

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u> 1 </u>	DATED <u> 2/5/2020 </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>
ADDENDUM NO. <u> </u>	DATED <u> </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>
ADDENDUM NO. <u> </u>	DATED <u> </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>

Number	Description
1	Revised Notice To Bidders No. 2185; Amendment EBSx Download Required.

TOTAL ADDENDA: 1
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____	President	Address
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_____	Secretary	Address
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_____	Treasurer	Address
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The following is my (our) itemized proposal.

STP-0024-05 (010) / 107628301

Kemper County(ies)

Revised 01/26/2016

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 2185

CODE: (SP)

DATE: 01/14/2020

SUBJECT: Scope of Work

PROJECT: STP-0024-05(010) / 107628301 -- Kemper County

The contract documents do not include an official set of plans, but may by reference include some Standard Drawings or Special Drawings. All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on this project shall consist of the following:

Mill and overlay approximately 12 miles of SR 16 in Kemper County. The BOP is at the intersection of SR 39 and SR 16 in DeKalb and the EOP is at the intersection of SR 16 and US 45 in Scooba.

The existing pavement consists of 6” to 15” of asphalt over 1” to 3” of clay gravel over 6” to 12” of topping with 14-foot lanes and 2-foot paved shoulders, and 3’ and variable gravel shoulders. Also, there is two (2) miles of 3-lane section in the project that consists of 6” to 15” of asphalt with 12-foot lanes and 2-foot paved shoulders, and 3-foot and variable gravel shoulders.

Construction signage shall be installed as per the detail sheets included prior to the beginning of work.

The existing asphalt roadway shall be fine milled 2” and overlaid with 2” of 12.5-mm, MT, asphalt. Extreme care shall be taken in the town of DeKalb to ensure that the milling does not enter any inlets and block the drainage structure.

The existing widened shoulder shall be removed and reconstructed using pay item 202-B: Removal of Asphalt Paved Shoulders, All Depths, and pay item 403-C: 19-mm, MT, Asphalt Pavement, Trench Widening. The reconstructed trench widening shall be 6” thick (2 lifts at 3”). Payment for hauling the removed asphalt off the project shall be included in pay item 202-B: Removal of Asphalt Paved Shoulders. The shoulder material shall be spread across the existing shoulders and back slopes at the Engineer’s discretion. Any shoulder material that cannot be adequately blended in to the shoulder and back slopes shall be excessed using Pay Item 203-G: Excess Excavation, LVM, AH.

Failed areas shall be repaired full depth using 12.5-mm, MT, Asphalt Pavement, Leveling. A table showing failed area locations is shown below.

Traffic on the milled surface shall be limited to seven (7) days. The Contractor will be assessed a penalty of **\$5,000 per calendar day** afterwards until the milled surfaces are covered with the next lift of asphalt.

Local public roads shall be milled 2" and overlaid 2" of 12.5-mm, MT, asphalt to the end of the existing asphalt pavement, end of MDOT maintenance, to right-of-way, or as directed. Where a minimum of five feet (5') of shoulder width can be paved at the beginning of local road radii, a 100-foot asphalt pavement taper shall be constructed. Said taper shall be 6" thick (2 lifts at 3" 12.5-mm, MT, asphalt) and shall conform to the detail drawings.

Temporary pavement markings shall be placed at the end of each day's paving operations and prior to opening the road to traffic. Permanent pavement markings shall be constructed after completion of all paving operations.

Permanent raised pavement markers shall be installed on mainline and local public roads after completion of all paving operations.

Any existing asphalt curbs that are deemed unsuitable by the Engineer shall be removed by the contractor. The cost associated with this shall be included in other items bid.

The face of all existing undisturbed bituminous curbs shall be painted with at least two coats of white traffic paint with glass beads being required in the top coat. The cost associated with the painting of new or existing curb is to be included in other items bid.

Crushed stone granular material shall be used to bring roadway shoulders to grade. It is not anticipated that the granular material will be required throughout the length of the project but only in areas deficient of shoulder material and as directed. Crushed stone granular material shall meet the requirements of Subsection 703.04 for ¾" and Down, Size No. 610, or Size No. 825B. The Contractor has the option to use any of the three alternate materials. However, the Contractor shall choose one of these alternates and use it throughout the project. No substitutions will be allowed without permission from the Engineer. Crushed concrete may not be used for shoulder material.

Guardrails are required to be replaced at various locations within the project (see attached Table for locations/quantities). All guardrail removed is to be replaced the same day and prior to reopening the adjacent lane of traffic. This work shall consist of the following sequence of operations: removal of the existing guardrail and posts, removal of the entire guardrail pad and repaving the guardrail pad, and installation of the new guardrail. Voids created by the removal of posts, concrete anchors, footings, etc. shall be backfilled and tamped in accordance with Section 203 of the Standard Specifications. All guard rails, including rail, terminal end sections, bridge end sections, and metal posts will become property of MDOT and shall be delivered by the Contractor to the MDOT DeKalb Maintenance Facility located at 17015 Highway 16 East DeKalb, MS. The Contractor shall coordinate the delivery of these items with MDOT in advance. Concrete anchors, wooden posts, wooden rails will become the property of the Contractor and will be removed from the Project Site. The Contractor shall not damage or disturb the existing guardrail or posts during the grading or the paving operation. The guardrail pad shall be constructed using 12.5-mm, MT, asphalt and shall be 4" thick. The cost of the removal of the guardrail delineators

and object marker signs shall be included in other items bid. The length of guardrail required is based on a terminal section of 37.5' being used. For any other length terminal section, the length of normal guardrail shall be adjusted.

The existing Type G-Modified Bridge End Section connectors on the project shall be replaced with Type G-Modified connectors shown in the attached Standard Drawing. The trailing guardrail connector is to be installed as shown. The existing bridge rails do not contain the guardrail anchor assemblies shown on the Standard Drawing. Therefore, the Contractor shall drill 1-inch holes through the concrete bridge rail in the configuration matching the terminal connector and Plate "Y". All work and materials required for the installation are to be included in the bid price for the Type G Modified Bridge End Section.

Rumble Strip shall be placed according to the attached typical sections. Rumble strip shall not be placed within the DeKalb and Scooba city limits.

The joint repair shall include removal of all existing joint material, joint preparation, saw cutting, installation of the preformed joint seal and other necessary work per the included standard drawing or as directed by the Engineer. All concrete approach slab joints shall be sealed. If the bridge has an asphalt approach, the joint between the asphalt and concrete shall not be disturbed. Removal of existing silicone sealed, compression, and AC sealed joint materials will not be paid directly and shall be considered an absorbed item of work.

After the existing joint material has been removed, the joints shall then be saw cut as per the Joint Repair Standard Drawing. Saw cuts will be paid for under pay item 907-823-B: Saw Cut, Type 1. The joint shall then be sealed by one of the approved Manufacturers and installed according to the Manufacturer's specifications.

Any bridges and curb section shall be swept off and cleaned at the end of the project to remove any existing debris plus any debris accumulated from construction activities. The sweeping and cleaning of bridges shall be absorbed in other items bid.

GENERAL NOTES

Milling and paving operations shall be performed such that a -2% slope from centerline is provided in normal crown roadway sections. Superelevation through curves shall be maintained as it currently exists or improved as directed.

Temporary asphalt joints (aka paper joints) shall be constructed at the end of each day's milling operations where the milled surface joins the existing asphalt pavement surface. Paper joints shall be a minimum of nine feet (9') in length and for the full width of the milled surface. Paper joints shall be adequately maintained.

The Contractor is responsible for providing shoulder drainage outlets as applicable in milled areas. Payment for these outlets shall be included in the bid price for the milling of bituminous pavement.

The Reclaimed Asphalt Pavement (RAP) material removed by the milling operation shall become the property of the Contractor.

