

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u> 1 </u>	DATED <u> 1/23/2020 </u>	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. _____	DATED _____	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. _____	DATED _____	ADDENDUM NO. _____	DATED _____

Number Description

- 1 Revised Table of Contents; Revised NTB Nos. 2066, & 2068; Added NTB No. 2172; NTB No. 2206 replaces NTB No. 1206; NTB No. 2214 replaces NTB No. 7 with Supplement; Revised Wage Rates; Revised or Added Plan Sheet Nos. 8001-8002, 8080, & 8147; Amendment EBSx Download Required.

TOTAL ADDENDA: 1
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

President Address

Secretary Address

Treasurer Address

The following is my (our) itemized proposal.
NHAPD-0078-01(011)N/ 102076301000
Itawamba County(ies)

Revised 01/26/2016

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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01/23/2020 04:46 PM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2066

CODE (SP)

DATE: 12/09/2019

SUBJECT: Placement of Fill Material in Federally Regulated Areas

PROJECT: NH-APD-0078-01(011)N / 102076301 - Itawamba County

A Permit (404, General, Nationwide, etc.) for placing fill material in federally regulated sites is required on this project.

The Department has acquired the following permits for permanently filling at regulated sites that are identified during project development:

General Permit No. 46 (Wetlands & Waters of the U.S.) (ID No. SAM-2019-1097)	All Sites
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Copies of said permit(s) are available at the below referenced link for the appropriate letting date under the column titled "Permit Doc."

<http://mdot.ms.gov/Applications/BidSystem/Home.aspx>

The permit may have plan sheets attached as reference but these sheets are not to be used for construction. Official plans sheets are those included in the Project Plans.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2068

CODE: (SP)

DATE: 01/17/2020

SUBJECT: Disturbed Area

PROJECT: NH-APD-0078-01(011)N / 102076301 – Itawamba County

Bidders are advised of Notice to Bidders No. 757 and Special Provision No. 907-107-2 that limit the maximum total acreage that can be disturbed at one time to 19 acres. However, if the Contractor provides a schedule for earthwork activity by means of a haul-mass diagram, narrative description of the phasing for the haul-mass diagram (including proposed acreages for each phase that includes anticipated locations for multiple crews) and any proposed commitments to help maintain embankment stability during each phase, the expansion of the 19-acre limit will be considered for a maximum disturbed acreage of no more than seventy-five (75) acres. A written request with supporting documentation shall be submitted to the Project Engineer who will review the request before submitting it to Construction Division for approval. MDOT reserves the right to decrease the acreage requested by the Contractor based on the Contractor's erosion control performance and the complexity of the project.

Time associated with the submittal, review, and approval/denial of this change will run concurrent with the 60 days set aside in Special Provision No. 907-107-2 for the approval of the Contractor's Erosion Control Plan.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2172

CODE: (SP)

DATE: 01/06/2020

SUBJECT: App for Storm Water Reports

Bidders are advised that the Department has created a smart phone App for completing and submitting storm water reports required on this project. The Contractor who monitors storm water activities and completes storm water reports will be required to download and use this App when completing and submitting storm water reports. The reports will then be readily available to all persons who need access to the forms. The App is free and is available for downloading at the following location.

<https://extacctmgmt.mdot.state.ms.us/>

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2206

CODE: (IS)

DATE: 01/14/2020

SUBJECT: MASH Compliant Devices

Bidders are hereby advised that compliance associated with the requirements of meeting either the National Cooperative Highway Research Program (NCHRP) Report 350 or the Manual for Assessing Safety Hardware (MASH) for installations of certain traffic control devices and permanent safety hardware devices (guardrails, guardrail terminals, permanent portable barriers, cast-in-place barriers, all other permanent longitudinal barriers, crash cushions, cable barriers, cable barrier terminals, bridge rails, bridge rail transitions, all other terminals, sign supports, and all other breakaway hardware) as listed throughout the Standard Specifications and/or the Standard Drawings, or both, is now replaced with the requirements of meeting the 2016 version of MASH after December 31, 2019. This change applies to new permanent installations and to full replacements of existing installations.

At the preconstruction conference or prior to starting any work on the project, the Contractor shall submit a letter stating that the traffic control devices and permanent safety hardware devices as outlined within the paragraph above that are to be used on the project are certified to meet MASH 2016.

When a MASH 2016-compliant device does not exist for the new permanent installations and/or full replacement installations of permanent safety hardware devices, as listed above, a MASH 2009-compliant or a NCHRP 350-compliant device may be proposed by the Contractor for the project. A written request for such instances must be submitted by the Contractor either at the preconstruction conference or prior to starting any work on the project. The Contractor shall submit the following items to the Project Engineer: (1) a detailed list of the proposed devices and locations thereof; and (2) certification letters indicating that the proposed devices are compliant with either MASH 2009 or NCHRP 350.

When a MASH 2016-compliant device does not exist for the temporary work zone traffic control devices (Category 1, Category 2, and Category 3 devices), a MASH 2009-compliant or a NCHRP 350-compliant device may be proposed by the Contractor for the project. Temporary work zone traffic control devices (Category 1, Category 2, and Category 3 devices) that are MASH 2009-compliant or NCHRP 350-compliant that have been in use prior to December 31, 2019, and that have a remaining service life may be proposed for use throughout their normal service life on the project by the Contractor. For either of these scenarios for temporary work zone traffic control devices, a written request must be submitted by the Contractor either at the preconstruction conference or prior to starting any work on the project. The Contractor shall submit the following items to the Project Engineer: (1) a detailed list of the proposed devices and locations thereof; and (2) certification letters indicating that the proposed devices are compliant with either MASH 2009 or NCHRP 350.

Work will only be allowed to proceed after the Department has granted written concurrence(s) with the proposed request(s) as listed above.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SUPPLEMENT TO NOTICE TO BIDDERS NO. 2214

DATE: 01/22/2020

The goal is 5 percent for the Disadvantaged Business Enterprise. All Bidders are required to submit Form OCR-481 for all DBEs. Bidders are advised to check the bid tabulation link for this project on the MDOT website at:

<http://sp.gomdot.com/Contract%20Administration/BidSystems/Pages/letting%20calendar.aspx>

Bid tabulations are usually posted by 3:00 pm on Letting Day.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2214

CODE: (SP)

DATE: 01/22/2020

SUBJECT: Disadvantaged Business Enterprises In Federal-Aid Highway Construction

This contract is subject to the "Moving Ahead for Progress in the 21st Century Act (MAP-21)" and applicable requirements of "Part 26, Title 49, Code of Federal Regulations". Portions of the Act are set forth in this Notice as applicable to compliance by the Contractor and all of the Act, and the MDOT DBE Program, is incorporated by reference herein.

The Department has developed a Disadvantaged Business Enterprise Program that is applicable to this contract and is made a part thereof by reference.

Copies of the program may be obtained from:

Office of Civil Rights
Mississippi Department of Transportation
P. O. Box 1850
Jackson, Mississippi 39215-1850

POLICY

It is the policy of the Mississippi Department of Transportation to provide a level playing field, to foster equal opportunity in all federally assisted contracts, to improve the flexibility of the DBE Program, to reduce the burdens on small businesses, and to achieve that amount of participation that would be obtained in a non-discriminatory market place. In doing so, it is the policy of MDOT that there will be no discrimination in the award and performance of federally assisted contracts on the basis of race, color, sex, age, religion, national origin, or any handicap.

ASSURANCES THAT CONTRACTORS MUST TAKE

MDOT will require that each contract which MDOT signs with a sub-recipient or a Contractor, and each subcontract the Prime Contractor signs with a Subcontractor, includes the following assurances:

“The Contractor, sub-recipient or Subcontractor shall not discriminate on the basis of race, color, national origin, or sex in the performance of this contract. The Contractor shall carry out applicable requirements of 49 CFR 26 in the award and administration of federally assisted contracts. Failure by the Contractor to carry out these requirements is a material breach of this contract, which may result in the termination of this contract or such other remedy as MDOT deems appropriate.”

DEFINITIONS

For purposes of this provision the following definitions will apply:

"Disadvantaged Business" means a small business concern: (a) which is at least 51 percent owned by one or more socially and economically disadvantaged individual(s) or in the case of any publicly owned business, at least 51 percent of the stock of which is owned by one or more socially and economically disadvantaged individual(s); and (b) whose management and daily business operations are controlled by one or more of the socially and economically disadvantaged individual(s) who own it. It is important to note that the business owners themselves must control the operations of the business. Absentee ownership or title ownership by an individual who does not take an active role in controlling the business is not consistent with eligibility as a DBE under CFR 49 Part 26.71.

