



SM No. CMP5011380101

PROPOSAL AND CONTRACT DOCUMENTS

FOR THE CONSTRUCTION OF

16

Mill & Overlay approximately 11 miles of US 11 from Russell to Alabama State Line, known as State Project No. MP-5011-38(010) / 306547301 in Lauderdale County.

Project Completion: 137 Working Days

(STATE DELEGATED)

NOTICE

**BIDDERS MUST COMPLETE AN ONLINE REQUEST
FOR PERMISSION TO BID THIS PROJECT.**

Electronic addendum updates will be posted on www.gomdot.com

SECTION 900

OF THE CURRENT 2017 STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION

JACKSON, MISSISSIPPI

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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PROJECT: MP-5011-38(010)/306547301 - Lauderdale

(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

03/27/2019 05:53 PM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 901 - ADVERTISEMENT

Electronic bids will be received by the Mississippi Transportation Commission at 10:00 o'clock A.M., Tuesday, April 23, 2019, from the Bid Express Service and shortly thereafter publicly read on the Sixth Floor for:

Mill & Overlay approximately 11 miles of US 11 from Russell to Alabama State Line, known as State Project No. MP-5011-38(010) / 306547301 in Lauderdale County.

The attention of bidders is directed to the predetermined minimum wage rate set by the U. S. Department of Labor under the Fair Labor Standards Act.

The Mississippi Department of Transportation hereby notifies all bidders that it will affirmatively insure that in any contract entered into pursuant to this advertisement, disadvantaged business enterprises will be afforded full opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of race, color, sex, age, disability, religion or national origin in consideration for an award.

The specifications are on file in the offices of the Mississippi Department of Transportation.

Contractors may request permission to bid online at <http://shopmdot.ms.gov> at no cost. Upon approval, Contractors shall be eligible to submit a bid using Bid Express at <http://bidx.com>. Specimen proposals may be viewed and downloaded online at no cost at <http://mdot.ms.gov> or purchased online at <http://shopmdot.ms.gov> at a cost of Ten Dollars (\$10.00) per proposal plus a small convenience fee. Cash or checks will not be accepted as payment.

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

MELINDA L. MCGRATH
EXECUTIVE DIRECTOR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Governing Specifications

The current (2017) Edition of the Standard Specifications for Road and Bridge Construction adopted by the Mississippi Transportation Commission is made a part hereof fully and completely as if it were attached hereto, except where superseded by special provisions, or amended by revisions of the Specifications contained within this proposal. Copies of the specification book may be purchased from the MDOT Construction Division, or online at shopmdot/default.aspx?StoreIndex=1.

A reference in any contract document to controlling requirements in another portion of the contract documents shall be understood to apply equally to any revision or amendment thereof included in the contract.

In the event the plans or proposal contain references to the 2004 Edition of the Standard Specifications for Road and Bridge Construction, it is to be understood that such references shall mean the comparable provisions of the 2017 Edition of the Standard Specifications.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 3

CODE: (SP)

DATE: 01/17/2017

SUBJECT: Final Clean-Up

Immediately prior to final inspection for release of maintenance, the Contractor shall pick up, load, transport and properly dispose of all litter from the entire highway right-of-way that is within the termini of the project.

Litter shall include, but not be limited to, solid wastes such as glass, paper products, tires, wood products, metal, synthetic materials and other miscellaneous debris.

Litter removal is considered incidental to other items of work and will not be measured for separate payment.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 9

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Federal Bridge Formula

Bidders are hereby advised that the latest revision of Federal Highway Administration Publication No. FHWA-HOP-06-105, **BRIDGE FORMULA WEIGHTS**, dated August 2006, is made a part of this contract when applicable.

Prior to the preconstruction conference, the Contractor shall advise the Engineer, in writing, what materials, if any, will be delivered to the jobsite via Interstate route(s).

Copies of the **BRIDGE FORMULA WEIGHTS** publication may be obtained by contacting:

Federal Highway Administration
400 7th Street, SW
Washington, DC 20590
(202) 366-2212

or

http://www.ops.fhwa.dot.gov/Freight/publications/brdg_frm_wgths/bridge_formula_all_rev.pdf

An on line **BRIDGE FORMULA WEIGHTS CALCULATOR** is available at

http://ops.fhwa.dot.gov/freight/sw/brdgcalt/calc_page.htm

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 13

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Safety Edge

Bidders are hereby advised that the Shoulder Wedge (Safety Edge) specified in Section 401, Asphalt Pavements, shall only apply to the top two (2) lifts of asphalt. Open Graded Friction Courses (OGFC) are not to be considered a lift as it pertains to safety edge. Attached is a drawing showing the safety edge. Note that the shoulder dimensions in the bottom two drawings will be less than three feet (3').



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SUPPLEMENT TO NOTICE TO BIDDERS NO. 14

DATE: 5/30/2018

PROJECT: MP-5011-38(010) / 306547301 -- Lauderdale County

After the second paragraph on page 1, add the following:

Name Insured: *Meridian Speedway, LLC.*

Site 1: Repair of guardrail and paving 5.33 miles west of the intersection of US Hwy 11/80 and Interstate 59 near Toomsuba.

Description and Designation: *Bridge Repair on SR 19.*

Mile Post: *1,750± Feet North of I-20 in Meridian, Mississippi.*

Site 2: Milling and paving of location road intersection 0.81 miles east of the intersection of US Hwy 11/80 and Interstate 59 near Toomsuba.

Description and Designation: *Bridge Repair on SR 19.*

Mile Post: *1,750± Feet North of I-20 in Meridian, Mississippi.*

After the fourth paragraph on page 1, add the following:

Sri Honnur, P.E.

Track and Bridge Construction Director

The Kansas City Southern Railway Co.

421 West 12th Street,

Kansas City, MO 64105

Ph: 816-983-1138

Shonnur@KCSouthern.com

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 14

CODE: (SP)

DATE: 01/17/2017

SUBJECT: Railway-Highway Provisions

Prior to bidding, the Contractor shall contact the Railroad concerning insurance coverage required for this project. In case the railroad requires coverage over and above that required by the Standard Specifications, the railroad requirements shall be met.

The name insured, description of the work and designation of the job site to be shown on the Policy are as follows:

Notice of starting to work, completion of any required forms, and correspondence pertaining to railroad liability insurance shall be directed to the person below.

The Contractor shall not commence, or carry on, any work for installation, maintenance, repair, changing or renewal of any FACILITY, under, over or on RAILROAD property at any location without giving at least ten (10) working days prior notice to the RAILROAD authorized representative at the RAILROAD's office(s) below.

If in the opinion of the RAILROAD, the presence of an authorized representative of the RAILROAD is required to supervise the same, the RAILROAD shall render bills to the Contractor for all expenses incurred by it for such supervision. This includes all labor costs for flagmen or cable locate supplied by the RAILROAD to protect RAILROAD operation, and for the full cost of furnishing, installation and later removal of any temporary supports for said tracks, as the RAILROAD's Chief Engineer's Office may deem necessary.

It will be the Contractor's responsibility to pay all bills associated with railroad flagging and cable locating. Generally, the flagging rate is \$700.00 per day (1 to 8 hours) plus overtime at \$125.00 per hour, however, the Contractor shall contact the RAILROAD to verify all rates.

A flagman is required anytime a Contractor does any work on or near RAILROAD property within twenty-five (25) feet horizontally of the centerline or any work over any railroad track. The RAILROAD, however, also reserves the right to require a flagman for work on RAILROAD property, which is more than twenty-five (25) feet from the centerline of a railroad track when there are other conditions or considerations that would dictate the need for a flagman to safeguard the RAILROAD's operations, property and safety of working personnel.

A cable locate of RAILROAD owned facilities may be required to identify and protect Signal & Communication cables that have been installed to provide power, signal control, wayside communications. These cables are vital to a safe and reliable railway operation. The cable locate will be performed by a qualified RAILROAD employee.

Outside Contractors are prohibited from driving on, along, or across any track that does not have a RAILROAD installed crossing. They may utilize an existing public crossing. The practice of allowing rubber tired equipment to operate over track with no crossing has been banned.

Exceptions to this rule will require the express approval from the RAILROAD Engineers.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 113

CODE: (SP)

DATE: 04/18/2017

SUBJECT: Tack Coat

Bidders are advised that in addition to the products listed on the Department's APL as referenced in Subsection 401.03.1.2 on page 256, the Contractor may use one of the following as a tack coat.

- CSS-1
- CSS-1h
- SS-1
- SS-1h

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 246

CODE: (SP)

DATE: 06/27/2017

SUBJECT: Kansas City Southern Construction Requirements

Bidders are hereby advised that provisions which are required as per the Notice to Bidders entitled “Railway-Highway Provisions” shall also include the following.

The Contractor shall submit to the Project Engineer and the Railroad detailed plans and design data for temporary construction clearances, stages of construction, erection plans, demolition plans, false-work plans, excavation plans, and temporary shoring plans and calculations, as required, and shall be sealed by a Mississippi Registered Professional Engineer. All submittals must be approved by the Railroad before excavation or construction can begin within Railroad Right-of-Way. All construction submittals for work performed within the Kansas City Southern Railway Company (KCS) right-of-way shall be made per the KCS “Guidelines for the Design and Construction of Railroad Overpasses and Underpasses” as updated in May 2008.

Prior to beginning any work on the KCS right-of-way, the Contractor shall obtain a Right of Entry Permit. To request a permit application, the Contractor should contact Sylvia Schmidt. Mrs. Schmidt’s contact information is as follows.

Sylvia Schmidt
Permit Manager
Jones Lang LaSalle Americas, Inc.
3017 Lou Menk Drive, Suite 100
Fort Worth, Texas 76131-2800
817-230-2688

The Contractor shall be responsible for payment of all application fees.

This project will require construction activities on the right-of-way of active railroad tracks which are currently owned and/or operated by KCS. When work requires that equipment or personnel be within the KCS right-of-way or the “foul zone” adjacent to the right-of-way, a qualified “Employee-in-Charge” (EIC) must be present for the purpose of providing on-track safety and flagging protection for the work crews. The EIC shall also be responsible for the coordination of the Contractor’s activities within the KCS right-of-way with the operation of the Railroad. The EIC must be certified under the KCS General Code of Operation Rules (GCOR) and must be approved by the local KCS Roadmaster prior to beginning work on the KCS right-of-way. The Contractor will be required to provide radios for the EIC, all equipment operators, supervisors, and foremen in charge of employees working within the KCS right-of-way. All personnel who must enter upon the KCS right-of-way must check in and out with the EIC and be logged in and out of the site.

All personnel who must work within the KCS right-of-way at any time shall be trained and certified as a KCS "Roadway Worker" and must at all times have their certification card with them and available for random inspection. The Contractor will be responsible for providing this training for Contractor employees or any subcontractor(s) employees. The Contractor shall contact Mr. Larry Slater of Track Sense Inc. at 330-847-8661 or 330-219-4721 (lslater@neo.rr.com) for approximate fees and scheduling the necessary training sessions. The Contractor shall also contact the MDOT Project Engineer to see if any MDOT employees need this training. If so, the Contractor shall include the MDOT employees in the list of participants for training. The Contractor shall bear the cost of training the MDOT employees. Costs for training the MDOT employees will be reimbursed to the Contractor by supplemental agreement.

Prior to commencing work, the Contractor shall provide to the Railroad Engineer or the Railroad Engineer's designated representative, [with copies to the Project Engineer](#), a detailed construction schedule for its work on Railroad's right-of-way, including the proposed temporary horizontal and vertical clearances and construction sequence for all work to be performed on Railroad right-of-way. This schedule shall also include the anticipated dates when the milestone events listed below will occur. The Contractor shall update the schedule for these milestone events as necessary, but at least monthly, and shall provide a copy of all updates to the Railroad so that site visits may be scheduled.

- Preconstruction meetings.
- Excavations, shoring placement/removal, pile driving, drilling of caissons or drilled shafts adjacent to tracks.
- Reinforcement and concrete placement for near track piers.
- Erection of precast concrete or steel overpass bridge superstructure.
- Reinforcement and concrete placement of overpass bridge decks.
- Completion of the bridge structure.

The Contractor shall so arrange and conduct construction operations in such a manner that there will be no interference with Railroad operations, including train, signal, telephone and telegraphic services, or damage to the property of the Railroad or to poles, cables or wires (whether overhead or underground) and other facilities or tenants on the rights-of-way of the Railroad. Before undertaking any work within Railroad right-of-way and before placing any obstruction over any track, the Contractor shall:

- Notify the Railroad's representative at least 72 hours in advance of the work.
- Provide assurance to the Railroad's representative that arrangements have been made for any required flagging service.
- Receive permission from the Railroad Engineer to proceed with the work.
- Ascertain that the Project Engineer has received copies of notice to the Railroad and the Railroad's response.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 296

CODE: (SP)

DATE: 07/25/2017

SUBJECT: Reduced Speed Limit Signs

Bidders are advised that when the plans or contract documents require the speed limit on a project to be reduced, the Contractor shall begin work within 48 hours of installing the reduced speed limit signs. Should the Contractor not start work or have no plans to start work within 48 hours of installing the signs, the reduced speed limit signs shall be covered and existing speed limit signs uncovered.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 401

CODE: (SP)

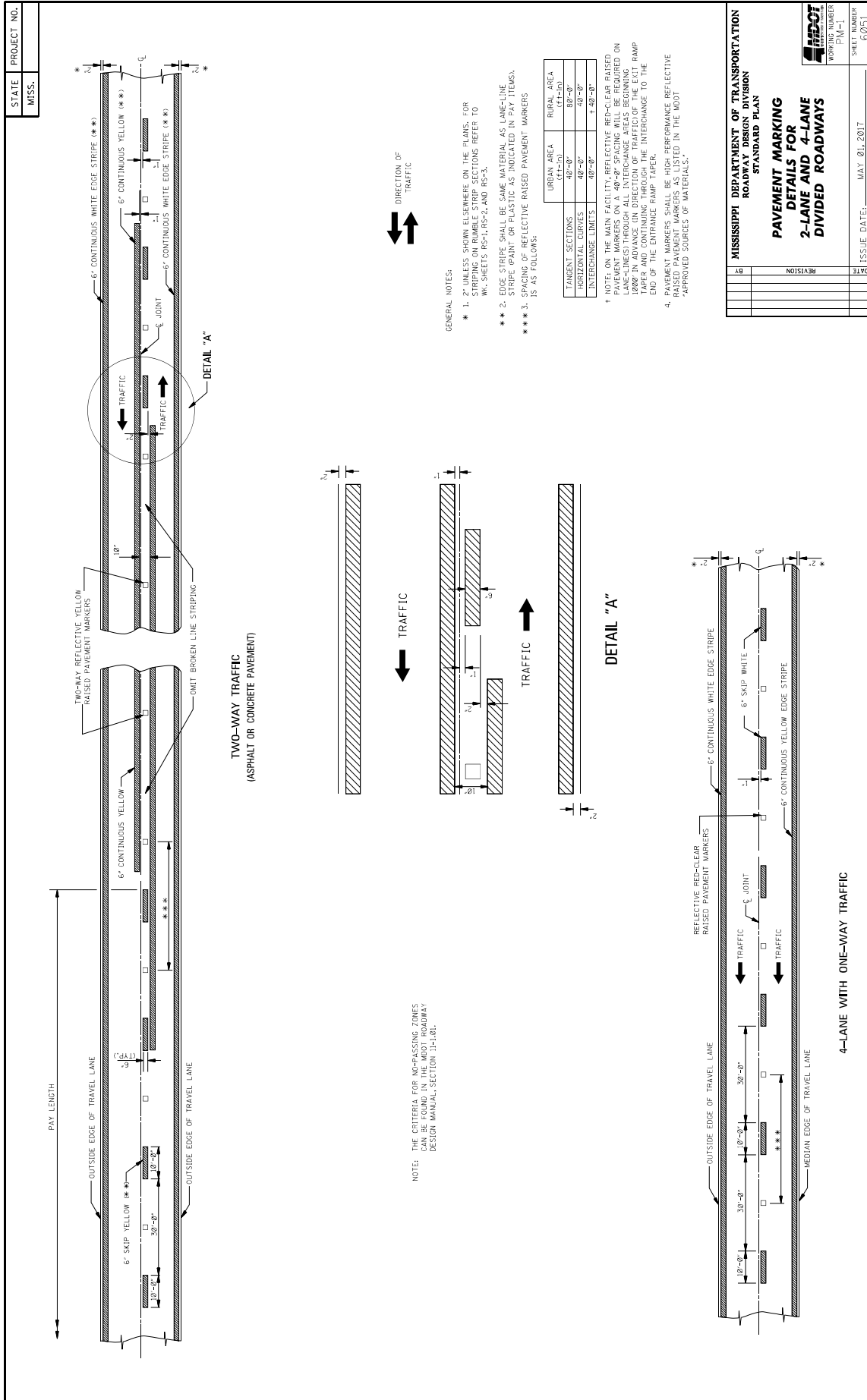
DATE: 09/12/2017

SUBJECT: Standard Drawings

Standard Drawings attached hereto shall govern appropriate items of required work.

Larger copies of Standard Drawings may be purchased from:

MDOT Plans Print Shop
MDOT Shop Complex, Building C, Room 114
2567 North West Street
P.O. Box 1850
Jackson, MS 39215-1850
Telephone: (601) 359-7460
or FAX: (601) 359-7461
or e-mail: plans@mdot.state.ms.us



STATE MISS.	PROJECT NO.	
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TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 3-LANE SECTION

TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION

TYPICAL STRIPING AND RAISED PAVEMENT MARKERS AT LEFT TURN LANES

* NOTE: USE DETAIL STRIPING IF LENGTH < 150' AT THIS LOCATION, OTHERWISE USE CONTINUOUS STRIPING.

TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION

TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 3-LANE SECTION

TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION

DETAIL A
LATERAL PLACEMENT OF PAVEMENT MARKERS

DETAIL B
LATERAL PLACEMENT OF PAVEMENT MARKERS

TYPICAL TWO-WAY ARROW INSTALLATION

NOTES:

- CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY.
- IF SEGMENT IS GREATER THAN 300' IN LENGTH, USE TWO-WAY ARROWS SET TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS (250' O.C.).
- IF SEGMENT IS GREATER THAN 300' IN LENGTH, USE TWO-WAY ARROWS SET TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS (250' O.C.).

GENERAL NOTE:

- PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MOST APPROVED SOURCE OF MATERIALS.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN**

**PAVEMENT MARKING
DETAILS FOR 3-LANE
4-LANE AND 3-LANE
UNDIVIDED ROADWAYS**

DATE: _____

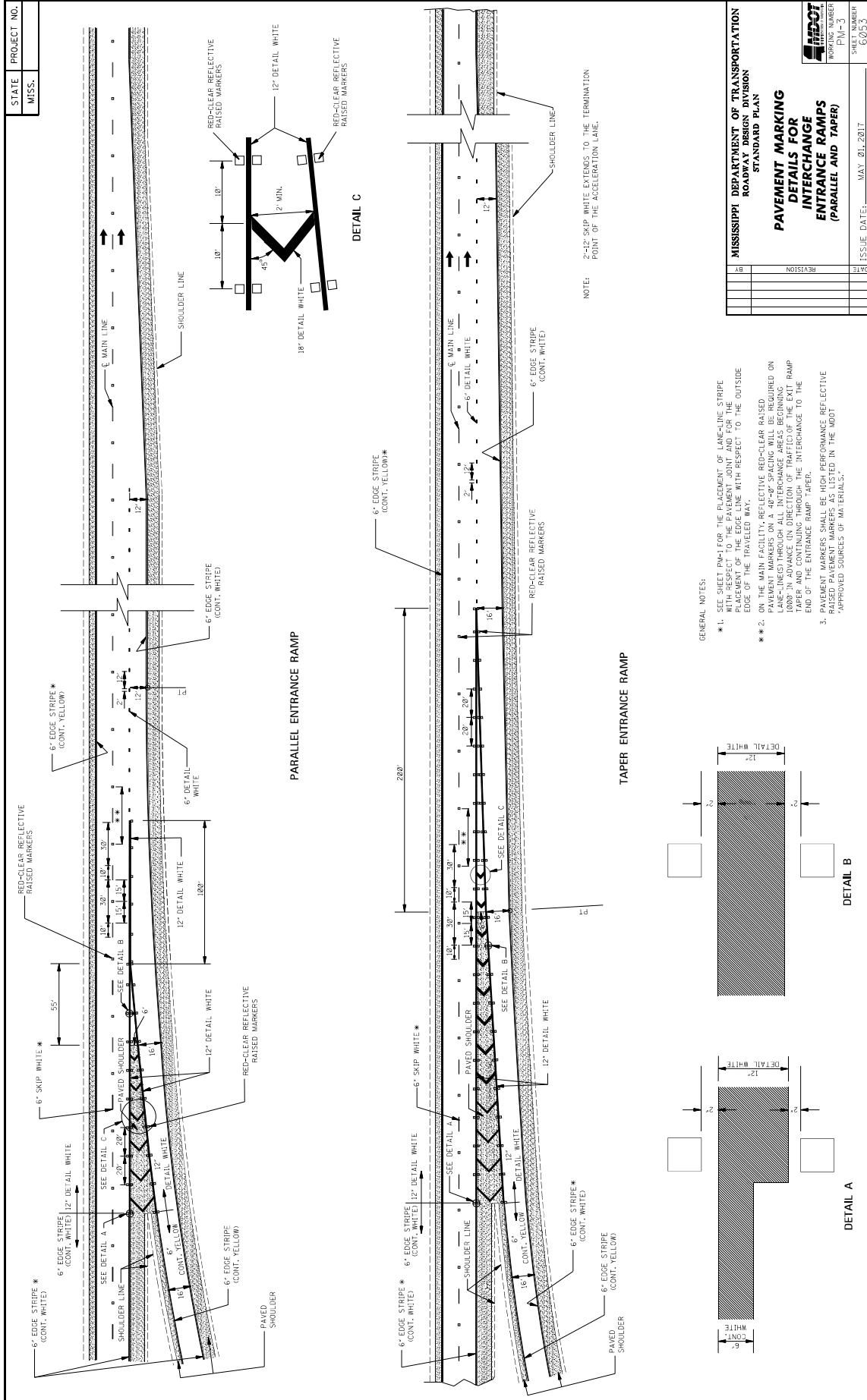
ISSUE DATE: MAY 21, 2017

PROJECT NO. _____

WORKING NUMBER: _____

SHEET NUMBER: _____

DATE: _____



DATE	REVISION

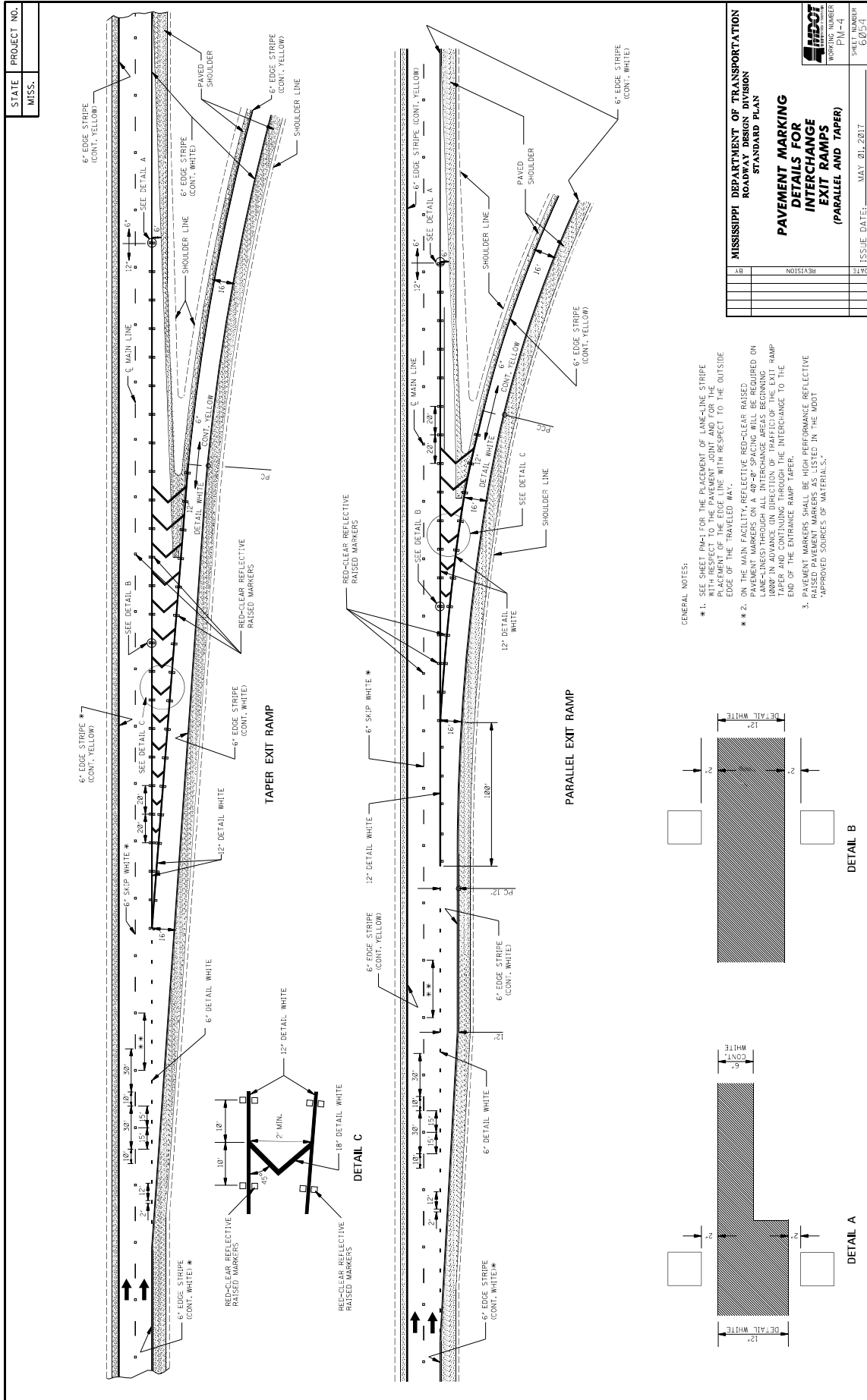
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
DETAILS FOR
INTERCHANGE
ENTRANCE RAMPS
(PARALLEL AND TAPER)**

MSDOT
WORKING NUMBER
PM-3

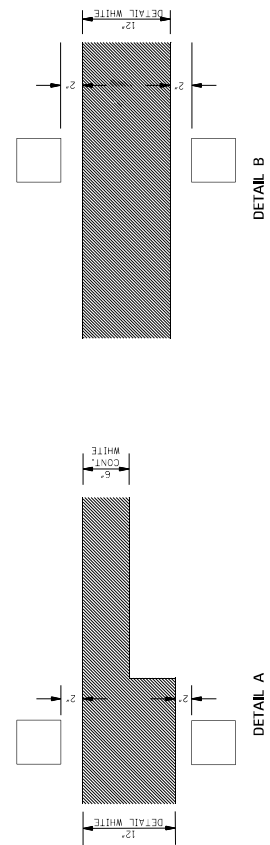
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60253

ISSUE DATE: MAY 21, 2017

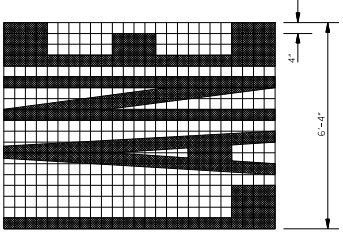
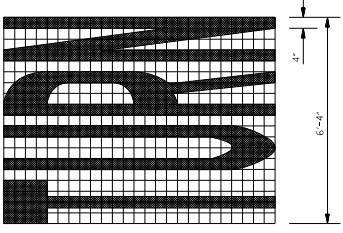
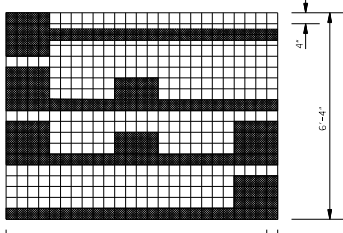
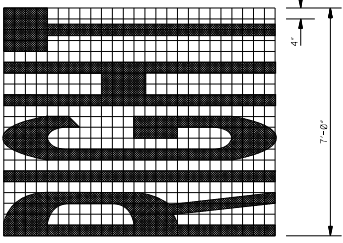
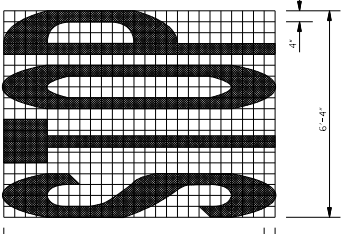


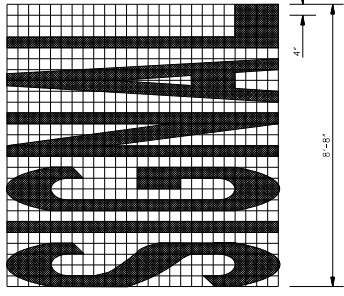
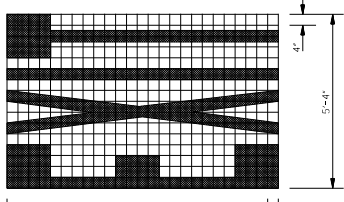
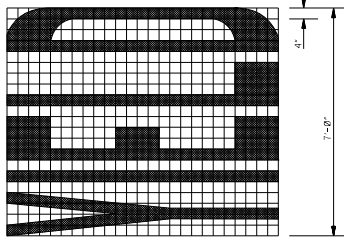
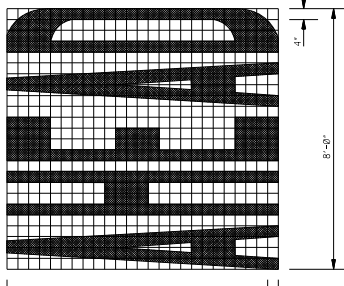
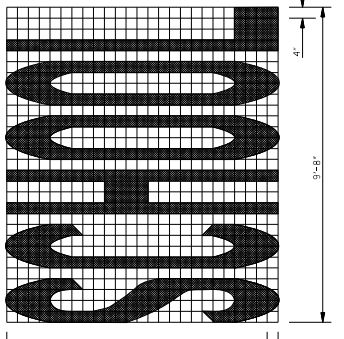
STATE PROJECT NO.		MISS.
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN		
PAVEMENT MARKING DETAILS FOR INTERCHANGE EXIT RAMP (PARALLEL AND TAPER)		
SHEET NUMBER		PM-4
WORKING NUMBER		6025-1
ISSUE DATE:		MAY 01, 2017
DATE	REVISION	

- GENERAL NOTES:
- ** 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF PAVED SHOULDER WITH RESPECT TO THE OUTSIDE EDGE OF MAIN CASTER REFLECTIVE BEAD-ON-GLASS RASSES.
 - ** 2. PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE IN DIRECTION OF TRAFFIC OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - 3. RAISED PAVEMENT MARKERS BE HIGH PERFORMANCE REFLECTIVE 'APPROVED SOURCES OF MATERIALS.'