Existing asphalt/concrete driveway connections shall be milled 2" or removed and replaced with new asphalt connections using 12.5-mm, MT, asphalt. The existing asphalt/concrete driveway connections shall be paved to the shoulder line per the included typical drawing. Pads shall be shaped horizontally and vertically to prevent excessive drop-offs. Granular Material, Crushed Stone, shall be placed around the pads to prevent shoulder drop-offs as directed and shall be placed in a timely manner. Drop-offs exceeding 2½" shall be corrected within two (2) calendar days of the placement of the pad. Granular Material, Crushed Stone, will be used as directed by the Engineer to fill in existing low shoulders. Crushed concrete will not be allowed as a substitution for crushed stone.

Potholes that may exist or occur in the existing pavement are to be patched in a timely manner. Patching of potholes shall be considered an absorbed item.

Any failed areas that develop after the asphalt operations have been completed shall be repaired full depth using 12.5mm, MT, Asphalt Pavement, and Leveling. Saw Cuts shall be required for this operation and will be paid for under pay item 503-C010 Saw Cut, Full Depth.

Where applicable the existing shoulders are to be raised to match the new pavement elevation by placing variable depth crushed limestone on the existing shoulders. Placement of the granular material on the finished asphalt course shall not be permitted. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%). Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%). The cost of blading will be an absorbed item and is not to be included in the price of pay items bid.

Removal of the existing shoulder material shall be coincident with the milling/overlaying operation to prevent the possible ponding of water. No payment will be made for blading or removal of the existing shoulder material. Any material excavated from the existing shoulder shall be used to raise the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer and will be an absorbed item. Material which cannot be placed in adjacent areas and deemed to be excess excavations by the Engineer will be an absorbed item.

Temporary stripe will be required immediately after milling and overlaying and prior to opening the area to traffic. Temporary stripe is to be placed in the same location and layout as permanent stripe.

All permanent striping will be thermoplastic. The width of the permanent stripe will be six inches (6").

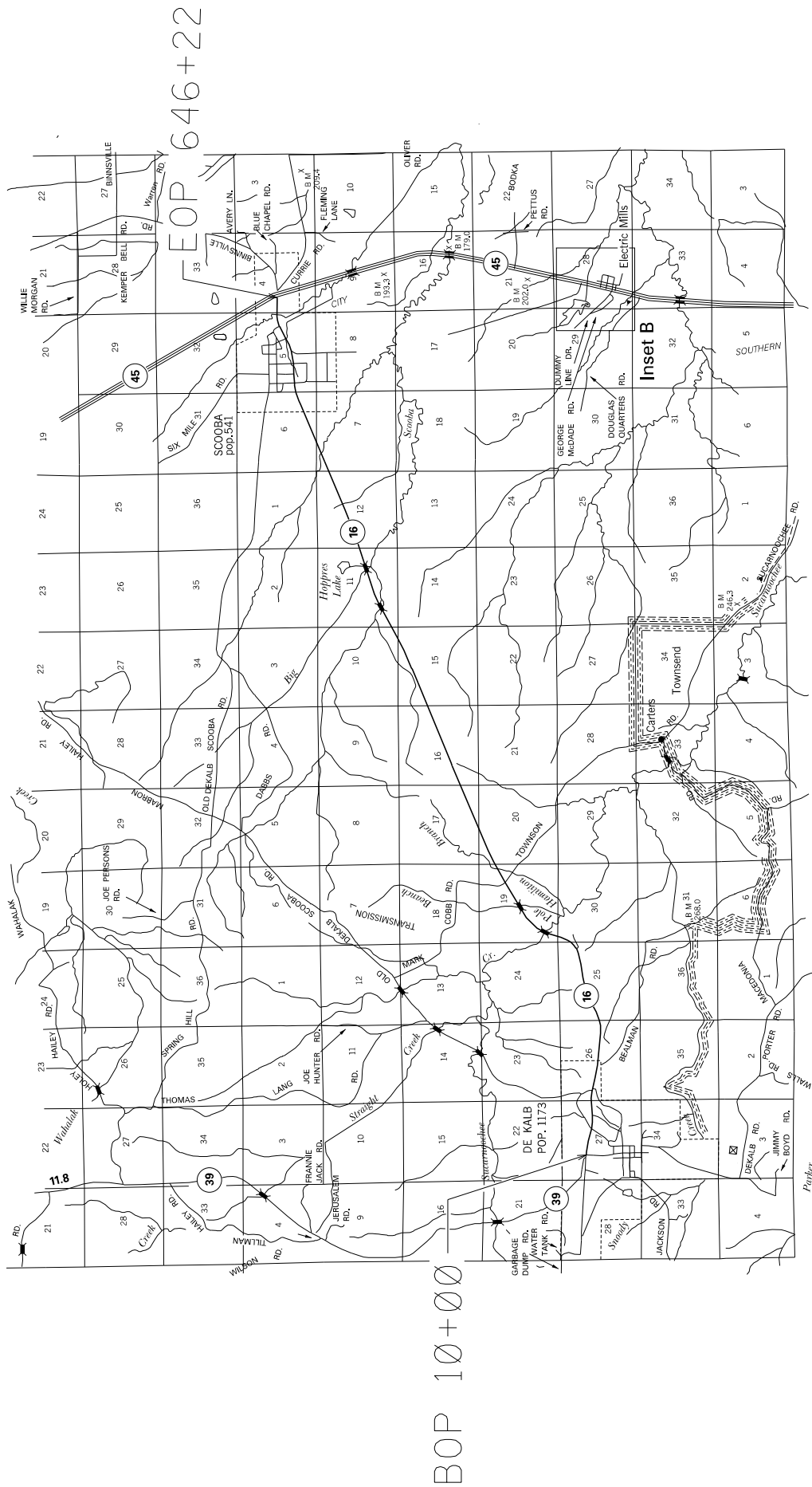
The Contractor shall erect and maintain construction signing, and provide and maintain all temporary signs and traffic control devices necessary to safely conduct traffic through the work area in accordance with the Traffic Control Plan and the MUTCD.

All traffic control devices shall meet current MDOT and MUTCD requirements.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal. The cost is to be included in the prices of items bid. Failure of the Contractor to remove debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor. As described in the applicable Notice To Bidders, final project cleanup is required and will be completed prior to the scheduling of the final inspection.

It shall be the responsibility of the Contractor to protect existing structures such as pipes, aprons, signs, utilities, etc. from damage occurring as a result of construction activities. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged during the life of the contract. No payment will be made for replacements and or repairs resulting from such damages.

