

STATE	PROJECT NUMBER	SHEET NO.
MISSISSIPPI	IM-0006-03(039)	1

GENERAL INDEX

INCLUDED THIS PROJECT	BEGIN WITH SHEET
<input checked="" type="checkbox"/> ROADWAY	1
<input type="checkbox"/> PERMANENT SIGNS	1001
<input type="checkbox"/> TRAFFIC SIGNALS	2001
<input type="checkbox"/> ITS COMPONENTS	3001
<input type="checkbox"/> LIGHTING	4001
<input type="checkbox"/> (RESERVED)	5001
<input checked="" type="checkbox"/> ROADWAY STANDARD DWGS	6001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (LRFD)	7001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (STD. SPEC.)	7501
<input type="checkbox"/> BRIDGE	8001
<input type="checkbox"/> CROSS SECTIONS	9001

STATE OF MISSISSIPPI
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE OF PROPOSED STATE HIGHWAY
FEDERAL AID PROJECT NO. IM-0006-03(039)

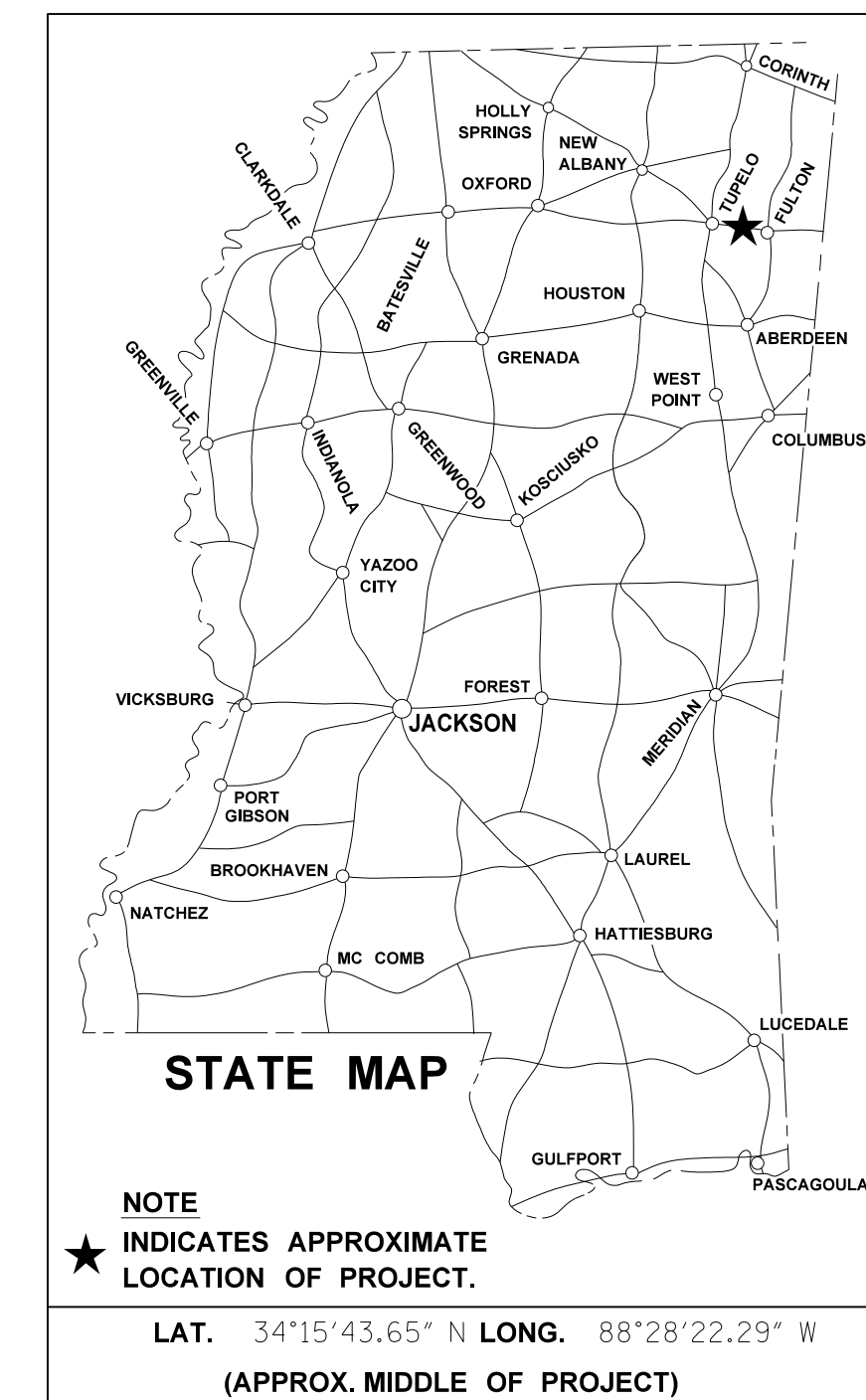
I-22 from Lee CL to SR 25 South (Fulton)

FMS CON. NO. 107298/ 301000

ITAWAMBA COUNTY

SCALES

PLAN	1 IN. = 100 FT.
PROFILE	HOR. 1 IN. = 100 FT.
	VERT. 1 IN. = 10 FT.
LAYOUT	1 IN. = FT.



BRIDGE STRUCTURES REQ'D.

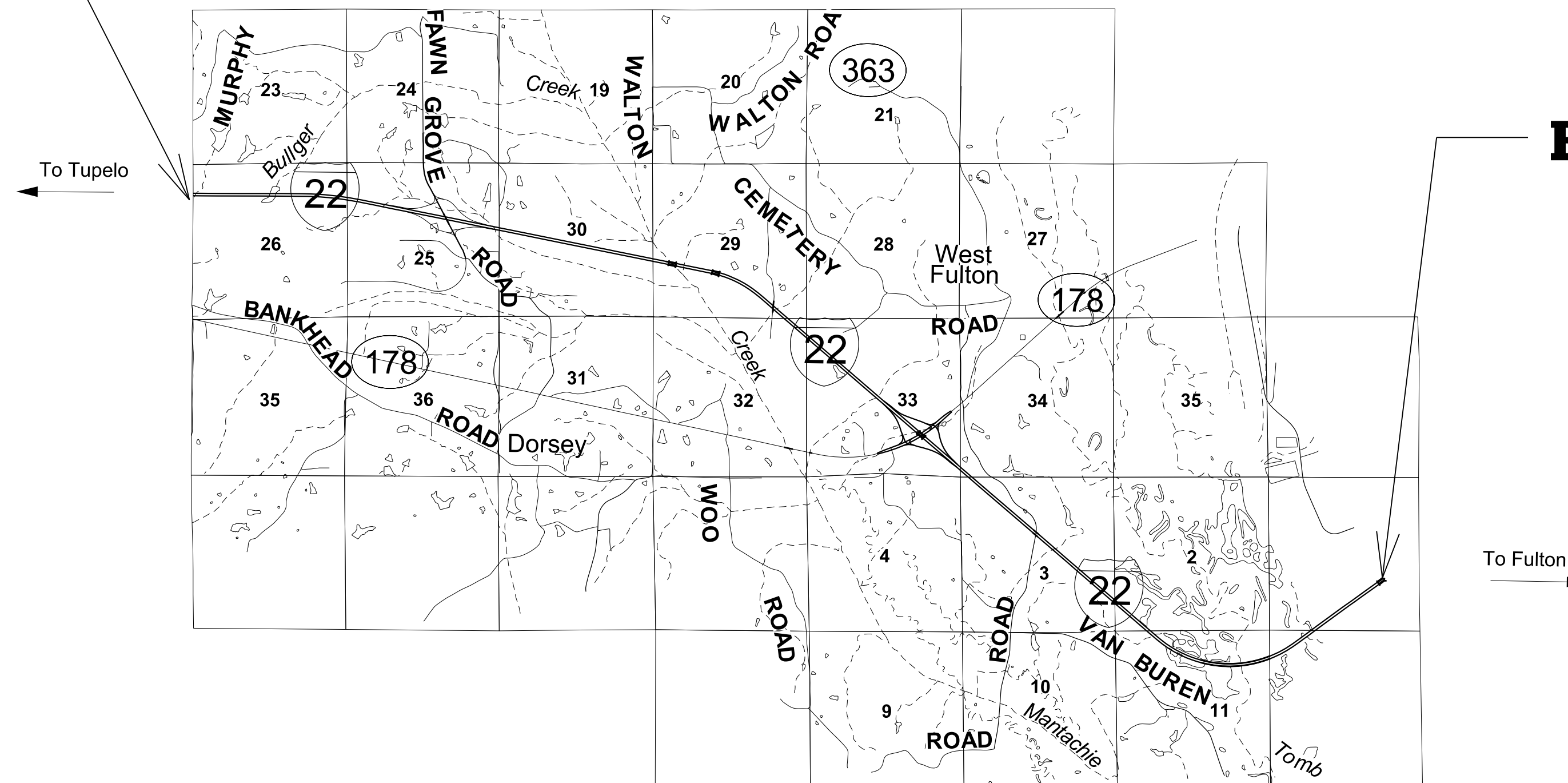
NONE

B.O.P STA. 531 + 36

BOX BRIDGES REQ'D.

NONE

E.O.P STA. 970 + 75



DESIGN CONTROL

MPH = V (SPEED DESIGN)

ADT () = : ADT () =

DHV = : D = % T = %

PERMITS ACQUIRED BY MDOT

WETLANDS AND WATERS PERMITS

	WATERS	WETLANDS
NATIONWIDE #14	N	N
NATIONWIDE (OTHER)*	N	N
GENERAL*	N	N
INDIVIDUAL (404)*	N	N

STORMWATER PERMIT

Y REQUIRED CNOI SUBMITTED BY MDOT (DISTURBED AREA = 5 ACRES)

S REQUIRED SCNOI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES)

N NO STORMWATER PERMIT REQUIRED (<1 ACRE)

APPROVED BY: _____

CONVENTIONAL SYMBOLS

COUNTY LINE	-----
TOWN CORPORATION LINE	-----
SECTION LINE	-----
EXISTING ROAD OR TRAVELED WAY	-----
PROPOSED ROAD OR TRAVELED WAY	-----
RAILROAD	-----
SURVEY LINE	-----
BRIDGES	-----

EQUATIONS

NONE

EXCEPTIONS

NONE

LENGTH DATA

LENGTH OF ROADWAY	40184 FT.	7.61 MI.
LENGTH OF BRIDGES	3784 FT.	.72 MI.
LENGTH OF PROJECT (NET)		8.33 MI.
LENGTH OF EXCEPTIONS	0 FT.	0 MI.
LENGTH OF PROJECT (GROSS)		8.33 MI.

P S & E DATE: 12 /11 /19

APPROVED:

DEPUTY EXECUTIVE DIRECTOR / CHIEF ENGINEER

EXECUTIVE DIRECTOR



001 00 AMPM DGNF LLENAM MISSISSIPPI DEPARTMENT OF TRANSPORTATION

STATE	PROJECT NO.
MISS.	IM-0006-03(039)

DESCRIPTION OF SHEET

WKG. SH.
NO. NO.

TITLE SHEET (1)			1
DETAILED INDEX & GENERAL NOTES (1)			
DETAILED INDEX & GENERAL NOTES	DI-1		2
TYPICAL SECTION SHEETS (3)			
TYPICAL OVERLAY SECTION FOR MAINLINE I-22	TS-1		3
TYPICAL OVERLAY SECTION FOR MAINLINE FAWN GROVE RD	TS-2		4
TYPICAL OVERLAY SECTION FOR RAMPS	TS-3		5
QUANTITY SHEETS (2)			
SUMMARY OF QUANTITIES	SQ-1		6
SUMMARY OF QUANTITIES	SQ-2		7
SPECIAL DESIGN SHEETS (5)			
DETAIL OF CONSTRUCTION SIGNING	CS-1		8
DETAIL OF CONSTRUCTION SIGNING	CS-2		9
DETAIL OF CONSTRUCTION SIGNING	CS-3		10
DETAIL OF MILLING AT BRIDGES	DM-1		11
DETAIL OF MILLING TRANSITON AT FAWN GROVE BRIDGE	DM-2		12
STANDARD DRAWINGS - ROADWAY SHEETS (13)			
PAVEMENT MARKING DETAILS FOR 2-LANE AND 4-LANE DIVIDED HIGHWAYS	8/01/17	PM-1	6051
PAVEMENT MARKING DETAILS FOR INTERCHANGE ENTRANCE RAMPS	8/01/17	PM-3	6053
PAVEMENT MARKING DETAILS FOR INTERCHANGE EXIT RAMPS	8/01/17	PM-4	6054
PAVEMENT MARKING LEGEND DETAILS	8/01/17	PM-6	6056
DETAIL OF RUMBLE STRIPES	8/01/17	RS-2	6065
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOUSURE) (EXTENDED PERIOD)	8/01/17	TCP-4	6354
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (WORK DAY ONLY)	8/01/17	TCP-5	6355
HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS	8/01/17	TCP-8	6358
TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO-LANE ROADS	8/01/17	TCP-9	6359
DETAILS OF OUTSIDE LANE CLOSURE AT EXIT AND ENTRANCE RAMPS	8/01/17	TCP-10	6360
TRAFFIC CONTROL PLANS UNEVEN PAVEMENT DETAILS	8/01/17	TCP-12	6362
TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4- LANE DIVIDED HIGHWAYS	8/01/17	TCP-13	6363
TRAFFIC CONTROL DETAIL DRUM PLACEMENT AND SHOULDER CLOSURE	8/01/17	TCP-16	6366

GENERAL NOTES

- (1) THE LOCATION AND SPACING OF SIGNS, SHOWN ON THE TRAFFIC CONTROL PLANS, ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.
- (2) MAXIMUM LANE CLOSURE ALLOWED IS 3 MILES. A MINIMUM INTERVAL OF 3 MILES IS REQUIRED BETWEEN WORK ZONE LANE CLOSURES SET IN ADJACENT LANES. A MINIMUM INTERVAL OF 2 MILES IS REQUIRED BETWEEN WORK ZONE LANE CLOSURES LOCATED IN THE SAME LANE.
- (3) FLUORESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.
- (4) BIDDERS ARE ADVISED THAT HARD COPIES OF ANY ADDENDA FOR THIS PROJECT WILL NO LONGER BE MAILED. ALL ADDENDA FOR THIS PROJECT WILL BE POSTED TO www.mdot.ms.gov UNDER THE PROPOSAL ADDENDA COLUMN. IT IS THE BIDDER'S RESPONSIBILITY TO CHECK AND SEE IF ANY ADDENDA HAVE BEEN POSTED FOR THIS PROJECT. PLEASE CONTACT CONTRACT ADMINISTRATION DIVISION AT 601-359-7700 FOR ANY QUESTIONS REGARDING ELECTRONIC ADDENDA.
- (5) STORAGE OF FLAMMABLE MATERIALS WILL NOT BE ALLOWED UNDER ANY BRIDGE STURCTURES.

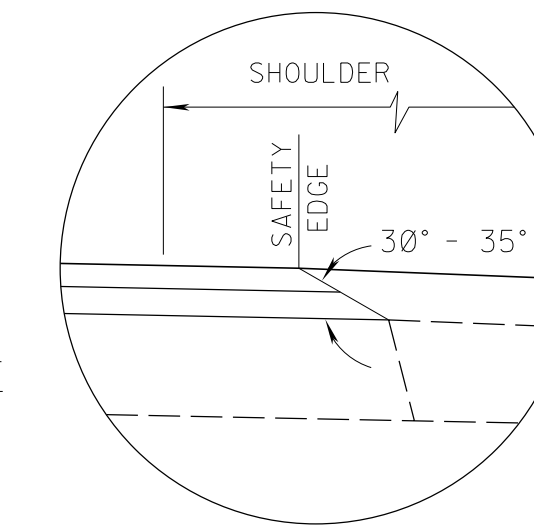
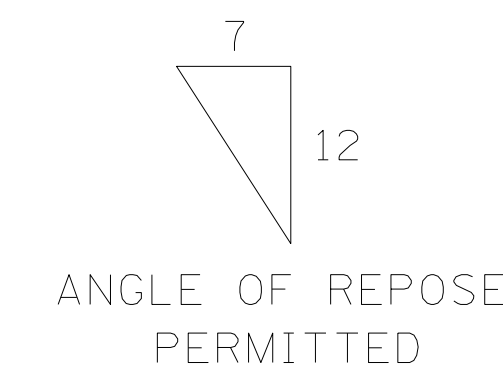
PS & E PLANS-DATE...12/11/19		
FMS CON. # 107298/301000		
REVISIONS		
DATE	SHEET NO.	BY

TOTAL NUMBER OF SHEETS (25)

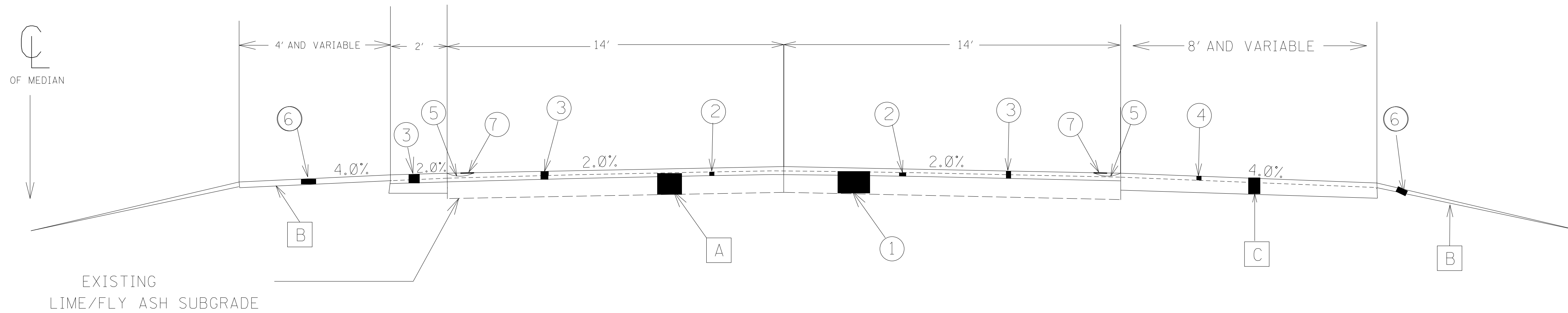
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
DETAILED INDEX	
PROJ. NO.: IM-0006-03(039) COUNTY: ITAWAMBA	
WORKING NUMBER DI-1	SHEET NUMBER 2
FILENAME: _____ DESIGN TEAM _____ CHECKED _____ DATE _____	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 001 00 AMPM DGNFL LENAME
 00DDYY

TYPICAL OVERLAY SECTION MAINLINE I-22
DIVIDED HIGHWAY



SAFETY EDGE REQ'D
TOP 2 LIFTS ONLY
(NOT A PAY ITEM)
OVERLAY



- NOTE:
1. PRIOR TO PLACING ASPHALT, THE SHOULDERS ARE TO BE BLADED FOR A MINIMUM DISTANCE OF ONE FOOT BEYOND THE EXISTING PAVEMENT EDGE TO REMOVE VEGETATION AND EXCESS SHOULDER MATERIAL. (NOT A PAY ITEM)
 2. NO GRANULAR MATERIAL SHALL BE PLACED DIRECTLY UPON THE FINISHED SURFACE COURSE.

STATION
531+36-970+75 LT & RT LN

FAILED AREAS

- STATION 551+84-552+54 = 70' X 14 RT OF RT LANE
- STATION 701+90-702+78 = 88' X 14 RT OF RT LANE
- STATION 719+91-720+40 = 49' X 14 RT OF RT LANE

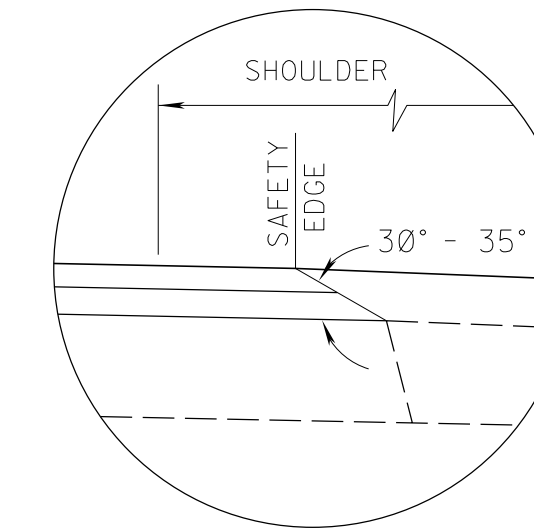
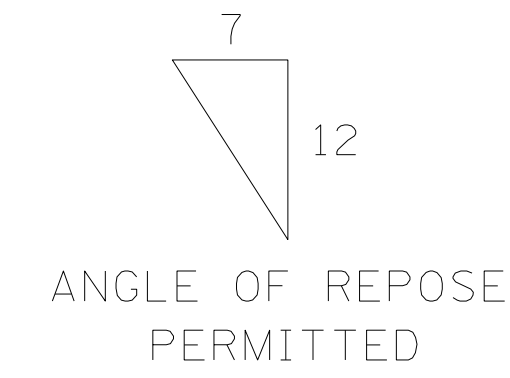
- 1 REMOVE AND UNDERCUT FAILED AREAS AS DIRECTED BY THE ENGINEER. REMOVAL OF ASPHALT PAVEMENT TO BE PAID FOR UNDER PAY ITEM NO. 202-B007. ANY ADDITIONAL REMOVAL TO BE PAID FOR UNDER EXCESS EXCAVATION, FM, AH, PAY ITEM NO. 203-G001. BACKFILL WITH 19-MM,HT, ASPHALT MIXTURE, PAY ITEM NO. 403-A004, FOR A MAX. DEPTH OF 1'. IF ADDITIONAL BACKFILL MATERIAL IS REQUIRED USE SIZE 825 B CRUSHED STONE BASE, LVM, PAY ITEM NO. 304-H003, OR 3/4" AND DOWN CRUSHED STONE BASE, LVM, PAY ITEM NO. 304-H001, OR SIZE 610 CRUSHED STONE BASE, LVM, PAY ITEM NO. 304-H002.
- 2 2" FINE MILLING OF BITUMINOUS PAVEMENT, ALL DEPTHS PAY ITEM NO. 406-D001.
- 3 2 LIFTS @ 1 1/2" ASPHALT PAVEMENT, 9.5-MM STONE MATRIX ASPHALT, REQUIRED ON SURFACE COURSE, PAY ITEM NO. 405-A002.
- 4 1" ASPHALT PAVEMENT, ST, 9.5-MM ASPHALT, REQUIRED ON SURFACE COURSE, PAY ITEM NO. 403-A015.
- 5 RUMBLE STRIPS, GROUND IN, PAY ITEM NO. 423-A001. * SEE SHEET RS-2 FOR ADDITIONAL DETAILS.
- 6 GRANULAR MATERIAL, LVM, CLASS 3, GROUP D, REQUIRED ON SHOULDERS AS DIRECTED BY THE ENGINEER. PAY ITEM NO. 304-A003.
- 7 6" THERMOPLASTIC STRIPE SHALL BE APPLIED OVER GROUND IN RUMBLE STRIPS.

