

STATE	PROJECT NUMBER	SHEET NO.
MISSISSIPPI	STP-0037-01(062)	1

GENERAL INDEX

INCLUDED THIS PROJECT	BEGIN WITH SHEET
<input checked="" type="checkbox"/> ROADWAY	1
<input type="checkbox"/> PERMANENT SIGNS	1001
<input type="checkbox"/> TRAFFIC SIGNALS	2001
<input type="checkbox"/> ITS COMPONENTS	3001
<input type="checkbox"/> LIGHTING	4001
<input type="checkbox"/> (RESERVED)	5001
<input checked="" type="checkbox"/> ROADWAY STANDARD DWGS	6001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (LRFD)	7001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (STD. SPEC.)	7501
<input type="checkbox"/> BRIDGE	8001
<input type="checkbox"/> CROSS SECTIONS	9001

BRIDGE STRUCTURES REQ'D.

NONE

BOX BRIDGES REQ'D.

NONE

STATE OF MISSISSIPPI

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE OF PROPOSED STATE HIGHWAY FEDERAL AID PROJECT NO. STP-0037-01(062)

S.R. 13

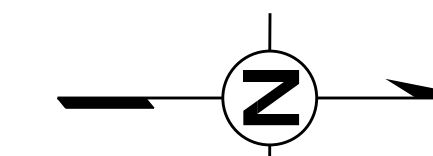
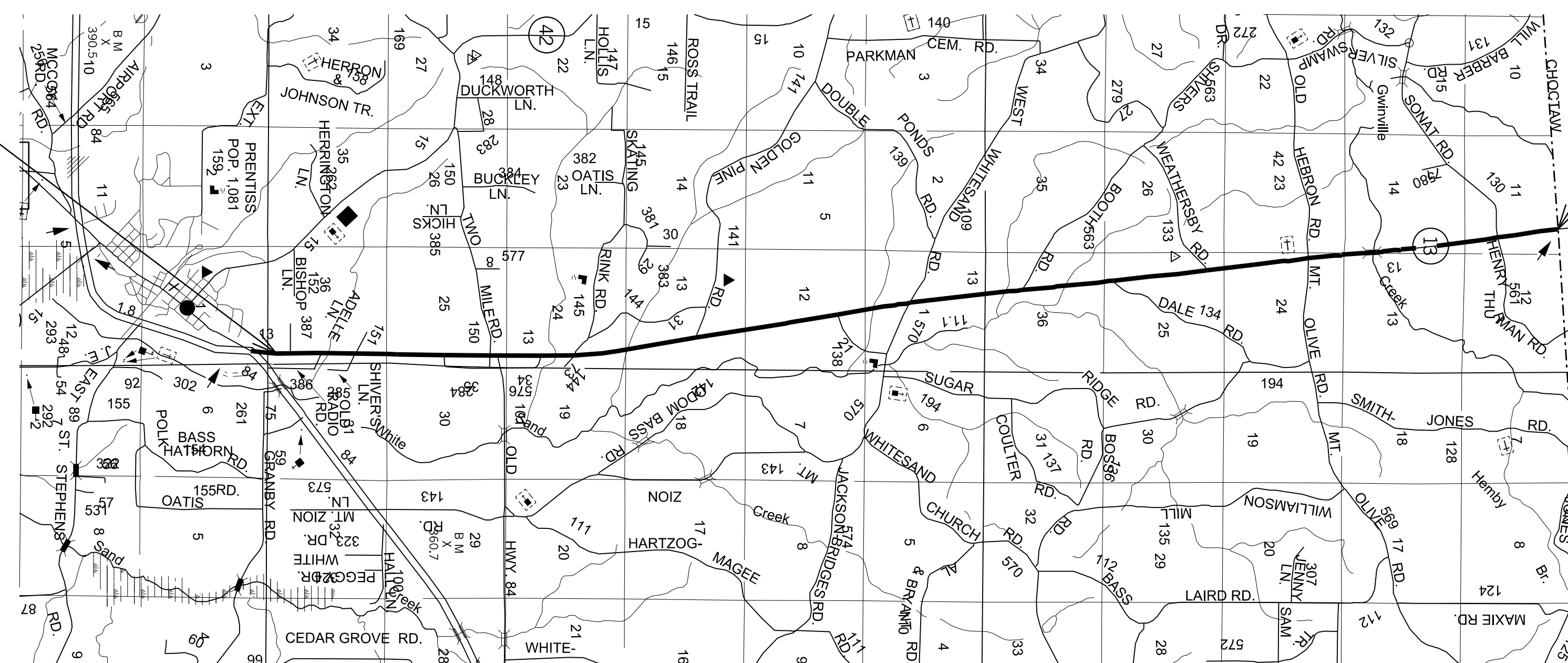
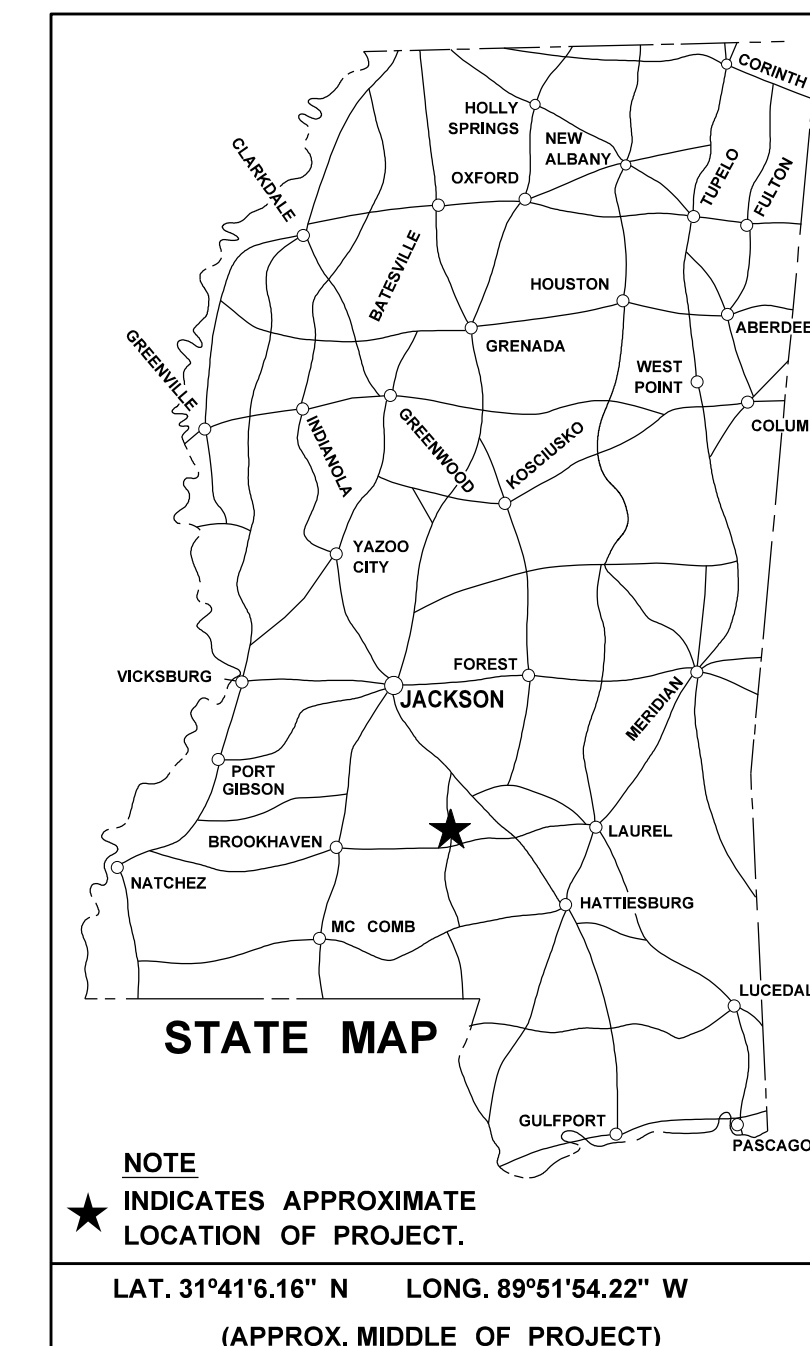
JCT US 84 NORTH 10.5 MILES TO SIMPSON CL

FMS CON. NO. 107801 / 301000

JEFFERSON DAVIS COUNTY

SCALES

PLAN	1 IN. = 100 FT.
PROFILE	HOR. 1 IN. = 100 FT.
	VERT. 1 IN. = 10 FT.
LAYOUT	1 IN. = 4,464 FT.



