P.E.107681/101000

STATE	PROJECT NUMBER	SHEET NO.
MISSISSIPPI	NH-0011-03(085)	1

GENERAL INDEX

BRIDGE STRUCTURES REQ'D.
NONE

BOX BRIDGES REQ'D.
NONE

B.O.P. STA. 10 + 00

STATE OF MISSISSIPPI

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

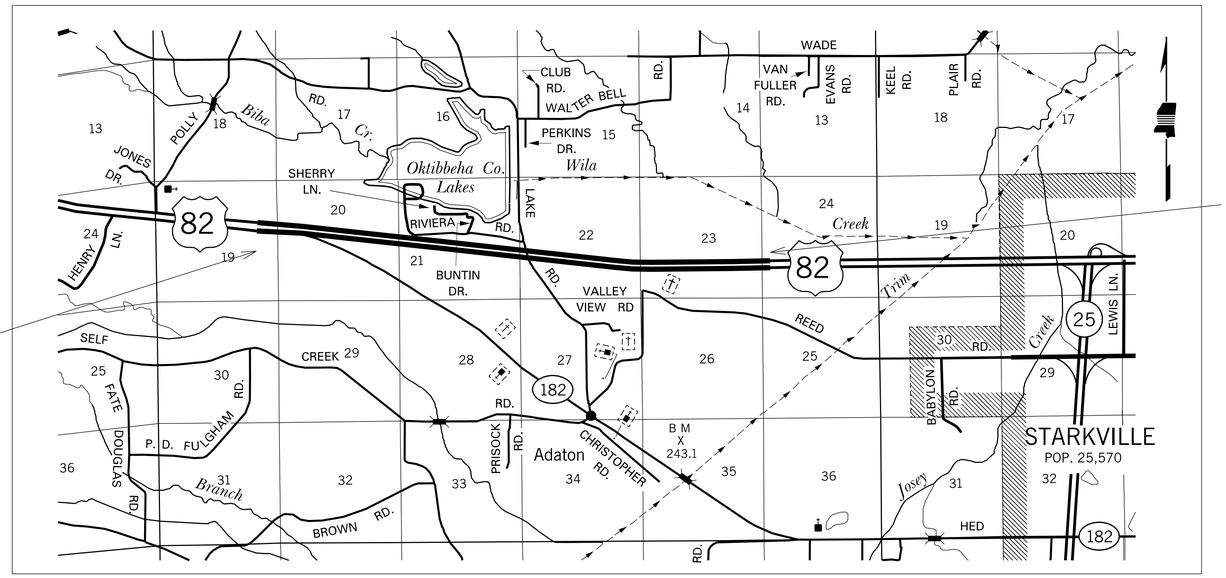
PLAN AND PROFILE OF PROPOSED STATE HIGHWAY FEDERAL AID PROJECT NO. NH–0011–03(085)

US 82 FROM BEGINING STARKVILLE
BYPASS TO BEGINING OF CONCRETE SECTION
OKTIBBEHA COUNTY

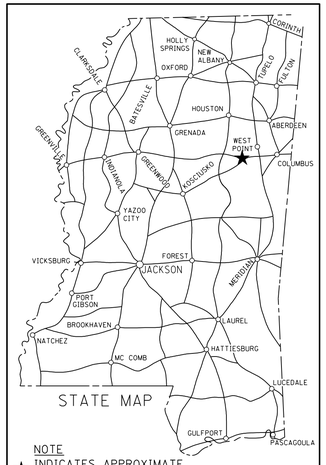
FMS. CONST. NO. 107681/301000

SCALES

PLAN		1 IN. =	100 FT.
PROFILE	HOR.	1 IN. =	100 FT.
	VERT.	1 IN. =	10 FT.
LAYOUT		1 IN. =	FT.



E.O.P. STA. 229 + 02



NOTE
★ INDICATES APPROXIMATE LOCATION OF PROJECT.

LAT. 33°29'47" W LONG. 88°55'56" W
(APPROX. MIDDLE OF PROJECT)

DESIGN CONTROL

_____ MPH = V (SPEED DESIGN)

ADT (____) = _____: ADT (____) = _____
DHV = _____ : D = _____% T = _____%

PERMITS ACQUIRED BY MDOT

WETLANDS AND WATERS PERMITS (NECESSARY FOR ULTIMATE IMPROVEMENTS ONLY):

	WATERS	WETLANDS
NATIONWIDE #14	N	N
NATIONWIDE (OTHER)*	N	N
GENERAL*	N	N
INDIVIDUAL (404)*	N	N

* ACQUISITION OF PERMITS FOR TEMPORARY IMPACTS DURING CONSTRUCTION ARE THE RESPONSIBILITY OF THE CONTRACTOR

STORMWATER PERMIT N

Y	REQUIRED, CNOI SUBMITTED BY MDOT (DISTURBED AREA = 5 ACRES)
S	REQUIRED, SCNOI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES)
N	NO STORMWATER PERMIT REQUIRED (<1 ACRE)

APPROVED BY: _____

CONVENTIONAL SYMBOLS

COUNTY LINE .....
TOWN CORPORATION LINE .....
SECTION LINE .....
EXISTING ROAD OR TRAVELED WAY .....
PROPOSED ROAD OR TRAVELED WAY .....
RAILROAD .....
SURVEY LINE .....
BRIDGES .....

EQUATIONS
NONE

EXCEPTIONS
NONE

LENGTH DATA

LENGTH OF ROADWAY	21,902	FT.	4.148	MI.
LENGTH OF BRIDGES	0	FT.	0	MI.
LENGTH OF PROJECT (NET)	21,902		4.148	MI.
LENGTH OF EXCEPTIONS	0	FT.	0	MI.
LENGTH OF PROJECT (GROSS)	21,902		4.148	MI.

P S & E DATE: 8/1/2018

APPROVED:

DEPUTY EXECUTIVE DIRECTOR / CHIEF ENGINEER

EXECUTIVE DIRECTOR

REVISIONS BY DATE

MDOT MISSISSIPPI DEPARTMENT OF TRANSPORTATION

OKTIBBEHA COUNTY 1

TITLE
ROADWAY DIST. DIVISION
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

7/19/2019 11:52AM 82 STARKVILLE BYPASS 2018.DGN

STATE	PROJECT NO.
MISS.	NH-0011-03(085)

DESCRIPTION OF SHEET	REVISION DATE	WKG. NO.	SH. NO.
TITLE SHEET (1)			1
DETAIL INDEX & GENERAL NOTES (1)		DI-1	2
TYPICAL OVERLAY SECTIONS (2)			
TYPICAL OVERLAY SECTION FOUR LANE ROADWAY US 82		TS-1	3
TYPICAL OVERLAY SECTION FOUR LANE ROADWAY US 82		TS-2	4
QUANTITY SHEETS (1)			
SUMMARY OF QUANTITIES		SQ-1	5
SPECIAL DESIGN-ROADWAY ITEMS (2)			
DETAIL OF CONSTRUCTION SIGNS US 82		DCS-1	6
STOP SIGN RUMBLE STRIPE DETAIL		SSR-1	7
STANDARD DRAWINGS - ROADWAY SHEETS (12)			
PAVEMENT MARKING DETAIL FOR 2-LANE AND 4-LANE DIVIDED HIGHWAYS		PM-1	6051
PAVEMENT MARKING LEGEND DETAILS		PM-6	6056
TYPICAL PAVEMENT MARKING DETAIL FOR MEDIAN CROSSOVERS		PM-9	6059
2-WAY RAISED PAVEMENT MARKERS AT INTERSECTING ROADS (4-LANE)		PM-12	6062
RUMBLE STRIPES 4-LANE HIGHWAYS (ASPHALT LANES, 2-FT OR WIDER ASPHALT SHOULDERS)		RS-2	6065
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATE AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (EXTENDED PERIOD)		TCP-4	6354
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATE AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDELANE CLOSURE) (WORK DAY ONLY)		TCP-5	6355
HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS		TCP-8	6358
TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO LANE ROADS		TCP-9	6359
TRAFFIC CONTROL PLAN UNEVEN PAVEMENT DETAILS		TCP-12	6362
TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS		TCP-13	6363
LOCATION OF R16-3 SIGNS		TCP-15	6365

GENERAL NOTES:

1. WHERE MILLING OF THE ROADWAY LANES IS REQUIRED, THE CONTRACTOR SHALL PROVIDE OUTLETS IN THE EXISTING SHOULDERS AT SUFFICENT INTERVALS TO PREVENT POOLING OR STANDING WATER ON THE MILLED SURFACE. (ABSORBED COST)
2. FLUORESENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON THE WHITE BACKGROUND.
3. THE LOCATION AND SPACING OF SIGNS, SHOWN ON THE TRAFFIC CONTROL PLANS, ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.
4. ALL TRAFFIC CONTROL DEVICES ON THIS PROJECT SHALL COMPLY WITH PART VI OF THE MUTCD (LATEST EDITION).
5. THIS CONTRACT IS TO INCLUDE BLADING AND GRADING OF EXISTING AGGREGATE SHOULDERS. COST TO BE ABSORBED INTO OTHER ITEMS BID.
6. REMOVAL OF RAISED PAVEMENT MARKERS THAT ARE IN CONFLICT WITH REQUIRED CONSTRUCTION IS NOT CONSIDERED A SEPARATE PAY ITEM. COST TO BE ABSORBED INTO OTHER ITEMS BID.
7. BIDDERS ARE ADVISED THAT HARD COPIES OF ANY ADDENDA FOR THIS PROJECT WILL NO LONGER BE MAILED. ALL ADDENDA FOR THIS PROJECT WILL BE POSTED TØ www.mdot.ms.gov UNDER THE PROPOSAL ADDENDA COLUMN. IT IS THE BIDDERS RESPONSIBILITY TO CHECK AND SEE IF ANY ADDENDA HAVE BEEN POSTED FOR THIS PROJECT. PLEASE CONTACT CONTRACT ADMINISTRATION DIVISION AT 601-359-7700 FOR ANY QUESTION REGARDING ELECTRONIC ADDENDA.
8. 12.5 mm ASPHALT SHOULD BE PLACED IN A MILL AND FILL SEQUENCE BEHIND ANY MILLING THAT GOES TO A DEPTH OF 3 1/2".

SEQUENCE OF OPERATIONS

1. REPAIR OF FAILED AREAS AS DIRECTED
2. FINE MILLING
3. PRE-LEVELING AS DIRECTED
4. PLACEMENT OF TEMPORARY STRIPE.
5. PLACEMENT OF SURFACE COURSE
6. PLACEMENT OF TEMPORARY STRIPE.
7. PLACEMENT OF PERMANENT STRIPE.