CONTRACTOR'S OBLIGATION

The Contractor and all Subcontractors shall take all necessary and reasonable steps to ensure that DBE firms can compete for and participate in the performance of a portion of the work in this contract and shall not discriminate on the basis of race, color, national origin, religion or sex. Failure on the part of the Contractor to carry out the DBE requirements of this contract constitutes a breach of contract and after proper notification the Department may terminate the contract or take other appropriate action as determined by the Department.

When a contract requires a zero percent (0%) DBE goal, the Contractor still has the responsibility to take all necessary and reasonable steps to ensure that DBE firms can compete for and participate in the performance of the work in the contract. In this case, all work performed by a certified DBE firm is considered to be a "race neutral" measure and the Department will receive DBE credit towards the overall State goals when the DBE firm is paid for their work. If the Prime Contractor is a certified DBE firm, the Department can receive DBE credit only for the work performed by the Prime Contractor's work force or any work subcontracted to another DBE firm. Work performance by a non-DBE Subcontractor is not eligible for DBE credit.

CONTRACT GOAL

The goal for participation by DBEs is established for this contract in the attached Supplement. The Contractor shall exercise all necessary and reasonable steps to ensure that participation is equal to or exceeds the contract goal.

If the percentage of the contract that is proposed for DBEs is 1% or greater, the Contractor shall agree to meet or exceed the contract goal on the last bid sheet of the proposal.

All Bidders shall submit to the Office of Civil Rights Form OCR-481, signed by the Prime Contractor and the DBE Subcontractors, no later than the 3rd business day after opening of the bids.

Form OCR-481 is available on the MDOT website at GoMDOT.com, then Divisions, Civil Rights, Forms, DBE, MDOT Projects, or by calling 601-359-7466.

The OCR-481 Form must contain the following information:

The name and address of each certified DBE Contractor / Supplier;

The Reference Number, percent of work and the dollar amount of each item. If a portion of an item is subcontracted, a breakdown of that item including quantities and unit price must be attached, detailing what part of the item the DBE firm is to perform and who will perform the remainder of the item.

If the DBE Commitment shown on the last bid sheet of the proposal, does not equal or exceed the contract goal, the bidder must submit, to MDOT Contract Administration Division prior to bid opening, information to satisfy the Department that adequate good faith efforts have been made to meet the contract goal.

Failure of the lowest bidder to furnish acceptable proof of good faith efforts, submitted to MDOT Contract Administration Division prior to bid opening, shall be just cause for rejection of the proposal. Award may then be made to the next lowest responsive bidder or the work may be re-advertised.

The following factors are illustrative of matters the Department will consider in judging whether or not the bidder has made adequate good faith effort to satisfy the contract goal.

- (1) Whether the bidder attended the pre-bid meeting that was scheduled by the Department to inform DBEs of subcontracting opportunities;
- (2) Whether the bidder advertised in general circulation, trade association, and minority-focus media concerning the subcontracting opportunities;
- (3) Whether the bidder provided written notice to a reasonable number of specific DBEs that their interest in the contract is being solicited;
- (4) Whether the bidder followed up initial solicitations of interest by contacting DBEs to determine with certainty whether they were interested;
- (5) Whether the bidder selected portions of the work to be performed by DBEs in order to increase the likelihood of meeting the contract goal;
- (6) Whether the bidder provided interested DBEs with adequate information about the plans, specifications and requirements of the contract;
- (7) Whether the bidder negotiated in good faith with interested DBEs and did not reject them as unqualified without sound reasons based on a thorough investigation of their capabilities; and
- (8) Whether the bidder made efforts to assist interested DBEs in obtaining any required bonding or insurance.
- (9) Whether the bidder has written notification to certified DBE Contractors soliciting subcontracting for items of work in the contract.
- (10) Whether the bidder has a statement of why an agreement was not reached.

The bidder's execution of the signature portion of the proposal shall constitute execution of the following assurance:

The bidder hereby gives assurance pursuant to the applicable requirements of "Moving Ahead for Progress in the 21st Century Act (MAP-21)" and applicable requirements of "Part 26, Title 49, Code of Federal Regulations" that the bidder has made a good faith effort to meet the contract goal for DBE participation for which this proposal is submitted.

DIRECTORY

A list of "Certified DBE Contractors" which have been certified as such by the Mississippi Department of Transportation and other Unified Certification Partners (UPC) can be found on the Mississippi Department of Transportation website at www.gomdot.com. The list is in the top left corner of the current Letting Calendar under Contracts & Letting. The DBE firm must be certified at the time the project is let and approved by MDOT to count towards meeting the DBE goal.

REPLACEMENT

If a DBE Subcontractor cannot perform satisfactorily, and this causes the OCR-481 commitment to fall below the contract goal, the Contractor shall take all necessary reasonable steps to replace the DBE with another certified DBE Subcontractor or submit information to satisfy the Mississippi Department of Transportation that adequate good faith efforts have been made to replace the DBE. The replacement DBE must be a DBE who was on the Department's list of "Certified DBE Contractors" when the job was let, and who is still active. All DBE replacements must be approved by the Department.

Under no circumstances shall the Prime or any Subcontractor perform the DBE's work (as shown on the OCR-481) without prior written approval from the Department. See "Sanctions" at the end of this document for penalties for performing DBE's work.

When a Contractor proposes to substitute/replace/terminate a DBE that was originally named on the OCR-481, the Contractor must obtain a release, in writing, from the named DBE explaining why the DBE Subcontractor cannot perform the work. A copy of the original DBE's release must be attached to the Contractor's written request to substitute/replace/terminate along with appropriate Subcontract Forms for the substitute/replacement/terminated Subcontractor, all of which must be submitted to the DBE Coordinator and approved, in advance, by MDOT.

GOOD FAITH EFFORTS

To demonstrate good faith efforts to replace any DBE that is unable to perform successfully, the Contractor must document steps taken to subcontract with another certified DBE Contractor. Such documentation shall include no less than the following:

- (1) Proof of written notification to certified DBE Contractors by certified mail that their interest is solicited in subcontracting the work defaulted by the previous DBE or in subcontracting other items of work in the contract.

- (2) If the Prime Contractor is a certified DBE firm, only the value of the work actually performed by the DBE Prime can be counted towards the project goal, along with any work subcontracted to a certified DBE firm.
- (3) If the Contractor is not a DBE, the work subcontracted to a certified DBE Contractor will be counted toward the goal.
- (4) The Contractor may count toward the goal a portion of the total dollar value of a contract with a joint venture eligible under the standards of this provision equal to the percentage of the DBE partner in the joint venture.
- (5) Expenditures to DBEs that perform a commercially useful function may be counted toward the goal. A business is considered to perform a commercially useful function when it is responsible for the execution of a distinct element of the work and carries out its responsibilities by actually performing, managing, and supervising the work involved.
- (6) The Contractor may count 100% of the expenditures for materials and supplies obtained from certified DBE suppliers and manufacturers that produce goods from raw materials or substantially alters them for resale provided the suppliers and manufacturers assume the actual and contractual responsibility for the provision of the materials and supplies. The Contractor may count sixty percent (60%) of the expenditures to suppliers that are not manufacturers, provided the supplier performs a commercially useful function in the supply process. Within 30 days after receipt of the materials, the Contractor shall furnish to the DBE Coordinator invoices from the certified supplier to verify the DBE goal.
- (7) Any work that a certified DBE firm subcontracts or sub-subcontracts to a non-DBE firm will not count towards the DBE goal.
- (8) Only the dollars actually paid to the DBE firm may be counted towards the DBE goal.

Failure of the Contractor to demonstrate good faith efforts to replace a DBE Subcontractor that cannot perform as intended with another DBE Subcontractor, when required, shall be a breach of contract and may be just cause to be disqualified from further bidding for a period of up to 12 months after notification by certified mail.

PRE-BID MEETING

A pre-bid meeting will be held in [the Commission Room on the 1st Floor of the MDOT Administration Building in Jackson](#), at 2:00 P.M. on the day preceding the date of the bid opening.

This meeting is to inform DBE firms of subcontracting and material supply opportunities. Attendance at this meeting is considered of prime importance in demonstrating good faith effort to meet the contract goal.