B.O.P. STA. 45 + 87

E.O.P. STA. 595 + 46

To Mendenhall

SIMPSON COUNTY

DESIGN CONTROL

MPH = V (SPEED DESIGN)

ADT () = : ADT () =

DHV = : D = % T = %

PERMITS ACQUIRED BY MDOT

WETLANDS AND WATERS PERMITS		
	WATERS	WETLANDS
NATIONWIDE #14	<input type="checkbox"/>	<input type="checkbox"/>
NATIONWIDE (OTHER)*	<input type="checkbox"/>	<input type="checkbox"/>
GENERAL*	<input type="checkbox"/>	<input type="checkbox"/>
INDIVIDUAL (404)*	<input type="checkbox"/>	<input type="checkbox"/>
STORMWATER PERMIT <input checked="" type="checkbox"/>		
Y	REQUIRED, CNOI SUBMITTED BY MDOT (DISTURBED AREA = 5 ACRES)	
S	REQUIRED, SCNOI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES)	
N	NO STORMWATER PERMIT REQUIRED (<1 ACRE)	
APPROVED BY: _____		

CONVENTIONAL SYMBOLS

COUNTY LINE	
TOWN CORPORATION LINE	
SECTION LINE	
EXISTING ROAD OR TRAVELED WAY	
PROPOSED ROAD OR TRAVELED WAY	
RAILROAD	
SURVEY LINE	
BRIDGES	

EQUATIONS

47 + 51.27 =	47 + 49.26 (2.01)
119 + 99.36 =	119 + 99.00 (0.36)
210 + 00.42 =	210 + 00.00 (0.42)
255 + 50.91 =	255 + 50.15 (0.76)
300 + 57.00 =	300 + 56.80 (0.20)
339 + 00.08 =	338 + 98.13 (1.95)

LENGTH DATA

LENGTH OF ROADWAY	54,539.20 FT.	10.329 MI.
LENGTH OF BRIDGES	325.00 FT.	0.062 MI.
LENGTH OF PROJECT (NET)	54,864.20 FT.	10.391 MI.
LENGTH OF EXCEPTIONS	FT.	MI.
LENGTH OF PROJECT (GROSS)	54,864.20 FT.	10.391 MI.

EXCEPTIONS

NONE

P S & E DATE: 11/07/18

APPROVED:

DEPUTY EXECUTIVE DIRECTOR / CHIEF ENGINEER

EXECUTIVE DIRECTOR



11/17/2018 8:02 AM (01) SRV-TITLE MISSISSIPPI DEPARTMENT OF TRANSPORTATION

STATE	PROJECT NO.
MISS.	STP-0037-01(062)

DESCRIPTION OF SHEET

WKG. NO. SH. NO.

DESCRIPTION OF SHEET

WKG. NO. SH. NO.

TITLE AND LAYOUT SHEET

1

- DETAILED INDEX
- GENERAL NOTES
- TYPICAL SECTION
- TYPICAL SECTION
- TYPICAL SECTION

- DI-1 2
- GN-1 3
- TS-1 4
- TS-2 5
- TS-3 6

QUANTITY SHEETS (3)

- SUMMARY OF QUANTITIES
- ESTIMATED QUANTITIES FOR ASPHALT, MILLING, GRANULAR MATERIAL
- ESTIMATED QUANTITIES FOR TRAFFIC CONTROL SIGNS

- SQ-1 7
- EQ-1 8
- TCPQ-1 9

PLAN PROFILE SHEETS (19)