DISTRICT 1

PS & E PLANS-DATE 8/7/2019		
FMS CON. # 107681/301000		
REVISIONS		
DATE	SHEET NO.	BY

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

DETAILED INDEX

PROJECT NO.: NH-0011-03(085)

COUNTY: OKTIBBEHA

WORKING NUMBER DI-1

SHEET NUMBER 2

FILENAME: HWY 82 STARKVILLE BYPASS 2019.DGN

DESIGN TEAM _____ CHECKED _____ DATE _____

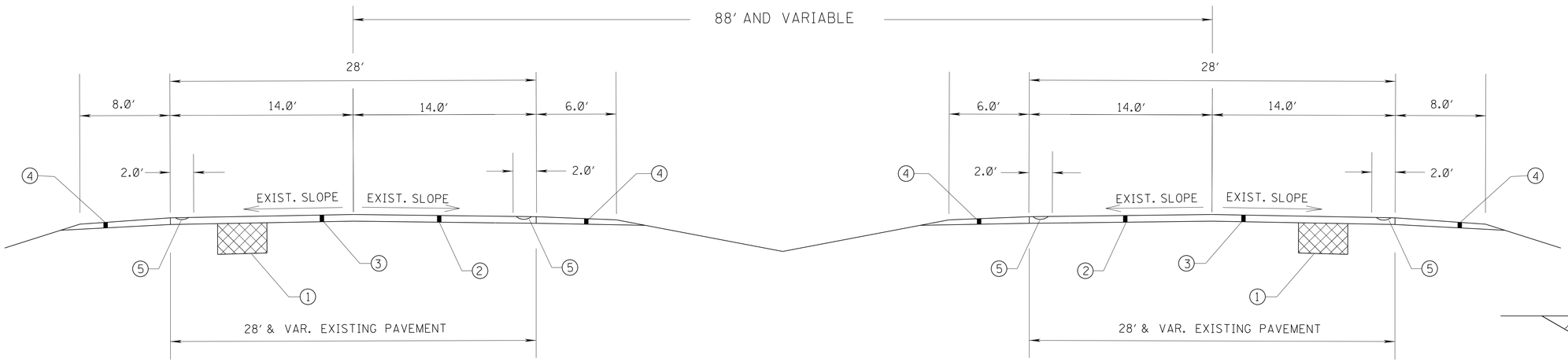
BY | REVISION | DATE

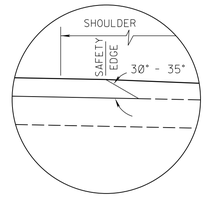
PLAN
ROADWAY DESIGN DIVISION
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

MM/DD/YY 00:00 AM/PM CONFILENAME

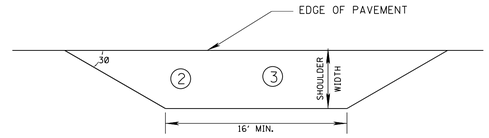
FMS CON: 107681/301000

STATE	PROJECT NO.
MISS.	NH-Ø011-03(Ø85)





SAFETY EDGE REQ'D (NOT A PAY ITEM) OVERLAY 14' PAV'MT WITHOUT TRENCH



DETAIL OF RAMP APRON

STA. 1Ø+ØØ TO STA. 2Ø+ØØ US 82
STA. 123+ØØ TO STA. 187+ØØ US 82

NOTES :

1. REMOVE AND UNDERCUT FAILED AREAS AS DIRECTED BY THE ENGINEER. REMOVAL OF ASPHALT PAVEMENT TO TO BE PAID FOR UNDER PAY ITEM 202-BØØ7. REMOVAL OF UNDERCUT MATERIAL TO BE PAID FOR UNDER EXCESS EXCAVATION (LVM)(AH)(CONTRACTOR DISPOSAL OFF OF R.O.W.) PAY ITEM NO. 2Ø3-GØØ2. BACKFILL WITH 9.5mm MIXTURE, MT, PAY ITEM NO. 4Ø3-AØ14, FOR A MAX. DEPTH OF 1.Ø′. IF ADDITIONAL BACKFILL MATERIAL IS REQUIRED USE GRAN. MAT"L. (CL.3 GP.D), PAY ITEM NO. 3Ø4-AØØ3.

2. 1 ½" FINE MILLING REQUIRED, PAY ITEM NO. 4Ø6-DØØ1

3. 1 ½" 9.5 mm, MT, ASPHALT PAVEMENT, PAY ITEM NO. 4Ø3-AØ14.

4. GRANULAR MATERIAL (CL. 3, GP. D) REQUIRED ON THE SHOULDERS AS DIRECTED BY THE ENGINEER, PAY ITEM NO. 3Ø4-AØØ3

5. RUMBLE STRIPS GROUND IN PAY ITEM 423-AØØ1

PLAN
ROADWAY DESIGN DIVISION
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

MMDDYY 00:00AMPM DGNFILENAME

DATE	REVISION	BY

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

TYPICAL OVERLAY SECTIONS
FOUR LANE ROADWAY
US 82

PROJECT NO.: NH-0011-03(085)

COUNTY: OKTIBBEHA

WORKING NUMBER TS-1

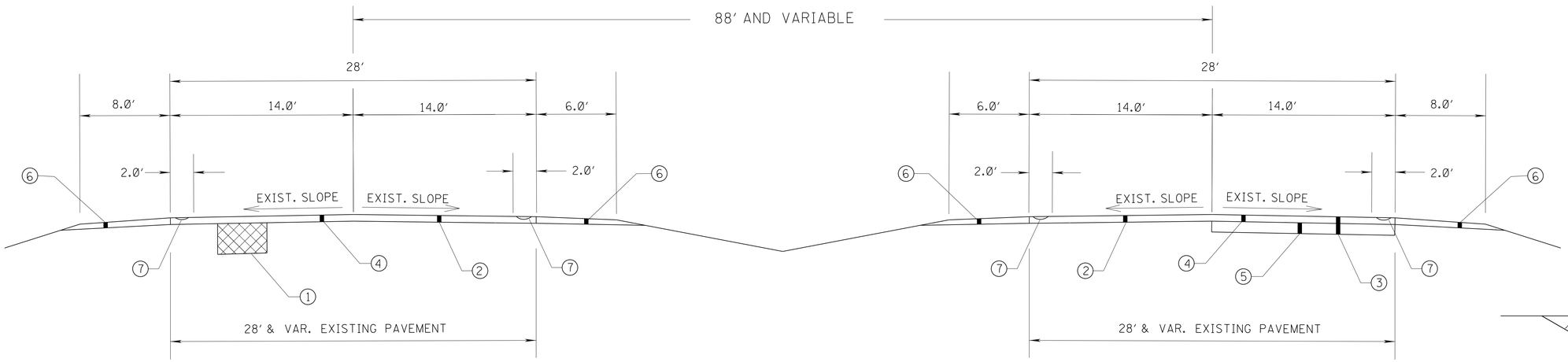
FILENAME: HWY 82 STARKVILLE BYPASS 20TBEDGN

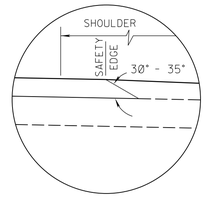
DESIGN TEAM _____ CHECKED _____ DATE _____

SHEET NUMBER 3

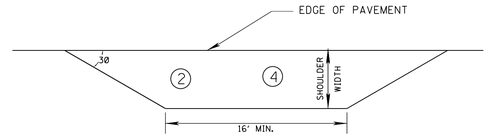
FMS CON: 107681/301000

STATE	PROJECT NO.
MISS.	NH-0011-03(085)





SAFETY EDGE REQ'D (NOT A PAY ITEM) OVERLAY 14' PAV'MT WITHOUT TRENCH



DETAIL OF RAMP APRON

STA. 20+00 TO STA. 123+00 US 82
STA. 187+00 TO STA. 229+02 US 82

NOTES :

① REMOVE AND UNDERCUT FAILED AREAS AS DIRECTED BY THE ENGINEER. REMOVAL OF ASPHALT PAVEMENT TO TO BE PAID FOR UNDER PAY ITEM 202-B007. REMOVAL OF UNDERCUT MATERIAL TO BE PAID FOR UNDER EXCESS EXCAVATION (LVM)(AH)(CONTRACTOR DISPOSAL OFF OF R.O.W.) PAY ITEM NO. 203-G002. BACKFILL WITH 9.5mm MIXTURE, MT, PAY ITEM NO. 403-A014, FOR A MAX. DEPTH OF 1.0'. IF ADDITIONAL BACKFILL MATERIAL IS REQUIRED USE GRAN. MAT"L. (CL.3 GP.D), PAY ITEM NO. 304-A003.

② 1 ½" FINE MILLING REQUIRED, PAY ITEM NO. 406-D001

③ 3 ½" FINE MILLING REQUIRED, PAY ITEM NO. 406-D001

④ 1 ½" 9.5 mm, MT, ASPHALT PAVEMENT, PAY ITEM NO. 403-A014.

⑤ 2" 12.5 mm, MT, ASPHALT PAVEMENT, PAY ITEM NO. 403-A002 TO BE PLACED IN A MILL AND FILL SEQUENCE BEHIND ANY MILLING THAT GOES TO A DEPTH OF 3 1/2".

⑥ GRANULAR MATERIAL (CL. 3, GP. D) REQUIRED ON THE SHOULDERS AS DIRECTED BY THE ENGINEER, PAY ITEM NO. 304-A003

⑦ RUMBLE STRIPS GROUND IN PAY ITEM 423-A001

PLAN
ROADWAY DESIGN DIVISION
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

MMDDYY 00:00AM DGNFILENAME

BY	REVISION	DATE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

TYPICAL OVERLAY SECTIONS
FOUR LANE ROADWAY
US 82

PROJECT NO.: NH-0011-03(085)

COUNTY: OKTIBBEHA

WORKING NUMBER TS-2

SHEET NUMBER 4

FILENAME: HWY 82 STARKVILLE BYPASS 20TSD.DGN

DESIGN TEAM _____ CHECKED _____ DATE _____

STATE	PROJECT NO.
MISS	NH-0011-03(085)

SUMMARY OF QUANTITIES (SHEET 1)

PAY ITEM NO.	PAY ITEM	UNIT	OKTIBBEHA : 107681-301000		
			Prelim	Final	
202-B007	Removal of Asphalt Pavement, All Depths	SY	50		(1)
203-G002	Excess Excavation, LVM, AH	CY	50		(1)
304-A003	Granular Material, LVM, Class 3, Group D	CY	4,000		(1) (2)
403-A002	12.5-mm, MT, Asphalt Pavement	TON	3,200		
403-A014	9.5-mm, MT, Asphalt Pavement	TON	17,000		(3)
406-D001	Fine Milling of Bituminous Pavement, All Depths	SY	160,000		
407-A001	Asphalt for Tack Coat	GAL	16,000		
423-A001	Rumble Strips, Ground In	MI	16		
618-A001	Maintenance of Traffic	LS	1		
618-B001	Additional Construction Signs	SF	1		
619-A1001	Temporary Traffic Stripe, Continuous White	MI	20		(4)
619-A2001	Temporary Traffic Stripe, Continuous Yellow	MI	18		(4)
619-A3001	Temporary Traffic Stripe, Skip White	MI	18		(4)
619-A5001	Temporary Traffic Stripe, Detail	LF	40,600		
619-A6002	Temporary Traffic Stripe, Legend	LF	2,800		
620-A001	Mobilization	LS	1		
626-A004	6" Thermoplastic Traffic Stripe, Skip White	MI	9		
626-C004	6" Thermoplastic Edge Stripe, Continuous White	MI	10		
626-F003	6" Thermoplastic Edge Stripe, Continuous Yellow	MI	9		
626-G002	Thermoplastic Detail Stripe, White	LF	10,400		
626-G003	Thermoplastic Detail Stripe, Yellow	LF	9,900		
626-H004	Thermoplastic Legend, White	SF	45		
626-H005	Thermoplastic Legend, White	LF	1,400		
627-J001	Two-Way Clear Reflective High Performance Raised Markers	EA	210		
627-K001	Red-Clear Reflective High Performance Raised Markers	EA	620		
627-L001	Two-Way Yellow Reflective High Performance Raised Markers	EA	45		

(1) ESTIMATED QUANTITY. ACTUAL QUANTY AND PLACEMENT TO BE AS DIRECTED BY THE ENGINEER.

(2) QUANTITY INCLUDES 50% SHRINKAGE FACTOR.