PARTICIPATION / DBE CREDIT

Participation shall be counted toward meeting the goal in this contract as follows:

- (1) If the Prime Contractor is a certified DBE firm, only the value of the work actually performed by the DBE Prime can be counted towards the project goal, along with any work subcontracted to a certified DBE firm.
- (2) If the Contractor is not a DBE, the work subcontracted to a certified DBE Contractor will be counted toward the goal.
- (3) The Contractor may count toward the goal a portion of the total dollar value of a contract with a joint venture eligible under the standards of this provision equal to the percentage of the DBE partner in the joint venture.
- (4) Expenditures to DBEs that perform a commercially useful function may be counted toward the goal. A business is considered to perform a commercially useful function when it is responsible for the execution of a distinct element of the work and carries out its responsibilities by actually performing, managing, and supervising the work involved.
- (5) The Contractor may count 100% of the expenditures for materials and supplies obtained from certified DBE suppliers and manufacturers that produce goods from raw materials or substantially alters them for resale provided the suppliers and manufacturers assume the actual and contractual responsibility for the provision of the materials and supplies. The Contractor may count sixty percent (60%) of the expenditures to suppliers that are not manufacturers, provided the supplier performs a commercially useful function in the supply process. Within 30 days after receipt of the materials, the Contractor shall furnish to the DBE Coordinator invoices from the certified supplier to verify the DBE goal.
- (6) Any work that a certified DBE firm subcontracts or sub-subcontracts to a non-DBE firm will not count towards the DBE goal.
- (7) Only the dollars actually paid to the DBE firm may be counted towards the DBE goal. The participation of a DBE Firm cannot be counted towards the Prime Contractor’s DBE goal until the amount being counted towards the goal has been paid to the DBE.

AWARD

Award of this contract to the low bidder will be contingent upon the following conditions:

- (1) Concurrence from Federal Highway Administration, when applicable.
- (2) **All Bidders** must submit to the Office of Civil Rights for approval, Form OCR-481 (DBE Commitment) no later than the 3rd business day after opening of the bids to satisfy the Department and that adequate good faith efforts have been made to meet the contract goal. For answers to questions regarding Form OCR-481, contact the MDOT Office of Civil Rights at (601) 359-7466.
- (3) Bidder must include OCR-485 information with their bid proposal listing all firms that submitted quotes for material supplies or items to be subcontracted. OCR-485 information must be included with the bid proposal. If the OCR-485 information is not included as part of bid proposal, your bid will be deemed irregular.

Prior to the start of any work, the bidder must notify the Project Engineer, in writing, of the name of the designated "DBE Liaison Officer" for this project. This notification must be posted on the bulletin board at the project site.

DEFAULT

If the contract goal established by MDOT in this proposal is 1% or greater, it must be met to fulfill the terms of the contract. The Contractor may list DBE Subcontractors and items that exceed MDOT's contract goal, but should unforeseen problems arise that would prevent a DBE from completing its total commitment percentage, the Contractor will meet the terms of the contract as long as it meets or exceeds MDOT's Contract Goal. For additional information, refer to "Replacement" section of this Notice.

DBE REPORTS

- (1) OCR-481: Refer to "CONTRACT GOAL" section of this Notice to Bidders for information regarding this form.
- (2) OCR-482: At the conclusion of the project, before the final estimate is paid and the project is closed out, the Prime Contractor will submit to the Project Engineer for verification of quantities and further handling Form OCR-482 whereby the Contractor certifies to the amounts of payments made to all Contractors / Suppliers over the life of the contract. The Project Engineer shall submit the completed Form OCR-482 to the DBE Coordinator (Office of Civil Rights). Final acceptance of the project is dependent upon Contract Administration Division's receipt of completed Form OCR-482 which they will receive from the Office of Civil Rights.
- (3) OCR-483: The Project Engineer/Inspector will complete Form OCR-483, the Commercially Useful Function (CUF) Performance Report, in accordance with MDOT S.O.P. No. OCR-03-09-01-483. Evaluations reported on this form are used to determine whether or not the DBE firm is performing a CUF. The Prime Contractor should take corrective action when the report contains any negative evaluations. DBE credit may be disallowed and/or other sanctions imposed if it is determined the DBE firm is not performing a CUF. This form should also be completed and returned to the DBE Coordinator (Office of Civil Rights).
- (4) OCR-484: Each month, the Prime Contractor will submit to the Project Engineer OCR-484 that certifies payments to all Subcontractors and shows all firms even if the Prime Contractor has paid no monies to the firm during that estimate period (negative report). The Project Engineer will attach the form to the monthly estimate before forwarding to the Contract Administration Division for further processing. Failure of the Contractor to submit the OCR-484 will result in the estimate not being processed and paid.
- (5) OCR-485: ALL BIDDERS must submit signed form with bid proposal of all firms that submitted quotes for material supplies or items to be subcontracted. If the OCR-485 information is not included as part of bid proposal, the bid will be deemed irregular.

- (6) OCR-487: Only used by Prime Contractors that are certified DBE firms. This form is used in determining the exact percentage of DBE credit for the specified project. The low Bidder should return this form to MDOT with the OCR-481 form, or can also be returned with the Permission to Subcontract Forms (CAD-720, CAD-725 and CAD-521).

DBE Forms, can be obtained from the Office of Civil Rights Division, MDOT Administration Building, 401 North West Street, Jackson, MS, or at www.gomdot.com under Divisions, Civil Rights, and Forms.

SANCTIONS

The Department has the option to enforce any of the following penalties for failure of the Prime Contractor to fulfill the DBE goal as stated on the OCR-481 form or any violations of the DBE program guidelines:

- (1) Disallow credit towards the DBE goal
- (2) Withhold progress estimate payments
- (3) Deduct from the final estimate or recover an amount equal to the unmet portion of the DBE goal which may include additional monetary penalties as outlined below based on the number of offenses and the severity of the violation as determined by MDOT.

1 st Offense	10% of unmet portion of goal	or	\$5,000 lump sum payment	or	Both
2 nd Offense	20% of unmet portion of goal	or	\$10,000 lump sum payment	or	Both
3 rd Offense	40% of unmet portion of goal	or	\$20,000 lump sum payment	or	\$20,000 lump sum payment and debarment

- (4) Debar the Contractor involved from bidding on MDOT federally funded projects.

"General Decision Number: MS20200091 01/03/2020

Superseded General Decision Number: MS20190091

State: Mississippi

Construction Type: Highway

County: Itawamba County in Mississippi.

HIGHWAY CONSTRUCTION PROJECTS

Note: Under Executive Order (EO) 13658, an hourly minimum wage of \$10.80 for calendar year 2020 applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2015. If this contract is covered by the EO, the contractor must pay all workers in any classification listed on this wage determination at least \$10.80 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in calendar year 2020. If this contract is covered by the EO and a classification considered necessary for performance of work on the contract does not appear on this wage determination, the contractor must pay workers in that classification at least the wage rate determined through the conformance process set forth in 29 CFR 5.5(a)(1)(ii) (or the EO minimum wage rate, if it is higher than the conformed wage rate). The EO minimum wage rate will be adjusted annually. Please note that this EO applies to the above-mentioned types of contracts entered into by the federal government that are subject to the Davis-Bacon Act itself, but it does not apply to contracts subject only to the Davis-Bacon Related Acts, including those set forth at 29 CFR 5.1(a)(2)-(60). Additional

LABORER: Landscape.....\$ 9.83	0.00
LABORER: Mason Tender - Cement/Concrete.....\$ 11.08	0.00
LABORER: Pipelayer.....\$ 10.76	0.00
LABORER: Laborer-Cones/ Barricades/Barrels - Setter/Mover/Sweeper.....\$ 10.38	0.00
OPERATOR: Asphalt Spreader.....\$ 16.03	0.00
OPERATOR: Backhoe/Excavator/Trackhoe.....\$ 13.42	0.00
OPERATOR: Boring Machine.....\$ 15.14	0.00
OPERATOR: Broom/Sweeper.....\$ 10.94	0.00
OPERATOR: Bulldozer.....\$ 14.38	0.00
OPERATOR: Concrete Saw.....\$ 15.68	0.00
OPERATOR: Crane.....\$ 18.32	0.00
OPERATOR: Distributor.....\$ 12.59	0.00
OPERATOR: Drill.....\$ 19.22	0.00
OPERATOR: Grader/Blade.....\$ 14.13	0.00
OPERATOR: Loader.....\$ 11.05	0.00
OPERATOR: Mechanic.....\$ 16.00	0.00
OPERATOR: Milling Machine.....\$ 15.12	0.00
OPERATOR: Oiler.....\$ 12.33	0.00

OPERATOR: Paver (Asphalt, Aggregate, and Concrete).....	\$ 16.47	0.00
OPERATOR: Piledriver.....	\$ 15.13	0.00
OPERATOR: Roller (All Types)....	\$ 11.54	0.00
OPERATOR: Scraper.....	\$ 13.15	0.00
OPERATOR: Tractor.....	\$ 10.22	0.00
OPERATOR: Trencher.....	\$ 15.00	0.00
TRUCK DRIVER: Flatbed Truck.....	\$ 13.79	0.00
TRUCK DRIVER: Lowboy Truck.....	\$ 11.00	0.00
TRUCK DRIVER: Mechanic.....	\$ 14.23	0.00
TRUCK DRIVER: Off the Road Truck.....	\$ 12.29	0.00
TRUCK DRIVER: Water Truck.....	\$ 10.58	0.00
TRUCK DRIVER: Dump Truck (All Types).....	\$ 10.49	0.00
TRUCK DRIVER: Semi/Trailer Truck.....	\$ 15.81	0.00

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

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Note: Executive Order (EO) 13706, Establishing Paid Sick Leave for Federal Contractors applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2017. If this contract is covered by the EO, the contractor must provide

employees with 1 hour of paid sick leave for every 30 hours they work, up to 56 hours of paid sick leave each year. Employees must be permitted to use paid sick leave for their own illness, injury or other health-related needs, including preventive care; to assist a family member (or person who is like family to the employee) who is ill, injured, or has other health-related needs, including preventive care; or for reasons resulting from, or to assist a family member (or person who is like family to the employee) who is a victim of, domestic violence, sexual assault, or stalking. Additional information on contractor requirements and worker protections under the EO is available at www.dol.gov/whd/govcontracts.