STATE MISS.	PROJECT NO.		
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GENERAL NOTES:

- TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTIONS) OF 1/2" LESS THAN THE STENDING OR LESS THAN THE STENDING LETTER WIDTH ARE PERMITTED AT LETTER JOINTS.
- FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

LEGEND	AREA (FT ²)
STOP	24.6
RIGHT	28.6
LEFT	19.5
TRAFFIC	22.7
AHEAD	32.3
YIELD	26.8
EXIT	18.5
SIGNAL	32.5
SCHOOL	35.5

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
LEGEND DETAILS**

ISSUE DATE: MAY 01, 2017

WORKING NUMBER: PM-5
SHEET NUMBER: 60355

STATE MISS.	PROJECT NO.	
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THRU ARROW

TURN ARROW

LANE-REDUCTION ARROW

COMBINATION ARROW

ONLY

YIELD LINE

1-WAY ARROW

GENERAL NOTES:

- TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTORS OF 1/16" OR LESS AND EXTENDING THE FULL WIDTH) ARE PERMITTED IN EACH LETTER.
- FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- DIMENSIONS OF THE YIELD LINE MAY VARY WITH APPROVAL OF THE ENGINEER. SEE MUTCD, LATEST EDITION, FOR ALLOWABLE DIMENSIONS.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

PAY QUANTITIES	
LEGEND/SYMBOL	AREA (FT ²)
ONLY	22.0
TURN ARROW	16.4
THRU ARROW	12.3
COMB. ARROW	27.5
1-WAY ARROW	24.3
LANE REDUCTION ARROW	40.0

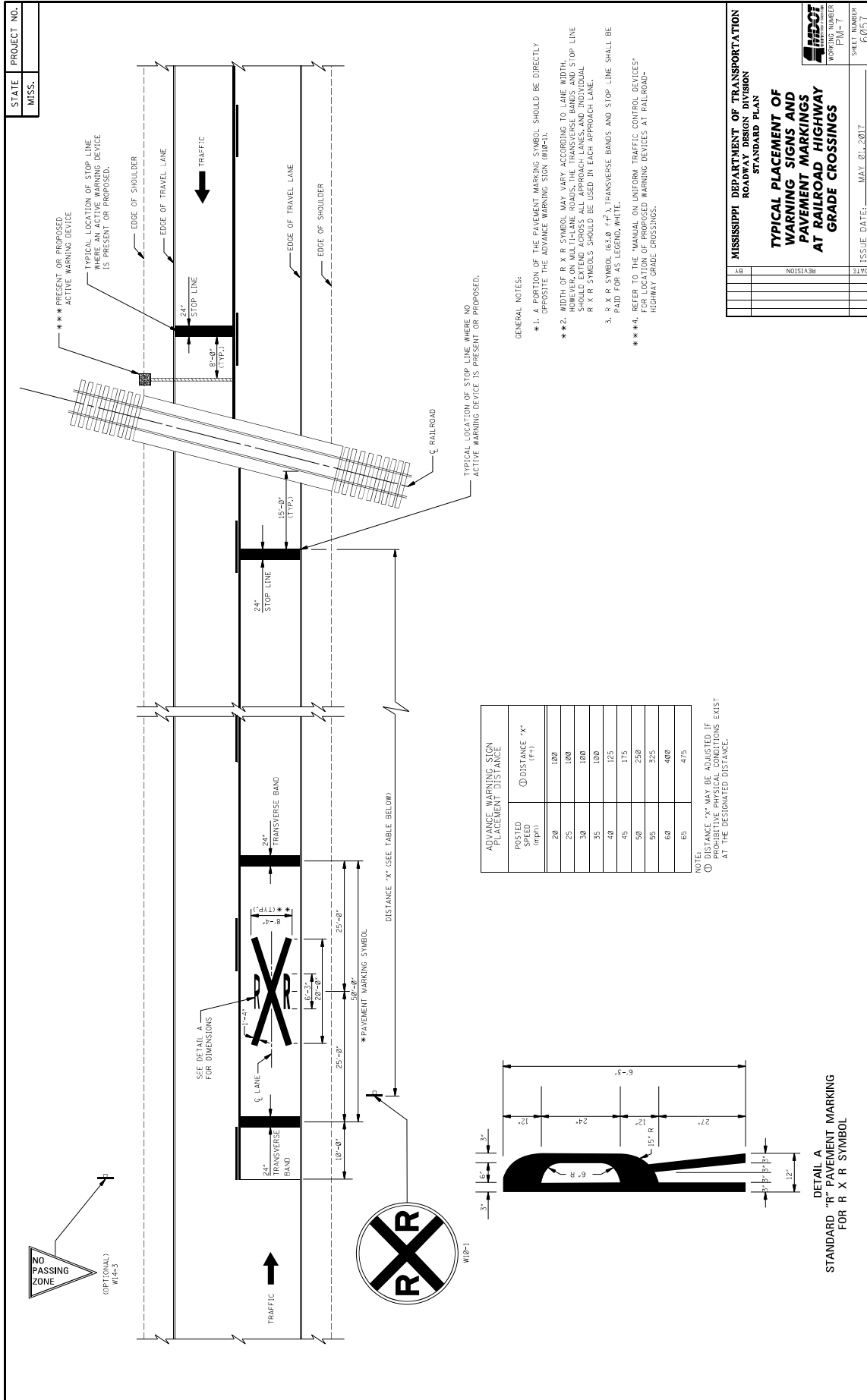
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

PAVEMENT MARKING
LEGEND DETAILS

DATE	BY	REVISION	DATE	BY	REVISION	DATE	BY	REVISION

ISSUE DATE: MAY 01, 2017

SHEET NUMBER
PM-6
60/56

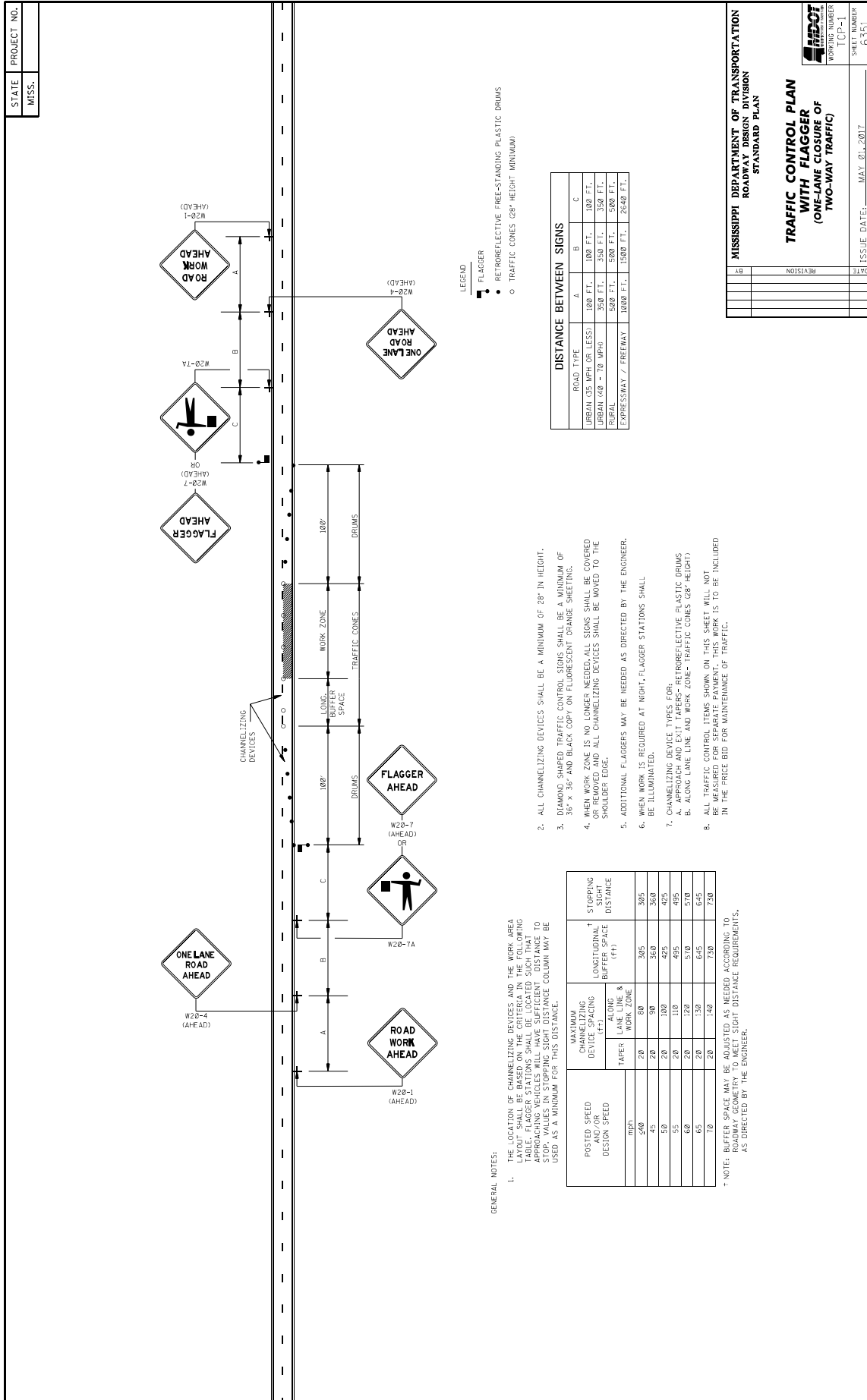


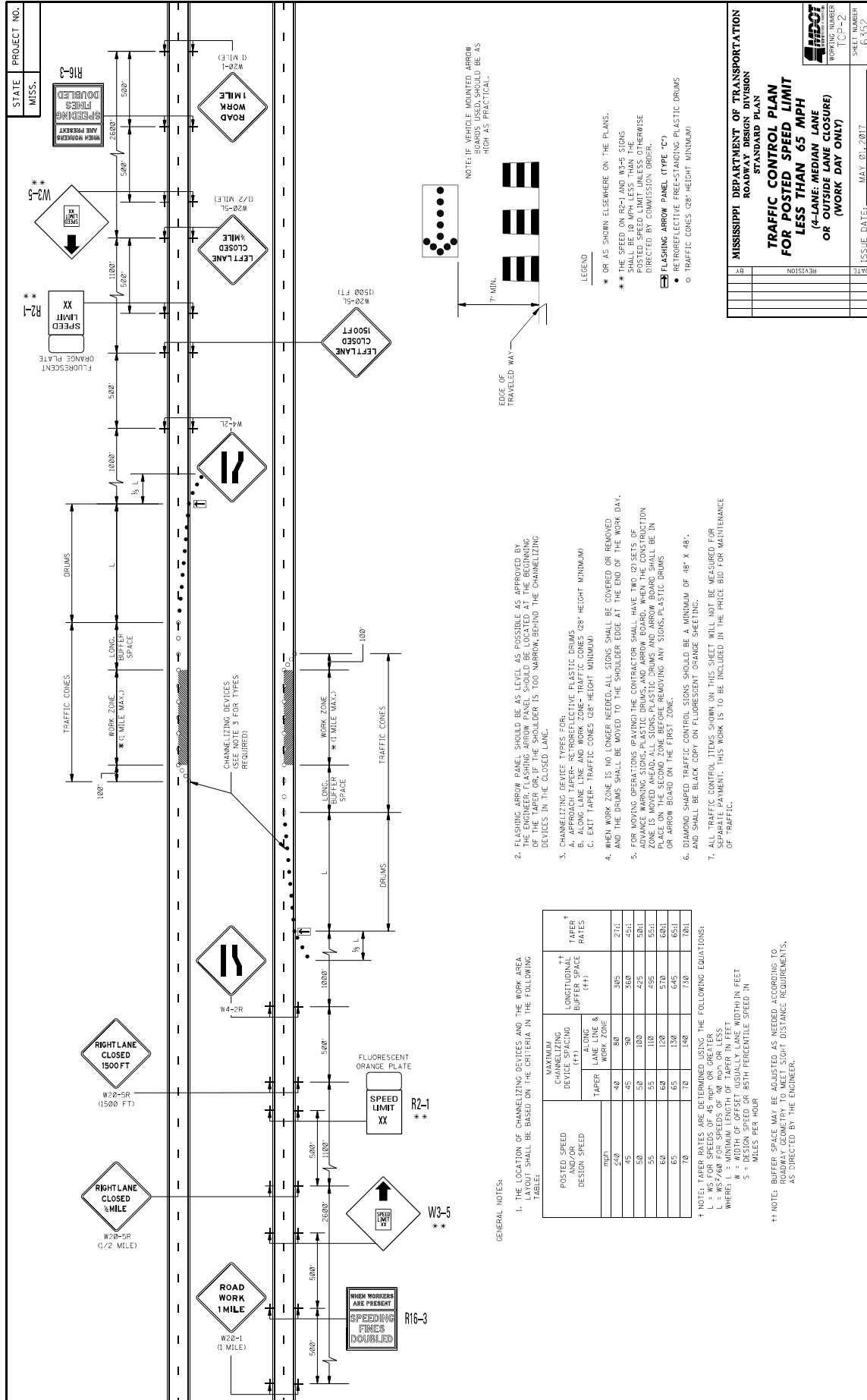
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TYPICAL PLACEMENT OF WARNING SIGNS AND PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSINGS

WORKING NUMBER: P10-1
SHEET NUMBER: 6031

ISSUE DATE: MAY 01, 2017





STATE PROJECT NO.
MISS. R16-3

W20-1 (1 MILE)
W20-5L (1/2 MILE)
W20-5R (1500 FT)
W20-5P (1/2 MILE)
W4-2R
W20-2R (1500 FT)

WHEN WORKERS ARE PRESENT
SPEEDING FINES DOUBLED
R16-3

ROAD WORK 1 MILE
LEFT LANE CLOSED 1/2 MILE
LEFT LANE CLOSED 1500 FT

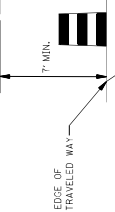
RIGHT LANE CLOSED 1500 FT
RIGHT LANE CLOSED 1/2 MILE
SPEED LIMIT XX
W3-5

TRAFFIC CONES
DRUMS
LONG. BUFFER SPACE
WORK ZONE
CHANNELIZING DEVICES
REQUIRED

TRAFFIC CONES
DRUMS
LONG. BUFFER SPACE
WORK ZONE

FLUORESCENT ORANGE PLATE
SPEED LIMIT XX
W3-5
R2-1

LEGEND
* OR AS SHOWN ELSEWHERE ON THE PLANS.
** THE SPEED ON R2-1 AND W3-5 SIGNS SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT UNLESS OTHERWISE DIRECTED BY COMMISSION ORDER.
[Symbol] FLASHING ARROW PANEL (TYPE 'C')
• RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
○ TRAFFIC CONES (28" HEIGHT MINIMUM)



NOTE: IF VEHICLE MOUNTED ARROW SIGN IS USED, IT SHOULD BE AS HIGH AS PRACTICAL.

- FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. THE PANEL SHOULD BE PLACED AT THE END OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
- CHANNELIZING DEVICES (TYPE 'C') FOR:
 - APPROACH TAPER- RETROREFLECTIVE PLASTIC DRUMS
 - ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT MINIMUM)
 - EXIT TAPER- TRAFFIC CONES (28" HEIGHT MINIMUM)
- WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED
- FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
- DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48" AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

GENERAL NOTES:

- THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (FT)		LONGITUDINAL BUFFER SPACE (FT)	TAPER RATES
	LANE LINE & WORK ZONE	TAPER		
45	40	80	305	27:1
50	45	90	360	45:1
55	50	100	425	50:1
60	55	110	495	55:1
65	60	120	570	60:1
70	65	130	645	65:1
75	70	140	730	70:1

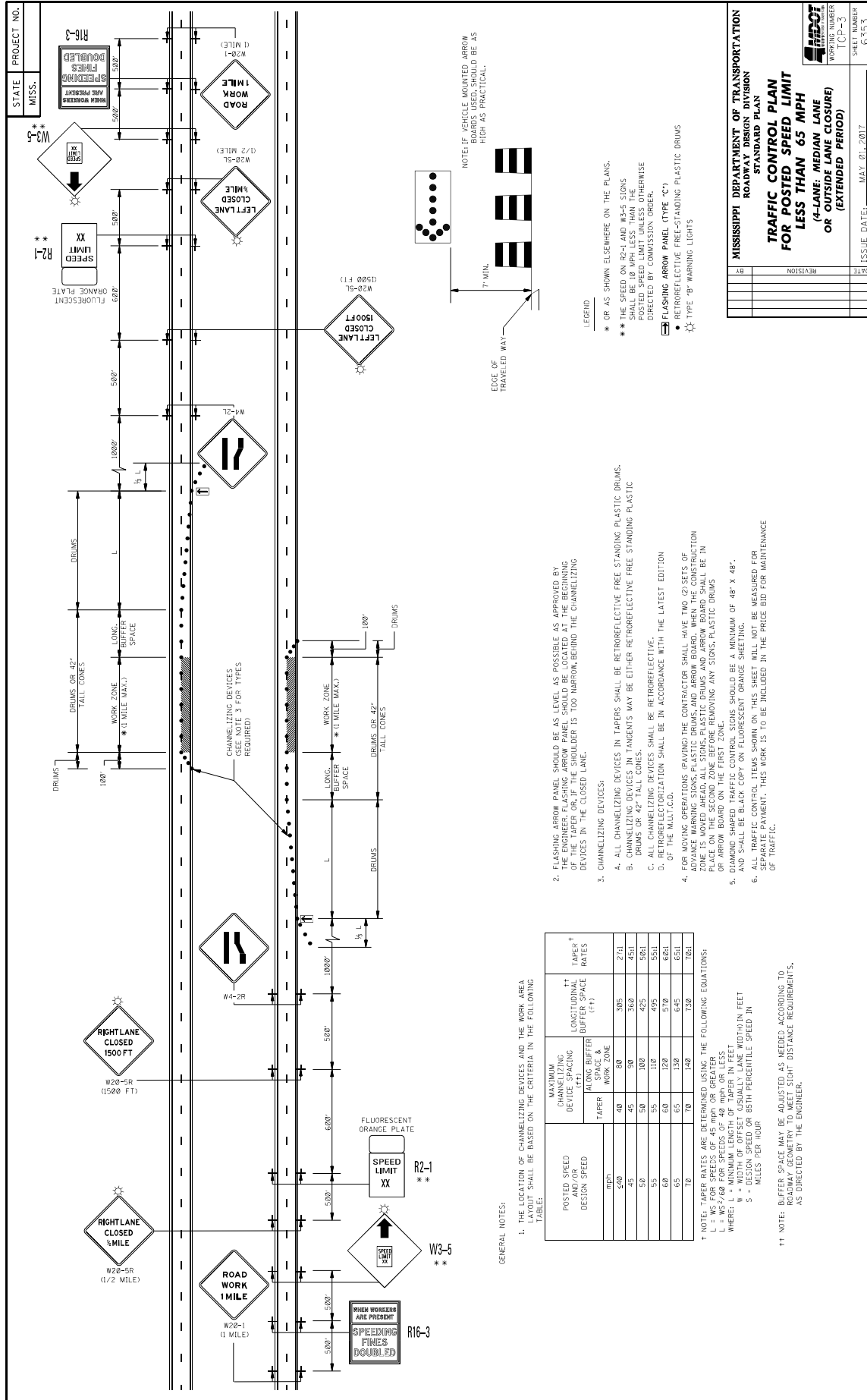
† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 $L = WS$ FOR SPEEDS OF 45 MPH OR GREATER
 $L = WS + 10$ FOR SPEEDS OF 30 MPH OR GREATER
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

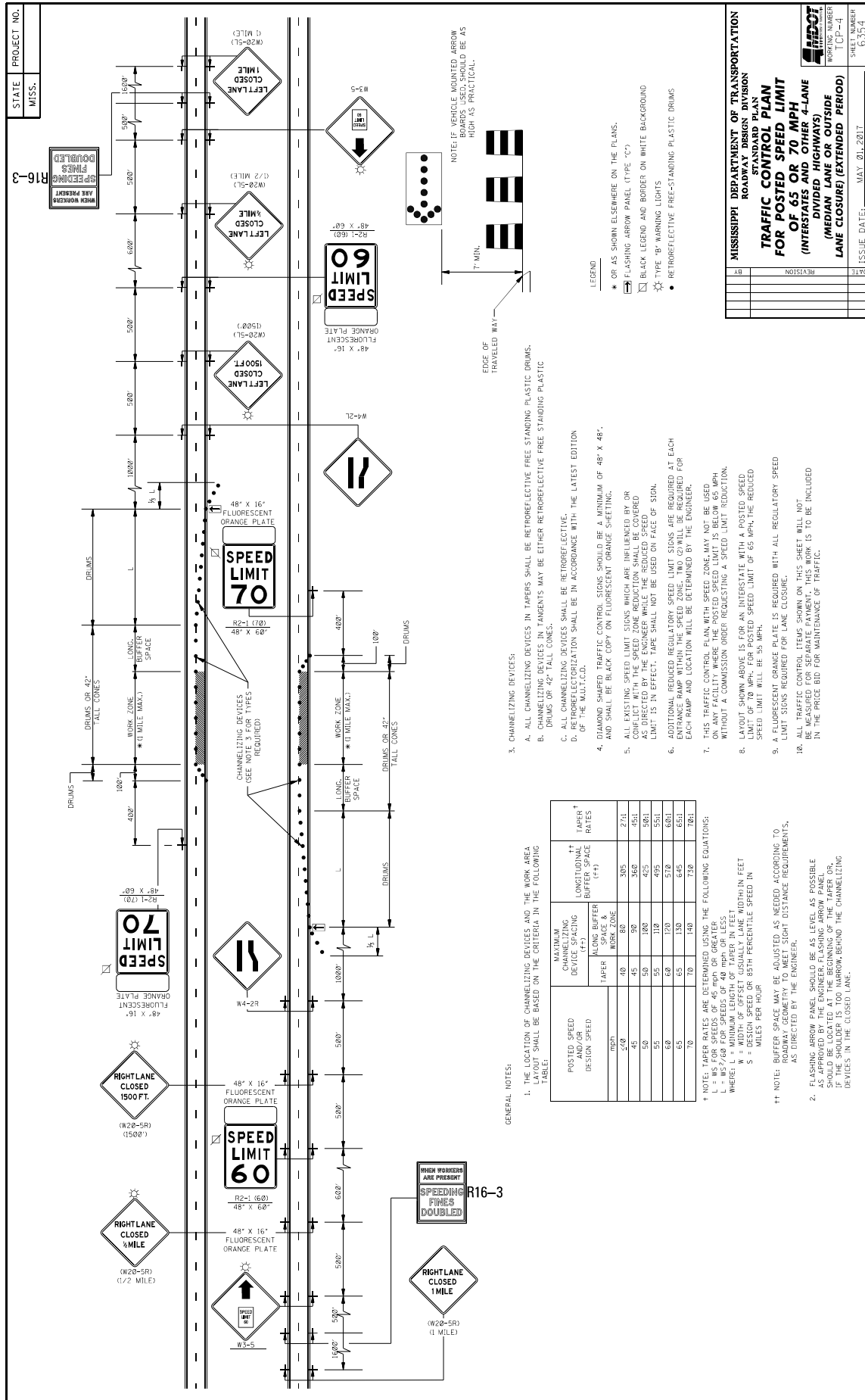
†† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TRAFFIC CONTROL PLAN
FOR POSTED SPEED LIMIT
LESS THAN 65 MPH
(4-LANE; MEDIAN LANE
OR OUTSIDE LANE CLOSURE)
OR (WORK DAY ONLY)**

ISSUE DATE: MAY 01, 2017
SHEET NUMBER: 1CP-2
WORKING NUMBER: 6352





STATE PROJECT NO.
MISS. R16-3

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
TRAFFIC CONTROL PLAN
FOR POSTED SPEED LIMIT
OF 65 OR 70 MPH
(INTERSTATES AND OTHER 4-LANE
DIVIDED HIGHWAYS)
(MEDIAN LANE OR OUTSIDE
LANE CLOSED) (EXTENDED PERIOD)

ISSUE DATE: MAY 01, 2017

WORKING NUMBER: TCF-4
SHEET NUMBER: 6534

3. CHANNELIZING DEVICES:
- ALL CHANNELIZING DEVICES IN TAPERS SHALL BE RETROREFLECTIVE FREE STANDING PLASTIC DRUMS.
 - CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER RETROREFLECTIVE FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.
 - ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE.
 - RETROREFLECTIVIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MUTCD.
 - DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48" AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
 - ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR COMPLECT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED WITH A BLACK LEGEND AND BORDER ON WHITE BACKGROUND.
 - ADDITIONAL REQUIRED REGULATORY SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENRANCE RAMP WITHIN THE SPEED ZONE. TWO CONES ARE REQUIRED FOR EACH RAMP AND LOCATION WILL BE DETERMINED BY THE ENGINEER.
 - THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
 - LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.
 - A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS REQUIRED FOR LANE CLOSURE.
 - ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT, THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

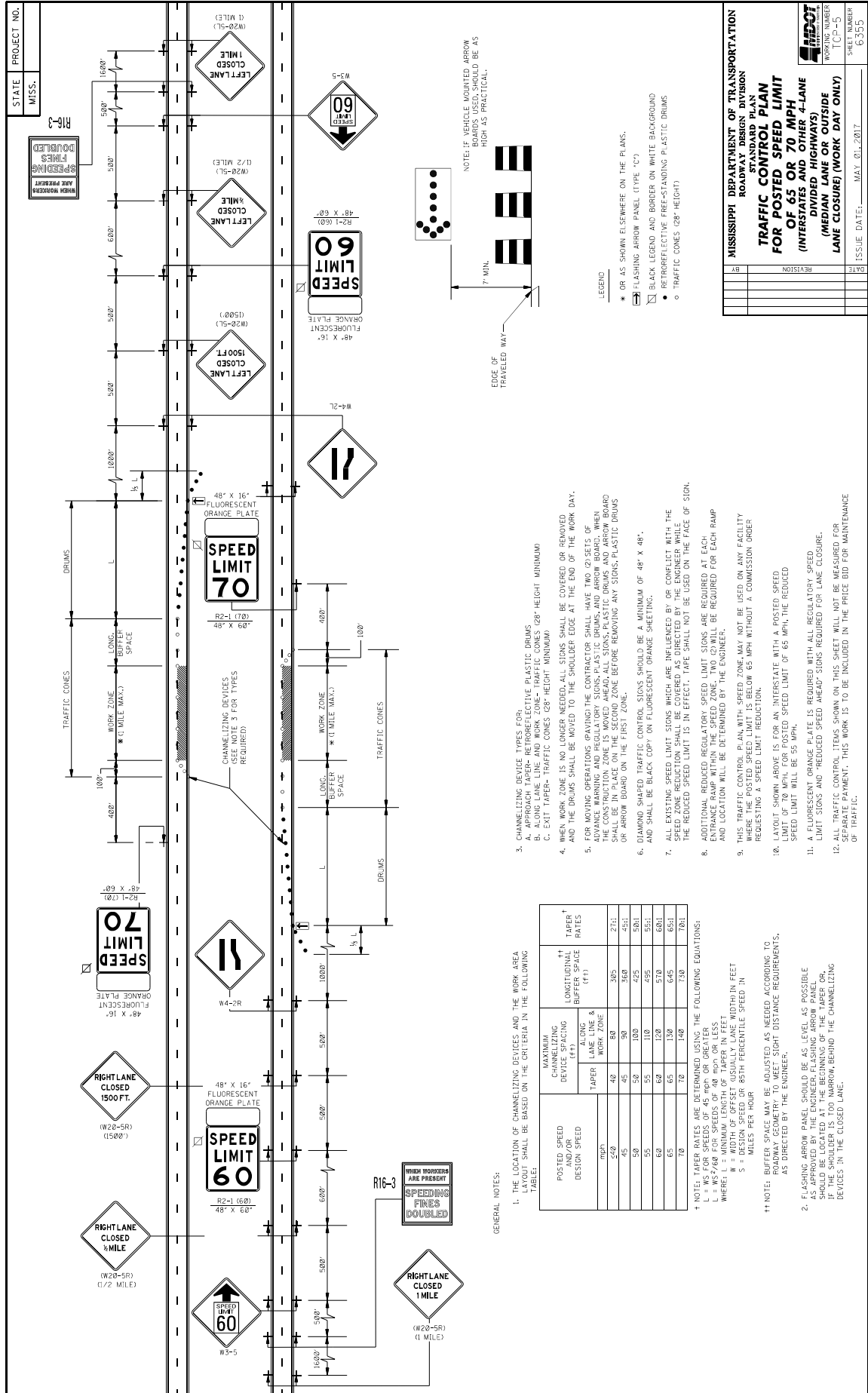
1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA TAPER RATES SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED DESIGN SPEED (mph)	MAXIMUM CHANNELIZING DEVICE SPACING		LONGITUDINAL BUFFER SPACE (FT)	TAPER RATES
	TAPER	WORK ZONE		
50	40	80	305	2:1
45	35	70	260	4:1
40	30	60	220	5:1
35	25	50	180	6:1
30	20	40	140	7:1
25	15	30	100	8:1
20	10	20	70	9:1
15	5	10	40	10:1

NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 T = 1000 / S FOR SPEEDS OF 45 MPH OR GREATER
 T = 1000 / S FOR SPEEDS OF 40 MPH OR LESS
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

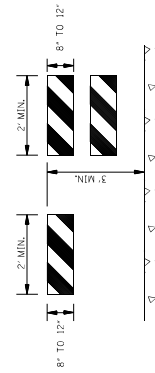
2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR AT THE END OF THE WORK ZONE, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.



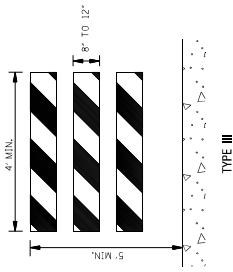
GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:
- | POSTED SPEED AND/OR DESIGN SPEED (MPH) | MAXIMUM CHANNELIZING DEVICE SPACING (FT) | | LONGITUDINAL BUFFER SPACE (FT) | TAPER RATES |
|--|--|-----------|--------------------------------|-------------|
| | LANE LINE & WORK ZONE | WORK ZONE | | |
| 45 | 40 | 80 | 305 | 27:1 |
| 50 | 45 | 90 | 360 | 45:1 |
| 55 | 50 | 100 | 425 | 50:1 |
| 60 | 55 | 110 | 495 | 55:1 |
| 65 | 60 | 120 | 570 | 60:1 |
| 70 | 65 | 130 | 645 | 65:1 |
| 75 | 70 | 140 | 730 | 70:1 |
- † NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 $L = WS$ FOR SPEEDS OF 45 MPH OR GREATER
 $L = W^2$ FOR SPEEDS OF 50 MPH OR GREATER
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
- †† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO LOCAL LIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.
2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AND SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR IF THE SHOULDER IS TOO NARROW BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.

STATE PROJECT NO.
MISS.



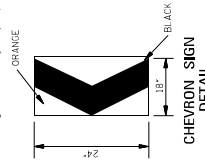
TYPE I
TYPE II



TYPE III

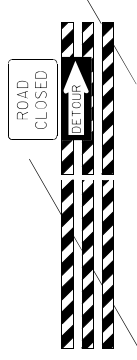
STANDARD BARRICADES

1. THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION OF TRAFFIC IS TO PASS).
2. RAIL STRIPE SHALL BE 6 INCHES, EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.
3. DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
4. FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.
5. BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES WHICH REQUIRE SUCCESSFUL CRASH TESTING. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE: http://safety.fhwa.dot.gov/roadway_dept/policy_guidance/road_hardware/cat2.cfm



CHEVRON SIGN
DETAIL

1. A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
2. THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.
3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.

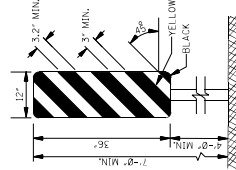


BARRICADE CLOSING A ROAD

BARRICADE CHARACTERISTICS

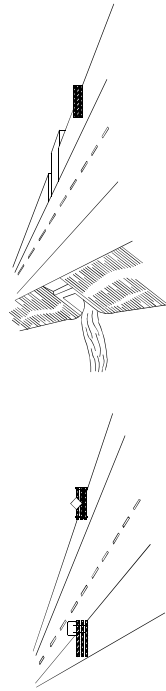
	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF RETROREFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS

- * 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- ** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREeways AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 IN² OF REFLECTIVE AREA FACING TRAFFIC.