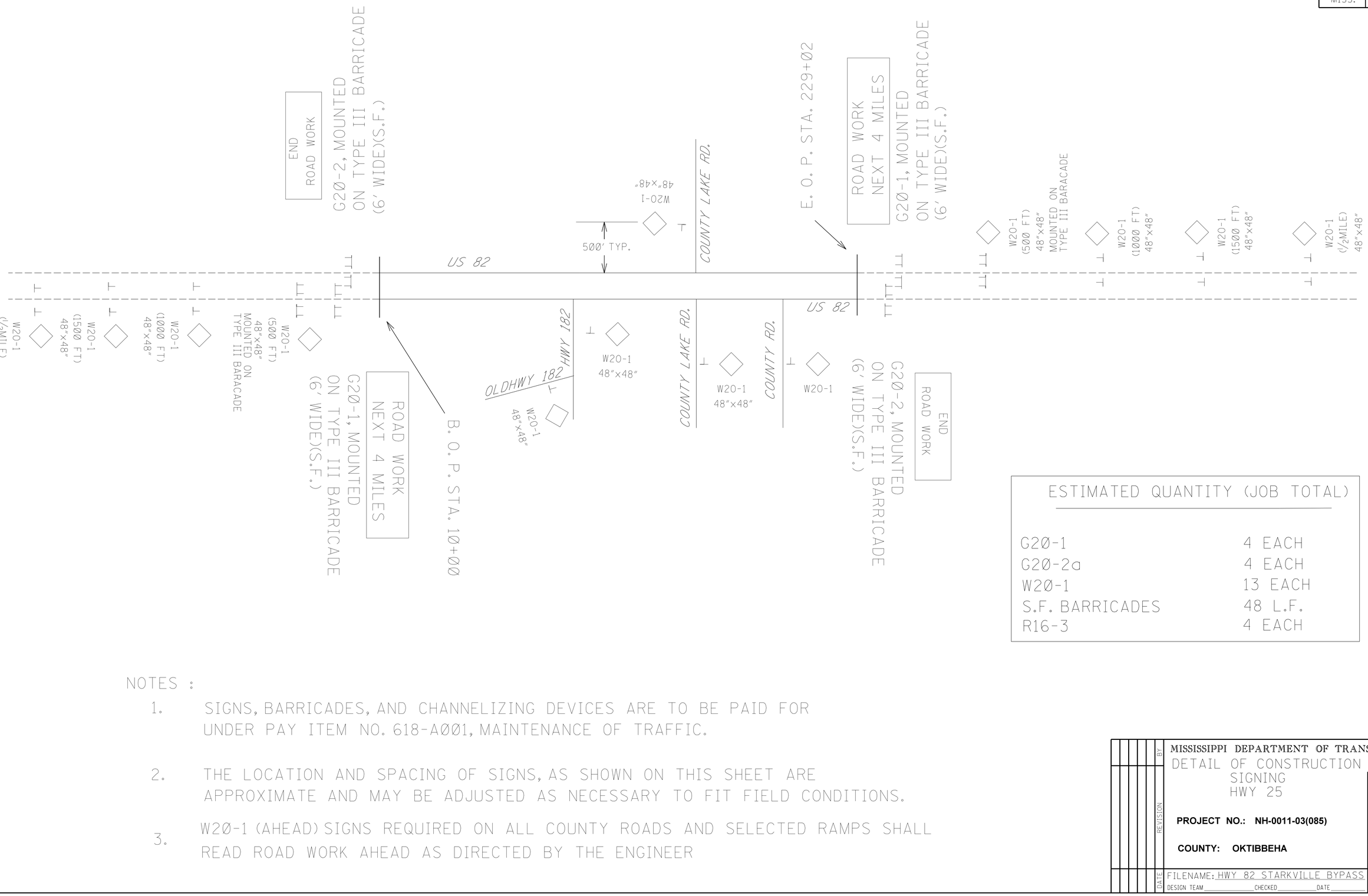
(3) 2,000 TONS TO BE USED FOR CROSS-OVERS, LOCAL ROADS, AND DRIVEWAY PADS. 3,300 TONS TO BE USED FOR PRELEVELING AS DIRECTED BY THE ENGINEER.

(4) OFFSET TEMPORARY STRIPE ACCORDING TO PAVING AND FINE MILLING OPPERATIONS.

By	**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**	
Revision	**SUMMARY OF QUANTITIES**	
	PROJ NO: NH-0011-03(085)	Working Number
	COUNTY: OKTIBBEHA	SQ-1
Date	FILENAME: 107681/301000 NH-001-03(	Sheet Number
	Design Team Checked Date	5

FMS CON: 107681/301000

STATE	PROJECT NO.
MISS.	NH-0011-03(085)



ESTIMATED QUANTITY (JOB TOTAL)

G20-1	4 EACH
G20-2a	4 EACH
W20-1	13 EACH
S.F. BARRICADES	48 L.F.
R16-3	4 EACH

NOTES :

1. SIGNS, BARRICADES, AND CHANNELIZING DEVICES ARE TO BE PAID FOR UNDER PAY ITEM NO. 618-A001, MAINTENANCE OF TRAFFIC.

2. THE LOCATION AND SPACING OF SIGNS, AS SHOWN ON THIS SHEET ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.

3. W20-1 (AHEAD) SIGNS REQUIRED ON ALL COUNTY ROADS AND SELECTED RAMPS SHALL READ ROAD WORK AHEAD AS DIRECTED BY THE ENGINEER

REVISION / BY / DATE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
DETAIL OF CONSTRUCTION SIGNING
HWY 25

PROJECT NO.: NH-0011-03(085)

COUNTY: OKTIBBEHA

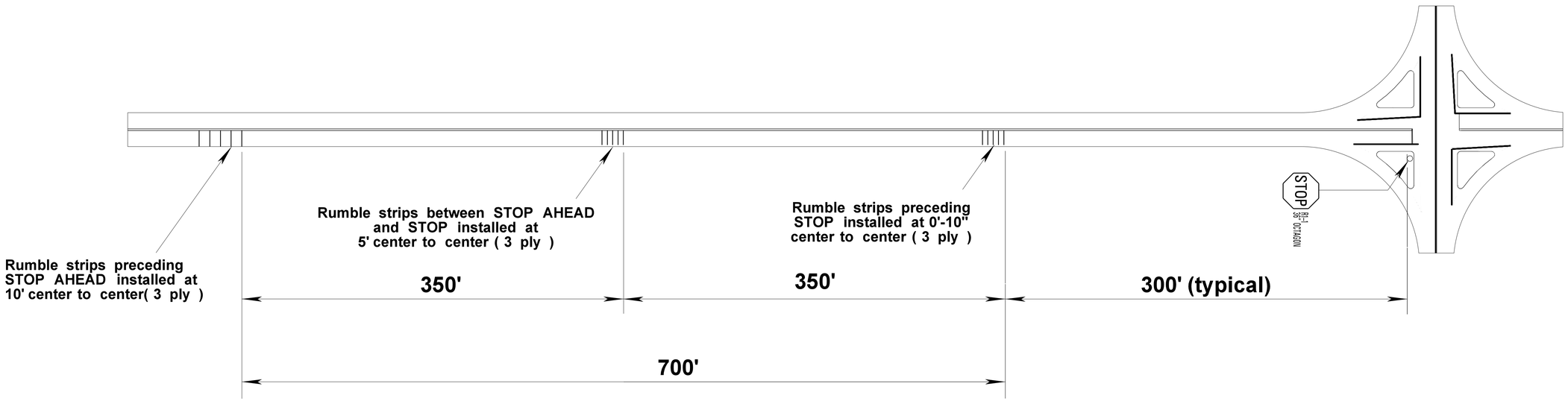
WORKING NUMBER DCS-1

FILENAME: HWY 82 STARKVILLE BYPASS 2018.DGN
DESIGN TEAM _____ CHECKED _____ DATE _____

SHEET NUMBER 6

FMS CON: 107681/301000

STATE	PROJECT NO.
MISS.	NH-0011-03(085)



NOTES:
Install rumble strips as shown :

1. 1 set of rumble strips approx. 250' from STOP AHEAD
2. 1 set of rumble strips approx. 300' from STOP
3. 1 set of rumble strips approx. Halfway between first and last set
4. Rumble strips to be 6" thermoplastic (120 mil/each ply, 360 mil total)
5. 5 rumble strips per set minimum
6. Installation may vary due to terrain
7. Signs should be 48" for channelized intersection, 36" for non-channelized intersection

THIS DRAWING IS NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

STOP SIGN RUMBLE STRIPE DETAIL

PROJECT NO.: NH-0011-03(085)

COUNTY: OKTIBBEHA

WORKING NUMBER SSR-1

SHEET NUMBER 7

REVISION BY DATE

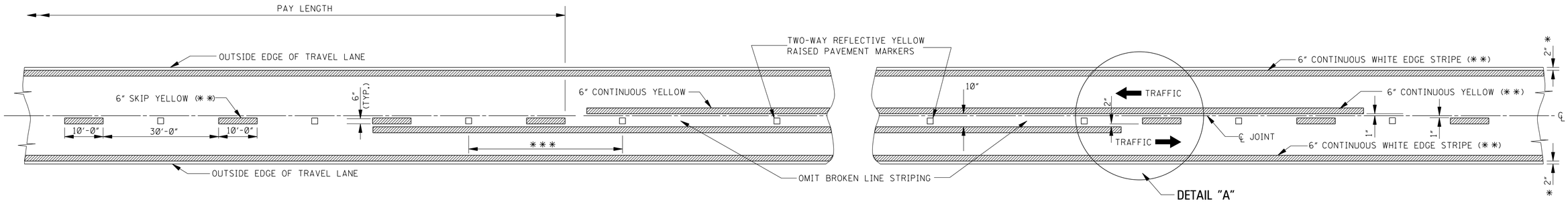
FILENAME:

DESIGN TEAM CHECKED DATE

PLAN
ROADWAY DESIGN DIVISION
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

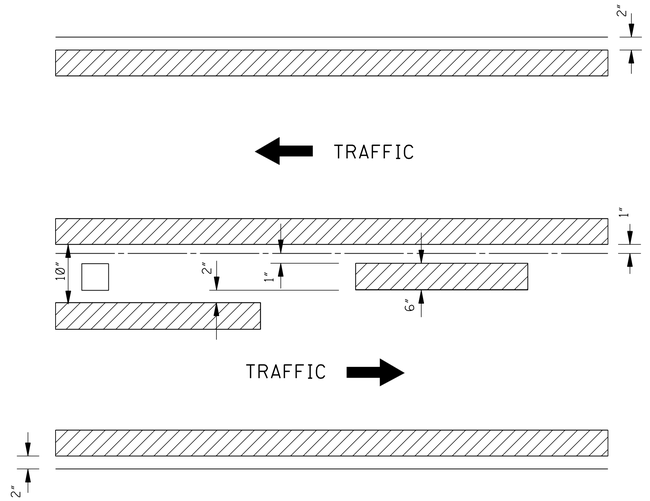
MMDDYY 00:00AMPM DGNFILENAME

STATE	PROJECT NO.
MISS.	



TWO–WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)

NOTE: THE CRITERIA FOR NO-PASSING ZONES CAN BE FOUND IN THE MDOT ROADWAY DESIGN MANUAL, SECTION 11-1.01.



DETAIL "A"

GENERAL NOTES:

* 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS. FOR STRIPING ON RUMBLE STRIP SECTIONS REFER TO WK. SHEETS RS-1, RS-2, AND RS-3.

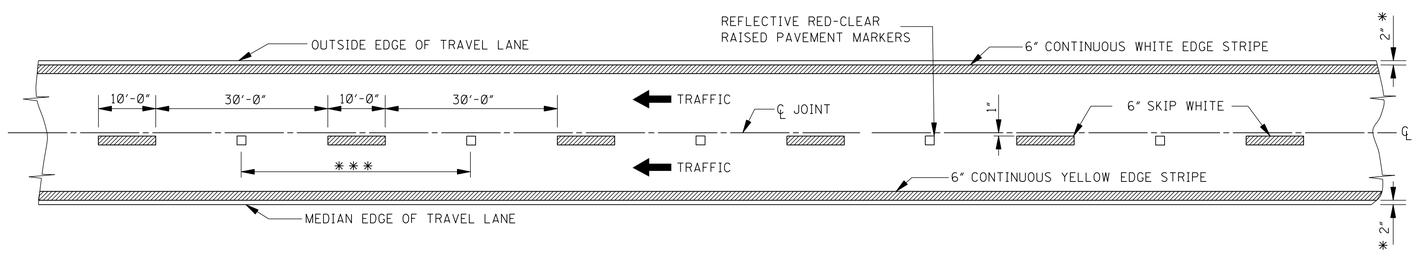
* * 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).

