS: GREEN ARROW, FOLLOWED BY A SOLID YELLOW ARROW, AND ENDING WITH A SOLID RED ARROW. THE PERMITTED PORTION OF THIS OPERATION SHALL START WITH A FLASHING YELLOW ARROW, FOLLOWED BY A SOLID YELLOW ARROW, AND ENDING WITH A SOLID RED ARROW. THERE SHALL BE A DELAY (AS DIRECTED BY THE ENGINEER) BETWEEN THE END OF THE PROTECTED PORTION OF THIS OPERATION AND THE BEGINNING OF THE PERMITTED PORTION OF THIS OPERATION. DURING THIS DELAY, THE OPPOSING PHASE THRU HEADS ARE CAPABLE OF DISPLAYING A GREEN BALL SIGNAL CONTROLLER WITH FIRMWARE NECESSARY TO ACCOMPLISH THIS DELAY SHALL BE PROVIDED.</p> <p>17. POLES AND FOUNDATIONS OF EXISTING SIGNAL INSTALLATION REMOVALS SHALL BE CUT OFF 6" BELOW GROUND, REMOVED AND AREA RESTORED TO MATCH ADJACENT SURFACE AS DIRECTED BY THE ENGINEER.</p> <p>18. ALL REMOVED EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BECOME THE PROPERTY OF THE CONTRACTOR, UNLESS SPECIFIC ITEMS ARE NOTED IN THE PLANS TO BE SALVAGED AS DIRECTED BY THE ENGINEER.</p> <p>19. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ELECTRICAL SERVICE FROM SPAN WIRE INSTALLATION. POWER SHALL RUN FROM THE POWER COMPANY SERVICE POINT ADJACENT TO THE SIGNAL POLE NEAREST THE CONTROLLER. THE SERVICE SHALL THEN RUN TO THE CONTROLLER AS SHOWN ON THE PLANS. FOR MAST ARM INSTALLATION, POWER SHALL RUN FROM THE ELECTRICAL SERVICE POINT UNDERGROUND TO THE CONTROLLER CABINET, AS SHOWN ON THE PLANS.</p> <p>20. POWER SERVICE METER SHALL NOT BE MOUNTED ON THE CONTROLLER CABINET OR MAST ARM POLE SHAFTS. A SEPARATE POWER SERVICE PEDESTAL FOR MOUNTING THESE ITEMS IS REQUIRED. (SEE TSD-6 &amp; TSD12).</p> <p>21. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MAKE THE NECESSARY ARRANGEMENTS WITH THE LOCAL POWER COMPANY TO PROVIDE THE POWER SUPPLY ASSEMBLY FOR THE PEDESTAL. THE CONTRACTOR SHALL PAY FOR THE OTHER SERVICE FEES REQUIRED BY THE POWER COMPANY FOR THE ESTABLISHMENT OF NEW SERVICE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF THE MONTHLY SERVICE BILL DURING THE LIFE OF THE PROJECT. THE COST OF ALL SUCH FEES SHALL BE CONSIDERED INCIDENTAL AND ABSORBED WITHIN EXISTING PAY ITEMS. THE DEPARTMENT OR THE LOCAL AGENCY WILL BE RESPONSIBLE FOR PAYMENT OF THE MONTHLY SERVICE BILL FOR THE NEW POWER SERVICE INSTALLATION AFTER THE PROJECT COMPLETION. THE CONTRACTOR SHALL OVER TO THE DEPARTMENT AFTER PROJECT ACCEPTANCE. WHEN ELECTRIC POWER SERVICE EXISTS AND IS USED FOR THE OPERATION OF AN EXISTING SYSTEM, THE MONTHLY SERVICE FEES SHALL CONTINUE TO BE PAID BY THE DEPARTMENT OR THE LOCAL AGENCY. IF THE EXISTING POWER SERVICE IS INTENDED FOR USE WITH A NEW SIGNAL SYSTEM, THEN ANY SERVICE CHARGE FEES</p> <p>22. WHEN CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING TEMPORARY SIGNALS TO ACCOMMODATE ROADWAY CONSTRUCTION. IT SHALL BE PAID FOR UNDER PAY ITEM 616H-1, TRAFFIC SIGNAL LUMP SUM, UNLESS OTHERWISE NOTED ON PLANS.