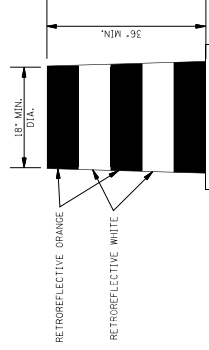
TYPE 3 OBJECT MARKER
(OM-3R)

1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
2. THE OM-3R IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
3. THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



WING BARRICADES

1. WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER OF A ROADWAY TO RESTRICT TRAFFIC FROM ENTERING OR EXITING A ROADWAY OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
2. WING BARRICADES SHOULD BE USED:
 - A. IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - B. IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.



PLASTIC DRUM STRIPING DETAIL

1. PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDITED METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH MUTCD. DRUMS SHALL BE MOUNTED ON A CONCRETE OR METAL STRIPING 12 ORANGE & 2 WHITE 6" WIDE.
2. DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
3. WHERE PRACTICAL PLASTIC DRUMS SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS

WORKING NUMBER: TSP-5
SHEET NUMBER: 6530B

ISSUE DATE: MAY 01, 2017

STATE MISS.	PROJECT NO.	
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MOBILE OPERATIONS ON MULTILANE ROAD

MOBILE OPERATIONS ON MULTILANE ROAD

MOBILE OPERATIONS ON TWO-LANE ROAD

MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES FOR MULTILANE LANE OPERATION:

- VEHICLES USED FOR THESE OPERATIONS SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT, SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLASERS, SIGNS, OR ARROW PANELS.
- SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK MOUNTED ATTENUATOR (TMA), AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE LEGIBLY DISPLAYED ON SHADOW VEHICLE 2 SO AS NOT TO OBSCURE THE ARROW PANEL.
- SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK-MOUNTED ATTENUATOR (TMA).
- SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
- WHEN ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, SHADOW VEHICLE 2 SHOULD BE ELIMINATED.
- ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE SHOULD BE USED (I.e., VEHICLE 3 ON THE SHOULDER OF PRACTICALLY, VEHICLE 2 IN THE CLOSED LANE, AND VEHICLE 1 IN THE CLOSED LANE).
- ARROW PANELS SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCO.
- WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

NOTES FOR TWO-LANE OPERATION:

- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. SHADOW VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE DISTANCE CANNOT BE MAINTAINED, THE SHADOW VEHICLE SHOULD BE USED TO MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW VEHICLE SHALL BE EQUIPPED WITH BEACONS. HIGH-BEAM LIGHTS AND WORK LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN, SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ARROW BOARD TO BE USED IN CAUTION MODE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

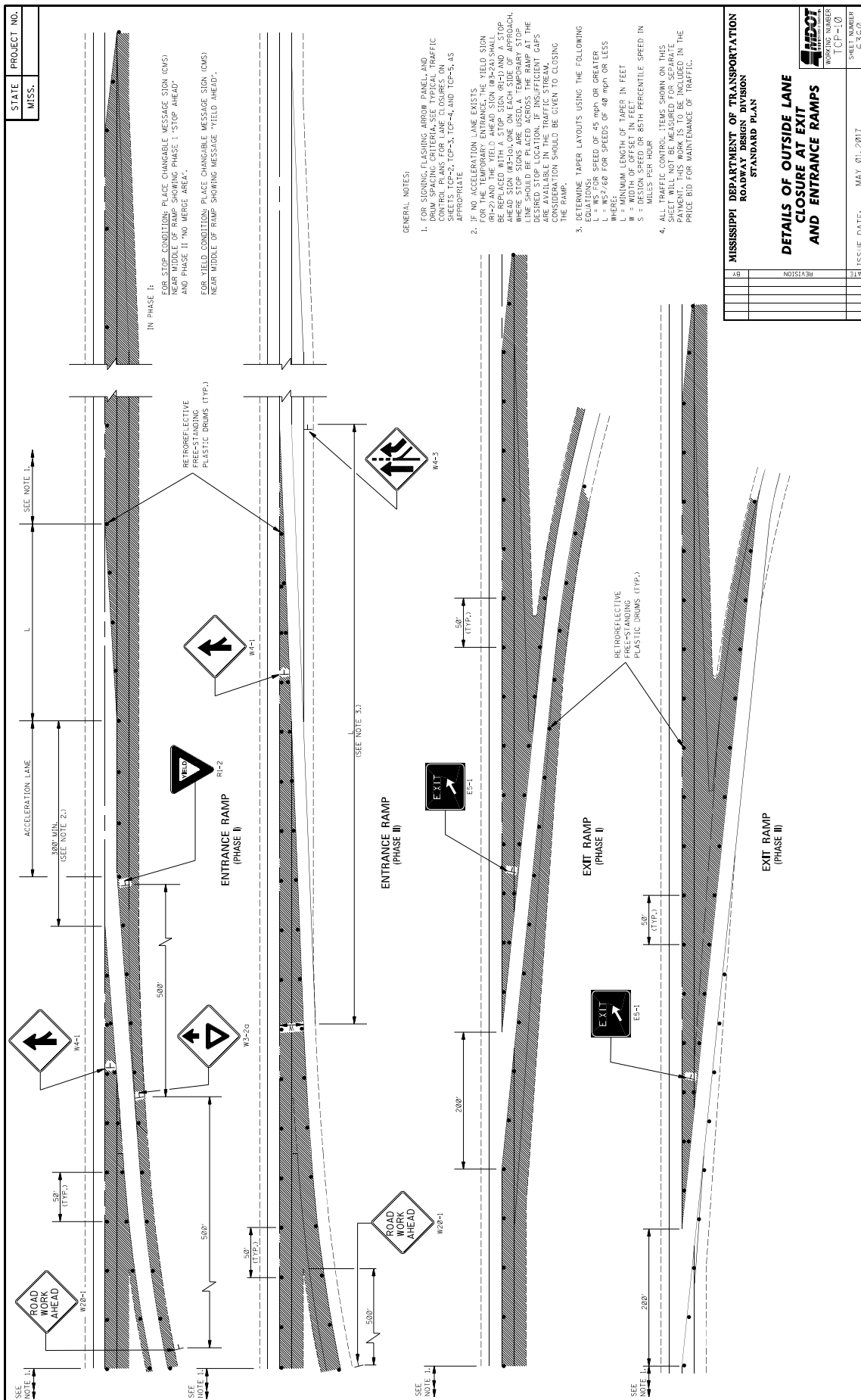
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TRAFFIC CONTROL PLAN
MOBILE OPERATIONS
MULTILANE ROADS
TWO-LANE ROADS

NO.	REVISION	DATE

ISSUE DATE: MAY 01, 2017

SHEET NUMBER TCP-9	PROJECT NUMBER G339
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GENERAL NOTES:

1. FOR SIGNING: FLASHING ARROW PANEL AND DRUM SPACING CRITERIA: SEE TYPICAL TRAFFIC CONTROL PLANS FOR LANE CLOSURES ON APPROXIMATELY 70, 75, 80, 85, 90, 95, 100, 105, 110, 115, 120, 125, 130, 135, 140, 145, 150, 155, 160, 165, 170, 175, 180, 185, 190, 195, 200, 205, 210, 215, 220, 225, 230, 235, 240, 245, 250, 255, 260, 265, 270, 275, 280, 285, 290, 295, 300, 305, 310, 315, 320, 325, 330, 335, 340, 345, 350, 355, 360, 365, 370, 375, 380, 385, 390, 395, 400, 405, 410, 415, 420, 425, 430, 435, 440, 445, 450, 455, 460, 465, 470, 475, 480, 485, 490, 495, 500, 505, 510, 515, 520, 525, 530, 535, 540, 545, 550, 555, 560, 565, 570, 575, 580, 585, 590, 595, 600, 605, 610, 615, 620, 625, 630, 635, 640, 645, 650, 655, 660, 665, 670, 675, 680, 685, 690, 695, 700, 705, 710, 715, 720, 725, 730, 735, 740, 745, 750, 755, 760, 765, 770, 775, 780, 785, 790, 795, 800, 805, 810, 815, 820, 825, 830, 835, 840, 845, 850, 855, 860, 865, 870, 875, 880, 885, 890, 895, 900, 905, 910, 915, 920, 925, 930, 935, 940, 945, 950, 955, 960, 965, 970, 975, 980, 985, 990, 995, 1000.
2. IF NO ACCELERATION LANE EXISTS FOR THE TEMPORARY LANE ENTRIES (R4-2) AND THE YIELD AHEAD SIGN (R4-2a) SHALL BE REPLACED WITH A STOP SIGN (R4-2) AND A STOP SIGN (R4-2a) SHALL BE REPLACED WITH A STOP SIGN (R4-2). WHERE STOP SIGNS ARE USED, A TEMPORARY STOP LINE SHOULD BE PLACED ACROSS THE RAMP AT THE DESIRED STOP LOCATION. IF INSUFFICIENT GAPS OR INSUFFICIENT LANE LENGTH FOR SAFE STOPPING CONSIDERATION SHOULD BE GIVEN TO CLOSING THE RAMP.
3. DETERMINE TAPER LAYOUTS USING THE FOLLOWING EQUATIONS: SPEEDS OF 45 MPH OR GREATER
 $L = WS^2/60$ FOR SPEEDS OF 40 MPH OR LESS
 WHERE:
 L = MINIMUM LENGTH OF TAPER IN FEET
 S = DESIGN SPEED OR 85th PERCENTILE SPEED IN MILES PER HOUR
 W = DESIRED STOP LINE TO THE CENTER OF THE ROADWAY
4. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PRICES. THEY WILL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

DETAILS OF OUTSIDE LANE CLOSURE AT EXIT AND ENTRANCE RAMPs

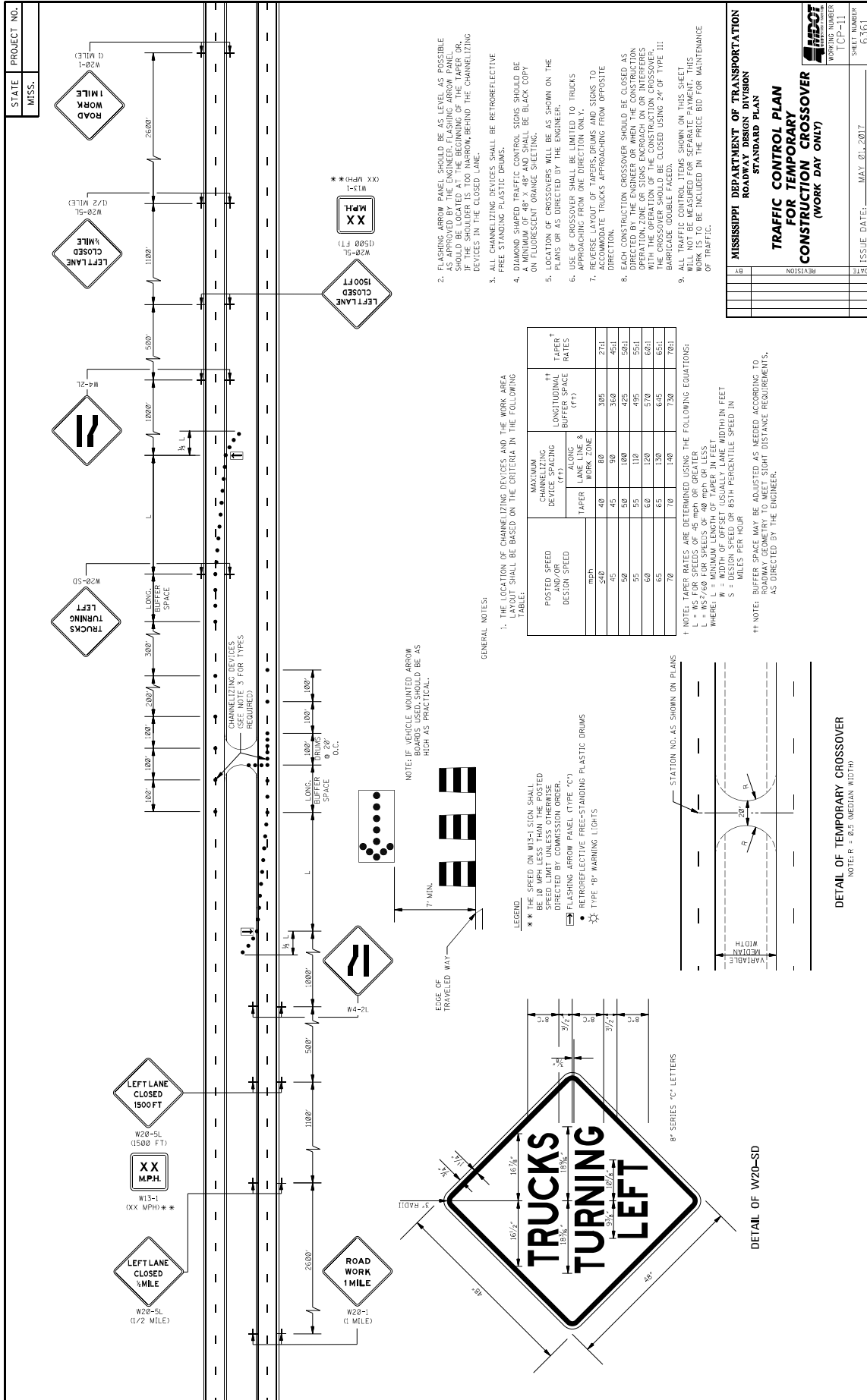
STATE PROJECT NO.
MISS.

ISSUE DATE: MAY 01, 2017

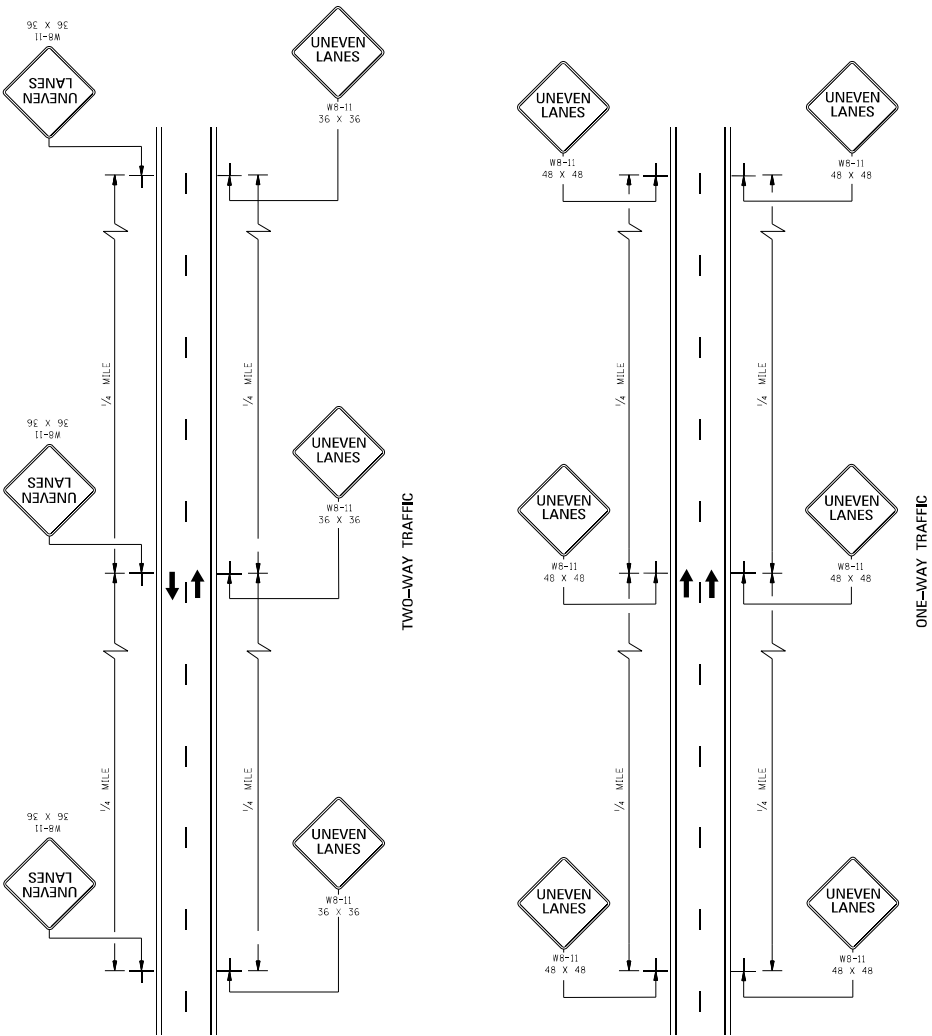
WORKING NUMBER
TCP-10

SHEET NUMBER
G500

NO.	REVISION	DATE



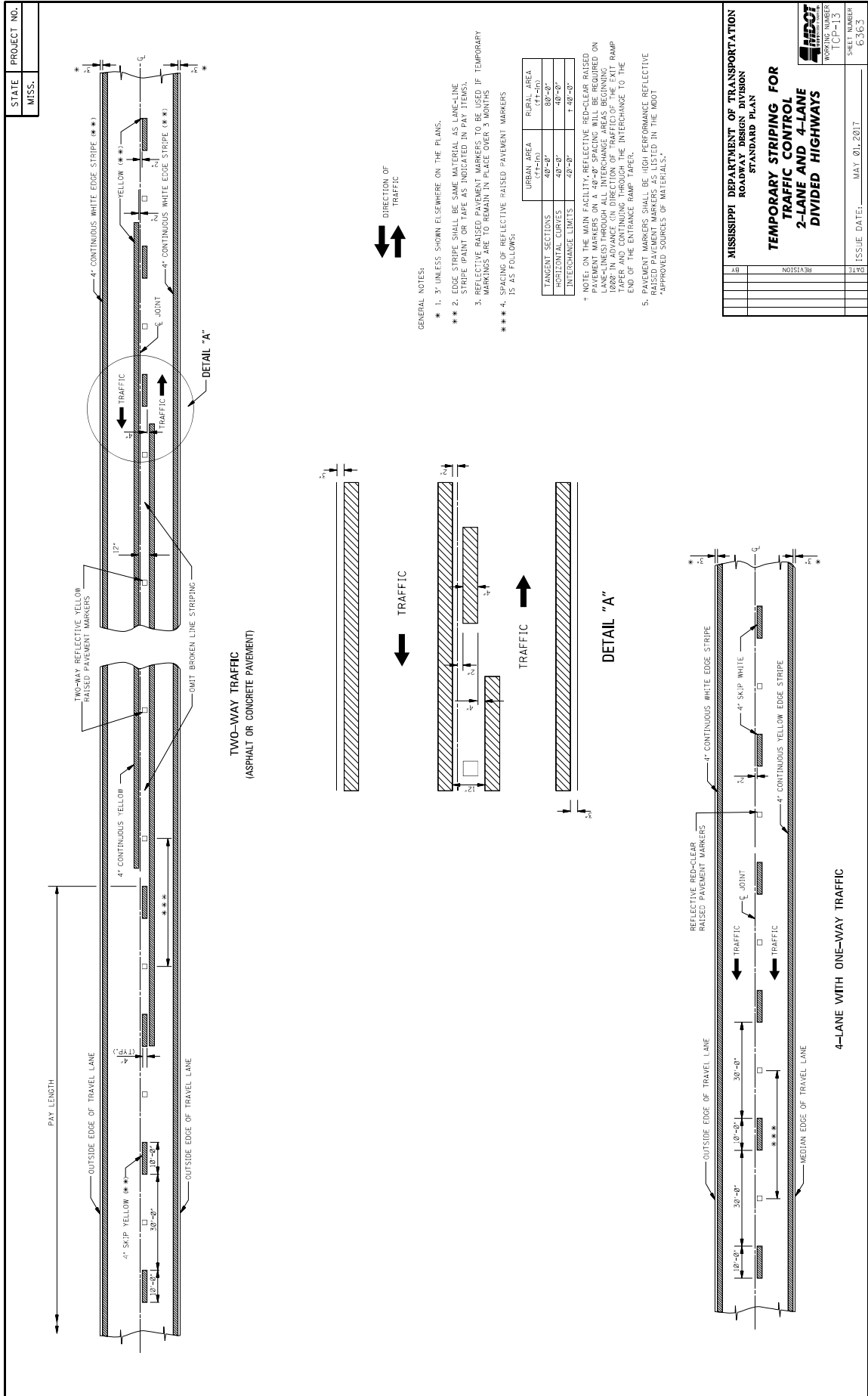
STATE	PROJECT NO.
MISS.	



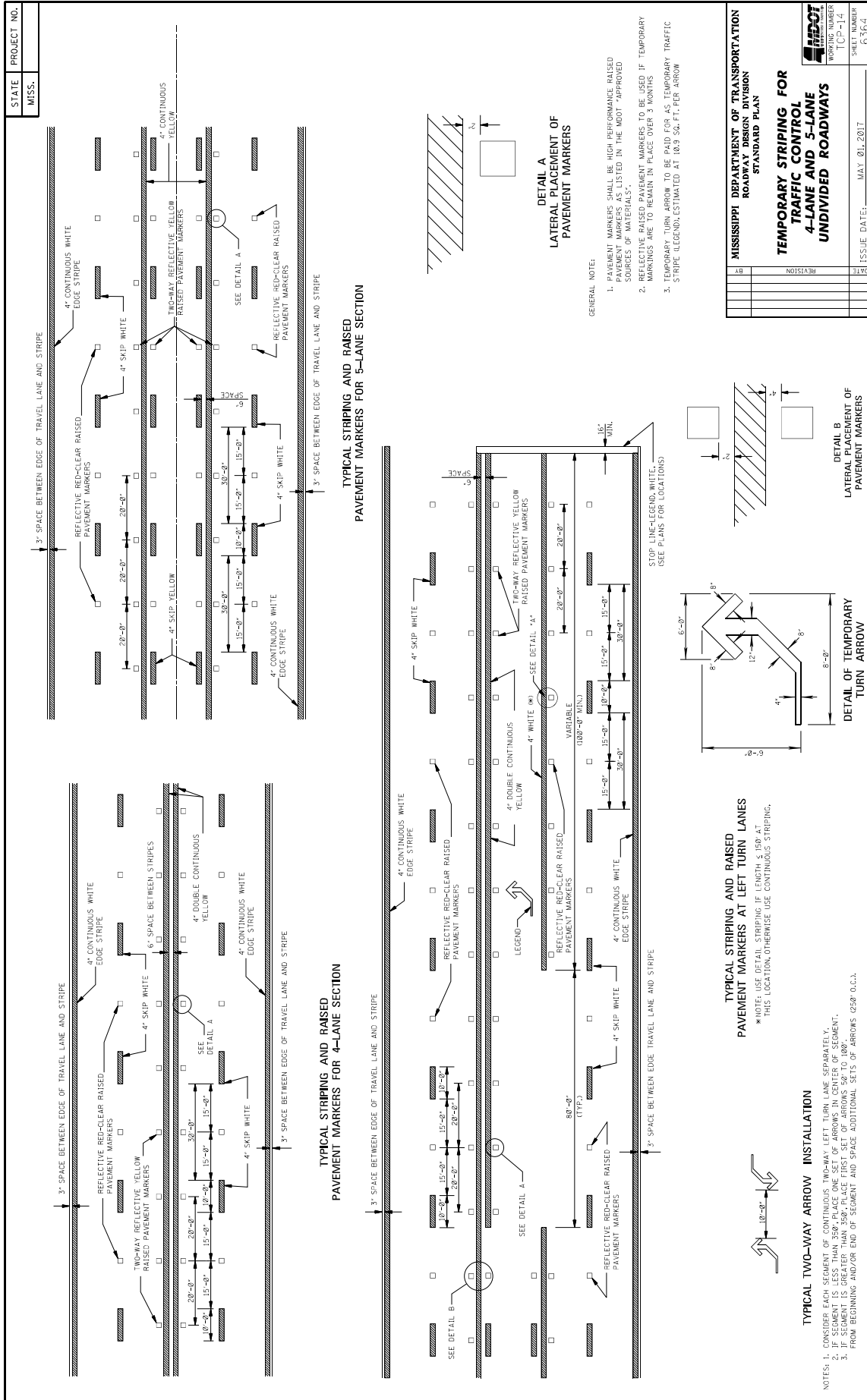
GENERAL NOTES:

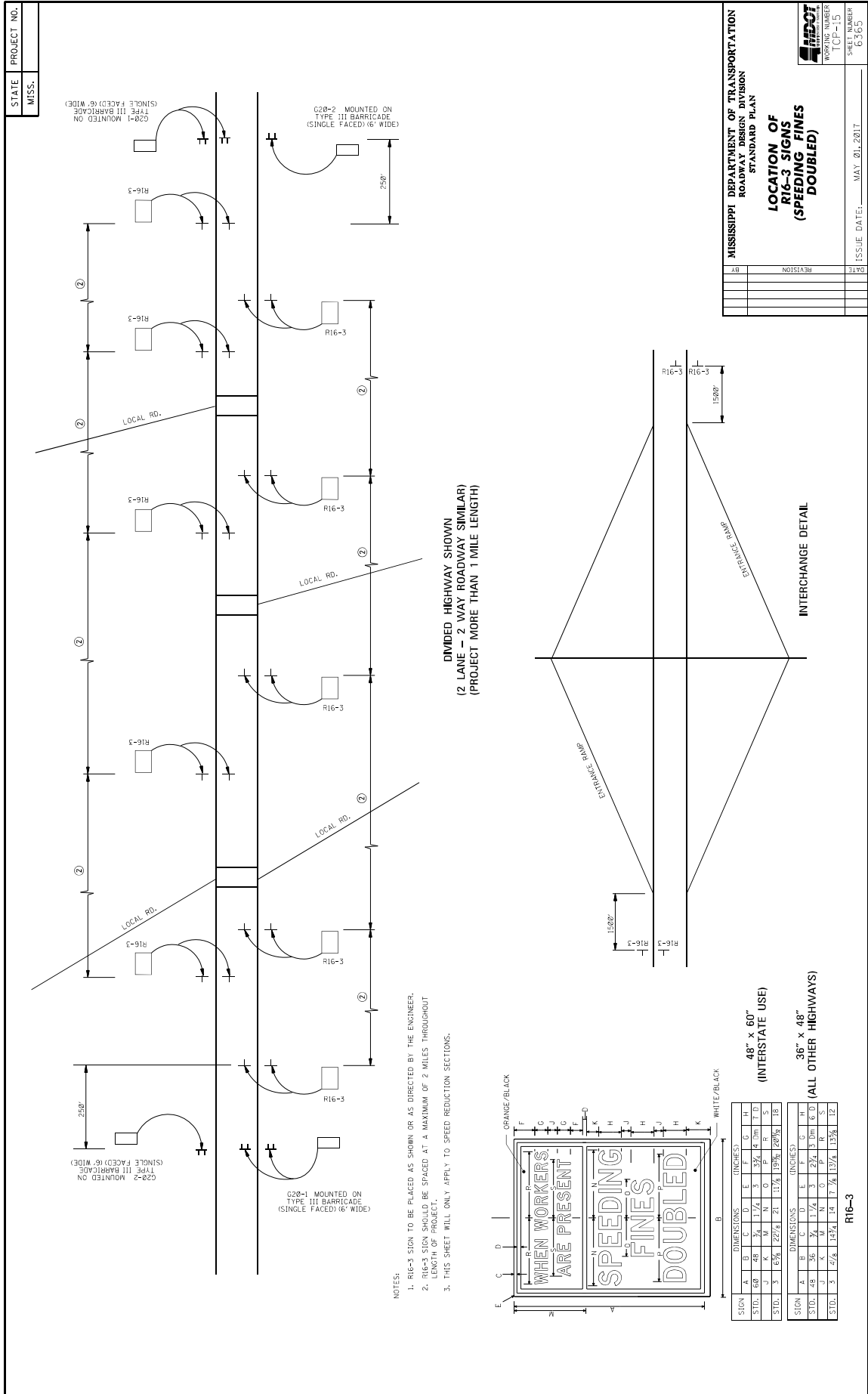
- UNEVEN LANE LINE.
 - IF LESS THAN OR EQUAL TO 1/4 MILE, NO SIGNS REQUIRED.
 - IF MORE THAN 1/4 MILE AND LESS THAN 1/2 MILE, PLACE SIGNS AS SHOWN ON THIS SHEET.
 - IF MORE THAN 1/2 MILE, PLACE SIGNS AS SHOWN ON THIS SHEET.
 - IF GREATER THAN 1/4 MILE, TRAFFIC SHOULD NOT BE ALLOWED TO CROSS UNEVEN LANE LINE.
- THE WB-11 SIGNS SHOULD BE SPACED AT 1/4-MILE INTERVALS THROUGHOUT UNEVEN LANE LINE LIMITS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TRAFFIC CONTROL PLANS UNEVEN PAVEMENT DETAILS	
WORKING NUMBER TCP-12	SHEET NUMBER 6262
DATE	ISSUE DATE: MAY 01, 2017
BY	REVISION



MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY DESIGN DIVISION	
STANDARD PLAN	
TEMPORARY STRIPING FOR	
2-LANE AND 4-LANE	
DIVIDED HIGHWAYS	
WORKING NUMBER TCP-113	SHEET NUMBER 6363
DATE	ISSUE DATE: MAY 01, 2017
REVISION	





STATE MISS.	PROJECT NO.	
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TYPICAL SHOULDER CLOSURE

(1) TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
(2) TO BE USED WHEN CONSTRUCTION VEHICLES (EQUIPMENT) ENCRUSHES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.

TYPICAL SHOULDER WORK #1
(SEE NOTE A-I THIS SHEET)

TYPICAL SHOULDER WORK #2

NOTE:
WORK OUTSIDE TWO (2) FOOT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA, OR SEE NOTE A-3 THIS SHEET.

DETAIL OF DRUM PLACEMENT AT PAVEMENT EDGE DROP-OFF

GRANULAR MATERIAL REQUIRED (SAME CLASSIFICATION AS SHOULDER MATERIAL, SEE TYPICAL SECTIONS)

NOTES:

- * A. PAVEMENT EDGE DROP-OFF
 - 1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE B (1538'+O.C.).
 - 2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 120 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. CONES MAY BE USED IN PLACE OF DRUMS, PANELS, AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE MULTIPLIER $L/3L$, WHERE L IS THE TAPER LENGTH IN FEET.
 - 3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 4:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED.
 - 4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
 - 5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.
- B. DRUM SPACING
 - 1. TANGENTS = $2 \times S$
 - 2. WHERE $S =$ SPEED IN MPH (POSTED OR 85 PERCENTILE)
 - 3. WHERE $L =$ TAPER LENGTH IN FEET
 - 4. WHERE $W =$ WIDTH OF OFFSET IN FEET
- C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

TABLE V-1. GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE	
X X SPEED (MPH)	LENGTH (FEET)
25	35
30	45
35	55
40	65
45	75
50	85
55	95
60	105
65	115
70	125
75	135
80	145
85	155
90	165
95	175
100	185

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

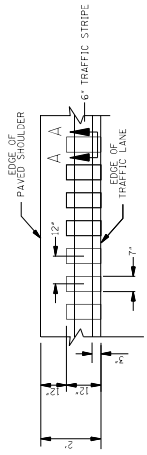
**TRAFFIC CONTROL DETAILS
DRUM PLACEMENT
AND SHOULDER CLOSURE**

WORKING NUMBER
TCP-16

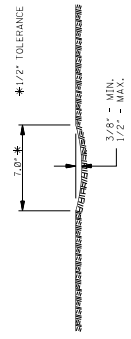
ISSUE DATE: MAY 01, 2017

STATE	PROJECT NO.
MISS.	