* * * 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

	URBAN AREA (ft-in)	RURAL AREA (ft-in)
TANGENT SECTIONS	40'-0"	80'-0"
HORIZONTAL CURVES	40'-0"	40'-0"
INTERCHANGE LIMITS	40'-0"	† 40'-0"

† NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED SOURCES OF MATERIALS."



4–LANE WITH ONE–WAY TRAFFIC

BY	REVISION	DATE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

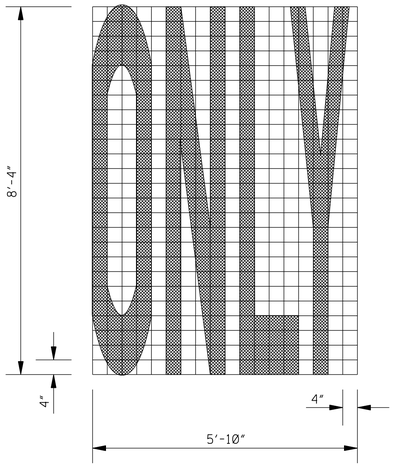
PAVEMENT MARKING DETAILS FOR 2–LANE AND 4–LANE DIVIDED ROADWAYS

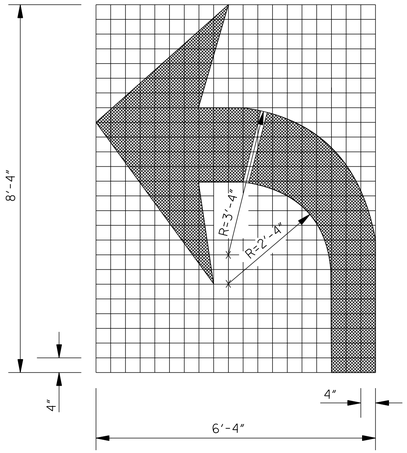
ISSUE DATE: AUGUST 01, 2017

WORKING NUMBER
PM-1

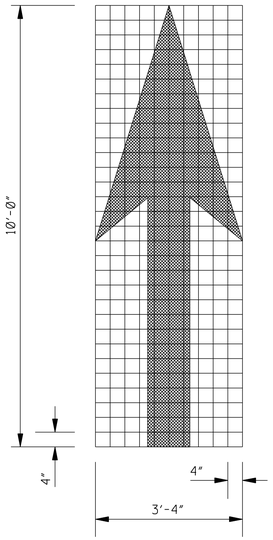
SHEET NUMBER
6051

STATE	PROJECT NO.
MISS.	

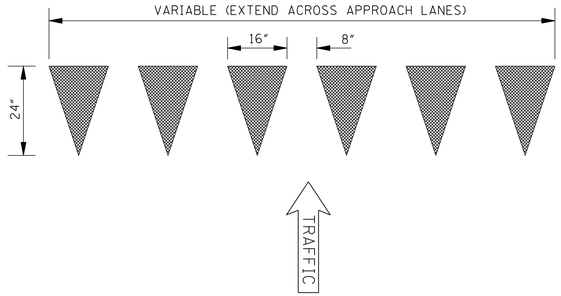




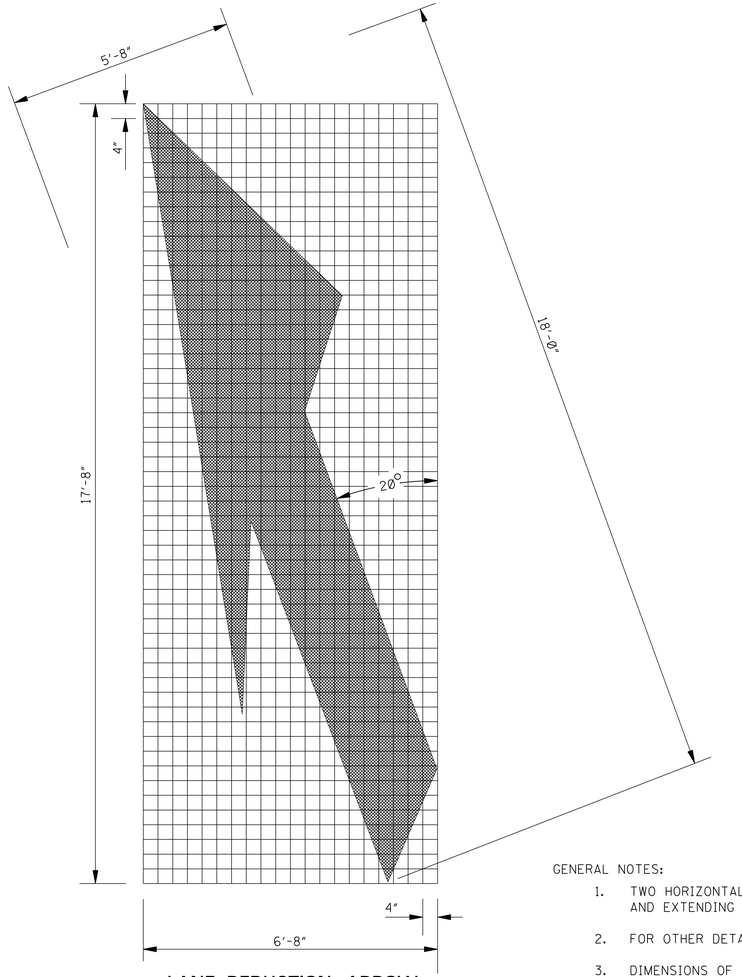
TURN ARROW



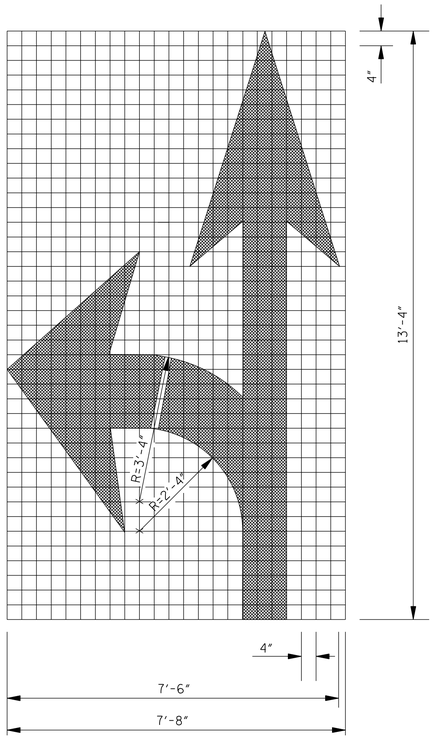
THRU ARROW



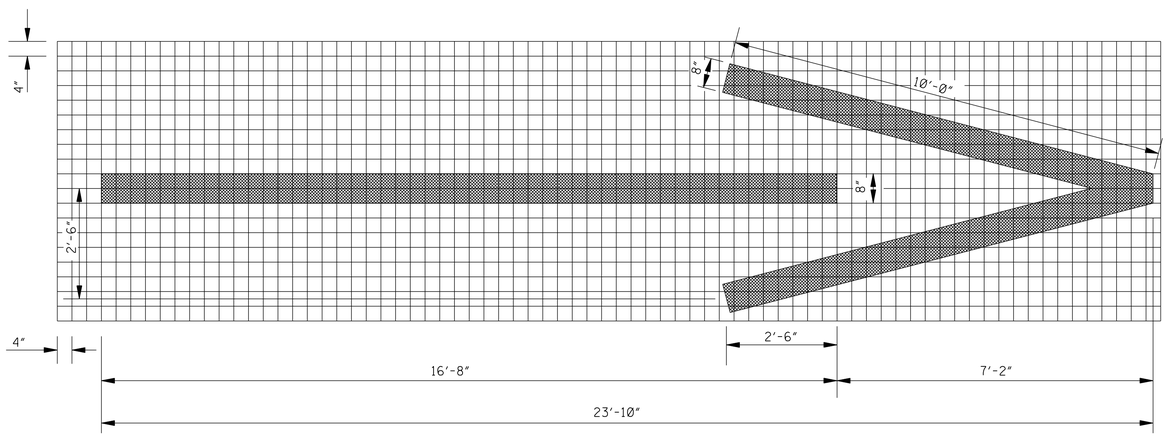
YIELD LINE



LANE–REDUCTION ARROW



COMBINATION ARROW



1–WAY ARROW

GENERAL NOTES:

1. TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTORS) OF 1½" OR LESS AND EXTENDING THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
2. FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
3. DIMENSIONS OF THE YIELD LINE MAY VARY WITH APPROVAL OF THE ENGINEER. SEE MUTCD, LATEST EDITION, FOR ALLOWABLE DIMENSIONS.
4. PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

PAY QUANTITIES	
LEGEND/SYMBOL	AREA (ft^2)
ONLY	22.0
TURN ARROW	16.4
THRU ARROW	12.3
COMB. ARROW	27.5
1-WAY ARROW	24.3
LANE REDUCTION ARROW	40.0