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (ii)).

The body of each wage determination lists the classification and wage rates that have been found to be prevailing for the cited type(s) of construction in the area covered by the wage determination. The classifications are listed in alphabetical order of ""identifiers"" that indicate whether the particular rate is a union rate (current union negotiated rate for local), a survey rate (weighted average rate) or a union average rate (weighted union average rate).

Union Rate Identifiers

A four letter classification abbreviation identifier enclosed in dotted lines beginning with characters other than ""SU"" or ""UAVG"" denotes that the union classification and rate were prevailing for that classification in the survey. Example: PLUM0198-005 07/01/2014. PLUM is an abbreviation identifier of the union which prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number

where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. 07/01/2014 is the effective date of the most current negotiated rate, which in this example is July 1, 2014.

Union prevailing wage rates are updated to reflect all rate changes in the collective bargaining agreement (CBA) governing this classification and rate.

Survey Rate Identifiers

Classifications listed under the "SU" identifier indicate that no one rate prevailed for this classification in the survey and the published rate is derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As this weighted average rate includes all rates reported in the survey, it may include both union and non-union rates. Example: SULA2012-007 5/13/2014. SU indicates the rates are survey rates based on a weighted average calculation of rates and are not majority rates. LA indicates the State of Louisiana. 2012 is the year of survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. 5/13/2014 indicates the survey completion date for the classifications and rates under that identifier.

Survey wage rates are not updated and remain in effect until a new survey is conducted.

Union Average Rate Identifiers

Classification(s) listed under the UAVG identifier indicate that no single majority rate prevailed for those classifications; however, 100% of the data reported for the classifications was union data. EXAMPLE: UAVG-OH-0010 08/29/2014. UAVG indicates that the rate is a weighted union average rate. OH indicates the state. The next number, 0010 in the example, is an internal number used in producing the wage determination. 08/29/2014 indicates the survey completion date for the classifications and rates under that identifier.

A UAVG rate will be updated once a year, usually in January of each year, to reflect a weighted average of the current negotiated/CBA rate of the union locals from which the rate is based.

WAGE DETERMINATION APPEALS PROCESS

1.) Has there been an initial decision in the matter? This can be:

- * an existing published wage determination
- * a survey underlying a wage determination
- * a Wage and Hour Division letter setting forth a position on a wage determination matter
- * a conformance (additional classification and rate) ruling

On survey related matters, initial contact, including requests for summaries of surveys, should be with the Wage and Hour Regional Office for the area in which the survey was conducted because those Regional Offices have responsibility for the Davis-Bacon survey program. If the response from this initial contact is not satisfactory, then the process described in 2.) and 3.) should be followed.

With regard to any other matter not yet ripe for the formal process described here, initial contact should be with the Branch of Construction Wage Determinations. Write to:

Branch of Construction Wage Determinations
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

2.) If the answer to the question in 1.) is yes, then an interested party (those affected by the action) can request

review and reconsideration from the Wage and Hour Administrator

(See 29 CFR Part 1.8 and 29 CFR Part 7). Write to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and by any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3.) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

4.) All decisions by the Administrative Review Board are final.

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END OF GENERAL DECISION

"

ADDENDUM

DESCRIPTION OF SHEET

WORKING NO. SHEET NO.

DETAILED INDEX (BRIDGE) DI-BR-1 8001
 DETAILED INDEX (BRIDGE) DI-BR-2 8002
 SUMMARY OF QUANTITIES (BRIDGE) 50-BR-1 8003

BRIDGE A - APV CORR. V ACROSS GUM CREEK TRIBUTARY

ESTIMATED QUANTITIES & GENERAL NOTES A1 8004
 ELEVATION A2 8005
 FOUNDATION PLAN A3 8006
 END BENT NO. 1R DETAILS A4 8007
 END BENT NO. 1R DETAILS A5 8008
 END BENT NO. 5R DETAILS A6 8009
 END BENT NO. 5R DETAILS A7 8010
 END BENT NO. 1L DETAILS A8 8011
 END BENT NO. 1L DETAILS A9 8012
 END BENT NO. 5L DETAILS A10 8013
 END BENT NO. 5L DETAILS A11 8014
 END BENT DETAILS A12 8015
 INT. BENT NO. 2R DETAILS A13 8016
 INT. BENT NO. 3R DETAILS A14 8017
 INT. BENT NO. 4R DETAILS A15 8018
 INT. BENT NO. 2L DETAILS A16 8019
 INT. BENT NO. 3L DETAILS A17 8020
 INT. BENT NO. 4L DETAILS A18 8021
 INT. BENT PILE COLLAR DETAILS A19 8022
 100 FT. SPAN DETAILS (RIGHT LANE) A20 8023
 100 FT. SPAN NO. 1R DETAILS A21 8024
 100 FT. SPAN NO. 2R DETAILS A22 8025
 100 FT. SPAN NO. 4R DETAILS A23 8026
 110 FT. SPAN DETAILS (RIGHT LANE) A24 8027
 110 FT. SPAN NO. 3R DETAILS A25 8028
 100 FT. SPAN DETAILS (LEFT LANE) A26 8029
 100 FT. SPAN NO. 1L DETAILS A27 8030
 100 FT. SPAN NO. 2L DETAILS A28 8031
 100 FT. SPAN NO. 4L DETAILS A29 8032
 110 FT. SPAN DETAILS (LEFT LANE) A30 8033
 110 FT. SPAN NO. 3L DETAILS A31 8034
 MISCELLANEOUS SPAN DETAILS A32 8035
 2'-8" RAILING DETAILS A33 8036
 BEAM DETAILS A34 8037
 100 FT. BEAM DETAILS - BEAMS 100-1 THRU 100-18 - TYPE IV A35 8038
 110 FT. BEAM DETAILS - BEAMS 110-1 THRU 110-6 - TYPE IV A36 8039
 GENERALIZED SOIL PROFILE FOR RIGHT LANE A37 8040
 GENERALIZED SOIL PROFILE FOR LEFT LANE A38 8041

BRIDGE B - APV CORR. V ACROSS PANTHER CREEK

ESTIMATED QUANTITIES & GENERAL NOTES B1 8042
 ELEVATION B2 8043
 FOUNDATION PLAN B3 8044
 END BENT NO. 1R DETAILS B4 8045
 END BENT NO. 1L DETAILS B5 8046
 END BENT DETAILS BENTS NO. 1R & 1L B6 8047
 END BENT NO. 6R DETAILS B7 8048
 END BENT NO. 6L DETAILS B8 8049
 END BENT DETAILS BENTS NO. 6R & 6L B9 8050
 INT. BENT NO. 2R DETAILS B10 8051
 INT. BENT NO. 3R DETAILS B11 8052
 INT. BENT NO. 4R DETAILS B12 8053
 INT. BENT NO. 5R DETAILS B13 8054
 INT. BENT NO. 2L DETAILS B14 8055
 INT. BENT NO. 3L DETAILS B15 8056
 INT. BENT NO. 4L DETAILS B16 8057
 INT. BENT NO. 5L DETAILS B17 8058
 60 FT. SPAN DETAILS (RIGHT LANE) B18 8059
 60 FT. SPAN NO. 1R DETAILS B19 8060
 130 FT. SPAN DETAILS (RIGHT LANE) B20 8061
 130 FT. SPAN NO. 2R DETAILS B21 8062
 80 FT. SPAN DETAILS (RIGHT LANE) B22 8063
 80 FT. SPANS NO. 3R & 4R DETAILS B23 8064
 80 FT. SPANS NO. 4R & 5R DETAILS B24 8065
 80 FT. SPANS NO. 3R & 4R REINFORCING STEEL B25 8066
 80 FT. SPANS NO. 4R & 5R REINFORCING STEEL B26 8067
 60 FT. SPAN DETAILS (LEFT LANE) B27 8068
 60 FT. SPAN NO. 1L DETAILS B28 8069
 130 FT. SPAN DETAILS (LEFT LANE) B29 8070
 130 FT. SPAN NO. 2L DETAILS B30 8071

DESCRIPTION OF SHEET

WORKING NO. SHEET NO.