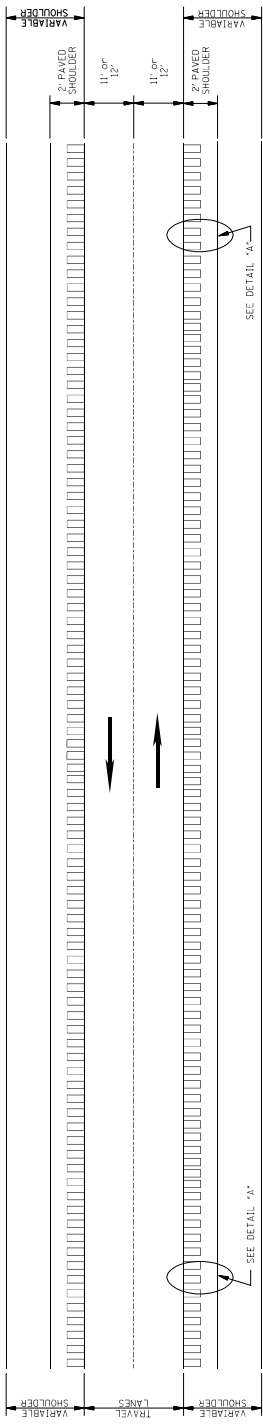
- GENERAL NOTES
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT.
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL PAVED SHOULDERS ON ROADWAYS OR OTHER INTERUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER.
 - COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS.
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
 - MAINLINE
 - INTERSECTING ROADWAY IF OVERLAD OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.W.
 - ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.
 - DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.



DETAIL "A"



SECTION "A-A"

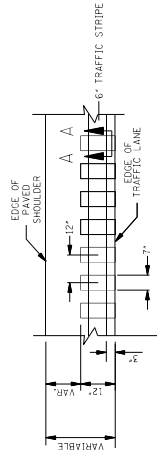


PLAN
NOT TO SCALE

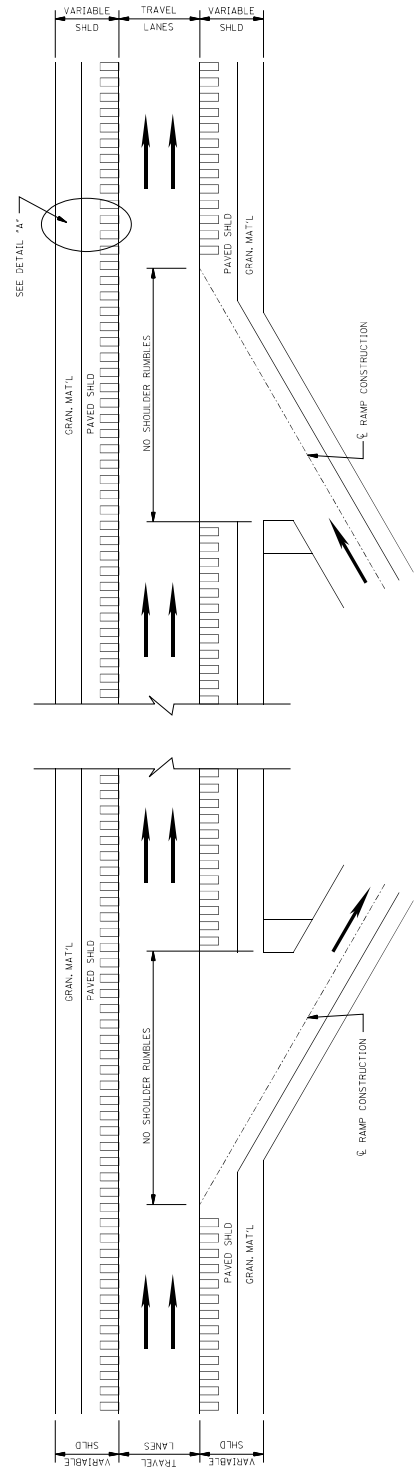
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
RUMBLE STRIPES 2-LANE HIGHWAYS (ASPHALT LANES, 2-FT ASPHALT SHOULDERS)	
WORKING NUMBER RS-1	SHEET NUMBER 606-1
DATE	ISSUE DATE: MAY 21, 2017
BY	REVISION

STATE	PROJECT NO.
MISS.	

- GENERAL NOTES**
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL PAVED SHOULDERS ON ROADWAYS OR OTHER INTERUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 - COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
 - MAINLINE
 - INTERSECTING ROADWAY IF OVERLAP OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.W.
 - ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.

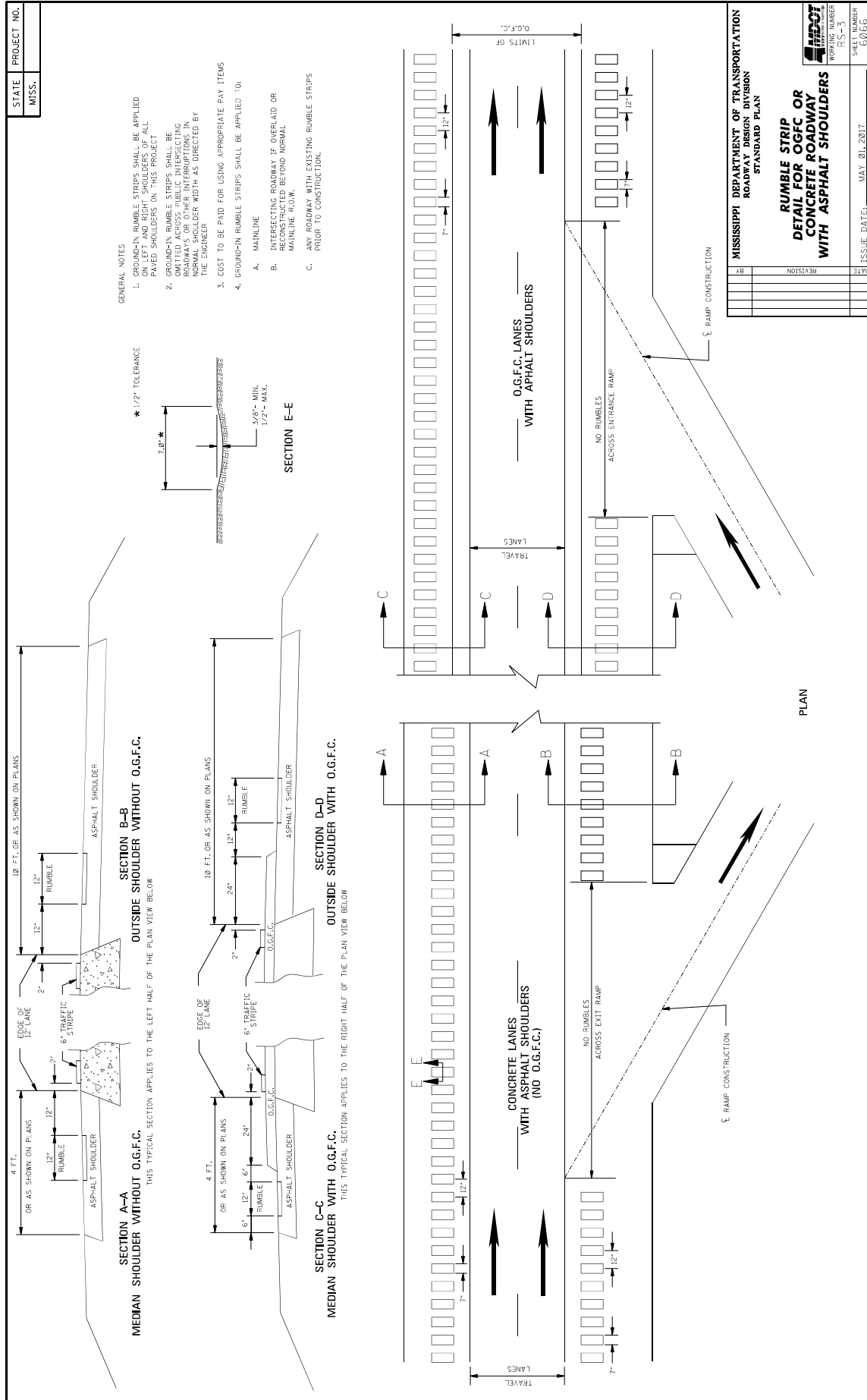


SECTION "A-A"



PLAN
NOT TO SCALE
DETAILS OF
RUMBLE STRIPES

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
RUMBLE STRIPES 4-LANE HIGHWAYS (ASPHALT LANES, 2-FT OR WIDER, ASPHALT SHOULDERS)	
WORKING NUMBER R-3-2	SHEET NUMBER 60/65
DATE	ISSUE DATE: MAY 21, 2017
REVISION	
BY	



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 445

CODE: (SP)

DATE: 10/10/2017

SUBJECT: Mississippi Agent or Qualified Nonresident Agent

Bidders are hereby advised of the requirements of Subsections 102.08, 103.05.2, and 107.14.2.1 of the *2017 Standard Specifications for Road and Bridge Construction* as it refers to bonding agents. Proposal guaranties, bonds, and liability insurance policies must be signed by a **Mississippi Agent or Qualified Nonresident Agent.**

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 447

CODE: (SP)

DATE: 10/18/2017

SUBJECT: Traffic on Milled Surface in Rural Areas

Bidders are hereby advised that when the main lanes of a roadway are fine milled, traffic will be allowed to run on a milled surfaces for up to seven (7) calendar days. The Contractor will be assessed a penalty of \$5,000 per calendar day afterwards until the milled surfaces are covered with the next lift of asphalt. It shall be the Contractor's responsibility to ensure that the milling operations do not commence until such time as forecasted weather conditions are suitable enough to allow the placement of the asphalt pavement after the milling operations.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 516

CODE: (IS)

DATE: 11/28/2017

SUBJECT: Errata and Modifications to the 2017 Standard Specifications

<u>Page</u>	<u>Subsection</u>	<u>Change</u>
16	102.06	In the seventh full paragraph, change “Engineer” to “Director.”
33	105.05.1	In the sixth sentence, change “Contract Administration Engineer” to “Contract Administration Director.”
34	105.05.2.1	In subparagraph 2, change “SWPPP, ECP” to “SWPPP and the ECP”
35	105.05.2.2	In subparagraphs 2, add “ and” to the end of the sentence. In subparagraph 3, remove “, and” and add “.”.
90	109.04.2	In the last paragraph of subparagraph (a), place a period “.” at the end of the sentence.
93	109.04.2	In the last paragraph of subparagraph (g), place a period “.” at the end of the sentence. Also, in the first paragraph of subparagraph (h), place a period “.” at the end of the sentence.
97	109.07	Under ADJUSTMENT CODE, subparagraph (A1), change “HMA mixture” to “Asphalt mixtures.”
98	109.11	In the third sentence, change “Engineer” to “Director.”
219	308.04	In the last sentence of the last paragraph, change “Contractor’s decision” to “Engineer’s decision.”
300	405.02.5.9	In the first sentence of the second paragraph, change “Hot Mix Asphalt” to “Asphalt Mixtures.”
502	630.01.1	In the first paragraph, change “AASHTO” to “AASHTO’s LRFD”.
636	646.05	Change “each” to “per each” for the pay item units of payment.
640	656.02.6.2	In item 7), change “down stream” to “downstream”.
688	630.03.2	Change the subsection number from “630.03.2” to “680.03.2.”

- | | | |
|-----|---------------|---|
| 725 | 702.08.3 | In the second sentence of the first paragraph, change “hot-mix” to “asphalt.” |
| 954 | 804.02.13.1.6 | In the definition for “M” in the % Reduction formulas, change “paragraph 7.3” to “paragraph 5.3.” |

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1206

CODE: (SP)

DATE: 10/16/2018

SUBJECT: MASH Compliant Devices

Bidders are hereby advised that the Standard Specifications may require certain traffic control and permanent safety hardware devices to meet the requirements of the Manual for Assessing Safety Hardware (MASH). However, devices meeting the requirements of NCHRP Report 350 will be allowed until the mandatory effective date for MASH compliance. The following table shows the effective dates for MASH compliant devices.

Device	Effective Date for MASH Compliance
W-beam barriers, cast-in-place concrete barriers	December 31, 2017
W-beam terminals - non-flared	June 30, 2018
Crash cushions	December 31, 2018
Cable barriers, cable barrier terminals, bridge rails, transitions, all other longitudinal barriers including portable barriers installed permanently, W-beam terminals - flared, all other terminals, sign supports, all other breakaway hardware	December 31, 2019

Temporary work zone devices, including portable barriers manufactured after December 31, 2019, must have been successfully tested to the 2016 Edition of MASH. Such devices manufactured on or before this date and successfully tested to NCHRP Report 350 or the 2009 Edition of MASH may continue to be used throughout their normal service lives.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1226

CODE: (SP)

DATE: 11/16/2018

SUBJECT: Material Storage Under Bridges

Bidders are advised that Subsection 106.08 of the Standard Specifications allows the Contractor to store materials and equipment on portions of the right-of-way. However, the Contractor will not be allowed to store or stockpile materials under bridges without written permission from the Project Engineer. The Contractor shall submit a detailed request of all proposed materials to be stored under bridges to the Engineer a minimum of 14 calendar days prior to anticipated storage. This detail shall include, but not limited to, bridge location, material type, material quantity, and duration of storage. The Project Engineer and any other needed Division will review this information and determine whether to grant approval. The Contractor shall not store any material under any bridge without written approval from the Project Engineer.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1241

CODE: (SP)

DATE: 11/27/2018

SUBJECT: Fuel and Material Adjustments

Bidder's attention is brought to the last paragraph of Subsection 109.07 of the Standard Specifications which states that no fuel or material adjustment will be made after the completion of contract time. Any fuels consumed or materials incorporated into the work during the monthly estimate period falling wholly after the expiration of contract time will not be subject a fuel or material adjustment.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1481

CODE: (SP)

DATE: 03/26/2019

SUBJECT: Contract Time

PROJECT: MP-5011-38(010) / 306547301 -- Lauderdale County

The completion of work to be performed by the Contractor for this project will not be a specified date but shall be when all allowable working days are assessed, or any extension thereto as provided in Subsection 108.06. It is anticipated that the Notice of Award will be issued no later than be **May 14, 2019** and the effective date of the Notice to Proceed / Beginning of Contract Time will be **July 1, 2019**.

AN EARLY NOTICE TO PROCEED / BEGINNING OF CONTRACT TIME WILL NOT BE ALLOWED ON THIS PROJECT.

137 Working Days have been allowed for the completion of work on this project.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1482

CODE: (SP)

DATE: 03/26/2019

SUBJECT: Scope of Work

PROJECT: MP-5011-38(010) / 306547301 -- Lauderdale County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings". All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following:

Milling and overlaying approximately 10.8 miles of U.S. Highways 11 / 80 from the Alabama state line (Station 1+74) to the pavement change located east of Russell (Station 582+18) in Lauderdale County.

From the BOP at Station 1+74 to the EOP at Station 582+18

Work in this area shall consist of repairing failed areas in the travel lanes, widening the travel lanes, and milling and overlaying the travel lanes.

Prior to milling and overlaying, failed areas shall be removed full depth within the limits shown in the attached table. Failed joints listed in the attached table shall be removed and repaired full depth as per the Failed Joint Repair Detail. Failed areas and failed joints shall be backfilled with 19-mm, ST, asphalt. A maximum lift thickness of 3½" shall be used for backfilling. Material generated from the removal of failed areas and failed joints shall be hauled off the project. Hauling of the removal material from the project will not be measured for separate payment and will be included in the price of other items bid. Any granular base material deemed unsuitable by the Engineer shall be removed as directed. Payment for the excavation of base material will be made using pay item 203-G: Excess Excavation. Longitudinal and transverse saw cuts shall be made to provide a neat removal area. Pavement repairs shall be completed as a continuous operation in order to minimize the impact on traffic. Lane closures shall be in place until the failed area has been completely repaired.

Prior to mill and overlay, failed transverse joints shall be cleaned and filled as listed in the attached table. Cleaning and filling joints shall be performed as laid out in Section 413 of the Standard Specifications. Where seal aggregate is required, it shall be Size No. 89 meeting applicable requirements of Section 703 of the Standard Specifications. Payment for all work necessary to clean and fill joints shall be made under pay item 413-D: Cleaning and Filling Joints in PCC Pavement, 10" or Less as per Subsections 413.04 & 413.05 of the Standard Specifications.

Prior to milling and overlaying, trench widening shall be performed in order to accommodate an asphalt roadway that is 26 feet wide, with 11-foot lanes and 2-foot paved shoulders. The limits of trench widening operations are shown in the attached table. Prior to trench widening, the existing asphalt widening that has been placed beyond the original jointed reinforced concrete pavement (JRCP) shall be removed and hauled off the project. Payment for hauling the removed asphalt off the project shall be included in pay item 202-B: Removal of Asphalt Paved Shoulders, All Depths. Trench widening shall consist of excavating the shoulder six inches (6") below the existing asphalt elevation and approximately three feet (3') in width beyond the JRCP and replacing with 6" of 19-mm, ST, asphalt. The cost of blading will be an absorbed item and is to be included in the price of other items bid. The excavated granular material shall be retained and used to bring the shoulder to grade. Material which cannot be placed and blended in adjacent areas and is deemed to be excess excavation by the Engineer shall be removed under pay item 203-G: Excess Excavation. Trench widening shall not be performed through county roads. Existing concrete driveways that are in conflict with trench widening shall be saw cut and removed in order to accommodate the widening. Existing asphalt driveway pads shall be removed by excavation. Access to driveways must be provided at all times during trench widening operations.

Upon completion of the above described work, the entire roadway shall be milled 2" and overlaid with 2" of 12.5-mm, ST, asphalt. The cross slope of the new asphalt shall be 2% in tangents. If the existing asphalt pavement is not at a 2% cross slope then the cross slope shall be corrected by milling. Prior to overlay, asphalt leveling shall be performed in areas listed in the attached table in order to correct the superelevation rates of horizontal curves. Leveling will only be performed on the "high" side of horizontal curves. Asphalt leveling shall be performed using 12.5-mm, ST, asphalt, leveling mix. Maximum lift thicknesses as laid out in the Standard Specifications shall be observed during leveling operations.

Shoulders are to be graded back on a daily basis in conjunction with the asphalt overlay. The material on the shoulders shall be flush with the new asphalt pavement edge and shall be graded to an approximate 4% cross slope in order to provide for proper drainage of the roadway. All existing material generated from construction shall be used prior to the placement of granular material, crushed stone.

The asphalt pavement shall be sawed and sealed over all transverse concrete joints. The Contractor shall be responsible for collecting the locations of the existing joints in order to be able to locate the joints upon completion of the overlay. The sawing and sealing operation for each segment of roadway shall be completed within (7) days of placing the asphalt overlay. Sawing and sealing shall be performed as per Section 413 of the Standard Specifications.

General Notes:

Milling will not begin until an **approved** asphalt mix design has been received, nor until such time that, in the opinion of the Engineer, weather conditions have been consistently suitable enough to allow placement of the asphalt pavement after the milling operations. Traffic will be allowed to run on milled surfaces for no more than seven (7) days. The Contractor will be assessed a penalty of **\$5,000 per calendar day** afterwards until the milled surfaces are covered with the next lift of asphalt.

The reclaimed asphalt pavement (RAP) material removed by the milling operation shall become the property of the Contractor.

Milling should be full depth (2") at bridge approach slabs to avoid a thin lift of asphalt which will tend to shell out over time. Bridge decks on this project were previously overlaid. In order to remove the asphalt overlay on the existing bridge deck, the following criteria must be met. Any modifications to these criteria must be approved by the Director of Structures, State Bridge Engineer.

- A milling machine may be used to remove the asphalt overlay to a depth such that the milling head does not come in contact with the bridge deck. However, the asphalt thickness remaining on the bridge after milling shall not be less than one-half inch ($\frac{1}{2}$ "). Once this depth is reached, the Engineer shall examine the remaining asphalt to determine whether it requires removal. If the remaining appears to have a sufficient bond to the bridge deck, then the $\frac{1}{2}$ -inch lift may be left in place upon approval by the Director of Structures, State Bridge Engineer.
- If the remaining asphalt is not sufficiently bonded to the bridge deck, as determined by the Engineer, the Contractor must utilize another approach to remove the rest of the asphalt in a fashion that does no harm to the existing structure. Once this process is complete, the bare bridge deck shall be inspected before work continues. Upon inspection, if the deck has no signs of deterioration and the riding surface is acceptable for traffic, the deck shall be left bare upon approval by the Director of Structures, State Bridge Engineer. The removal of the $\pm\frac{1}{2}$ " will be absorbed in the milling pay item.
- In addition to milling the bridge deck, the Contractor shall also mill the existing roadway in order to provide a smooth transition between the existing alignment and the new asphalt overlay. The amount of milling necessary for this transition shall be determined in the field and approved by the Engineer.
- If the remaining $\frac{1}{2}$ -inch lift of asphalt is properly bonded or if the bridge deck shows signs of deterioration, a new lift of asphalt shall be placed upon approval by the Director of Structures, State Bridge Engineer. The new overlay course shall be $1\frac{1}{2}$ " thick.
- The use of dynamic rollers is prohibited for compaction of the asphalt lifts on the bridge deck. Only static compaction will be allowed.

Any damages that occur to the existing structure during the construction process shall be repaired as directed by the Engineer at no additional cost to the State.

It is the Contractor's responsibility to contact MS 811 anytime that excavation, post driving or other sub-surface work is taking place that may affect utilities.

Temporary traffic stripe will be required immediately after the milling and/or required overlay and prior to opening area to traffic. Temporary stripe is to be placed in the same location and configuration as the permanent stripe.

The county roads within the project limits are to be milled 2" and overlaid with 2" of 12.5-mm, ST, asphalt to the right-of-way limits. The overlay should be performed as soon as possible to

prevent damage to the pavement structure. The Contractor will be responsible for any failures that occur in the pavement structure due to delays in overlaying the roadway.

Potholes that may exist or occur in the existing pavement are to be patched in a timely manner and prior to beginning the asphalt overlay. Patching of potholes shall be considered an absorbed pay item.

Guardrails are required to be replaced at various locations within the project in order to meet MASH requirements (see attached Table for locations/quantities). All guardrail removed is to be replaced the same day and prior to reopening the adjacent lane of traffic. It is the Contractors responsibility to make necessary corrections to compensate for raising the guardrail on bridge ends if needed. A 25-foot transition may be used on Type 'A' bridge end sections to adjust to the height of the MASH compliant W-beam. No transition is needed for the Type 'E' or Type 'F' bridge end sections. The existing Type 'A' Modified bridge end sections at Bridge # 152.2 are in good condition and shall remain in place. Voids created by the removal of posts, concrete anchors, footings, etc. shall be backfilled and tamped in accordance with Section 203 of the Standard Specifications.

Some of the thrie beam blockout connections are missing at the railroad underpass (known as Bridge #: 147.4). The missing blockouts shall be replaced as per the As-Built drawing labeled "Railroad Underpass Site #3 Bridge #147.4 Blockout Details". Payment for replacing the missing blockouts shall be included in pay item 606-B: Guard Rail, Class A, Type 1, Thrie Beam.

All guardrail pads shall be cleaned and swept in conjunction with replacement operations. Grading should be performed behind the guardrail to allow water to drain off the shoulder of the roadway. Cleaning, sweeping and grading around guardrails will not be measured for separate payment and will be considered included in the price of other items bid.

All guard rails, including rail, terminal end sections, bridge end sections, posts and other appurtenances, will become property of MDOT and shall be delivered by the Contractor to the Meridian Maintenance Headquarters. The Contractor shall coordinate the delivery of these items with MDOT in advance. The Contractor shall not damage or disturb the existing guardrail or posts during the grading or the paving operation.

Temporary pavement transition joints (paper joints) shall be at least three (3) paper widths long, shall be used at milled tie-ins, and shall be adequately maintained. Sand or dirt from the edge of roadway shall not be used as a substitute for treated paper.

Privately owned entrances shall be paved to the shoulder line per the included typical drawing. The asphalt thickness shall be consistent with that of the surface lift. All existing driveway pads shall be overlaid. Existing pads will be milled at a depth of 2" and paved with 2" of 12.5-mm, ST, asphalt. Additional pads shall be placed at locations as directed by the Engineer. Pads shall be shaped horizontally and vertically to prevent excessive drop-offs. Grading for the placement of new pads shall be done as required. Grading for new pads will not be paid separately. The excavated material shall be retained and used to raise the existing shoulder to match the new pavement elevation. Granular Material, Crushed Stone, if needed, shall be provided around the

pads to prevent shoulder drop-offs as directed and shall be placed in a timely manner. Drop-offs exceeding 2½" shall be corrected within two (2) calendar days of the placement of the pad. Prior to placing granular material, all existing shoulder material shall be used first.

It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the engineer, any structures damaged during the life of the contract. No payment will be made for replacement or repair of damaged items.

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost is to be included in the price bid for pay item 618-A: Maintenance of Traffic, unless shown on the Construction Signing Schedule. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background. Standard roadside construction signs and barricades will be paid for using the appropriate pay items.

Temporary Portable Rumble Strips, as described in Special Provision No. 907-619, shall be used in advance of each lane closure. Payment shall be made under pay item 907-619-B: Temporary Portable Rumble Strips. At the conclusion of the project, MDOT will take possession of the temporary portable rumble strips used for the project.

Roadside construction signs, barrels, etc. shall be placed in accordance with the attached drawings or as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed.

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

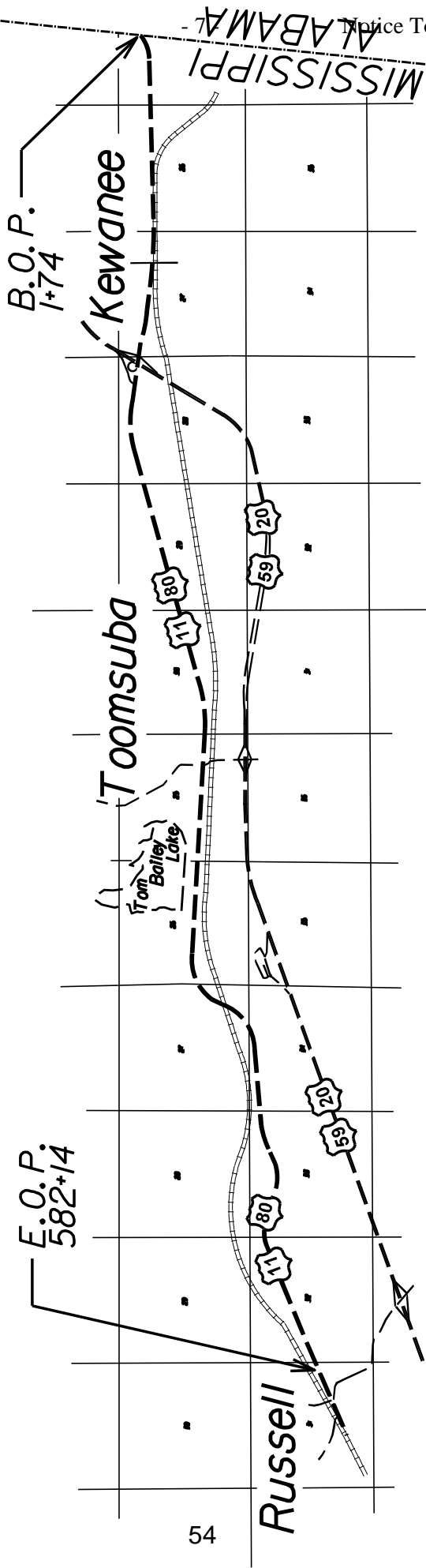
The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal; the cost is to be included in the prices of items bid. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor. As described in the applicable Notice-To-Bidders, final project cleanup is required and will be completed prior to the scheduling of the final inspection.

Permanent pavement markers are to be placed in accordance with the attached drawings and Standard Drawings. Two-way yellow markers are to be placed on two-way roads and county roads. Two-way clear markers are to be placed on county roads as per the attached detail.

All permanent striping will be thermoplastic. Edge lines shall be placed to accommodate the lane widths shown on the applicable typical sections unless prevented by field conditions. Striping on centerline, edge lines, and lane lines shall be 90-mil Double Drop Thermoplastic with RPM's on centerline. Symbols shall be 120-mil Double Drop Thermoplastic. Striping on bridge deck centerlines, edge lines, and lane lines shall be 90-mil Double Drop Thermoplastic. Rumble stripe will be placed on all 2-foot widening in accordance with the attached detail. Permanent striping on county roads shall be placed in accordance with the attached drawings or as directed by the Engineer.

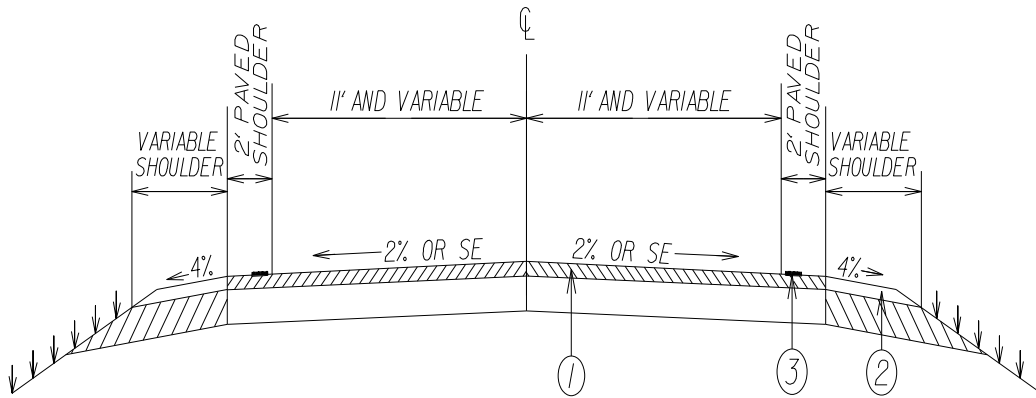
Prior to final inspection, all bridges and curb & gutter are to be swept clean of debris.

MP-5011-38(010)/306547301



MILL AND OVERLAY TYPICAL SECTION

STATIONS:
BOP 1+74 THROUGH STA 122+00
STA 180+00 THROUGH EOP 582+14

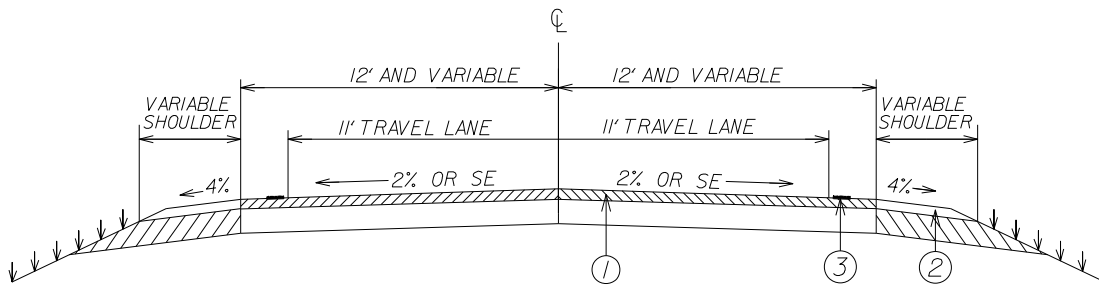


- ① MILL 2" AND VARIABLE EXISTING ASPHALT ON MAINLINE AND PLACE BACK 2" 12.5mm ST ASPHALT.
- ② VARIABLE DEPTH GRANULAR MATERIAL (CRUSHED STONE) AS REQUIRED.
- ③ RUMBLE STRIPE (AS SHOWN ON THE ATTACHED DRAWING).

*** EXISTING STRUCTURE: 3" TO 4.5" OF HMA OVER ORIGINAL 7" JRCP

MILL AND OVERLAY TYPICAL SECTION

STATIONS:
STA 122+00 THROUGH STA 180+00



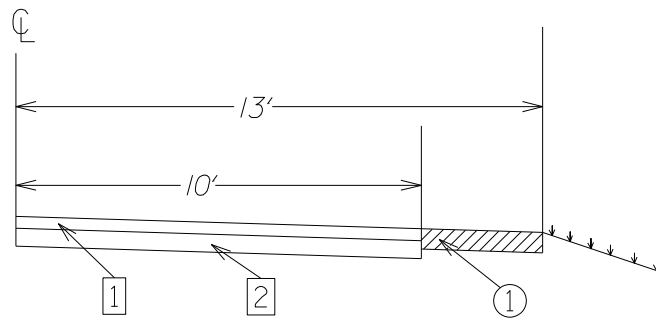
- ① MILL 2" AND VARIABLE EXISTING ASPHALT ON MAINLINE AND PLACE BACK 2" 12.5mm ST ASPHALT.
- ② VARIABLE DEPTH GRANULAR MATERIAL (CRUSHED STONE) AS REQUIRED.
- ③ RUMBLE STRIPE (AS SHOWN ON THE ATTACHED DRAWING).