STP-0024-05(010)/107628-301000
SR 16 - KEMPER COUNTY
PROJECT LOCATION MAP

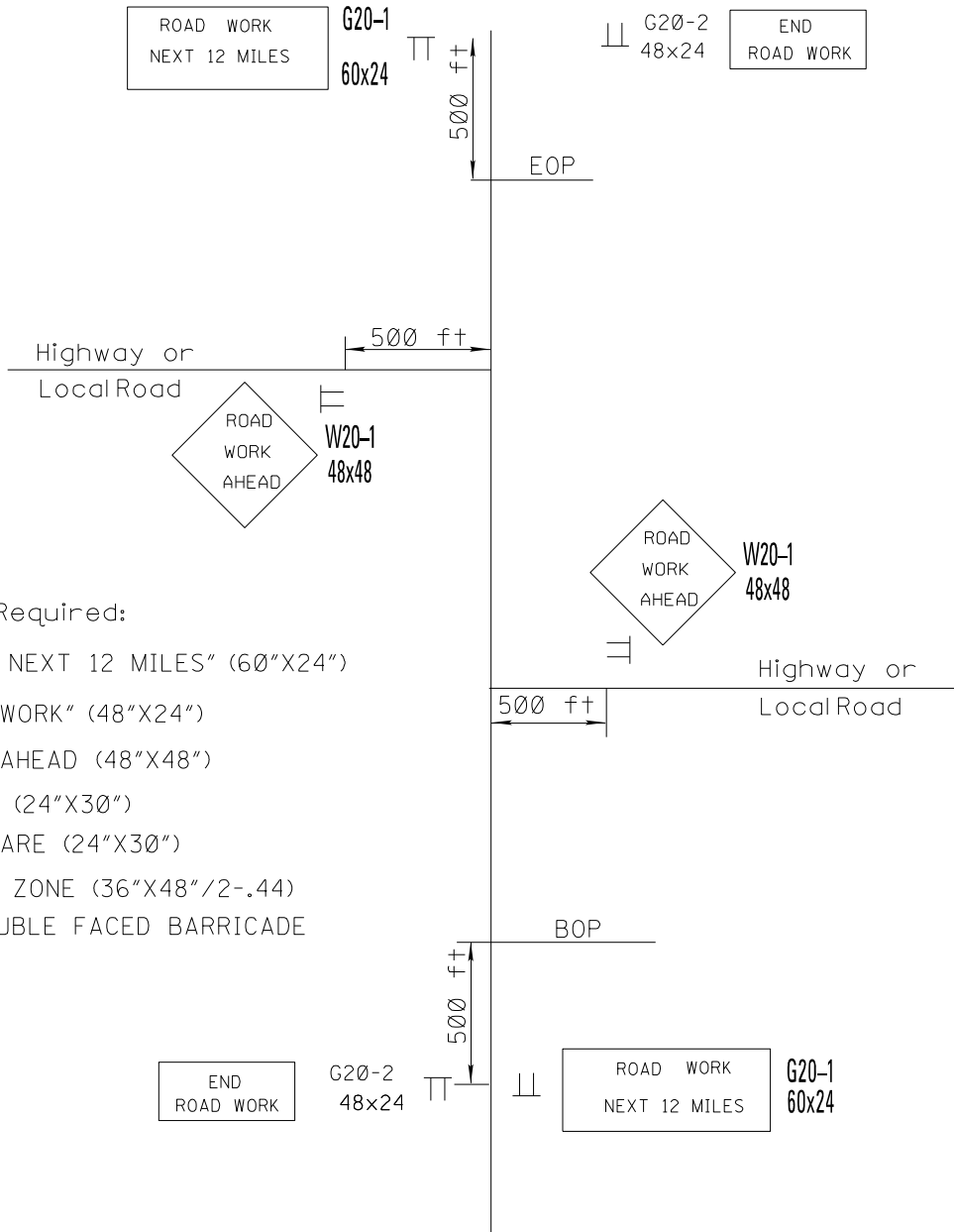


BOP 10+00

EOP 646+22

Inset B

SR 16 - KEMPER COUNTY CONSTRUCTION SIGNING



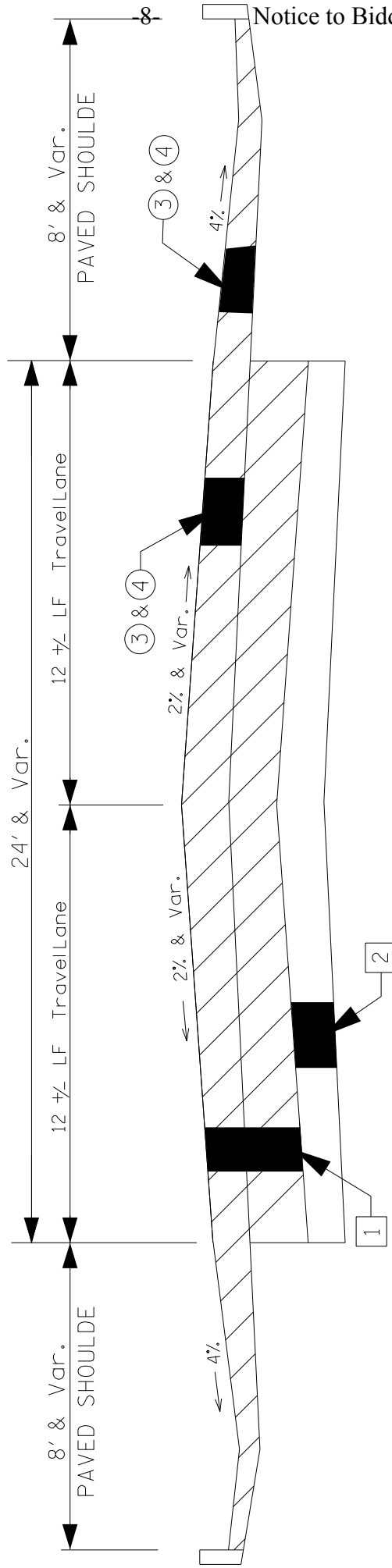
Traffic Control Signs Required:

- 2 - G20-1 "ROAD WORK NEXT 12 MILES" (60"x24")
- 2 - G20-2 "END ROAD WORK" (48"x24")
- 21 - W20-1 ROAD WORK AHEAD (48"x48")
- 103 - R4-1 DO NOT PASS (24"x30")
- 25 - R4-2 PASS WITH CARE (24"x30")
- 27 - W14-3 NO PASSING ZONE (36"x48"/2-.44)
- 4 619-G TYPE III DOUBLE FACED BARRICADE

- NOTES: ① One (1) W20-1 "ROAD WORK AHEAD" Sign is Required at each Local Road, Street or Highway Entering the Project.
- ② G20-1 and G20-2 signs mounted on Type III Double Faced Barricade.
- ③ R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE", and W14-3 "NO PASSING ZONE" signs are required in accordance with Subsection 618.03.3 and as specified in the MUTCD. If No Passing zones are 1000 ft or more, install additional "DO NOT PASS" signs on maximum spacing of 750 ft.
- ④ Placement of W20-1 signs on intersecting roads may vary from typical shown as conditions warrant.

SR 16 - KEMPER COUNTY TYPICAL SECTION - MILL & OVERLAY CURB SECTION

STATIONS: 10+00 - 27+00



EXISTING

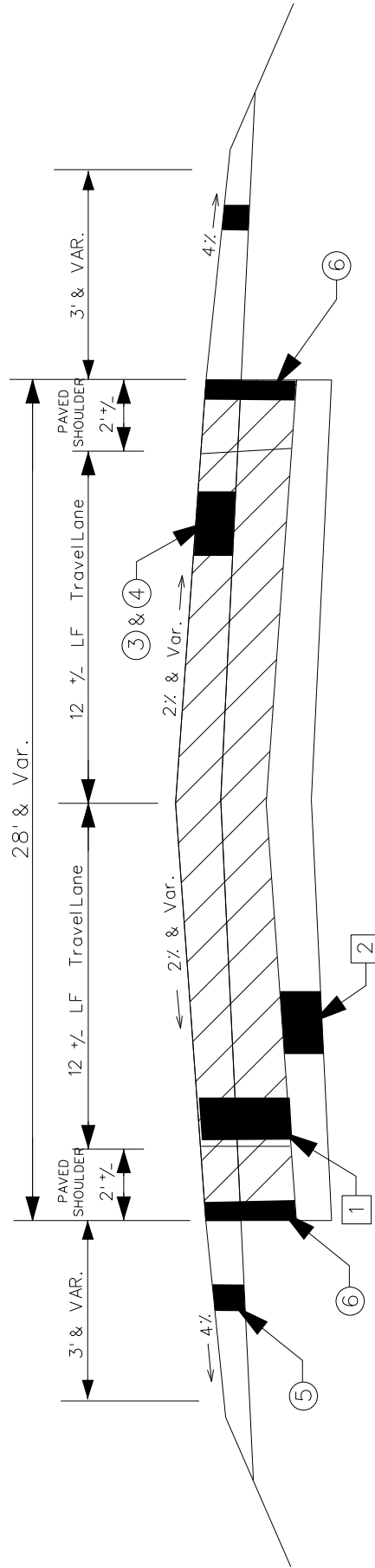
PROPOSED

- 1 6"-15" Existing Thickness based on Core Evaluation
- 2 1"-3" Granular Material over 6"-12" Topping
- 3 2" Milling (Correct to 2% Normal Crown or SE Where Needed)
- 4 2" Overlay 12.5mm, MT, Asphalt Pavement

Prior to overlay, Repair any failed areas full depth with 12.5mm Mix, MT, Asphalt Pavement, Leveling.

