

STATE	PROJECT NUMBER	SHEET NO.
MISSISSIPPI	NH-0011-03(086)	1

**GENERAL INDEX**

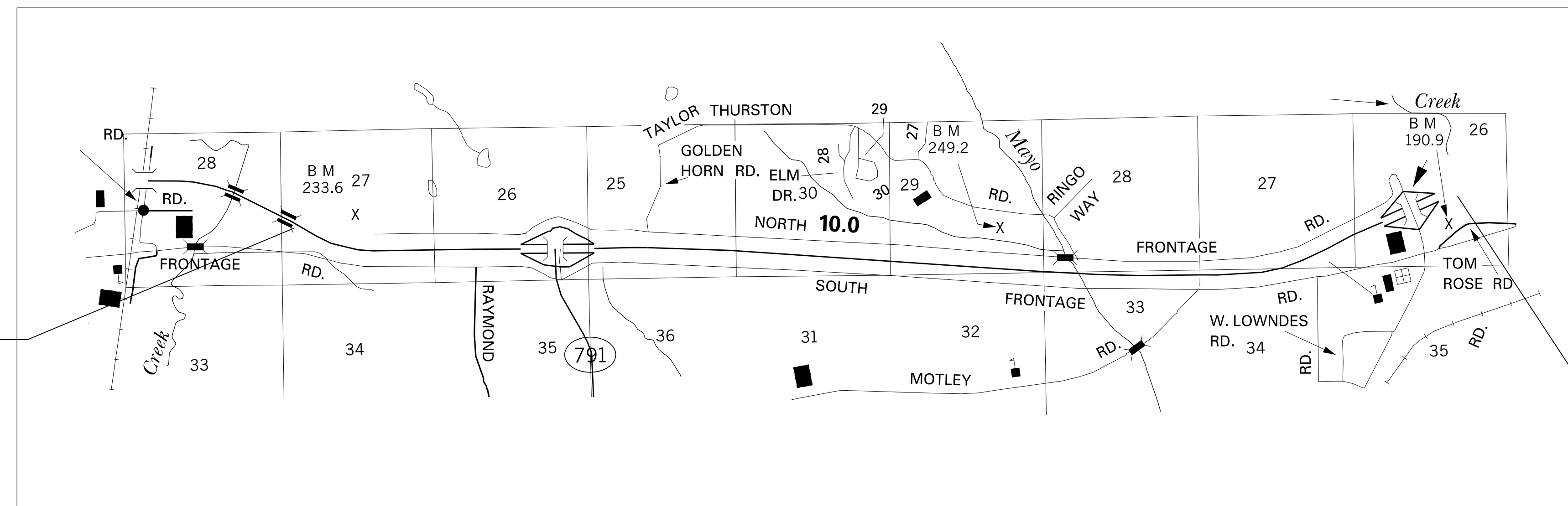
INCLUDED THIS PROJECT	BEGIN WITH SHEET
<input checked="" type="checkbox"/> ROADWAY .....	1
<input type="checkbox"/> PERMANENT SIGNS .....	1001
<input type="checkbox"/> TRAFFIC SIGNALS .....	2001
<input type="checkbox"/> ITS COMPONENTS .....	3001
<input type="checkbox"/> LIGHTING .....	4001
<input type="checkbox"/> (RESERVED) .....	5001
<input checked="" type="checkbox"/> ROADWAY STANDARD DWGS .....	6001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (LRFD) ....	7001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (STD. SPEC.)	7501
<input type="checkbox"/> BRIDGE .....	8001
<input type="checkbox"/> CROSS SECTIONS .....	9001

**BRIDGE STRUCTURES REQ'D.**  
**NONE**

**BOX BRIDGES REQ'D.**  
**NONE**

**B.O.P. 553+00**

**E.O.P. 955+80**



**STATE OF MISSISSIPPI**

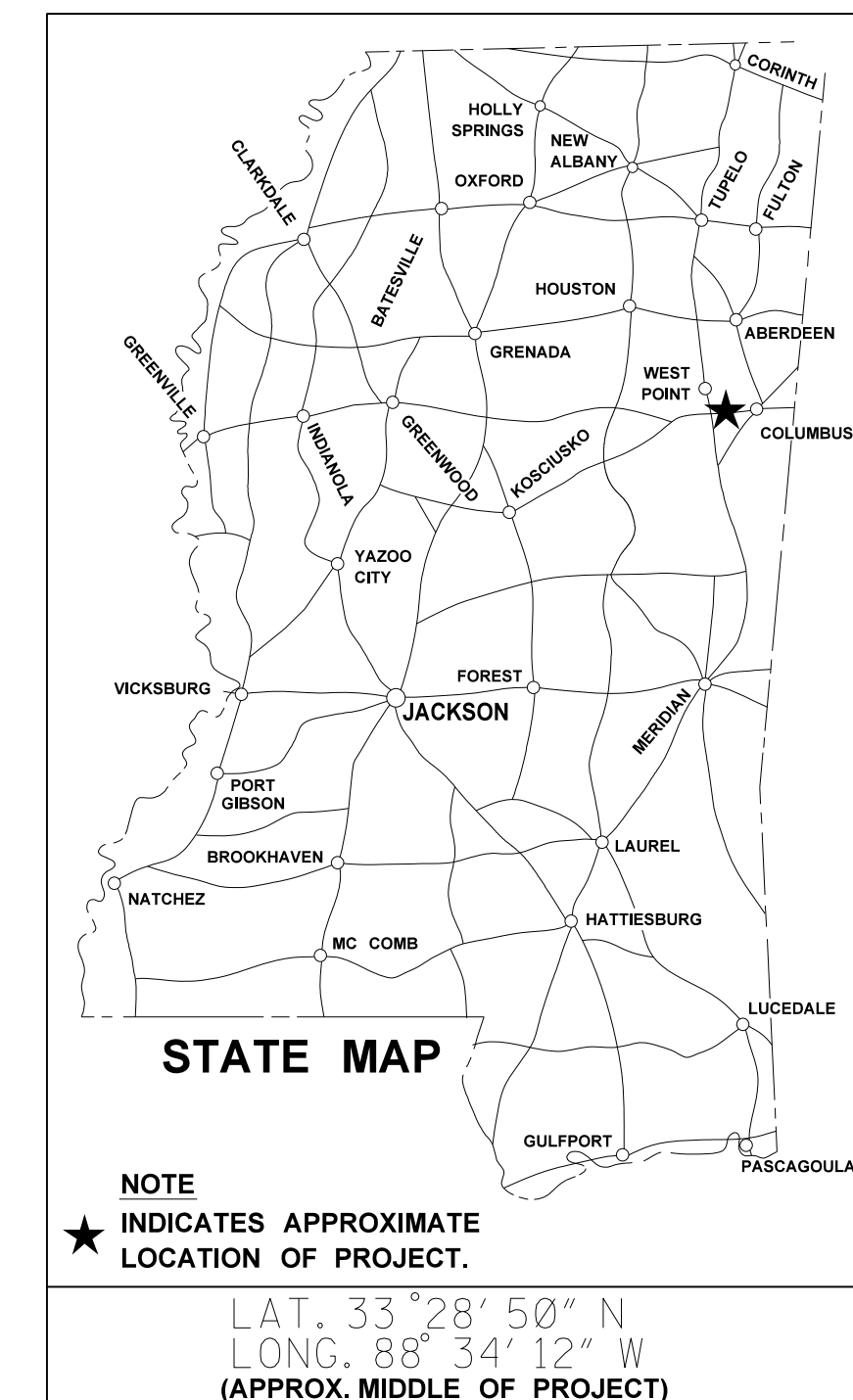
**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**PLAN AND PROFILE OF PROPOSED STATE HIGHWAY STATE PROJECT NO. NH-0011-03(086)**

**US 82 FROM CATALPA CREEK FMS CON. NO. 107779/301000 TO NEAR HWY 45 S LOWNDES CO**

**SCALES**

PLAN	1 IN. = 100 FT.
PROFILE	HOR. 1 IN. = 100 FT.
	VERT. 1 IN. = 10 FT.
LAYOUT	1 IN. = FT.



**DESIGN CONTROL**

MPH = V (SPEED DESIGN)

ADT ( ) = : ADT ( ) =

DHV = : D = % T = %

**PERMITS ACQUIRED BY MDOT**

WETLANDS AND WATERS PERMITS		
	WATERS	WETLANDS
NATIONWIDE #14	N	N
NATIONWIDE (OTHER)*	N	N
GENERAL*	N	N
INDIVIDUAL (404)*	N	N
STORMWATER PERMIT	N	
Y	REQUIRED CNOI SUBMITTED BY MDOT (DISTURBED AREA = 5 ACRES)	
S	REQUIRED SCNOI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES)	
N	NO STORMWATER PERMIT REQUIRED (<1 ACRE)	

APPROVED BY: \_\_\_\_\_

**CONVENTIONAL SYMBOLS**

COUNTY LINE	-----
TOWN CORPORATION LINE	-----
SECTION LINE	-----
EXISTING ROAD OR TRAVELED WAY	-----
PROPOSED ROAD OR TRAVELED WAY	-----
RAILROAD	-----
SURVEY LINE	-----
BRIDGES	-----

**EQUATIONS**

**NONE**

**EXCEPTIONS**

**NONE**

**LENGTH DATA**

LENGTH OF ROADWAY	40280 FT.	7.63 MI.
LENGTH OF BRIDGES	0 FT.	0 MI.
LENGTH OF PROJECT (NET)	40280 FT.	7.63 MI.
LENGTH OF EXCEPTIONS	0 FT.	0 MI.
LENGTH OF PROJECT (GROSS)	40280 FT.	7.63 MI.

P S & E DATE: 12/13/2019

APPROVED:	_____
DEPUTY EXECUTIVE DIRECTOR / CHIEF ENGINEER	_____
EXECUTIVE DIRECTOR	_____



001 00 AMPM DGNF LLENAM MISSISSIPPI DEPARTMENT OF TRANSPORTATION

STATE	PROJECT NO.
MISS.	NH-0011-03(086)

GENERAL NOTES:

- 1.) WHERE MILLING OF THE ROADWAY LANES IS REQUIRED, THE CONTRACTOR SHALL PROVIDE OUTLETS IN THE EXISTING SHOULDERS AT SUFFICIENT INTERVALS TO PREVENT POOLING OR STANDING WATER ON THE MILLED SURFACE.(ABSORBED ITEM)
- 2.) VEGETATIVE MATERIAL WILL BE REMOVED PRIOR TO PLACEMENT OF GRANULAR MATERIAL. COST TO BE ABSORBED IN OTHER ITEMS.
- 3.) NO GRANULAR MATERIAL SHALL BE PLACED DIRECTLY UPON THE FINISHED SURFACE COURSE.
- 4.) FLOURESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.
- 5.) ALL TRAFFIC CONTROL DEVICES ON THIS PROJECT SHALL COMPLY WITH PART VI OF THE MUTCD (LATEST EDITION).
- 6.) ALL PLASTIC DRUMS SHALL HAVE A BALLASTING COLLAR MADE FROM RECYCLED TRUCK TIRES OR OTHER SUITABLE MATERIAL.
- 7.) THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING STRUCTURES SUCH AS, BUT NOT LIMITED TO, PIPES, INLETS, APRONS, AND BRIDGES FROM DAMAGE WHICH MIGHT OCCUR DURING CONSTRUCTION. THE CONTRACTOR SHALL REPLACE OR REPAIR, AS DIRECTED BY THE ENGINEER, ANY STRUCTURES DAMAGED DURING THE LIFE OF THE CONTRACT. NO PAYMENT WILL BE MADE FOR REPLACEMENT OR REPAIR OF DAMAGED ITEMS.
- 8.) BIDDERS ARE ADVISED THAT HARD COPIES OF ANY ADDENDA FOR THIS PROJECT WILL NO LONGER BE MAILED. ALL ADDENDA FOR THIS PROJECT WILL BE POSTED TO [www.mdof.ms.gov](http://www.mdof.ms.gov) UNDER THE PROPOSAL ADDENDA COLUMN. IT IS THE BIDDER'S RESPONSIBILITY TO CHECK AND SEE IF ANY ADDENDA HAVE BEEN POSTED FOR THIS PROJECT. PLEASE CONTACT CONTRACT ADMINISTRATION DIVISION AT 601-359-7700 FOR ANY QUESTIONS REGARDING ELECTRONIC ADDENDA.
- 9.) AFTER THE PERMANENT SIGNS HAVE BEEN INSTALLED, THE CONTRACTOR SHALL SUBMIT TO THE PROJECT ENGINEER A DIGITAL COPY OF A MICROSOFT EXCEL SPREADSHEET WITH THE FOLLOWING INVENTORY DATA CAPTURED FOR EACH SIGN: LOCATION OF SIGN (LATITUDE-LONGITUDE GPS COORDINATES), MUTCD SIGN CODE, SIZE, BACKGROUND AND LEGEND COLORS, SUPPORT TYPE (POST, PIPE, SQUARE POST, OR I-BEAM), NUMBER OF SUPPORTS, DATE OF INSTALLATION, SIGN FACE DIRECTION, ROUTE NAME, OR NUMBER, DIRECTION OF VEHICLE TRAVEL, AND LEGEND ON SIGN IF APPLICABLE. EACH SIGN SHALL BE ASSIGNED A UNIQUE ID NUMBER AND A DIGITAL PHOTO OF EACH SIGN SHALL BE SUBMITTED IN A BITMAP FORMAT. THE PHOTO FILENAME SHALL CORRESPOND WITH THE UNIQUE ID NUMBER.

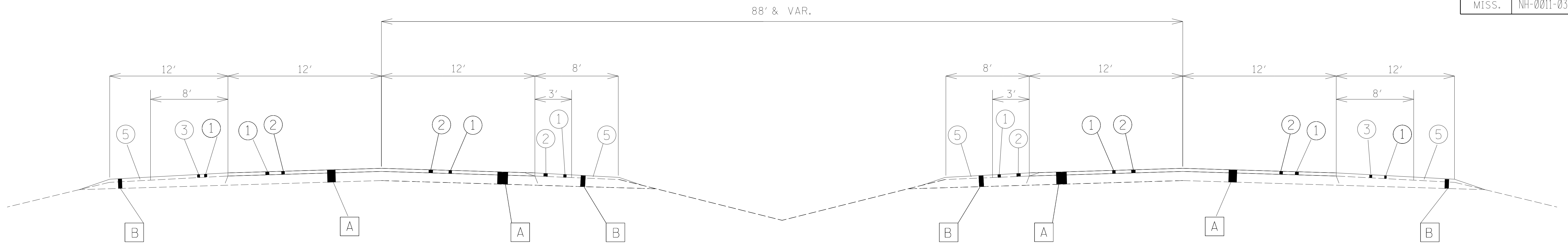
DESCRIPTION OF SHEET

DESCRIPTION OF SHEET	WKG. NO.	SH. NO.
TITLE SHEET (1)		1
DETAILED INDEX AND GENERAL NOTES (1)	DI-1	2
TYPICAL SECTION SHEET (4)		
TYPICAL MILLINGS AND OVERLAY SECTIONS OF MAINLINE	TS-1	3
TYPICAL SECTION INTERSECTION DETAIL	MD-1	4
TYPICAL SECTION PAVEMENT DETAIL	MD-2	5
INTERSECTION DETAIL PAVEMENT MARKINGS	MD-3	6
QUANTITY SHEETS (2)		
SUMMARY OF QUANTITIES	SQ-1	7-8
STANDARD ROADSIDE SIGN QUANTITIES (1)	SRS-1	9
DETAIL OF CONSTRUCTION SIGNING (1)	CS-1	10
STANDARD DRAWINGS - ROADWAY SHEETS (13)		
PAVEMENT MARKING DETAILS FOR 2-LANE AND 4-LANE DIVIDED HIGHWAYS	PM-1	6051
PAVEMENT MARKING DETAILS FOR INTERCHANGE ENTRANCE RAMPS	PM-3	6053
PAVEMENT MARKING DETAILS FOR INTERCHANGE EXIT RAMPS	PM-4	6054
2-WAY RAISED PAVEMENT MARKERS AT INTERSECTING ROADS (4-LANE)	PM-12	6062
RUMBLE STRIPES 4-LANE HIGHWAYS (ASPH. LANES, 2-FT ASPH. SHLD)	RS-2	6065
STANDARD ROADSIDE SIGN ASSEMBLY AND INSTALLATION	SN-4B	6308
TYPICAL GUARDRAIL DELINEATION	SN-8C	6317
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT LESS THAN 65 MPH	TCP-2	6352
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH	TCP-5	6355
HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS	TCP-8	6358
TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO-LANE ROADS	TCP-9	6359
DETAILS OF OUTSIDE LANE CLOSURE AT EXIT AND ENTRANCE RAMPS	TCP-10	6360
TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS	TCP-13	6363
TOTAL SHEETS = 23		

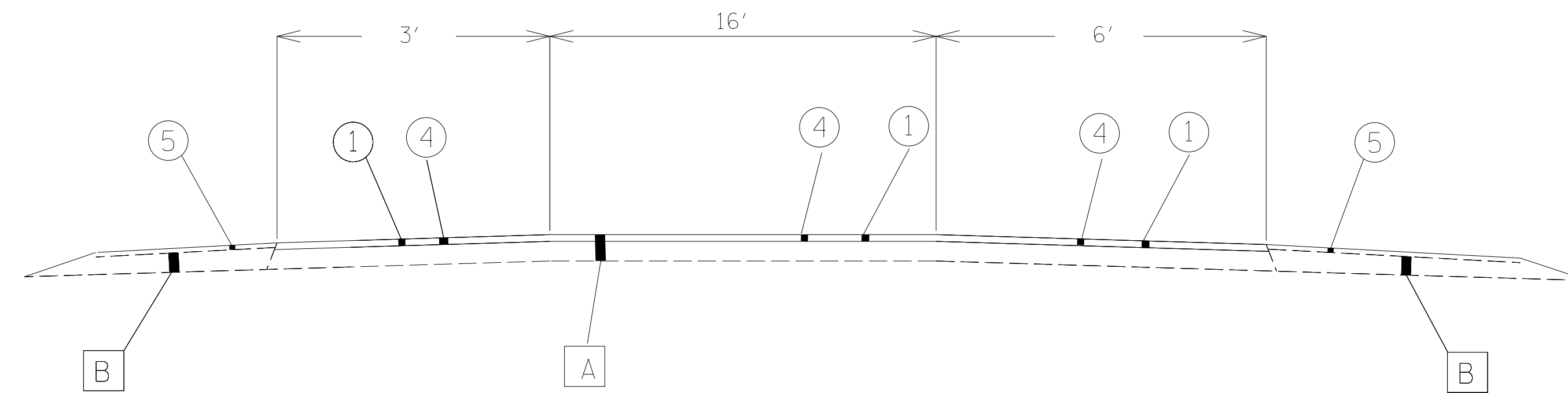
DISTRICT 1

PS & E PLANS-DATE 12/13/2019		
FMS CON. # 107779/301000		
REVISIONS		
DATE	SHEET NO.	BY

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
<b>DETAILED INDEX</b>	
PROJECT NO: NH-0011-03(086) COUNTY: LOWNDES	
WORKING NUMBER DI-1	SHEET NUMBER 2
DATE	FILENAME: DESIGN TEAM _____ CHECKED _____ DATE _____



TYPICAL MILLING AND OVERLAY SECTION FOR 4-LANE SECTION  
STA 553+00 - 955+80



TYPICAL MILLING AND OVERLAY SECTION FOR INTERCHANGE RAMP  
HWY 82

A VAR. THICKNESS ASPHALT PAVEMENT IN PLACE.  
B VAR. THICKNESS GRANULAR MATERIAL IN PLACE.