*** EXISTING STRUCTURE: 6.5" HMA OVER 16" GRANULAR MATERIAL

TYPICAL SECTION
TRENCH WIDENING

STATIONS:

* REFER TO TABLE FOR LOCATIONS



EXISTING

- 1 3" TO 4.5" AND VARIABLE ASPHALT PAVEMENT
- 2 7" OF JOINTED REINFORCED CONCRETE PAVEMENT

PROPOSED

- 1 PRIOR TO MILL AND OVERLAY, WIDEN EXISTING 10' LANES TO 13' BY TRENCH WIDENING. REMOVE THE EXISTING VARIABLE DEPTH, 2 FOOT WIDE ASPHALT SHOULDER WIDENING AND EXCAVATE 6" DEEP X 3' WIDE AND PLACE 6" 19mm MIX ST. NOTE: THE LIMITS OF TRENCH WIDENING VARY (SEE ATTACHED SPREADSHEET).

**US 11/80 From Alabama Line to East of Russell
403-B003: 12.5-MM, ST, ASPHALT PAVEMENT, LEVELING**

Begin Station	End Station	Lane	Max. Depth Required (Inches)	Estimated Asphalt Required (Tons)	Comments
1+74	13+50	Left	3	65	Right Hand Curve: Level "High Side" to achieve 3% Superelevation
395+00	421+00	Right	5	120	Left Hand Curve: Level "High Side" to achieve 3% Superelevation
424+00	446+50	Left	5	160	Right Hand Curve: Level "High Side" to achieve 3% Superelevation
494+00	517+00	Left	5	190	Right Hand Curve: Level "High Side" to achieve 3% Superelevation
523+25	524+75	Right	4	15	Dip in Roadway: Level to re-establish -2% cross-slope

TOTAL FOR ASPHALT LEVELING: 550 TONS

Note: Single Lift Laying Thickness, as specified in Section 401.02.4 of the Standard Specifications shall be observed during asphalt leveling operations.

US 11/80 From Alabama Line to East of Russell
403-C003: 19-MM, ST, ASPHALT PAVEMENT, TRENCH WIDENING

*Note: The linear footages shown below are field measured. The station numbers do not accurately represent the linear footages due to equation stations.

Left of Left Lane		
Begin Station	End Station	Linear Feet
1+74	17+35	1,561
18+10	96+90	7,840
98+80	104+00	520
111+30	125+30	1,400
175+50	210+45	3,344
211+15	217+75	667
218+55	307+93	8,928
316+00	366+32	5,015
369+40	421+45	5,209
424+45	447+52	2,048
448+50	477+20	2,773
478+00	532+00	5,353
532+65	550+90	1,777
551+83	563+23	840
563+94	582+16	1,836

Total Lt/Lt Ln = 49,111

Right of Right Lane		
Begin Station	End Station	Linear Feet
1+74	20+65	1,891
22+20	23+40	120
25+32	97+10	7,140
100+20	102+70	250
111+30	123+00	1,170
175+50	187+16	1,028
187+90	188+72	82
189+45	220+65	3,120
221+18	222+35	117
225+05	226+10	105
228+00	231+45	345
232+30	301+88	6,958
302+38	305+15	277
316+00	340+20	2,420
343+30	380+15	3,685
381+00	421+75	4,075
424+75	465+25	3,716
465+75	508+90	3,968
509+85	524+25	1,416
525+00	582+16	5,736

Total Rt/Rt Ln = 47,619

TOTAL FOR TRENCH WIDENING: 96,730 LINEAR FEET
--

MDOT PROJECT NO. MP-5011-38(010)/306547301		
MILL AND OVERLAY HWY 11/80 FROM EAST OF RUSSELL TO ALABAMA LINE		
CLEANING AND FILLING JOINTS IN PCC PAVEMENT, 10" OR LESS		
Station	Location	Length (LF)
06+60	Rt.Ln	10
06+86	Rt.Ln.	10
11+86	Rt. & Lt.Ln.	20
13+40	Rt. & Lt.Ln	20
14+40	Rt. & Lt.Ln	20
20+95	Rt. & Lt.Ln.	20
23+65	Lt.Ln	10
25+00	Rt. & Lt.Ln	20
27+00	Rt. & Lt.Ln	20
35+50	Lt.Ln	10
43+05	Lt.Ln	10
95+13	Lt.Ln	10
106+47	Rt. & Lt.Ln	20
118+65	Rt. & Lt.Ln	20
185+15	Lt.Ln	10
214+70	Rt.Ln	10
222+10	Lt.Ln	10
222+20	Lt.Ln	10
222+25	Rt.Ln	10
232+00	Lt.Ln	10
240+90	Lt.Ln	10
251+60	Lt.Ln	10
255+33	Rt.Ln	10
266+12	Rt.Ln	10
274+90	Rt. & Lt.Ln	20
280+90	Lt.Ln	10
281+10	Rt.Ln	10
287+65	Rt. & Lt.Ln	20
291+90	Rt. & Lt.Ln	20

MDOT PROJECT NO. MP-5011-38(010)/306547301		
MILL AND OVERLAY HWY 11/80 FROM EAST OF RUSSELL TO ALABAMA LINE		
CLEANING AND FILLING JOINTS IN PCC PAVEMENT, 10" OR LESS		
Station	Location	Length (LF)
294+67	Lt.Ln	10
297+70	Rt. & Lt.Ln	20
300+65	Rt. & Lt.Ln	20
309+25	Rt.Ln	10
312+60	Rt.Ln	10
350+40	Rt.Ln	10
363+85	Rt.Ln	10
366+00	Lt.Ln	10
370+80	Rt.Ln	10
378+80	Rt.Ln	10
392+60	Centerline	140
404+00	Rt. & Lt.Ln	20
419+98	Rt. & Lt.Ln	20
432+40	Rt. & Lt.Ln	20
440+75	Rt.Ln	10
441+50	Lt.Ln	10
462+30	Lt.Ln	10
468+25	Rt.Ln	10
473+62	Rt. & Lt.Ln	20
479+75	Rt.Ln	10
480+65	Lt.Ln	10
489+20	Lt.Ln	10
518+25	Rt.Ln	10
520+40	Rt. & Lt.Ln	20
524+70	Rt. & Lt.Ln	20
525+75	Rt.Ln	10
531+50	Rt. & Lt.Ln	20
534+25	Rt.Ln	10

MDOT PROJECT NO. MP-5011-38(010)/306547301		
MILL AND OVERLAY HWY 11/80 FROM EAST OF RUSSELL TO ALABAMA LINE		
CLEANING AND FILLING JOINTS IN PCC PAVEMENT, 10" OR LESS		
Station	Location	Length (LF)
549+12	Lt.Ln	10
549+45	Rt.Ln	10
575+30	Rt. & Lt.Ln	20
TOTAL		940 LF
Average asphalt depth is assumed at 5" thick		

MDOT PROJECT NO. MP-5011-38(010)/306547301

MILL AND OVERLAY HWY 11/80 FROM EAST OF RUSSELL TO ALABAMA LINE

JOINT REPAIR AND FAILED AREAS

Station	Location	Length (LF)	Width (LF)	Area (SF)	Removal of concrete overlaid with asphalt (SY)	Asphalt, 19mm Mix (Tons)	Saw Cut Full Depth (LF)
FAILED AREAS							
394+00	Lt.Ln	150	10	1500	166.667	146	176
397+00	Lt.Ln	60	10	600	66.667	58.5	86
403+00	Rt.&Lt.Ln	150	20	3000	333.333	292.5	202
JOINT REPAIR AREAS							
02+50	Lt.Ln.	6	10	60	6.667	4.5	26
02+72	Rt.Ln.	6	10	60	6.667	4.5	26
09+40	Rt. & Lt.Ln.	6	20	120	13.333	9	46
23+50	Rt. & Lt.Ln	6	20	120	13.333	9	46
30+50	Rt. & Lt.LN	6	20	120	13.333	9	46
37+95	Rt. & Lt.Ln.	6	20	120	13.333	9	46
43+50	Rt. & Lt.Ln	6	20	120	13.333	9	46
43+65	Lt.Ln	6	10	60	6.667	4.5	26
47+15	Rt.Ln.	6	10	60	6.667	4.5	26
48+55	Rt. & Lt.Ln	6	20	120	13.333	9	46
52+25	Rt. & Lt.Ln	6	20	120	13.333	9	46
59+30	Rt. & Lt.Ln	6	20	120	13.333	9	46
61+67	Lt.Ln.	6	10	60	6.667	4.5	26
66+95	Rt. & Lt.Ln	6	20	120	13.333	9	46
72+55	Rt. & Lt.Ln	6	20	120	13.333	9	46
76+05	Rt.Ln	6	10	60	6.667	4.5	26
81+63	Rt & Lt.Ln.	6	20	120	13.333	9	46
88+40	Rt. & Lt.Ln.	6	20	120	13.333	9	46
92+06	Rt. & Lt.Ln	6	20	120	13.333	9	46
98+55	Rt. & Lt.Ln	6	20	120	13.333	9	46
102+58	Rt. & Lt.Ln.	6	20	120	13.333	9	46
114+10	Rt. & Lt.Ln	6	20	120	13.333	9	46
118+15	Rt.Ln	6	10	60	6.667	4.5	26
188+40	Rt.Ln	6	10	60	6.667	4.5	26
228+05	Rt. & Lt.Ln	6	20	120	13.333	9	46
231+95	Rt.Ln	6	10	60	6.667	4.5	26
240+40	Rt.Ln	6	10	60	6.667	4.5	26
244+55	Rt. & Lt.Ln	6	20	120	13.333	9	46
249+05	Rt. & Lt.Ln	6	20	120	13.333	9	46

MDOT PROJECT NO. MP-5011-38(010)/306547301

MILL AND OVERLAY HWY 11/80 FROM EAST OF RUSSELL TO ALABAMA LINE

JOINT REPAIR AND FAILED AREAS

Station	Location	Length (LF)	Width (LF)	Area (SF)	Removal of concrete overlaid with asphalt (SY)	Asphalt, 19mm Mix (Tons)	Saw Cut Full Depth (LF)
251+35	Rt.Ln	6	10	60	6.667	4.5	26
257+15	Lt.Ln	6	10	60	6.667	4.5	26
263+20	Lt.Ln	6	10	60	6.667	4.5	26
263+70	Rt.Ln	6	10	60	6.667	4.5	26
322+20	Rt.Ln	6	10	60	6.667	4.5	26
326+50	Rt.Ln	6	10	60	6.667	4.5	26
335+10	Rt.& Lt.Ln	6	20	120	13.333	9	46
346+05	Rt.& Lt.Ln	6	20	120	13.333	9	46
354+80	Rt.& Lt.Ln	6	20	120	13.333	9	46
359+45	Lt.Ln	6	10	60	6.667	4.5	26
360+75	Rt.Ln	6	10	60	6.667	4.5	26
367+10	Rt.Ln	6	10	60	6.667	4.5	26
370+50	Rt.Ln	6	10	60	6.667	4.5	26
390+00	Rt.& Lt.Ln	6	20	120	13.333	9	46
397+10	Rt.Ln	6	10	60	6.667	4.5	26
407+50	Rt.& Lt.Ln	6	20	120	13.333	9	46
424+18	Lt.Ln	6	10	60	6.667	4.5	26
425+70	Rt.Ln	6	10	60	6.667	4.5	26
454+00	Rt.Ln	6	10	60	6.667	4.5	26
454+25	Lt.Ln	6	10	60	6.667	4.5	26
457+35	Lt.Ln	6	10	60	6.667	4.5	26
468+05	Lt.Ln	6	10	60	6.667	4.5	26
485+30	Rt.& Lt.Ln	6	20	120	13.333	9	46
494+30	Rt.& Lt.Ln	6	20	120	13.333	9	46
511+60	Lt.Ln	6	10	60	6.667	4.5	26
553+10	Rt.& Lt.Ln	6	20	120	13.333	9	46
574+34	Rt.Ln	6	10	60	6.667	4.5	26
575+35	Lt.Ln	6	10	60	6.667	4.5	26
TOTAL					1126.67	875.00	2486.00

Assuming asphalt is 5" thick, and concrete is 7" thick for a total of 1'
 Excess excavation shall only be used if underlying material is deemed unsuitable for use

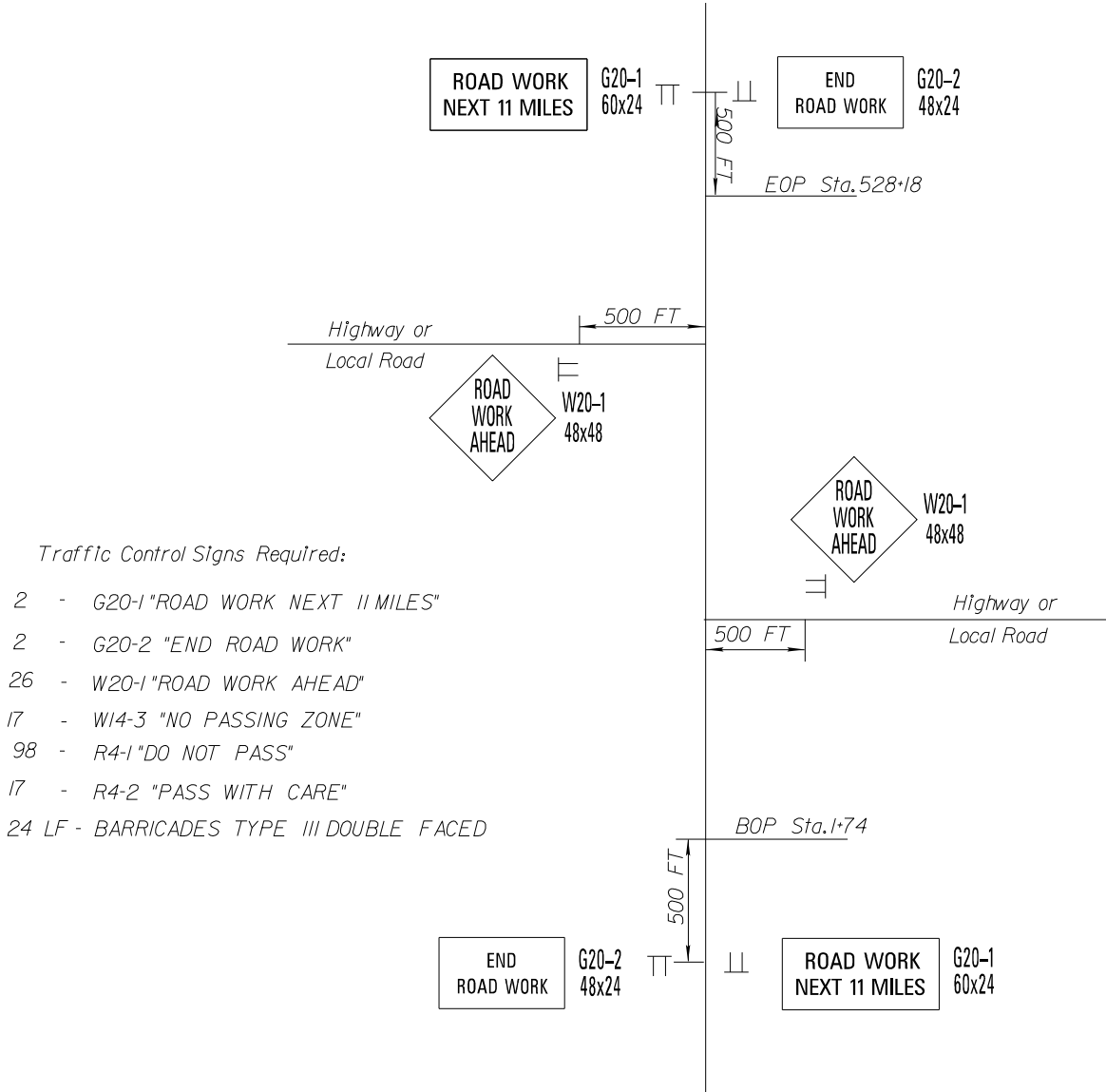
MDOT PROJECT NO. MP-5011-38(010)/306547301
MILL AND OVERLAY HWY 11/80 FROM EAST OF RUSSELL TO ALABAMA LINE

GUARD RAIL REMOVAL AND INSTALLATION

BRIDGE NO	DIRECTION	REMOVAL (LF)	BRIDGE END SECTION TYPE			"W" BEAM (LF)	THRIE BEAM (LF)	THRIE BEAM TRANSITION SECTION (LF)	SPECIAL SECTION, BRIDGE CONNECTOR	TERMINAL END SECTION, FLARED (EA)	DELINEATORS (WHITE) (EA)	OBJECT MARKERS	
			TYPE "A"	TYPE "E"	TYPE "F"							OM-3R (EA)	OM-3L (EA)
147.4	EAST BOUND	316				179	50	12.5		2	11	1	1
147.4	WEST BOUND	316				179	50	12.5		2	11	1	1
148.9	EAST BOUND	241.5	1		1	140			1	2	11	1	1
148.9	WEST BOUND	241.5	1		1	140			1	2	11	1	1
152.2	EAST BOUND	255				140			2	2	11	1	1
152.2	WEST BOUND	255				140			2	2	11	1	1
152.6	EAST BOUND	292.5	2						2	2	11	1	1
152.6	WEST BOUND	250	2						2	2	11	1	1
TOTALS			4	2	2	918	100	25	10	16	88	8	8

* Removal of guardrails, including rails, posts, and terminal ends will be paid under ,pay item 202-B158
 * Existing Type 'A' Modified Bridge End Sections on Bridge 152.2 Shall Remain in Place.

CONSTRUCTION SIGNING DETAIL

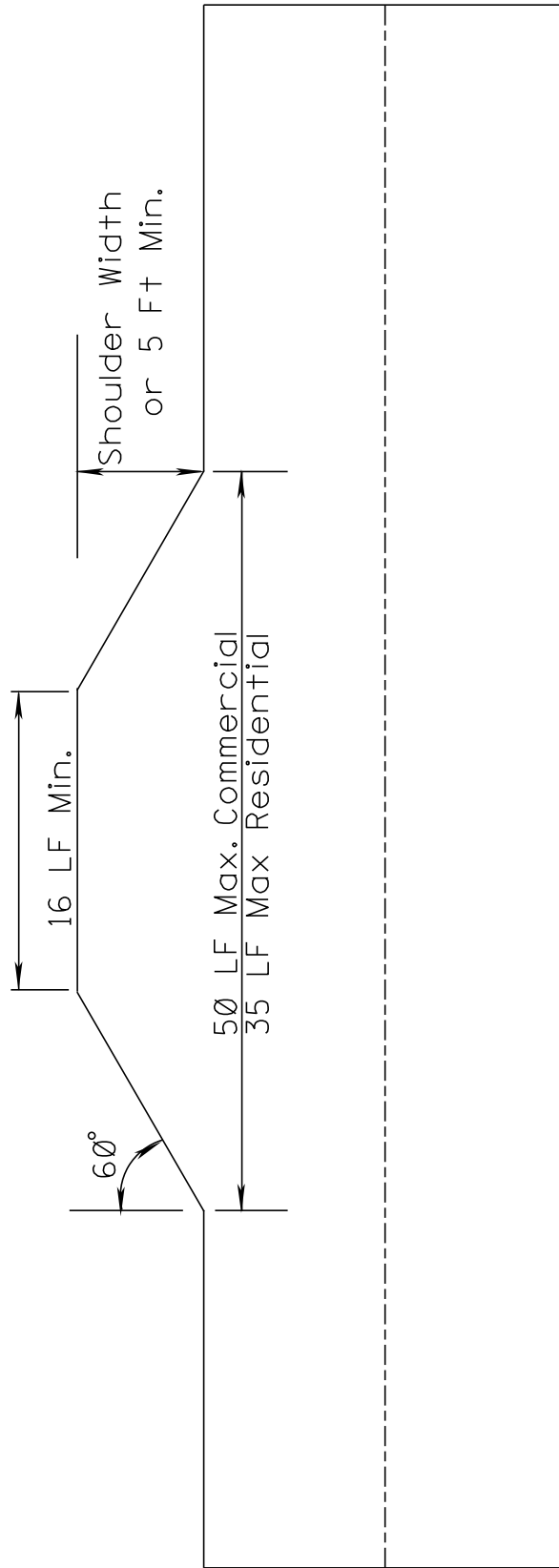


NOTES: One (1) W20-1 "ROAD WORK AHEAD" Sign Is Required at each Local Road, street or highway Entering the Project.
 Location of additional W20-1 (ROAD WORK AHEAD) signs are shown on the attached table.
 G20-1 and G20-2 signs mounted on Type III Double Faced Barricade. Placed a minimum of 500' before the BOP and EOP.

**MDOT PROJECT NO. MP-5011-38(010)/306547301
W20-1 "ROAD WORK AHEAD" SIGNS FOR COUNTY ROADS**

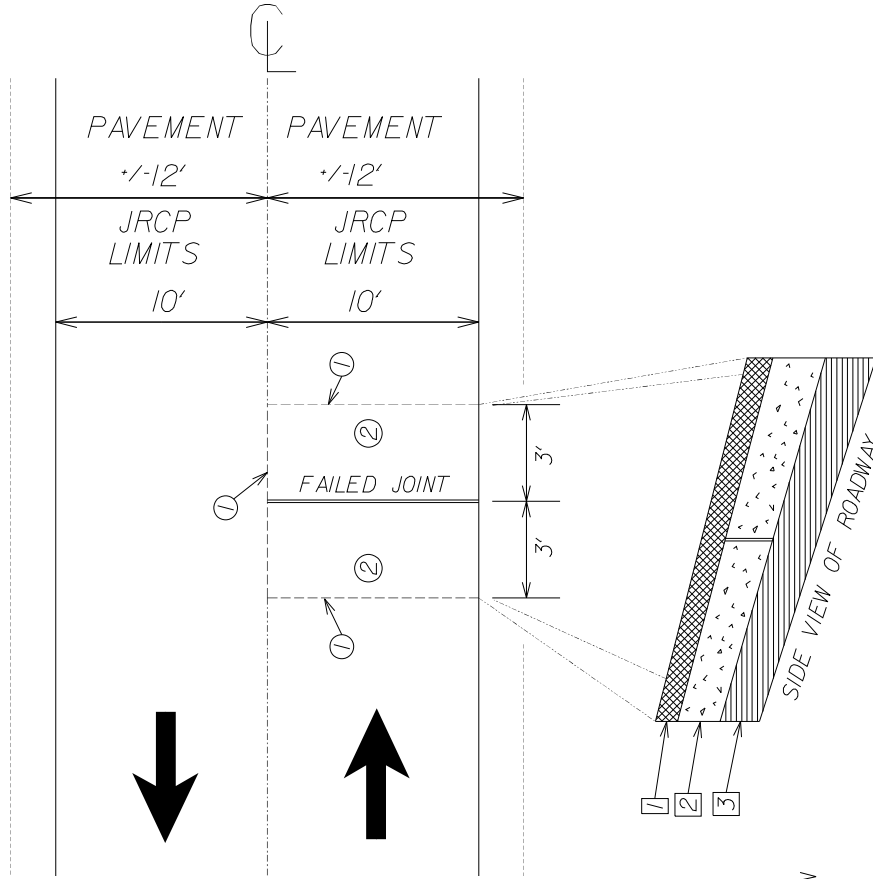
STATION	ROAD NAME	W20-1 (16 SF)
18+00	RICHARD JOHNSON	16
31+00	LARKIN	16
97+50	KEWANEE Lt.Ln	16
97+50	KEWANEE Rt.Ln	16
138+00	I-20 /US 11 EAST	16
148+00	I-20 /US 11 WEST	16
188+00	JO THOMAS	16
189+00	JO THOMAS	16
211+00	J.B. GILL	16
218+00	SMITH SPURR	16
221+00	PRISOCK	16
232+00	BUTTS	16
262+00	DEAVERS	16
302+00	DIXIE HIGHWAY SCHOOL	16
312+00	JEFF PRICE	16
315+00	LAUDERDALE TOOMSUBA	16
315+00	WILL GARRETT	16
381+00	T. BAILEY	16
448+00	TELEPHONE STATION	16
465+50	WHITE YARRELL	16
477+50	BETTS RADCLIFF	16
509+00	BILLY HARPER	16
524+50	BILLY HARPER	16
532+00	SHIRLEY CIRCLE	16
551+00	SHIRLEY CIRCLE	16
563+00	DEES	16
TOTAL W20-1 "ROAD WORK AHEAD" SIGNS		416 SF

TYPICAL RAMP/PAD DETAIL



FAILED JOINT REPAIR
TYPICAL SECTION

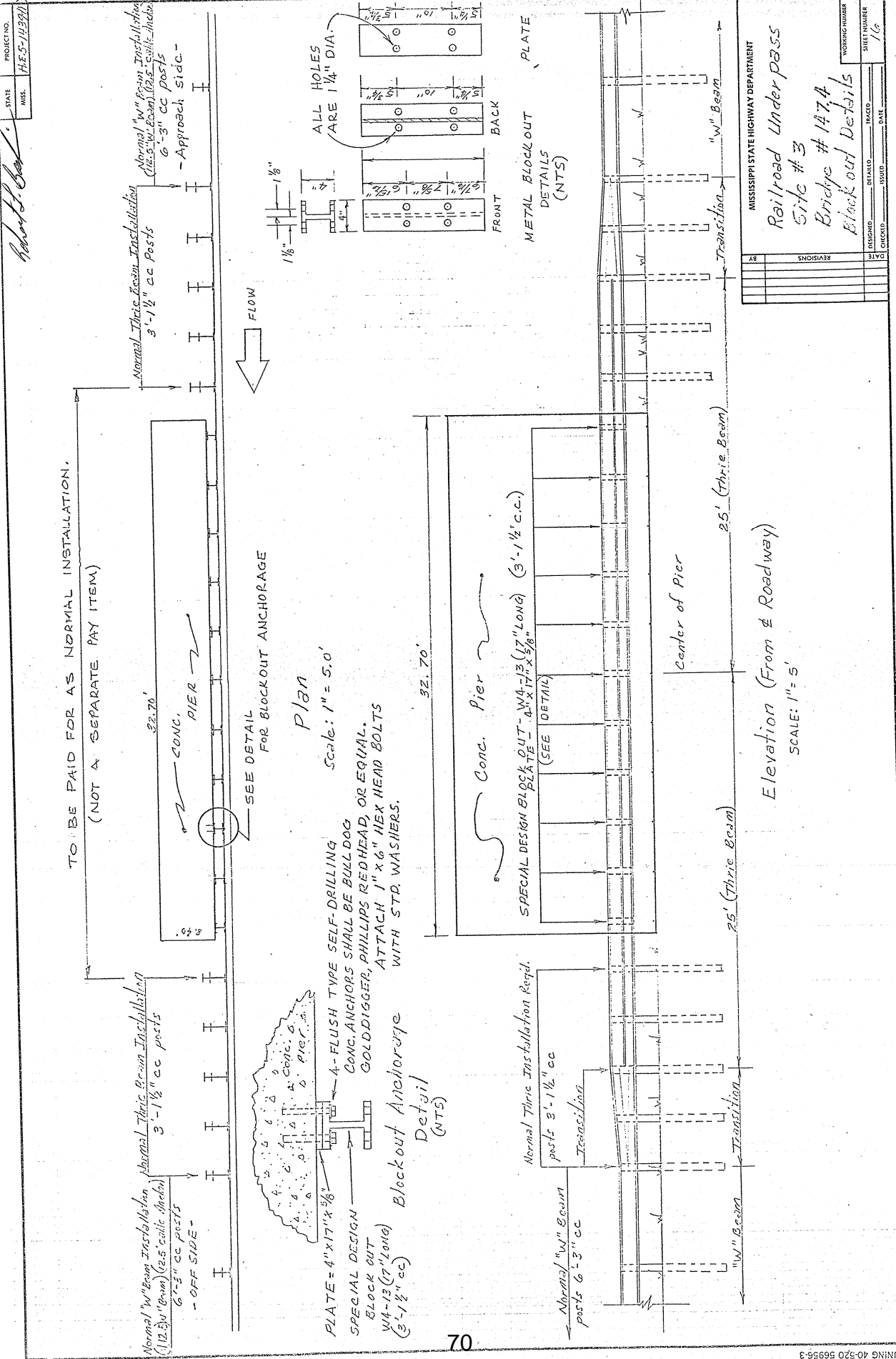
SEE ATTACHED TABLE FOR LOCATIONS



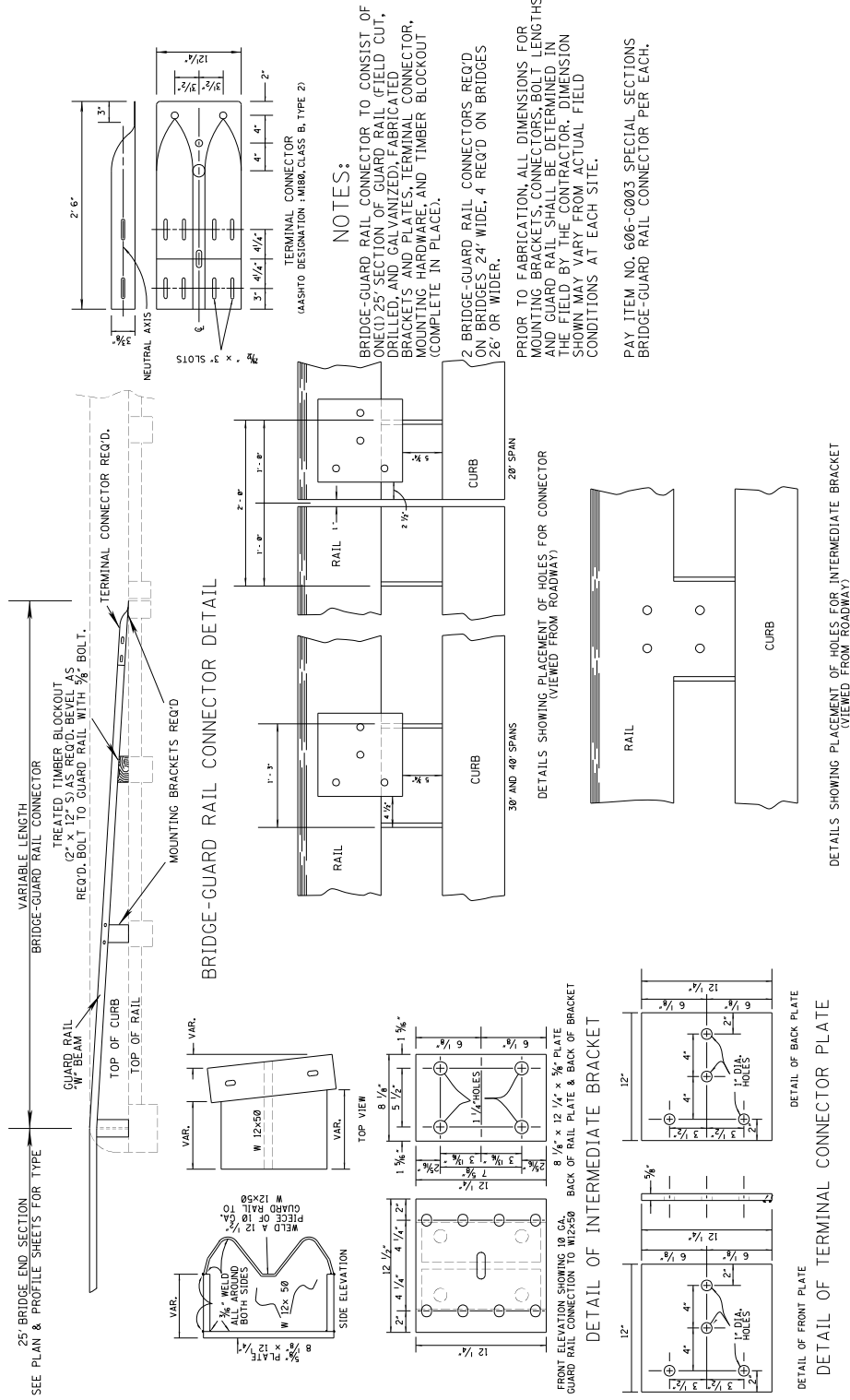
- ① SAW CUT FULL DEPTH 3' ON BOTH SIDES OF FAILED JOINT AND ALONG CENTERLINE
- ② REMOVE CONCRETE OVERLAY WITH ASPHALT AND FILL WITH 19 mm ST ASPHALT PAVEMENT TO EXISTING GRADE OF TRAVEL LANE. BACK FILL ANY VOIDS ON SHOULDER CAUSED BY REMOVAL OF FAILED JOINT WITH EXISTING MATERIAL