REVISION	BY	DATE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

PAVEMENT MARKING LEGEND DETAILS

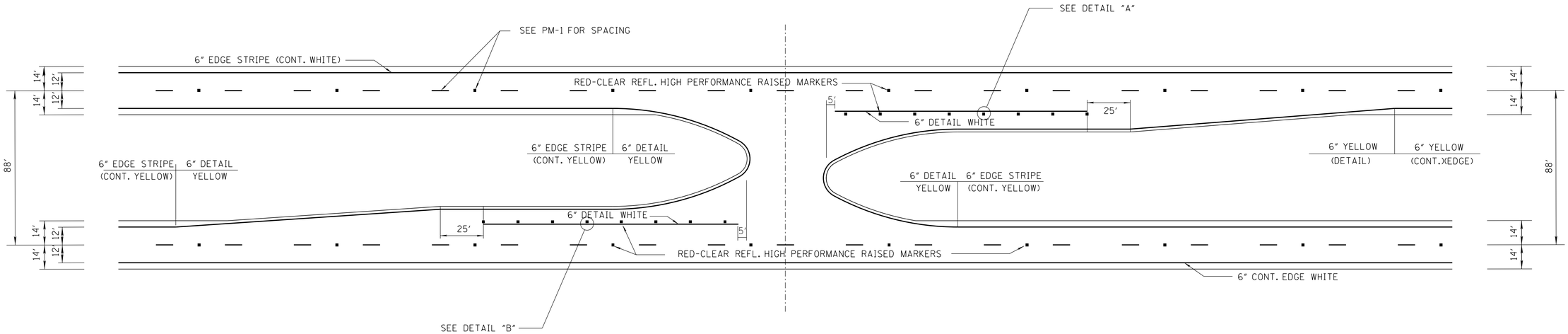
MDOT MISSISSIPPI DEPARTMENT OF TRANSPORTATION

WORKING NUMBER
PM-6

ISSUE DATE: AUGUST 01, 2017

SHEET NUMBER
6056

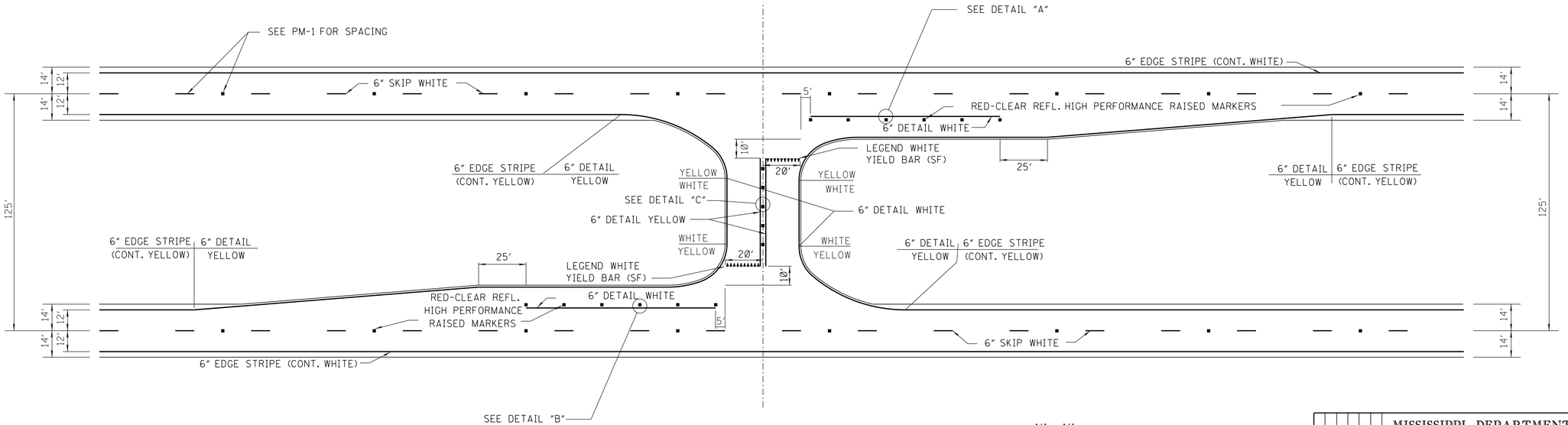
STATE	PROJECT NO.
MISS.	



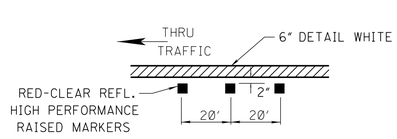
℄ TO ℄ ≤ 88'

NOTE: FOR WIDER ℄ TO ℄ SPACINGS,
REFER TO OTHER SHEETS IN PLANS

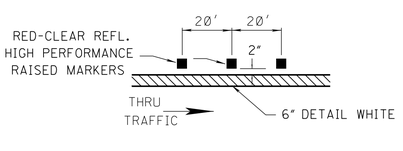
NOTE: SEE PM-6 FOR
YIELD BAR DETAILS



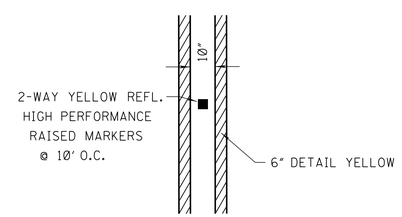
125' ≥ ℄ TO ℄ > 88'



DETAIL "A"



DETAIL "B"



DETAIL "C"

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TYPICAL PAVEMENT MARKING DETAIL FOR MEDIAN CROSSOVERS

BY	REVISION	DATE

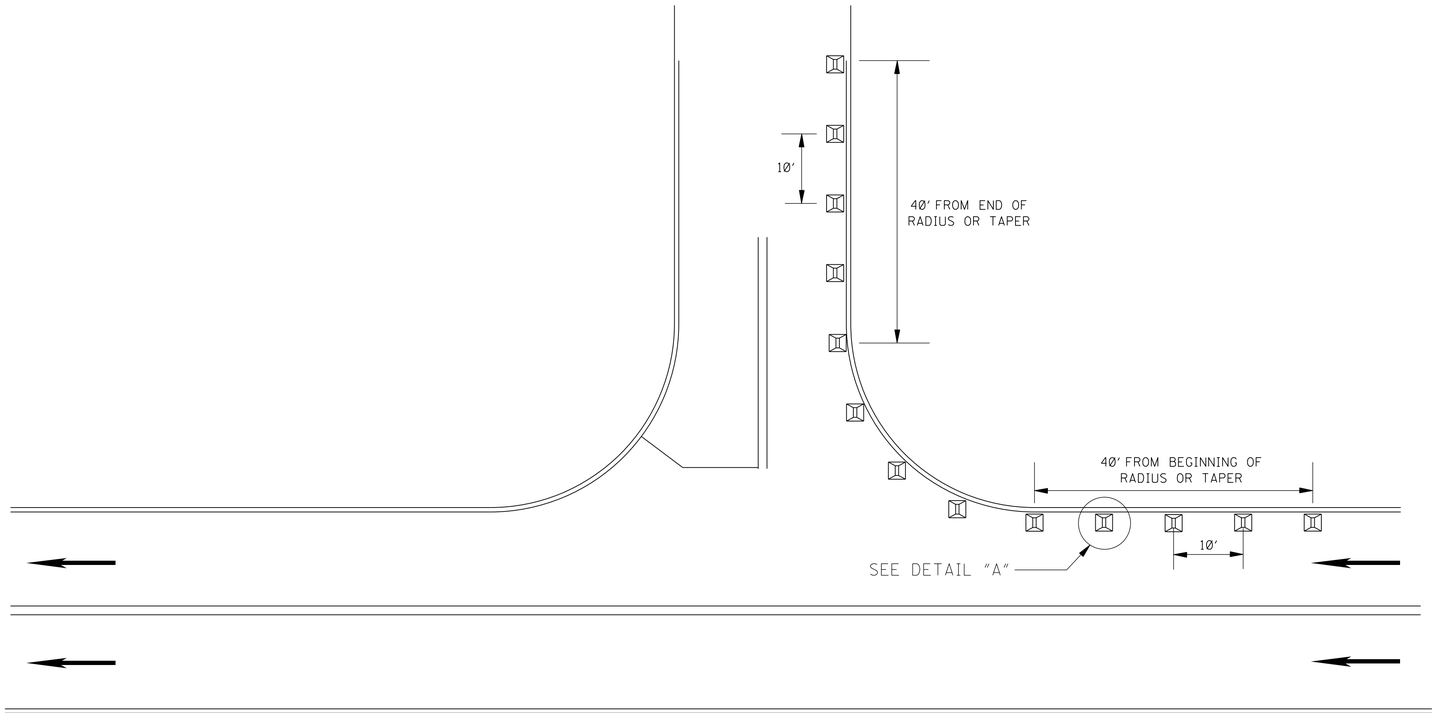
WORKING NUMBER PM-9

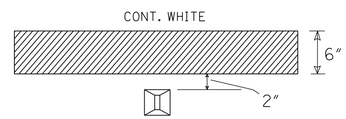
ISSUE DATE: AUGUST 01, 2017

SHEET NUMBER 6059

STATE	PROJECT NO.
MISS.	

TYPICAL PLACEMENT OF RAISED PAVEMENT MARKERS ON SIDE ROAD RADIUS 4–LANE, TWO WAY TRAFFIC





DETAIL A

GENERAL NOTES:

1. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
2. MARKERS SHALL BE HIGH PERFORMANCE TWO-WAY CLEAR.
3. MARKERS SHALL NOT BE ROTATED WHEN BEING PLACED ALONG RADIUS AND TANGENT SECTIONS OF LOCAL ROAD.
4. MARKERS SHALL BE INSTALLED AT SIMPLE AND CHANNELIZED INTERSECTIONS TO THE LIMITS SHOWN ABOVE.

→ DIRECTION OF TRAFFIC

BY	REVISION	DATE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

2–WAY RAISED PAVEMENT MARKERS AT INTERSECTING ROADS (4–LANE)

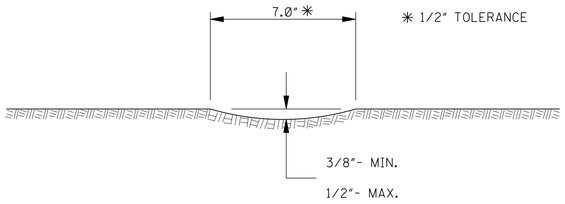
MDOT MISSISSIPPI DEPARTMENT OF TRANSPORTATION

WORKING NUMBER PM-12

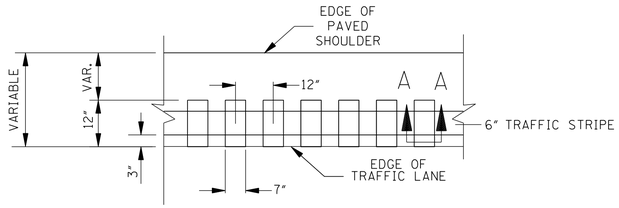
ISSUE DATE: AUGUST 01, 2017

SHEET NUMBER 6062

STATE	PROJECT NO.
MISS.	



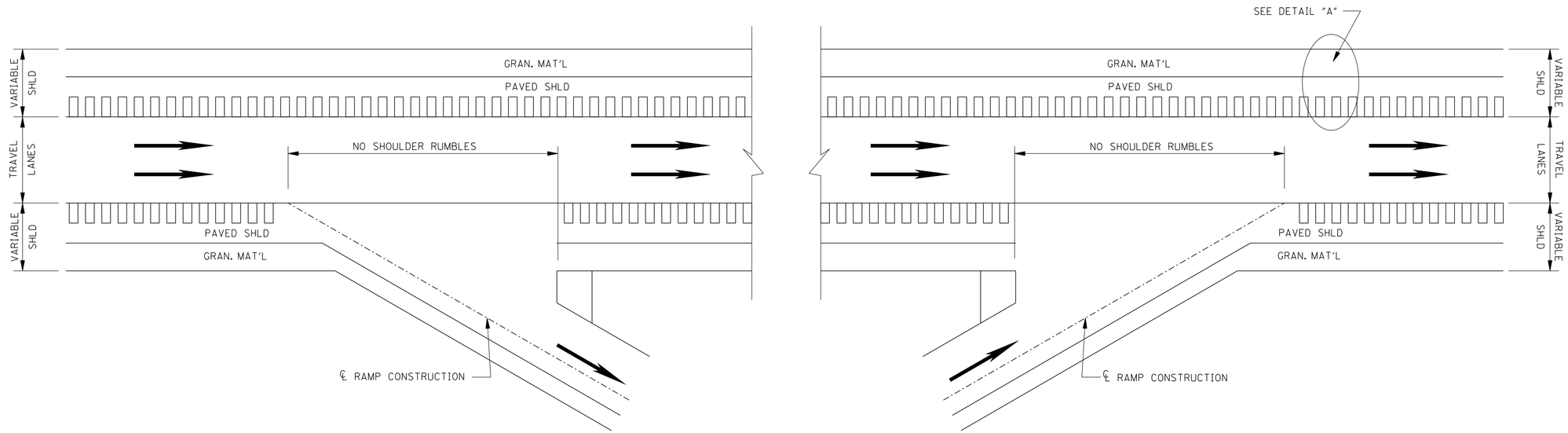
SECTION "A–A"



DETAIL "A"

GENERAL NOTES

1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
2. GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PUBLIC INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
4. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
   - A. MAINLINE
   - B. INTERSECTING ROADWAY IF OVERLAID OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.W.
   - C. ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.