SR 16 - KEMPER COUNTY TYPICAL SECTION - MILL & OVERLAY

STATIONS: 27+00 - 67+00



EXISTING

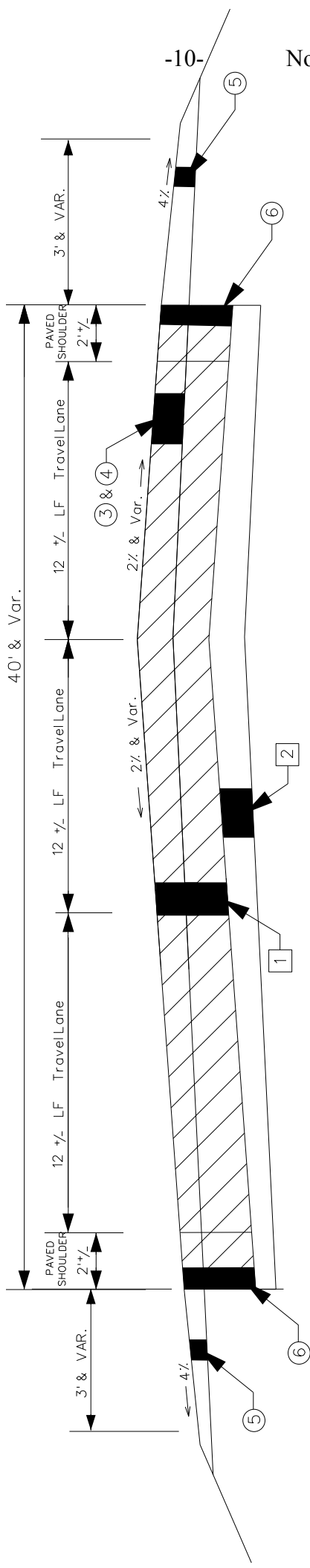
- 1 6"-15" Existing Thickness based on Core Evaluation
- 2 1"-3" Granular Material over 6"-12" Topping

PROPOSED

- 3 2" Milling (Correct to 2% Normal Crown or SE)
- 4 2" 12.5mm, MT, Asphalt Pavement
- 5 Crushed Limestone (As Directed)
- 6 Remove 2' widened shoulder and replace with two lifts of 3" 19mm, MT, Asphalt Pavement, Trench Widening.

Prior to overlay, Repair any failed areas full depth with 12.5mm Mix, MT, Asphalt Pavement, Leveling.

SR 16 - KEMPER COUNTY
 TYPICAL SECTION - MILL & OVERLAY
 3 LANE SECTION
 STATIONS: 67+00 - 72+30



EXISTING

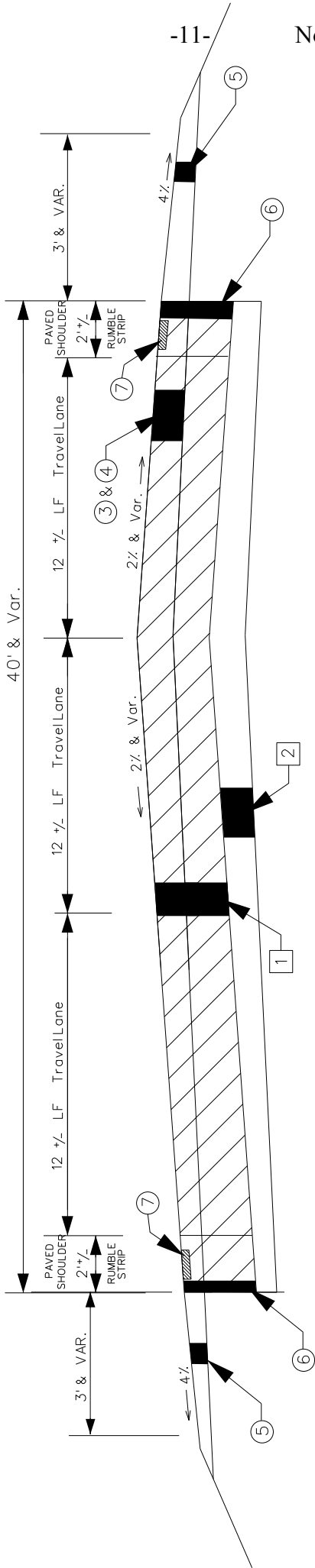
- 1 6"-15" Existing Thickness based on Core Evaluation
- 2 1"-3" Granular Material over 6"-12" Topping

PROPOSED

- 3 2" Milling (Correct to 2% Normal Crown or SE)
- 4 2" 12.5mm, MT, Asphalt Pavement
- 5 Crushed Limestone (As Directed)
- 6 Remove 2' widened shoulder and replace with two lifts of 3" 19mm, MT, Asphalt Pavement, Trench Widening.

Prior to overlay, Repair any failed areas full depth with 12.5mm Mix, MT, Asphalt Pavement, Leveling.

SR 16 - KEMPER COUNTY
 TYPICAL SECTION - MILL & OVERLAY
 3 LANE SECTION
 STATIONS: 72+30 - 169+50



EXISTING

- 1 6"-15" Existing Thickness based on Core Evaluation
- 2 1"-3" Granular Material over 6"-12" Topping

PROPOSED

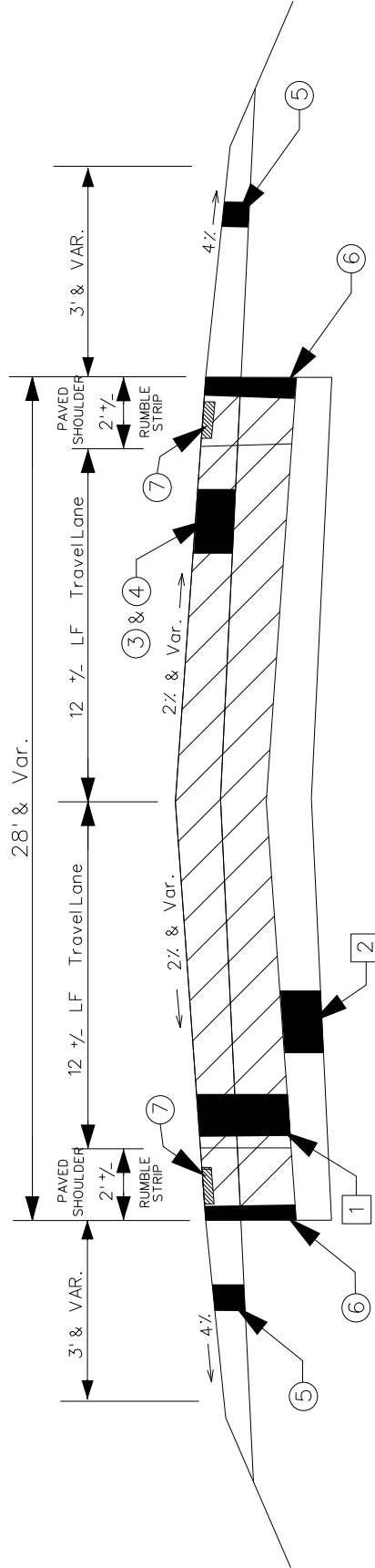
- 3 2" Milling (Correct to 2% Normal Crown or SE)
- 4 2" 12.5mm, MT, Asphalt Pavement
- 5 Crushed Limestone (As Directed)
- 6 Remove 2' widened shoulder and replace with two lifts of 3" 19mm, MT, Asphalt Pavement, Trench Widening.
- 7 Rumble Stripes (Ground In) as per Standard Drawing Sheet.