NOT TO SCALE

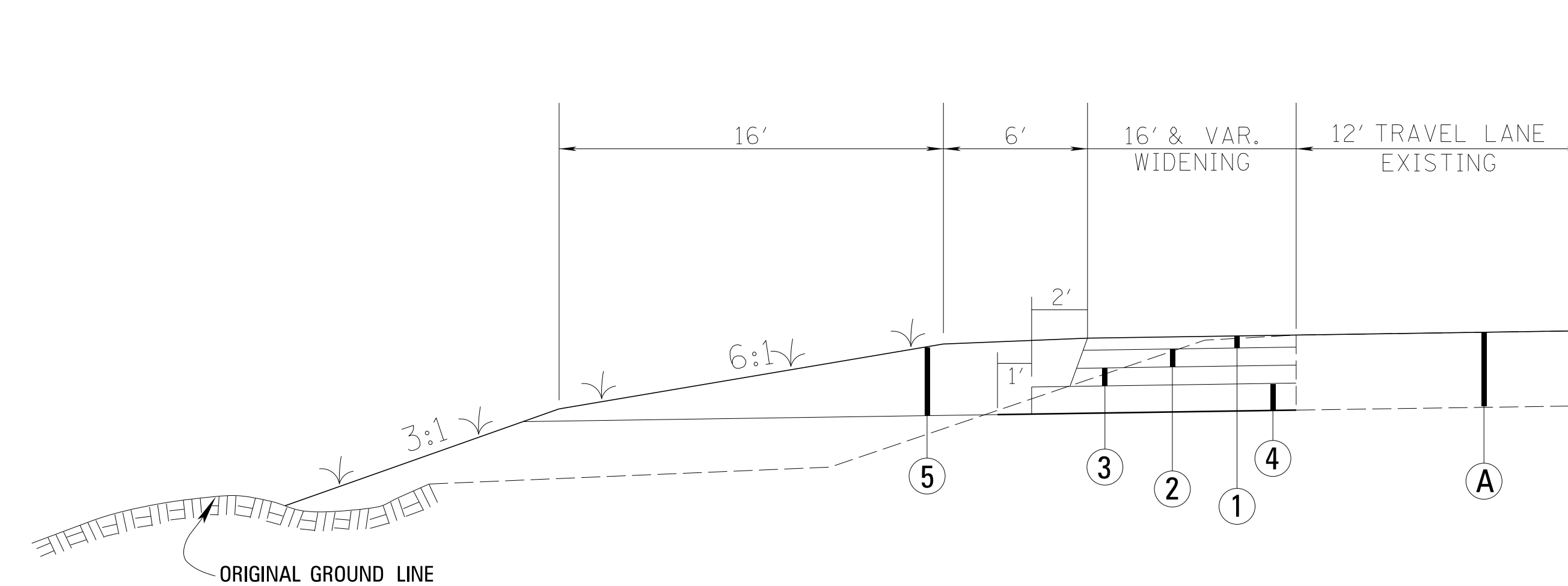
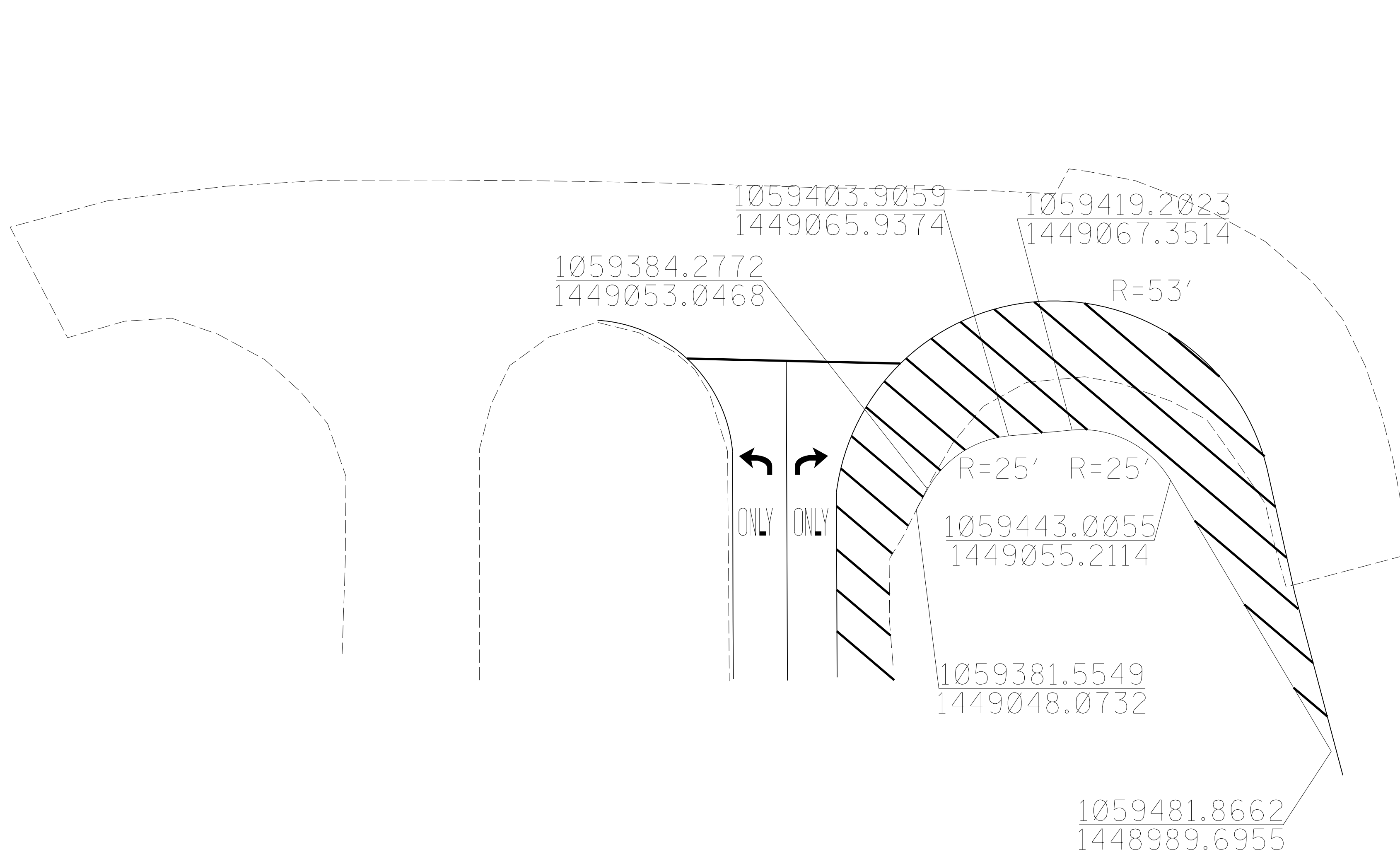
NOTE:

- 1 NO GRANULAR MATERIAL SHALL BE PLACED DIRECTLY UPON THE FINISHED SURFACE COURSE.
- 2 VEGETATIVE MATERIAL WILL BE REMOVED PRIOR TO PLACEMENT OF GRANULAR MATERIAL. COST TO BE ABSORBED IN OTHER ITEMS.
- 3 WHERE MILLING OF THE ROADWAY LANES IS REQUIRED, THE CONTRACTOR SHALL PROVIDE OUTLETS IN THE EXISTING SHOULDERS AT SUFFICIENT INTERVALS TO PREVENT POOLING OR STANDING WATER ON THE MILLED SURFACE. (ABSORBED ITEM)

- 1 1.5" & VARIABLE FINE MILLING OF BITUMINOUS PAVEMENT, ALL DEPTHS PAY ITEM NO. 406-D001.
- 2 1.5" ASPHALT, SMA, 9.5-MM MIXTURE, PAY ITEM NO. 405-A002
- 3 1.5" ASPHALT, ST, 9.5-MM MIXTURE, PAY ITEM NO. 403-A015
- 4 1.5" ASPHALT, MT, 9.5-MM MIXTURE, PAY ITEM NO. 403-A014
- 5 VARIABLE GRANULAR MATERIAL, LVM, CLASS 3, GROUP D, REQUIRED ON SHOULDERS PAY ITEM NO. 304-A003.

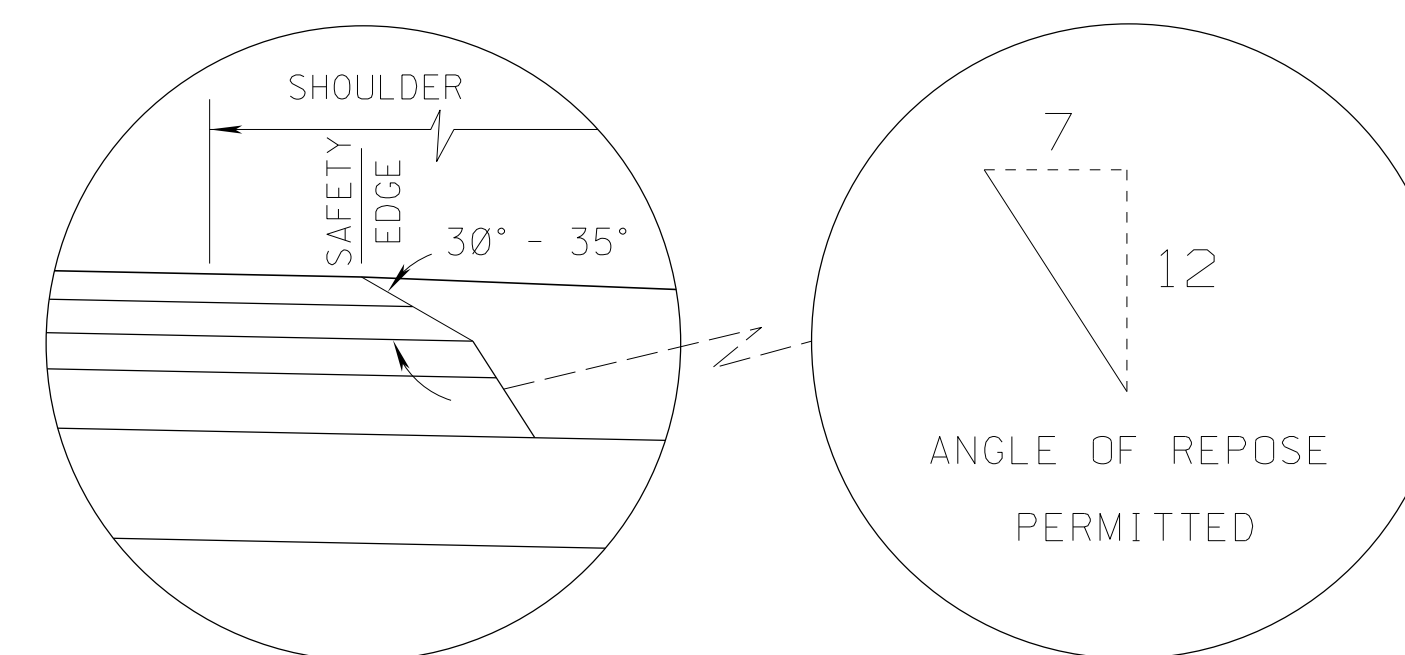
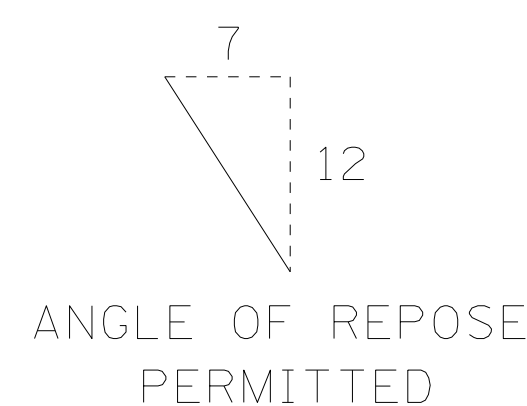
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TYPICAL MILLING AND OVERLAY SECTIONS	
PROJECT NO: NH-0011-03(086) COUNTY: LOWNDES	
FILENAME:	DESIGN TEAM _____ CHECKED _____ DATE _____
WORKING NUMBER TS-1	SHEET NUMBER 3

STATE	PROJECT NO.
MISS.	NH-0011-03(086)




### PROPOSED PAVEMENT STRUCTURE

- ① 1 1/2" - 9.5mm HT, Asphalt Pavement, Polymer Modified, PAY ITEM: 403-D007
- ② 2" - 12.5mm HT, Asphalt Pavement, Polymer Modified, PAY ITEM: 403-D001
- ③ 2 3/4" - 19mm HT, Asphalt Pavement, PAY ITEM: 403-A004
- ④ 6" - 3/4" and Down Crushed Stone Base LVM, PAY ITEM: 304-H001 or Size 610 Crushed Stone Base LVM, PAY ITEM: 304-H002 or Size 825B Crushed Stone Base LVM, PAY ITEM: 304-H003
- ⑤ 10 1/4" - Granular Material LVM, Class 3, Group D, PAY ITEM: 304-A003

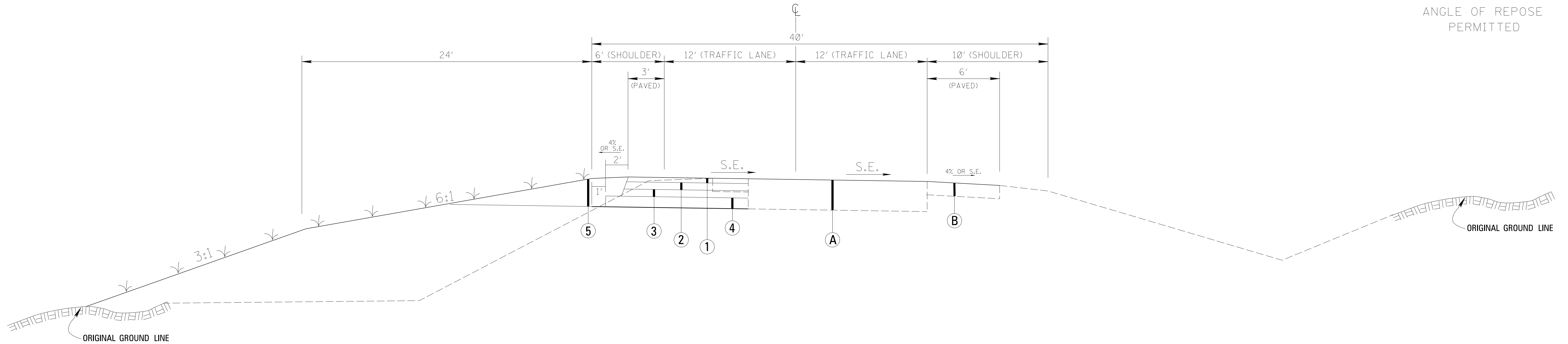
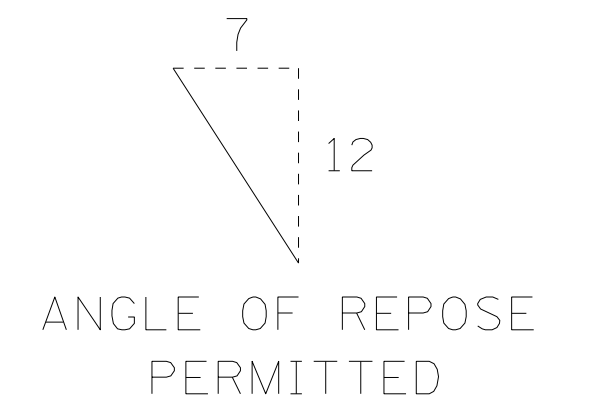


**SAFETY EDGE REQ'D  
TOP 2 LIFTS ONLY  
(NOT A PAY ITEM)  
NEW CONSTRUCTION**

### Ⓐ EXISTING ML PAVEM'T

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
<b>TYPICAL SECTION INTERSECTION DETAIL</b>	
	
PROJ. NO.:	NH-0011-03(086)
COUNTY:	LOWNDES
WORKING NUMBER	MD-1
FILE NAME:	
DATE:	
DESIGN TEAM:	CHECKED: DATE:
SHEET NUMBER	<b>4</b>