PLAN

NOT TO SCALE

DETAILS OF RUMBLE STRIPS

DATE	REVISION	BY
ISSUE DATE: AUGUST 01, 2017		

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

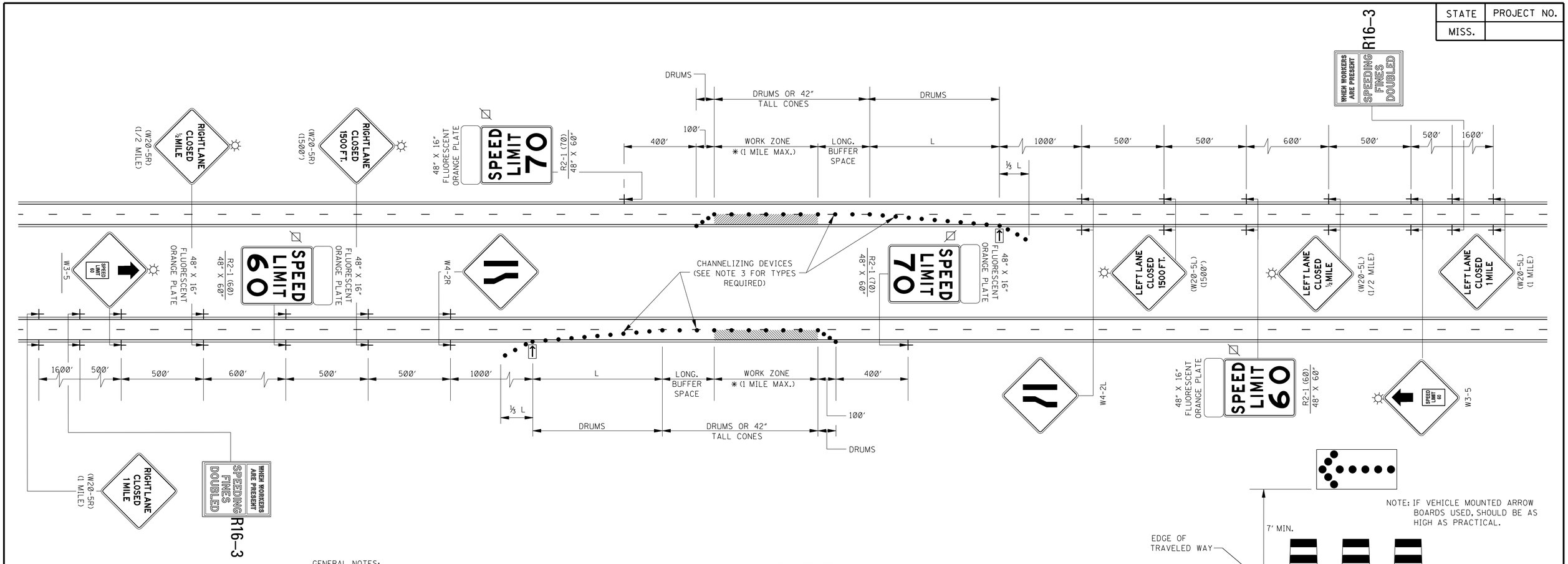
RUMBLE STRIPES 4–LANE HIGHWAYS (ASPHALT LANES, 2–FT OR WIDER ASPHALT SHOULDERS)

MDOT MISSISSIPPI DEPARTMENT OF TRANSPORTATION

WORKING NUMBER RS-2

SHEET NUMBER 6065

STATE	PROJECT NO.
MISS.	



GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		†† LONGITUDINAL BUFFER SPACE (ft)	TAPER† RATES
	TAPER	ALONG BUFFER SPACE & WORK ZONE		
mph				
≤40	40	80	305	27:1
45	45	90	360	45:1
50	50	100	425	50:1
55	55	110	495	55:1
60	60	120	570	60:1
65	65	130	645	65:1
70	70	140	730	70:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
L = WS FOR SPEEDS OF 45 mph OR GREATER
$L = WS^2/60$ FOR SPEEDS OF 40 mph OR LESS
WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

†† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.

3. CHANNELIZING DEVICES:
   A. ALL CHANNELIZING DEVICES IN TAPERS SHALL BE RETROREFLECTIVE FREE STANDING PLASTIC DRUMS.
   B. CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER RETROREFLECTIVE FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.
   C. ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE.
   D. RETROREFLECTORIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE M.U.T.C.D.

4. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48". AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.

5. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON FACE OF SIGN.

6. ADDITIONAL REDUCED REGULATORY SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. TWO (2) WILL BE REQUIRED FOR EACH RAMP AND LOCATION WILL BE DETERMINED BY THE ENGINEER.

7. THIS TRAFFIC CONTROL PLAN, WITH SPEED ZONE, MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.

8. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.

9. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS REQUIRED FOR LANE CLOSURE.

10. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

LEGEND

- * OR AS SHOWN ELSEWHERE ON THE PLANS.
- FLASHING ARROW PANEL (TYPE "C")
- BLACK LEGEND AND BORDER ON WHITE BACKGROUND
- TYPE "B" WARNING LIGHTS
- RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS

DATE	REVISION	BY

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH

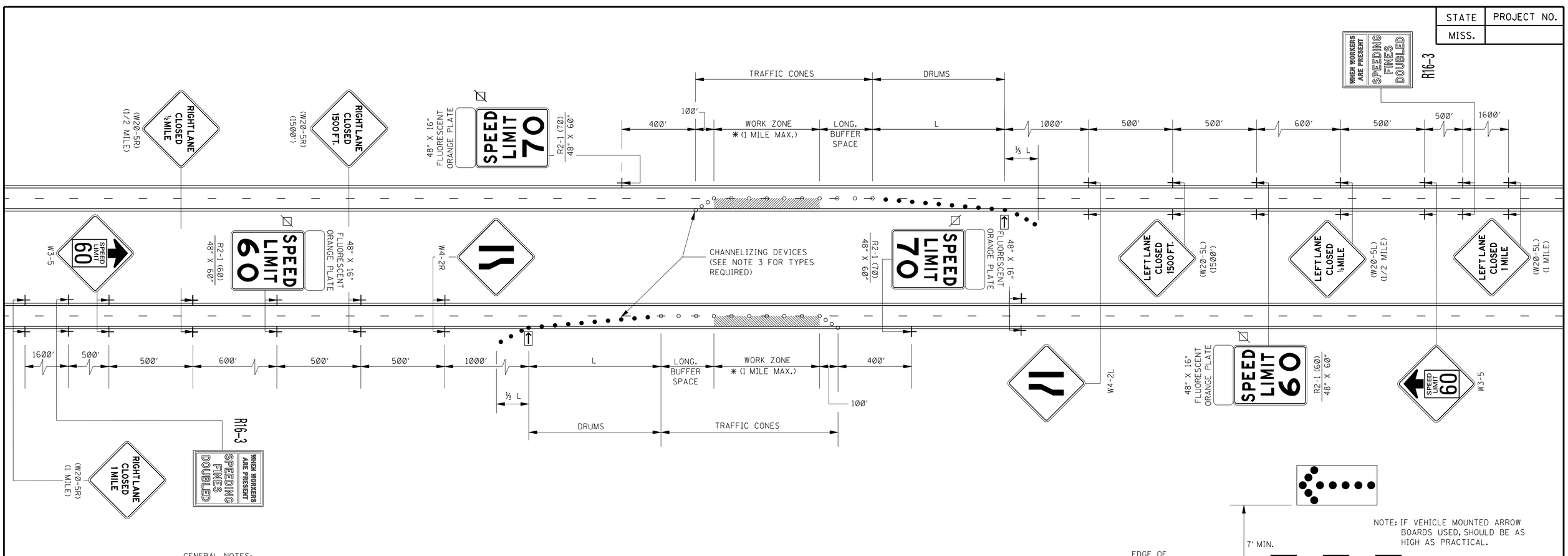
(INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (EXTENDED PERIOD)

ISSUE DATE: AUGUST 01, 2017

WORKING NUMBER TCP-4

SHEET NUMBER 6354

STATE	PROJECT NO.
MISS.	



GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		†† LONGITUDINAL BUFFER SPACE (ft)	TAPER† RATES
	TAPER	ALONG LANE LINE & WORK ZONE		
mph				
≤40	40	80	305	27:1
45	45	90	360	45:1
50	50	100	425	50:1
55	55	110	495	55:1
60	60	120	570	60:1
65	65	130	645	65:1
70	70	140	730	70:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
L = WS FOR SPEEDS OF 45 mph OR GREATER
L = $WS^2/60$ FOR SPEEDS OF 40 mph OR LESS
WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

†† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.

3. CHANNELIZING DEVICE TYPES FOR:
   A. APPROACH TAPER- RETROREFLECTIVE PLASTIC DRUMS
   B. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT MINIMUM)
   C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT MINIMUM)

4. WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.

5. FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING AND REGULATORY SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.

6. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48". AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.

7. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON THE FACE OF SIGN.

8. ADDITIONAL REDUCED REGULATORY SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. TWO (2) WILL BE REQUIRED FOR EACH RAMP AND LOCATION WILL BE DETERMINED BY THE ENGINEER.

9. THIS TRAFFIC CONTROL PLAN, WITH SPEED ZONE, MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.

10. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.

11. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS AND "REDUCED SPEED AHEAD" SIGNS REQUIRED FOR LANE CLOSURE.

12. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

LEGEND

* OR AS SHOWN ELSEWHERE ON THE PLANS.
FLASHING ARROW PANEL (TYPE "C")
BLACK LEGEND AND BORDER ON WHITE BACKGROUND
RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
TRAFFIC CONES (28" HEIGHT)

BY	REVISION	DATE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH

(INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (WORK DAY ONLY)

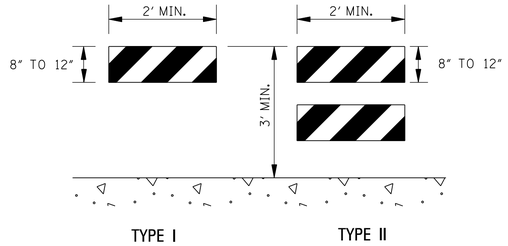
MDOT MISSISSIPPI DEPARTMENT OF TRANSPORTATION

WORKING NUMBER TCP-5

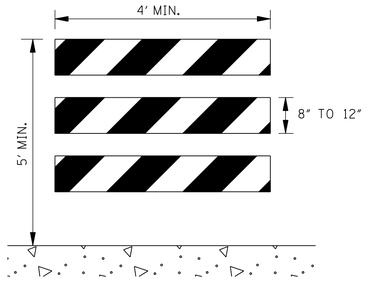
ISSUE DATE: AUGUST 01, 2017

SHEET NUMBER 6355

STATE	PROJECT NO.
MISS.	



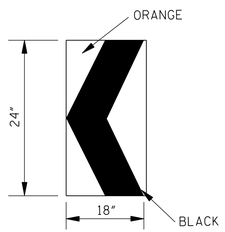
TYPE I TYPE II



TYPE III

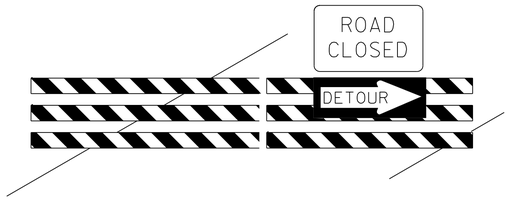
STANDARD BARRICADES

1. THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).
2. RAIL STRIPE SHOULD BE 6 INCHES, EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.
3. DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
4. FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.
5. BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES WHICH REQUIRE CRASHWORTHINESS ACCEPTANCE LETTERS. TO DATE, 2-IN. THICK TIMBER RAILS HAVE NOT BEEN SUCCESSFULLY CRASH TESTED. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATAGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE: http://safety.fhwa.dot.gov/roadway_dept/policy.guide/road_hardware/cat2.cfm



CHEVRON SIGN DETAIL

1. A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
2. THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.
3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.