</p> <p>23. VEHICLE LOOP ASSEMBLIES SHALL BE INSTALLED IN THE TOP LAYER OF BITUMEN OR EXISTING SURFACE BEFORE THE FINAL SURFACE COURSE IS APPLIED (BASED ON 2" FINAL LIFT MAXIMUM).</p> <p>24. WHEN RADAR VIDEO OR MULTI SENSOR DETECTION IS USED, THE SYSTEM MAY BE INSTALLED IN THE BAR AND ADVANCE DETECTION. TSI PLANS SHOW A GENERIC LAYOUT FOR DETECTION DETECTOR MAY BE RELOCATED PER MANUFACTURER'S RECOMMENDATIONS. NO EXTRA PAY FOR MOVING OF DETECTORS, MANUFACTURER TO HAVE FACTORY REP ON SITE DURING INSTALLATIONS UNLESS CERTIFIED BY THE MANUFACTURER. THERE SHOULD BE NO EXTRA PAY FOR MOVING OF DETECTORS. DETECTION CABLE WILL BE MEASURED BY THE LINEAR FOOT, MEASURED HORIZONTALLY ALONG THE CONDUIT, MESSENGER CABLE OR MAST ARM AND VERTICALLY ALONG THE POLE DETECTION CABLE FOR CAMERAS, THE POWER AND ALL DETECTION UNITS SHALL BE NETWORKABLE DEVICES AND BE ON THE MDOOT NETWORK IF NOTED ON PLANS.</p> <p>25. ALL GROUNDING EQUIPMENT SHALL BE COST ABSORBED.</p> <p>26. MESSENGER CABLE AND OTHER SUPPORTING DEVICES WHERE REQUIRED SHALL BE ABSORBED IN THE PAY ITEMS FOR SIGNAL CABLE.</p> <p>27. THE CONTRACTOR SHALL STAKE THE LOCATION OF EACH POLE FOUNDATION AND NOTIFY THE PROJECT ENGINEER FOR CONCURRENCE IN THE LOCATION BEFORE PROCEEDING WITH THE PURCHASE OF THE POLE.</p> <p>28. THE CONTRACTOR SHALL BE REQUIRED TO ADEQUATELY AND COMPLETELY COVER TRAFFIC SIGNAL HEADS DURING TIMES THAT THEY ARE NOT IN OPERATION WITH A DURABLE, OUTDOOR-HARDENED MATERIAL THAT ORIGINALLY COLORED TO MATCH THE HEAD COLORED MATERIAL. THE SIGNAL IS NOT IN "STOP AND GO" MODE. HEAD COVERS ARE TO BE APPROVED BY THE ENGINEER.</p> <p>29. A NEW TRAFFIC SIGNAL INSTALLATION SHALL BE PUT IN FLASH OPERATION FOR A PERIOD OF THREE (3) TO SEVEN (7) DAYS PRIOR TO THE ACTIVATION OF THE SIGNALS "STOP AND GO" OPERATION. ACTIVATION OF NEW TRAFFIC SIGNALS SHALL BE DURING A MID-WEEK WEEKEND (TUESDAY - THURSDAY) DURING A NON-PEAK TIME AND SHALL BE COORDINATED WITH THE ENGINEER. UPON INITIAL INSPECTION, THE CONTRACTOR SHALL BE TESTING OF THE FIRST OF THE 30 DAY BURIN PERIOD TO COMMENCE. AS OUTLINED IN SUBSECTION 63.08.4 OF THIS SPECIFICATION, ANY NOTED DEFICIENCIES FOUND WITHIN THAT 30 DAY PERIOD SHALL BE CORRECTED TO THE SATISFACTION OF THE ENGINEER. THE 30 DAY BURIN PERIOD MUST COMMENCE WITHIN THE CONTRACT TIME, AND BEFORE SUBSTANTIAL COMPLETION OF THE PROJECT IS GRANTED.</p> <p>30. CONTRACTOR IS RESPONSIBLE FOR SCHEDULING FINAL INSPECTION MEETING WITH DISTRICT OFFICE, PROJECT OFFICE AND TRAFFIC ENGINEERING FOR SIGNAL PORTION OF THE PROJECT.</p>			

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TRAFFIC SIGNAL GENERAL NOTES	
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