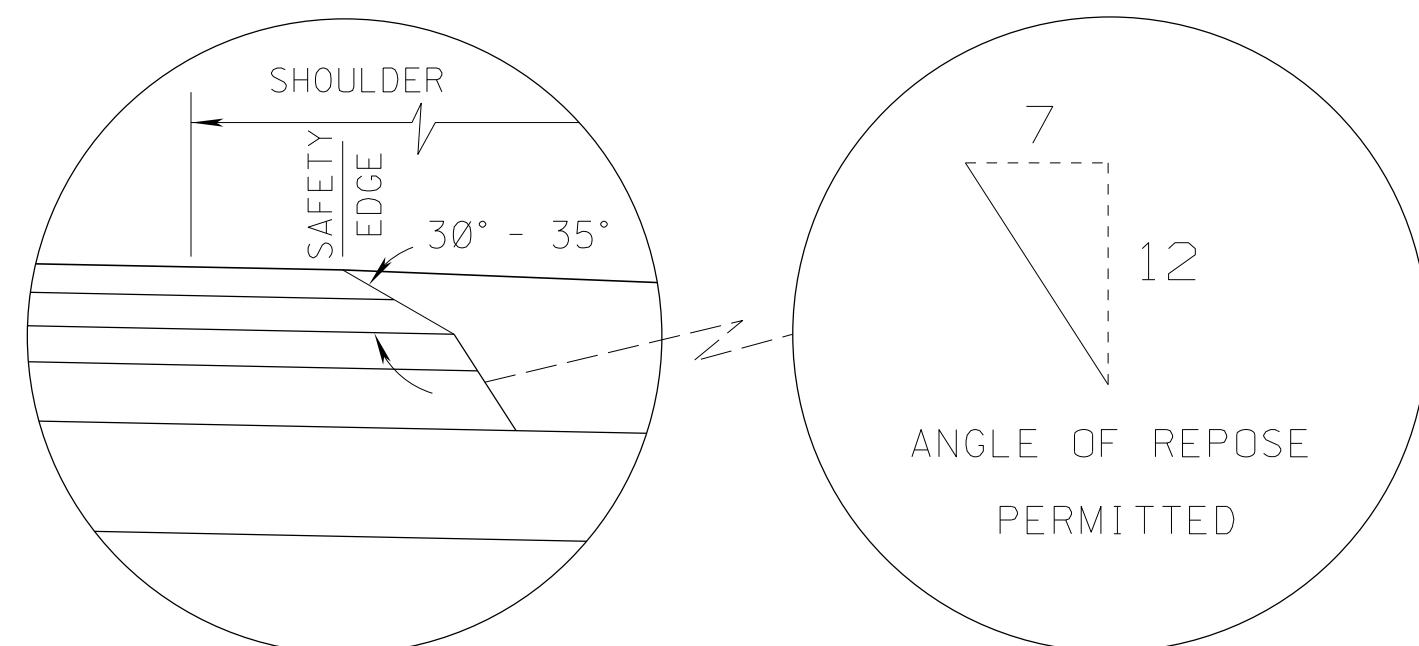
STATE	PROJECT NO.
MISS.	NH-0011-03(086)



### PROPOSED PAVEMENT STRUCTURE

- ① 1½" - 9.5mm HT, Asphalt Pavement, Polymer Modified, PAY ITEM: 403-D007
- ② 2" - 12.5mm HT, Asphalt Pavement, Polymer Modified, PAY ITEM: 403-D001
- ③ 2¾" - 19mm HT, Asphalt Pavement, PAY ITEM: 403-A004
- ④ 12" - ¾" and Down Crushed Stone Base LVM, PAY ITEM: 304-H001 or Size 610 Crushed Stone Base LVM, PAY ITEM: 304-H002 or Size 825B Crushed Stone Base LVM, PAY ITEM: 304-H003
- ⑤ 18¼" - Granular Material LVM, Class 3, Group D. PAY ITEM: 304-A003

- Ⓐ EXISTING ML PAVEM'T
- Ⓑ EXISTING PAVED SHOULDER



**SAFETY EDGE REQ'D  
TOP 2 LIFTS ONLY  
(NOT A PAY ITEM)  
NEW CONSTRUCTION**

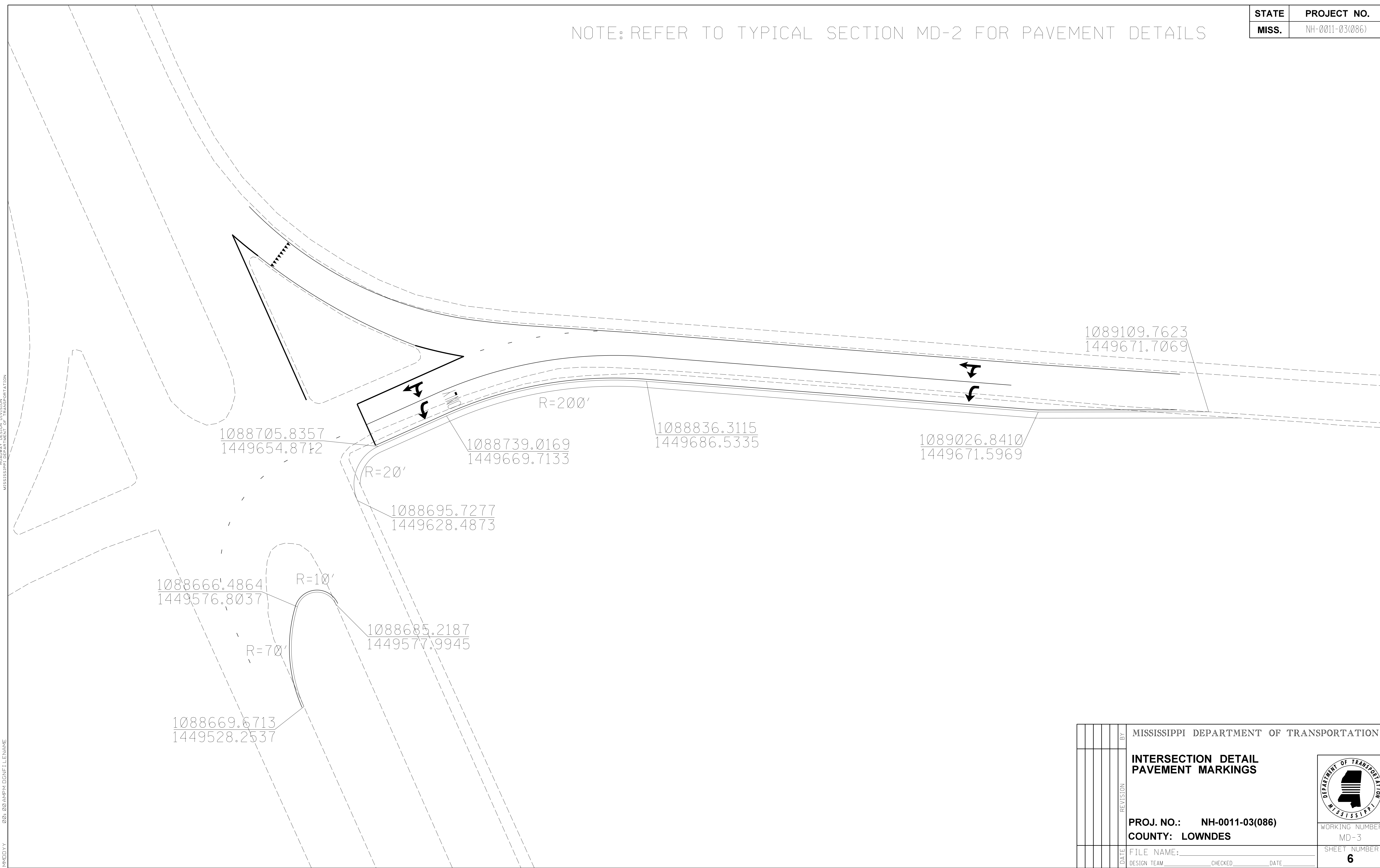
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
<b>TYPICAL SECTION PAVEMENT DETAIL</b>	
PROJ. NO.:	NH-0011-03(086)
COUNTY:	LOWNDES
FILE NAME:	DESIGN TEAM _____ CHECKED _____ DATE _____
WORKING NUMBER	MD-2
SHEET NUMBER	<b>5</b>




STATE	PROJECT NO.
MISS.	NH-0011-03(086)

NOTE: REFER TO TYPICAL SECTION MD-2 FOR PAVEMENT DETAILS

ROADWAY DESIGN DIVISION  
MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
001 00 AMPM DGNFILENAME  
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MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
<b>INTERSECTION DETAIL PAVEMENT MARKINGS</b>	
	
<b>PROJ. NO.: NH-0011-03(086)</b> <b>COUNTY: LOWNDES</b>	
WORKING NUMBER MD-3	SHEET NUMBER <b>6</b>
FILE NAME: _____ DESIGN TEAM _____ CHECKED _____ DATE _____	

STATE	PROJECT NO.
MISS	NH-0011-03(086)

① Quantity Includes 50% Shrinkage Factor


SUMMARY OF QUANTITIES (SHEET 1)				
PAY ITEM NO.	PAY ITEM	UNIT	LOWNDES : 107779-301000	
			Prelim	Final
202-B215	Removal of Sign Including Post & Footing	EA	3	
202-B240	Removal of Traffic Stripe	LF	2,750	
203-EX040	Borrow Excavation, AH, LVM, Class B9	CY	500	
203-G001	Excess Excavation, FM, AH	CY	400	
209-A005	Geotextile Stabilization, Type V, Non-Woven	SY	900	
216-A001	Solid Sodding	SY	55	
304-A003	Granular Material, LVM, Class 3, Group D	CY	2,300	①
304-H001	3/4" and Down Crushed Stone Base, LVM	CY	235	①
	OR			
304-H002	Size 610 Crushed Stone Base, LVM	CY	235	①
	OR			
304-H003	Size 825B Crushed Stone Base, LVM	CY	235	①
403-A004	19-mm, HT, Asphalt Pavement	TON	135	
403-A014	9.5-mm, MT, Asphalt Pavement	TON	3,500	
403-A015	9.5-mm, ST, Asphalt Pavement	TON	3,150	
403-D001	12.5-mm, HT, Asphalt Pavement, Polymer Modified	TON	100	
403-D007	9.5-mm, HT, Asphalt Pavement, Polymer Modified	TON	1,650	
405-A002	Stone Matrix Asphalt, 9.5 mm Mixture	TON	21,750	
406-D001	Fine Milling of Bituminous Pavement, All Depths	SY	355,000	
407-A001	Asphalt for Tack Coat	GAL	18,000	
423-A001	Rumble Strips, Ground In	MI	32	
618-A001	Maintenance of Traffic	LS	1	
618-B001	Additional Construction Signs	SF	1	
619-A1001	Temporary Traffic Stripe, Continuous White	MI	32	
619-A2001	Temporary Traffic Stripe, Continuous Yellow	MI	32	
619-A3001	Temporary Traffic Stripe, Skip White	MI	34	
619-A5001	Temporary Traffic Stripe, Detail	LF	39,000	
619-A6002	Temporary Traffic Stripe, Legend	LF	2,300	
620-A001	Mobilization	LS	1	
907-624-B002	6" Inverted Profile Thermoplastic Traffic Stripe, Continuous White	LF	1,250	
907-624-C001	6" Inverted Profile Thermoplastic Traffic Stripe, Skip Yellow	LF	1,250	
907-624-D002	6" Inverted Profile Thermoplastic Traffic Stripe, Continuous Yellow	LF	1,250	
	OR			
628-G001	6" High Performance Cold Plastic Traffic Stripe, Skip White	LF	1,250	
628-H001	6" High Performance Cold Plastic Traffic Stripe, Continuous White	LF	1,250	
628-J001	6" High Performance Cold Plastic Traffic Stripe, Continuous Yellow	LF	1,250	
626-A004	6" Thermoplastic Traffic Stripe, Skip White	MI	17	
626-C004	6" Thermoplastic Edge Stripe, Continuous White	MI	16	
626-F003	6" Thermoplastic Edge Stripe, Continuous Yellow	MI	16	
626-G002	Thermoplastic Detail Stripe, White	LF	39,000	
626-G003	Thermoplastic Detail Stripe, Yellow	LF	26,000	
626-H004	Thermoplastic Legend, White	SF	200	

By	<b>MISSISSIPPI DEPARTMENT OF TRANSPORTATION</b>	
	<b>SUMMARY OF QUANTITIES</b>	
Revision		Working Number
		SQ-1
Date	Design Team	Checked
	FILENAME: HWY 82 SQS Page 6-7	Date
		Sheet Number
		7

STATE	PROJECT NO.
MISS	NH-0011-03(086)

**SUMMARY OF QUANTITIES (SHEET 2)**

PAY ITEM NO.	PAY ITEM	UNIT	LOWNDES : 107779-301000	
			Prelim	Final
626-H005	Thermoplastic Legend, White	LF	2,300	
627-K001	Red-Clear Reflective High Performance Raised Markers	EA	2,500	
627-L001	Two-Way Yellow Reflective High Performance Raised Markers	EA	125	
630-A001	Standard Roadside Signs, Sheet Aluminum, 0.080" Thickness	SF	15	
630-A003	Standard Roadside Signs, Sheet Aluminum, 0.125" Thickness	SF	51	
630-F006	Delineators, Guard Rail, White	EA	20	
630-F007	Delineators, Guard Rail, Yellow	EA	20	
630-K001	Welded & Seamless Steel Pipe Posts, 3 1/2"	LF	46	

By	<b>MISSISSIPPI DEPARTMENT OF TRANSPORTATION</b>	
	<b>SUMMARY OF QUANTITIES</b>	
Revision		
	PROJ NO: NH-0011-03(086) COUNTY: LOWNDES	Working Number <b>SQ-2</b>
Date	FILENAME: HWY 82 SQS Page 6-7 Design Team	Sheet Number <b>8</b>
	Checked	Date



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STANDARD ROADSIDE SIGNS					SHEET ALUMINUM 0.080" THICKNESS
SIGN NUMBER	SIZE	UNIT AREA (SQ. FT.)	QUANTITY REQUIRED	TOTAL SIGN AREA (SQ. FT.)	REMARKS
R1-4	36"x15"	3.75	3	11.25	
R6-1L	36"x12"	3.00	1	3	
TOTAL (0.080" THICKNESS )				14.25	


STANDARD ROADSIDE SIGNS					SHEET ALUMINUM 0.125" THICKNESS
SIGN NUMBER	SIZE	UNIT AREA (SQ. FT.)	QUANTITY REQUIRED	TOTAL SIGN AREA (SQ. FT.)	REMARKS
R1-1	48" OCT	13.25	3	39.75	
R5-1	36"x36"	9	1	9	
TOTAL (0.125" THICKNESS )				48.75	

STANDARD ROADSIDE SIGN ASSEMBLIES REQUIRED				
LOCATION	CODE NUMBER THIS SHEET	STANDARD ROADSIDE SIGNS TO BE MOUNTED	PIPE POST	
HWY 45 RAMP	1	R1-1, R1-4, R5-1, R6-1R	48.75	
<b>TOTAL THIS BLOCK</b>			LIN. FT.	48.75

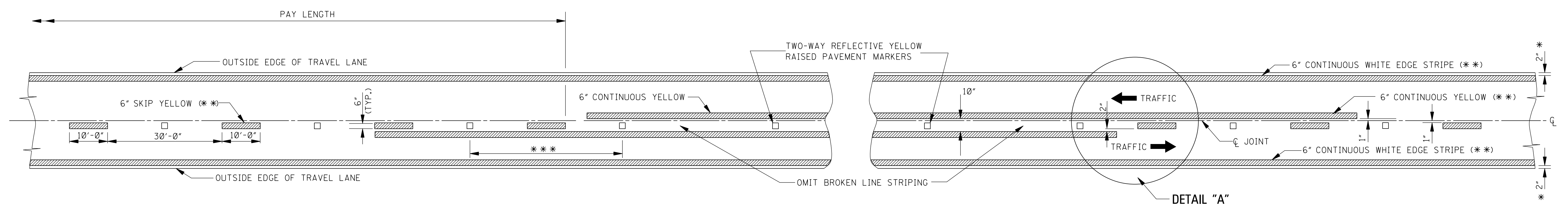
NOTE: VERIFY ALL POST LENGTHS IN THE FIELD PRIOR TO ORDERING MATERIAL.

ROADWAY DESIGN DIVISION  
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

001.00 AMPM DGNFILENAME

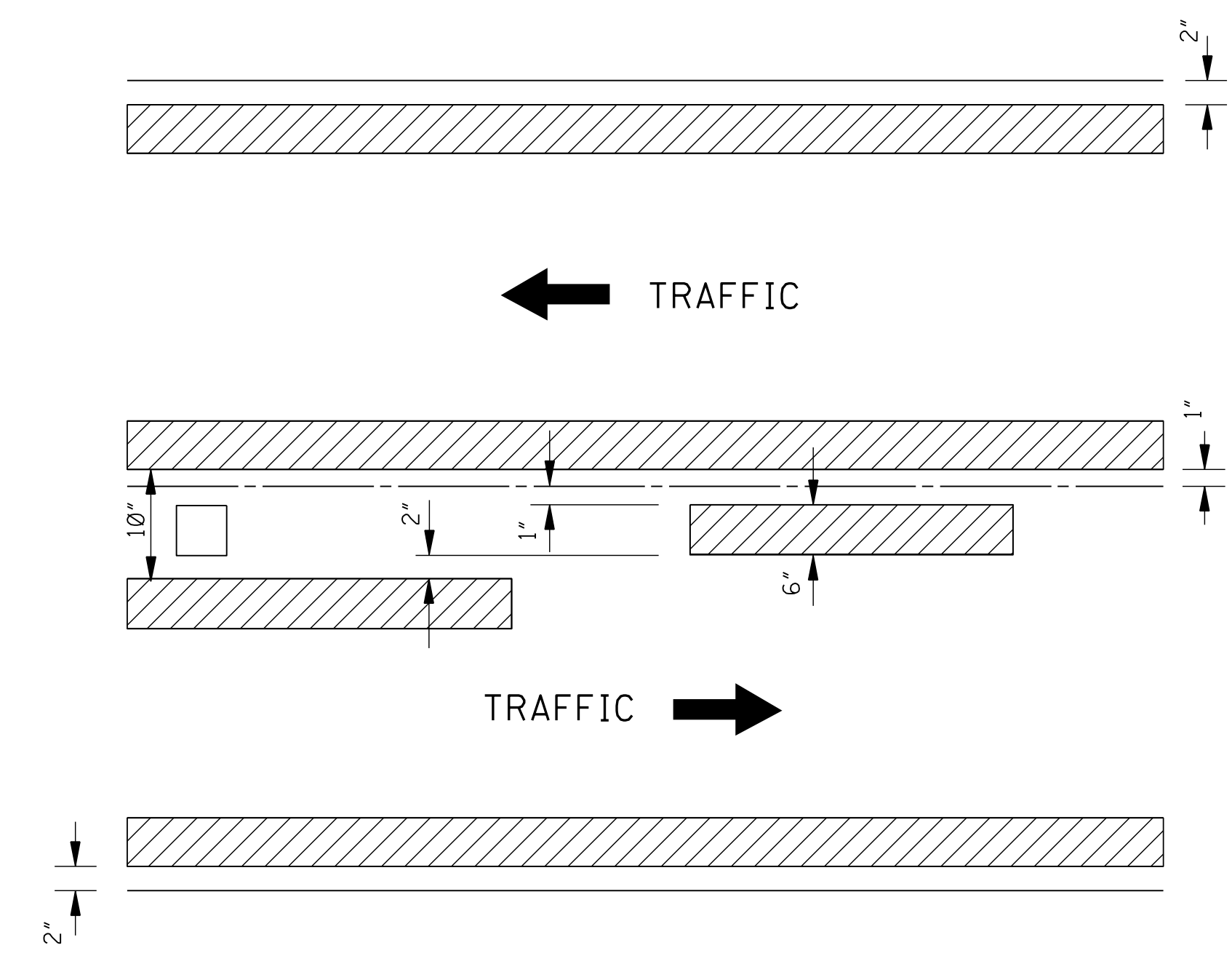
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
<b>STANDARD ROADSIDE SIGN QUANTITIES</b>	
	
<b>PROJ. NO.: NH-0011-03(086)</b> <b>COUNTY: LOWNDES</b>	
WORKING NUMBER	SRS-1
SHEET NUMBER	<b>9</b>
DATE	FILENAME: _____
DESIGN TEAM	CHECKED _____ DATE _____





**TWO-WAY TRAFFIC**  
(ASPHALT OR CONCRETE PAVEMENT)

NOTE: THE CRITERIA FOR NO-PASSING ZONES CAN BE FOUND IN THE MDT ROADWAY DESIGN MANUAL, SECTION 11-1.01.