Prior to overlay, Repair any failed areas full depth with 12.5mm Mix, MT, Asphalt Pavement, Leveling.

SR 16 - KEMPER COUNTY

TYPICAL SECTION - MILL & OVERLAY

STATIONS: 169+50 - 566+60



EXISTING

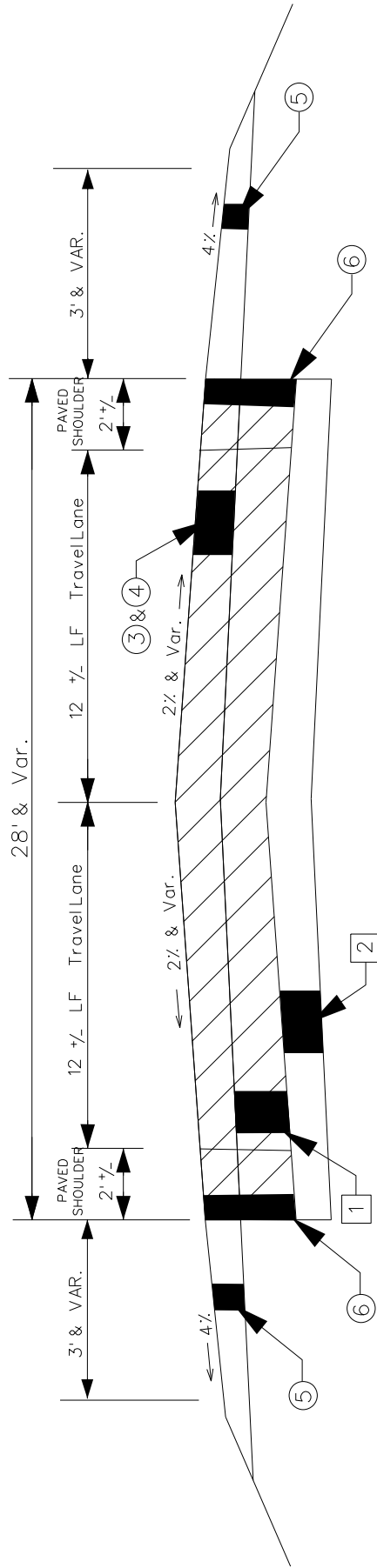
- 1 6"-15" Existing Thickness based on Core Evaluation
- 2 1"-3" Granular Material over 6"-12" Topping

PROPOSED

- 3 2" Milling (Correct to 2% Normal Crown or SE)
- 4 2" 12.5mm, MT, Asphalt Pavement
- 5 Crushed Limestone (As Directed)
- 6 Remove 2' widened shoulder and replace with two lifts of 3" 19mm, MT, Asphalt Pavement, Trench Widening.
- 7 Rumble Stripes (Ground In) as per Standard Drawing Sheet.

Prior to overlay, Repair any failed areas full depth with 12.5mm Mix, MT, Asphalt Pavement, Leveling.

SR 16 - KEMPER COUNTY
 TYPICAL SECTION - OVERLAY
 STATIONS: 566+60 - 646+22



EXISTING

- 1 6''-15'' Existing Thickness based on Core Evaluation
- 2 1''-3'' Granular Material over 6''-12'' Topping

PROPOSED

- 3 2'' Milling (Correct to 2% Normal Crown or SE)
- 4 2'' 12.5mm, MT, Asphalt Pavement
- 5 Crushed Limestone (As Directed)
- 6 Remove 2' widened shoulder and replace with two lifts of 3'' 19mm, MT, Asphalt Pavement, Trench Widening.

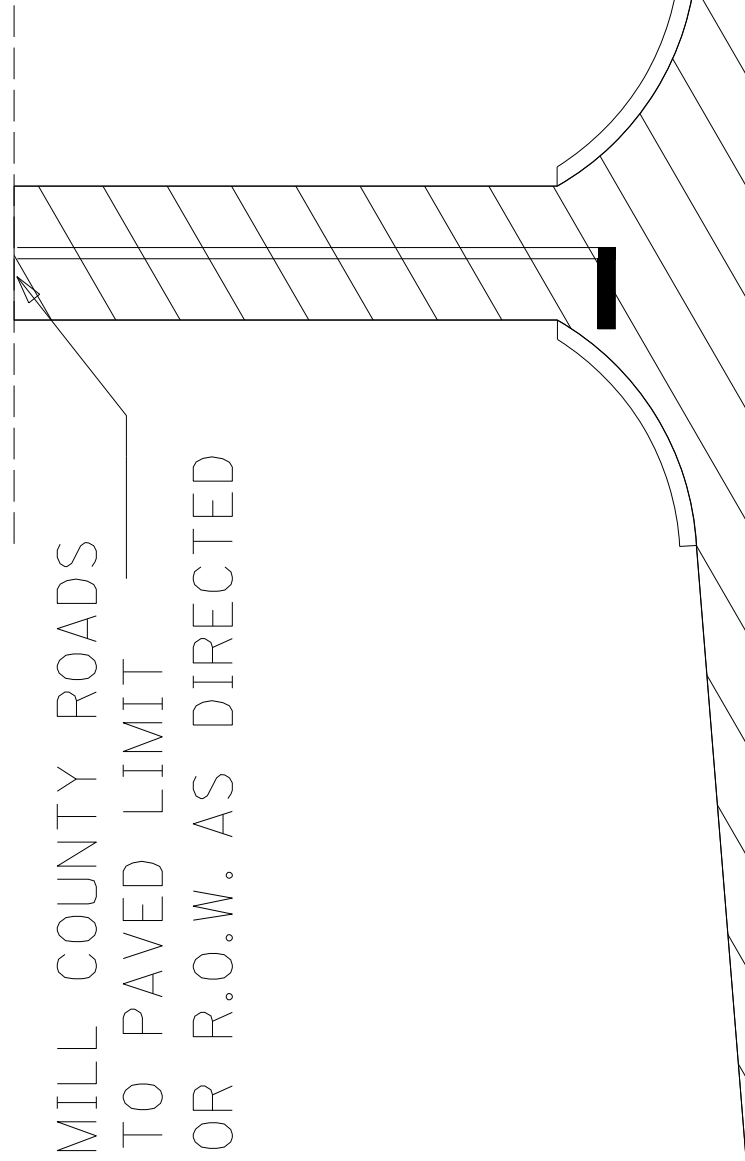
Prior to overlay, Repair any failed areas full depth with 12.5mm Mix, MT, Asphalt Pavement, Leveling.