VARIABLE DEPTH

- A 13" AND VARIABLE ASPHALT IN PLACE
- B GRANULAR MATERIAL IN PLACE.
- C VARIABLE ASPHALT IN PLACE

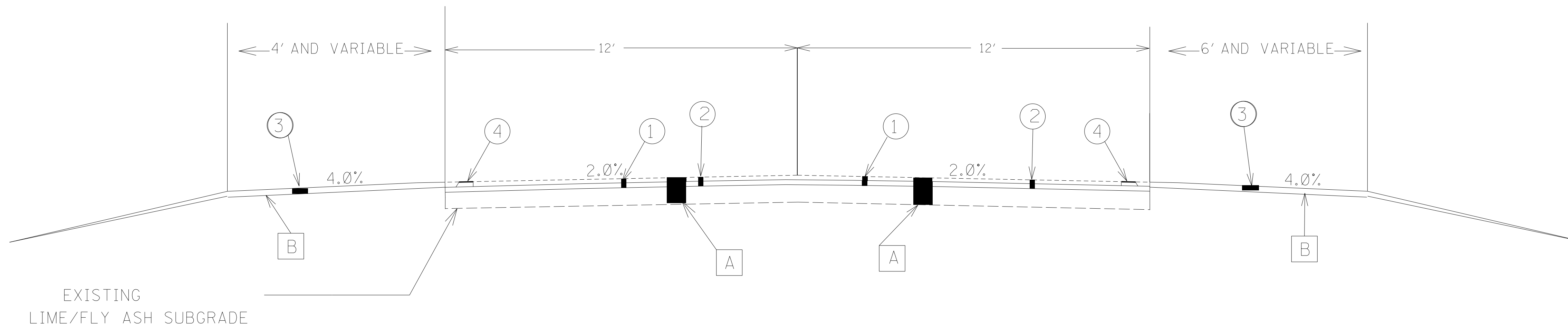
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTION MAINLINE I-22	
PROJ. NO.: IM-0006-03(039) COUNTY: ITAWAMBA	
FILENAME: IM-0006-03(039).dgn DESIGN TEAM UPDATE CHECKED UPDATE DATE UPDATE	WORKING NUMBER TS-1 SHEET NUMBER 3

TYPICAL OVERLAY SECTION MAINLINE FAWN GROVE RD



**SAFETY EDGE REQ'D
TOP 2 LIFTS ONLY
(NOT A PAY ITEM)
OVERLAY**

- NOTE:
1. PRIOR TO PLACING ASPHALT, THE SHOULDERS ARE TO BE BLADED FOR A MINIMUM DISTANCE OF ONE FOOT BEYOND THE EXISTING PAVEMENT EDGE TO REMOVE VEGETATION AND EXCESS SHOULDER MATERIAL. (NOT A PAY ITEM)
 2. NO GRANULAR MATERIAL SHALL BE PLACED DIRECTLY UPON THE FINISHED SURFACE COURSE.



STATION
17+68-40+77 LT&RT

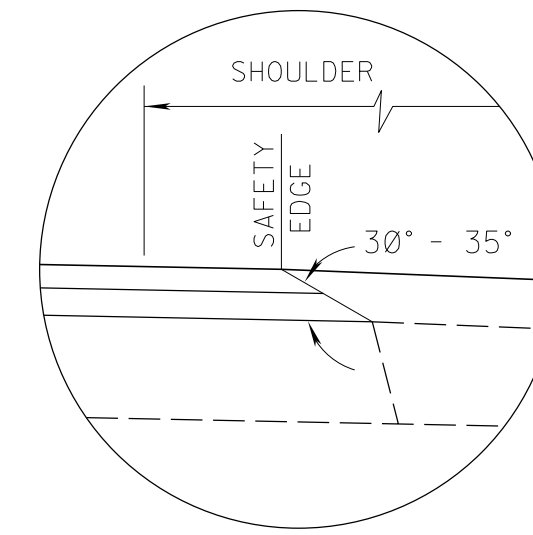
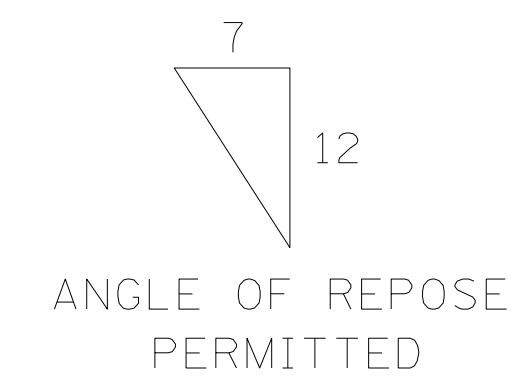
VARIABLE DEPTH

- A ASPHALT IN PLACE
- B GRANULAR MATERIAL IN PLACE.

- ① 2" FINE MILLING OF BITUMINOUS PAVEMENT, ALL DEPTHS PAY ITEM NO. 406-D001.
- ② 2" ASPHALT PAVEMENT, HT, 12.5-MM ASPHALT, REQUIRED ON SURFACE COURSE, PAY ITEM NO. 403-A001.
- ③ GRANULAR MATERIAL, LVM, CLASS 3, GROUP D, REQUIRED ON SHOULDERS AS DIRECTED BY THE ENGINEER. PAY ITEM NO. 304-A003.
- ④ 6" THERMOPLASTIC STRIPE.

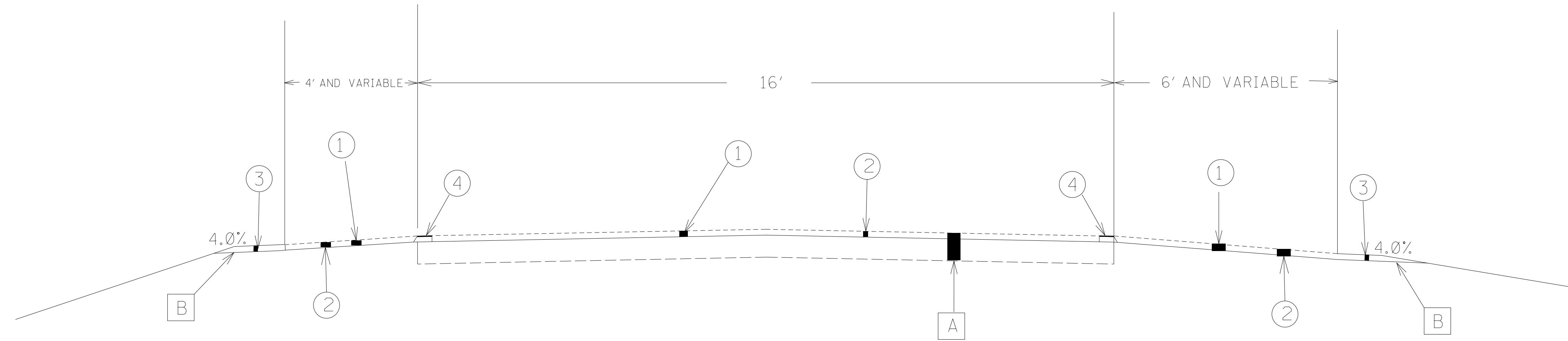
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTION FAWN GROVE	
PROJ. NO.: IM-0006-03(039) COUNTY: ITAWAMBA	
DATE: _____ DESIGN TEAM: _____	FILENAME: _____ CHECKED: _____ DATE: _____
WORKING NUMBER TS-2	SHEET NUMBER 4

STATE	PROJECT NO.
MISS.	IM-0006-03(039)



**SAFETY EDGE REQ'D
TOP 2 LIFTS ONLY
(NOT A PAY ITEM)
OVERLAY**

TYPICAL OVERLAY SECTION OF RAMPS



NOTE:

1. PRIOR TO PLACING ASPHALT, THE SHOULDERS ARE TO BE BLADED FOR A MINIMUM DISTANCE OF ONE FOOT BEYOND THE EXISTING PAVEMENT EDGE TO REMOVE VEGETATION AND EXCESS SHOULDER MATERIAL. (NOT A PAY ITEM)
2. NO GRANULAR MATERIAL SHALL BE PLACED DIRECTLY UPON THE FINISHED SURFACE COURSE.

FAWN GROVE RD
 S/E RAMP - 1,938'
 S/W RAMP - 2,385'
 N/E RAMP - 2,387'
 N/W RAMP - 2,203'

HWY 178
 S/E RAMP - 2,185'
 S/W RAMP - 1,895'
 N/E RAMP - 1,832'
 N/W RAMP - 2,162'

- ① 2" FINE MILLING OF BITUMINOUS PAVEMENT, ALL DEPTHS PAY ITEM NO. 406-D001.
- ② 2" ASPHALT PAVEMENT, HT, 12.5-MM ASPHALT, REQUIRED ON SURFACE COURSE, PAY ITEM NO. 403-A001.
- ③ GRANULAR MATERIAL, LVM, CLASS 3, GROUP D, REQUIRED ON SHOULDERS AS DIRECTED BY THE ENGINEER. PAY ITEM NO. 304-A003.
- ④ 6" THERMOPLASTIC STRIPE.

- A VAR. THICKNESS ASPHALT PAVEMENT IN PLACE.
- B GRANULAR MATERIAL IN PLACE.


BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
REVISION		<p align="center">TYPICAL SECTION RAMPS</p> <p>PROJ. NO.: IM-0006-03(039) COUNTY: ITAWAMBA</p>	
DATE			
DESIGN TEAM	CHECKED	DATE	<p>WORKING NUMBER TS-3</p> <p>SHEET NUMBER 5</p>

STATE	PROJECT NO.
MISS	IM-0006-03(039)

SUMMARY OF QUANTITIES (SHEET 1)

PAY ITEM NO.	PAY ITEM	UNIT	ITAWAMBA : 107298-301000	
			Prelim	Final
202-B007	Removal of Asphalt Pavement, All Depths	SY	922	
202-B240	Removal of Traffic Stripe	LF	16,028	
203-G001	Excess Excavation, FM, AH	CY	308	
304-A003	Granular Material, LVM, Class 3, Group D	CY	14,362	①
304-H001	3/4" and Down Crushed Stone Base, LVM	CY	461	①
	OR			
304-H002	Size 610 Crushed Stone Base, LVM	CY	461	①
	OR			
304-H003	Size 825B Crushed Stone Base, LVM	CY	461	①
503-C010	Saw Cut, Full Depth	LF	1,128	
403-A001	12.5-mm, HT, Asphalt Pavement	TON	7,975	
403-A004	19-mm, HT, Asphalt Pavement	TON	966	
403-A015	9.5-mm, ST, Asphalt Pavement	TON	4,204	
403-S002	Joint Sealant	LF	253,047	②
405-A002	Stone Matrix Asphalt, 9.5 mm Mixture	TON	48,456	
406-D001	Fine Milling of Bituminous Pavement, All Depths	SY	362,724	
407-A001	Asphalt for Tack Coat	GAL	71,112	
423-A001	Rumble Strips, Ground In	MI	31	
618-A001	Maintenance of Traffic	LS	1	
618-B001	Additional Construction Signs	SF	1	
620-A001	Mobilization	LS	1	
619-A1001	Temporary Traffic Stripe, Continuous White	MI	57	
619-A2001	Temporary Traffic Stripe, Continuous Yellow	MI	57	
619-A3001	Temporary Traffic Stripe, Skip White	MI	48	
619-A5001	Temporary Traffic Stripe, Detail	LF	35,349	
619-A6002	Temporary Traffic Stripe, Legend	LF	785	
907-624-A002	6" Inverted Profile Thermoplastic Traffic Stripe, Skip White	LF	7,915	
907-624-B002	6" Inverted Profile Thermoplastic Traffic Stripe, Continuous White	LF	8,890	
907-624-D002	6" Inverted Profile Thermoplastic Traffic Stripe, Continuous Yellow	LF	8,890	
	OR			
628-G001	6" High Performance Cold Plastic Traffic Stripe, Skip White	LF	7,915	
628-H001	6" High Performance Cold Plastic Traffic Stripe, Continuous White	LF	8,890	
628-J001	6" High Performance Cold Plastic Traffic Stripe, Continuous Yellow	LF	8,890	
626-A004	6" Thermoplastic Traffic Stripe, Skip White	MI	16	
626-C004	6" Thermoplastic Edge Stripe, Continuous White	MI	19	
626-F003	6" Thermoplastic Edge Stripe, Continuous Yellow	MI	19	


- ① INCLUDES 50% FOR SHRINKAGE.
- ② INCLUDES CENTERLINE AND OUTSIDE JOINTS.

Revision	By	MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
		SUMMARY OF QUANTITIES	
Date	Design Team		Working Number
		PROJ NO: IM-0006-03(039) COUNTY: ITAWAMBA FILENAME: SQS	SQ-1 Sheet Number 6
		Checked	Date

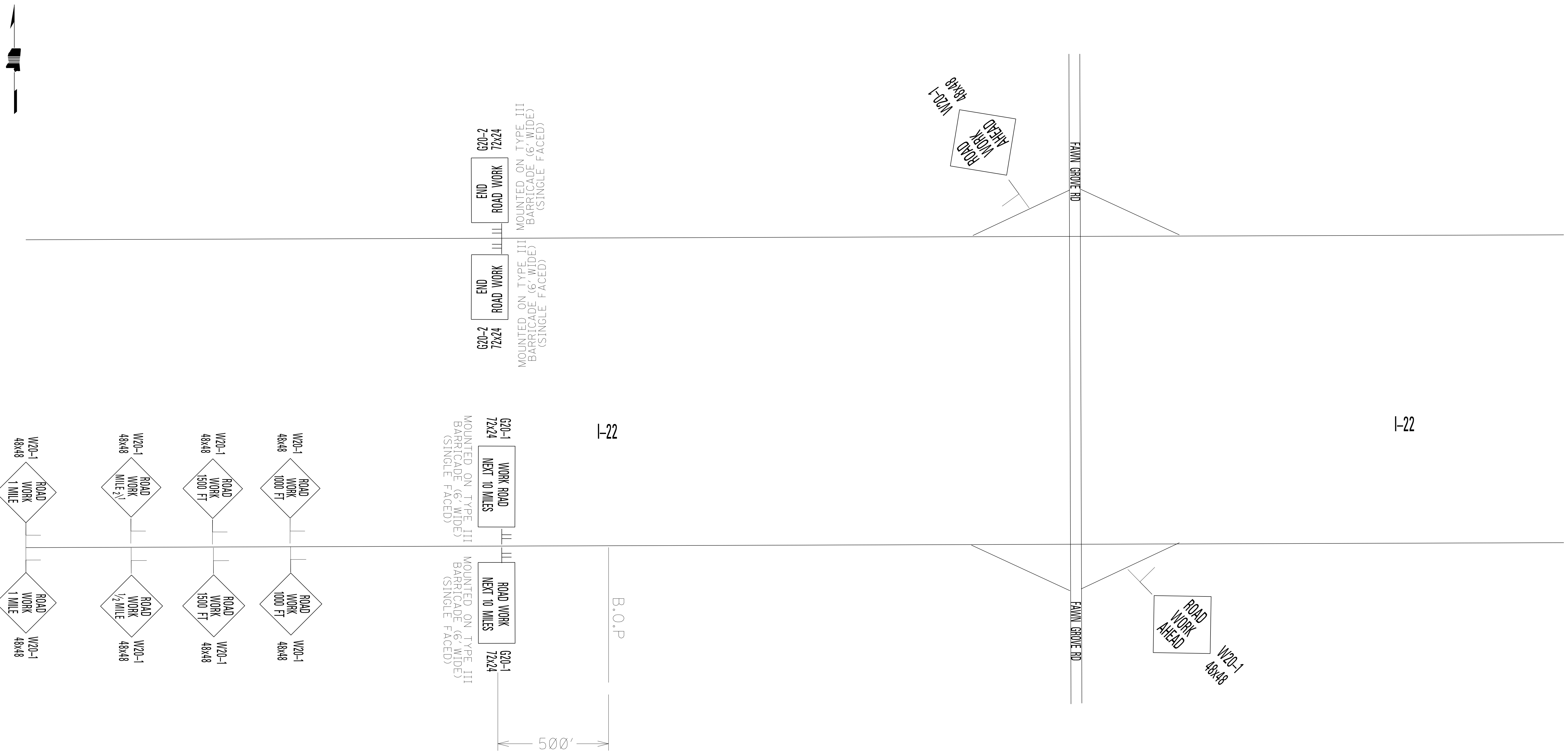
STATE	PROJECT NO.
MISS	IM-0006-03(039)

SUMMARY OF QUANTITIES (SHEET 2)

PAY ITEM NO.	PAY ITEM	UNIT	ITAWAMBA : 107298-301000	
			Prelim	Final
626-G002	Thermoplastic Detail Stripe, White	LF	11,783	
626-H004	Thermoplastic Legend, White	SF	135	
626-H005	Thermoplastic Legend, White	LF	349	
627-K001	Red-Clear Reflective High Performance Raised Markers	EA	2,667	


Revision	MISSISSIPPI DEPARTMENT OF TRANSPORTATION		
	SUMMARY OF QUANTITIES		
Date	PROJ NO: IM-0006-03(039)	Working Number	SQ-2
	COUNTY: ITAWAMBA	Sheet Number	7
	FILENAME: SQS	Design Team	Checked
			Date

STATE	PROJECT NO.
MISS.	IM-0006-03(039)

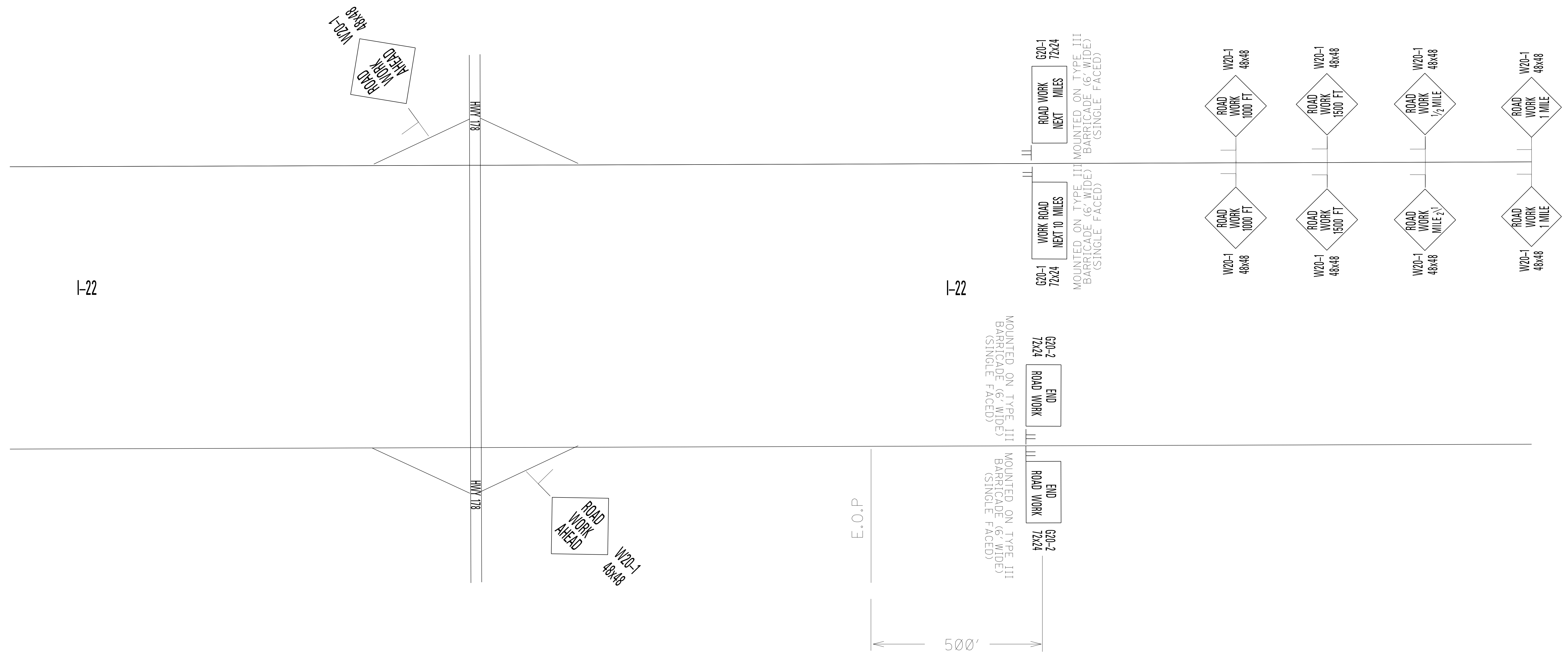


NOTES:

1. ALL SIGNS, BARRICADES AND CHANNELIZING DEVICES ARE TO BE PAID FOR UNDER MAINTENANCE OF TRAFFIC. PAY ITEM NO. 618-A001.
2. THE LOCATION AND SPACING OF SIGNS AS SHOWN ON THIS SHEET ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.
3. FLUORESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
DETAIL OF CONSTRUCTION SIGNING	
	
PROJ. NO.: IM-0006-03(039) COUNTY: ITAWAMBA	
WORKING NUMBER CS-1	SHEET NUMBER 8
FILENAME: _____ DESIGN TEAM _____ CHECKED _____ DATE _____	DATE _____

STATE	PROJECT NO.
MISS.	IM-0006-03(039)



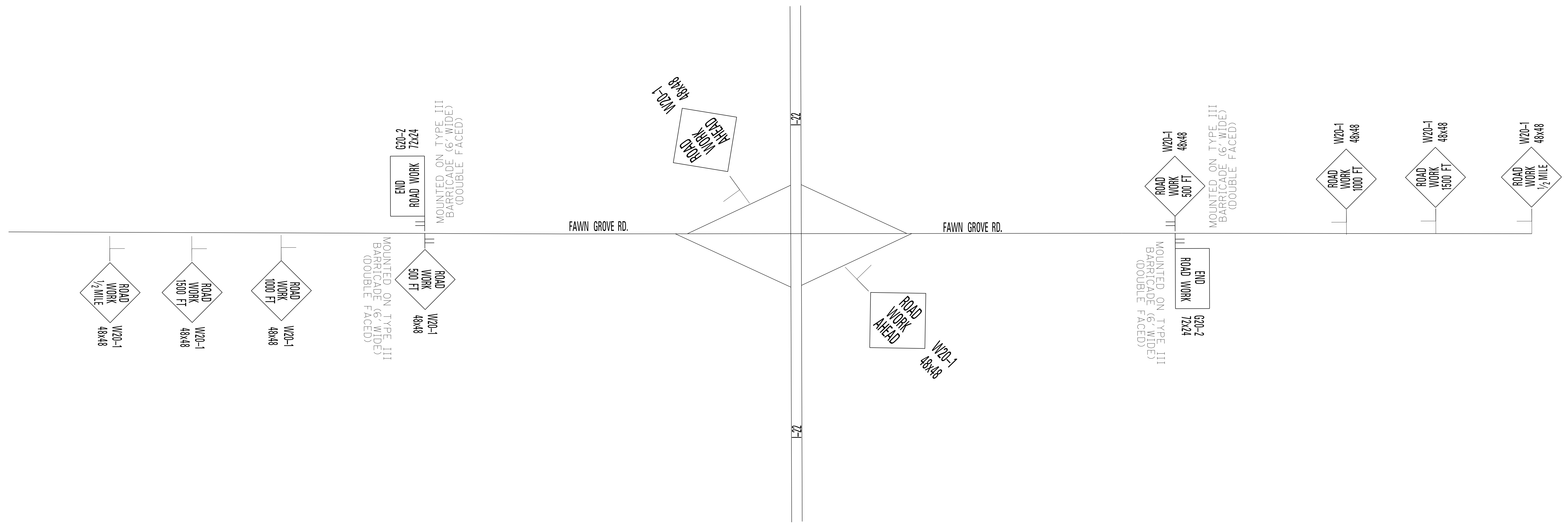
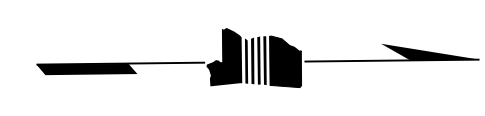
NOTES:

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2. THE LOCATION AND SPACING OF SIGNS AS SHOWN ON THIS SHEET ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.
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MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
DETAIL OF CONSTRUCTION SIGNING	
PROJ. NO.: IM-0006-03(039) COUNTY: ITAWAMBA	
WORKING NUMBER CS-2	SHEET NUMBER 9
FILENAME: _____ DESIGN TEAM _____ CHECKED _____ DATE _____	DATE _____ REVISION _____ BY _____

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 PLANNING DIVISION
 ROADWAY DESIGN
 001 00 AMPM DGNF LLENAM
 000000

STATE	PROJECT NO.
MISS.	IM-0006-03(039)



NOTES:

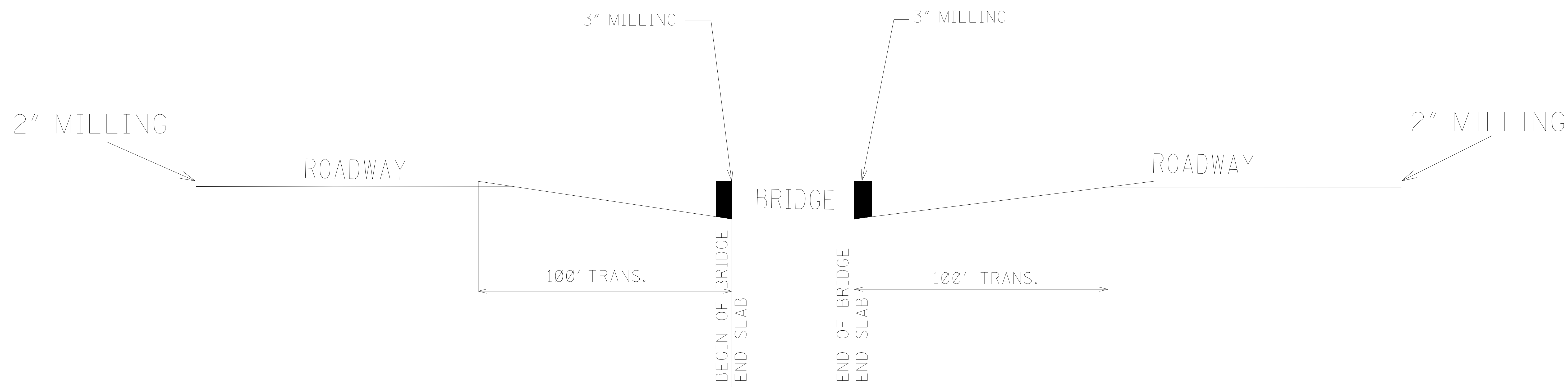
1. ALL SIGNS, BARRICADES AND CHANNELIZING DEVICES ARE TO BE PAID FOR UNDER MAINTENANCE OF TRAFFIC. PAY ITEM NO. 618-A001.
2. THE LOCATION AND SPACING OF SIGNS AS SHOWN ON THIS SHEET ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.
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MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
DETAIL OF CONSTRUCTION SIGNING	
PROJ. NO.: IM-0006-03(039) COUNTY: ITAWAMBA	
FILENAME: DESIGN TEAM _____ CHECKED _____ DATE _____	WORKING NUMBER CS-3 SHEET NUMBER 10


MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 DIVISION OF DESIGN
 004 00 AMPM DGNF LLENAM

STATE	PROJECT NO.
MISS.	IM-0006-03(039)

DETAIL OF MILLING TRANSITIONS AT BRIDGES

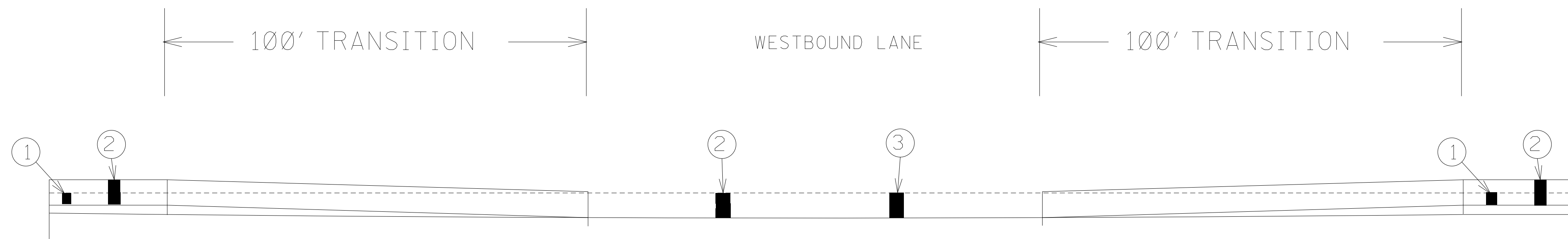


001 00 AMPM DGNF LLENAM MISSISSIPPI DEPARTMENT OF TRANSPORTATION

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
DETAIL OF MILLING AT BRIDGES	
PROJ. NO.: IM-0006-03(039)	
COUNTY: ITAWAMBA	
DATE	FILENAME: _____
DESIGN TEAM	CHECKED _____ DATE _____
 WORKING NUMBER DM-1 SHEET NUMBER 11	

STATE	PROJECT NO.
MISS.	IM-0006-03(039)

DETAIL OF MILLING TRANSITION AT FAWN GROVE BRIDGE



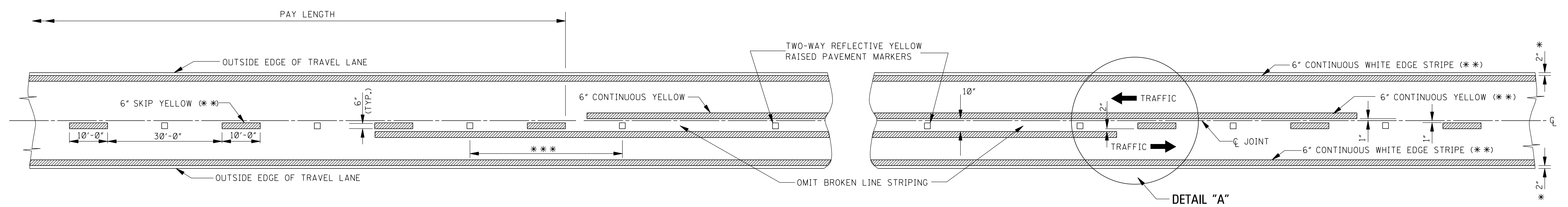
- ① 2" FINE MILLING OF BITUMINOUS PAVEMENT, ALL DEPTHS PAY ITEM NO. 406-D001.
- ② 2 LIFTS @ 1 1/2" ASPHALT PAVEMENT, 9.5-MM STONE MATRIX ASPHALT, REQUIRED ON SURFACE COURSE, PAY ITEM NO. 405-A002.
- ③ 3" COLD MILLING OF BITUMINOUS PAVEMENT, ALL DEPTHS PAY ITEM NO. 406-A002.

ROADWAY DESIGN DIVISION
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

001 00 AMPM DGNFILENAME

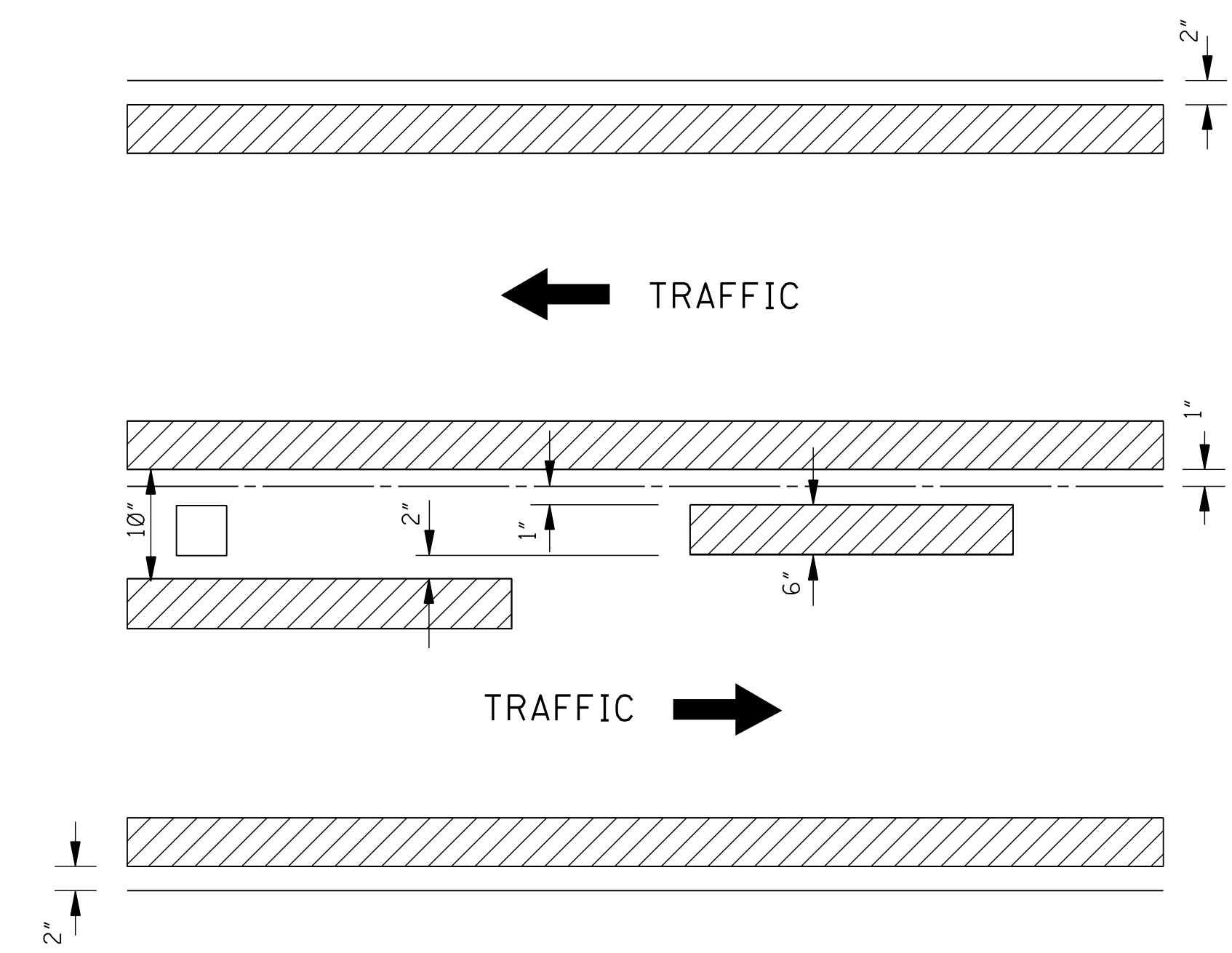
		MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
		DETAIL OF MILLING AT FAWN GROVE	
		PROJ. NO.: IM-0006-03(039) COUNTY: ITAWAMBA	
		FILENAME: _____ DESIGN TEAM _____ CHECKED _____ DATE _____	WORKING NUMBER DM-2 SHEET NUMBER 12





TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)

NOTE: THE CRITERIA FOR NO-PASSING ZONES CAN BE FOUND IN THE MDT ROADWAY DESIGN MANUAL, SECTION 11-1.01.



DETAIL "A"

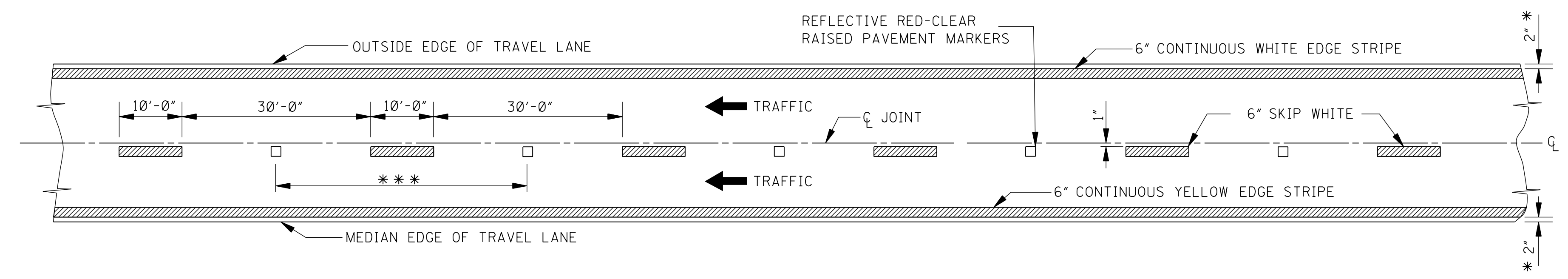
GENERAL NOTES:

- * 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS. FOR STRIPING ON RUMBLE STRIP SECTIONS REFER TO WK. SHEETS RS-1, RS-2, AND RS-3.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
- *** 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

	URBAN AREA (ft-in)	RURAL AREA (ft-in)
TANGENT SECTIONS	40'-0"	80'-0"
HORIZONTAL CURVES	40'-0"	40'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

† NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDT "APPROVED SOURCES OF MATERIALS."

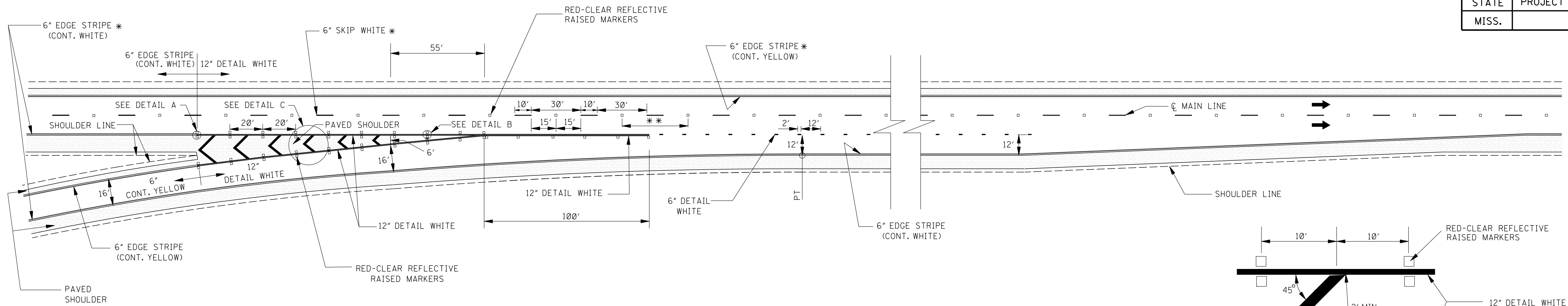


4-LANE WITH ONE-WAY TRAFFIC

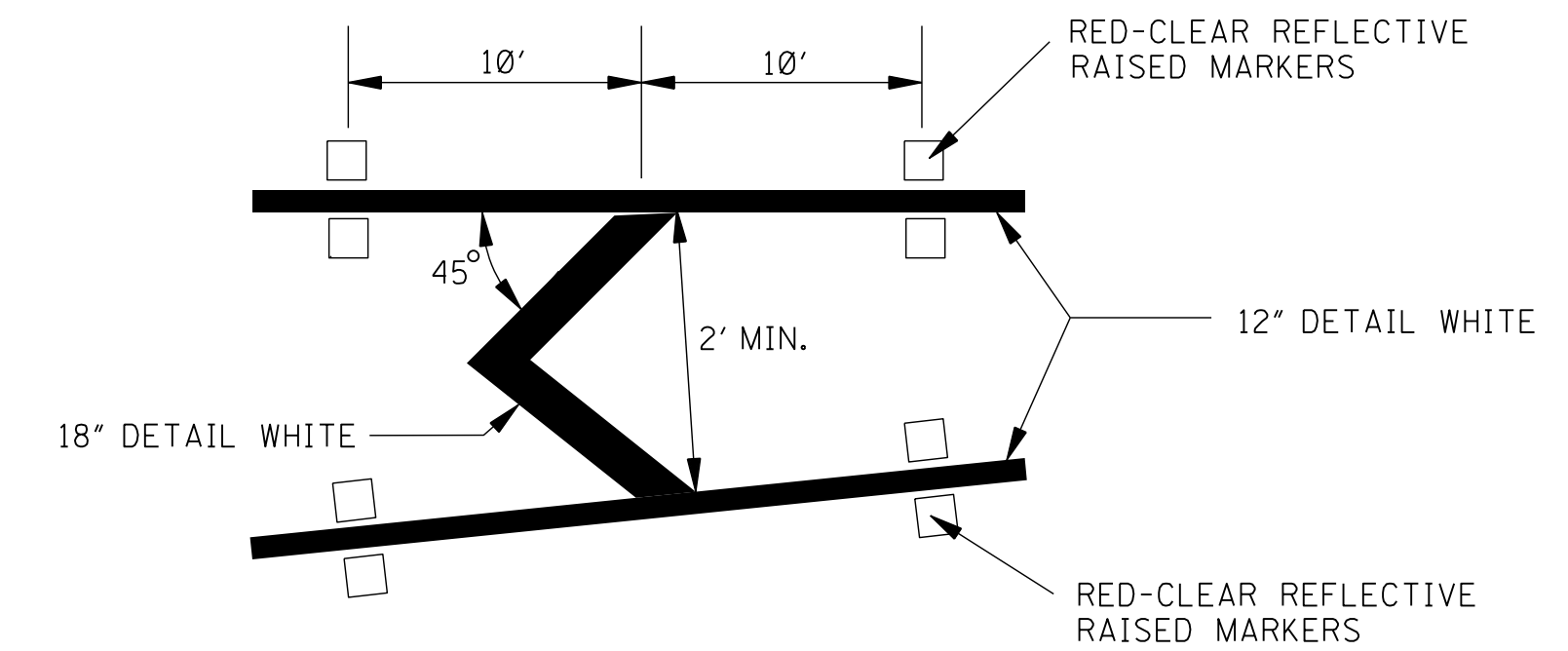
BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
REVISION	PAVEMENT MARKING DETAILS FOR 2-LANE AND 4-LANE DIVIDED ROADWAYS
DATE	ISSUE DATE: AUGUST 01, 2017



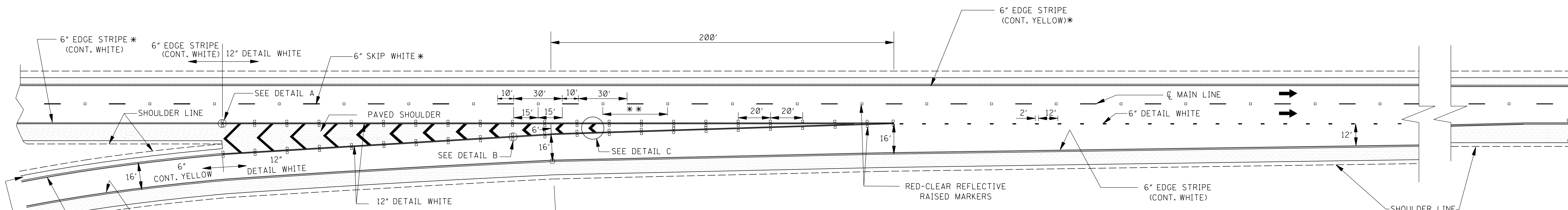
WORKING NUMBER
PM-1
SHEET NUMBER
6051



PARALLEL ENTRANCE RAMP

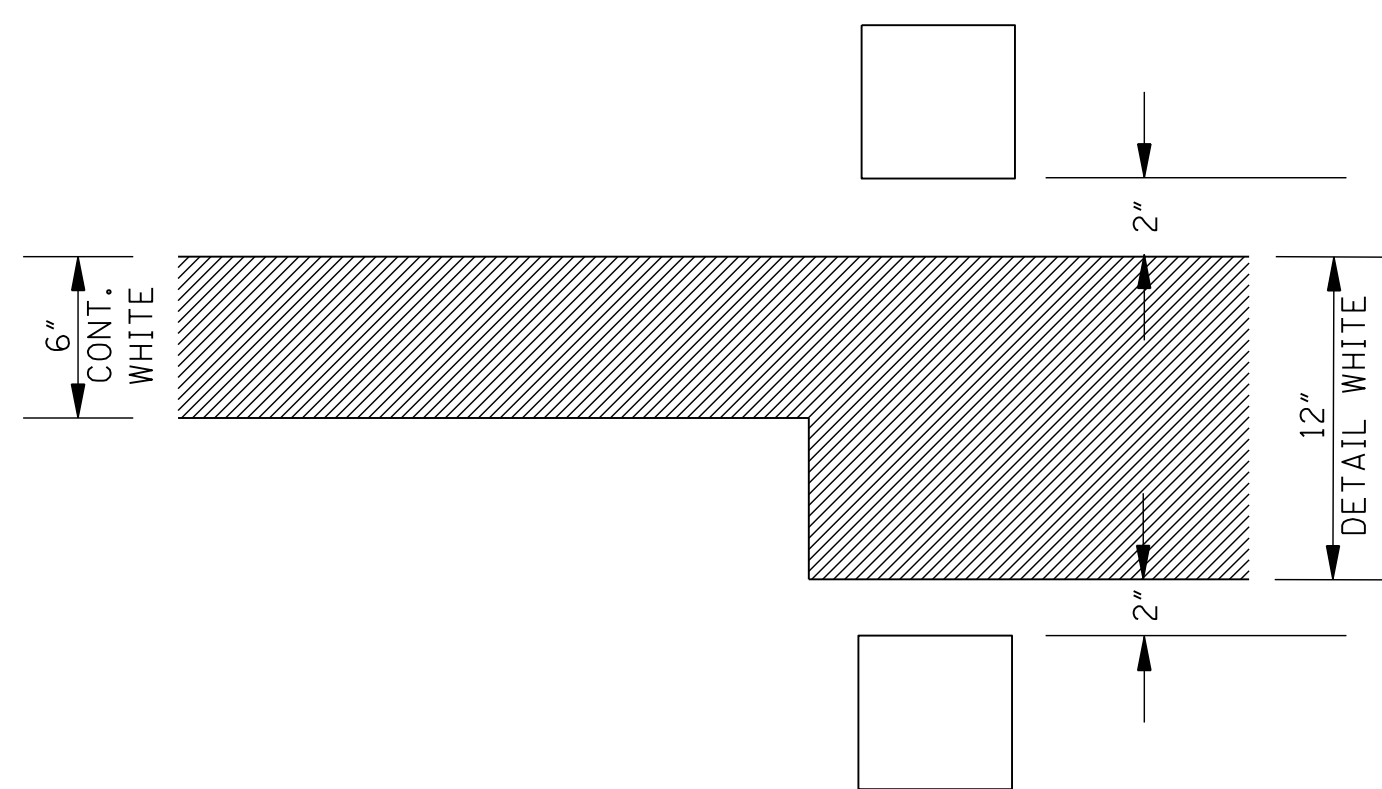


DETAIL C

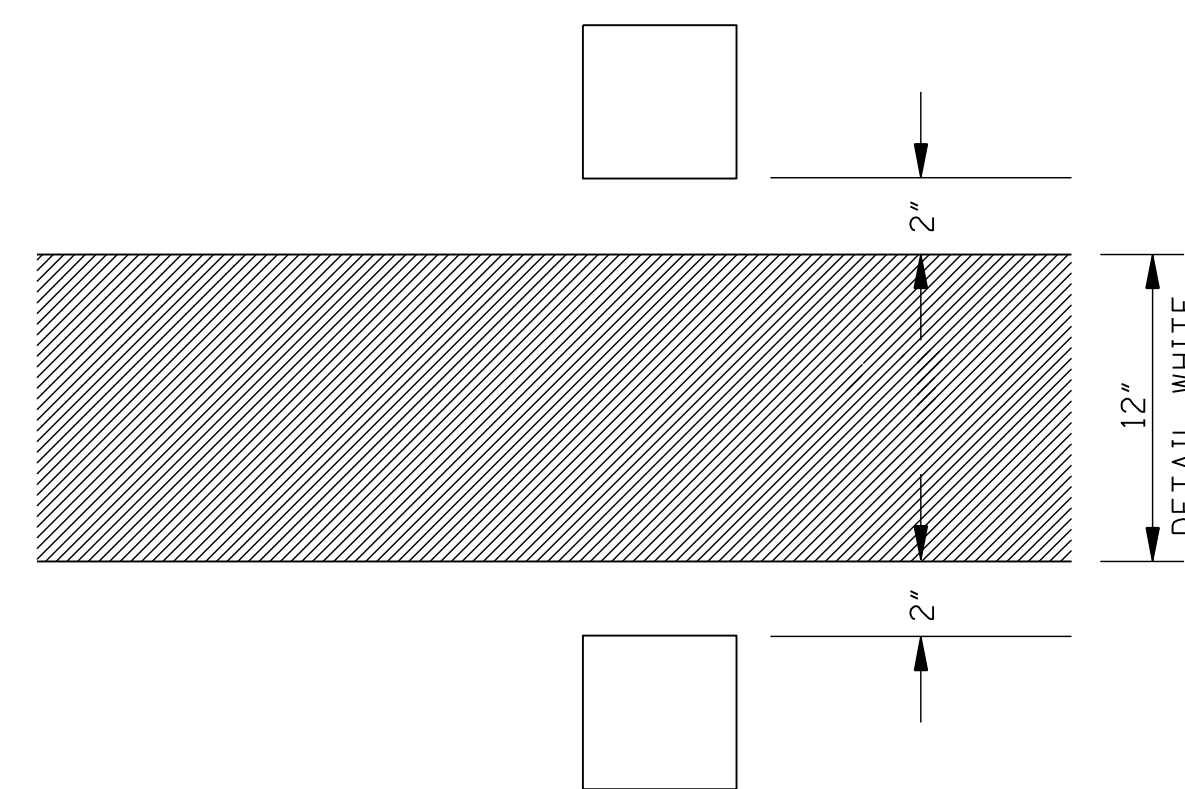


TAPER ENTRANCE RAMP

NOTE: 2'-12' SKIP WHITE EXTENDS TO THE TERMINATION POINT OF THE ACCELERATION LANE.