BRIDGE B - APV CORR. V ACROSS PANTHER CREEK
 80 FT. SPAN DETAILS (LEFT LANE) B31 8072
 80 FT. SPANS NO. 3L & 4L DETAILS B32 8073
 80 FT. SPANS NO. 4L & 5L DETAILS B33 8074
 80 FT. SPANS NO. 3L & 4L REINFORCING STEEL B34 8075
 80 FT. SPANS NO. 4L & 5L REINFORCING STEEL B35 8076
 MISCELLANEOUS SPAN DETAILS B36 8077
 60 FT. BEAM DETAILS - BEAM 60-1 - TYPE II+2 B37 8078
 130 FT. BEAM DETAILS - BEAMS 130-1 THRU 130-3 - TYPE BT-72 B38 8079
 80 FT. BEAM DETAILS - BEAM 80-3 - TYPE III B39 8080
 80 FT. BEAM DETAILS - BEAM 80-4 - TYPE III B40 8081
 80 FT. BEAM DETAILS - BEAM 80-5 - TYPE III B41 8082
 GENERALIZED SOIL PROFILE B42 8083

BRIDGE C - APV CORR. V ACROSS CHUBBY CREEK RELIEF

ESTIMATED QUANTITIES & GENERAL NOTES C1 8084
 ELEVATION C2 8085
 FOUNDATION PLAN C3 8086
 END BENT NO. 1R DETAILS C4 8087
 END BENT NO. 10R DETAILS C5 8088
 END BENT NO. 1L DETAILS C6 8089
 END BENT NO. 10L DETAILS C7 8090
 END BENT DETAILS C8 8091
 INT. BENTS NO. 2R - 5R & 7R - 9R DETAILS C9 8092
 INT. BENT NO. 6R DETAILS C10 8093
 INT. BENTS NO. 2L - 5L & 7L - 9L DETAILS C11 8094
 INT. BENT NO. 6L DETAILS C12 8095
 60 FT. SPAN DETAILS (RIGHT LANE) C13 8096
 60 FT. SPANS NO. 1R, 2R & 3R DETAILS C14 8097
 60 FT. SPANS NO. 3R, 4R & 5R DETAILS C15 8098
 60 FT. SPANS NO. 1R, 2R & 3R REINFORCING STEEL C16 8099
 60 FT. SPANS NO. 3R, 4R & 5R REINFORCING STEEL C17 8100
 60 FT. SPANS NO. 6R & 7R DETAILS C18 8101
 60 FT. SPANS NO. 8R & 9R DETAILS C19 8102
 60 FT. SPANS NO. 6R & 7R REINFORCING STEEL C20 8103
 60 FT. SPANS NO. 8R & 9R REINFORCING STEEL C21 8104
 60 FT. SPAN DETAILS (LEFT LANE) C22 8105
 60 FT. SPANS NO. 1L, 2L & 3L DETAILS C23 8106
 60 FT. SPANS NO. 3L, 4L & 5L DETAILS C24 8107
 60 FT. SPANS NO. 1L, 2L & 3L REINFORCING STEEL C25 8108
 60 FT. SPANS NO. 3L, 4L & 5L REINFORCING STEEL C26 8109
 60 FT. SPANS NO. 6L & 7L DETAILS C27 8110
 60 FT. SPANS NO. 8L & 9L DETAILS C28 8111
 60 FT. SPANS NO. 6L & 7L REINFORCING STEEL C29 8112
 60 FT. SPANS NO. 8L & 9L REINFORCING STEEL C30 8113
 MISCELLANEOUS SPAN DETAILS C31 8114
 60 FT. BEAM DETAILS - BEAMS 60-1L/R THRU 60-12L/R - TYPE II+2 C32 8115
 60 FT. BEAM DETAILS - BEAMS 60-13L/R THRU 60-18L/R - TYPE II+2 C33 8116
 GENERALIZED SOIL PROFILE C34 8117

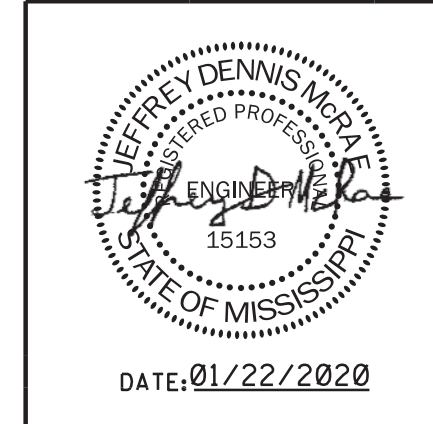
BRIDGE D - APV CORR. V ACROSS CHUBBY CREEK

ESTIMATED QUANTITIES & GENERAL NOTES D1 8118
 ELEVATION D2 8119
 FOUNDATION PLAN D3 8120
 END BENT NO. 1R DETAILS D4 8121
 END BENT NO. 6R DETAILS D5 8122
 END BENT NO. 1L DETAILS D6 8123
 END BENT NO. 6L DETAILS D7 8124
 END BENT DETAILS D8 8125
 INT. BENTS NO. 2R & 3R DETAILS D9 8126
 INT. BENT NO. 4R DETAILS D10 8127
 INT. BENT NO. 5R DETAILS D11 8128
 INT. BENTS NO. 2L & 3L DETAILS D12 8129
 INT. BENT NO. 4L DETAILS D13 8130
 INT. BENT NO. 5L DETAILS D14 8131
 40 FT. CONT. SPAN DETAILS (RIGHT LANE) D15 8132
 40 FT. SPANS NO. 1R THRU 3R DETAILS D16 8133
 40 FT. SPANS NO. 1R THRU 3R REINFORCING STEEL D17 8134
 100 FT. SPAN DETAILS (RIGHT LANE) D18 8135
 100 FT. SPAN NO. 4R DETAILS D19 8136
 40 FT. SPAN NO. 5 DETAILS (RIGHT LANE) D20 8137
 40 FT. SPAN NO. 5R DETAILS D21 8138
 40 FT. CONT. SPAN DETAILS (LEFT LANE) D22 8139
 40 FT. SPANS NO. 1L THRU 3L DETAILS D23 8140
 40 FT. SPANS NO. 1L THRU 3L REINFORCING STEEL D24 8141

STATE	PROJECT NO.
MISS.	NH-APD-0078-01(011)N

BRIDGE DIVISION		
DATE	SHEET NO.	BY
1/22/2020	8080, 8147	JDM

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
REVISIONS		DETAILED INDEX (BRIDGE)	
DATE		PROJECT NO. 102076/301000 NH-APD-0078-01(011)N ITAWAMBA COUNTY	
DESIGNED		MICHAEL BAKER INTL. - CONSULTING ENGINEERS Jackson, Mississippi	
CHECKED		WORKING NUMBER DI-BR-1	
DATE		SHEET NUMBER 8001	
DESIGNED <u>JDM</u> DETAILED <u>RQH</u> TRACED <u>CADD</u>		DATE <u>2019</u>	
CHECKED <u>MKC</u> ISSUED _____		DATE <u>2019</u>	



ADDENDUM

DESCRIPTION OF SHEET

WORKING NO. SHEET NO.

BRIDGE D - APV CORR. V ACROSS CHUBBY CREEK

100 FT. SPAN DETAILS (LEFT LANE)	D25	8142
100 FT. SPAN NO. 4L DETAILS	D26	8143
40 FT. SPAN NO. 5 DETAILS (LEFT LANE)	D27	8144
40 FT. SPAN NO. 5L DETAILS	D28	8145
MISCELLANEOUS SPAN DETAILS	D29	8146
40 FT. BEAM DETAILS - BEAMS 40-1L/R THRU 40-6L/R - TYPE 1+2	D30	8147
40 FT. BEAM DETAILS - BEAMS 40-7L/R THRU 40-12L/R - TYPE 1+2	D31	8148
100 FT. BEAM DETAILS - BEAMS 100-1L/R THRU 100-6L/R - TYPE IV	D32	8149
40 FT. BEAM DETAILS - BEAMS 40-13L/R THRU 40-18L/R - TYPE 1+2	D33	8150
GENERALIZED SOIL PROFILE	D34	8151

DESCRIPTION OF SHEET

WORKING NO. SHEET NO.