- STA. 45+00 - STA. 75+00
- STA. 75+00 - STA. 105+00
- STA. 105+00 - STA. 135+00
- STA. 135+00 - STA. 165+00
- STA. 165+00 - STA. 195+00
- STA. 195+00 - STA. 225+00
- STA. 225+00 - STA. 255+00
- STA. 255+00 - STA. 285+00
- STA. 285+00 - STA. 315+00
- STA. 315+00 - STA. 345+00
- STA. 345+00 - STA. 375+00
- STA. 375+00 - STA. 405+00
- STA. 405+00 - STA. 435+00
- STA. 435+00 - STA. 465+00
- STA. 465+00 - STA. 495+00
- STA. 495+00 - STA. 525+00
- STA. 525+00 - STA. 555+00
- STA. 555+00 - STA. 585+00
- STA. 585+00 - STA. 595+45.40

- WK3 10
- WK4 11
- WK5 12
- WK6 13
- WK7 14
- WK8 15
- WK9 16
- WK10 17
- WK11 18
- WK12 19
- WK13 20
- WK14 21
- WK15 22
- WK16 23
- WK17 24
- WK18 25
- WK19 26
- WK20 27
- WK21 28

SPECIAL DESIGN SHEETS (2)

- DETAILED CONSTRUCTION SIGNING
- GUARDRAIL TYPICAL INSTALLATION AT BRIDGE APPROACHES FOR 2-LANE, 2-WAY HIGHWAYS

- DCS-1 29
- GR-4A-MOD 30

STANDARD DESIGN SHEETS (14)

- PAVEMENT MARKING DETAILS FOR 2-LANE AND 4-LANE DIVIDED HIGHWAYS
- PAVEMENT MARKING LEGEND DETAILS
- 4-LANE TO 2-LANE TRANSITION AT INTERCHANGE
- 2-WAY RAISED PAVEMENT MARKERS AT INTERSECTING ROADS (2 LANE)
- RUMBLE STRIPES 2-LANE HIGHWAYS (ASPHALT LANES, 2-FT ASPHALT SHOULDERS)
- TRAFFIC CONTROL PLAN WITH FLAGGER
- SHORT DURATION CLOSING OF TWO-LANE TWO-WAY HIGHWAYS
- HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS
- TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTI LANE ROADS AND TWO-LANE ROADS
- TRAFFIC CONTROL PLAN UNEVEN PAVEMENT DETAILS
- TRAFFIC CONTROL STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS
- LOCATION OF R16-3 SIGNS
- TRAFFIC CONTROL DETAILS DRUM PLACEMENT AND SHOULDER CLOSURE
- RURAL DRIVEWAYS

- PM-1 6051
- PM-6 6056
- PM-8 6058
- PM-11 6061
- RS-1 6064
- TCP-1 6351
- TCP-6 6356
- TCP-8 6358
- TCP-9 6359
- TCP-12 6362
- TCP-13 6363
- TCP-15 6365
- TCP-16 6366
- RD-1 6403

TOTAL SHEETS: 44

11/1/2018 08:28:25 (02)DETAIL INDEX.DGN

DISTRICT 7		
PS & E PLANS-DATE 11/07/18		
FMS CON. # 107801/301000		
REVISIONS		
DATE	SHEET NO.	BY

REVISION	MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
	DETAILED INDEX	
	PROJ. NO.: STP-0037-01(062) COUNTY: JEFFERSON_DAVIS	
	WORKING NUMBER	DI-1
DATE	FILENAME: (02)DETAIL INDEX	SHEET NUMBER
DESIGN TEAM	DISTRICT 7	CHECKED DATE
		2



STATE	PROJECT NO.
MISS.	STP-0037-01(062)