DETAIL A



DETAIL B

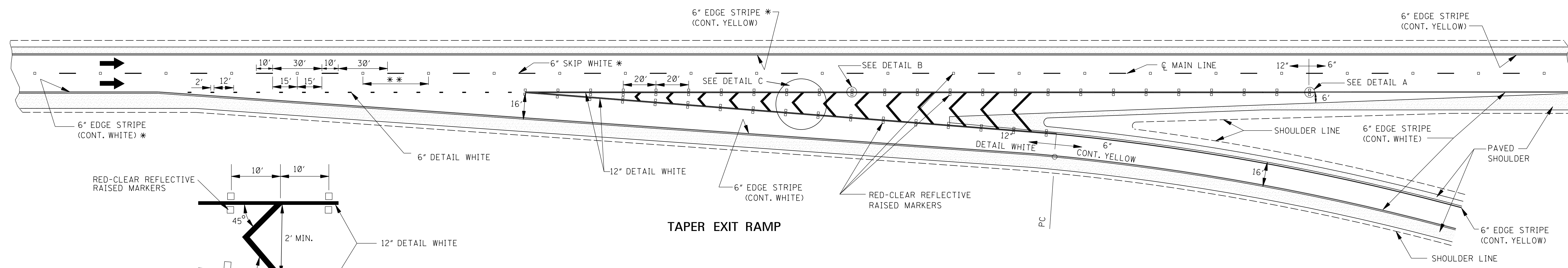
GENERAL NOTES:

- * 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELED WAY.
- ** 2. ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- 3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED SOURCES OF MATERIALS."

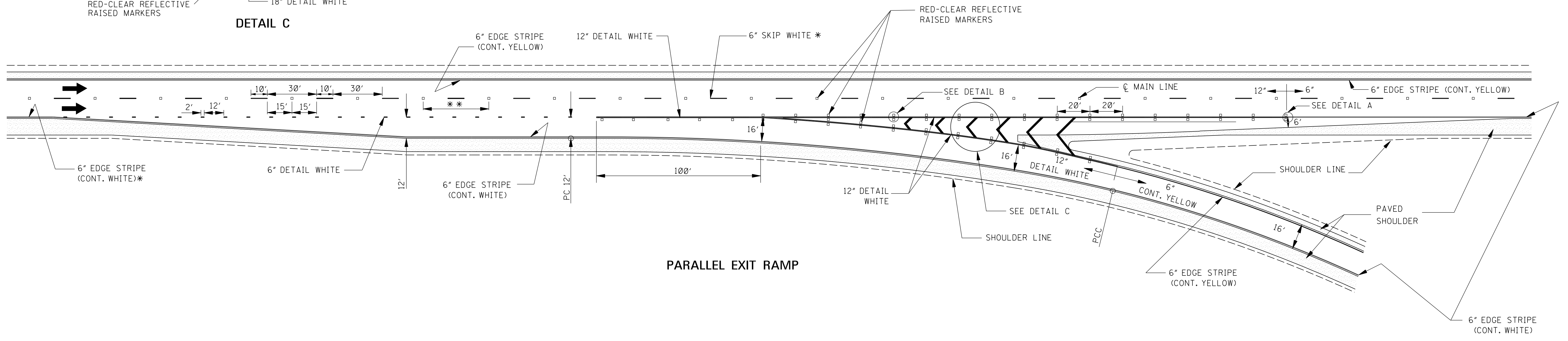
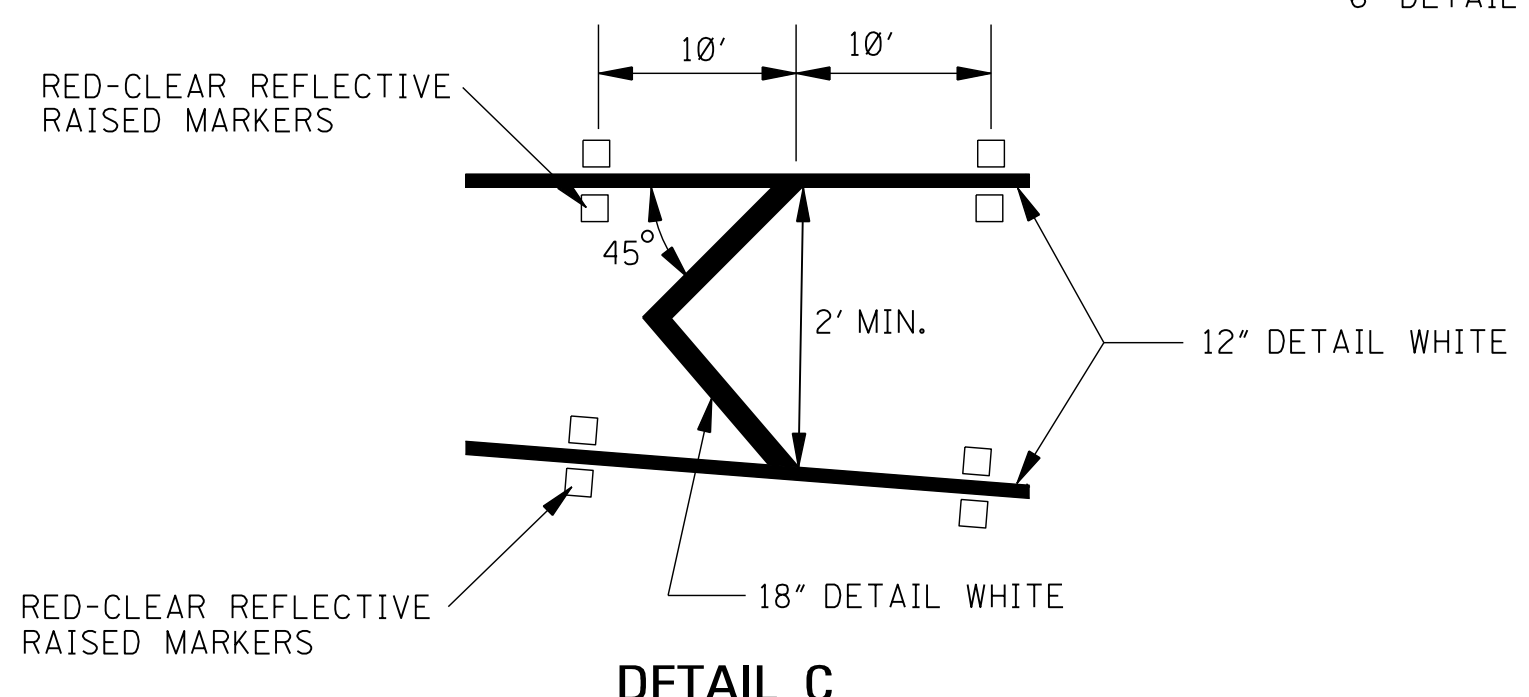
BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		PAVEMENT MARKING DETAILS FOR INTERCHANGE ENTRANCE RAMPS (PARALLEL AND TAPER)	
DATE			
ISSUE DATE:		AUGUST 01, 2017	



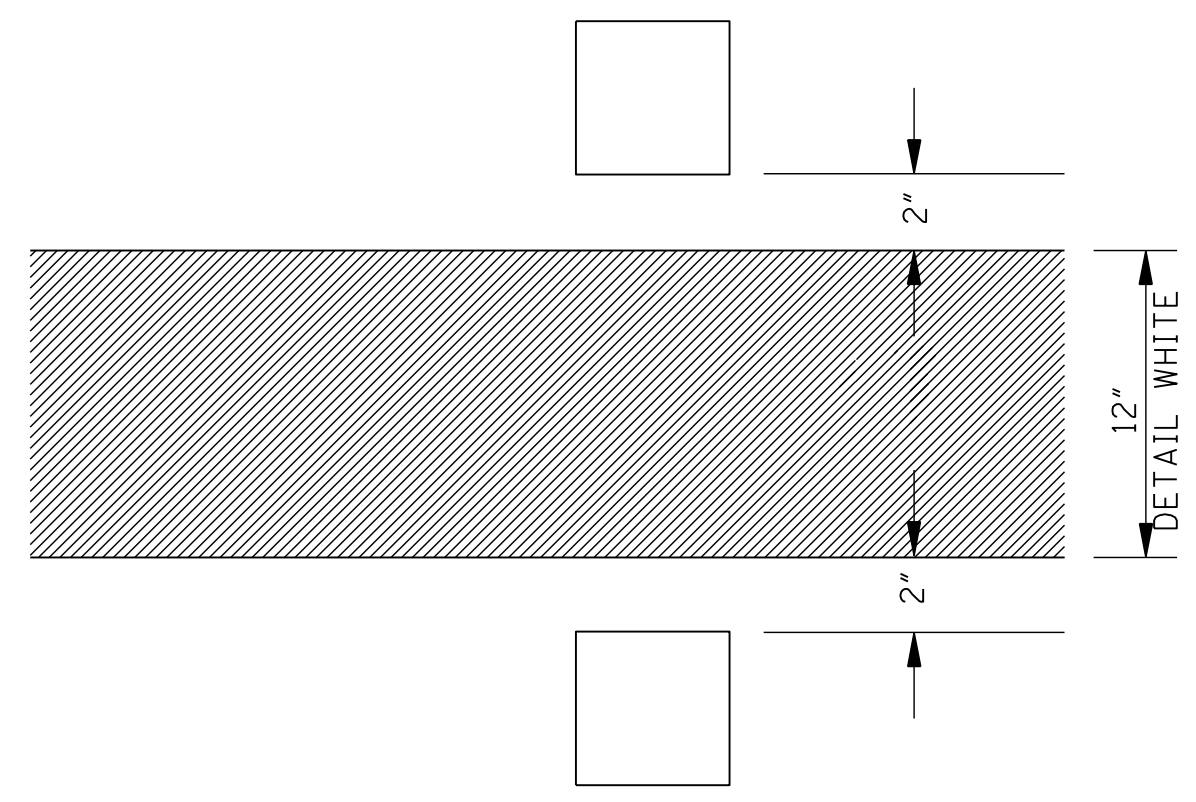
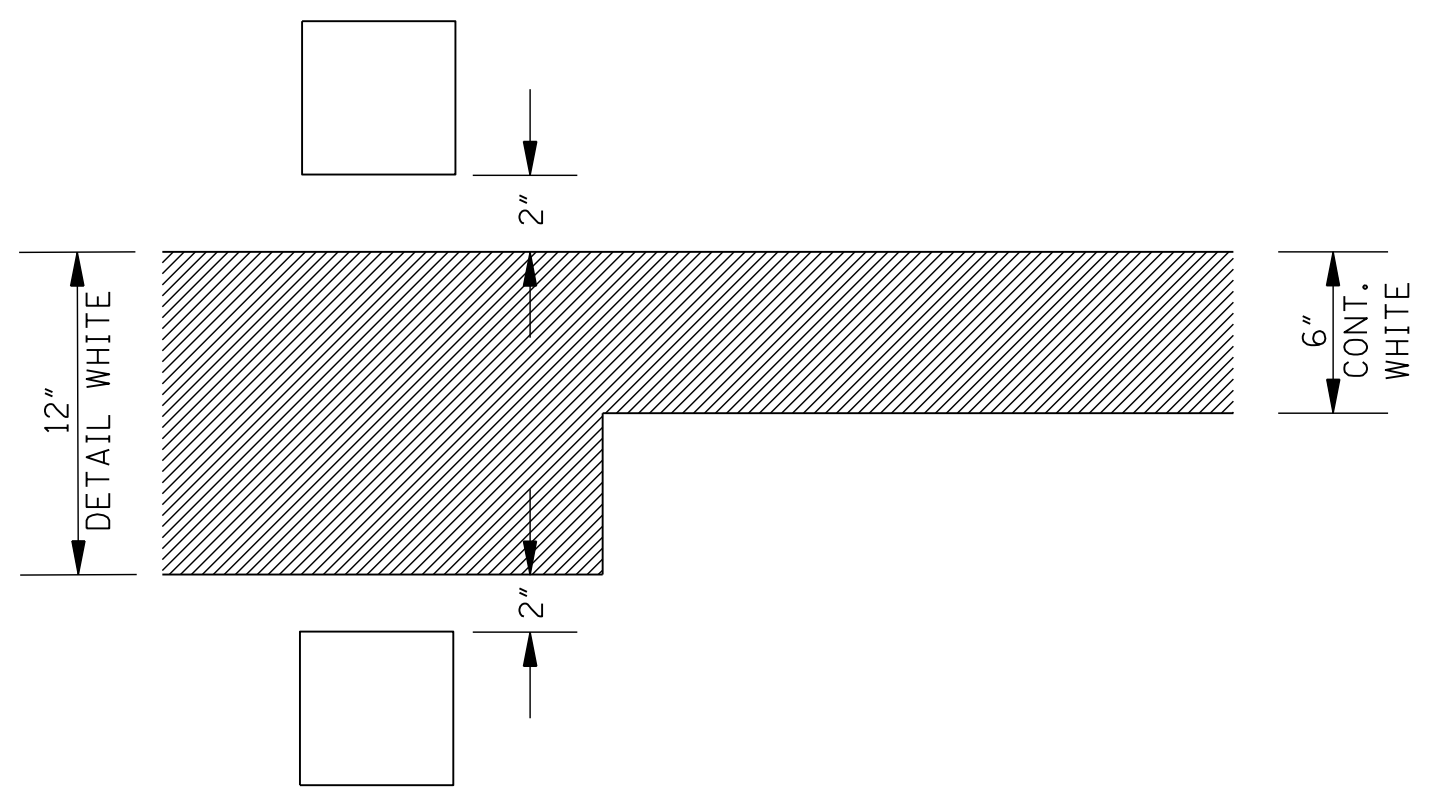
WORKING NUMBER
PM-3
SHEET NUMBER
6053



TAPER EXIT RAMP

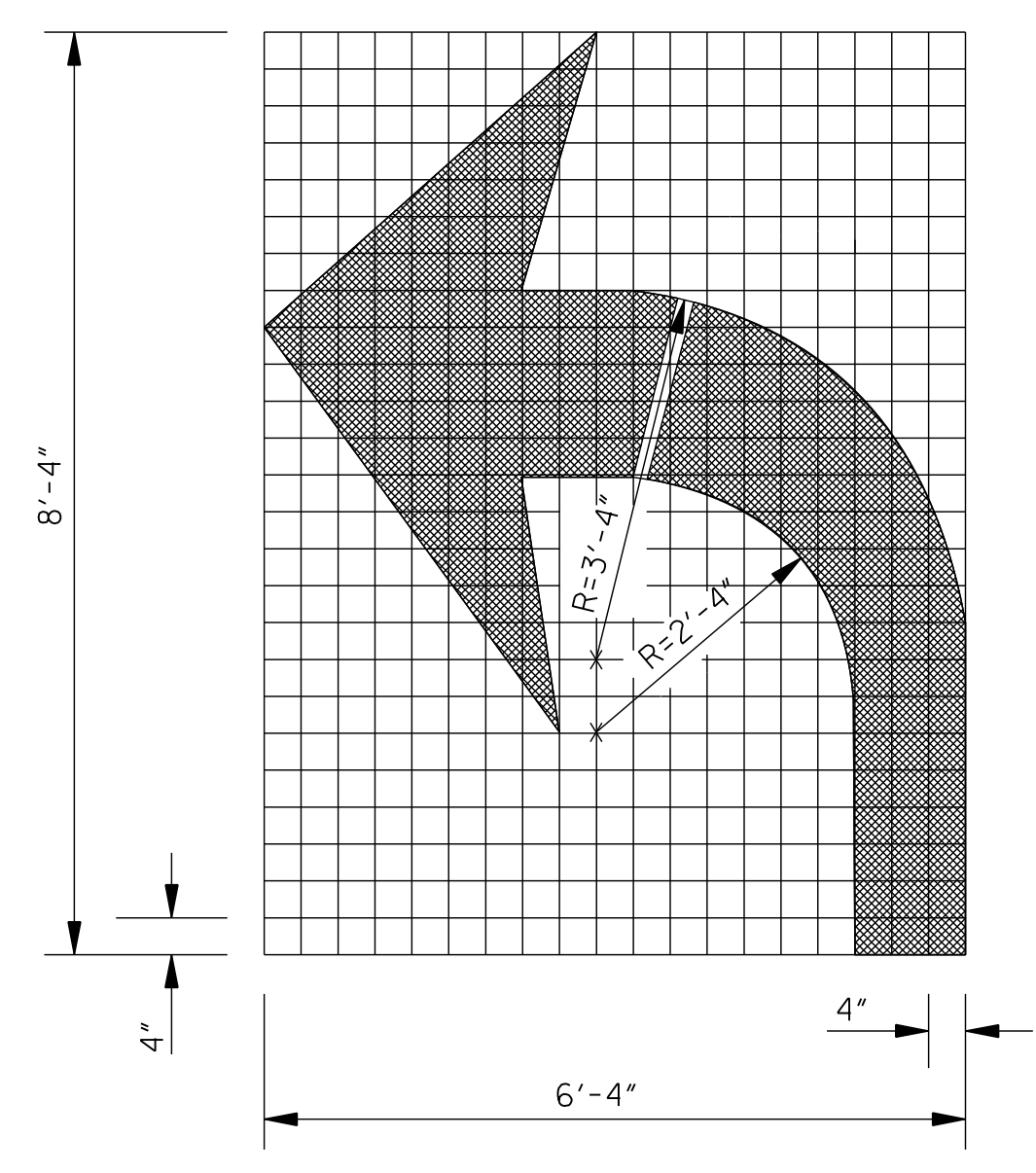
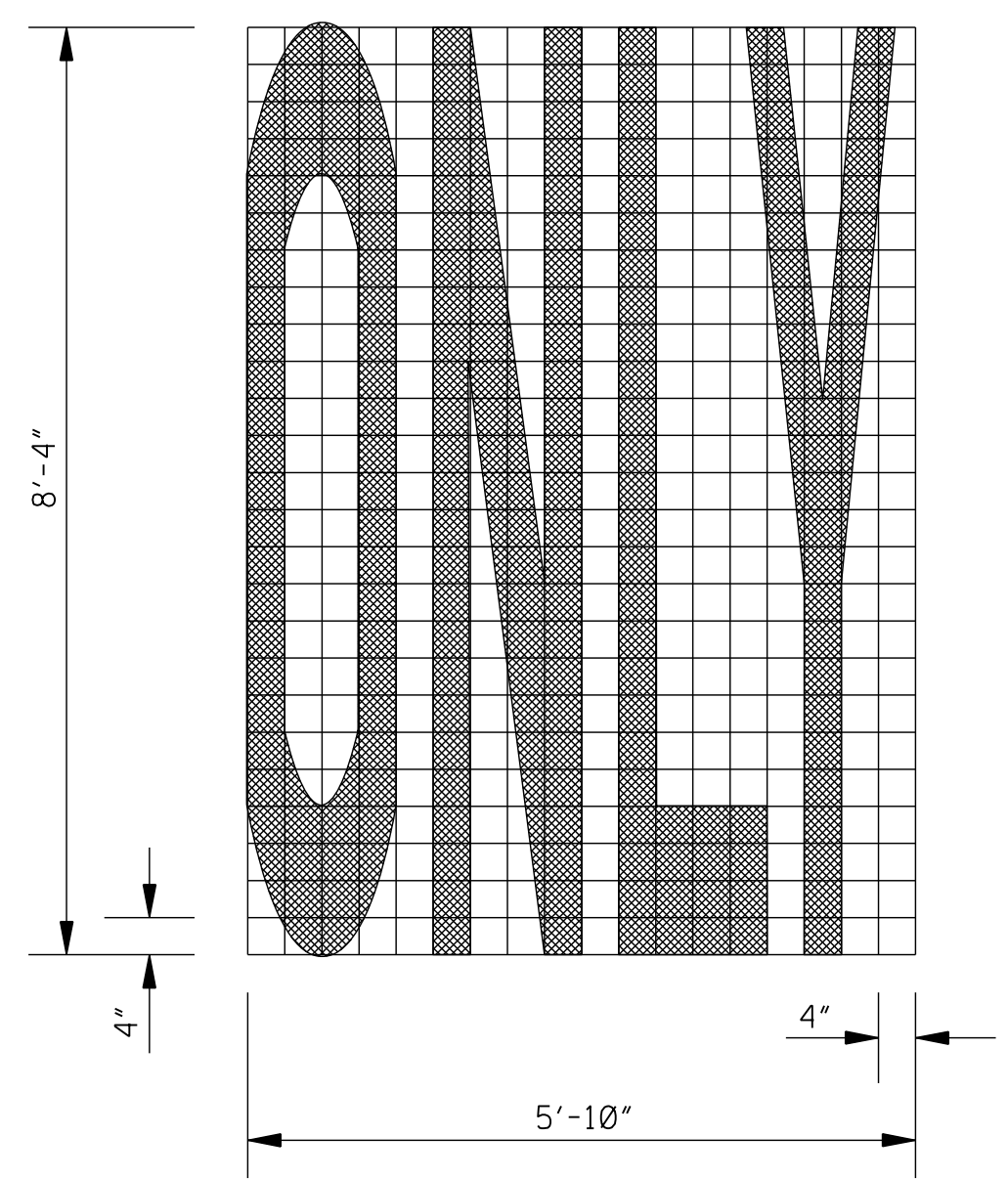