- 1 EXISTING 3" TO 4.5" AND VARIABLE ASPHALT PAVEMENT
- 2 EXISTING 7" JRCP (JOINTED REINFORCED CONCRETE PAVEMENT)
- 3 EXISTING UNDERLYING BASE MATERIAL

NOTE:
BASE MATERIAL DEEMED UNSUITABLE BY THE ENGINEER SHALL BE REMOVED USING PAY ITEM 203-G002:EXCESS EXCAVATION



LAUDERDALE COUNTY - 306547/301000
HIGHWAY 11 & 80
SPECIAL DESIGN
BRIDGE-GUARD RAIL CONNECTOR

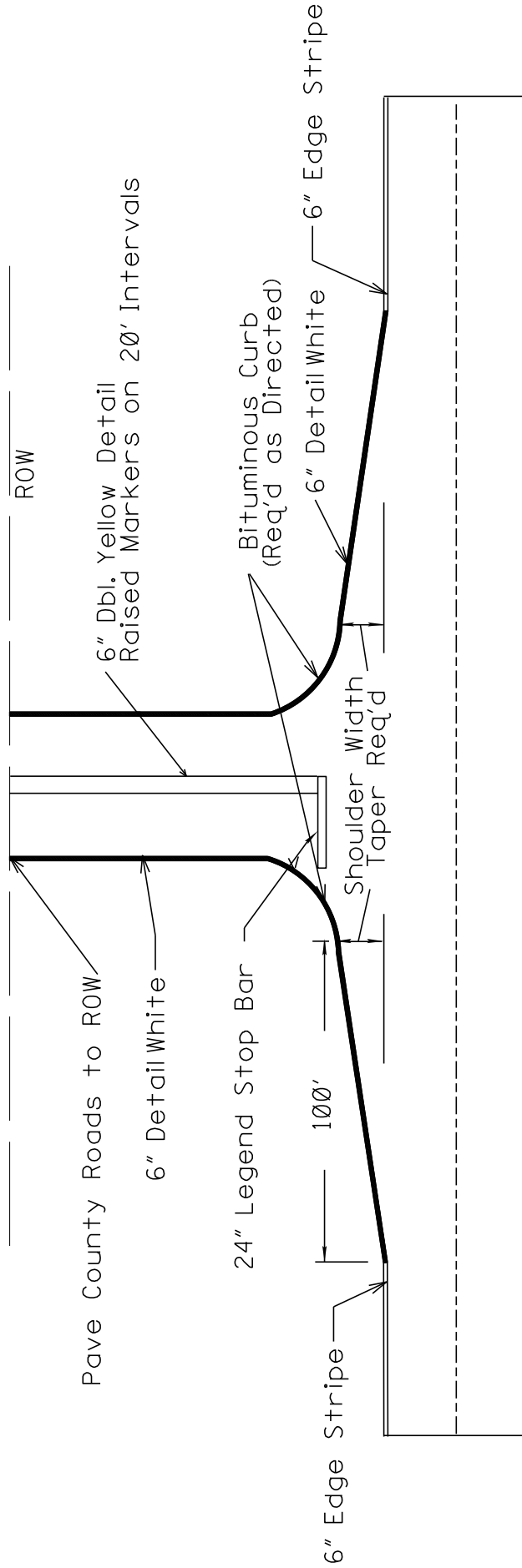


NOTES:

- BRIDGE-GUARD RAIL CONNECTOR TO CONSIST OF ONE(1) 25' SECTION OF GUARD RAIL (FIELD CUT, DRILLED AND GALVANIZED), FABRICATED BRACKETS AND PLATES, TERMINAL CONNECTOR, MOUNTING HARDWARE, AND TIMBER BLOCKOUT (COMPLETE IN PLACE).
- 2 BRIDGE-GUARD RAIL CONNECTORS REQ'D ON BRIDGES 24' WIDE, 4 REQ'D ON BRIDGES 26' OR WIDER.
- PRIOR TO FABRICATION, ALL DIMENSIONS FOR MOUNTING BRACKETS, CONNECTORS, BOLT LENGTHS AND GUARD RAIL SHALL BE DETERMINED IN THE FIELD BY THE CONTRACTOR. DIMENSION SHOWN MAY VARY FROM ACTUAL FIELD CONDITIONS AT EACH SITE.

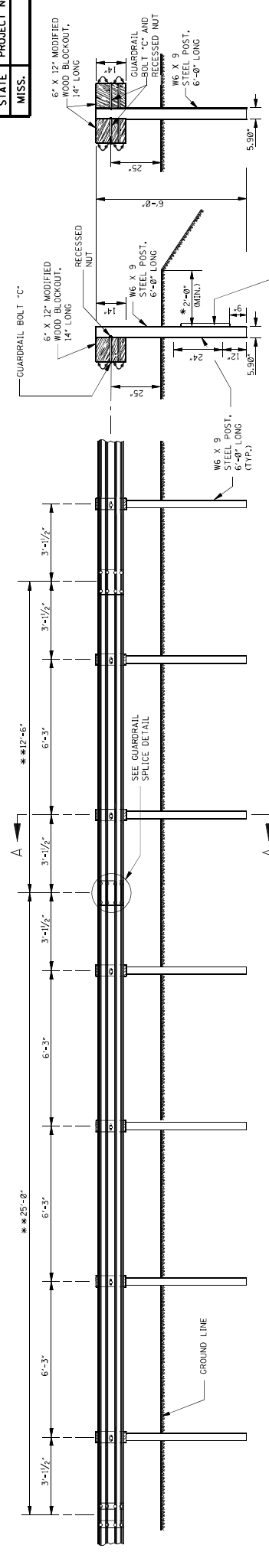
PAY ITEM NO. 606-C003 SPECIAL SECTIONS
 BRIDGE-GUARD RAIL CONNECTOR PER EACH.

Typical Section - County Roads

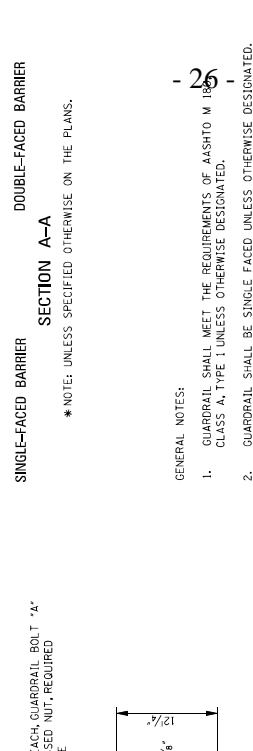


*SEE ATTACHED SHEET FOR TWO-WAY CLEAR RPM DETAIL

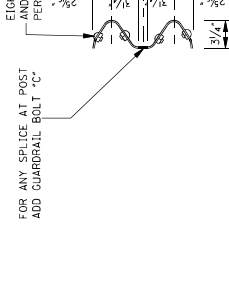
STATE	PROJECT NO.
MISS.	



ELEVATION FROM \bar{C} ROADWAY
 ** NOTE: OPTIONAL BEAM LENGTHS.



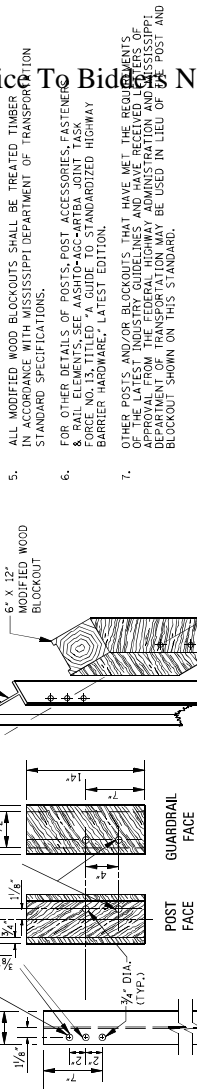
SECTION A-A
 DOUBLE-FACED BARRIER
 ** NOTE: UNLESS SPECIFIED OTHERWISE ON THE PLANS.



GUARDRAIL SPLICE DETAIL



DETAILS OF ADJUSTABLE HEIGHT BLOCKOUT ASSEMBLY



FASTENER DETAILS

NOTES:
 1. ALL GUARDRAIL BOLTS ARE 3/8" - 11 UNC. THREAD PITCH.
 2. IF ANY BOLT EXTENDS MORE THAN 1/2" FROM THE NUT, THE BOLT SHOULD BE TRIMMED BACK.

BOLT	L (MIN.)	T
"A"	1 1/2"	1"
"B"	2"	1 3/4"
"C"	14"	2"

GENERAL NOTES:

- GUARDRAIL SHALL MEET THE REQUIREMENTS OF AASHTO M 180 CLASS A, TYPE 1 UNLESS OTHERWISE DESIGNATED.
- GUARDRAIL SHALL BE SINGLE FACED UNLESS OTHERWISE DESIGNATED.
- GUARDRAIL SECTIONS SHALL BE LAPPED IN THE DIRECTION OF TRAFFIC FLOW FOR THE LANE NEAREST THE GUARDRAIL. THE ONLY EXCEPTION NOTED IS THAT GUARDRAIL IS TO BE LAPPED FOR APPROACHING TRAFFIC ON A BRIDGE WITH 2-WAY TRAFFIC.
- STEEL POSTS SHALL CONFORM TO ASTM A36 (EXCEPT ULTRASONIC TESTING). THEY SHALL BE GALVANIZED IN ACCORDANCE WITH AASHTO M 111 EXCEPT WHEN CORROSION RESISTANT STEEL IS REQUIRED IN AREAS OF HIGH CORROSION POTENTIAL. GALVANIZING SHALL BE PERMITTED FOR CORROSION RESISTANCE AND SHALL NOT BE PERMITTED FOR GALVANIZING EXCEPT FOR HOLES TO MOUNT GUARDRAIL DELTA BOLTS.
- ALL MODIFIED WOOD BLOCKOUTS SHALL BE TREATED TIMBERS IN ACCORDANCE WITH MISSISSIPPI DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS.
- FOR OTHER DETAILS OF POSTS, POST ACCESSORIES, FASTENERS, AND OTHER HARDWARE, REFER TO THE MISSISSIPPI DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS, SECTION 602.01, "ROADWAY BARRIER HARDWARE," LATEST EDITION.
- OTHER POSTS, AND/OR BLOCKOUTS THAT HAVE MET THE REQUIREMENTS OF THE LATEST INDUSTRY GUIDELINES AND HAVE RECEIVED THE APPROVAL FROM THE FEDERAL HIGHWAY ADMINISTRATION AND MISSISSIPPI DEPARTMENT OF TRANSPORTATION MAY BE USED IN LIEU OF THE POST AND BLOCKOUT SHOWN ON THIS STANDARD.

DATE	REVISION	ISSUE DATE:	FEBRUARY, XX, 2017

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN
**GUARDRAIL:
 "W" BEAM
 (STEEL POSTS)**

CALVANIZED STEEL 16G COMMON NAIL (TYP.) DRIVE NAIL AT CENTER OF BLOCK AND POST AFTER BOLT IS INSTALLED.

GUARDRAIL BOLT "E" WITH PLAIN ROUND WASHER "F" UNDER HEAD FOR POSTS 1 THRU 3.

GUARDRAIL BOLT "D" WITH PLAIN ROUND WASHER "F" UNDER HEAD FOR POSTS 4 THRU 6.

6" X 8" WOOD POST, 6'-0" LONG, WITH BLOCKOUT, 14" LONG, FOR POSTS 1 THRU 3.

10" X 10" WOOD POST, 6'-0" LONG, WITH BLOCKOUT, 14" LONG, FOR POSTS 4 THRU 6.

GUARDRAIL BOLT "E" WITH PLAIN ROUND WASHER "F" UNDER HEAD FOR POSTS 1 THRU 3.

GUARDRAIL BOLT "D" WITH PLAIN ROUND WASHER "F" UNDER HEAD FOR POSTS 4 THRU 6.

6" X 8" WOOD POST, 6'-0" LONG, WITH BLOCKOUT, 14" LONG, FOR POSTS 1 THRU 3.

10" X 10" WOOD POST, 6'-0" LONG, WITH BLOCKOUT, 14" LONG, FOR POSTS 4 THRU 6.

GUARDRAIL BOLT "E" WITH PLAIN ROUND WASHER "F" UNDER HEAD FOR POSTS 1 THRU 3.

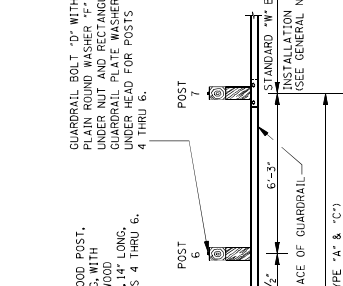
GUARDRAIL BOLT "D" WITH PLAIN ROUND WASHER "F" UNDER HEAD FOR POSTS 4 THRU 6.

6" X 8" WOOD POST, 6'-0" LONG, WITH BLOCKOUT, 14" LONG, FOR POSTS 1 THRU 3.

10" X 10" WOOD POST, 6'-0" LONG, WITH BLOCKOUT, 14" LONG, FOR POSTS 4 THRU 6.

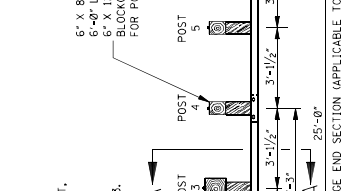
GUARDRAIL BOLT "E" WITH PLAIN ROUND WASHER "F" UNDER HEAD FOR POSTS 1 THRU 3.

GUARDRAIL BOLT "D" WITH PLAIN ROUND WASHER "F" UNDER HEAD FOR POSTS 4 THRU 6.



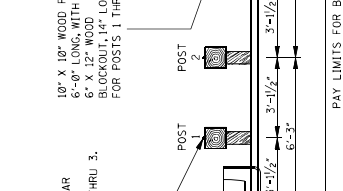
SECTION A-A

* NOTE: UNLESS OTHERWISE SPECIFIED ON THE PLANS.



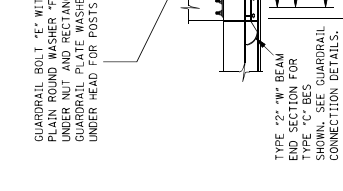
PLAN OF BRIDGE END SECTION

PAY LIMITS FOR BRIDGE END SECTION APPLICABLE TO TYPE "A", "B", "C".



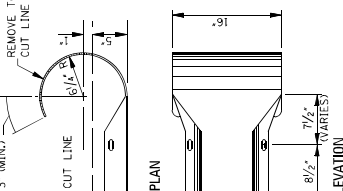
SECTION B-B

* NOTE: UNLESS OTHERWISE SPECIFIED ON THE PLANS.



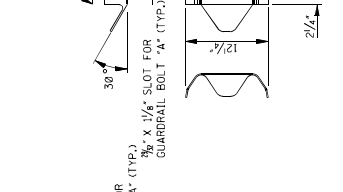
SECTION C-C

* NOTE: UNLESS OTHERWISE SPECIFIED ON THE PLANS.



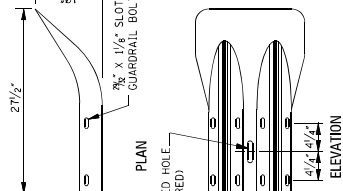
SECTION D-D

* NOTE: UNLESS OTHERWISE SPECIFIED ON THE PLANS.



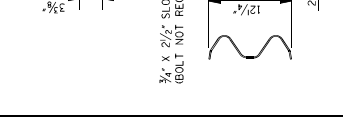
SECTION E-E

* NOTE: UNLESS OTHERWISE SPECIFIED ON THE PLANS.



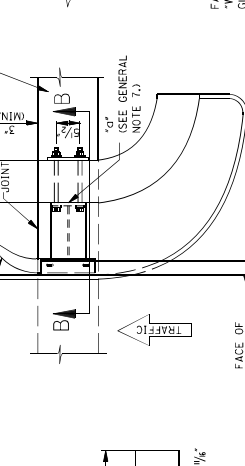
SECTION F-F

* NOTE: UNLESS OTHERWISE SPECIFIED ON THE PLANS.

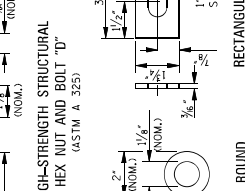


SECTION G-G

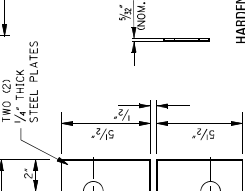
* NOTE: UNLESS OTHERWISE SPECIFIED ON THE PLANS.



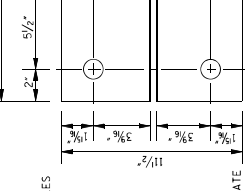
FASTENER DETAILS



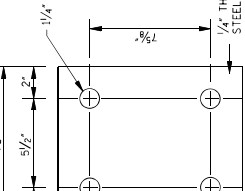
DETAIL OF PLATE "B" STANDARD



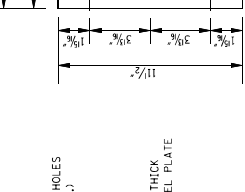
DETAIL OF PLATE "B" ALTERNATIVE



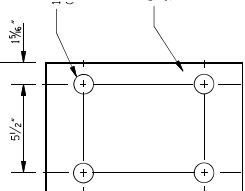
DETAIL OF PLATE "Z"



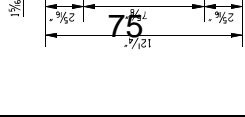
DETAIL OF PLATE "Z" SECTION



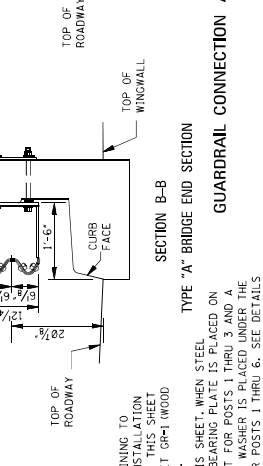
DETAIL OF PLATE "Z" SECTION



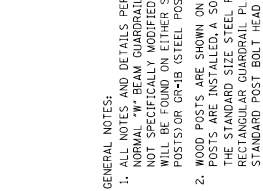
DETAIL OF PLATE "Z" SECTION



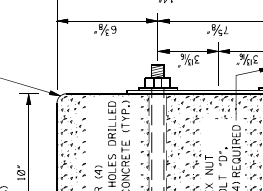
DETAIL OF PLATE "Z" SECTION



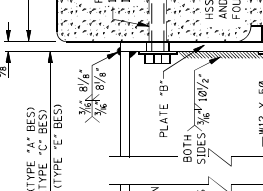
GUARDRAIL CONNECTION AT BRIDGE END



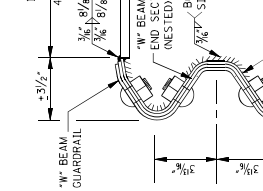
GUARDRAIL CONNECTION AT BRIDGE END



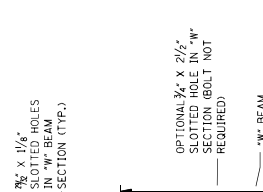
GUARDRAIL CONNECTION AT BRIDGE END



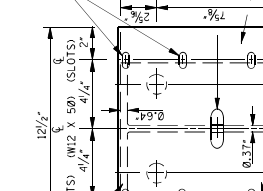
GUARDRAIL CONNECTION AT BRIDGE END



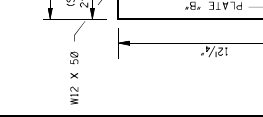
GUARDRAIL CONNECTION AT BRIDGE END



GUARDRAIL CONNECTION AT BRIDGE END



GUARDRAIL CONNECTION AT BRIDGE END



GUARDRAIL CONNECTION AT BRIDGE END

Notice To Bidders No. 1

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN

GUARDRAIL: BRIDGE END SECTIONS TYPE "A" & "C"

REVISION

ISSUE DATE: FEBRUARY XX, 2017

SHEET NUMBER CR-2

6205

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN

GUARDRAIL: BRIDGE END SECTIONS TYPE "A" & "C"

REVISION

ISSUE DATE: FEBRUARY XX, 2017

SHEET NUMBER CR-2

6205

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN

GUARDRAIL: BRIDGE END SECTIONS TYPE "A" & "C"

REVISION

ISSUE DATE: FEBRUARY XX, 2017

SHEET NUMBER CR-2

6205

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN

GUARDRAIL: BRIDGE END SECTIONS TYPE "A" & "C"

REVISION

ISSUE DATE: FEBRUARY XX, 2017

SHEET NUMBER CR-2

6205

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN

GUARDRAIL: BRIDGE END SECTIONS TYPE "A" & "C"

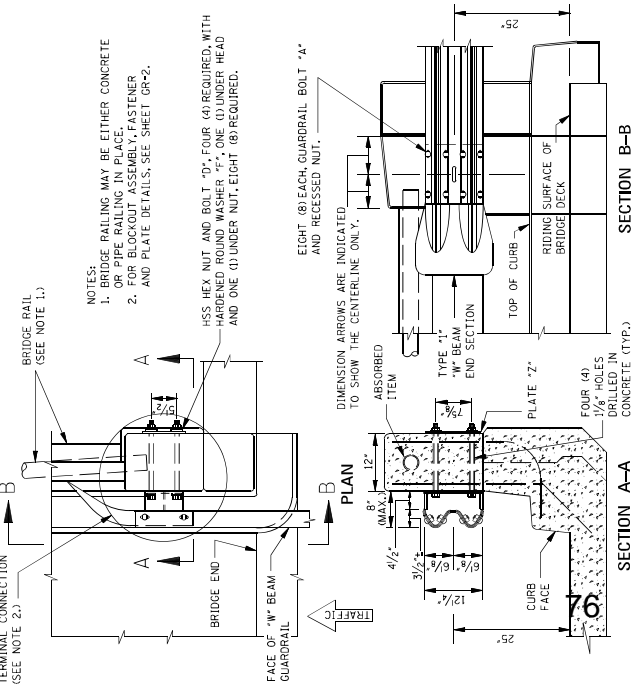
REVISION

ISSUE DATE: FEBRUARY XX, 2017

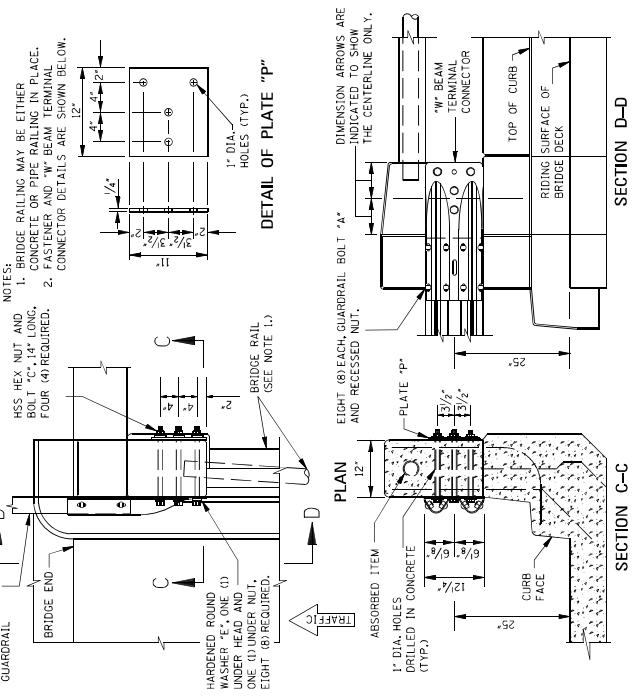
SHEET NUMBER CR-2

6205

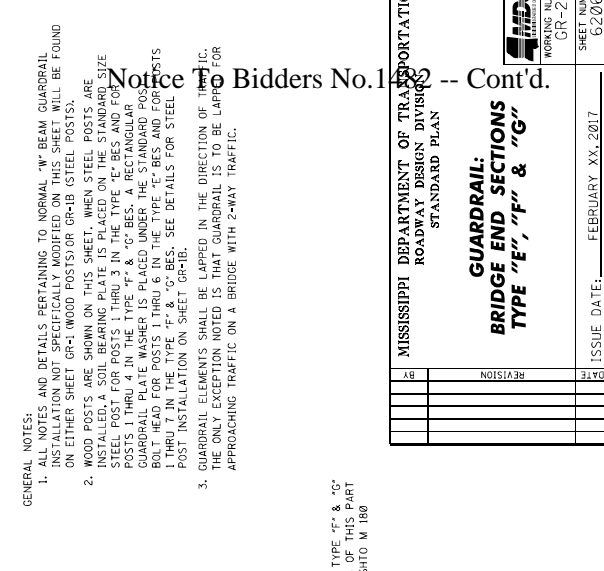
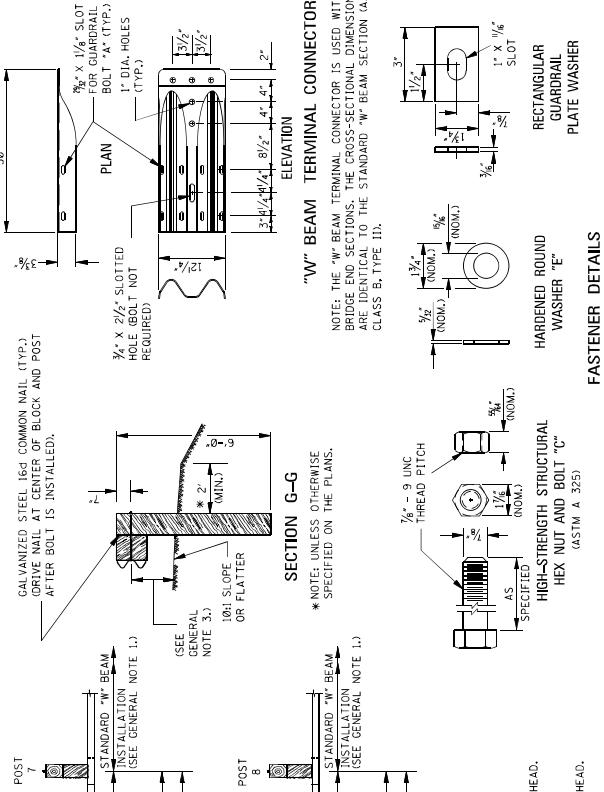
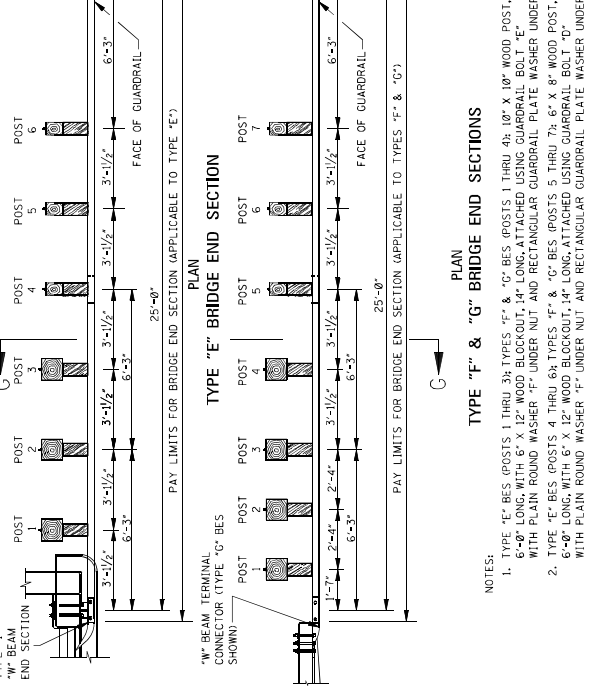
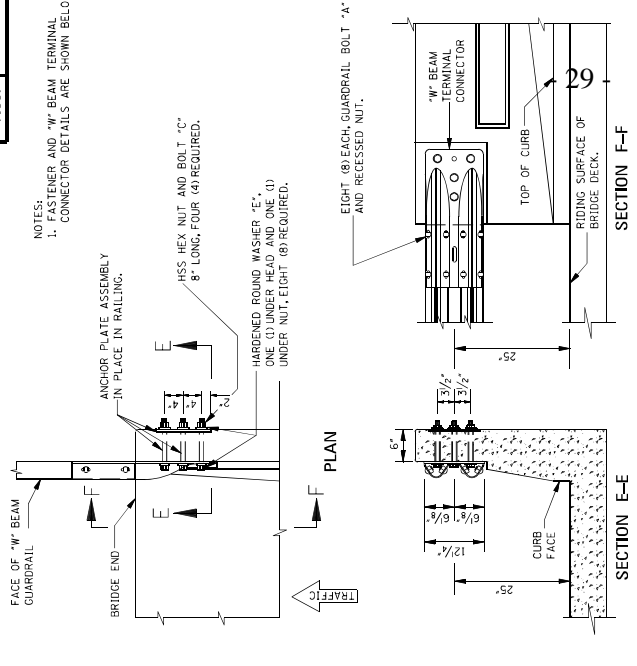
TYPE "E" BRIDGE END SECTION



TYPE "F" BRIDGE END SECTION



TYPE "G" BRIDGE END SECTION



Notice to Bidders No. 1 - Cont'd.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

GUARDRAIL: BRIDGE END SECTIONS TYPE "E", "F" & "G"

NO.	REVISION	DATE

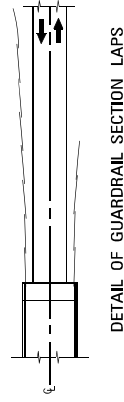
ISSUE DATE: FEBRUARY XX, 2017

WORKING NUMBER: GR-2A

SHEET NUMBER: 6206

NO.	DATE	REVISION

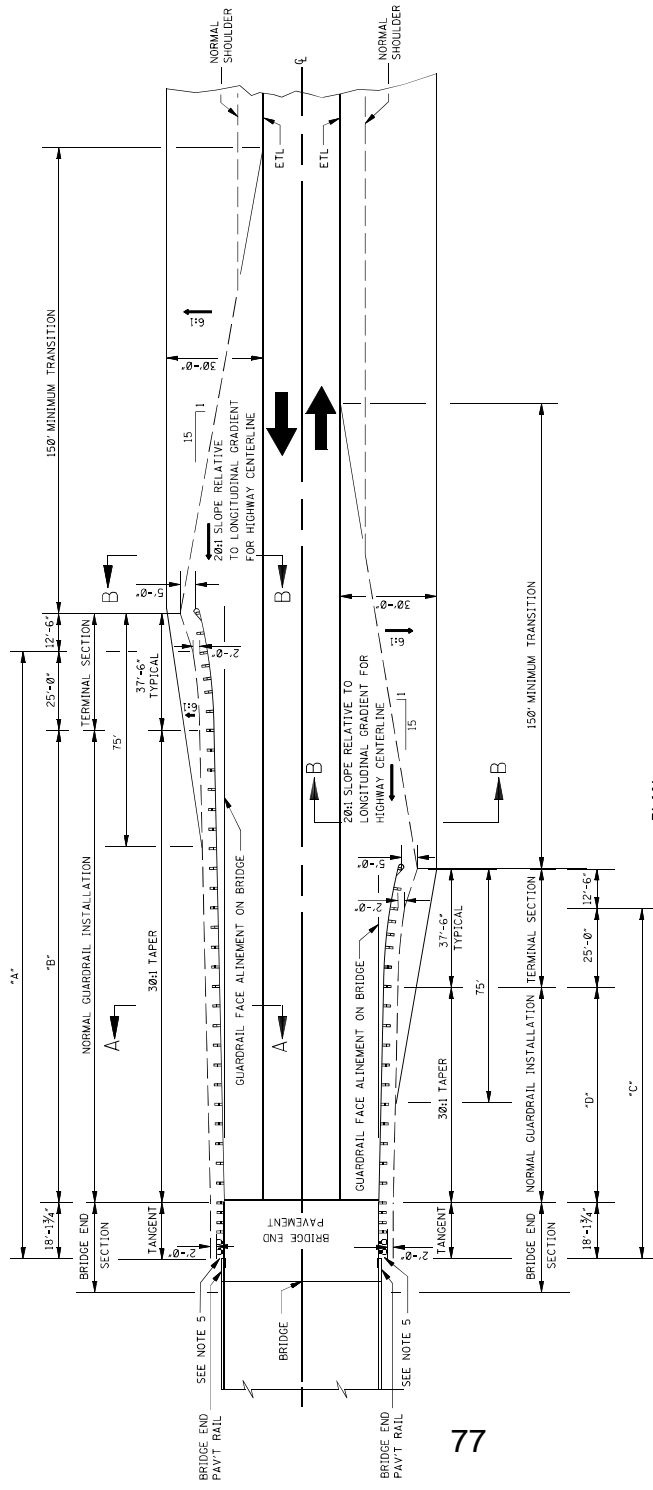
ISSUE DATE: FEBRUARY XX, 2017



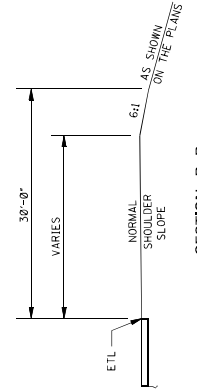
DETAIL OF GUARDRAIL SECTION LAPS

GENERAL NOTES:

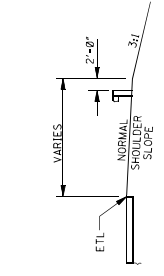
- VALUES FOR "A", "B", "C" AND "D" WILL BE SHOWN ELSEWHERE ON THE PLANS.
- FOR DETAILS PERTINENT TO INSTALLATION OF THE TERMINAL SECTION, SEE MANUFACTURER'S SPECIFICATIONS AND DRAWINGS ON ELSEWHERE ON PLANS.
- GUARDRAIL SECTIONS ARE TO BE LAPPED IN THE DIRECTION OF TRAFFIC APPROACHING THE BRIDGE.
- THE OVERALL LENGTH OF GUARDRAIL IS MEASURED FROM THE CONNECTING END ON THE BRIDGE.
- IN THE ABSENCE OF A BRIDGE END PAVEMENT RAIL, CONTACT THE BRIDGE END SECTION TO THE BRIDGE RAIL (SEE WK. NO. GR-2 THRU GR-20). THE SHOULDER WIDTH AT THE BRIDGE END PAVEMENT RAIL OR BRIDGE END RAIL SHALL BE SUFFICIENTLY WIDE TO PROVIDE A MINIMUM OF 2'-0" BEHIND THE BACK OF POST BEFORE SLOPE BREAK (HINGEPOINT).
- TYPE DETAILS AND LIMITS OF GUARDRAIL BRIDGE END SECTION WILL BE SHOWN ELSEWHERE ON THE PLANS.