**DETAIL "A"**

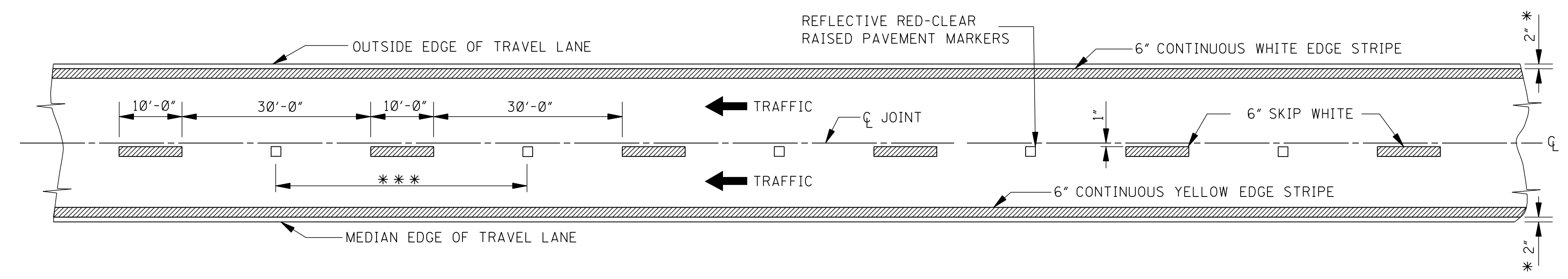
GENERAL NOTES:

- \* 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS. FOR STRIPING ON RUMBLE STRIP SECTIONS REFER TO WK. SHEETS RS-1, RS-2, AND RS-3.
- \*\* 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
- \*\*\* 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

	URBAN AREA (ft-in)	RURAL AREA (ft-in)
TANGENT SECTIONS	40'-0"	80'-0"
HORIZONTAL CURVES	40'-0"	40'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

† NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDT "APPROVED SOURCES OF MATERIALS."

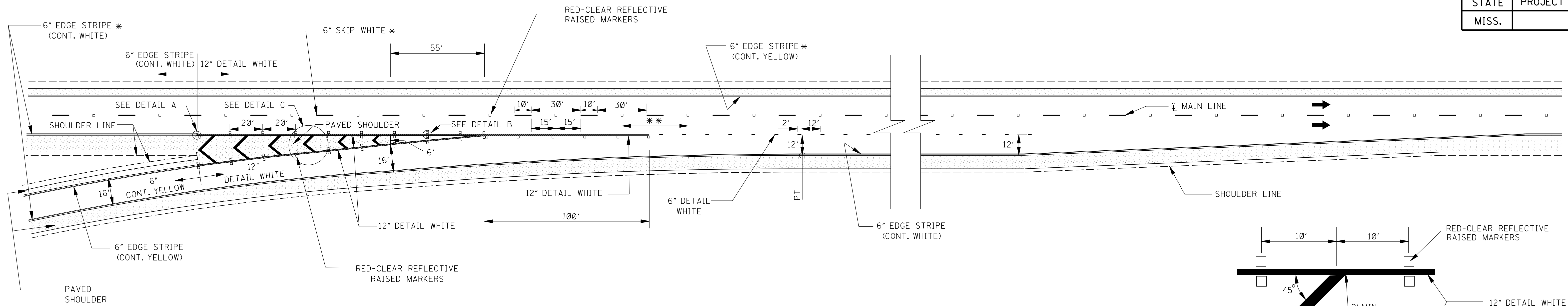


**4-LANE WITH ONE-WAY TRAFFIC**

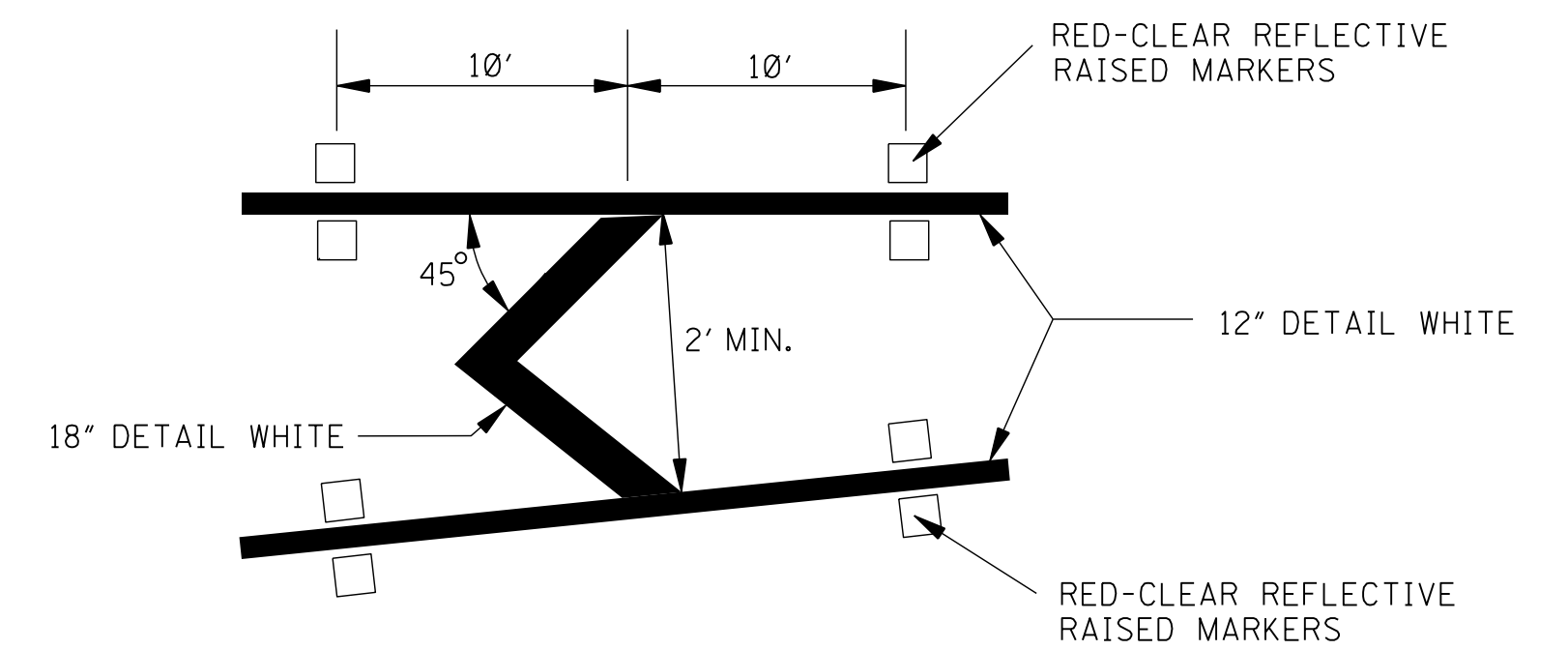
BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
REVISION	<b>PAVEMENT MARKING DETAILS FOR 2-LANE AND 4-LANE DIVIDED ROADWAYS</b>
DATE	ISSUE DATE: AUGUST 01, 2017



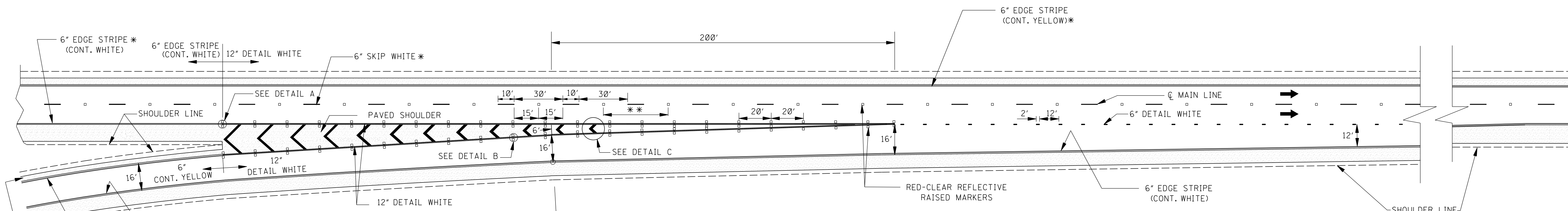
WORKING NUMBER  
PM-1  
SHEET NUMBER  
6051



PARALLEL ENTRANCE RAMP

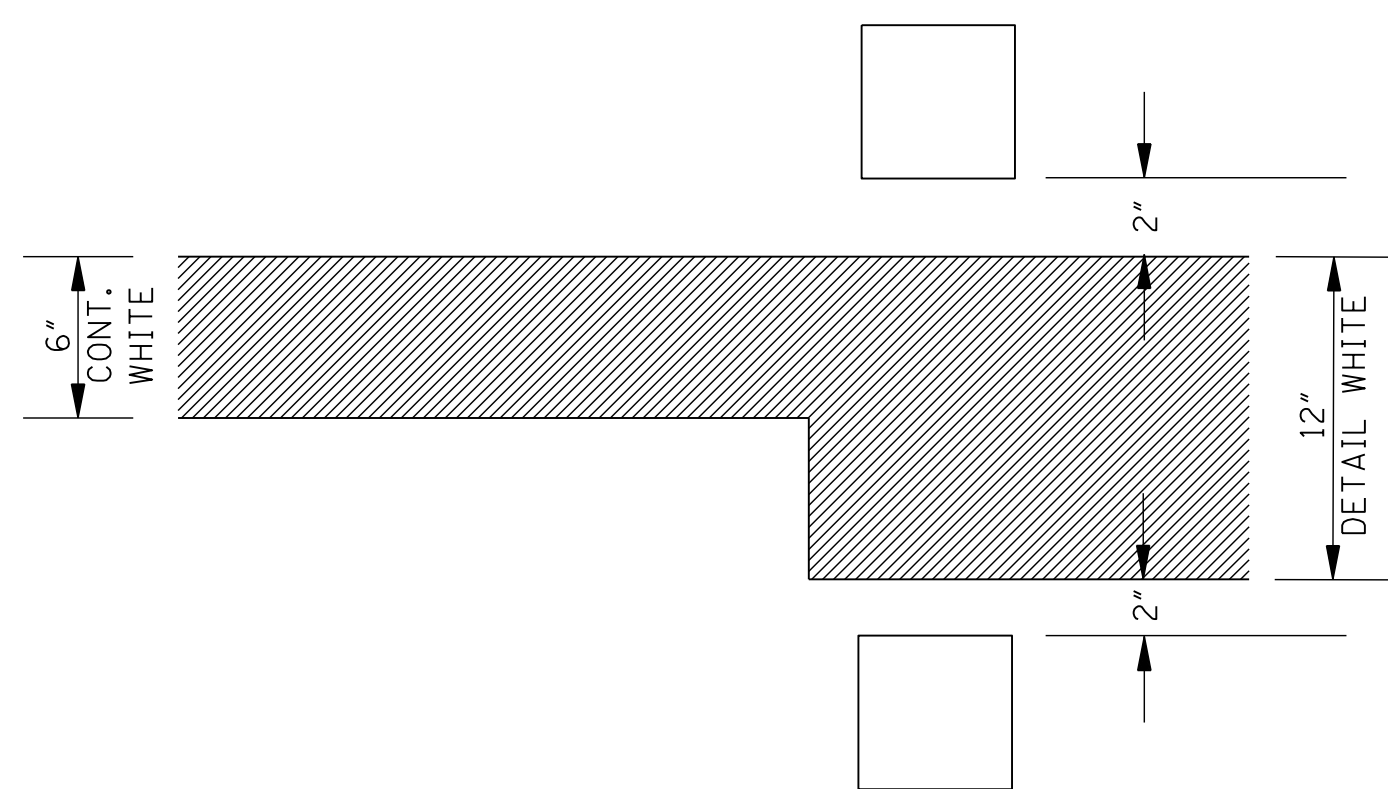


DETAIL C

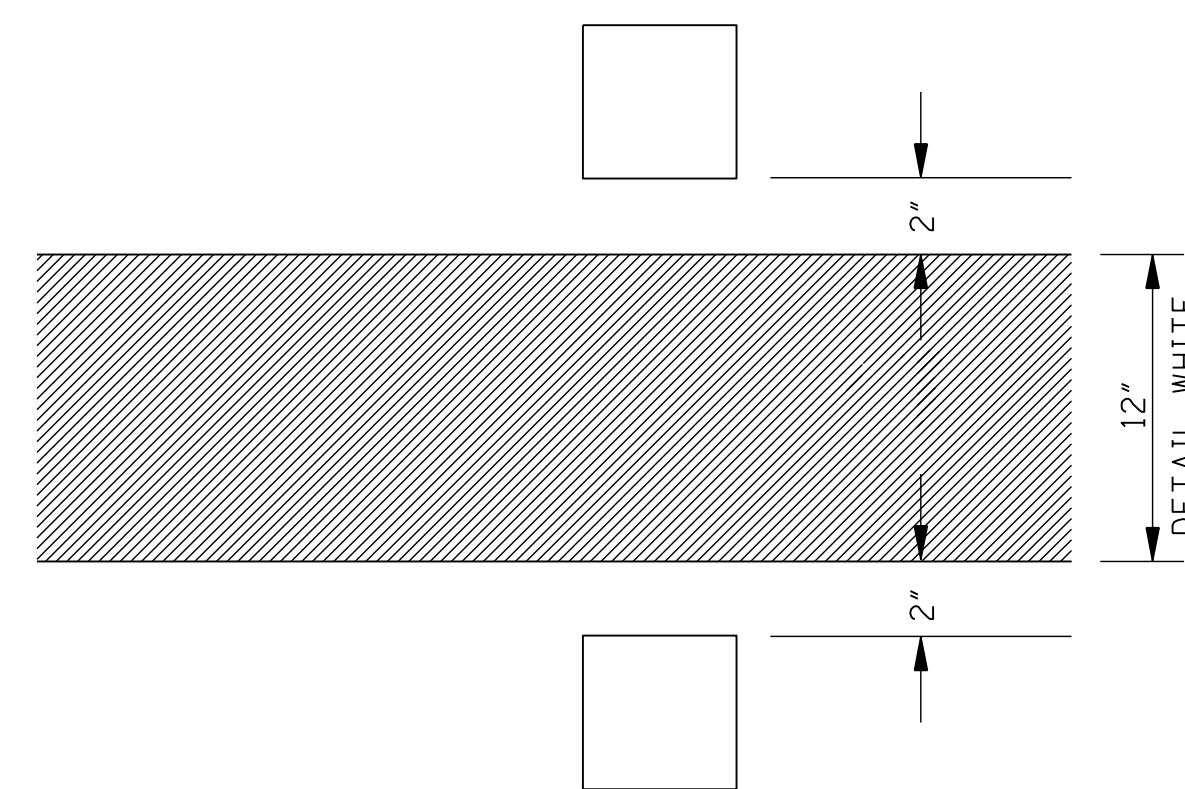


TAPER ENTRANCE RAMP

NOTE: 2'-12' SKIP WHITE EXTENDS TO THE TERMINATION POINT OF THE ACCELERATION LANE.