PARALLEL EXIT RAMP

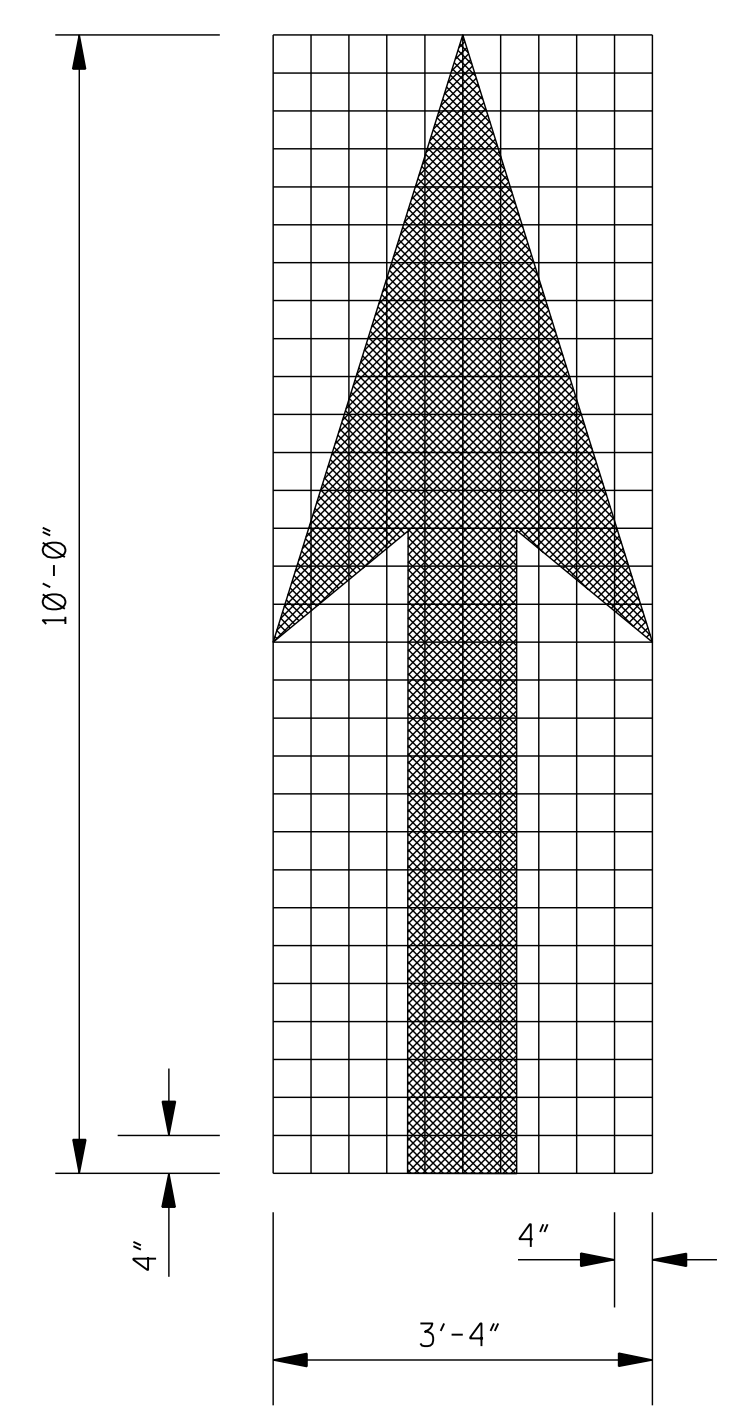


- GENERAL NOTES:
- * 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELED WAY.
 - ** 2. ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - 3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED SOURCES OF MATERIALS."

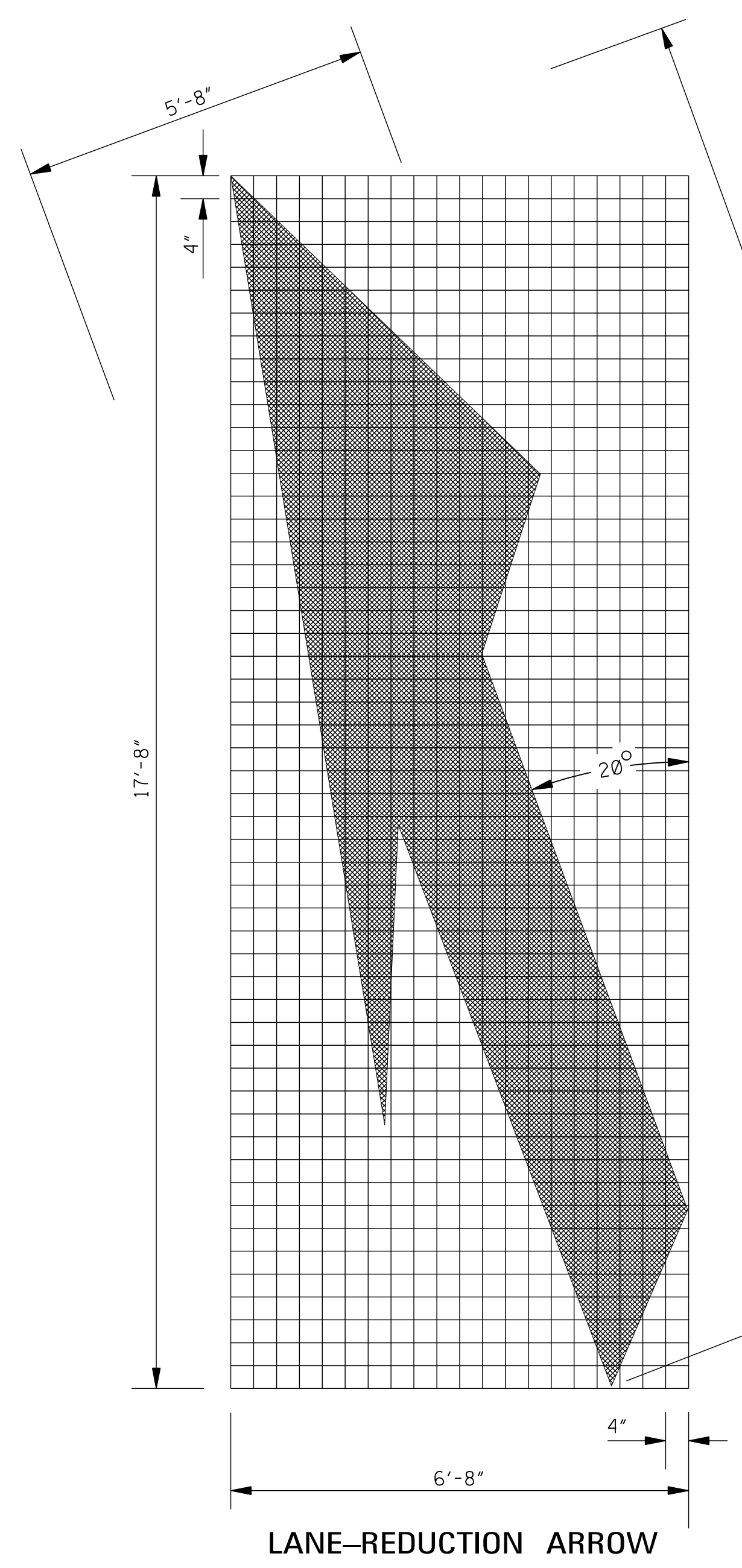
BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		<p align="center">PAVEMENT MARKING DETAILS FOR INTERCHANGE EXIT RAMP (PARALLEL AND TAPER)</p> <p align="right">  WORKING NUMBER PM-4 SHEET NUMBER 6054 </p>	
DATE	ISSUE DATE: AUGUST 01, 2017		



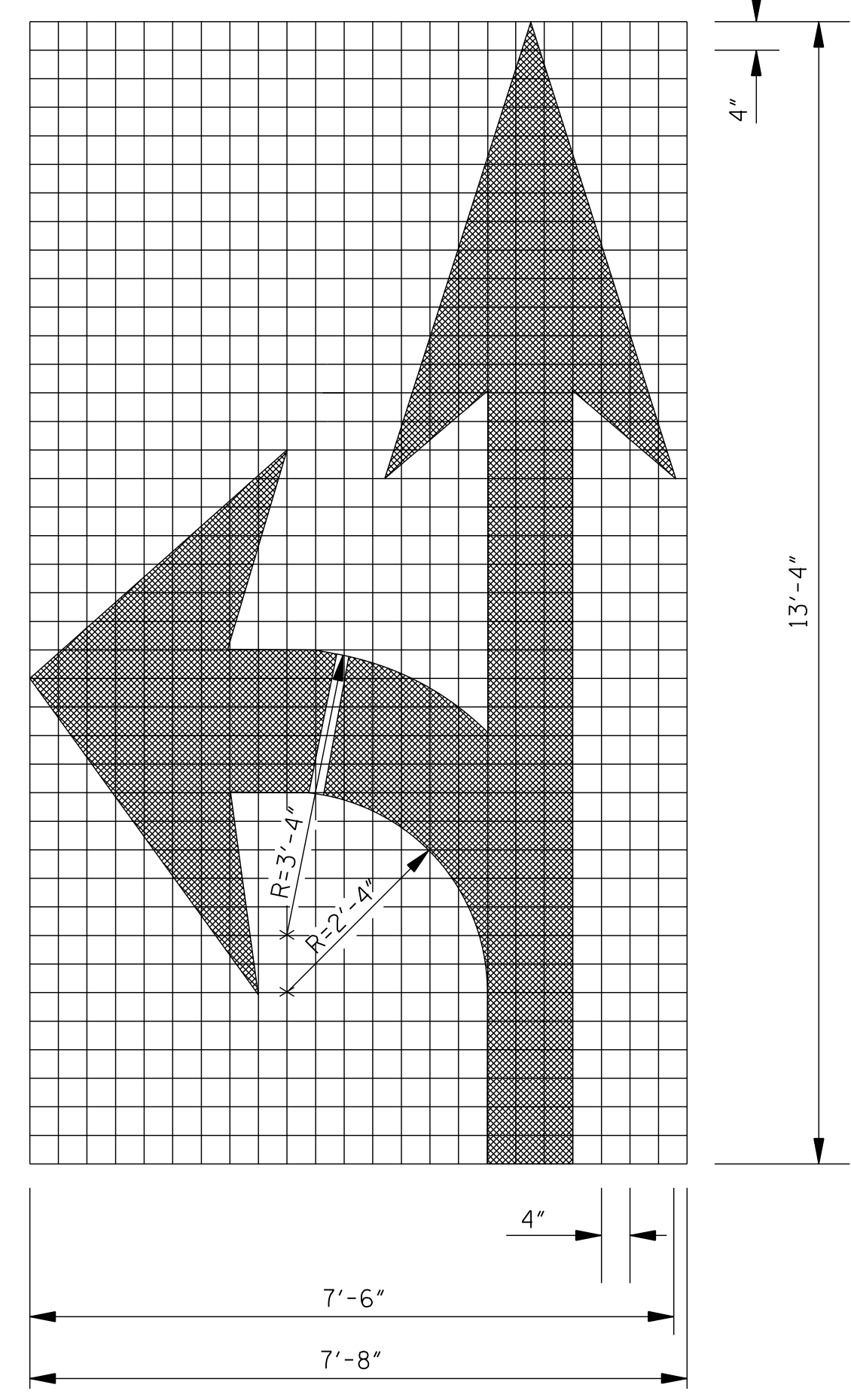
TURN ARROW



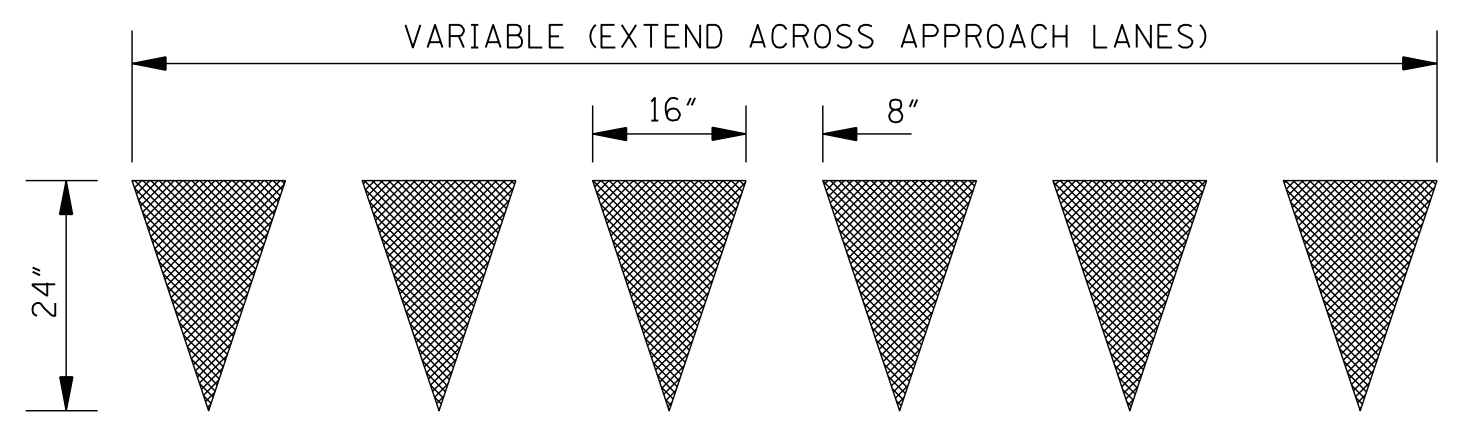
THRU ARROW



LANE-REDUCTION ARROW



COMBINATION ARROW

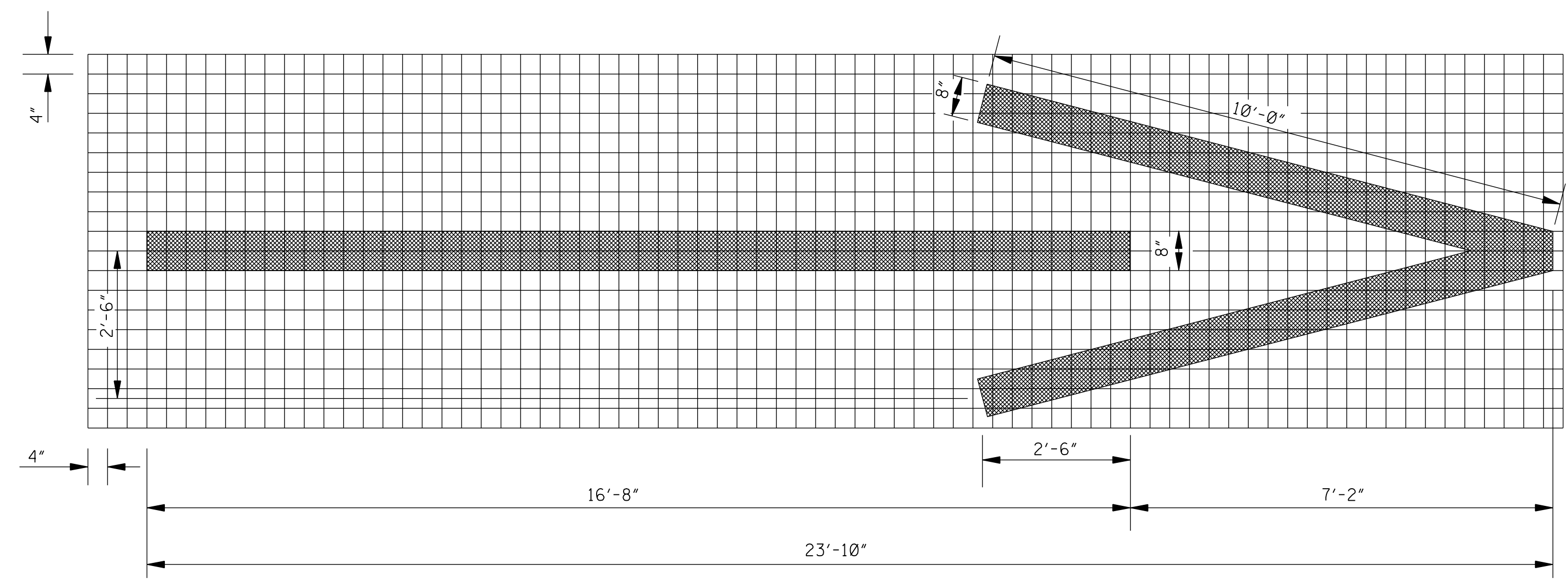


YIELD LINE

GENERAL NOTES:

1. TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTORS) OF 1/2" OR LESS AND EXTENDING THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
2. FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
3. DIMENSIONS OF THE YIELD LINE MAY VARY WITH APPROVAL OF THE ENGINEER. SEE MUTCD, LATEST EDITION, FOR ALLOWABLE DIMENSIONS.
4. PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

PAY QUANTITIES	
LEGEND/SYMBOL	AREA (ft ²)
ONLY	22.0
TURN ARROW	16.4
THRU ARROW	12.3
COMB. ARROW	27.5
1-WAY ARROW	24.3
LANE REDUCTION ARROW	40.0

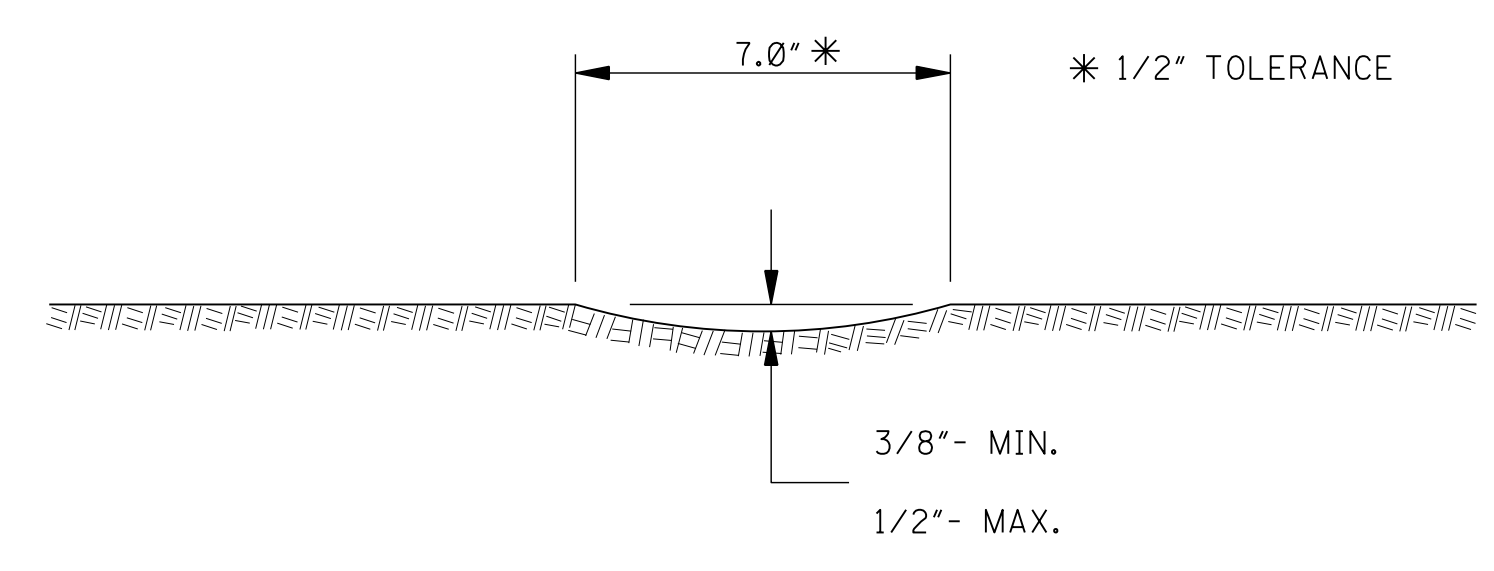


1-WAY ARROW

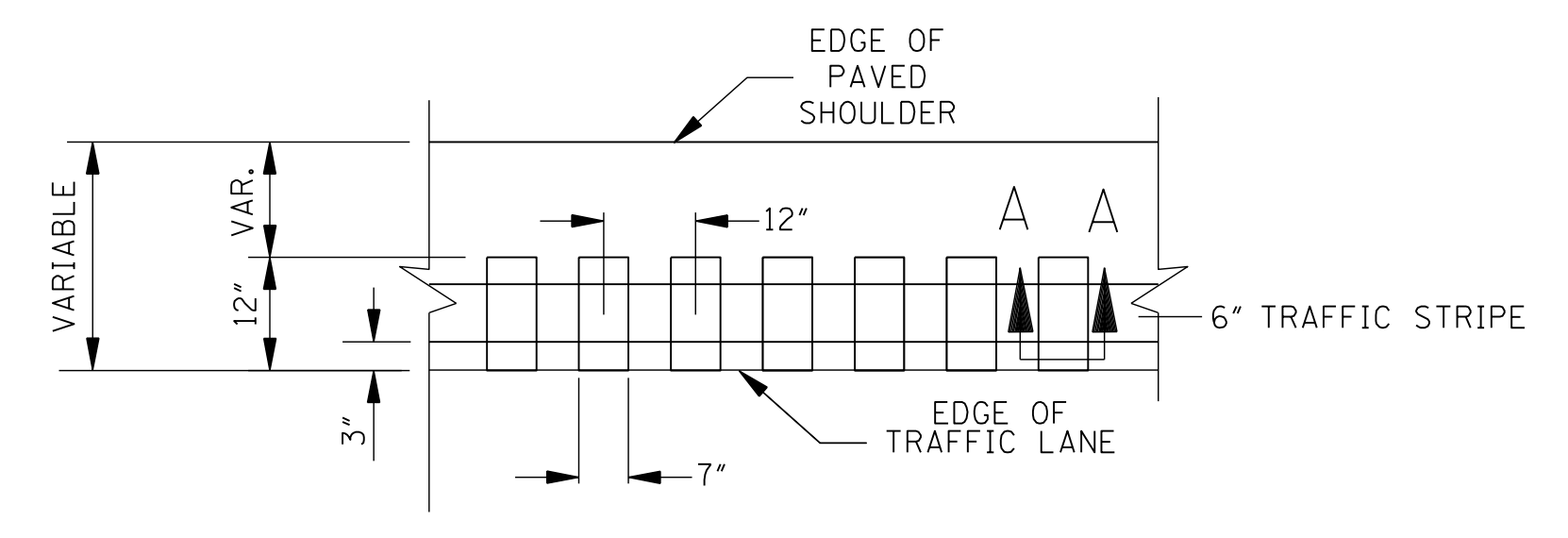
BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
REVISION	
DATE	ISSUE DATE: AUGUST 01, 2017