EROSION CONTROL - BRIDGE A - APV CORR. V ACROSS GUM CREEK TRIBUTARY

<i>EROSION CONTROL - ELEVATION</i>	<i>ECP-BR-A1</i>	<i>8152</i>
<i>EROSION CONTROL - FOUNDATION PLAN</i>	<i>ECP-BR-A2</i>	<i>8153</i>

EROSION CONTROL - BRIDGE B - APV CORR. V ACROSS PANTHER CREEK

<i>EROSION CONTROL - ELEVATION</i>	<i>ECP-BR-B1</i>	<i>8154</i>
<i>EROSION CONTROL - FOUNDATION PLAN</i>	<i>ECP-BR-B2</i>	<i>8155</i>

EROSION CONTROL - BRIDGE C - APV CORR. V ACROSS CHUBBY CREEK RELIEF

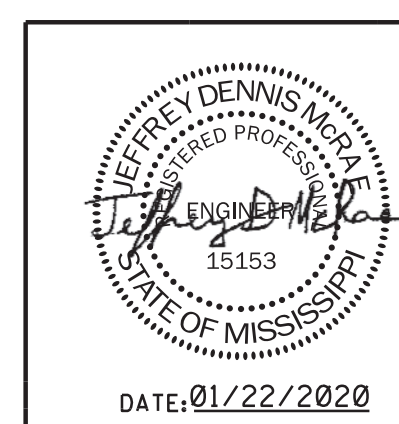
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<i>EROSION CONTROL - FOUNDATION PLAN</i>	<i>ECP-BR-C2</i>	<i>8157</i>

EROSION CONTROL - BRIDGE D - APV CORR. V ACROSS CHUBBY CREEK

<i>EROSION CONTROL - ELEVATION</i>	<i>ECP-BR-D1</i>	<i>8158</i>
<i>EROSION CONTROL - FOUNDATION PLAN</i>	<i>ECP-BR-D2</i>	<i>8159</i>