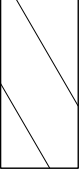
SR 16 FAILED AREA LOCATIONS										
LOCATION	STA.	TO	STA.	LENGTH (FT)	WIDTH (FT)	AREA (SF)	202-B009 REMOVAL OF ASPHALT, FAILED AREAS (SY)	503-C010 SAW CUT, FULL DEPTH	403-B002 12.5MM, MT, ASPHALT PAVEMENT, LEVELING	
								(LF)	(TONS)	
RT LANE	7+44	-	9+00	156	6	936	104	168	35	
RT LANE	9+38	-	10+00	62	6	372	41	136	14	
RT LANE	9+70	-	9+85	39	15	585	65	69	22	
RT LANE	10+00	-	12+00	200	6	1,200	133	212	45	
RT LANE	39+60	-	43+00	340	6	2,040	227	352	77	
RT LANE	56+60	-	58+00	140	6	840	93	152	32	
RT LANE	66+09	-	69+00	291	6	1,746	194	303	65	
RT LANE	78+50	-	90+00	1150	6	6,900	767	1162	259	
RT LANE	109+00	-	115+00	600	6	3,600	400	612	135	
RT LANE	172+00	-	174+15	215	12	2,580	287	239	97	
RT LANE	178+68	-	182+00	332	12	3,984	443	356	149	
RT LANE	612+00	-	615+32	332	6	1,992	221	344	75	
RT LANE	623+00	-	625+61	261	6	1,566	174	273	59	
RT LANE	635+00	-	637+11	211	6	1,266	141	223	47	
RT LANE	639+80	-	642+00	220	6	1,320	147	232	50	
LT LANE	10+00	-	10+70	70	6	420	47	82	16	
LT LANE	13+70	-	14+70	100	6	600	67	112	23	
LT LANE	16+50	-	18+42	192	6	1,152	128	204	43	
LT LANE	20+15	-	22+17	202	6	1,212	135	214	45	
LT LANE	22+80	-	23+75	95	6	570	63	107	21	
LT LANE	40+40	-	42+00	160	6	960	107	172	36	
LT LANE	42+85	-	44+31	146	6	876	97	158	33	
LT LANE	634+00	-	635+00	100	6	600	67	112	23	
LT LANE	640+56	-	641+61	105	6	630	70	117	24	
Total =							2754	3761	929	
Additional Quantities To Be Used As Directed By The Engineer:							275	376	93	

Note: Locations and Measurements are Approximate and may Vary With Field Conditions

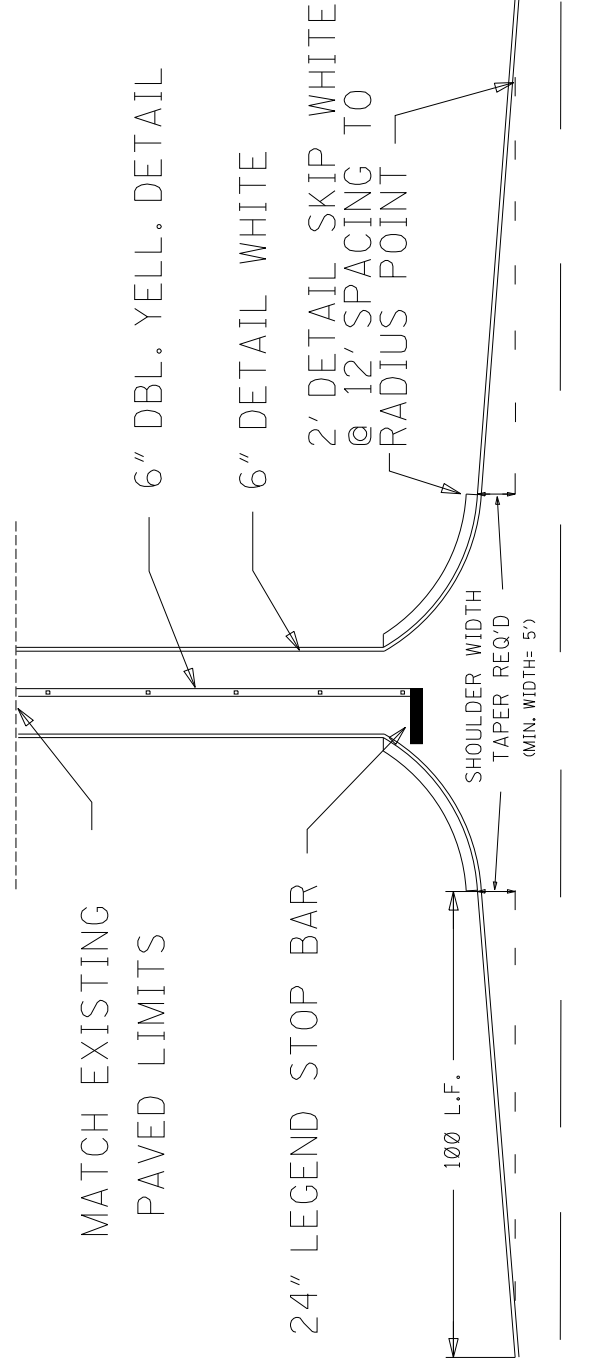
DEPTH = Variable

SR 16 - KEMPER COUNTY
MILLING COUNTY ROADS



 2" MILL AREA

SR 16 - KEMPER COUNTY
COUNTY ROAD PAVING/STRIPING



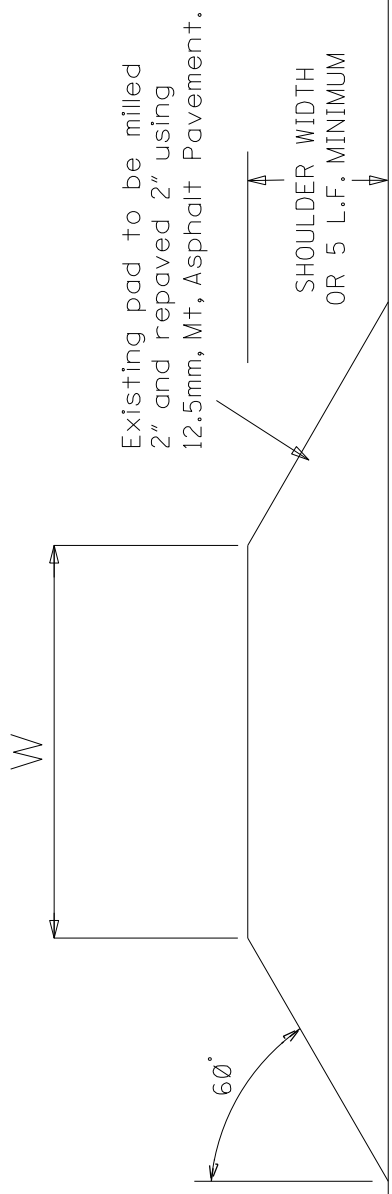
NOTE: 100' TAPERS TO BE CONSTRUCTED WHERE 5' SHOULDER WIDTH IS AVAILABLE AT THE BEGINNING OF LOCAL ROAD RADIUS.

NOTE: ASPHALT PAVEMENT THICKNESS IN TAPER SHALL BE 6".

NOTE: DETAIL SKIP SHALL BE PLACED ON LOCAL ROADS WITH TAPERS.