**PAVEMENT MARKING
LEGEND DETAILS**


 WORKING NUMBER
 PM-6
 SHEET NUMBER
 6056

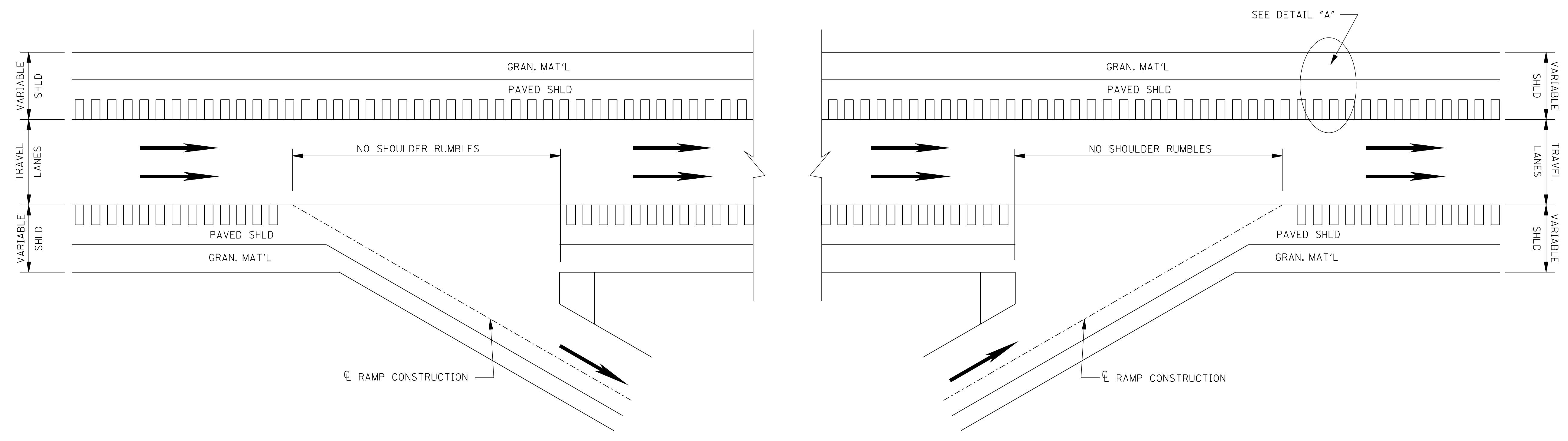


SECTION "A-A"



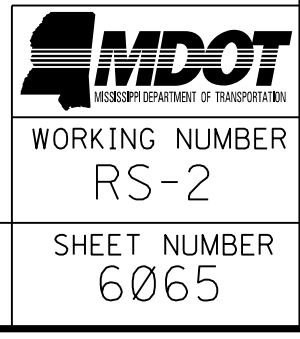
DETAIL "A"

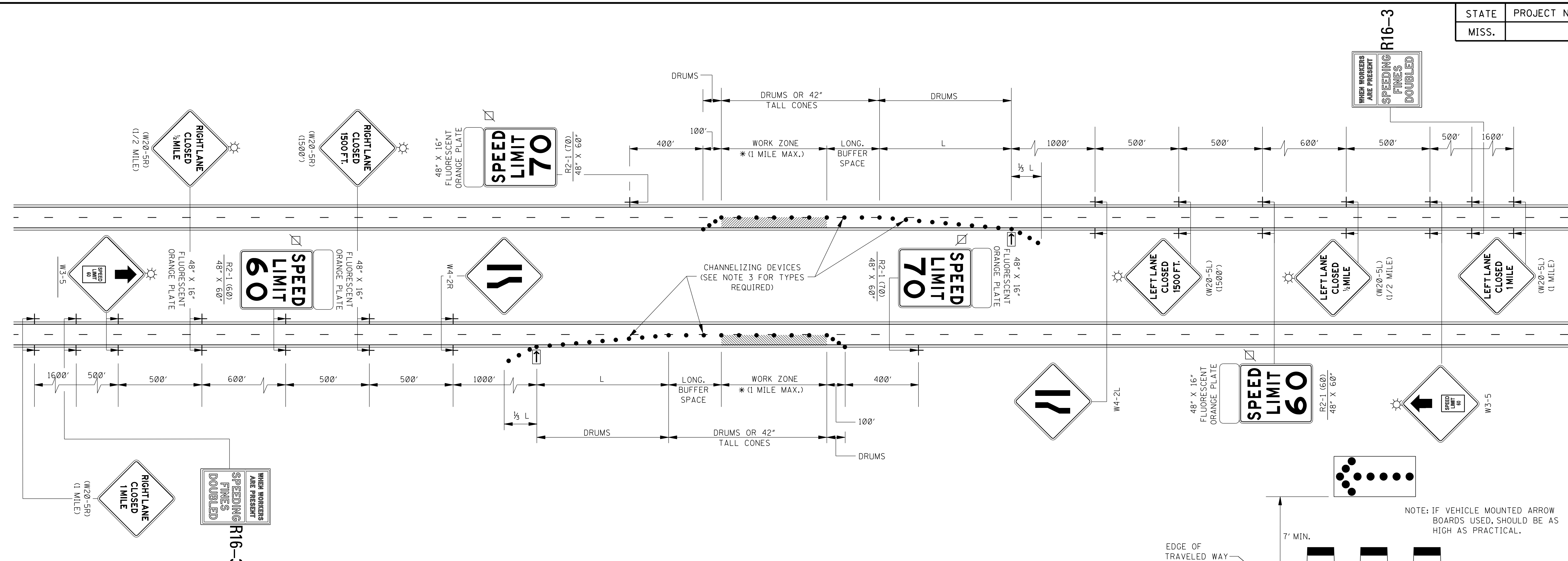
- GENERAL NOTES
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
 - GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PUBLIC INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 - COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
 - MAINLINE
 - INTERSECTING ROADWAY IF OVERLAID OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.W.
 - ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.



PLAN
NOT TO SCALE
DETAILS OF
RUMBLE STRIPS

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		RUMBLE STRIPES 4-LANE HIGHWAYS (ASPHALT LANES, 2-FT OR WIDER ASPHALT SHOULDERS)	
DATE			
ISSUE DATE:		AUGUST 01, 2017	
WORKING NUMBER		RS-2	
SHEET NUMBER		6065	





GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft)	TAPER RATES
	TAPER	ALONG BUFFER SPACE & WORK ZONE		
≤40	40	80	305	27:1
45	45	90	360	45:1
50	50	100	425	50:1
55	55	110	495	55:1
60	60	120	570	60:1
65	65	130	645	65:1
70	70	140	730	70:1

+ NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 $L = WS$ FOR SPEEDS OF 45 mph OR GREATER
 $L = WS^2/60$ FOR SPEEDS OF 40 mph OR LESS
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

++ NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.

3. CHANNELIZING DEVICES:

- A. ALL CHANNELIZING DEVICES IN TAPERS SHALL BE RETROREFLECTIVE FREE STANDING PLASTIC DRUMS.
- B. CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER RETROREFLECTIVE FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.
- C. ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE.
- D. RETROREFLECTORIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE M.U.T.C.D.

4. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48", AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.

5. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON FACE OF SIGN.

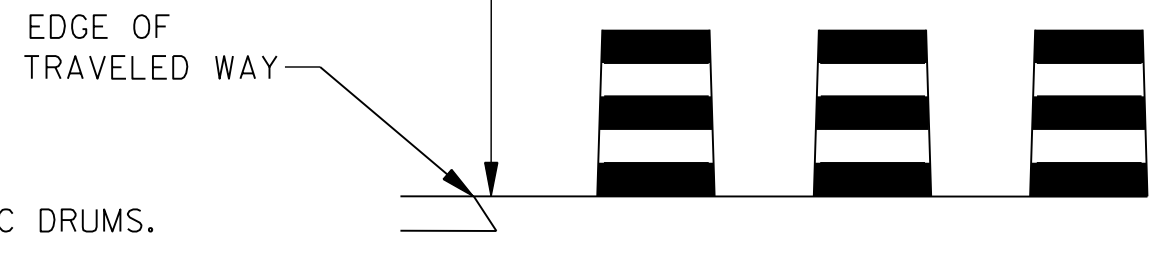
6. ADDITIONAL REDUCED REGULATORY SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. TWO (2) WILL BE REQUIRED FOR EACH RAMP AND LOCATION WILL BE DETERMINED BY THE ENGINEER.

7. THIS TRAFFIC CONTROL PLAN, WITH SPEED ZONE, MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.

8. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.

9. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS REQUIRED FOR LANE CLOSURE.

10. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

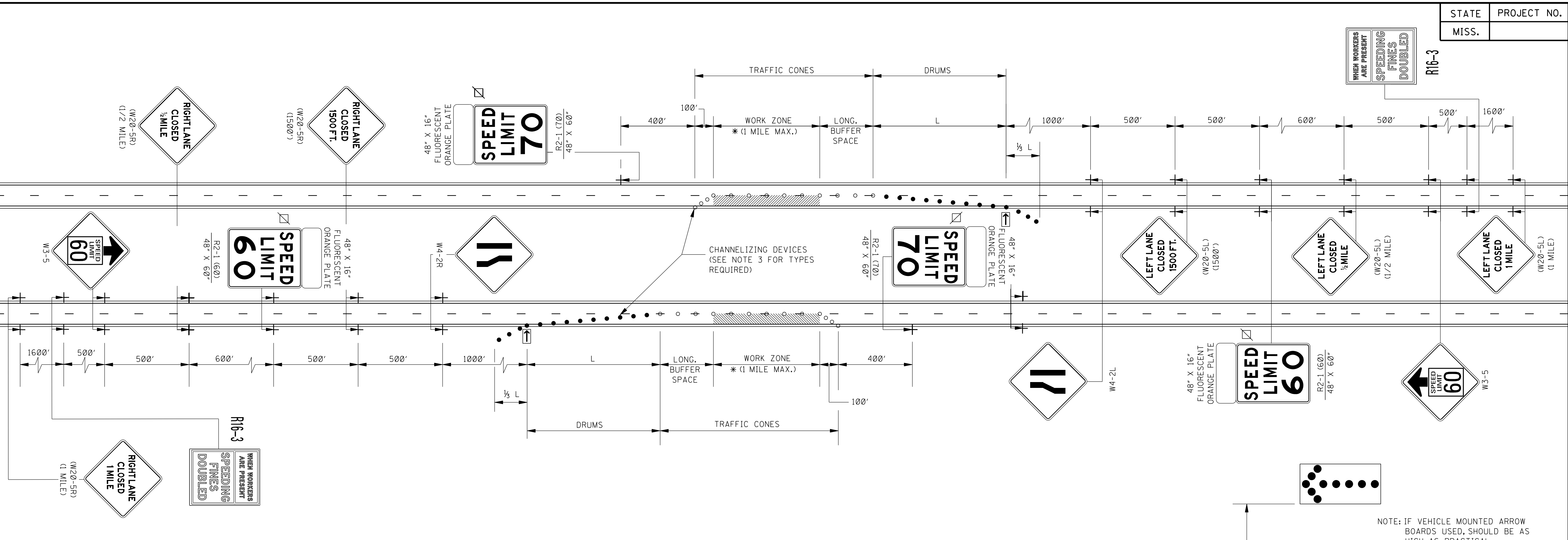


LEGEND

- * OR AS SHOWN ELSEWHERE ON THE PLANS.
- FLASHING ARROW PANEL (TYPE "C")
- BLACK LEGEND AND BORDER ON WHITE BACKGROUND
- ⚠ TYPE "B" WARNING LIGHTS
- RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION
REVISION	STANDARD PLAN TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (EXTENDED PERIOD)
DATE	ISSUE DATE: AUGUST 01, 2017


 WORKING NUMBER TCP-4
 SHEET NUMBER 6354



GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

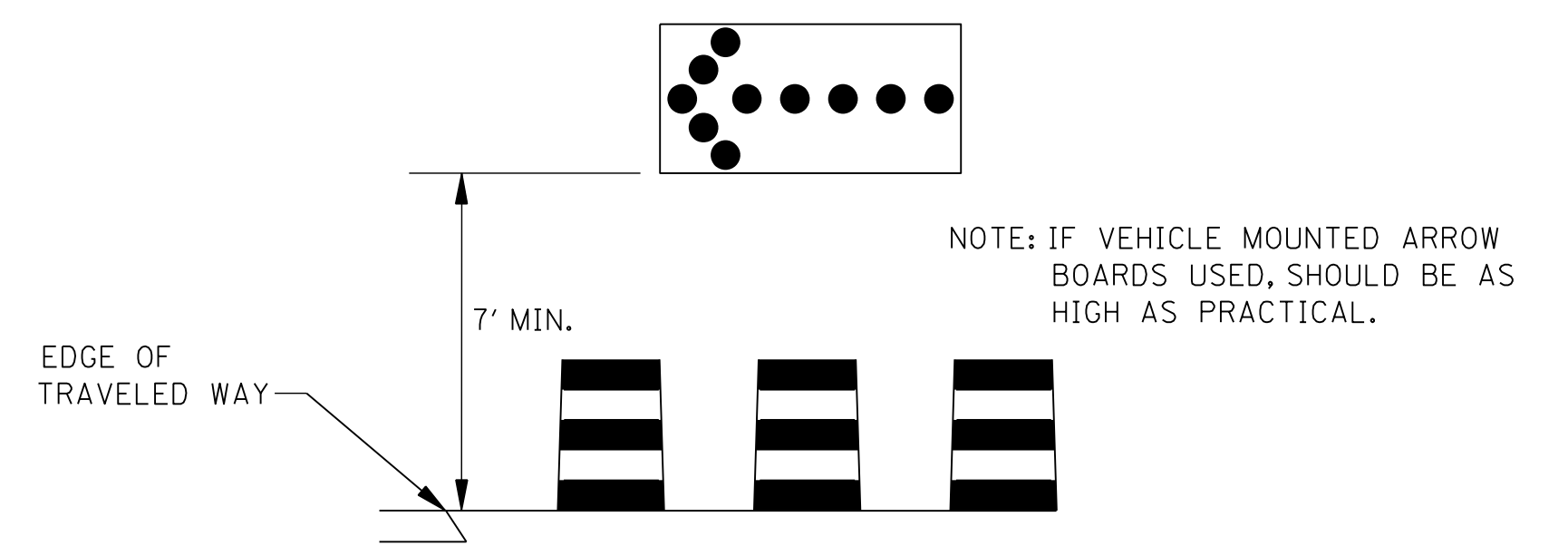
POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft)	TAPER RATES
	TAPER	ALONG LANE LINE & WORK ZONE		
40	40	80	305	27:1
45	45	90	360	45:1
50	50	100	425	50:1
55	55	110	495	55:1
60	60	120	570	60:1
65	65	130	645	65:1
70	70	140	730	70:1

+ NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 $L = WS$ FOR SPEEDS OF 45 mph OR GREATER
 $L = WS^2/60$ FOR SPEEDS OF 40 mph OR LESS
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

++ NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.

- CHANNELIZING DEVICE TYPES FOR:
 - APPROACH TAPER- RETROREFLECTIVE PLASTIC DRUMS
 - ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT MINIMUM)
 - EXIT TAPER- TRAFFIC CONES (28" HEIGHT MINIMUM)
- WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
- FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING AND REGULATORY SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
- DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48". AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
- ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON THE FACE OF SIGN.
- ADDITIONAL REDUCED REGULATORY SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. TWO (2) WILL BE REQUIRED FOR EACH RAMP AND LOCATION WILL BE DETERMINED BY THE ENGINEER.
- THIS TRAFFIC CONTROL PLAN, WITH SPEED ZONE, MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
- LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.
- A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS AND "REDUCED SPEED AHEAD" SIGNS REQUIRED FOR LANE CLOSURE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.



LEGEND

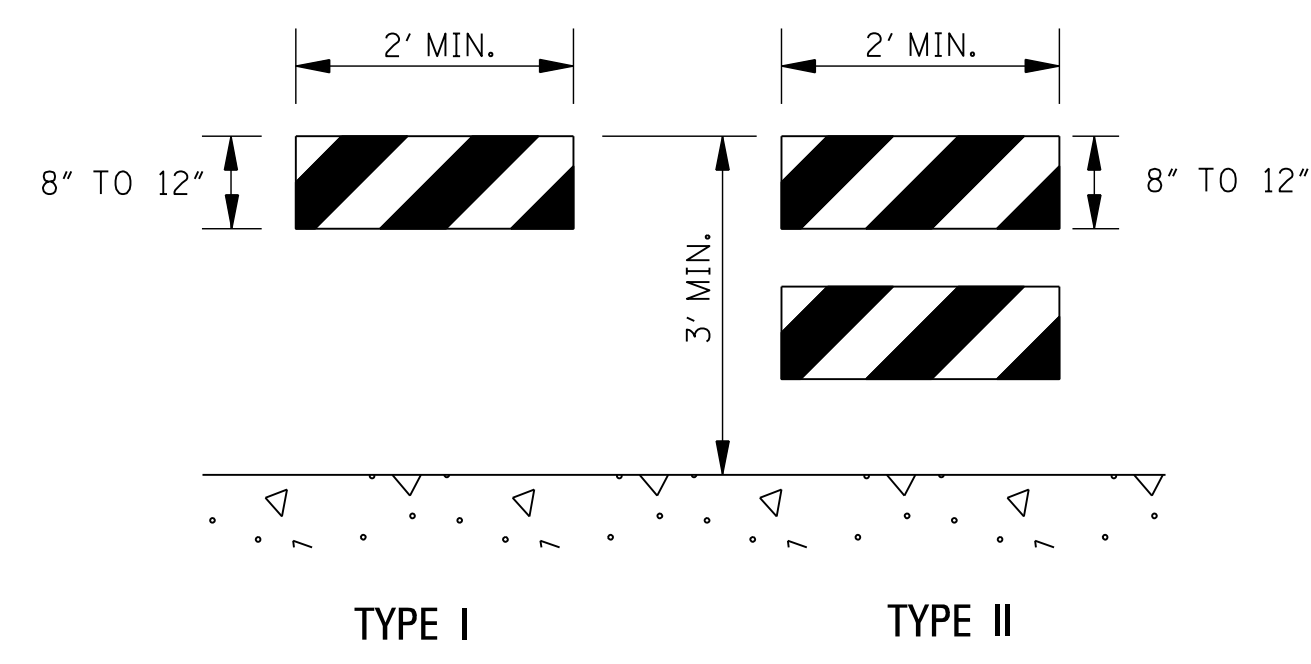
- * OR AS SHOWN ELSEWHERE ON THE PLANS.
- FLASHING ARROW PANEL (TYPE "C")
- BLACK LEGEND AND BORDER ON WHITE BACKGROUND
- RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
- TRAFFIC CONES (28" HEIGHT)

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION
REVISION	STANDARD PLAN TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (WORK DAY ONLY)
DATE	ISSUE DATE: AUGUST 01, 2017

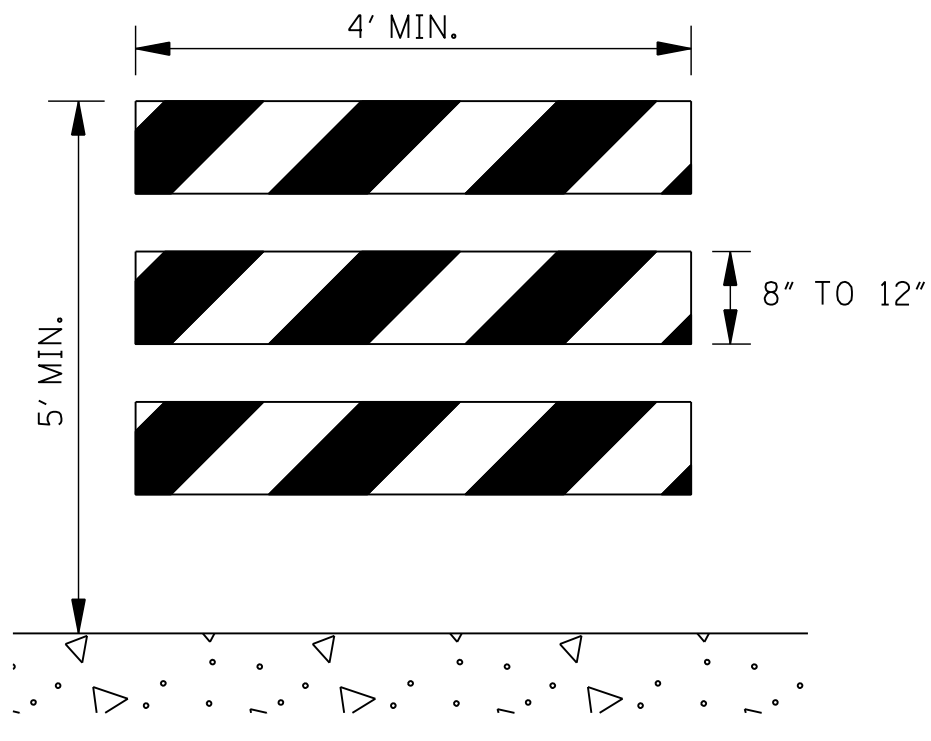
MDOT
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

WORKING NUMBER
TCP-5

SHEET NUMBER
6355



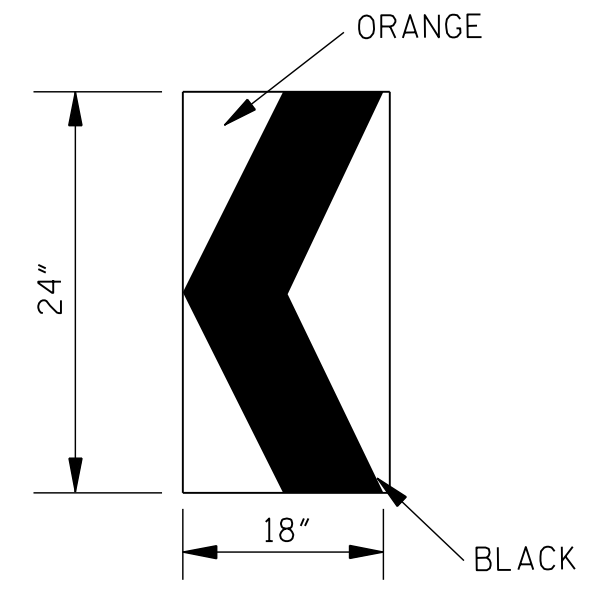
TYPE I TYPE II



TYPE III

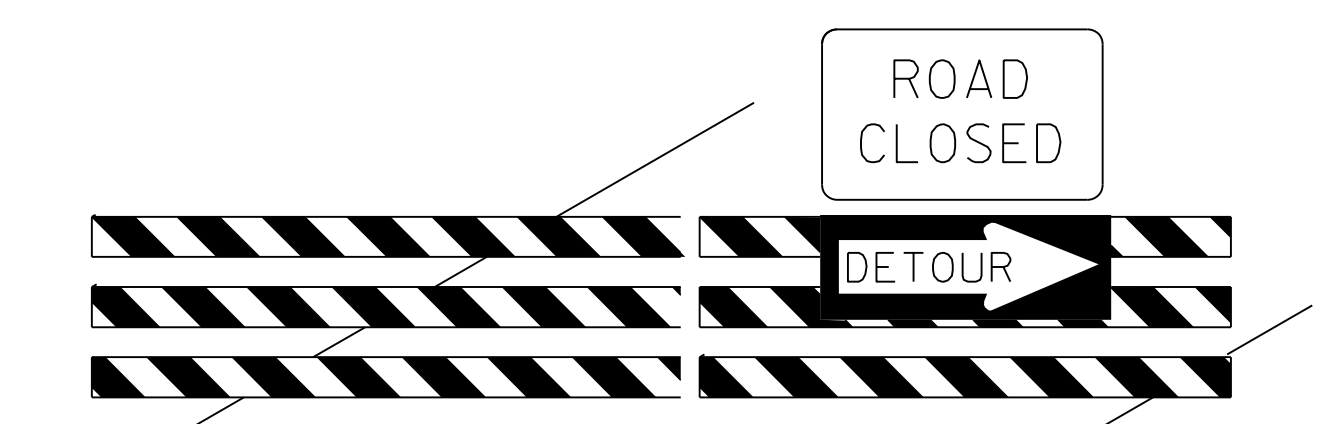
STANDARD BARRICADES

1. THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).
2. RAIL STRIPE SHOULD BE 6 INCHES, EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.
3. DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
4. FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.
5. BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES WHICH REQUIRE CRASHWORTHINESS ACCEPTANCE LETTERS. TO DATE, 2-IN. THICK TIMBER RAILS HAVE NOT BEEN SUCCESSFULLY CRASH TESTED. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE:
http://safety.fhwa.dot.gov/roadway_dept/policy_guide/road_hardware/cat2.cfm



**CHEVRON SIGN
DETAIL**

1. A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
2. THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.
3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.