DETAIL A

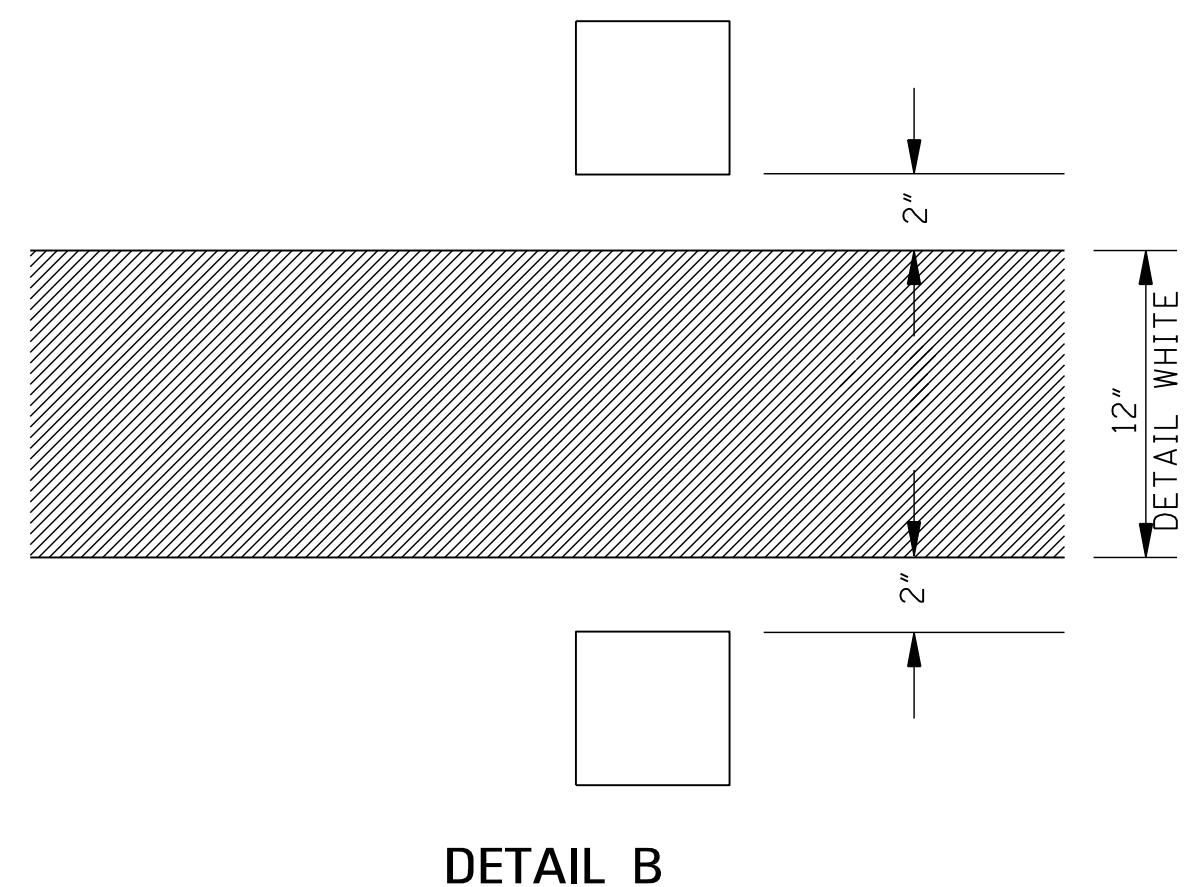
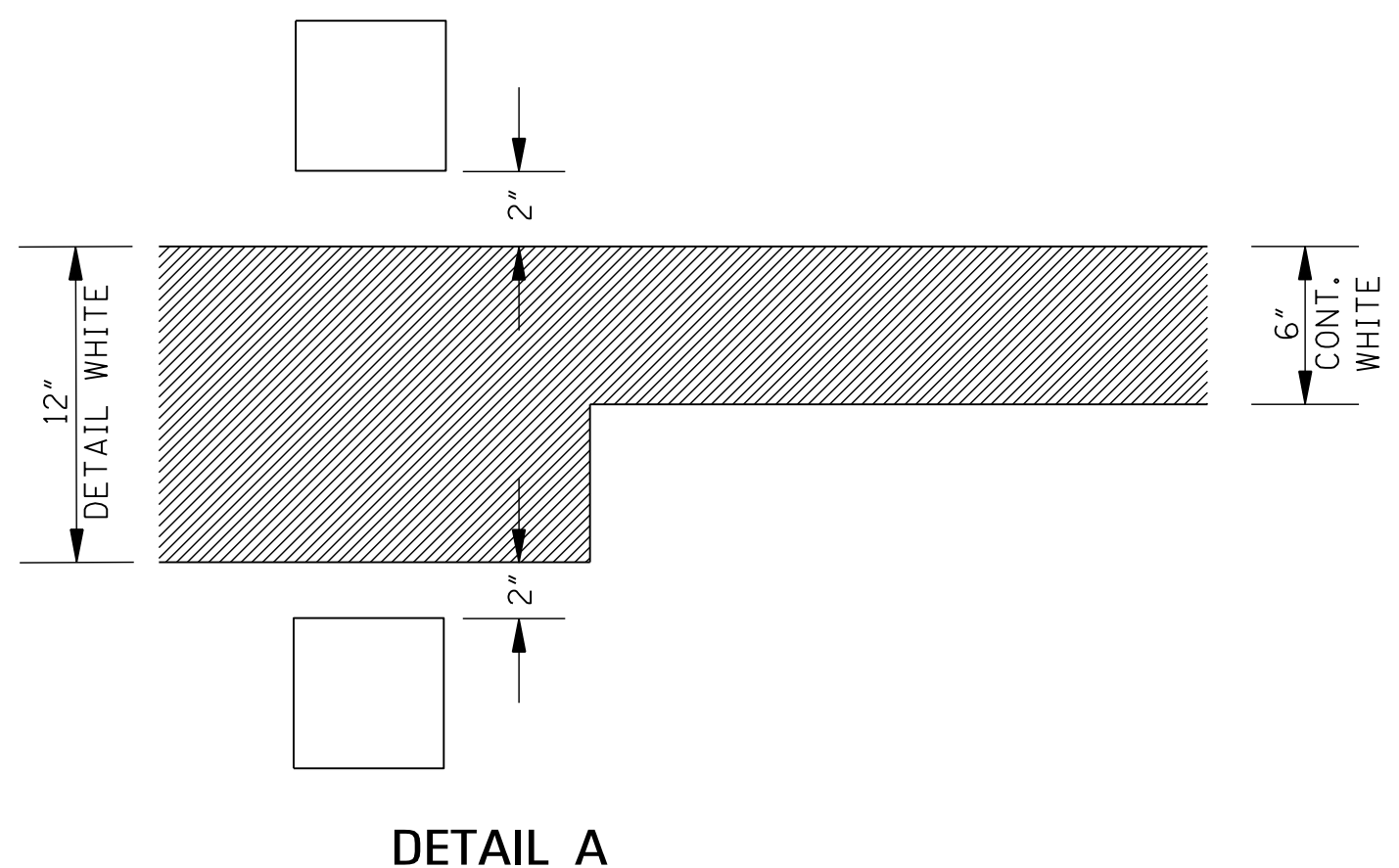
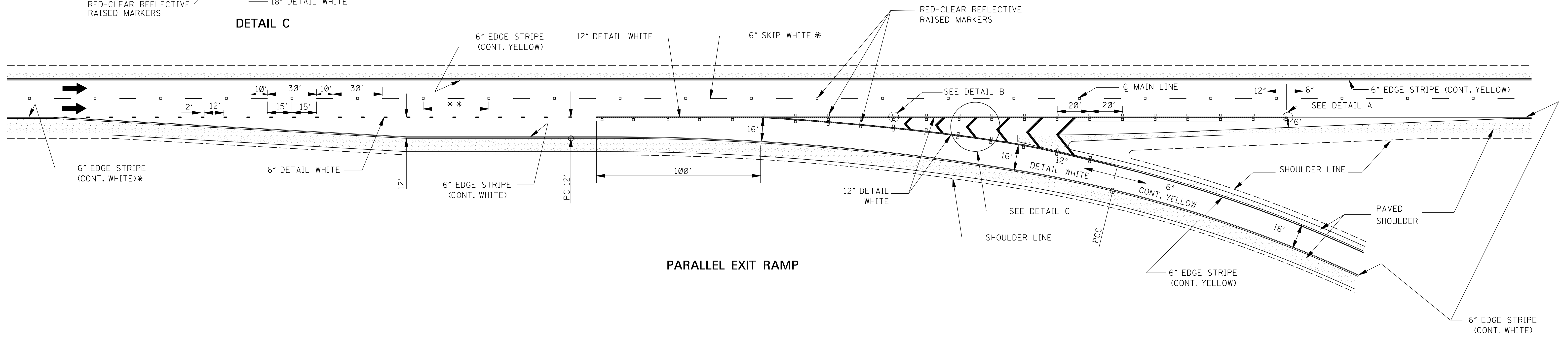
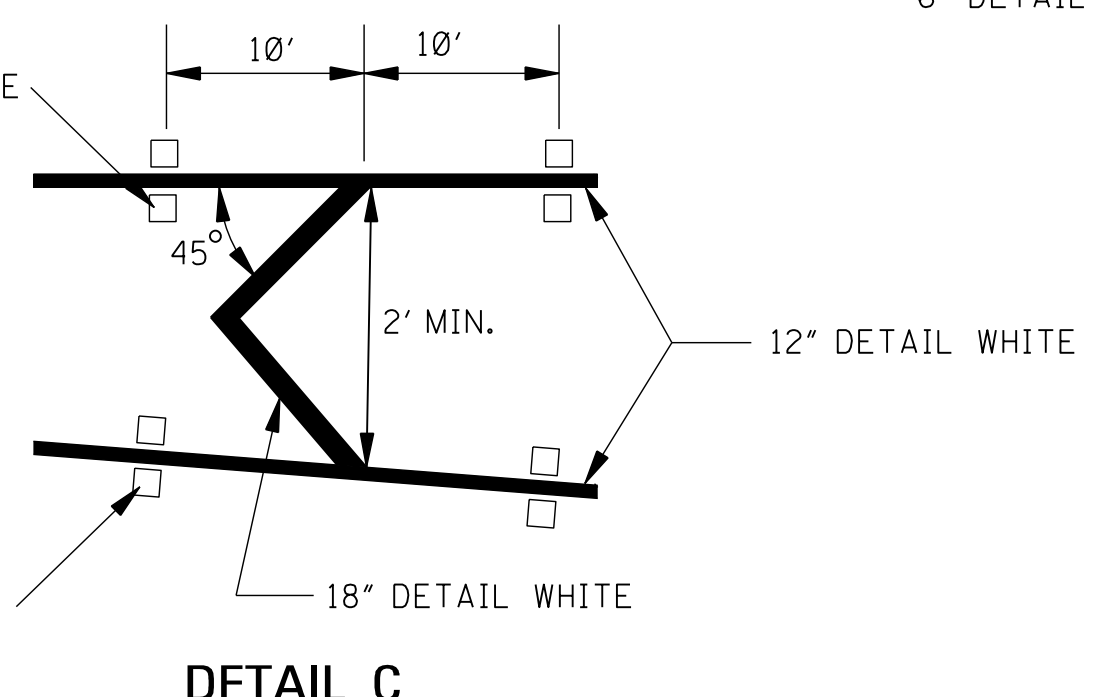
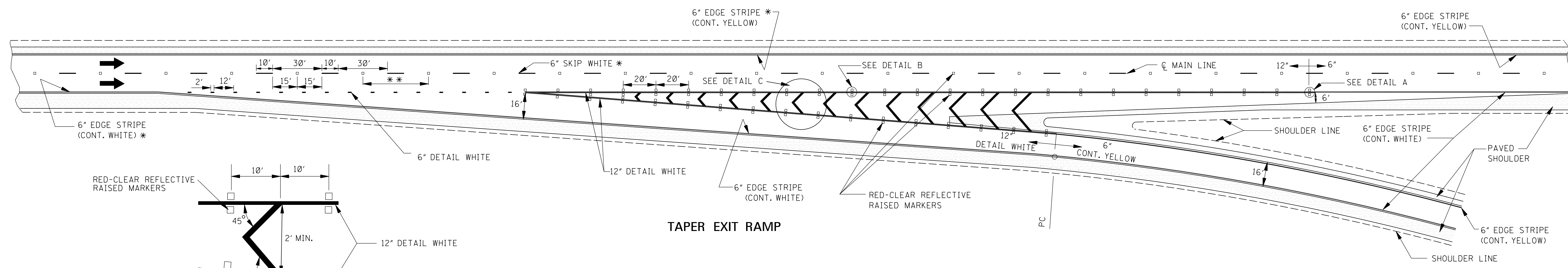


DETAIL B

GENERAL NOTES:

- \* 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELED WAY.
- \*\* 2. ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- 3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED SOURCES OF MATERIALS."

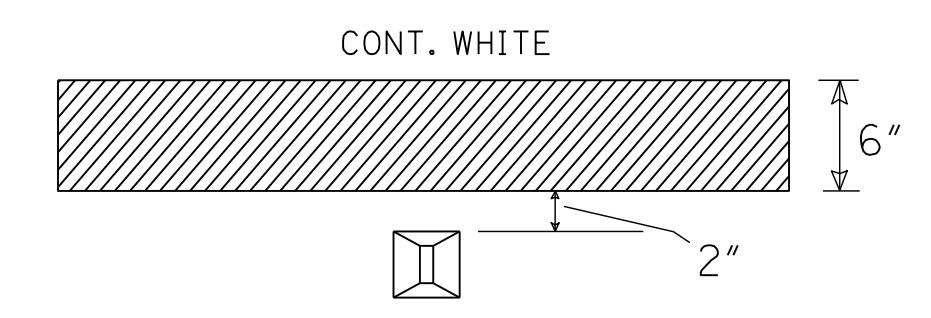
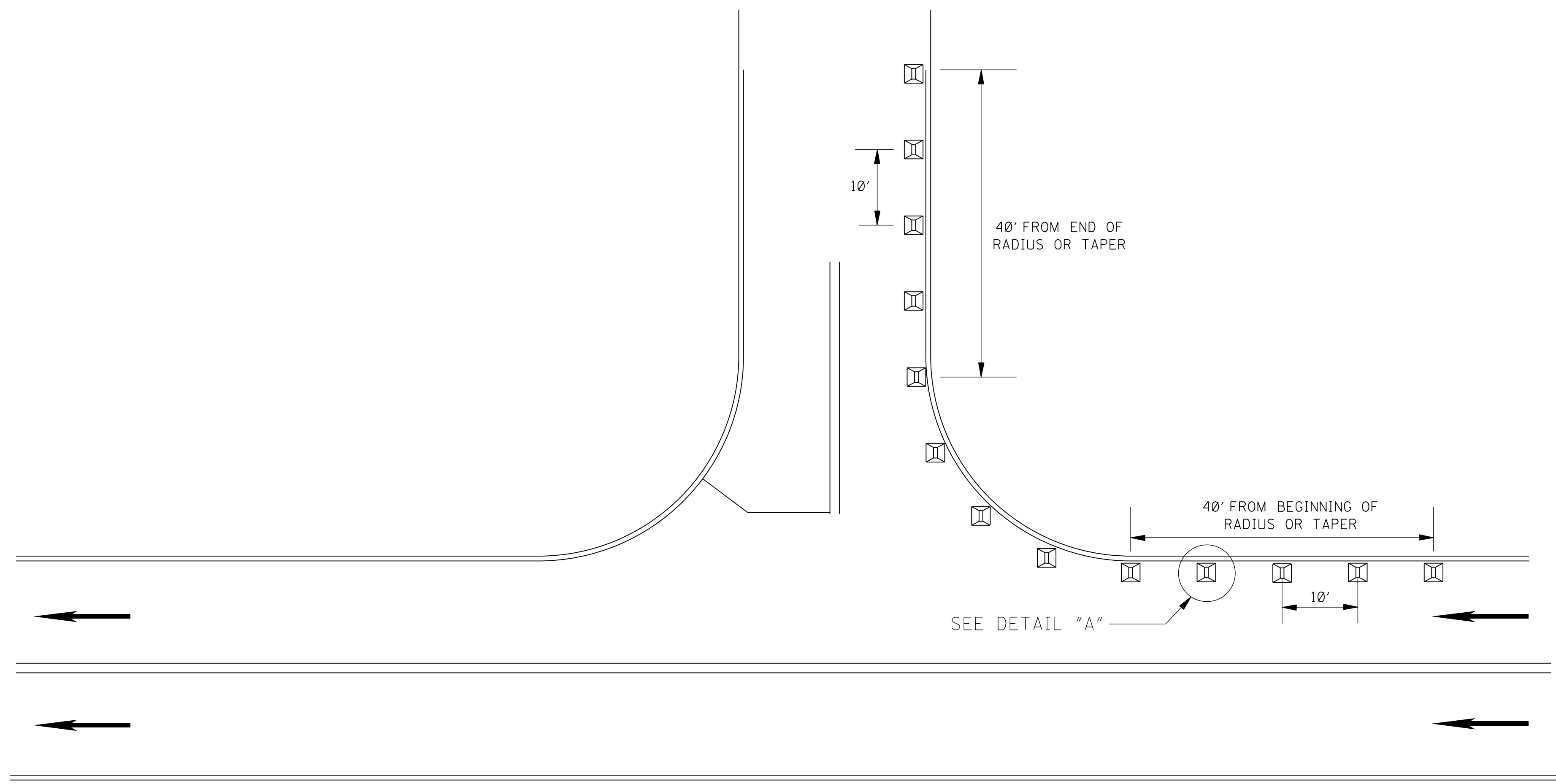
BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		<p align="center"><b>PAVEMENT MARKING DETAILS FOR INTERCHANGE ENTRANCE RAMPS (PARALLEL AND TAPER)</b></p>	
DATE			
ISSUE DATE:		AUGUST 01, 2017	



- GENERAL NOTES:**
- \* 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELED WAY.
  - \*\* 2. ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
  - 3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED SOURCES OF MATERIALS."

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN		
REVISION	<p align="center"><b>PAVEMENT MARKING DETAILS FOR EXIT RAMP (PARALLEL AND TAPER)</b></p> 		
DATE			
ISSUE DATE:	AUGUST 01, 2017		
WORKING NUMBER	PM-4		
SHEET NUMBER	6054		