GENERAL NOTES

- ① THE LOCATION AND SPACING OF SIGNS AS SHOWN ON THE TRAFFIC CONTROL PLANS ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.
- ② ALL TRAFFIC CONTROL DEVICES ON THIS PROJECT SHALL COMPLY WITH PART 6 OF THE MUTCD (LATEST EDITION).
- ③ FLUORESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.
- ④ ALL PLASTIC DRUMS SHALL HAVE A BALLASTING COLLAR MADE FROM RECYCLED TRUCK TIRES OR SUITABLE MATERIAL.
- ⑤ ALL SIGNS AND DELINEATORS THAT CONFLICT WITH THE CONSTRUCTION OF THIS PROJECT SHALL BE REMOVED AND RESET BY THE CONTRACTOR; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ⑥ THE GRASS ON EXISTING SHOULDERS SHALL BE REMOVED PRIOR TO THE PLACEMENT OF SHOULDER MATERIAL BY LIGHTLY BLADING OR CLOSELY MOWING; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ⑦ WHERE MILLING OF THE ROADWAY IS REQUIRED, THE CONTRACTOR SHALL PROVIDE OUTLETS IN THE EXISTING SHOULDER AT SUFFICIENT INTERVALS TO PREVENT POOLING OR STANDING WATER ON MILLED SURFACE; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ⑧ IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT EXISTING STRUCTURES SUCH AS PIPES, INLETS, APRONS, BRIDGES, ETC., FROM DAMAGE WHICH MIGHT OCCUR DURING CONSTRUCTION. THE CONTRACTOR SHALL REPLACE OR REPAIR, AS DIRECTED BY THE ENGINEER, ANY STRUCTURES DAMAGED DURING THE LIFE OF THE CONTRACT. NO PAYMENT WILL BE MADE FOR REPLACEMENT OR REPAIR OF DAMAGED ITEMS.
- ⑨ THE CONTRACTOR SHALL REMOVE EXISTING PAVEMENT MARKERS PRIOR TO PLACING ASPHALT; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ⑩ TEMPORARY STRIPING SHALL BE REQUIRED AFTER MILLING, SCRUB SEAL, PRELIMINARY LEVELING, AND OVERLAYING OPERATIONS; WEATHER PERMITTED OR PRIOR TO OPENING THE AREA TO TRAFFIC; TEMPORARY STRIPING SHALL BE PLACED IN THE SAME LOCATIONS AND LAYOUT AS PERMANENT STRIPE.
- ⑪ ALL ASPHALT AND CONCRETE CURBS ALONG RAMPS, LOCAL ROADS, ETC. FROM B.O.P. TO E.O.P. SHALL BE PAINTED (TWO APPLICATIONS) WITH WHITE TRAFFIC PAINT AND TRAFFIC BEADS ; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ⑫ IF THE ASPHALT CURB ALONG THE LOCAL ROAD IS SEVERELY DAMAGED, THE ENTIRE ASPHALT CURB WILL BE REMOVED AS DIRECTED BY THE ENGINEER; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ⑬ ALL LOCAL ROADS SHALL BE PAVED TO THE R.O.W. LIMITS OR AS DIRECTED BY THE ENGINEER.
- ⑭ ALL ADDENDA FOR THIS PROJECT WILL BE POSTED ON WWW.MDOT.MS.GOV UNDER THE PROPOSAL ADDENDA COLUMN. BIDDERS ARE ADVISED THAT HARD COPIES OF ANY ADDENDA FOR THIS PROJECT WILL NOT BE MAILED. IT'S THE BIDDERS'S RESPONSIBILITY TO CHECK AND SEE IF ANY ADDENDA HAVE BEEN POSTED FOR THIS PROJECT.
- ⑮ GRANULAR MATERIAL WILL NOT BE ALLOWED TO BE PLACED DIRECTLY ON THE SURFACE LIFT OF ASPHALT, BUT MUST BE PLACED DIRECTLY ON THE GRAVEL SHOULDER OR A ROAD WIDENER MACHINE USED AND APPROVED BY THE PROJECT ENGINEER.
- ⑯ STORAGE OF FLAMMABLE MATERIALS WILL NOT BE ALLOWED UNDER ANY BRIDGE STRUCTURES.

GENERAL NOTES (CONT.)