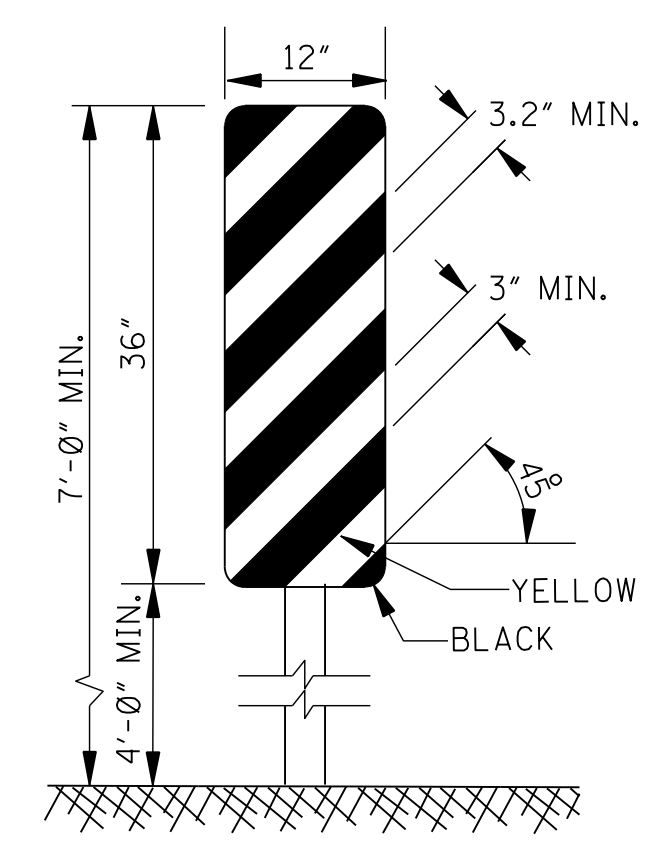


BARRICADE CLOSING A ROAD

BARRICADE CHARACTERISTICS

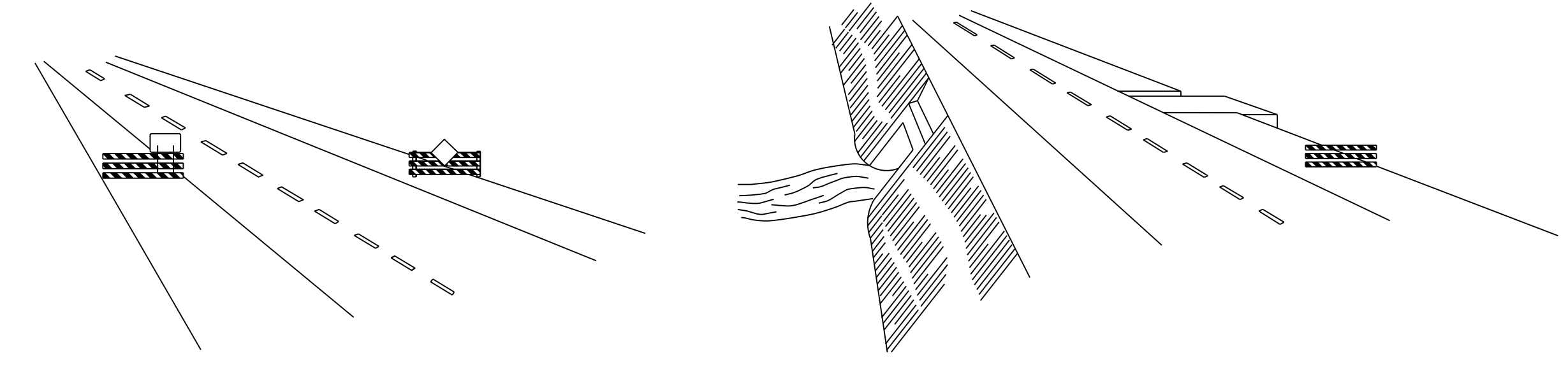
	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF RETROREFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS

- * 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- ** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 in² OF REFLECTIVE AREA FACING TRAFFIC.



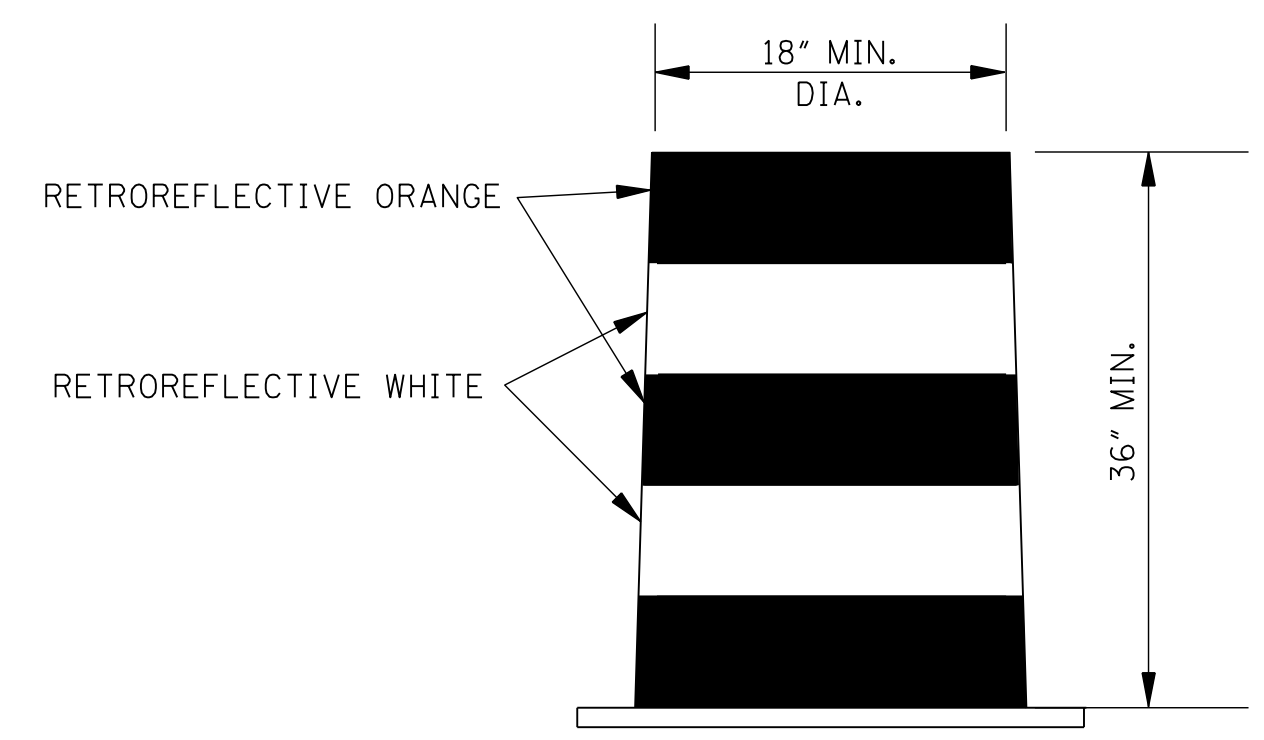
**TYPE 3 OBJECT MARKER
(OM-3R)**

1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
2. THE OM-3R IS SHOWN. THE OM-3L IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
3. THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



WING BARRICADES

1. WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
2. WING BARRICADES SHOULD BE USED:
 - A. IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - B. IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.

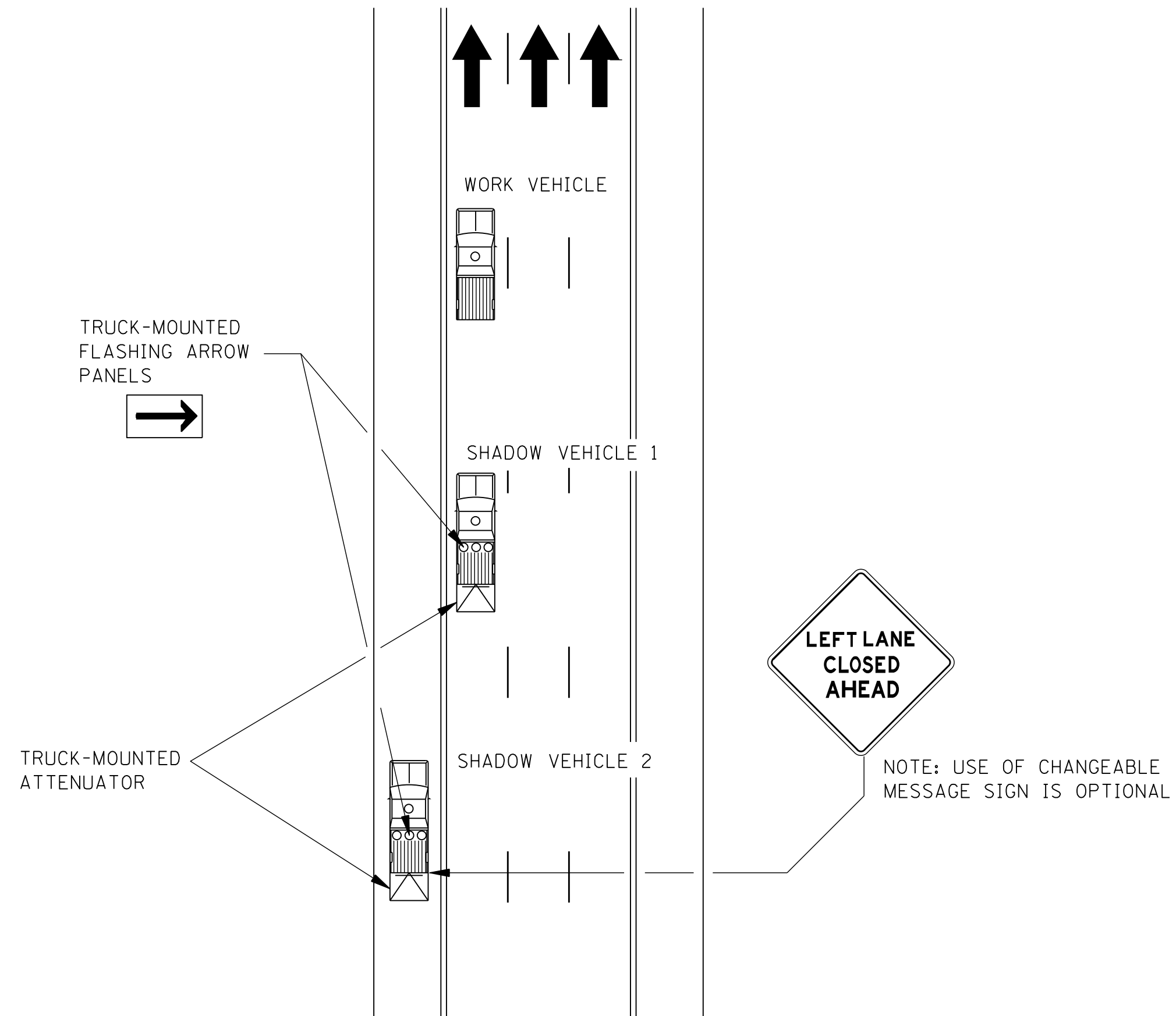


PLASTIC DRUM STRIPING DETAIL

1. PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT COLOR ON DRUMS SHALL BE ORANGE WITH FOUR (4) RETROREFLECTIVE, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
2. DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
3. WHERE PRACTICAL PLASTIC DRUMS SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		<p>HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS</p>	
DATE			
ISSUE DATE:		AUGUST 01, 2017	
		WORKING NUMBER TCP-8 SHEET NUMBER 6358	

MOBILE OPERATIONS ON MULTILANE ROAD

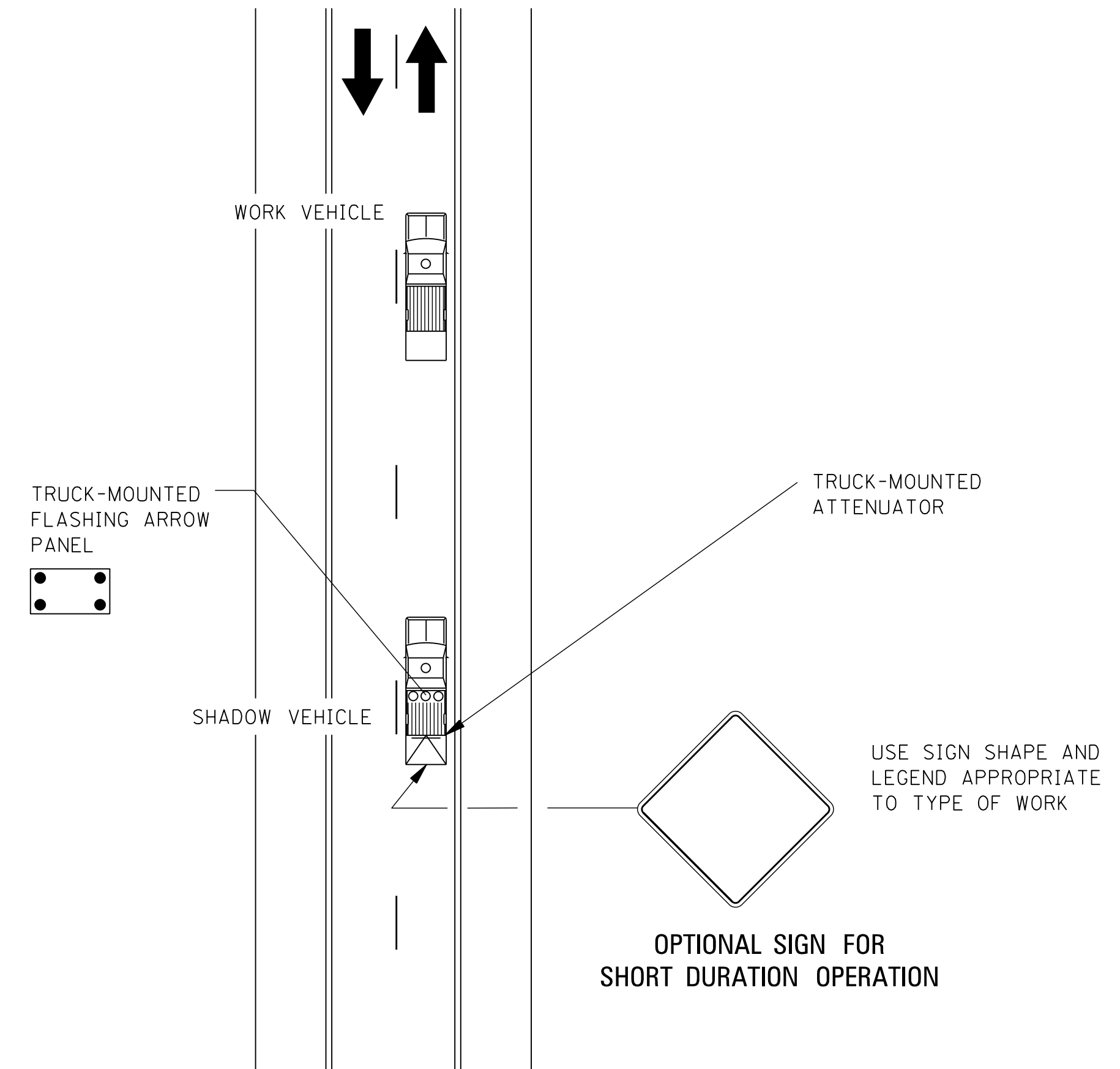


MOBILE OPERATIONS ON MULTILANE ROAD

NOTES FOR MULTILANE LANE OPERATION:

- VEHICLES USED FOR THESE OPERATIONS SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT, SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLAGS, SIGNS, OR ARROW PANELS.
- SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK MOUNTED ATTENUATOR (TMA). AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON SHADOW VEHICLE 2 SO AS NOT TO OBSCURE THE ARROW PANEL.
- SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK-MOUNTED ATTENUATOR (TMA).
- SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
- WHEN ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, SHADOW VEHICLE 2 SHOULD BE ELIMINATED.
- ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE SHOULD BE USED (i.e., VEHICLE 3 ON THE SHOULDER (IF PRACTICAL), VEHICLE 2 IN THE CLOSED LANE, AND VEHICLE 1 IN THE CLOSED LANE).
- ARROW PANELS SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
- WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MOBILE OPERATIONS ON TWO-LANE ROAD

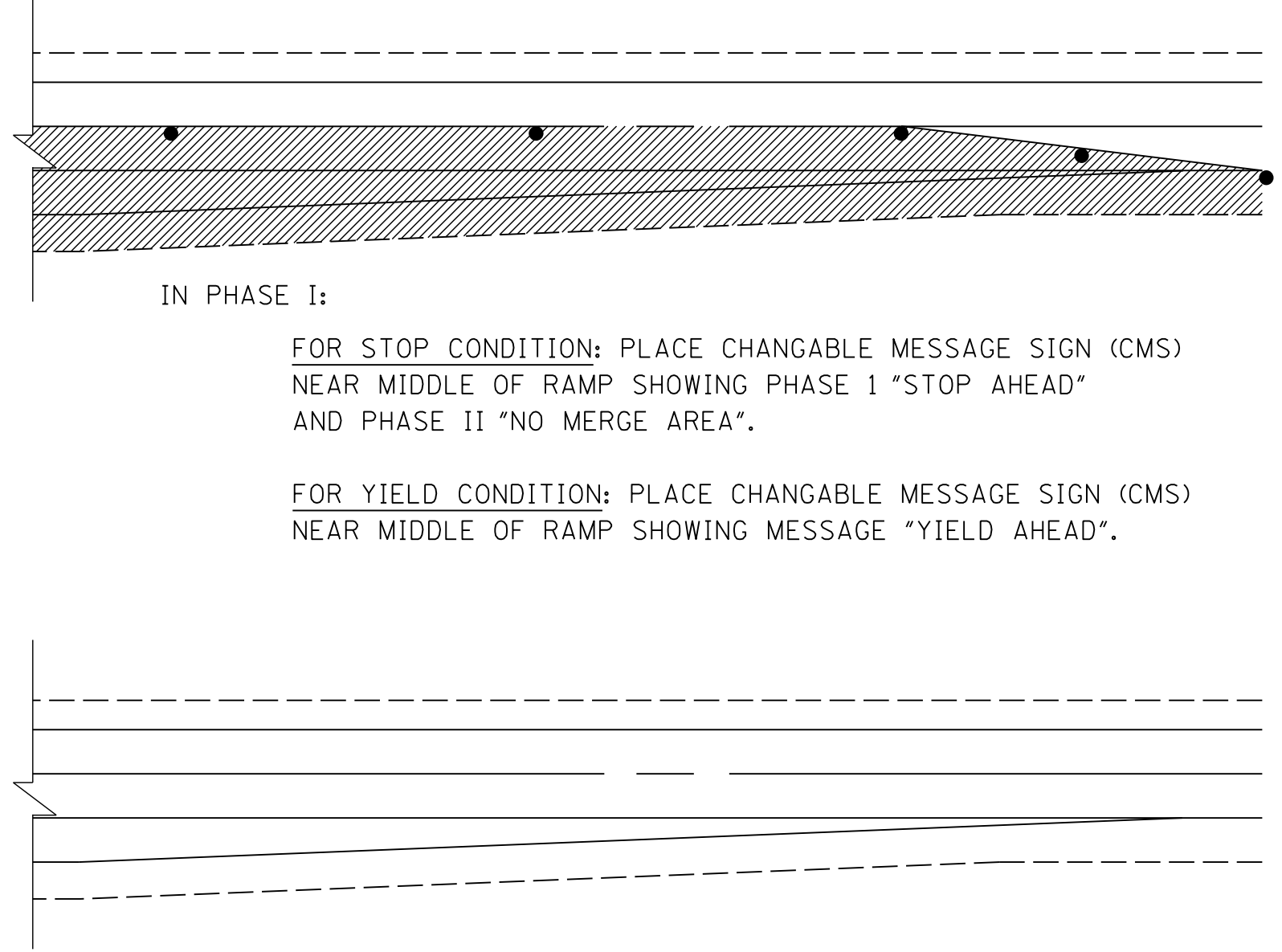
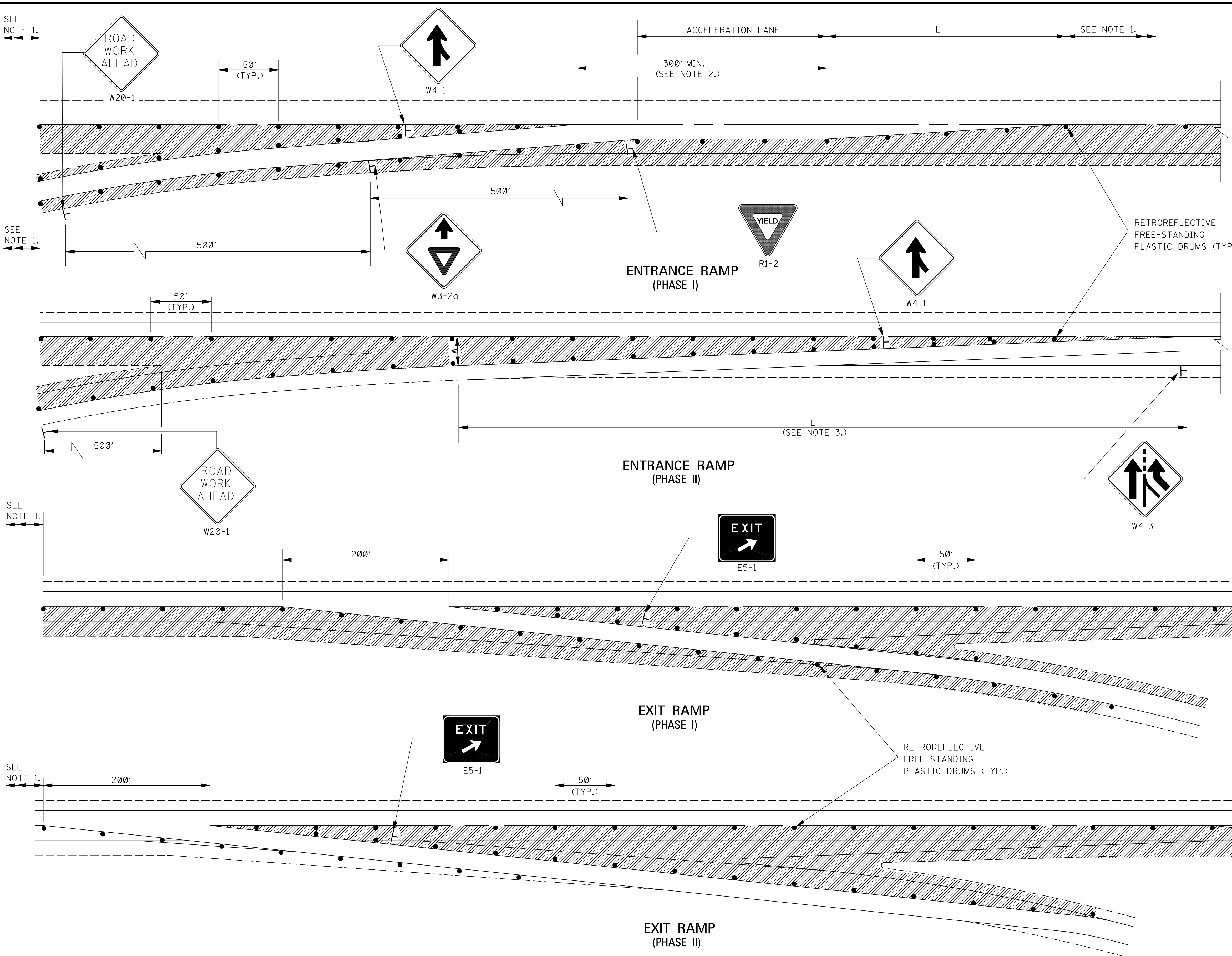


MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES FOR TWO-LANE OPERATION:

- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY, AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. SHADOW VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, THE SHADOW VEHICLE SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW VEHICLES SHALL BE EQUIPPED WITH TWO HIGH-INTENSITY FLASHING LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN. SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ARROW BOARD TO BE USED IN CAUTION MODE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

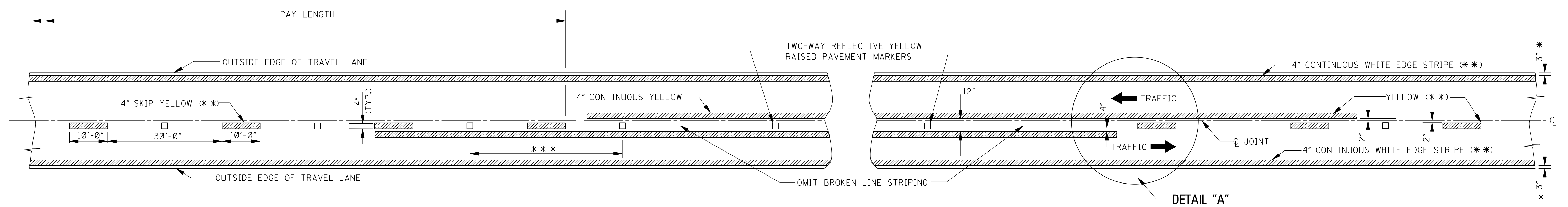
BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO-LANE ROADS	
DATE		ISSUE DATE: AUGUST 01, 2017	
		 WORKING NUMBER TCP-9 SHEET NUMBER 6359	



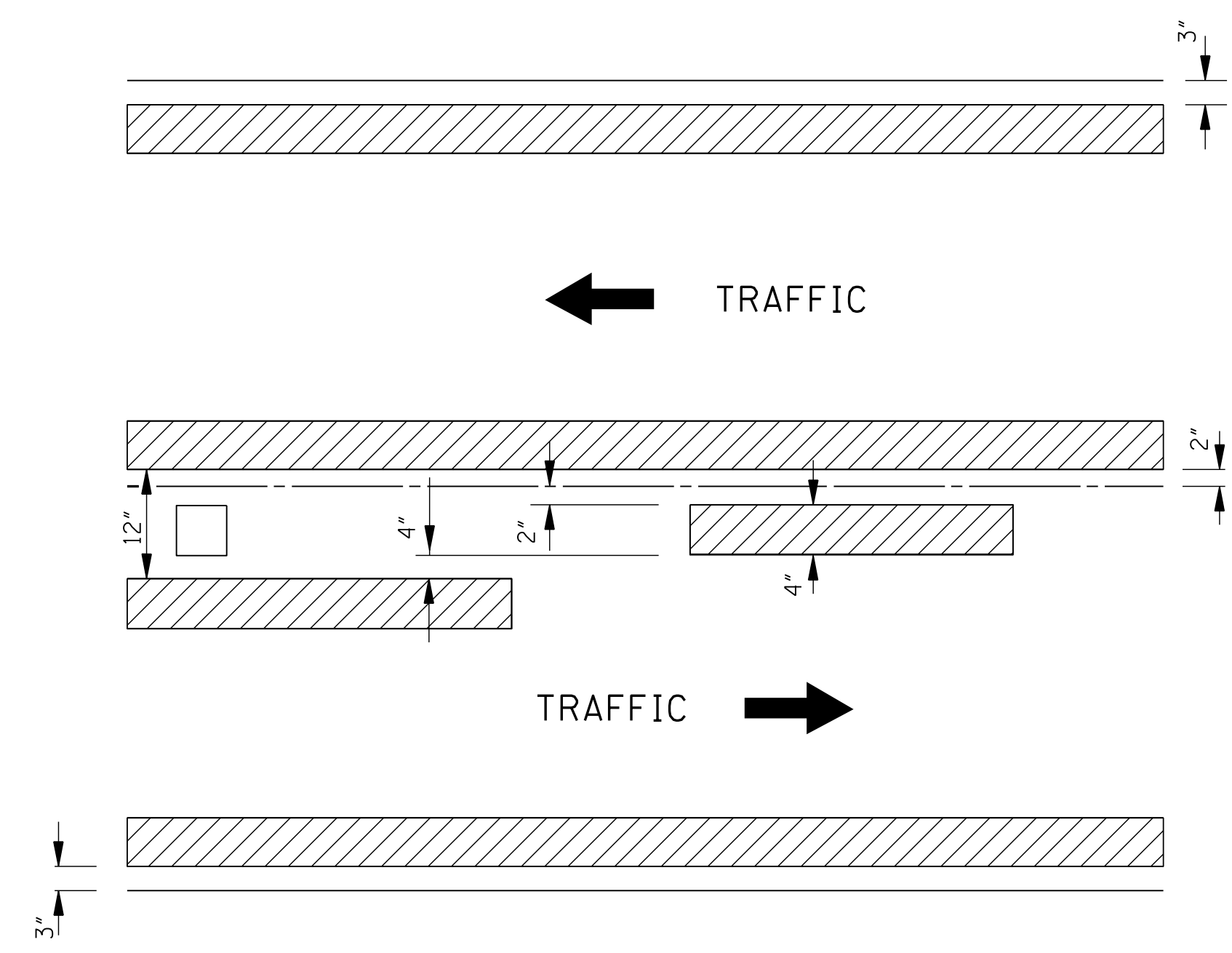
IN PHASE I:
 FOR STOP CONDITION: PLACE CHANGABLE MESSAGE SIGN (CMS) NEAR MIDDLE OF RAMP SHOWING PHASE I "STOP AHEAD" AND PHASE II "NO MERGE AREA".
 FOR YIELD CONDITION: PLACE CHANGABLE MESSAGE SIGN (CMS) NEAR MIDDLE OF RAMP SHOWING MESSAGE "YIELD AHEAD".

- GENERAL NOTES:
1. FOR SIGNING, FLASHING ARROW PANEL, AND DRUM SPACING CRITERIA, SEE TYPICAL TRAFFIC CONTROL PLANS FOR LANE CLOSURES ON SHEETS TCP-2, TCP-3, TCP-4, AND TCP-5, AS APPROPRIATE
 2. IF NO ACCELERATION LANE EXISTS FOR THE TEMPORARY ENTRANCE, THE YIELD SIGN (R1-2) AND THE YIELD AHEAD SIGN (W3-2A) SHALL BE REPLACED WITH A STOP SIGN (R1-1) AND A STOP AHEAD SIGN (W3-1a), ONE ON EACH SIDE OF APPROACH. WHERE STOP SIGNS ARE USED, A TEMPORARY STOP LINE SHOULD BE PLACED ACROSS THE RAMP AT THE DESIRED STOP LOCATION. IF INSUFFICIENT GAPS ARE AVAILABLE IN THE TRAFFIC STREAM, CONSIDERATION SHOULD BE GIVEN TO CLOSING THE RAMP.
 3. DETERMINE TAPER LAYOUTS USING THE FOLLOWING EQUATIONS:
 $L = WS$ FOR SPEED OF 45 mph OR GREATER
 $L = WS^2/60$ FOR SPEEDS OF 40 mph OR LESS
 WHERE:
 L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
 4. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		DETAILS OF OUTSIDE LANE CLOSURE AT EXIT AND ENTRANCE RAMP	
DATE		ISSUE DATE: AUGUST 01, 2017	
		 WORKING NUMBER TCP-10 SHEET NUMBER 6360	



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



DETAIL "A"



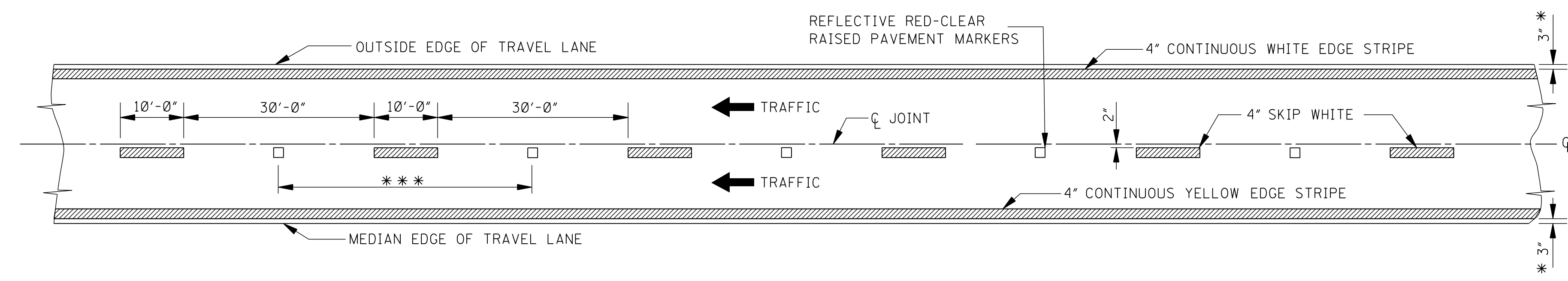
GENERAL NOTES:

- * 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
- 3. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS
- *** 4. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

	URBAN AREA (ft-in)	RURAL AREA (ft-in)
TANGENT SECTIONS	40'-0"	80'-0"
HORIZONTAL CURVES	40'-0"	40'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

† NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

5. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDT "APPROVED SOURCES OF MATERIALS."

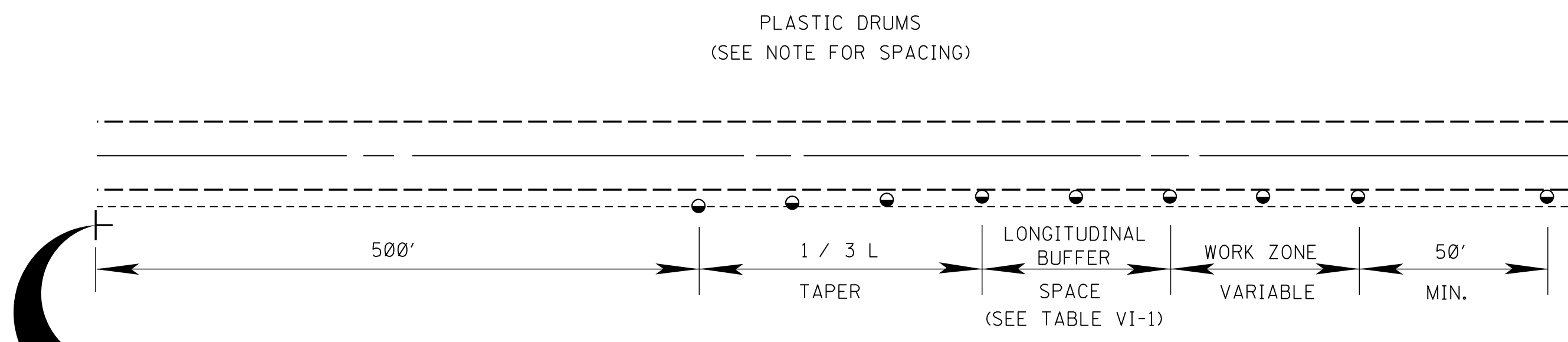


4-LANE WITH ONE-WAY TRAFFIC

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
REVISION	TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS
DATE	ISSUE DATE: AUGUST 01, 2017

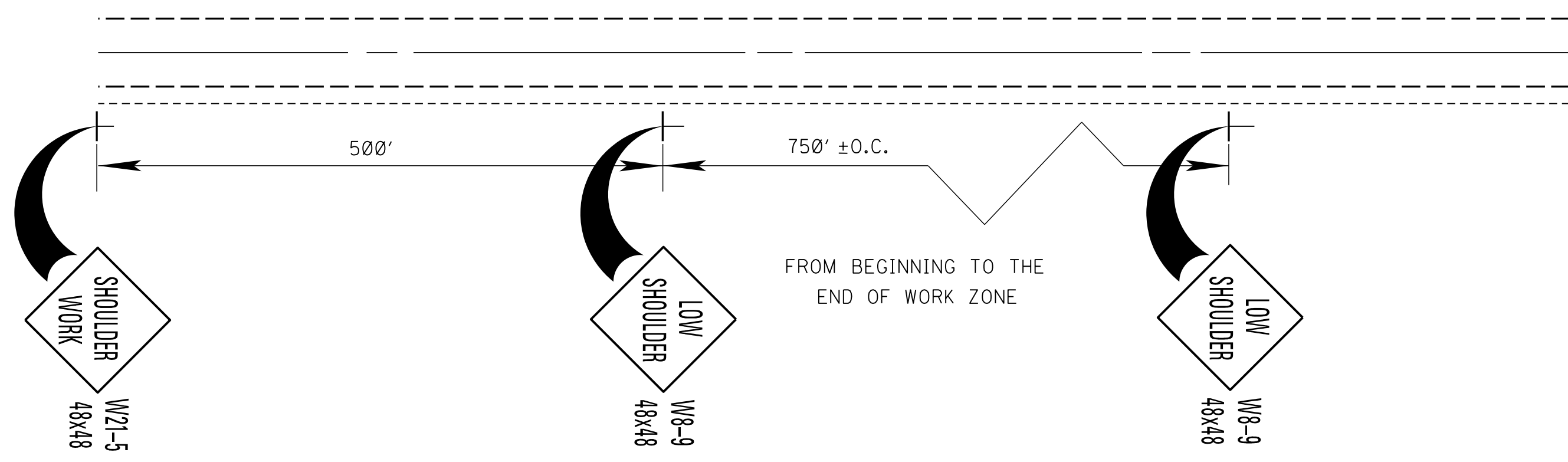
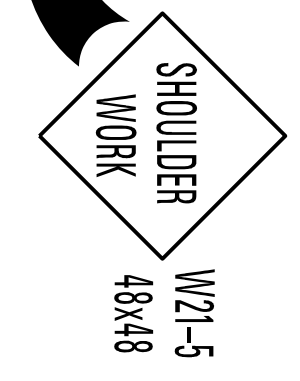


WORKING NUMBER
TCP-13
SHEET NUMBER
6363

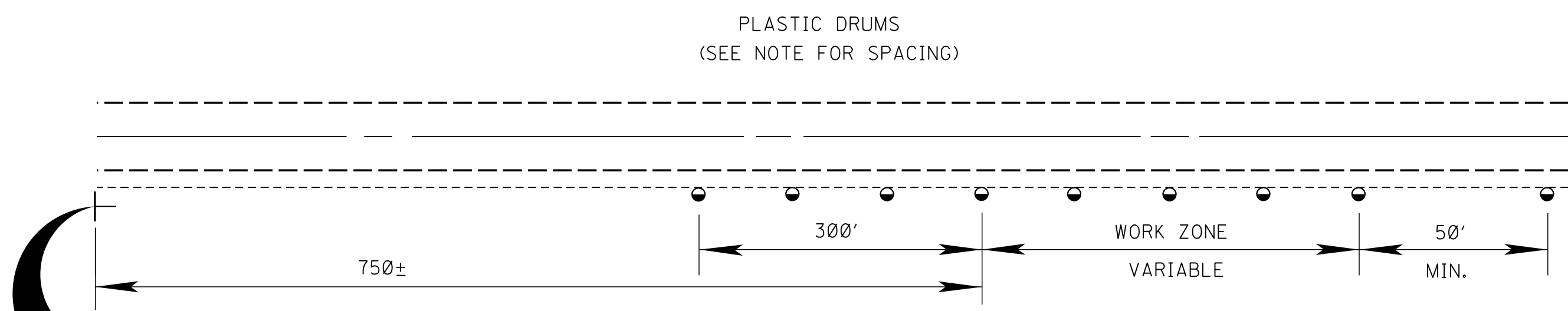


TYPICAL SHOULDER CLOSURE

- (1) TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
- (2) TO BE USED WHEN CONSTRUCTION VEHICLES (EQUIPMENT) ENCROACHES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.

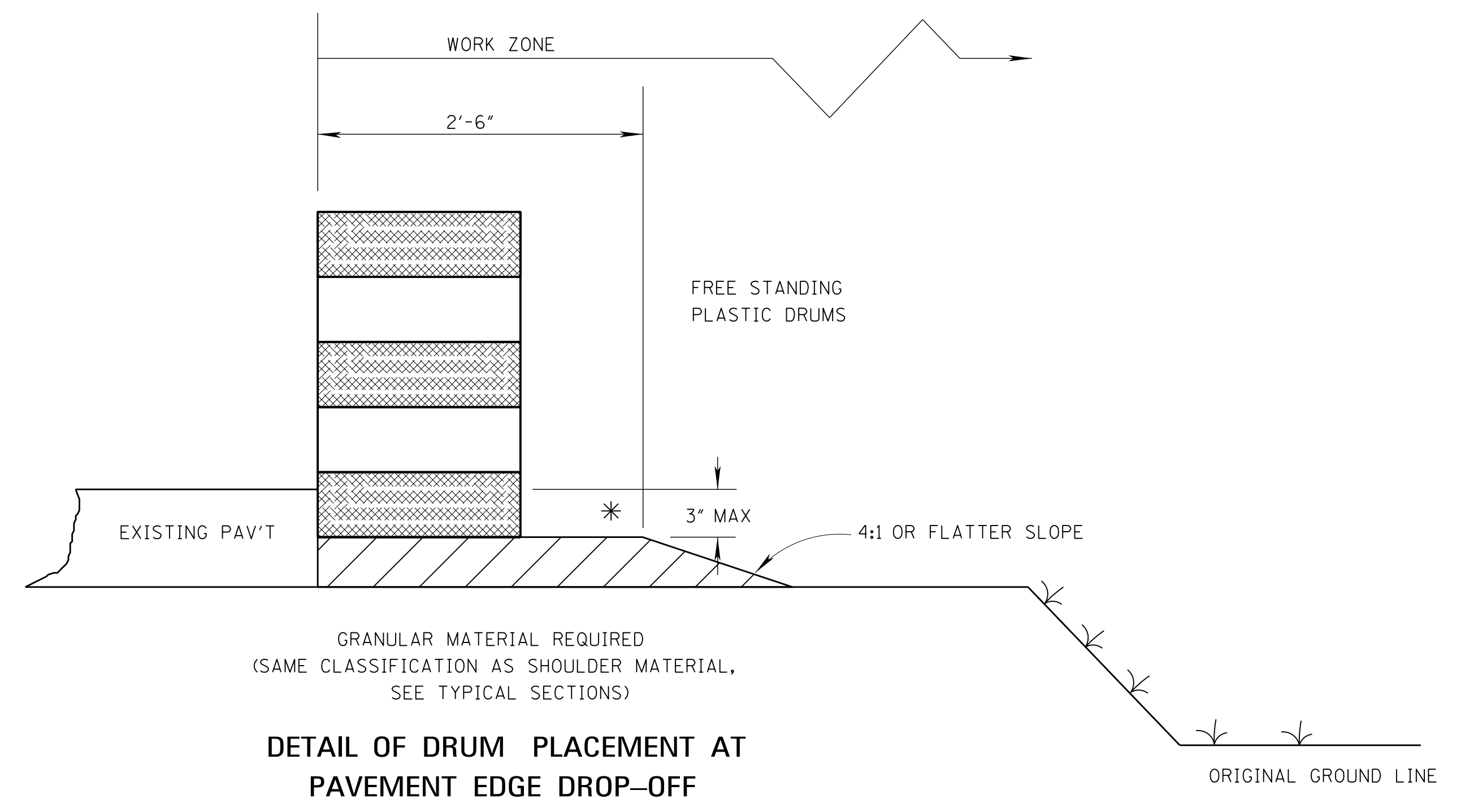
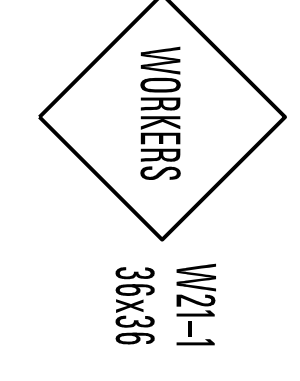


**TYPICAL SHOULDER WORK #1
(SEE NOTE A-1 THIS SHEET)**



TYPICAL SHOULDER WORK #2

NOTE:
WORK OUTSIDE TWO (2) FOOT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA, OR SEE NOTE A-3 THIS SHEET.



**DETAIL OF DRUM PLACEMENT AT
PAVEMENT EDGE DROP-OFF**

NOTES:

- * A. PAVEMENT EDGE DROP-OFF
 1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750'±O.C.).
 2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. CONES MAY BE USED IN PLACE OF DRUMS, PANELS, AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE M.U.T.C.D. (1 / 3 L, WHERE L IS THE TAPER LENGTH IN FEET.)
 3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 4:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED.
 4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
 5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.
- B. DRUM SPACING
 1. TANGENTS = 2 X S
 2. TAPERS = L / 3

WHERE L = S X W
L = TAPER LENGTH IN FEET
S = SPEED IN MPH (POSTED OR 85 PERCENTILE)
W = WIDTH OF OFFSET IN FEET
- C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

TABLE VI-1. GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

* * SPEED (MPH)	LENGTH (FEET)
20	35
25	55
30	85
35	120
40	170
45	220
50	280
55	335
60	415
65	485

* * POSTED SPEED, OFF-PEAK 85 PERCENTILE SPEED PRIOR TO WORK STARTING, OR THE ANTICIPATED OPERATING SPEED IN MPH.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TRAFFIC CONTROL DETAILS DRUM PLACEMENT AND SHOULDER CLOSURE	
BY	
REVISION	
DATE	ISSUE DATE: AUGUST 01, 2017
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