KEMPER COUNTY
HIGHWAY 16

DRIVEWAY PAD DETAIL

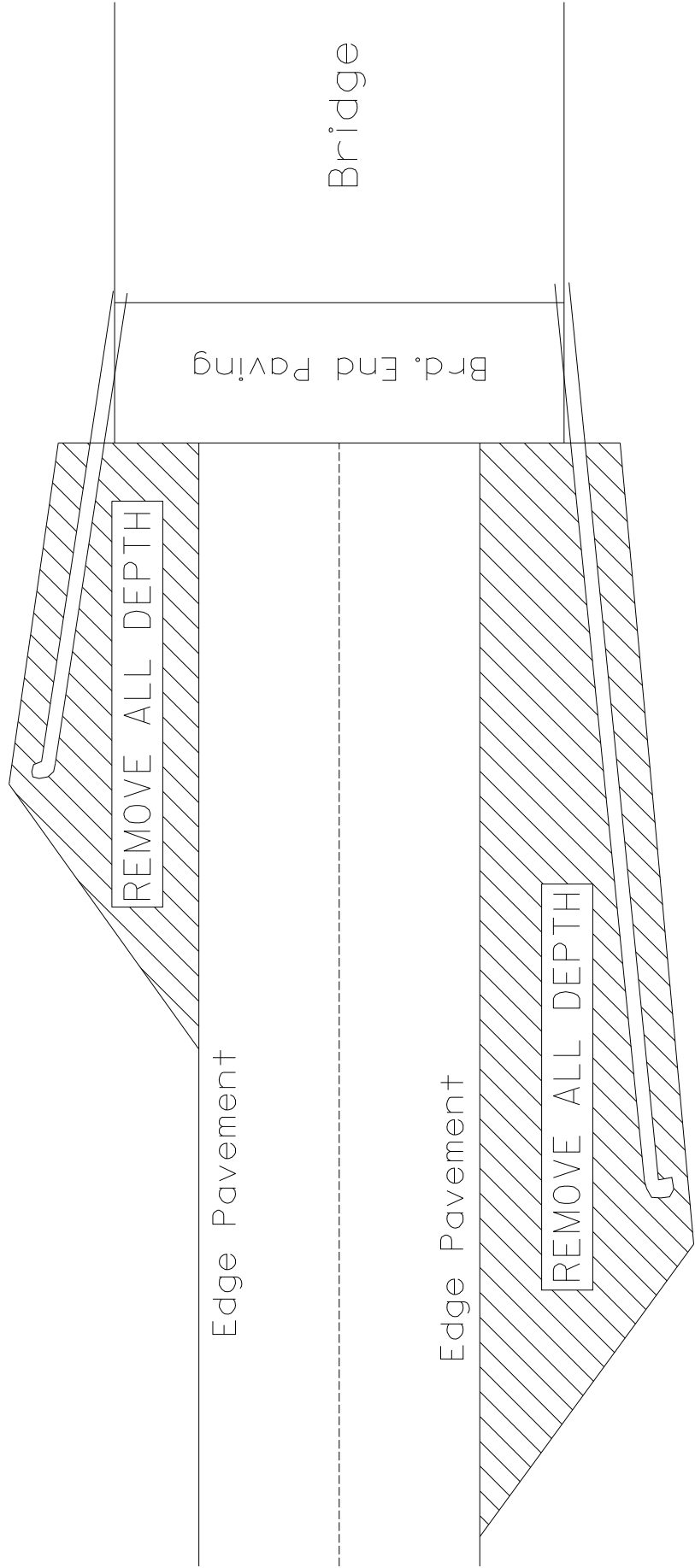


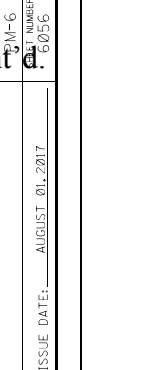
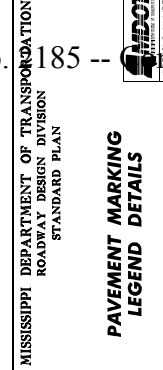
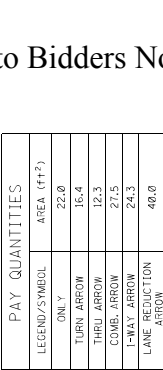
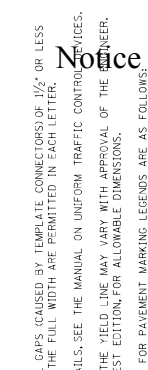
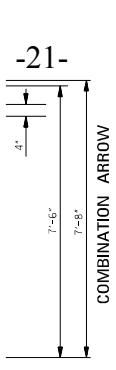
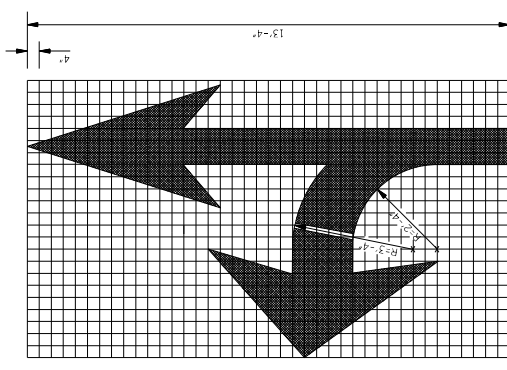
	MAINLINE PAVEMENT
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W = 16' MAX RESIDENTIAL
W = 30'-50' COMMERCIAL

NOTE: THE ASPHALT ON THE EXISTING DRIVEWAY/RAMP PADS ARE TO REMAIN IN THEIR CURRENT SIZE AND LOCATION AND MILLED/OVERLAID. IF, IN THE OPINION OF THE ENGINEER, A PAD SHOULD BE MODIFIED OR REPLACED, PAYMENT WILL BE MADE FOR THE WORK USING THE APPROPRIATE PAY ITEMS. GRANULAR MATERIAL AND/OR STABILIZER AGGREGATE SHOULD BE PLACED AROUND THE PADS AS REQUIRED.

SR 16 - KEMPER COUNTY REMOVE & REPAVE BRIDGE GUARDRAIL PAD





GENERAL NOTES:

1. TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTORS) OF 1/2" OR LESS AND EXTENDING THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
2. FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
3. DIMENSIONS OF THE YIELD LINE MAY VARY WITH APPROVAL OF THE ENGINEER. SEE MUTCD, LATEST EDITION, FOR ALLOWABLE DIMENSIONS.
4. PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

PAY QUANTITIES

LEGEND/SYMBOL	AREA (FT ²)
ONLY	22.0
TURN ARROW	16.4
THRU ARROW	12.3
COMB. ARROW	27.5
1-WAY ARROW	24.3
LANE-REDUCTION ARROW	40.0

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

PAVEMENT MARKING
LEGEND DETAILS

DATE	REVISION

ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 60056

PLAN NUMBER: 185

NOTICE TO BIDDERS NO. 185

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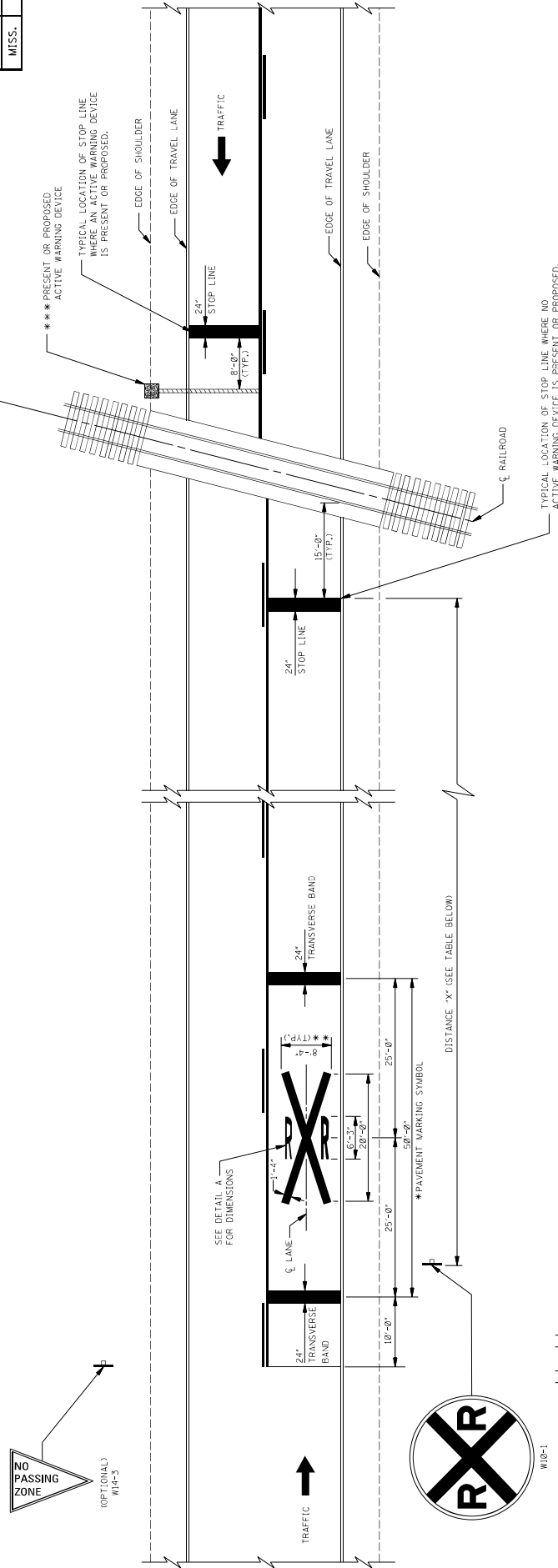
STATE PROJECT NO. MISS.