TYPICAL PLACEMENT OF RAISED PAVEMENT MARKERS ON SIDE ROAD RADIUS  
4-LANE, TWO WAY TRAFFIC



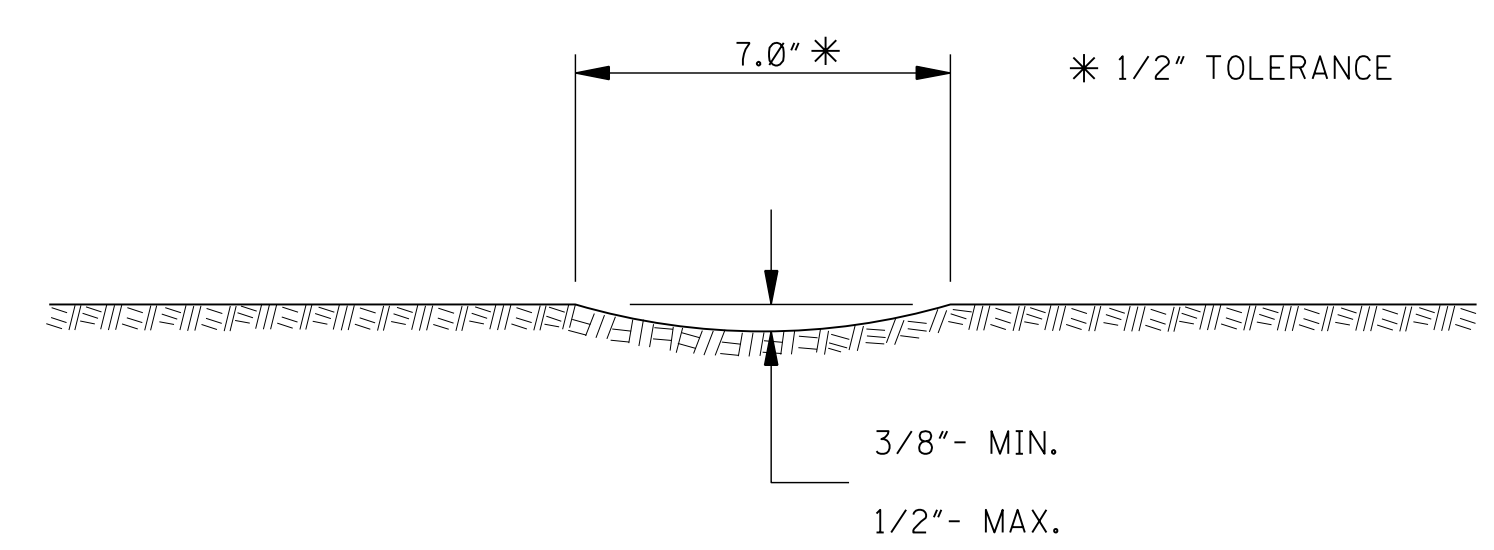
DETAIL A

➔ DIRECTION OF TRAFFIC

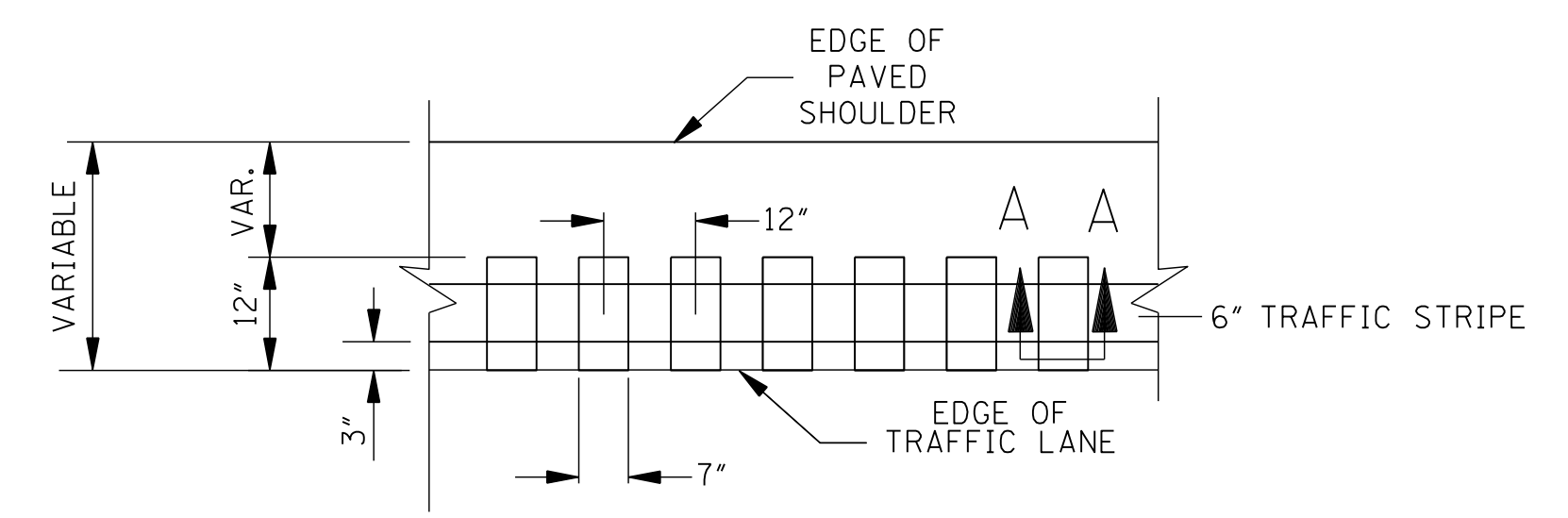
GENERAL NOTES:

1. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
2. MARKERS SHALL BE HIGH PERFORMANCE TWO-WAY CLEAR.
3. MARKERS SHALL NOT BE ROTATED WHEN BEING PLACED ALONG RADIUS AND TANGENT SECTIONS OF LOCAL ROAD.
4. MARKERS SHALL BE INSTALLED AT SIMPLE AND CHANNELIZED INTERSECTIONS TO THE LIMITS SHOWN ABOVE.

		BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
		REVISION		<b>2-WAY RAISED PAVEMENT MARKERS AT INTERSECTING ROADS (4-LANE)</b>	
		DATE			
		ISSUE DATE: AUGUST 01, 2017		 WORKING NUMBER PM-12 SHEET NUMBER 6062	

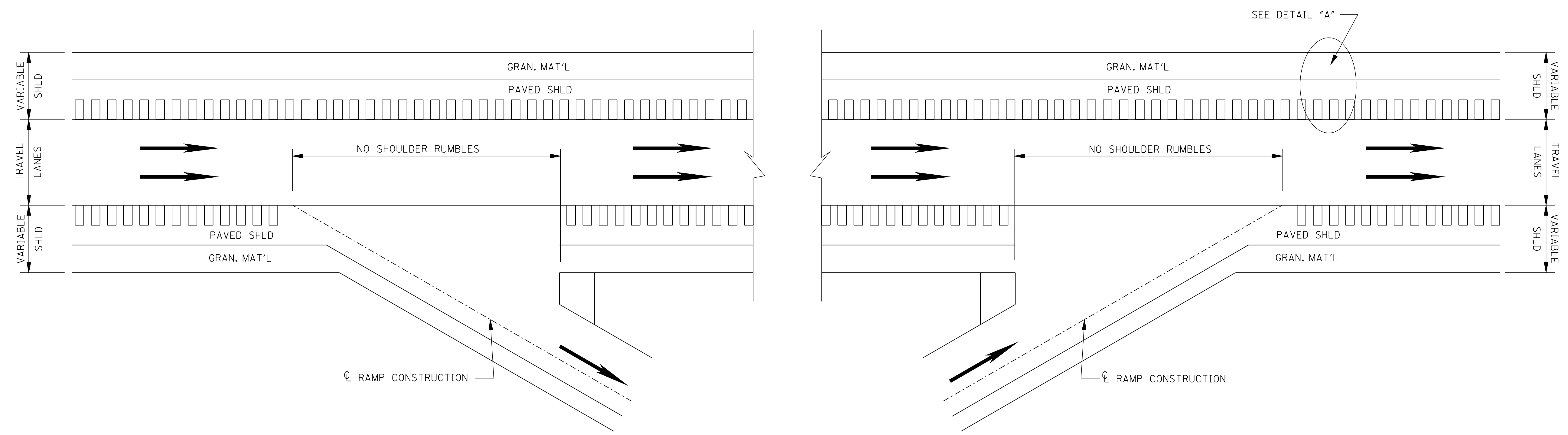


SECTION "A-A"



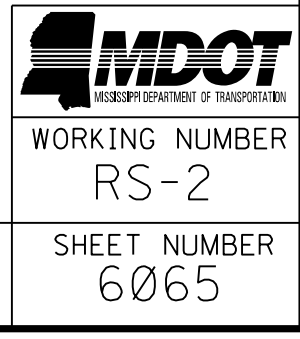
DETAIL "A"

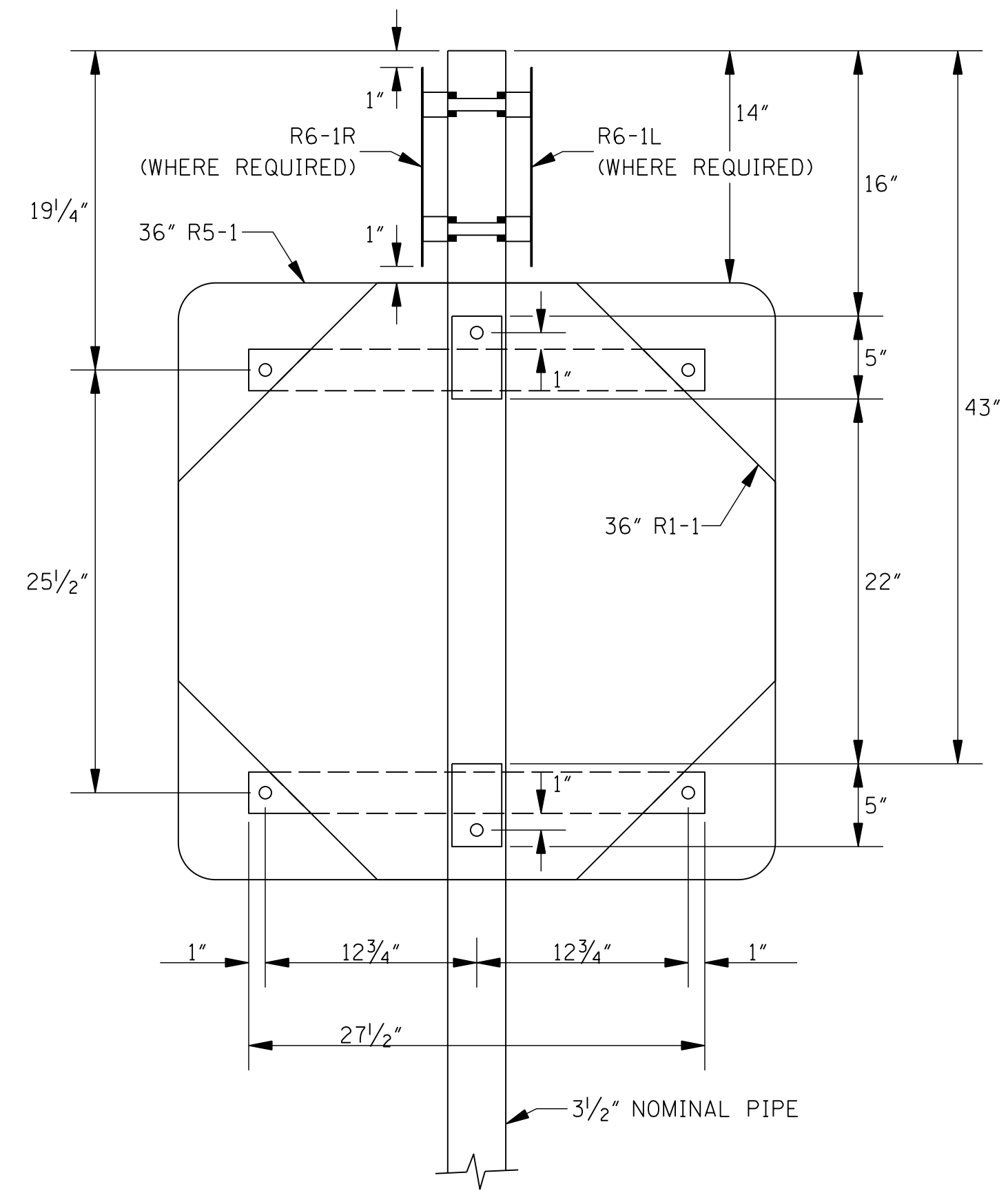
- GENERAL NOTES
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
  - GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PUBLIC INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
  - COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
  - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
    - MAINLINE
    - INTERSECTING ROADWAY IF OVERLAID OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.W.
    - ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.



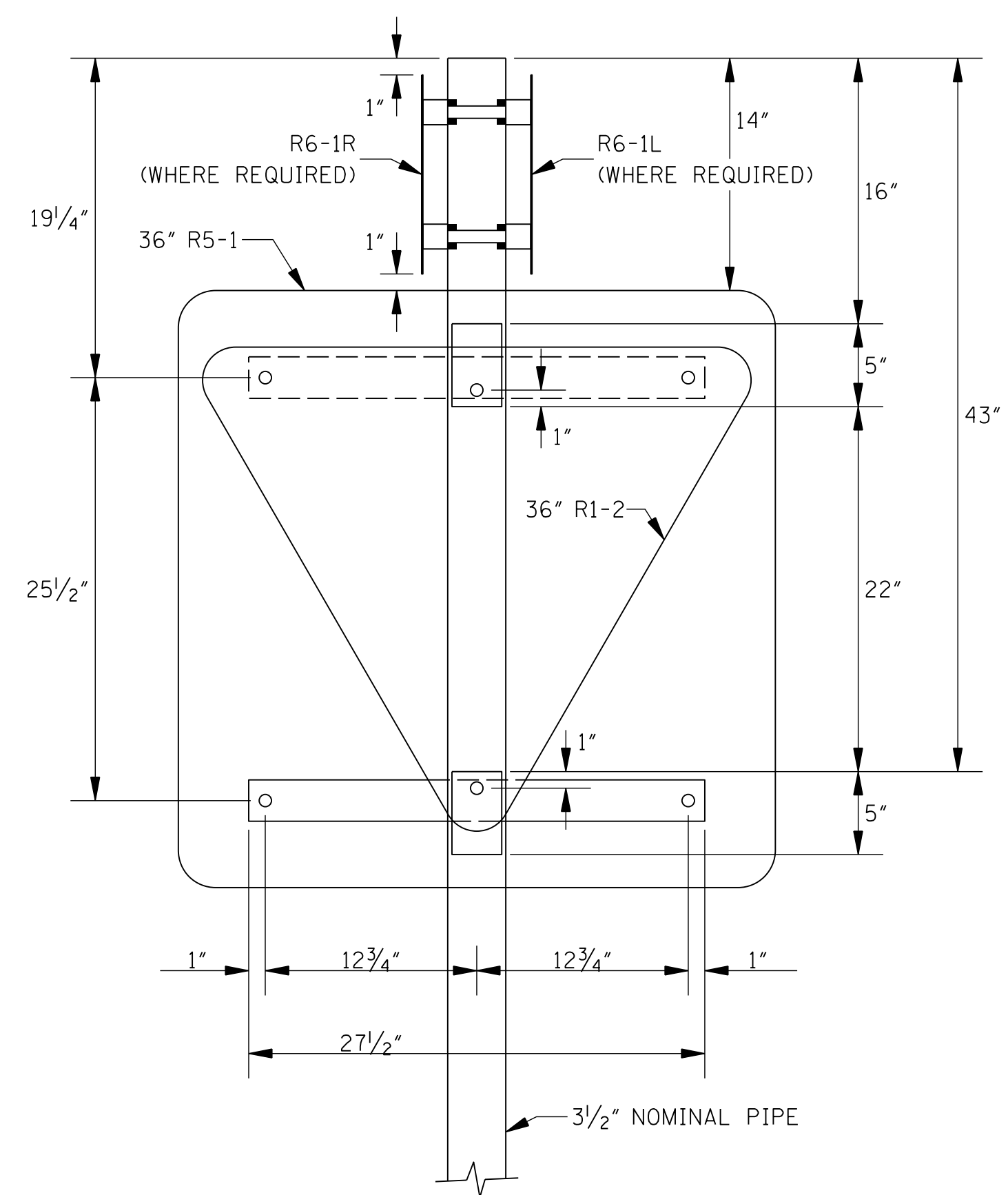
PLAN  
NOT TO SCALE  
DETAILS OF RUMBLE STRIPS

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		<b>RUMBLE STRIPES 4-LANE HIGHWAYS (ASPHALT LANES, 2-FT OR WIDER ASPHALT SHOULDERS)</b>	
DATE			
ISSUE DATE:		AUGUST 01, 2017	
WORKING NUMBER		RS-2	
SHEET NUMBER		6065	