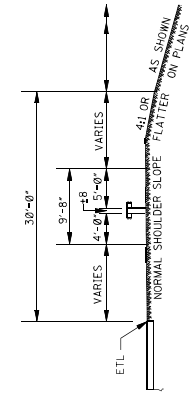
PLAN



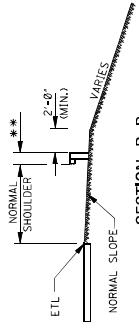
SECTION B-B



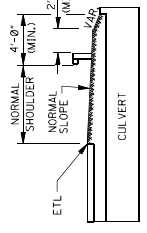
SECTION A-A



SECTION A-A



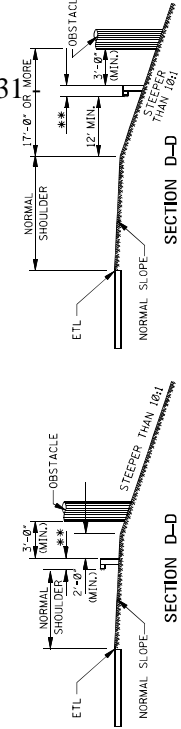
SECTION B-B



SECTION C-C

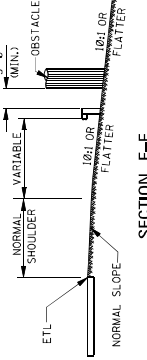
** NOTE: FOR STEEL POST AND MODIFIED WOOD BLOCKOUT, 28.78'. FOR WOOD POST AND WOOD BLOCKOUT, 23.25'.

SECTION D-D

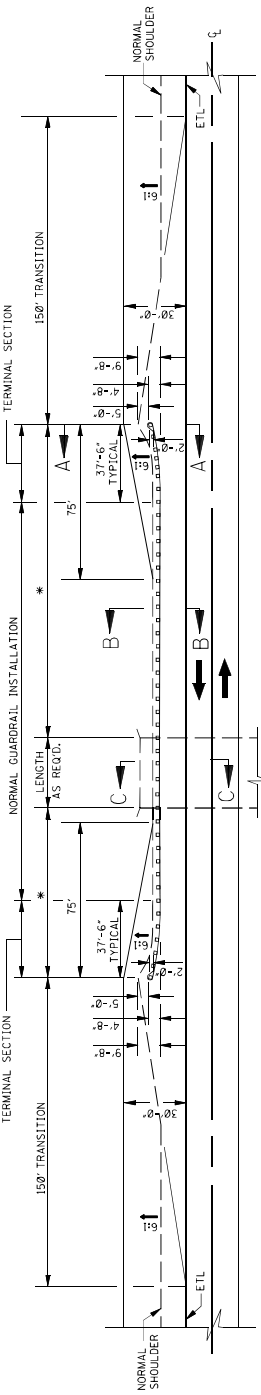


SECTION D-D

Notice To Bidders No. 1 - Cont'd.

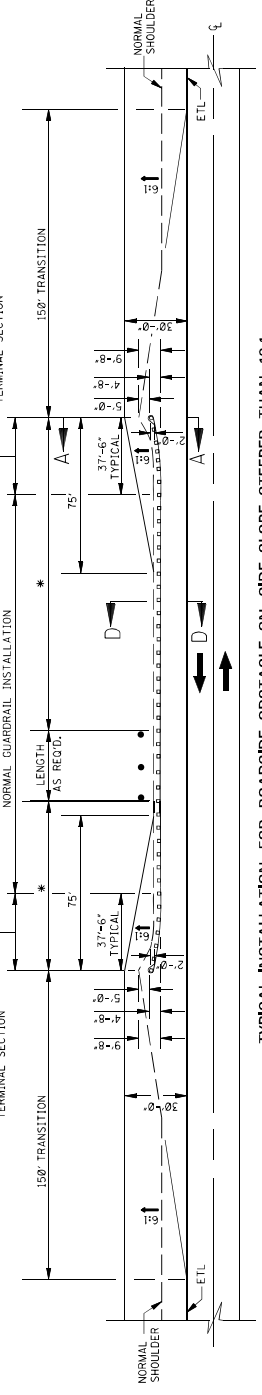


SECTION E-E



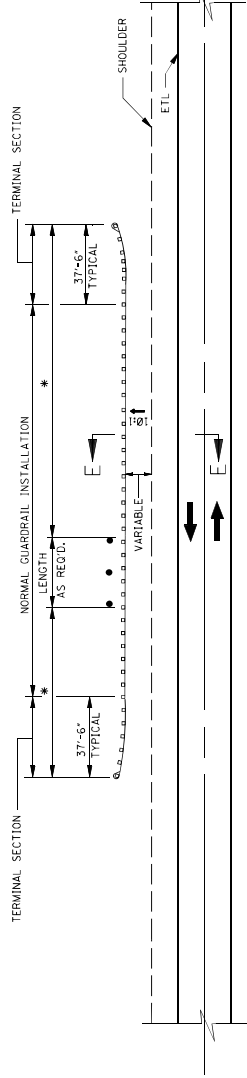
TYPICAL INSTALLATION FOR CULVERT

* NOTE: DISTANCE REQUIRED SHOWN ELSEWHERE ON PLANS.



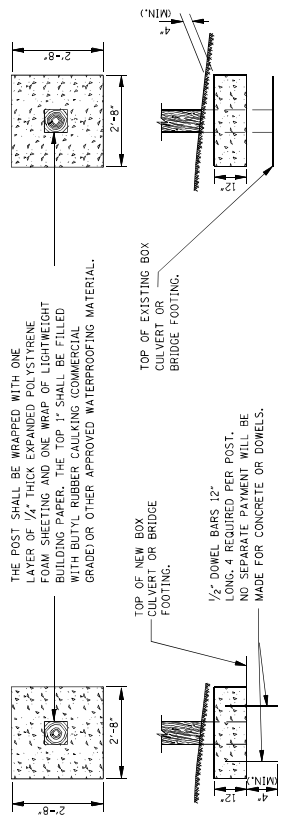
TYPICAL INSTALLATION FOR ROADSIDE OBSTACLE ON SIDE SLOPE STEEPER THAN 10:1

* NOTE: DISTANCE REQUIRED SHOWN ELSEWHERE ON PLANS.



TYPICAL INSTALLATION FOR ROADSIDE OBSTACLE ON SIDE SLOPE 10:1 OR FLATTER

* NOTE: DISTANCE REQUIRED SHOWN ELSEWHERE ON PLANS.



DETAIL OF POST INSTALLATION WITH COVER LESS THAN NORMAL POST LENGTH

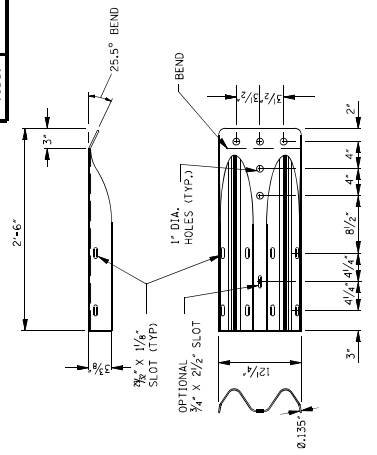
- GENERAL NOTES:
- FOR DETAILS PERTINENT TO INSTALLATION OF THE TERMINAL SECTION, SEE MANUFACTURER'S SPECIFICATIONS AND DRAWINGS OR ELSEWHERE ON PLANS.
 - GUARDRAIL SECTIONS TO BE LAPPED IN THE DIRECTION OF TRAFFIC FLOW NEAREST THE GUARDRAIL FACE.
 - PLAY LIMITS FOR NORMAL GUARDRAIL INSTALLATION WILL BE THE TOTAL LENGTH LESS THE LENGTHS OF END TERMINALS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

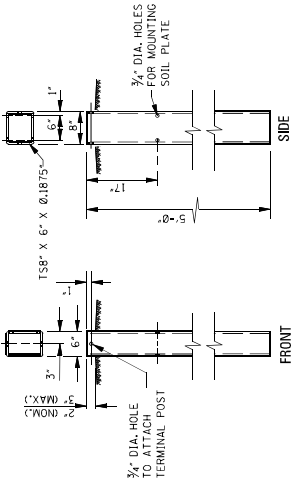
**GUARDRAIL:
TYPICAL INSTALLATION
FOR ROADSIDE OBSTACLE
ON 2-LANE, 2-WAY
HIGHWAYS**

DATE	REVISION

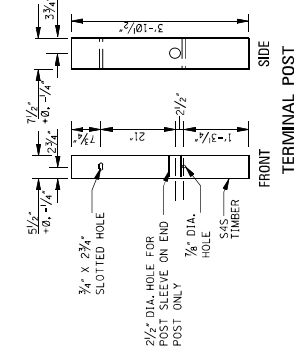
ISSUE DATE: FEBRUARY XX, 1917
SHEET NUMBER GR-4D
WORKING NUMBER
PROJECT NUMBER 6218



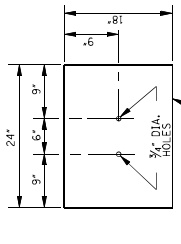
"W" BEAM TERMINAL CONNECTOR PLATE
 NOTES:
 1. THE "W" BEAM TERMINAL CONNECTOR IS TO BE ASPHO M 180 CORRUGATED SHEET STEEL, CLASS B, TYPE L.
 2. SPACER-BOLT SLOTS MAY ALSO BE ORIENTED AT 50° (ON THE FLAT) INSTEAD OF 8° AS SHOWN.



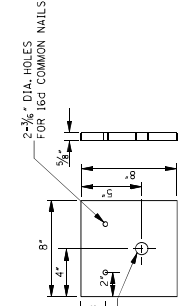
STEEL TUBE ANCHOR
 NOTES: TERMINAL POST SHOULD BE ABLE TO SLIDE INTO THE TOP OF THIS SECTION SO THE ACTUAL INSIDE DIMENSIONS OF THIS GALVANIZED TUBE CANNOT BE LESS THAN 7 1/2" X 5 1/2".



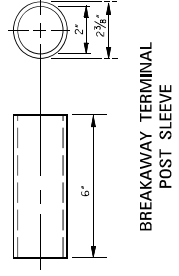
TERMINAL POST FOR FOUNDATION TUBE INSTALLATION
 NOTE: TERMINAL POST SHALL BE MADE OF S4S TIMBER WITH 2 STRESS GRADE OF 1200 LB/FT².



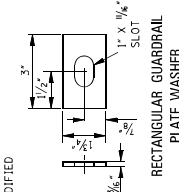
SOIL PLATE
 1/4" THICK PLATE
 NOTE: 2 REQUIRED



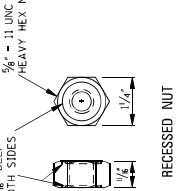
BEARING PLATE



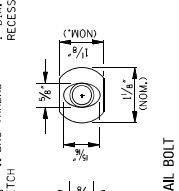
SHELF ANGLE BRACKET



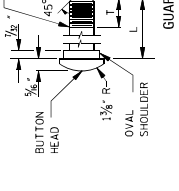
RECTANGULAR GUARDRAIL PLATE WASHER



RECESSED NUT



GUARDRAIL BOLT



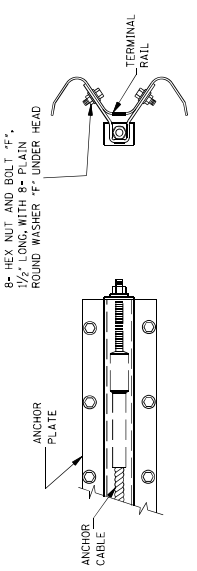
PLAIN ROUND WASHER

WASHER (NOM.)	A (NOM.)	B (NOM.)	C (NOM.)
"F"	1 1/8"	1 3/4"	3/4"
"H"	1 1/8"	2"	3/4"

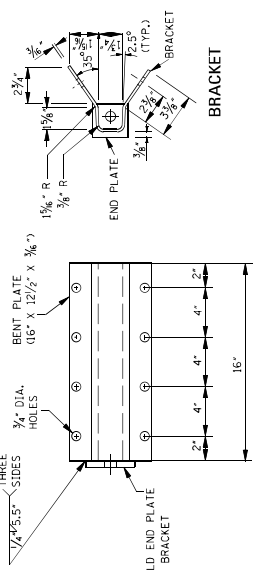
BOLT (NOM.)	L (MIN.)	T (NOM.)
"A"	1 1/4"	1"
"B"	2"	1 3/4"
"C"	1 1/4"	4"
"D"	2 1/2"	4"

NOTES:
 1. ALL GUARDRAIL BOLTS ARE 5/8" - 11 UNC THREAD PITCH.
 2. IF ANY BOLT EXTENDS MORE THAN 1/4" FROM THE NUT, THE BOLT SHOULD BE TRIMMED BACK.

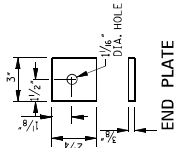
FASTENER DETAILS



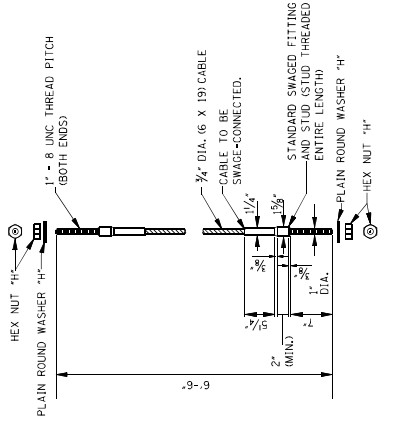
ANCHOR PLATE ASSEMBLY DETAILS



ANCHOR PLATE



END PLATE



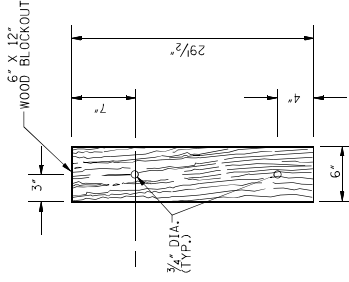
CABLE ANCHOR ASSEMBLY

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN

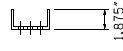
GUARDRAIL: MISCELLANEOUS HARDWARE

WORKING NUMBER GR-HW
 SHEET NUMBER 6222

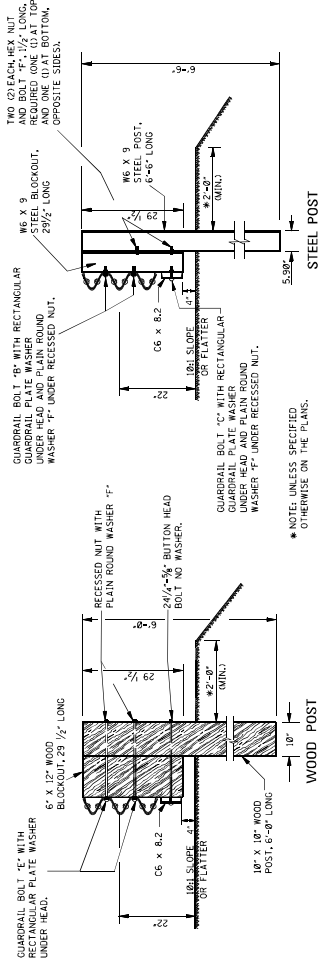
ISSUE DATE: FEBRUARY XX, 2017



BLOCKOUT FOR POST 9 OF
TYPE "I" BRIDGE END SECTION

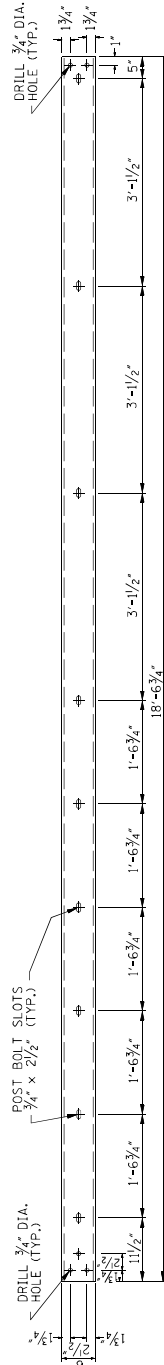


CROSS-SECTION
VIEW OF C6 X 8.2



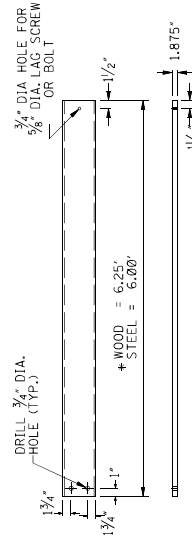
PROFILE VIEW OF POSTS

* NOTE UNLESS SPECIFIED OTHERWISE ON THE PLANS.

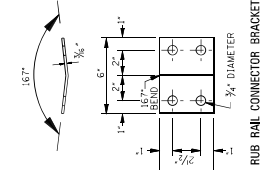
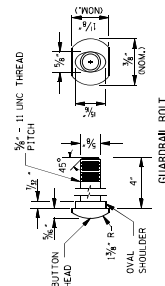


* MEASURED ALONG RUBRAIL

LONGITUDINAL RUB RAIL



DIAGONAL RUB RAIL



POSTS 1 THROUGH 9 REQUIRE AN ADDITIONAL HOLE TO ATTACH LOWER BLOCKOUT TO STEEL POST. REQUIRE 2 HOLES ON EITHER SIDE OF THE POST.
RUBRAIL BLOCKOUTS ARE TO BE SECURED WITH 3/4" BUTT-ON BOLT FOR STEEL POST AND 20" BOLT FOR WOOD POST. (SEE PROFILE VIEW FOR LENGTHS)
RUBRAIL & LONG BLOCKOUT (POST 9) ARE NOT REQUIRED IN CURB & GUTTER SECTIONS OF ROADWAY.

REVISION			
DATE			
ISSUE DATE: FEBRUARY XX, 2017		SHEET NUMBER 6219	
<p>MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN</p> <p>GUARDRAIL: RUB RAIL HARDWARE SHEET</p>			
WORKING NUMBER GR-RR		ISSUE DATE: FEBRUARY XX, 2017	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1483

CODE: (SP)

DATE: 3/21/2019

SUBJECT: Smoothness Tolerance

PROJECT: MP-5011-38(010) / 306547301 -- Lauderdale County

Bidders are advised that the smoothness tolerances for this project shall meet the requirements of a Category C project as addressed in Subsection 403.03.2.1. Bidders are responsible for the collection of a preliminary smoothness profile prior to any work being performed.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-102-2

CODE: (IS)

DATE: 11/22/2017

SUBJECT: **Bidding Requirements and Conditions**

Section 102, Bidding Requirements and Conditions, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-102.01--Prequalification of Bidders. Delete the last sentence of the third paragraph of Subsection 102.01 on page 13, and substitute the following.

The Bidder's Certificate of Responsibility number must be on file with the Department's Contract Administration Division prior to request for permission to bid.

907-102.02--Contents of Proposal Forms. Delete the fourth paragraph in Subsection 102.02 on page 13, and substitute the following.

Prospective bidders must complete an online request for permission to be eligible to bid a project. Upon approval, the bidder will be authorized to submit a bid electronically using Bid Express at <http://bidx.com>.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-103-2

CODE: (SP)

DATE: 06/22/2017

SUBJECT: Award and Execution of Contract

Section 103, Award and Execution of Contract, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-103.01--Consideration of Proposal. Delete the second and third paragraphs of Subsection 103.01 on page 19, and substitute the following.

907-103.01.1--For Projects Constructed Without Federal Funds. Resident Contractors actually domiciled in Mississippi are to be granted preference over nonresidents in awarding of Contracts financed 100% with State funds.

In consideration of proposals that are equal to or in excess of \$50,000 and financed 100% with State funds, a nonresident bidder domiciled in a state having laws granting preference to local Contractors will be considered for such contracts on the same basis as the nonresident bidder's state awards contracts to Mississippi Contractors bidding under similar circumstances. When a nonresident Contractor submits a bid equal to or in excess of \$50,000 on a contract financed 100% with State funds, a copy of the current laws from the state of domicile and an explanation thereof pertaining to treatment of nonresident Contractors shall be attached. If no preferential treatment is provided for Contractors in the state of domicile and contracts are awarded to the lowest responsible bidder, a statement to this effect shall be attached. Should the attachment not accompany the bid when submitted, the Contractor shall have 10 days following the opening of the bids to furnish the required information to the Contract Administration Director for attachment to the bid. Failure to provide the attachment within 10 days will result in the nonresident Contractor's bid being rejected and not considered for award. As used herein, the term "resident Contractor" includes a nonresident person, firm or corporation that has been qualified to do business in this State and has maintained a permanent full-time office in the State of Mississippi for two years prior to the submission of the bid, and the subsidiaries and affiliates of such a person, firm or corporation.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-619-6

CODE: (SP)

DATE: 03/21/2018

SUBJECT: Temporary Portable Rumble Strips

Section 619, Traffic Control for Construction Zones, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-619.02--Materials. After Subsection 619.02.15 on page 472, add the following.

907-619.02.16--Temporary Portable Rumble Strips. Temporary portable rumble strips shall be RoadQuake manufactured by PSS and meet the following requirements:

- capable of being installed without adhesives or bolts,
- have a minimum weight of 100 pounds,
- have a minimum overall length of 11 feet,
- have a minimum width of 12 inches, and
- have a maximum height of 3/4 inch.

Temporary portable rumble strips shall be installed in accordance with the attached details, or as directed by the Engineer.

907-619.03--Construction Requirements. After Subsection 619.03.11 on page 476, add the following.

907-619.03.16--Temporary Portable Rumble Strips. Temporary portable rumble strips shall be placed at locations shown on the traffic control plans, attached drawing, or as directed by the Engineer. The rumble strips shall be removed when lane closures are removed, relocated when lane closures are relocated, or as directed by the Engineer.

Prior to placement of the rumble strips, the roadway shall be cleaned to be free of dust, sand, and other materials that may cause slippage. The minimum roadway temperature at the time of installation shall be in accordance with manufacturer recommendations.

A minimum of three (3) temporary portable rumble strips shall be arranged in an array. The spacing of temporary portable rumble strips in each array shall be on 15-foot centers. One array of three (3) strips shall be used in each lane. The rumble strips shall be regularly monitored and maintained to ensure they stay in place under traffic.

907-619.04--Method of Measurement. At the end of Subsection 619.04 on page 478, add the following.

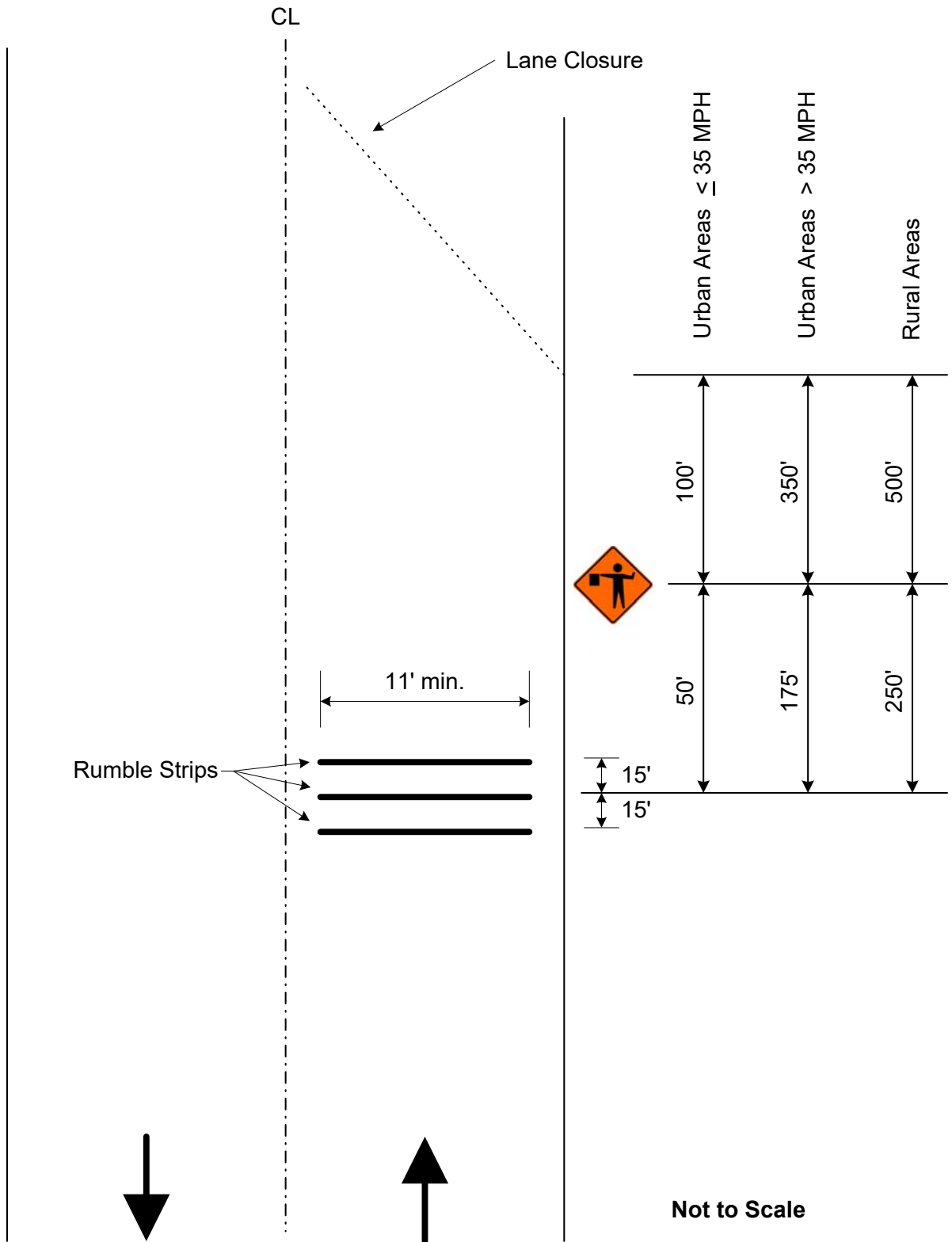
Temporary Portable Rumble Strips will be measured for payment by the linear foot only when a pay item for temporary portable rumble strips is included in the contract. Otherwise, temporary portable rumble strips will be included in the cost of pay item 618-A, Maintenance of Traffic. The quantity of temporary portable rumble strips will be the length of rumble strips approved by the Engineer to be in-place on the project at any one time.

907-619.05--Basis of Payment. After the fifth paragraph of Subsection 619.05 on page 478, add the following.

Temporary Portable Rumble Strips measured as prescribed above, will be paid for at the contract unit price per linear foot, which price shall be full compensation for cleaning the roadway surface, installing the rumble strips, maintenance and repair of the strips, cleaning and resetting of the strips, removal and replacement, and for all labor, equipment, tools, and incidentals necessary to complete the work.

After the last pay item listed on page 480, add the following.

907-619-B: Temporary Portable Rumble Strips - per linear foot



Detail of Temporary Portable Rumble Strips

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-701-1

CODE: (SP)

DATE: 10/23/2018

SUBJECT: Hydraulic Cement

Section 701, Hydraulic Cement, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-701.01--General. In the first sentence of the third paragraph of Subsection 701.01 on page 718, change “mills” to “plants.”

In the second sentence of the seventh paragraph of Subsection 701.01 on pages 718 and 719, change “shall” to “will.”

907-701.02--Portland Cement.

907-701.02.1-General.

907-701.02.1.2--Alkali Content. Delete the sentence in Subsection 701.02.1.2 on page 719, and substitute the following.

The Equivalent alkali content for all cement types in this Subsection shall not exceed 0.60%.

907-701.02.2--Replacement by Other Cementitious Materials. Delete the paragraph in Subsection 701.02.2 on page 719, and substitute the following.

The maximum replacement of cement by weight is 25% for fly ash or 50% for ground granulated blast furnace slag (GGBFS). Replacement contents below 20% for fly ash or 45% for GGBFS may be used, but will not be given any special considerations, such as the maximum acceptance temperature for portland cement concrete containing pozzolans in Subsection 804.02.13.1.5. Special considerations shall only apply for replacement of cement by fly ash or GGBFS.