DATE: AUGUST 01, 2017

ISSUE DATE: AUGUST 01, 2017

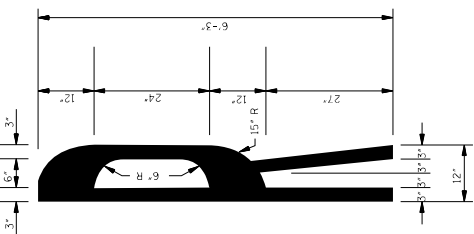
PROJECT NUMBER: 60056

PLAN NUMBER: 185



POSTED SPEED (mph)	⊙ DISTANCE "X" (ft)
20	100
25	100
30	100
35	100
40	125
45	175
50	250
55	325
60	400
65	475

NOTE:
⊙ DISTANCE "X" MAY BE ADJUSTED IF PROHIBITIVE PHYSICAL CONDITIONS EXIST AT THE DESIGNATED DISTANCE.



DETAIL A
STANDARD "R" PAVEMENT MARKING
FOR R X R SYMBOL

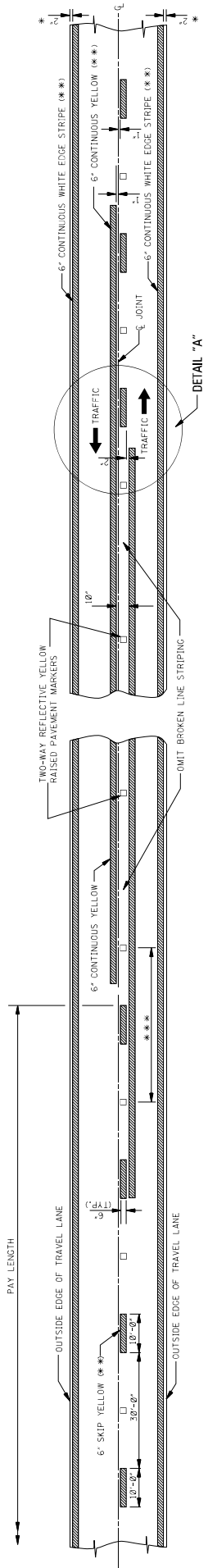
- GENERAL NOTES:
- *1. A PORTION OF THE PAVEMENT MARKING SYMBOL SHOULD BE DIRECTLY OPPOSITE THE ADVANCE WARNING SIGN (W19-1).
 - *2. WIDTH OF R X R SYMBOL MAY VARY ACCORDING TO LANE WIDTH. HOWEVER, ON MULTI-LANE ROADS, THE TRANSVERSE BANDS AND STOP LINE SHOULD BE PLACED IN THE CENTER OF EACH APPROACH LANE. R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.
 - *3. R X R SYMBOL (63.0 (ft²), TRANSVERSE BANDS AND STOP LINE SHOULD BE PAID FOR AS LEGEND, WHITE.
 - *4. REFER TO THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" FOR LOCATION OF PROPOSED WARNING DEVICES AT RAILROAD-HIGHWAY GRADE CROSSINGS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

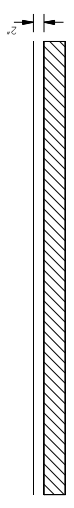
**TYPICAL PLACEMENT OF
WARNING SIGNS AND
PAVEMENT MARKINGS
AT RAILROAD HIGHWAY
GRADE CROSSINGS**

ISSUE DATE: AUGUST 01, 2017

60571



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



DIRECTION OF TRAFFIC

NOTE: THE CRITERIA FOR NO-PASSING ZONES CAN BE FOUND IN THE MOOT ROADWAY DESIGN MANUAL, SECTION 11-1.81.

GENERAL NOTES:

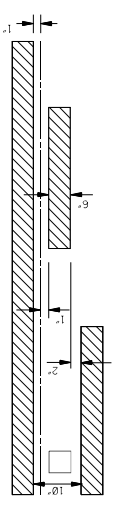
- * 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS, FOR STRIPING ON RUMBLE STRIP SECTIONS REFER TO WK. SHEETS RS-1, RS-2, AND RS-3.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
- *** 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	RURAL AREA
40'-0"	80'-0"
40'-0"	80'-0"
INTERCHANGE LIMITS	1-40'-0"

- 1. NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED THROUGHOUT THE INTERCHANGE AREA BEGINNING 1000' FROM THE INTERCHANGE AND ENDING 1000' FROM THE END OF THE INTERCHANGE. THE TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- 4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE.
- * RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT.
- ** APPROVED SOURCES OF MATERIALS.

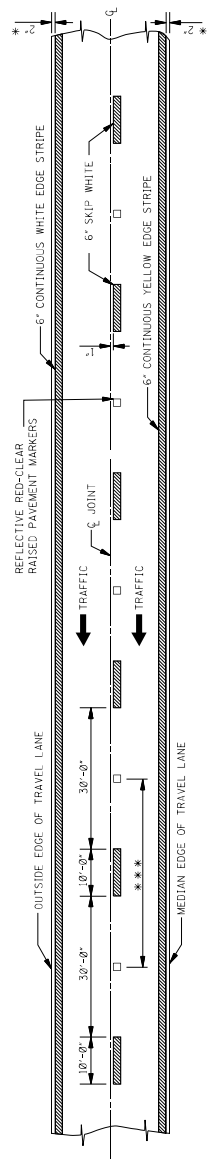


TRAFFIC



TRAFFIC

DETAIL "A"



4-LANE WITH ONE-WAY TRAFFIC

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
DETAILS FOR
2-LANE AND 4-LANE
DIVIDED ROADWAYS**

ISSUE DATE: AUGUST 01, 2017

PLAN NUMBER: 6/051

DATE	REVISION

MDOT PROJECT NO. STP-0024-05(010)/107628-301000

MILL AND OVERLAY SR 16

GUARD RAIL REMOVAL AND INSTALLATION

BRIDGE NO	DIRECTION	REMOVAL (LF)	TYPE "G" BRIDGE END (EA)	TYPE "G", Modified BRIDGE END (EA)	"W" BEAM (LF)	TERMINAL END SECTION, FLARED (EA)	TERMINAL END SECTION, NON-FLARED (EA)	DELINEATORS (WHITE) (EA)	OBJECT MARKERS	
									OM-3R (EA)	OM-3L (EA)
167.6	Eastbound	300	1	1	225		2	17	1	1
167.6	Westbound	300	1	1	225		2	17	1	1
167.9	Eastbound	300	1	1	225		2	17	1	1
167.9	Westbound	300	1	1	225		2	17	1	1
174.5	Eastbound	225			150	2		13		
174.5	Westbound	250			175	2		14		
176.4	Eastbound	250			175	2		9		
176.4	Westbound	250			175	2		9		
172.4	Eastbound	290			215	2		16		
172.4	Westbound	290			215	2		16		
TOTALS		2755	4	4	2005	12	8	145	4	4

* Removal of guardrails, including rails, posts, and terminal ends will be paid for under Pay Item No. 202-B158