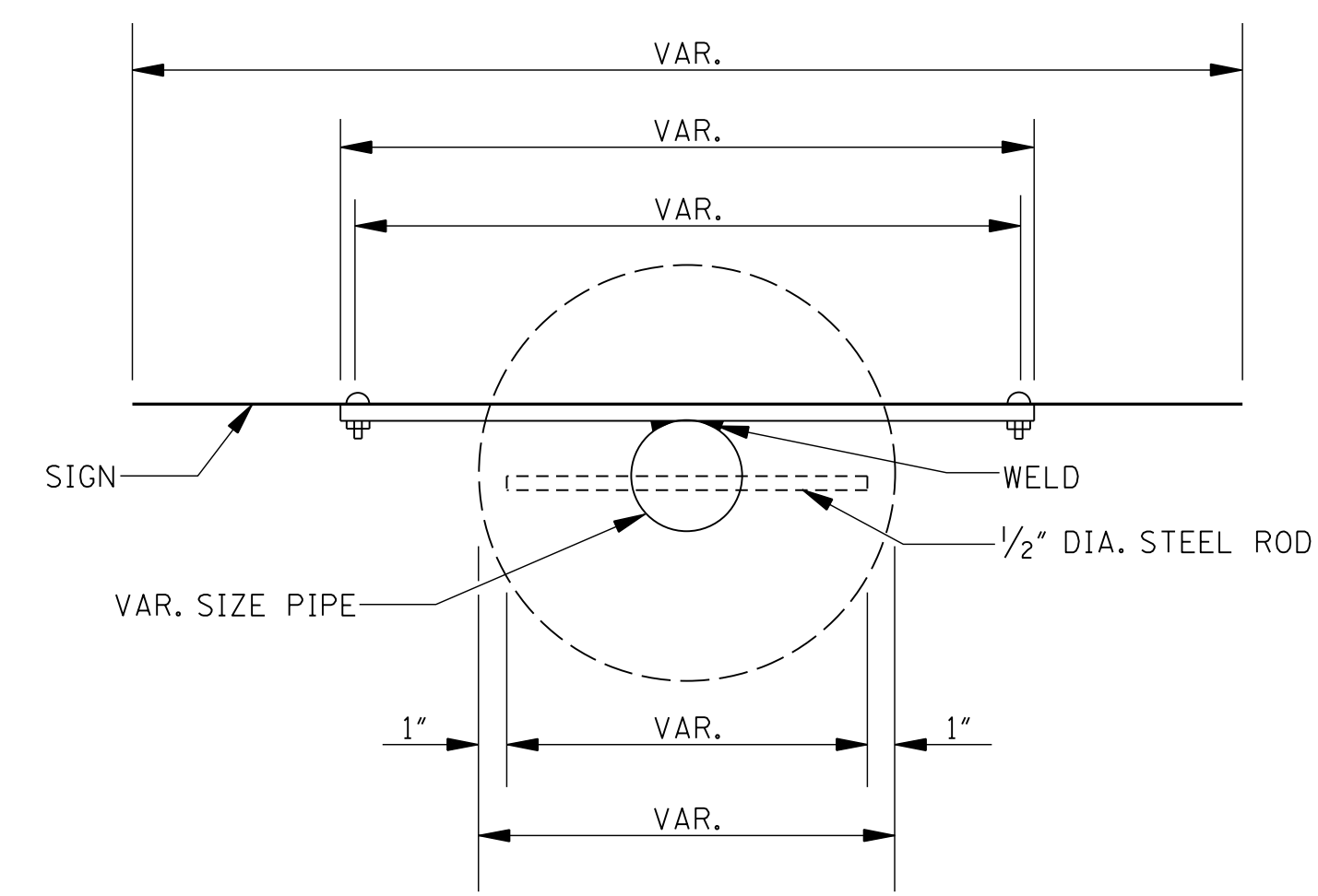




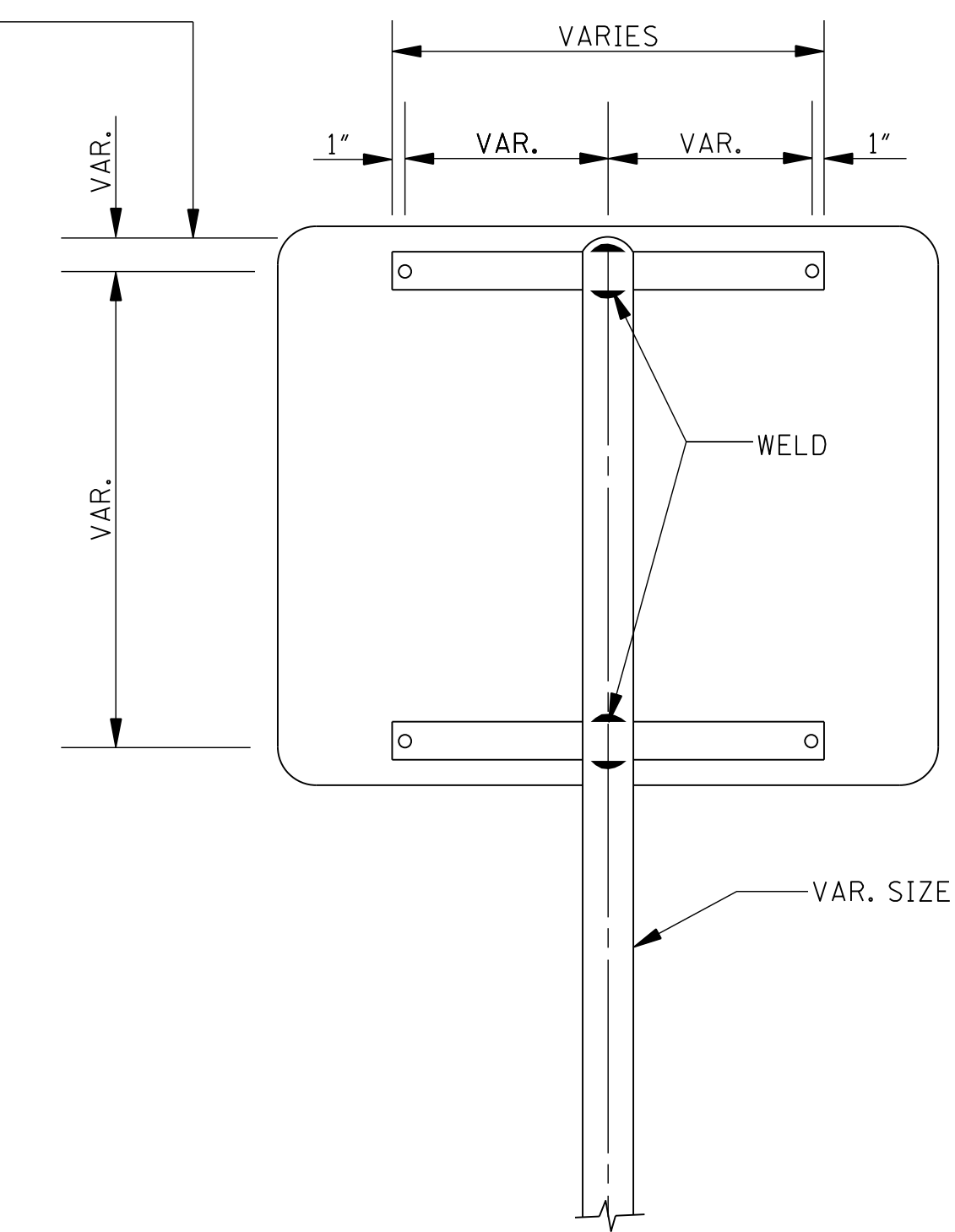
TYPICAL BACK-TO-BACK SIGN MOUNT  
SHOWING R5-1 WITH R1-1