Delete Subsection 701.02.2.1 on pages 719 and 720, and substitute the following.

907-701.02.2.1--Portland Cement Concrete Exposed to Soluble Sulfate Conditions or Seawater.

When portland cement concrete is exposed to moderate or severe soluble sulfate conditions, or to seawater, cement types and replacement of cement by Class F fly ash or GGBFS shall be as follows in Table 1. Class C fly ash shall not be used as a replacement for cement in any of the sulfate exposure conditions listed in Table 1.

Table 1- Cementitious Materials for Soluble Sulfate Conditions or Seawater

Sulfate Exposure	Water-soluble sulfate (SO ₄) in soil, % by mass	Sulfate (SO ₄) in water, ppm	Cementitious material required
Moderate and Seawater	0.10 - 0.20	150 - 1,500	Type I cement with one of the following replacements of cement by weight: 24.5 - 25.0% Class F fly ash, or 49.5 - 50.0% GGBFS or Type II ^{*,**} cement
Severe	0.20 - 2.00	1,500 - 10,000	Type I cement with a replacement by weight of 49.5 - 50.0% GGBFS, or Type II [*] cement with one of the following replacements of cement by weight: 24.5 - 25.0% Class F fly ash, or 49.5 - 50.0% GGBFS

* Type III cement conforming to AASHTO M85 with a maximum 8% tricalcium aluminate (C₃A) may be used in lieu of Type II cement as allowed in Subsection 701.02.1; this cement is given the designation “Type III(MS).”

** Class F fly ash or GGBFS may be added as a replacement for cement as allowed in Subsection 907-701.02.2.

Delete Subsection 701.02.2.2 on page 720, and substitute the following.

907-701.02.2.2--Portland Cement for Soil Stabilization Exposed to Soluble Sulfate Conditions or Seawater. When portland cement for use in soil stabilization is exposed to moderate or severe soluble sulfate conditions, or to seawater, cement types and replacement of cement by Class F fly ash or GGBFS shall meet the requirements of Subsection 701.02.2.1.

907-701.04--Blended Hydraulic Cement.

907-701.04.1--General. Delete Subsection 701.04.1.1 on page 720, and substitute the following.

907-701.04.1.1--Types of Blended Hydraulic Cement. Blended hydraulic cements (blended cements) shall be of the following types and conform to AASHTO M 240:

- Type IL – Portland-limestone cement
- Type IP – Portland-pozzolan cement
- Type IS – Portland blast-furnace slag cement

Blended cement Types IL, IP, and IS meeting the “MS” sulfate resistance requirement listed in AASHTO M 240, Table 3 shall have the “(MS)” suffix added to the type designation.

907-701.04.1.2--Alkali Content. Delete the sentence in Subsection 701.04.1.2 on page 720, and substitute the following.

All blended cement types shall be made with clinker that would result in cement meeting the requirements of Subsection 701.02.1.2 when used in the production of AASHTO M 85, Type I or Type II cement.

907-701.04.2--Replacement by Other Cementitious Materials. Delete the paragraph in Subsection 701.04.2 on page 720, and substitute the following.

The maximum replacement of blended cement Type IL by weight is 35% for fly ash or 50% for GGBFS. Replacement contents below 20% for fly ash or 45% for GGBFS may be used, but will not be given any special considerations, such as the maximum acceptance temperature for blended cement concrete containing pozzolans in Subsection 804.02.13.1.5. Special considerations shall only apply for replacement of blended cement by fly ash or GGBFS.

No additional cementitious materials, such as portland cement, blended cement, fly ash, GGBFS, or others, shall be added to or as a replacement for blended cement Types IP and IS.

Delete Subsection 701.04.2.1 on pages 720 and 721, and substitute the following.

907-701.04.2.1--Blended Cement Concrete Exposed to Soluble Sulfate Conditions or Seawater. When blended cement concrete is exposed to moderate or severe soluble sulfate conditions, or to seawater, cement types and replacement of cement by Class F fly ash or GGBFS shall be as follows in Table 2. Class C fly ash shall not be used as a replacement for cement in any of the sulfate exposure conditions listed in Table 2.

Table 2- Cementitious Materials for Soluble Sulfate Conditions or Seawater

Sulfate Exposure	Water-soluble sulfate (SO ₄) in soil, % by mass	Sulfate (SO ₄) in water, ppm	Cementitious material required
Moderate and Seawater	0.10 - 0.20	150 - 1,500	Type IL (MS)* cement, Type IL cement with one of the following replacements of cement by weight: 24.5 - 35.0% Class F fly ash, or 49.5 - 50.0% GGBFS, Type IP (MS) cement, or Type IS (MS) cement
Severe	0.20 - 2.00	1,500 - 10,000	Type IL cement with a replacement of cement by weight of 49.5 - 50.0% GGBFS, or Type IL (MS) cement with one of following replacements of cement by weight: 24.5 - 35.0% Class F fly ash, or 49.5 - 50.0% GGBFS

- * Class F fly ash or GGBFS may be added as a replacement for cement as allowed in Subsection 907-701.04.2.

Delete Subsection 701.04.2.2 on page 721, and substitute the following.

907-701.04.2.2--Blended Cement for Soil Stabilization Exposed to Soluble Sulfate Conditions or Seawater. When blended cement for use in soil stabilization is exposed to moderate or severe soluble sulfate conditions, or to seawater, cement types and replacement of cement by Class F fly ash or GGBFS shall meet the requirements of Subsection 701.04.2.1.

Delete Subsection 701.04.3 on page 721.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-702-4

CODE: (IS)

DATE: 09/11/2018

SUBJECT: Bituminous Materials

Section 702, Bituminous Materials, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-702.04--Sampling. Delete the sentence in Subsection 702.04 on page 722, and substitute the following.

Sampling of bituminous materials shall be as set out in AASHTO R 66.

907-702.07--Emulsified Asphalt. Delete the last sentence in Subsection 702.07 on page 724, and substitute the following.

Asphalt for fog seal shall conform to the requirements of Subsection 907-702.12, Table V.

907-702.12--Tables. Delete Table V in Subsection 702.12 on page 729, and substitute the following.

**TABLE V
SPECIFICATION FOR FOG SEAL**

Test Requirements	LD-7		CHPF-1		Test Method
	Min.	Max.	Min.	Max.	
Viscosity, Saybolt Furol, @ 25°C, Sec.	10	100	-	100	AASHTO T 72
Storage Stability Test, 24 hr, %	-	1	-	1	AASHTO T 59
Settlement, 5 day, %	-	5	-	-	AASHTO T 59
Oil Distillate, %	-	1	-	-	AASHTO T 59
Sieve Test, % *	-	0.3	-	0.1	AASHTO T 59
Residue by Distillation, %	40	-	40	-	AASHTO T 59
Test on Residue from Distillation					
Penetration @ 25°C, 100g, 5 sec	-	20	40	90	AASHTO T 49
Softening Point, °C	65	-	-	-	ASTM D 36
Solubility in trichloroethylene, %	97.5	-	-	-	AASHTO T 44
Elastic Recovery @ 25°C, %	-	-	40	-	AASHTO T 301
Original DSR @ 82° (G*/Sinδ, 10 rad/sec)	1	-	-	-	AASHTO T 111

* The Sieve Test result is tested for reporting purposes only and may be waived if no application problems are present in the field.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-703-1

CODE: (IS)

DATE: 06/13/2018

SUBJECT: Gradation

Section 703, Aggregates, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-703.03--Course Aggregates for Hydraulic Cement Concrete.

907-703.03.2--Detail Requirements.

907-703.03.2.4--Gradation. In the table in Subsection 703.03.2.4 on page 734, add 100 for the percent passing by weight on the 1½-inch sieve for Size No. 67 aggregates.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-705-1

CODE: (IS)

DATE: 06/13/2018

SUBJECT: Stone Riprap

Section 705, Stone Blanket Protection and Filter Blanket Materials, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-705.04--Stone Riprap. Delete the last sentence of the first paragraph of Subsection 705.04 on page 750, and substitute the following.

Quality requirements for rock to be furnished under these specifications will come from a pre-approved source and be visually approved prior to use.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-711-2

CODE: (IS)

DATE: 09/11/2018

SUBJECT: Plain Steel Wire

Section 711, Reinforcement and Wire Rope, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-711.02--Deformed and Plain Carbon-Steel Bars for Concrete Reinforcing.

907-711.02.3--Steel Welded and Non-Welded Wire Reinforcement, Plain and Deformed, for Concrete.

907-711.02.3.1--Plain Steel Wire. Delete the sentence in Subsection 711.02.3.1 on pages 780 and 781, and substitute the following.

Plain steel wire and plain steel welded wire shall conform to the requirements of AASHTO M 336.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-720-2

CODE: (IS)

DATE: 09/11/2018

SUBJECT: Acceptance Procedure for Glass Beads

Section 720, Pavement Marking Materials, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-720.01--Glass Beads.

907-720.01.4--Acceptance Procedures. Delete the last sentence of the paragraph in Subsection 720.01.4 on page 841, and substitute the following.

Acceptance sampling and testing of glass beads will be in accordance with the Department's Materials Division Inspection, Testing, and Certification Manual, Section 2.9.2 -- Glass Beads.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-899-1

CODE: (SP)

DATE: 01/17/2017

SUBJECT: Railway-Highway Provisions

Section 907-899, Railway-Highway Provisions, is hereby added to and made part of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction as follows:

SECTION 907-899--RAILWAY-HIGHWAY PROVISIONS

907-899.01--Description. This special provision addresses the Contractor's involvement with railroad flagging, Contractor Safety Orientation, Contractor Background Investigation, Contractor Roadway Worker on Track Safety Program and Safety Action Plan, and any other requirements set forth by the Railroad and any attached Exhibits.

Prior to bidding, the Contractor shall read and comply with the requirements of the Railroad and any attached Exhibits. The Contractor shall contact the Railroad concerning insurance coverage requirements, Railroad flagging costs, Contractor Safety Orientation, Contractor Background Investigation, Contractor Roadway Worker on Track Safety Program and Safety Action Plan, and any other requirements set forth by the Railroad and any attached Exhibits. In case the railroad requires coverage over and above that required by the Standard Specifications, the railroad requirements shall be met.

If in the opinion of the RAILROAD, the presence of an authorized representative of the RAILROAD is required to supervise the same, the RAILROAD shall render bills to the Contractor for all expenses incurred by it for such supervision. This includes all labor costs for flagmen or cable locate supplied by the RAILROAD to protect RAILROAD operation, and for the full cost of furnishing, installation and later removal of any temporary supports for said tracks, as the RAILROAD's Chief Engineer's Office may deem necessary.

It will be the Contractor's responsibility to pay all bills associated with the Railroad requirements and any attached Exhibits.

A cable locate of RAILROAD owned facilities may be required to identify and protect Signal & Communication cables that have been installed to provide power, signal control, wayside communications. These cables are vital to a safe and reliable railway operation. The cable locate will be performed by a qualified RAILROAD employee.

Outside Contractors are prohibited from driving on, along, or across any track that does not have a RAILROAD installed crossing. They may utilize an existing public crossing. The practice of allowing rubber tired equipment to operate over track with no crossing has been banned.

The Contractor shall complete and process any required forms addressed by the Railroad or any attached Exhibits. The Contractor shall not commence or carry on any form of work on, under, above or within the designated distance from the Railroad track prior to getting approval from the Railroad.

907-899.02--Blank.

907-899.03--Construction Requirements. The Contractor shall read and comply with the requirements of the Railroad and any attached Exhibits.

907-899.04--Method of Measurement. Railway-highway provisions will be measured as a unit lump sum quantity. Measurement for payment will be in accordance with the following schedule:

- a) On the first estimate, twenty five percent (25%) of the amount bid for Railway Highway Provision will be paid.
- b) When twenty five percent (25%) of the original contract amount is earned from all direct pay items, fifty percent (50%) of the amount bid for Railway Highway Provision will be paid.
- c) When fifty percent (50%) of the original contract amount is earned from all direct pay items, one hundred percent (100%) of the amount bid for Railway Highway Provision will be paid.

907-899.05--Basis of Payment. Railway-highway provisions, measured as prescribed above, will be paid for at the contract lump sum price, which price shall be payment in full for all insurance coverage requirements, railroad flagging costs, Contractor safety orientation, Contractor background investigation, Contractor safety programs and plans, and any other requirements set forth by the Railroad and any attached Exhibits, and other incidentals necessary to complete the requirements of this work.

Payment will be made under:

907-899-A: Railway-Highway Provisions

- lump sum

SECTION 905 - PROPOSAL

Date _____

Mississippi Transportation Commission
Jackson, Mississippi

Sirs: The following proposal is made on behalf of _____
_____ of _____

for constructing the following designated project(s) within the time(s) hereinafter specified.

The plans are composed of drawings and blue prints on file in the offices of the Mississippi Department of Transportation, Jackson, Mississippi.

The Specifications are the current Standard Specifications of the Mississippi Department of Transportation approved by the Federal Highway Administration, except where superseded or amended by the plans, Special Provisions and Notice(s) to Bidders attached hereto and made a part thereof.

I (We) certify that I (we) possess a copy of said Standard and any Supplemental Specifications.

Evidence of my (our) authority to submit the Proposal is hereby furnished. The proposal is made without collusion on the part of any person, firm or corporation. I (We) certify that I (we) have carefully examined the Plans, the Specifications, including the Special Provisions and Notice(s) to Bidders, herein, and have personally examined the site of the work. On the basis of the Specifications, Special Provisions, Notice(s) to Bidders, and Plans, I (we) propose to furnish all necessary machinery, tools, apparatus and other means of construction and do all the work and furnish all the materials in the manner specified. I (We) understand that the quantities mentioned herein are approximate only and are subject to either increase or decrease, and hereby propose to perform any increased or decreased quantities of work at the unit prices bid, in accordance with the above.

I (We) acknowledge that this proposal will be found irregular and/or non-responsive unless a certified check, cashier's check, or Proposal Guaranty Bond in the amount as required in the Advertisement (or, by law) is submitted electronically with the proposal or is delivered to the Contract Administration Engineer prior to the bid opening time specified in the advertisement.

INSTRUCTION TO BIDDERS: Alternate and Optional Items on Bid Schedule.

1. Two or more items entered opposite a single unit quantity WITHOUT DEFINITE DESIGNATION AS "ALTERNATE ITEMS" are considered as "OPTIONAL ITEMS". Bidders may or may not indicate on bids the Optional Item proposed to be furnished or performed WITHOUT PREJUDICE IN REGARD TO IRREGULARITY OF BIDS.
2. Items classified on the bid schedule as "ALTERNATE ITEMS" and/or "ALTERNATE TYPES OF CONSTRUCTION" must be preselected and indicated on bids. However, "Alternate Types of Construction" may include Optional Items to be treated as set out in Paragraph 1, above.
3. Optional items not preselected and indicated on the bid schedule MUST be designated in accordance with Subsection 102.06 prior to or at the time of execution of the contract.
4. Optional and Alternate items designated must be used throughout the project.

I (We) further propose to perform all "force account or extra work" that may be required of me (us) on the basis provided in the Specifications and to give such work my (our) personal attention in order to see that it is economically performed.

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) shall submit electronically with our proposal or deliver prior to the bid opening time a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

President Address

Secretary Address

Treasurer Address

The following is my (our) itemized proposal.

Mill & Overlay approximately 11 miles of US 11 from Russell to Alabama State Line, known as State Project No. MP-5011-38(010) / 306547301 in Lauderdale County.

Line no.	Item Code	Adj Code	Quantity	Units	Description[Fixed Unit Price]
Roadway Items					
0010	202-B006		21,500	Square Yard	Removal of Asphalt Paved Shoulders, All Depths
0020	202-B052		15	Square Yard	Removal of Concrete Driveways, All Depths
0030	202-B069		1,130	Square Yard	Removal of Concrete Pavement w/ Variable Depth Overlay
0040	202-B158		2,167	Linear Feet	Removal of Guard Rail, Including Rails, Posts and Terminal Ends
0050	202-B240		2,015	Linear Feet	Removal of Traffic Stripe
0060	203-G002	(E)	500	Cubic Yard	Excess Excavation, LVM, AH
0070	304-D002	(GT)	500	Ton	Granular Material, Crushed Stone
0080	403-A003	(BA1)	20,150	Ton	12.5-mm, ST, Asphalt Pavement
0090	403-A006	(BA1)	875	Ton	19-mm, ST, Asphalt Pavement
0100	403-B003	(BA1)	550	Ton	12.5-mm, ST, Asphalt Pavement, Leveling
0110	403-C003	(BA1)	11,825	Ton	19-mm, ST, Asphalt Pavement, Trench Widening
0120	406-D001		180,000	Square Yard	Fine Milling of Bituminous Pavement, All Depths
0130	407-A001	(A2)	18,000	Gallon	Asphalt for Tack Coat
0140	413-D004		940	Linear Feet	Cleaning and Filling Joints in PCC Pavement, 10" or Less
0150	413-E001		43,550	Linear Feet	Sawing and Sealing Transverse Joints in Asphalt Pavement
0160	423-A001		20	Mile	Rumble Strips, Ground In
0170	503-C010		2,622	Linear Feet	Saw Cut, Full Depth
0180	606-B003		918	Linear Feet	Guard Rail, Class A, Type 1, 'W' Beam, Metal Post
0190	606-B010		100	Linear Feet	Guard Rail, Class A, Type 1, Thrie Beam
0200	606-B013		25	Linear Feet	Guard Rail, Class A, Type 1, Thrie Beam, Transition Section
0210	606-D005		4	Each	Guard Rail, Bridge End Section, Type A
0220	606-D013		2	Each	Guard Rail, Bridge End Section, Type E
0230	606-D015		2	Each	Guard Rail, Bridge End Section, Type F
0240	606-E005		16	Each	Guard Rail, Terminal End Section, Flared
0250	606-G003		10	Each	Special Sections, Special Design Bridge Connector
0260	618-A001		1	Lump Sum	Maintenance of Traffic
0270	619-A1001		52	Mile	Temporary Traffic Stripe, Continuous White
0280	619-A2001		26	Mile	Temporary Traffic Stripe, Continuous Yellow
0290	619-A4002		12	Mile	Temporary Traffic Stripe, Skip Yellow
0300	619-A5001		12,800	Linear Feet	Temporary Traffic Stripe, Detail
0310	619-A6002		3,900	Linear Feet	Temporary Traffic Stripe, Legend
0320	619-D1001		686	Square Feet	Standard Roadside Construction Signs, Less than 10 Square Feet
0330	619-D2001		436	Square Feet	Standard Roadside Construction Signs, 10 Square Feet or More
0340	619-F3001		88	Each	Delineators, Guard Rail, White

Line no.	Item Code	Adj Code	Quantity	Units	Description[Fixed Unit Price]
0350	619-G4001		24	Linear Feet	Barricades, Type III, Double Faced
0360	620-A001		1	Lump Sum	Mobilization
0370	626-C002		21	Mile	6" Thermoplastic Double Drop Edge Stripe, Continuous White
0380	626-D001		6	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip Yellow
0390	626-E001		13	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow
0400	626-G004		4,000	Linear Feet	Thermoplastic Double Drop Detail Stripe, White
0410	626-G005		2,450	Linear Feet	Thermoplastic Double Drop Detail Stripe, Yellow
0420	626-H001		63	Square Feet	Thermoplastic Double Drop Legend, White
0430	626-H002		2,050	Linear Feet	Thermoplastic Double Drop Legend, White
0440	627-J001		590	Each	Two-Way Clear Reflective High Performance Raised Markers
0450	627-L001		1,104	Each	Two-Way Yellow Reflective High Performance Raised Markers
0460	630-G003		8	Each	Type 3 Object Markers, OM-3L, Post Mounted
0470	630-G007		8	Each	Type 3 Object Markers, OM-3R, Post Mounted
0480	907-619-B001		66	Linear Feet	Temporary Portable Rumble Strips
0490	907-899-A001		1	Lump Sum	Railway-Highway Provisions

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

CONDITIONS FOR COMBINATION BID

If a bidder elects to submit a combined bid for two or more of the contracts listed for this month's letting, the bidder must complete and execute these sheets of the proposal in each of the individual proposals to constitute a combination bid. In addition to this requirement, each individual contract shall be completed, executed and submitted in the usual specified manner.

Failure to execute this Combination Bid Proposal in each of the contracts combined will be just cause for each proposal to be received and evaluated as a separate bid.

It is understood that the Mississippi Transportation Commission not only reserves the right to reject any and all proposals, but also the right to award contracts upon the basis of lowest separate bids or combination bids most advantageous to the State.

It is further understood and agreed that the Combination Bid Proposal is for comparison of bids only and that each contract shall operate in every respect as a separate contract in accordance with its proposal and contract documents.

I (We) agree to complete each contract on or before its specified completion date.

COMBINATION BID PROPOSAL

This proposal is tendered as one part of a Combination Bid Proposal utilizing option ___* of Subsection 102.11 on the following contracts:

* Option to be shown as either (a), (b), or (c).

	<u>Project No.</u>	<u>County</u>	<u>Project No.</u>	<u>County</u>
1.	_____	_____	6.	_____
2.	_____	_____	7.	_____
3.	_____	_____	8.	_____
4.	_____	_____	9.	_____
5.	_____	_____	10.	_____

(a) If Combination A has been selected, your Combination Bid is complete.

(b) If Combination B has been selected, then complete the following page.

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
1.					
2.					
3.					
4.					
5.					
6.					
7.					
8.					

For Informational Purposes Only

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
9.					
10.					

(c) If Combination C has been selected, then initial and complete ONE of the following.

_____ I (We) desire to be awarded work not to exceed a total monetary value of \$ _____.

_____ I (We) desire to be awarded work not to exceed _____ number of contracts.



TO: EXECUTIVE DIRECTOR, MISSISSIPPI DEPARTMENT OF TRANSPORTATION
JACKSON, MISSISSIPPI

CERTIFICATE

If awarded this contract, I (we) contemplate that portions of the contract will be sublet. I (we) certify that those subcontracts which are equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

I (we) agree that this notification of intent DOES NOT constitute APPROVAL of the subcontracts.

_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)

NOTE: Failure to complete the above DOES NOT preclude subsequent subcontracts. Subsequent subcontracts, if any, equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

Contractor _____

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
CERTIFICATION

I, _____,
(Name of person signing bid)

individually, and in my capacity as _____ of
(Title of person signing bid)

(Name of Firm, partnership, or Corporation)

do hereby certify under penalty of perjury under the laws of the United States and the State of Mississippi

that _____, Bidder
(Name of Firm, Partnership, or Corporation)

on Project No. **MP-5011-38(010)/ 306547301000**

in **Lauderdale** County(ies), Mississippi, has not either directly or indirectly entered into any agreement, participated in any collusion; or otherwise taken any action in restraint of free competitive bidding in connection with this contract; nor have any of its corporate officers or principal owners.

Except as noted hereafter, it is further certified that said legal entity and its corporate officers, principal owners, managers, auditors and others in a position of administering federal funds are not currently under suspension, debarment, voluntary exclusion or determination of ineligibility; nor have a debarment pending; nor been suspended, debarred, voluntarily excluded or determined ineligible within the past three years by the Mississippi Transportation Commission, the State of Mississippi, any other State or a federal agency; nor been indicted, convicted or had a civil judgment rendered by a court of competent jurisdiction in any matter involving fraud or official misconduct within the past three years.

Do exceptions exist and are made a part thereof? Yes / No

Any exceptions shall address to whom it applies, initiating agency and dates of such action.

Note: Exceptions will not necessarily result in denial of award but will be considered in determining bidder responsibility. Providing false information may result in criminal prosecution or administrative sanctions.

All of the foregoing is true and correct.

(1/2016 S)

SECTION 902

CONTRACT FOR MP-5011-38(010)/ 306547301000

LOCATED IN THE COUNTY(IES) OF Lauderdale

STATE OF MISSISSIPPI,
COUNTY OF HINDS

This contract entered into by and between the Mississippi Transportation Commission on one hand, and the undersigned contractor, on the other witnesseth;

That, in consideration of the payment by the Mississippi Transportation Commission of the prices set out in the proposal hereto attached, to the undersigned contractor, such payment to be made in the manner and at the time of times specified in the specifications and the special provisions, if any, the undersigned contractor hereby agrees to accept the prices stated in the proposal in full compensation for the furnishing of all materials and equipment and the executing of all the work contemplated in this contract.

It is understood and agreed that the advertising according to law, the Advertisement, the instructions to bidders, the proposal for the contract, the specifications, the revisions of the specifications, the special provisions, and also the plans for the work herein contemplated, said plans showing more particularly the details of the work to be done, shall be held to be, and are hereby made a part of this contract by specific reference thereto and with like effect as if each and all of said instruments had been set out fully herein in words and figures.

It is further agreed that for the same consideration the undersigned contractor shall be responsible for all loss or damage arising out of the nature of the work aforesaid; or from the action of the elements and unforeseen obstructions or difficulties which may be encountered in the prosecution of the same and for all risks of every description connected with the work, exceptions being those specifically set out in the contract; and for faithfully completing the whole work in good and workmanlike manner according to the approved Plans, Specifications, Special Provisions, Notice(s) to Bidders and requirements of the Mississippi Department of Transportation.

It is further agreed that the work shall be done under the direct supervision and to the complete satisfaction of the Executive Director of the Mississippi Department of Transportation, or his authorized representatives, and when Federal Funds are involved subject to inspection at all times and approval by the Federal Highway Administration, or its agents as the case may be, or the agents of any other Agency whose funds are involved in accordance with those Acts of the Legislature of the State of Mississippi approved by the Governor and such rules and regulations issued pursuant thereto by the Mississippi Transportation Commission and the authorized Federal Agencies.

The Contractor agrees that all labor as outlined in the Special Provisions may be secured from list furnished by

It is agreed and understood that each and every provision of law and clause required by law to be inserted in this contract shall be deemed to be inserted herein and this contract shall be read and enforced as though it were included herein, and, if through mere mistake or otherwise any such provision is not inserted, then upon the application of either party hereto, the contract shall forthwith be physically amended to make such insertion.

The Contractor agrees that he has read each and every clause of this Contract, and fully understands the meaning of same and that he will comply with all the terms, covenants and agreements therein set forth.

Witness our signatures this the ___ day of _____, _____.

Contractor(s)

By _____

MISSISSIPPI TRANSPORTATION COMMISSION

Title _____

By _____

Signed and sealed in the presence of:
(names and addresses of witnesses)

Executive Director

Secretary to the Commission

Award authorized by the Mississippi Transportation Commission in session on the ___ day of _____, _____, Minute Book No. _____, Page No. _____.

Revised 8/06/2003

SECTION 903
PERFORMANCE AND PAYMENT BOND

CONTRACT BOND FOR: MP-5011-38(010)/ 306547301000

LOCATED IN THE COUNTY(IES) OF: Lauderdale

STATE OF MISSISSIPPI,
COUNTY OF HINDS

Know all men by these presents: that we, _____

(Contractor)

Principal, a _____

residing at _____ in the State of _____

and _____

(Surety)
residing at _____ in the State of _____,

authorized to do business in the State of Mississippi, under the laws thereof, as surety, effective as of the contract date shown below, are held and firmly bound unto the State of Mississippi in the sum of _____

_____ Dollars, lawful money of the United States of America, to be paid to it for which payment well and truly to be made, we bind ourselves, our heirs, administrators, successors, or assigns jointly and severally by these presents.

The conditions of this bond are such, that whereas the said _____

principal, has (have) entered into a contract with the Mississippi Transportation Commission, bearing the date of _____ day of _____ A.D. _____ hereto annexed, for the construction of certain projects(s) in the State of Mississippi as mentioned in said contract in accordance with the Contract Documents therefor, on file in the offices of the Mississippi Department of Transportation, Jackson, Mississippi.

Now therefore, if the above bounden _____ in all things shall stand to and abide by and well and truly observe, do keep and perform all and singular the terms, covenants, conditions, guarantees and agreements in said contract, contained on his (their) part to be observed, done, kept and performed and each of them, at the time and in the manner and form and furnish all of the material and equipment specified in said contract in strict accordance with the terms of said contract which said plans, specifications and special provisions are included in and form a part of said contract and shall maintain the said work contemplated until its final completion and acceptance as specified in Subsection 109.11 of the approved specifications, and save harmless said Mississippi Transportation Commission from any loss or damage arising out of or occasioned by the negligence, wrongful or criminal act, overcharge, fraud, or any other loss or damage whatsoever, on the part of said principal (s), his (their) agents, servants, or employees in the performance of said work or in any manner connected therewith, and shall be liable and responsible in a civil action instituted by the State at the instance of the Mississippi Transportation Commission or any officer of the State authorized in such cases, for double any amount in money or property, the State may lose or be overcharged or otherwise defrauded of, by reason of wrongful or criminal act, if any, of the Contractor(s), his (their) agents or employees, and shall promptly pay the said agents, servants and employees and all persons furnishing labor, material, equipment or supplies therefor, including premiums incurred, for Surety Bonds, Liability Insurance, and Workmen's Compensation Insurance; with the additional obligation that such Contractor shall promptly make payment of all taxes, licenses, assessments, contributions, damages,

any liquidated damages which may arise prior to any termination of said principal's contract, any liquidated damages which may arise after termination of the said principal's contract due to default on the part of said principal, penalties and interest thereon, when and as the same may be due this state, or any county, municipality, board, department, commission or political subdivision: in the course of the performance of said work and in accordance with Sections 31-5-51 et seq. Mississippi Code of 1972, and other State statutes applicable thereto, and shall carry out to the letter and to the satisfaction of the Executive Director of the Mississippi Department of Transportation, all, each and every one of the stipulations, obligations, conditions, covenants and agreements and terms of said contract in accordance with the terms thereof and all of the expense and cost and attorney's fee that may be incurred in the enforcement of the performance of said contract, or in the enforcement of the conditions and obligations of this bond, then this obligation shall be null and void, otherwise to be and remain in full force and virtue.

_____	_____
(Contractors) Principal	Surety
By _____	By _____
	(Signature) Attorney in Fact
	Address _____

Title _____	_____
(Contractor's Seal)	(Printed) MS Agent

	(Signature) MS Agent
	Address _____

	(Surety Seal)

	Mississippi Insurance ID Number



BID BOND

KNOW ALL MEN BY THESE PRESENTS, that we _____
Contractor

Address

City, State ZIP

As principal, hereinafter called the Principal, and _____
Surety

a corporation duly organized under the laws of the state of _____

as Surety, hereinafter called the Surety, are held and firmly bound unto **State of Mississippi, Jackson, Mississippi**

As Obligee, hereinafter called Obligee, in the sum of **Five Per Cent (5%) of Amount Bid**

Dollars(\$ _____)

for the payment of which sum will and truly to be made, the said Principal and said Surety, bind ourselves, our heirs, executors, administrators, successors and assigns, jointly and severally, firmly by these presents.

WHEREAS, the Principal has submitted a bid for **Mill & Overlay approximately 11 miles of US 11 from Russell to Alabama State Line, known as State Project No. MP-5011-38(010) / 306547301 in Lauderdale County.**

NOW THEREFORE, the condition of this obligation is such that if the aforesaid Principal shall be awarded the contract, the said Principal will, within the time required, enter into a formal contract and give a good and sufficient bond to secure the performance of the terms and conditions of the contract, then this obligation to be void; otherwise the Principal and Surety will pay unto the Obligee the difference in money between the amount of the bid of the said Principal and the amount for which the Obligee legally contracts with another party to perform the work if the latter amount be in excess of the former, but in no event shall liability hereunder exceed the penal sum hereof.

Signed and sealed this _____ day of _____, 20__

(Witness)

(Principal) (Seal)

By: _____
(Name) (Title)

(Surety) (Seal)

(Witness)

By: _____
(Attorney-in-Fact)

(MS Agent)

Mississippi Insurance ID Number