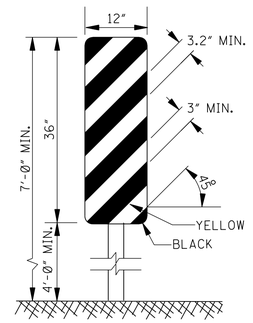
BARRICADE CLOSING A ROAD

BARRICADE CHARACTERISTICS

	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF RETROREFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS

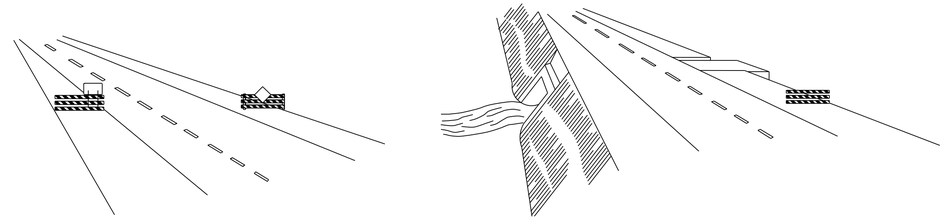
* 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.

** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 in^2 OF REFLECTIVE AREA FACING TRAFFIC.



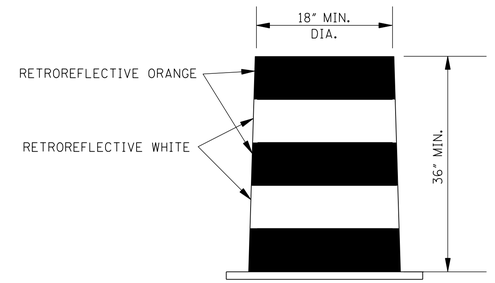
TYPE 3 OBJECT MARKER (OM-3R)

1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
2. THE OM-3R IS SHOWN. THE OM-3L IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
3. THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



WING BARRICADES

1. WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
2. WING BARRICADES SHOULD BE USED:
   A. IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
   B. IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.



PLASTIC DRUM STRIPING DETAIL

1. PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT COLOR ON DRUMS SHALL BE ORANGE WITH FOUR (4) RETROREFLECTIVE, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
2. DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
3. WHERE PRACTICAL PLASTIC DRUMS SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

BY	REVISION	DATE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS

ISSUE DATE: AUGUST 01, 2017

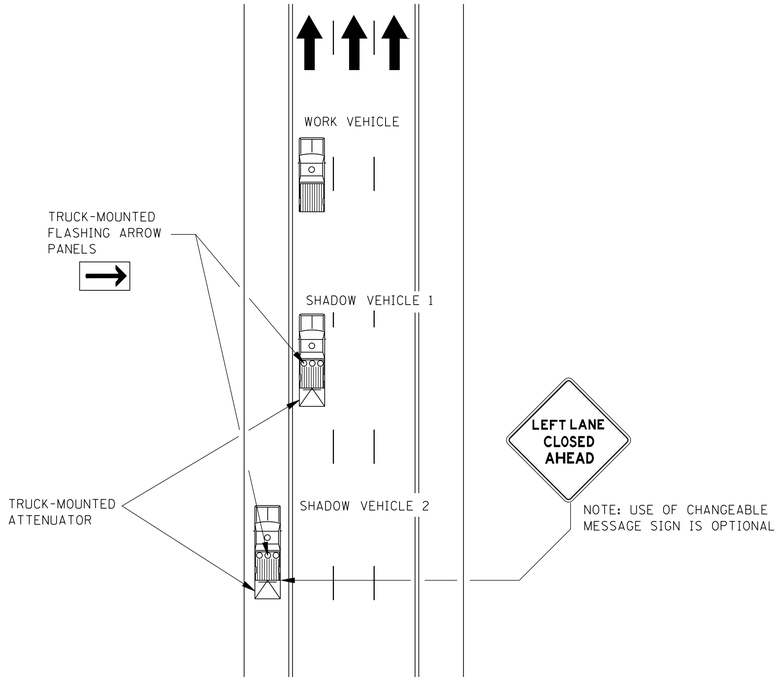
MDOT

WORKING NUMBER
TCP-8

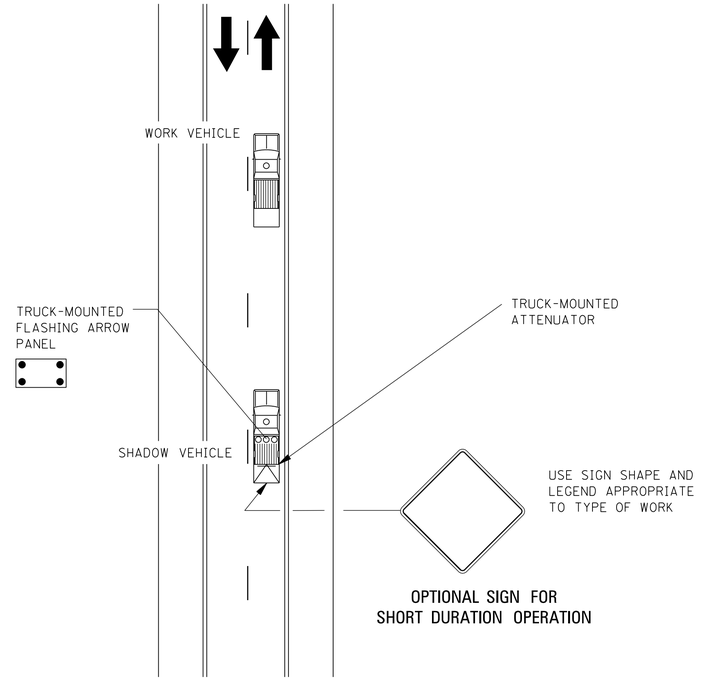
SHEET NUMBER
6358

STATE	PROJECT NO.
MISS.	

MOBILE OPERATIONS ON MULTILANE ROAD



MOBILE OPERATIONS ON TWO–LANE ROAD



MOBILE OPERATIONS ON MULTILANE ROAD

NOTES FOR MULTILANE LANE OPERATION:

1. VEHICLES USED FOR THESE OPERATIONS SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT, SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLAGS, SIGNS, OR ARROW PANELS.
2. SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK MOUNTED ATTENUATOR (TMA). AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON SHADOW VEHICLE 2 SO AS NOT TO OBSCURE THE ARROW PANEL.
3. SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK-MOUNTED ATTENUATOR (TMA).
4. SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
5. WHEN ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, SHADOW VEHICLE 2 SHOULD BE ELIMINATED.
6. ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE SHOULD BE USED (i.e., VEHICLE 3 ON THE SHOULDER (IF PRACTICAL), VEHICLE 2 IN THE CLOSED LANE, AND VEHICLE 1 IN THE CLOSED LANE).
7. ARROW PANELS SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
8. WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
9. VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
10. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MOBILE OPERATIONS ON TWO–LANE ROAD

NOTES FOR TWO-LANE OPERATION:

1. WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY, AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
2. THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. SHADOW VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, THE SHADOW VEHICLE SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
3. ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
4. A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
5. THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW VEHICLES SHALL BE EQUIPPED WITH TWO HIGH-INTENSITY FLASHING LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN. SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
6. VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
7. ARROW BOARD TO BE USED IN CAUTION MODE.
8. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

DATE	REVISION	BY

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

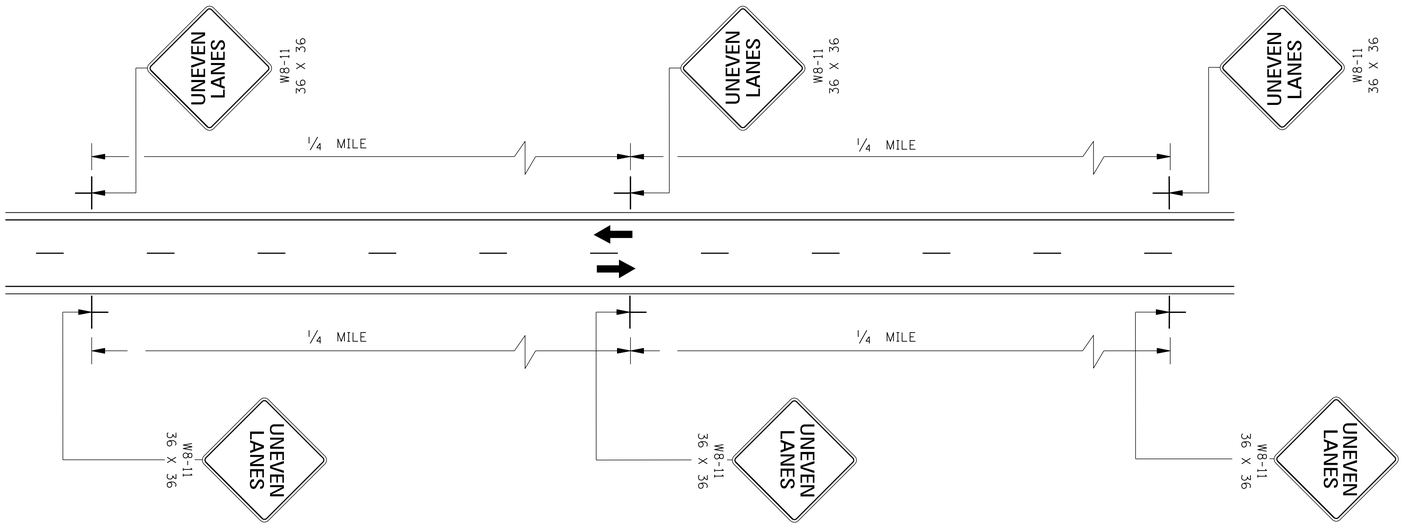
TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO–LANE ROADS

ISSUE DATE: AUGUST 01, 2017

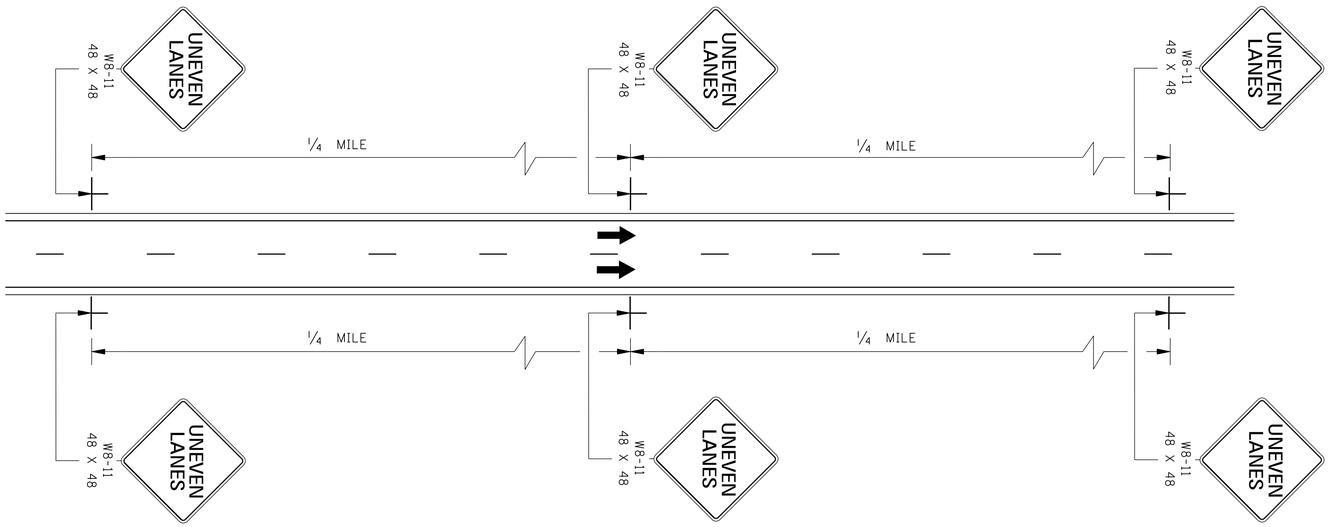
WORKING NUMBER
TCP-9

SHEET NUMBER
6359

STATE	PROJECT NO.
MISS.	