STATE	PROJECT NO.
MISS.	NH-APD-0078-01(011)N

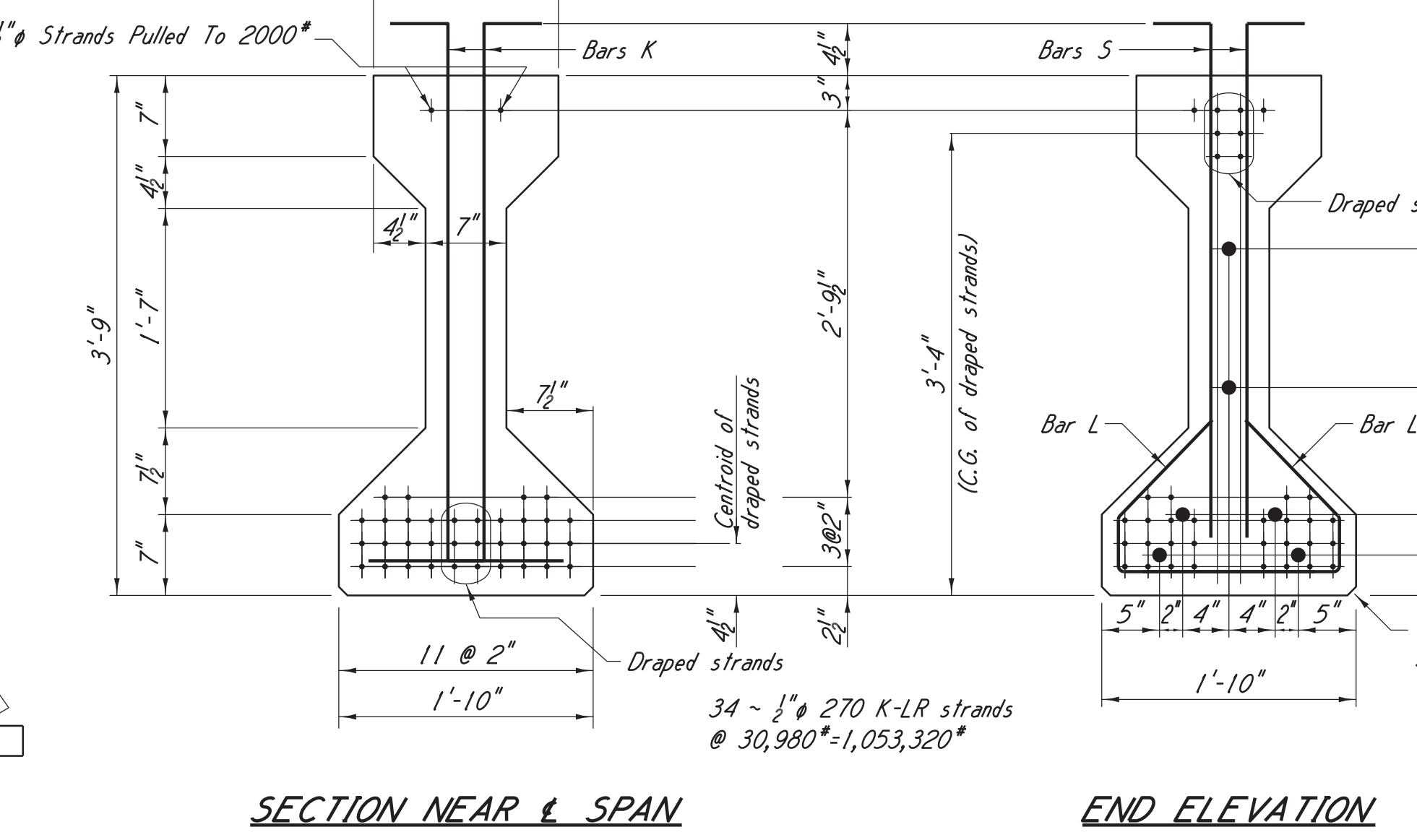
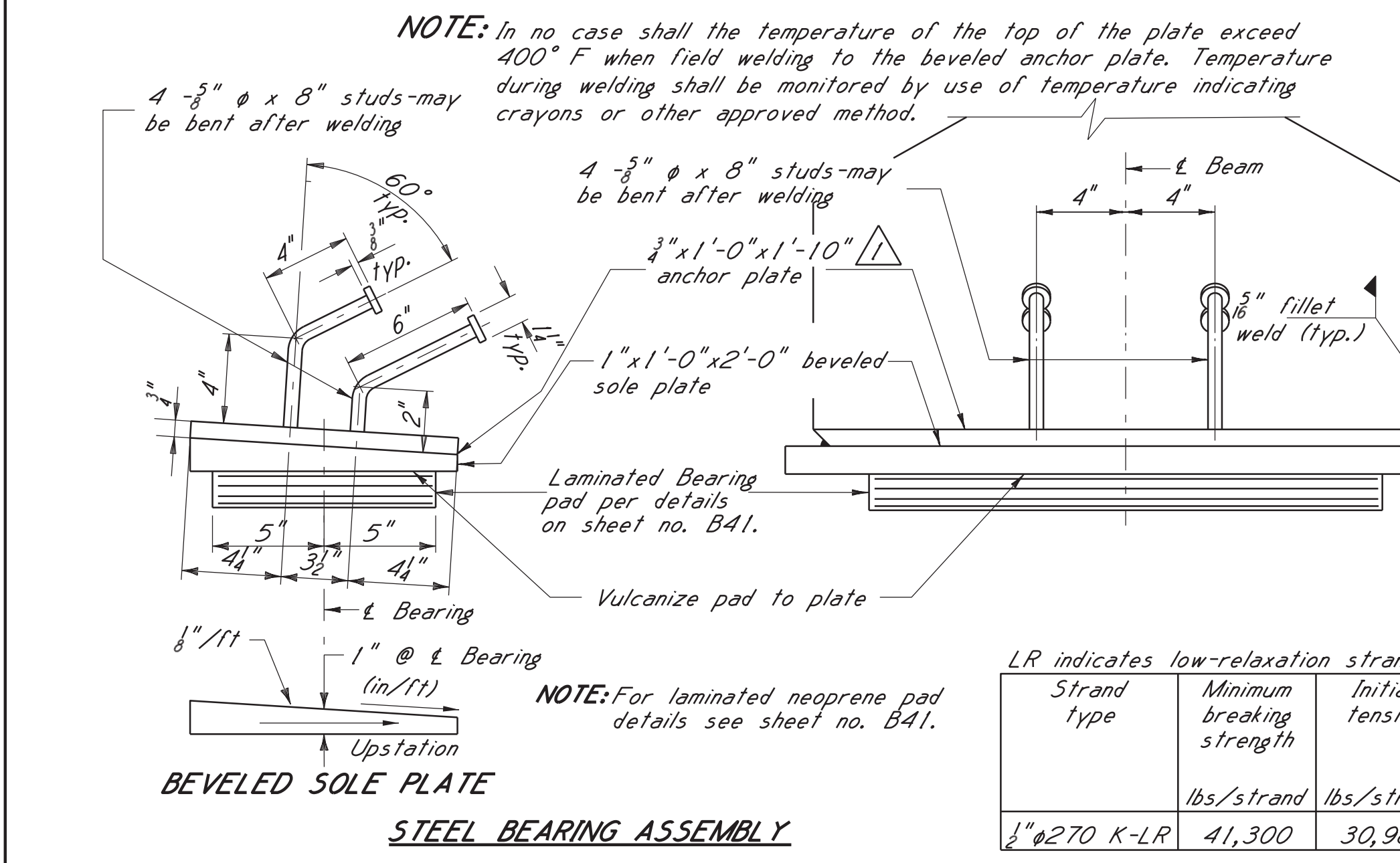
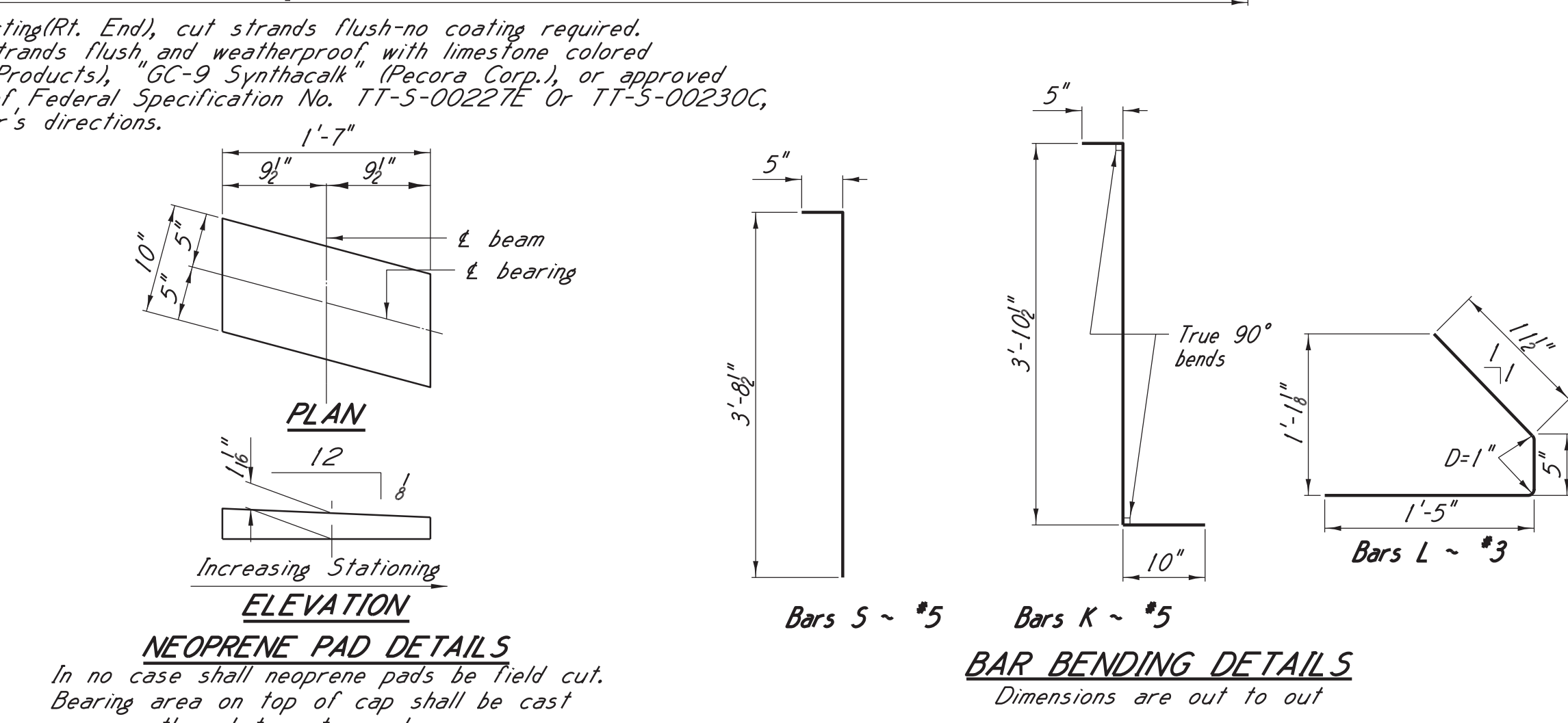
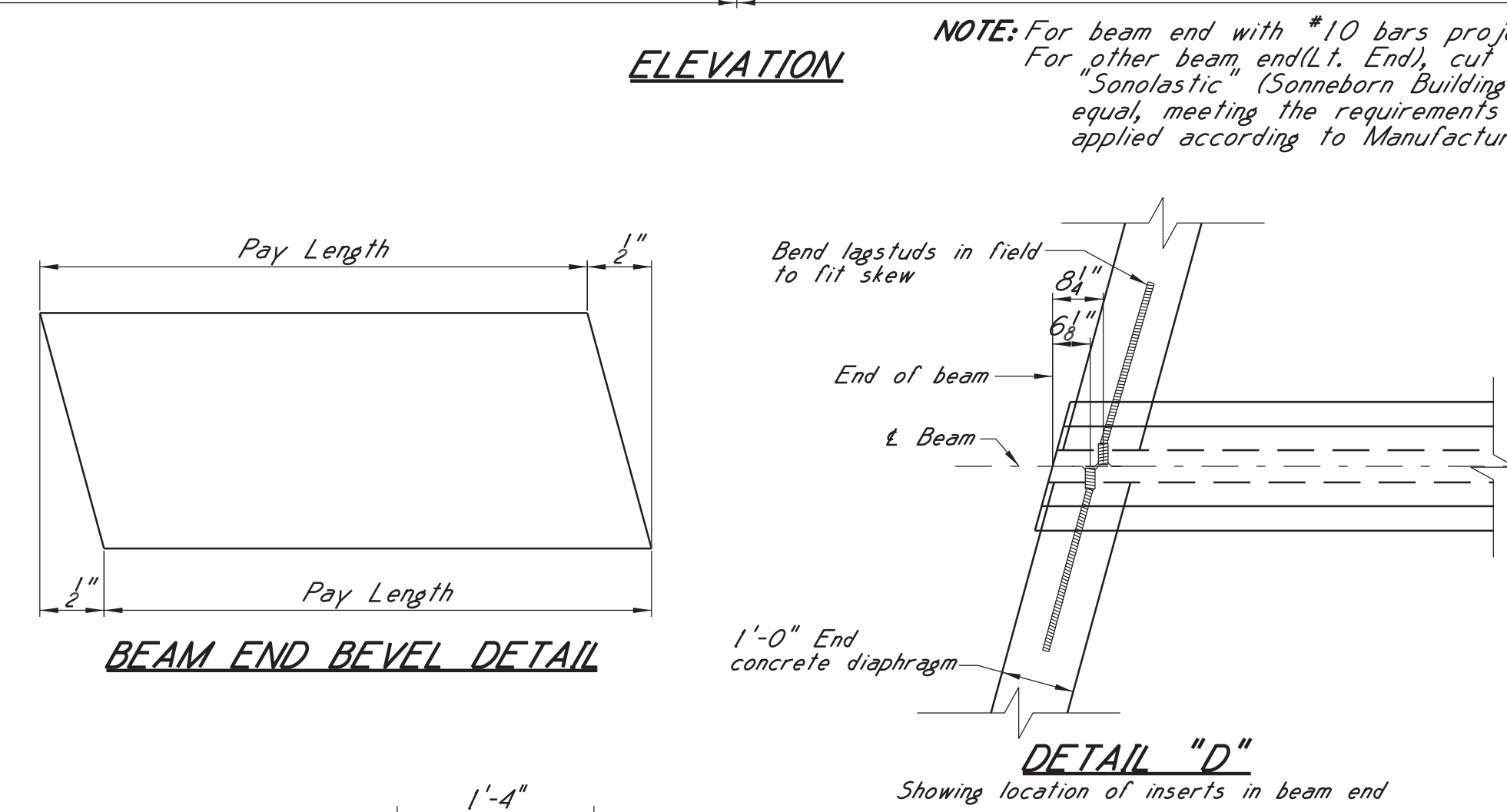
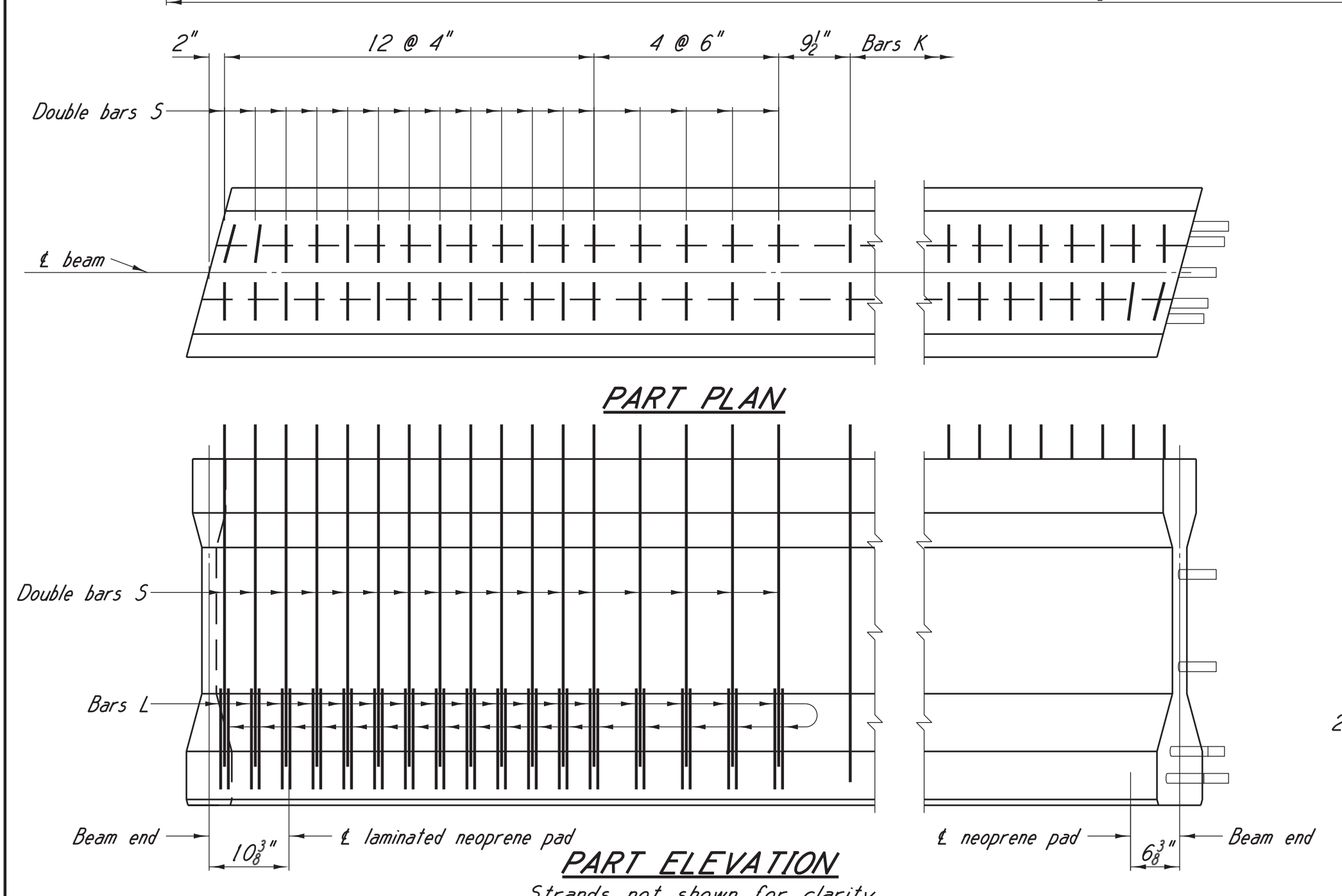
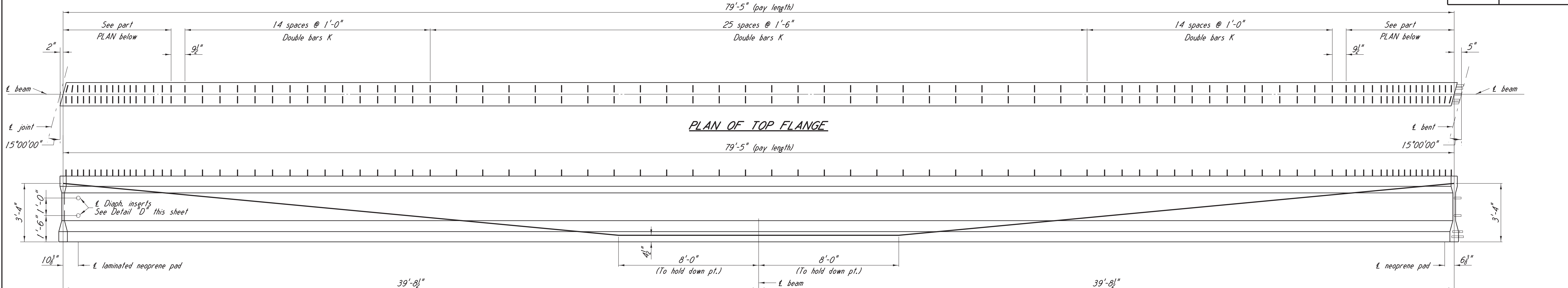
BRIDGE DIVISION		
DATE	SHEET NO.	BY
1/22/2020	8080, 8147	JDM



MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
DETAILED INDEX (BRIDGE)	
102076/301000 PROJECT NO. NH-APD-0078-01(011)N ITAWAMBA COUNTY	
MICHAEL BAKER INTL. - CONSULTING ENGINEERS Jackson, Mississippi	WORKING NUMBER DI-BR-2
DESIGNED <u>JDM</u> DETAILED <u>RQH</u> TRACED <u>CADD</u> CHECKED <u>MKC</u> ISSUED _____ DATE <u>2019</u>	SHEET NUMBER 8002

ADDENDUM

STATE	PROJECT NO.
MISS.	NH-APD-0078-01(011)N



GENERAL NOTES:
 Beams shall be manufactured in accordance with Mississippi Standard Specifications for Road & Bridge Construction, 2017.
 The tops of beams shall be rough floated. At approximately the time of initial set the entire tops of beams shall be scrubbed transversely with a coarse wire brush to remove all laitance and produce a roughened surface for bonding slab. Other surfaces shall be finished per specifications.
 Strand pattern detailed is for 2 @ 270 K-LR strands. Shop drawings of prestressed beams shall include the type and location of all strands.
 The Director of Structures, State Bridge Engineer shall be notified if the camber of the beam is not within the limits shown in table.
 Concrete shall be class "FX" and:
 (a) shall have a 28-day cylinder strength of 6000 p.s.i.
 (b) at transfer of the tensioning load, the cylinder strength of the concrete shall be as shown in table.
 At the Contractor's request a suggested concrete design mix will be furnished with the understanding that it is the Contractor's responsibility to maintain 6000 p.s.i. concrete.
 If any cylinder tests below 6000 p.s.i., the beam represented will be held on the yard until the 28-day strength is determined and acceptance or rejection has been established.

DESIGN DATA
 Unit stresses are in accordance with A.A.S.H.T.O., LRFD 2016.
 Stay-In-Place Metal Forms 18 lbs./ft.² (Between Flanges)

PRESTRESS REQUIREMENTS

Strand type	Minimum breaking strength lbs/strand	Initial tension lbs/strand	Required number and location of strands				Centroid for total number of strands (in.)		Distance from & span to hold-down point	Camber limits	Deflection diagram			Minimum concrete strength at time of release (psi)
			Total number strands	Straight strands	Draped strands		At & span	At beam end			A	B	C	
1/2" @ 270 K-LR	41,300	30,980	34	28	5.07	6	4.50	40.00	8'-0"	0 to 3 1/2"	1 1/8"	1 3/8"	5"	5100

For deflection diagram, see Misc. Span Details per sheet no. B36.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 BRIDGE AT OPP. STA. 694+47.84 LT. LN.
 OPP. STA. 694+43.83 RT. LN.
 80 FT. BEAM DETAILS
 BEAM 80-3 TYPE III

PROJECT NO. 102076/301000
 NH-APD-0078-01(011)N
 ITAWAMBA COUNTY

MICHAEL BAKER INTL. - CONSULTING ENGINEERS
 Jackson, Mississippi

WORKING NUMBER
B39 OF 42

DESIGNED: JDM DETAILED: RQH TRACED: CADD
 CHECKED: MKC ISSUED: DATE: 2019 SHEET NUMBER
8080

DATE: 01/22/2020