- ⑰ INSTALLATION DATES SHALL BE CLEARLY WRITTEN IN BOLD BLACK MARKINGS ON THE BACK BOTTOM HALF OF ALL SIGNS WITH A PERMANENT MARKING STICK THAT IS WATERPROOF, FADE RESISTANT AND MARKS ON WET OR DRY SURFACES.
- ⑱ ALL POST, PIPE, AND I-BEAM LENGTHS IN THESE PLANS ARE ESTIMATES. POST LENGTHS FOR ALL SIGNS SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO FABRICATION.
- ⑲ ALL EXISTING SIGNS WHICH ARE TO BE REMOVED AS A PART OF THIS PROJECT THAT ARE NOT IN CONFLICT WITH CONSTRUCTION SHALL REMAIN IN PLACE UNTIL NEW SIGNS ARE INSTALLED UNLESS NOTED OR DIRECTED OTHERWISE BY THE PROJECT ENGINEER. ROADWAY SIGNS THAT ARE IN CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND RELOCATED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
- ⑳ ALL EXISTING SIGNS AND SUPPORTS REMOVED UNDER THIS PROJECT SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND ARE NOT A SEPARATE PAY ITEM.
- ㉑ DIRECT-APPLIED LEGEND, BORDER, AND/OR SHIELDS ARE TO BE USED ON ALL GUIDE SIGNS. DIGITALLY PRODUCED SIGN COPY, SHIELDS, LEGEND, SYMBOLS, OR IMAGES WILL NOT BE ALLOWED WITHOUT WRITTEN APPROVAL FROM MDOT'S PROJECT ENGINEER.
- ㉒ ALL RAMPS AND DRIVEWAY PADS SHALL BE PAVED TO THE SHOULDER LINE, MINIMUM OR FURTHER, AS DIRECTED BY THE ENGINEER WITHIN THE R.O.W. LIMITS.
- ㉓ AFTER THE PERMANENT SIGNS HAVE BEEN INSTALLED, THE CONTRACTOR SHALL SUBMIT TO THE PROJECT ENGINEER A DIGITAL COPY OF A MICROSOFT EXCEL SPREADSHEET WITH THE FOLLOWING INVENTORY DATA CAPTURED FOR EACH SIGN: LOCATION OF SIGN (LATITUDE - LONGITUDE GPS COORDINATES), MUTCD SIGN CODE, SIZE, BACKGROUND AND LEGEND COLORS, SUPPORT TYPE (POST, PIPE, SQUARE POST, OR I-BEAM), NUMBER OF SUPPORTS, DATE INSTALLED, SIGN FACE DIRECTION, ROUTE NAME OR NUMBER, DIRECTION OF VEHICLE TRAVEL, AND LEGEND ON SIGN IF APPLICABLE. EACH SIGN SHALL BE ASSIGNED A UNIQUE ID NUMBER AND A DIGITAL PHOTO OF EACH SIGN SHALL BE SUBMITTED IN BITMAP FORMAT. THE PHOTO FILE NAME SHALL CORRESPOND WITH THE UNIQUE ID NUMBER.
- ㉔ THE ASPHALT PAVING /SCRUB SEAL OPERATION SHALL BE COMPLETED IN THE FOLLOWING ORDER:
 A: PRE-GRINDING
 B: SCRUB SEAL
 C: PRE-LEVELING
 D: 1.50" 9.5MM SURFACE COURSE
- ㉕ IF DURING THE WIDENING PROCESS A DRIVEWAY RAMP IS ENCOUNTERED WHICH HAS AN ELEVATION OR INTERSECTING SLOPE THAT WILL CONFLICT WITH THE DESIRED TRANSVERSE CROSS SLOPE OF THE MAINLINE PAVING, THE DRIVEWAY WILL BE REMOVED AND RECONSTRUCTED AS DIRECTED BY THE ENGINEERS SO AS NOT TO INTERFERE WITH THE MAINLINE CROSS SLOPE PAVING. PAYMENT FOR THE DRIVEWAY REMOVAL/RECONSTRUCTION WORK SHALL BE ABSORBED IN OTHER ITEMS BID ON THIS PROJECT. REPAVING OF THE DRIVEWAY RAMPS TO REPLACE ANY PAVEMENT REMOVED WILL BE PAID FOR UNDER THE PAY ITEM FOR ASPHALT.

11/1/2018 08:29:39 (03)SRV-GN.DGN

		MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
		GENERAL NOTES	
		PROJ. NO.: STP-0037-01(062) COUNTY: JEFFERSON_DAVIS	
			
		FILENAME: (03)SRV-GN DESIGN TEAM DISTRICT 7 CHECKED DATE	
		WORKING NUMBER GN-1 SHEET NUMBER 3	