TWO–WAY TRAFFIC



ONE–WAY TRAFFIC

GENERAL NOTES:

1. UNEVEN LANE LINE:
   A. IF LESS THAN OR EQUAL TO 1½", NO SIGNS REQUIRED.
   B. IF GREATER THAN 1½" AND LESS THAN OR EQUAL TO 2¼", PLACE SIGNS AS SHOWN ON THIS SHEET.
   C. IF GREATER THAN 2¼", TRAFFIC SHOULD NOT BE ALLOWED TO CROSS UNEVEN LANE LINE.
2. THE W8-11 SIGNS SHOULD BE SPACED AT ¼-MILE INTERVALS THROUGHOUT UNEVEN LANE LINE LIMITS.
3. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

DATE	REVISION	BY

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

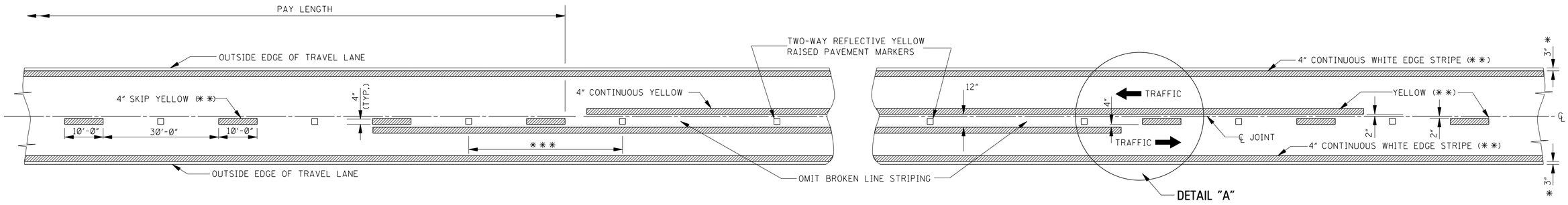
TRAFFIC CONTROL PLANS
UNEVEN PAVEMENT DETAILS

ISSUE DATE: AUGUST 01, 2017

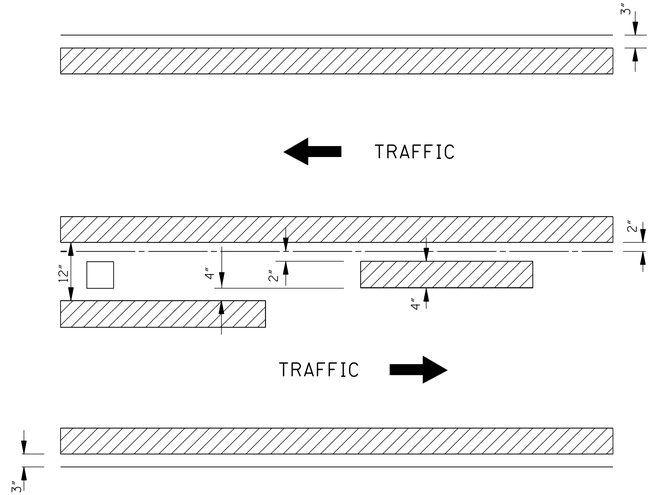
WORKING NUMBER
TCP-12

SHEET NUMBER
6362

STATE	PROJECT NO.
MISS.	



TWO–WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



DETAIL "A"

GENERAL NOTES:

* 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.

** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).

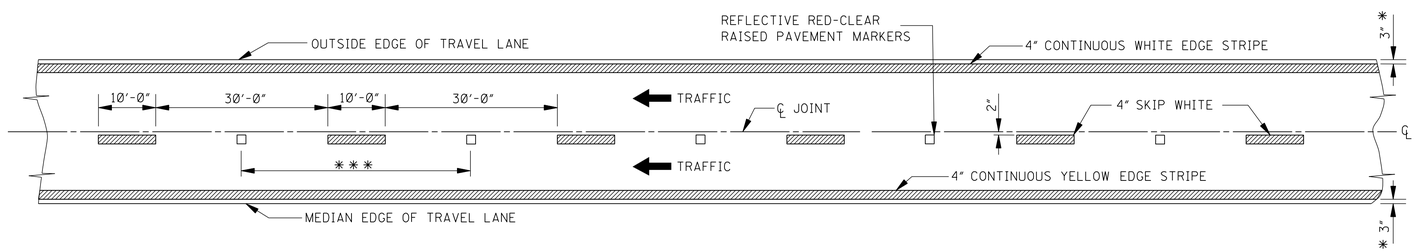
3. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS

*** 4. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

	URBAN AREA (ft-in)	RURAL AREA (ft-in)
TANGENT SECTIONS	40'-0"	80'-0"
HORIZONTAL CURVES	40'-0"	40'-0"
INTERCHANGE LIMITS	40'-0"	† 40'-0"

† NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

5. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED SOURCES OF MATERIALS."



4–LANE WITH ONE–WAY TRAFFIC

BY	REVISION	DATE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TEMPORARY STRIPING FOR TRAFFIC CONTROL 2–LANE AND 4–LANE DIVIDED HIGHWAYS

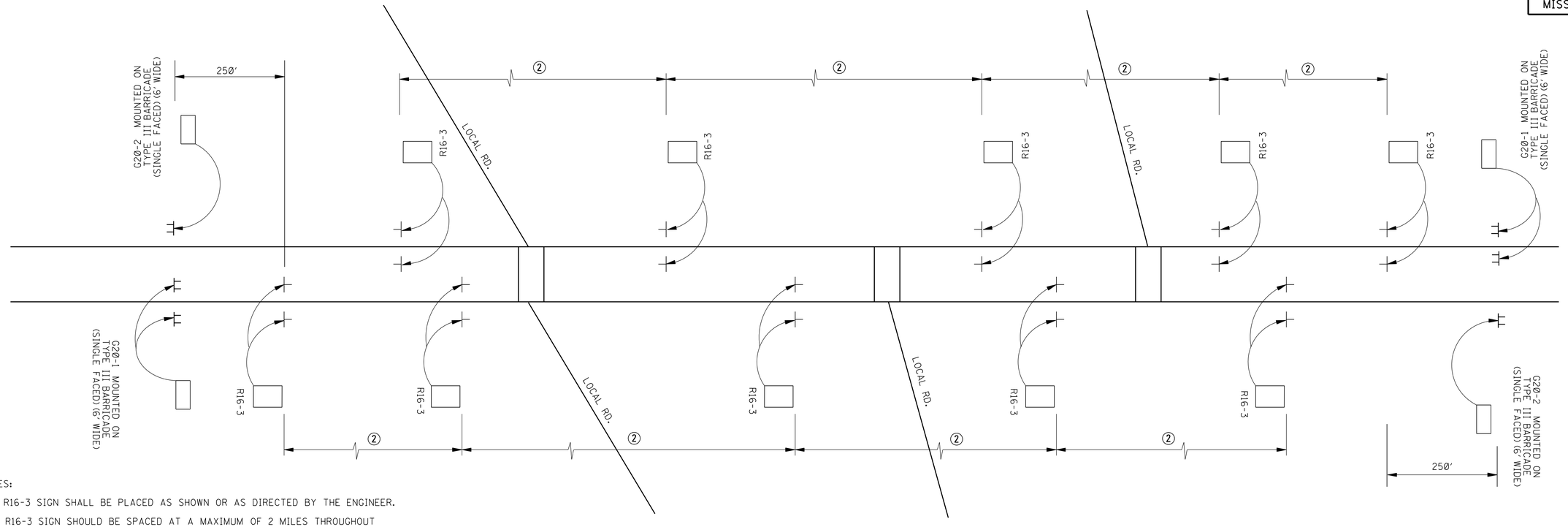
MDOT MISSISSIPPI DEPARTMENT OF TRANSPORTATION

WORKING NUMBER TCP-13

ISSUE DATE: AUGUST 01, 2017

SHEET NUMBER 6363

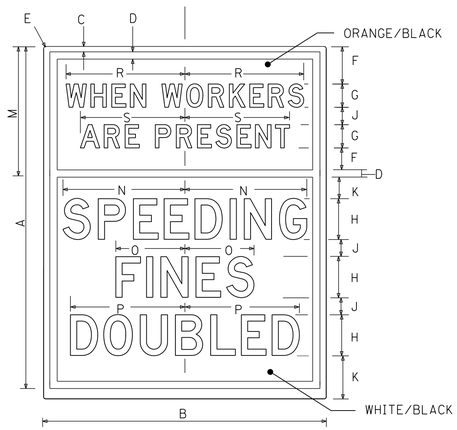
STATE	PROJECT NO.
MISS.	



NOTES:

1. R16-3 SIGN SHALL BE PLACED AS SHOWN OR AS DIRECTED BY THE ENGINEER.
2. R16-3 SIGN SHOULD BE SPACED AT A MAXIMUM OF 2 MILES THROUGHOUT LENGTH OF PROJECT.
3. THIS SHEET WILL ONLY APPLY TO SPEED REDUCTION SECTIONS.

DIVIDED HIGHWAY SHOWN
(2 LANE – 2 WAY ROADWAY SIMILAR)
(PROJECT MORE THAN 1 MILE LENGTH)



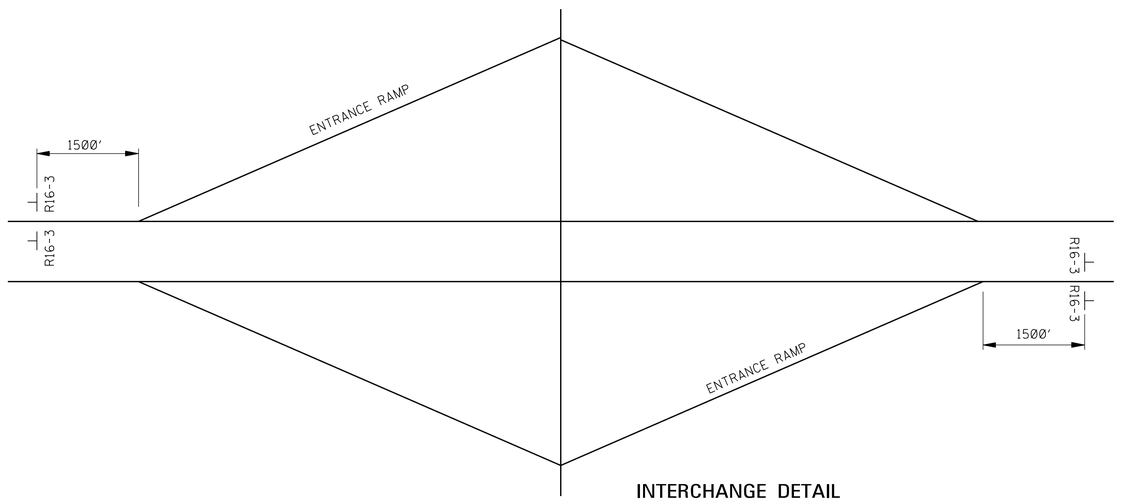
SIGN	DIMENSIONS (INCHES)							
	A	B	C	D	E	F	G	H
STD.	60	48	3/4	1 1/4	3	3 3/4	4 Dm	7 D
	J	K	M	N	O	P	R	S
STD.	3	6 5/8	22 1/8	21	11 7/8	19 23/32	20 5/32	18

48" x 60"
(INTERSTATE USE)

SIGN	DIMENSIONS (INCHES)							
	A	B	C	D	E	F	G	H
STD.	48	36	3/4	1 1/4	3	2 3/4	3 Dm	6 D
	J	K	M	N	O	P	R	S
STD.	3	4 1/8	14 3/4	14	7 7/8	13 1/8	13 5/8	12

36" x 48"
(ALL OTHER HIGHWAYS)

R16–3



INTERCHANGE DETAIL

BY	REVISION	DATE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

LOCATION OF R16–3 SIGNS (SPEEDING FINES DOUBLED)

MDOT

WORKING NUMBER
TCP-15

ISSUE DATE: AUGUST 01, 2017

SHEET NUMBER
6365