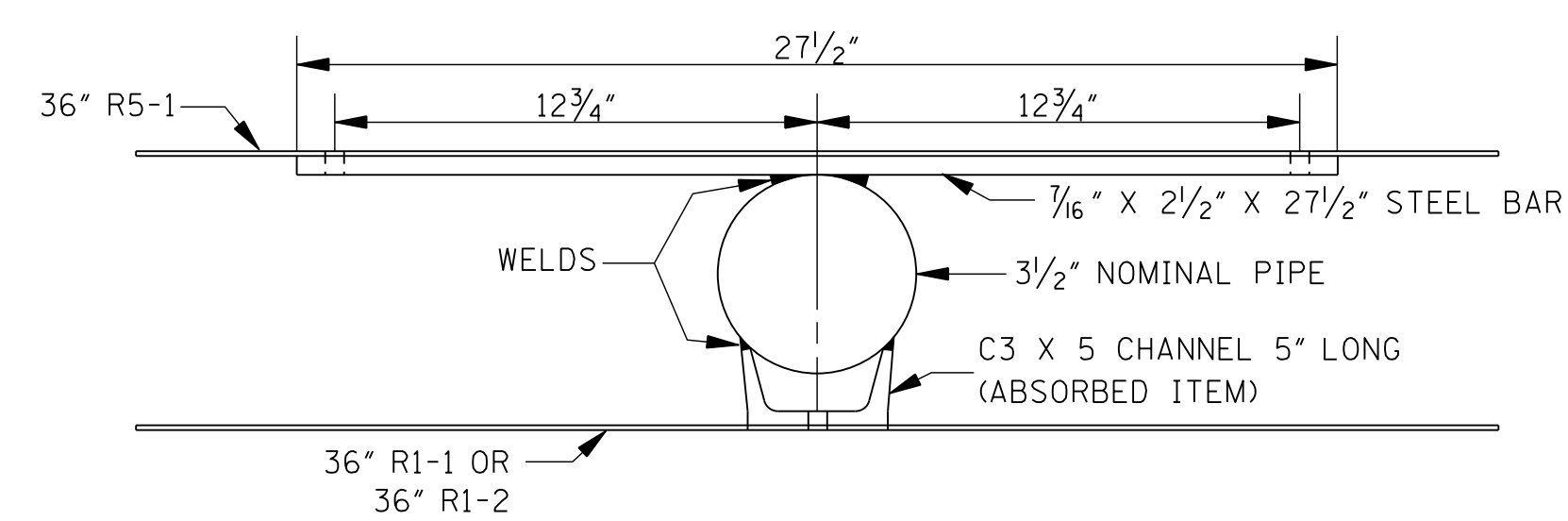
TYPICAL BACK-TO-BACK SIGN MOUNT  
SHOWING R5-1 WITH R1-2



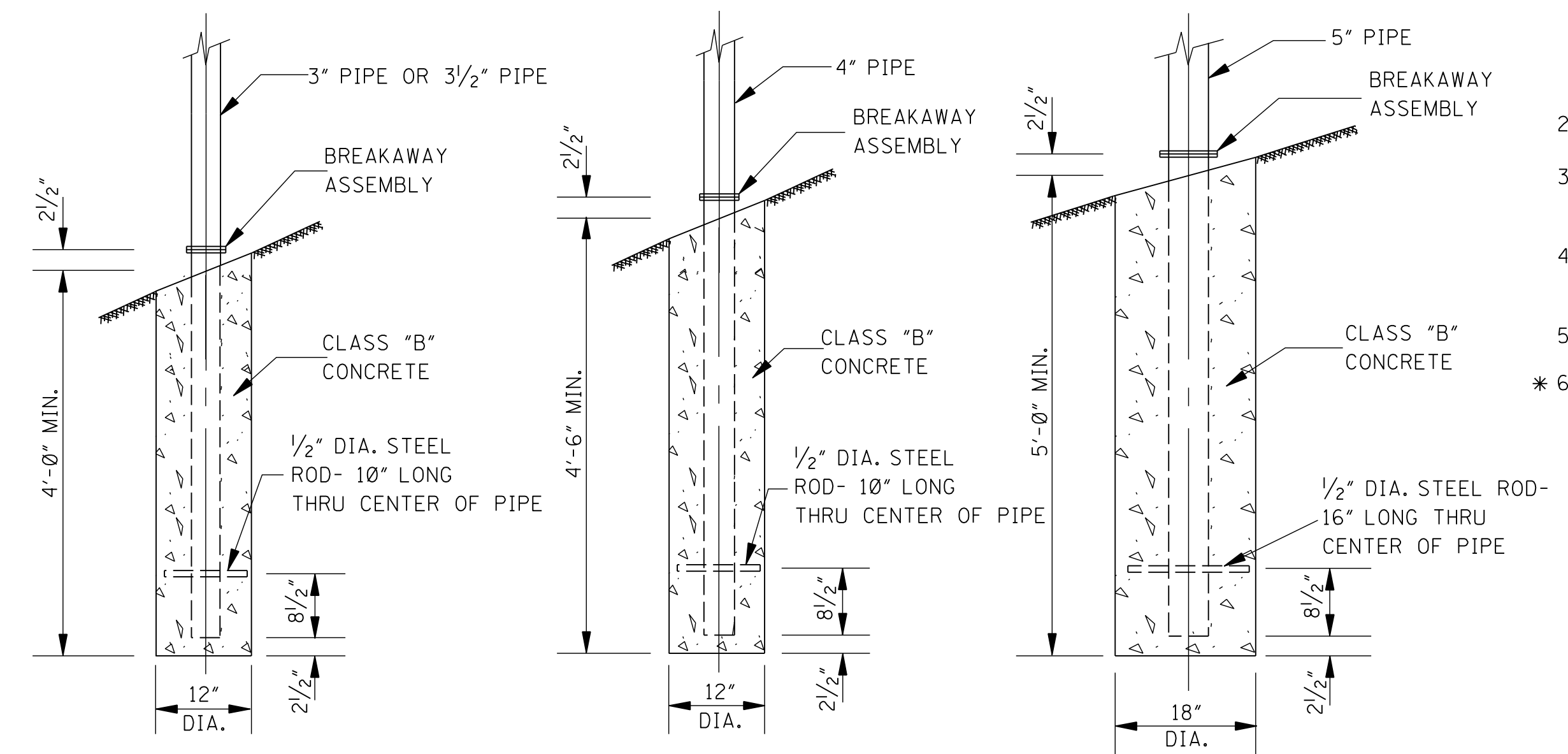
TYPICAL PLAN VIEW



TYPICAL ASSEMBLY ON PIPE



PLAN VIEW OF DOUBLE MOUNTING OF SIGNS



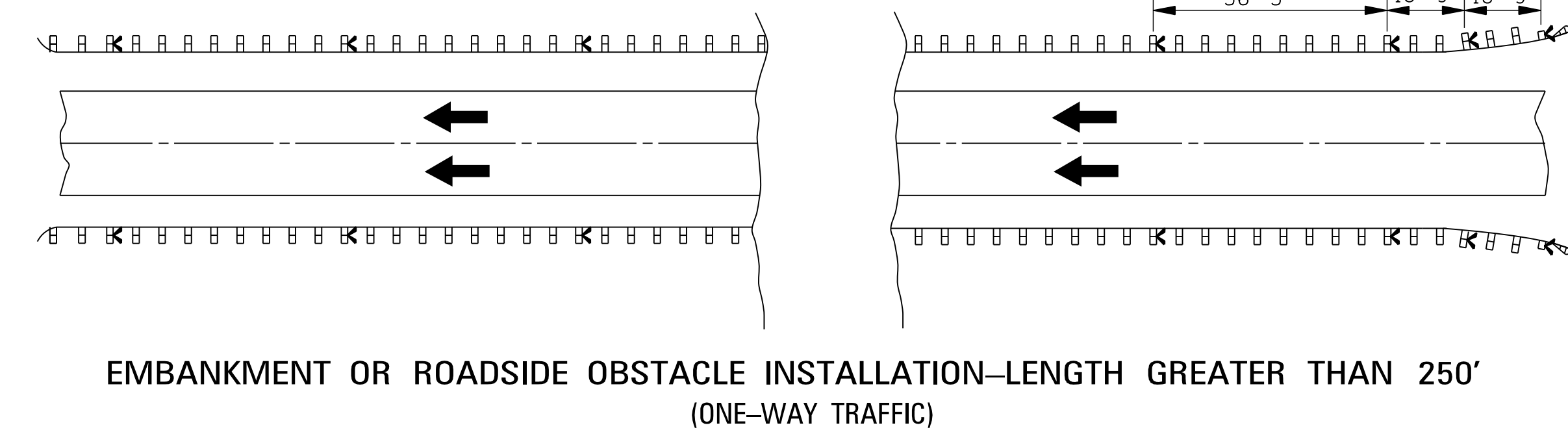
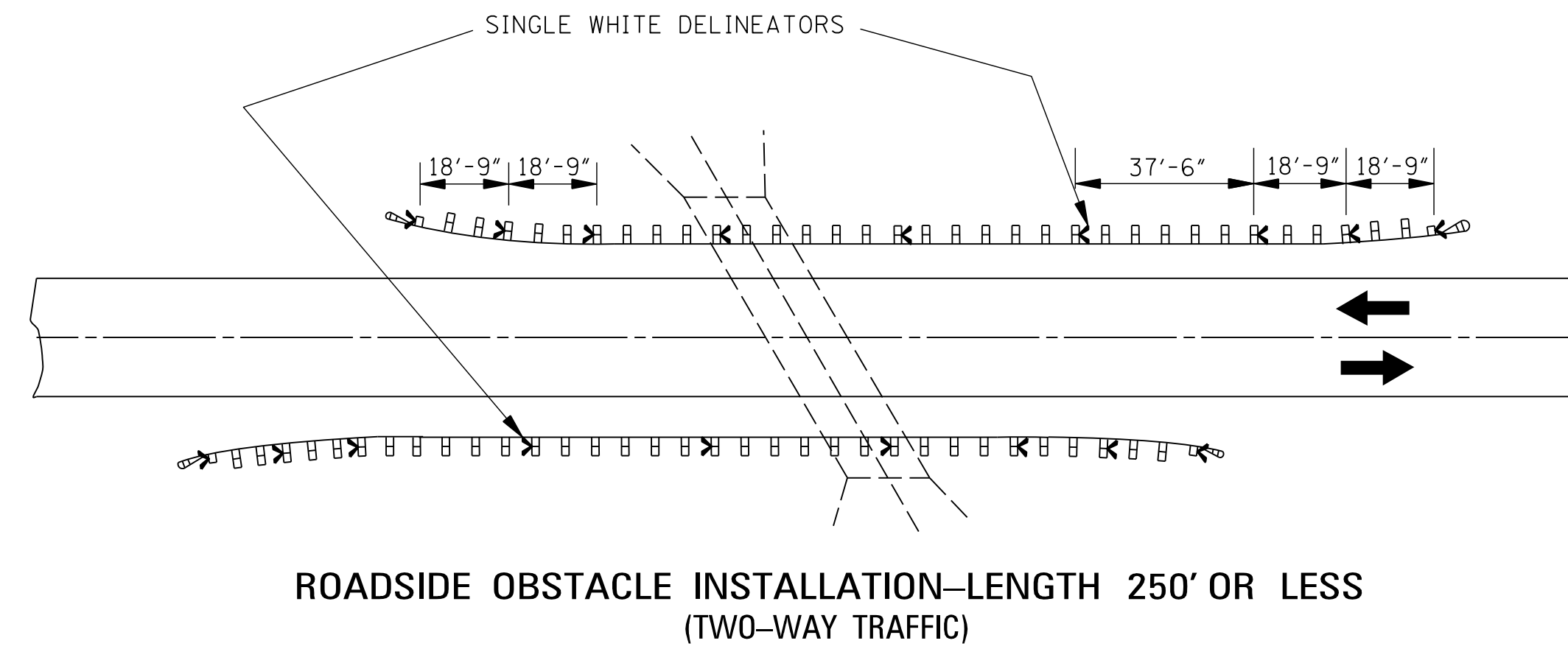
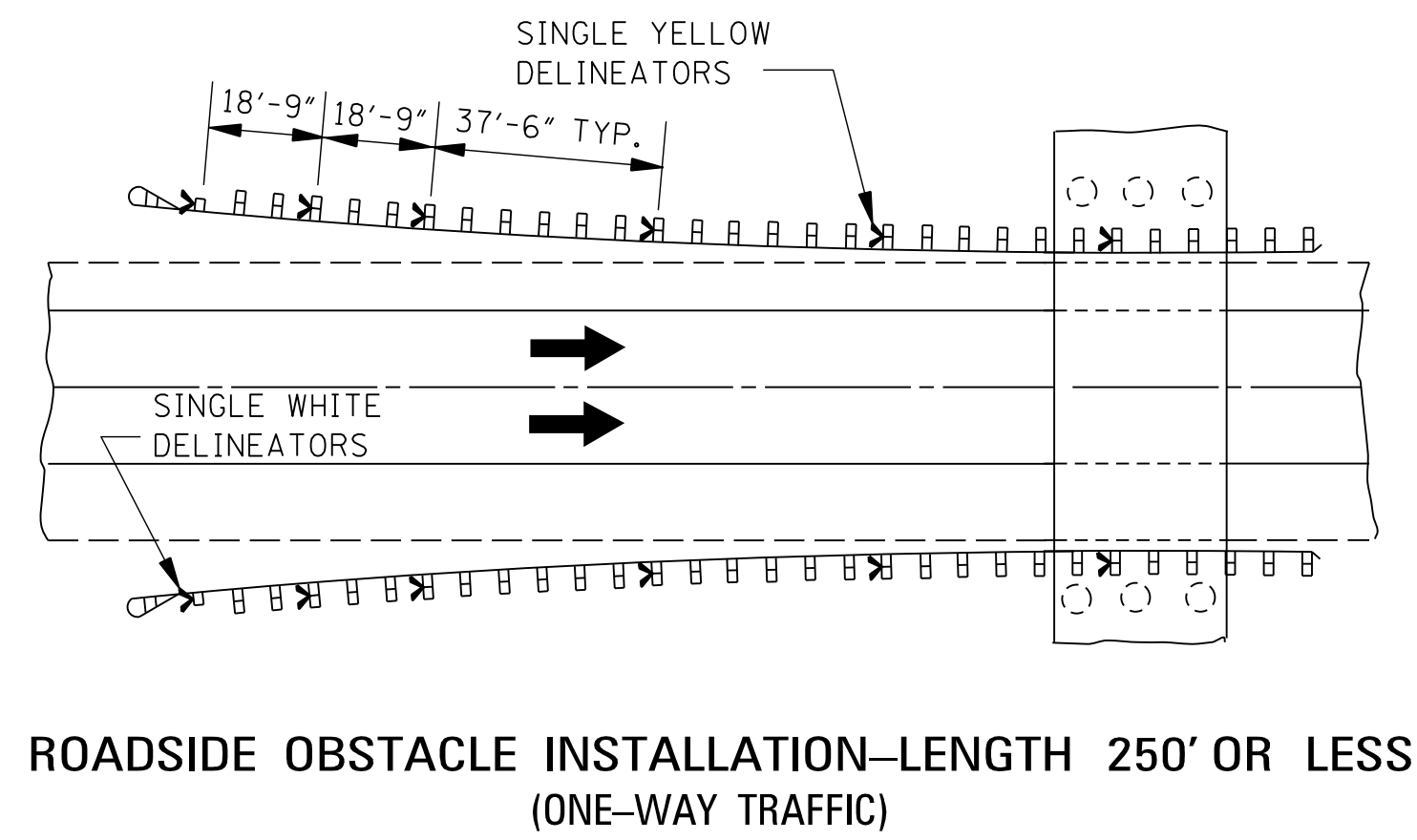
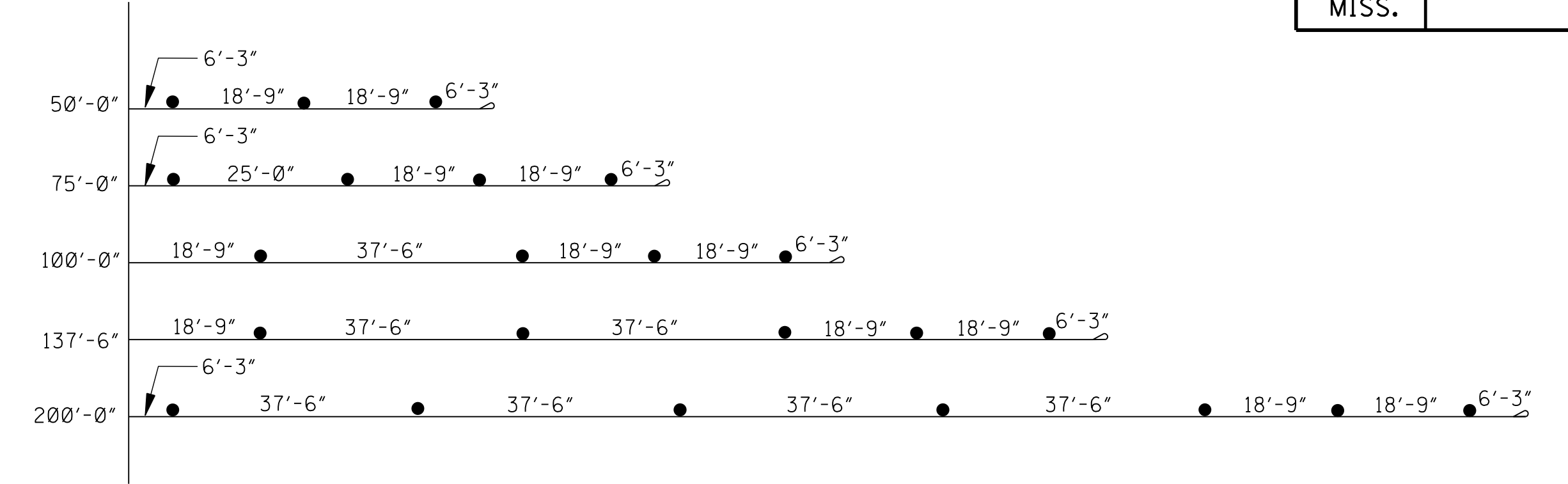
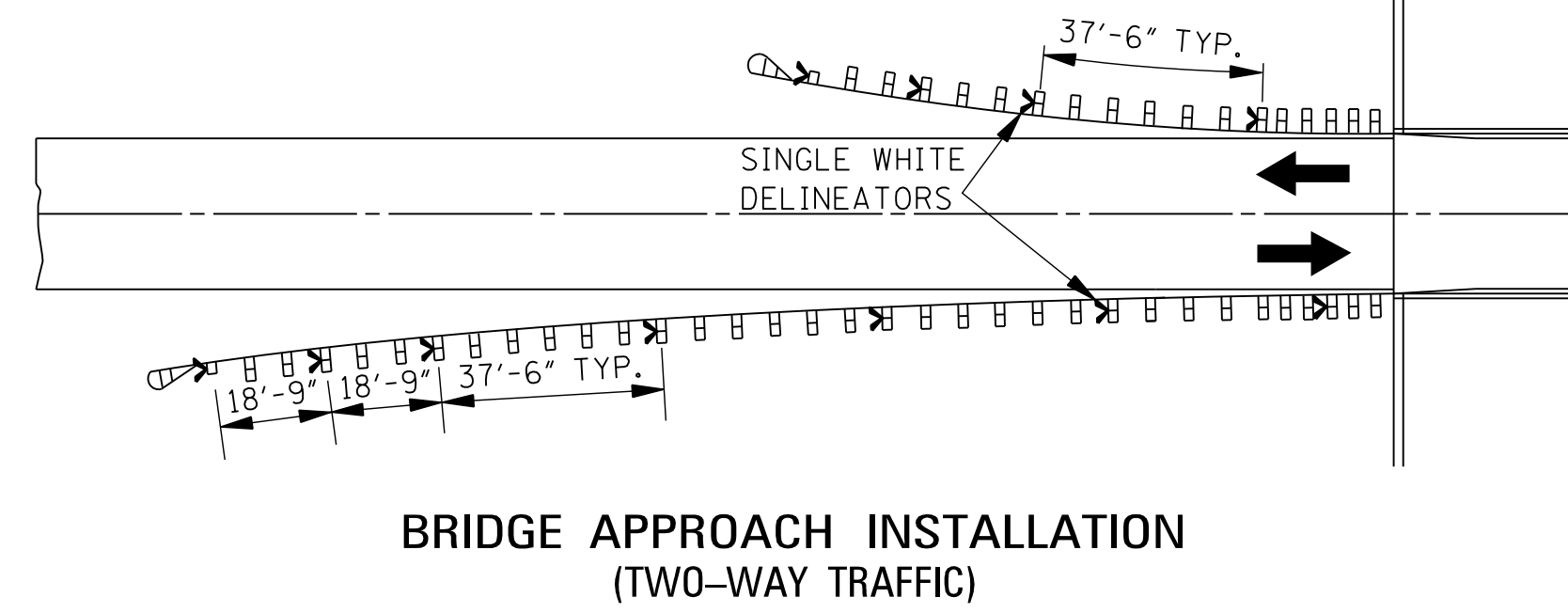
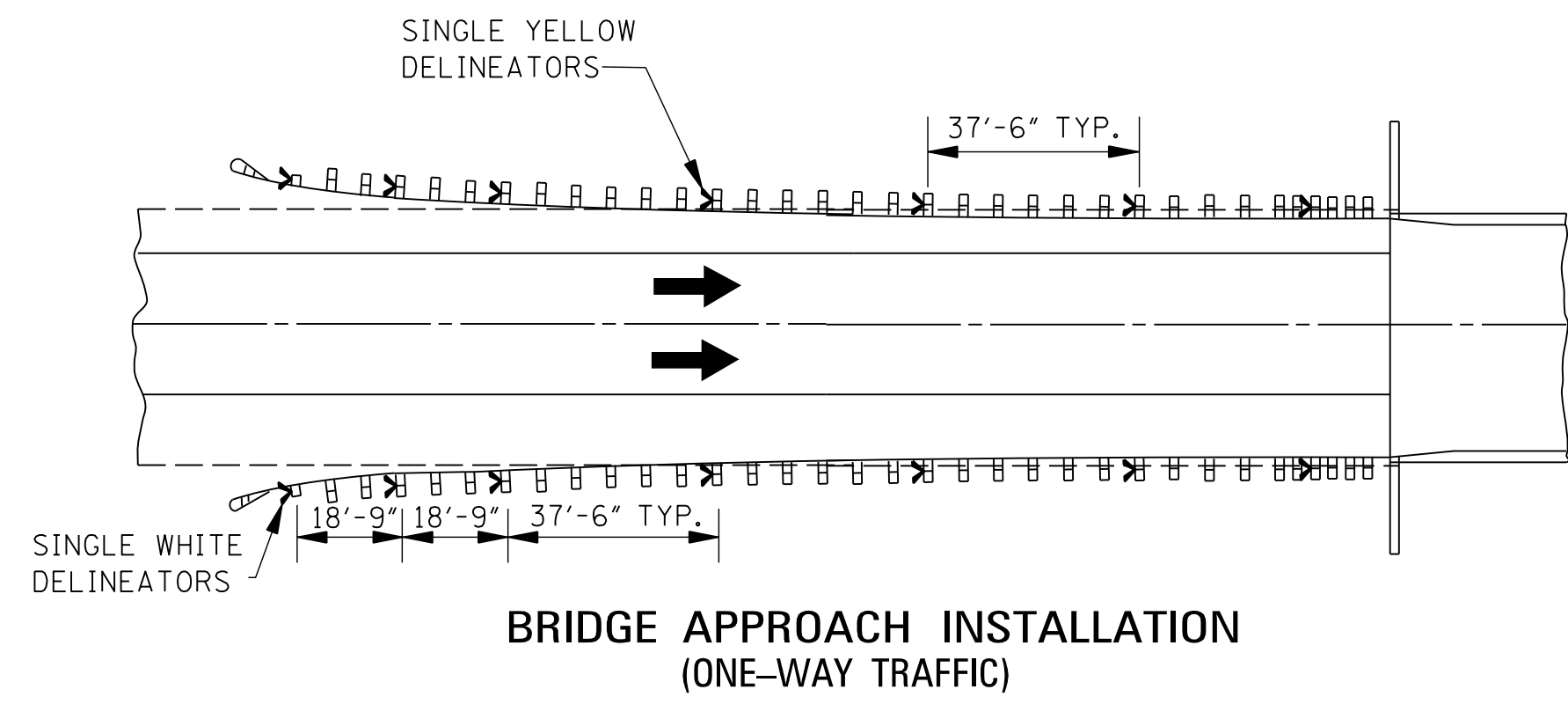
FOOTING DETAILS

GENERAL NOTES:

- UNLESS OTHERWISE SPECIFIED, HORIZONTAL BRACES ARE 3/16" X 2 1/2" X VARIABLE LENGTH FLAT STEEL BARS. BARS ARE WELDED TO PIPE AS SHOWN. WHEN FABRICATION IS COMPLETE, POSTS SHALL BE GALVANIZED AS PER SECTION 630 OF THE STANDARD SPECIFICATION.
- HOLES IN FLAT BARS ARE 3/8" DIAMETER.
- SIGNS ARE FASTENED TO THE FLAT BARS AND U-SECTION POSTS WITH M8 BOLTS WITH FLAT WASHERS AND LOCK NUTS.
- WHERE REQUIRED, SIGNS R6-1L AND R6-1R SHALL BE MOUNTED ON PIPES WITH CLAMPS OR BUCKLE BRACKETS (NOT A PAY ITEM).
- ALL WELDS SHALL BE 3/16" FILLET.
- \* TOP OF POST

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		<p align="center"><b>STANDARD ROADSIDE SIGN ASSEMBLY AND INSTALLATION</b></p>	
DATE			
ISSUE DATE:		AUGUST 01, 2017	

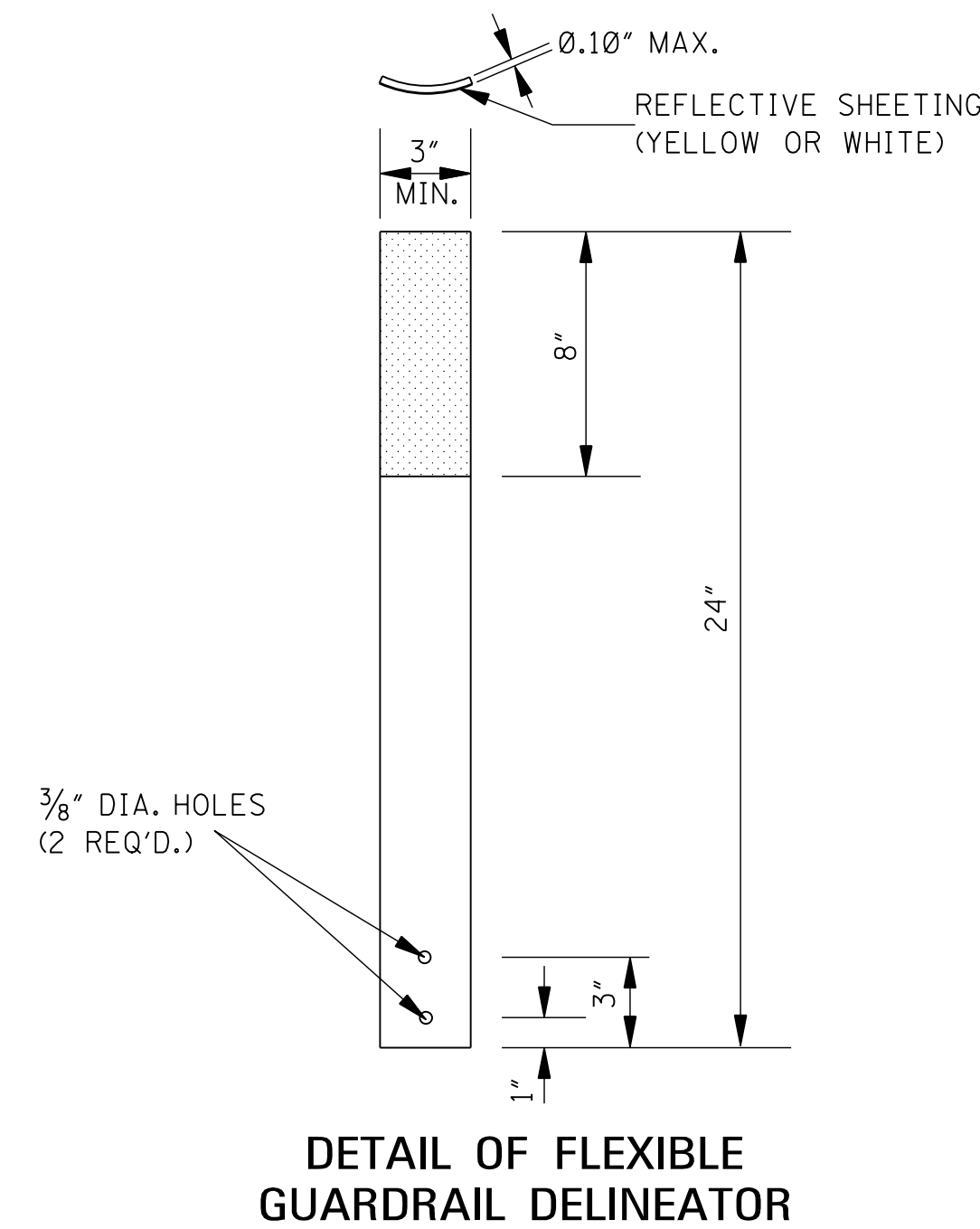
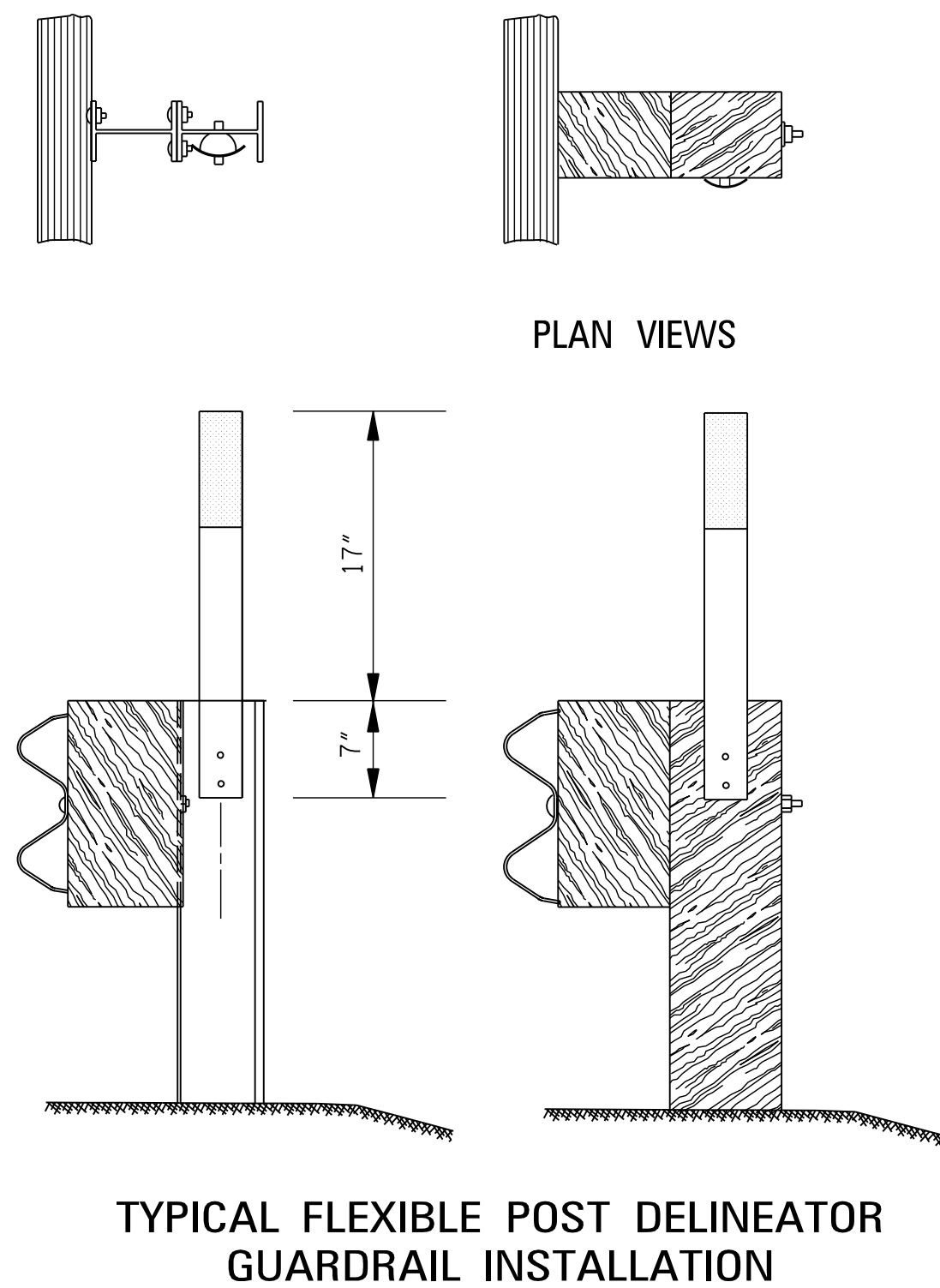




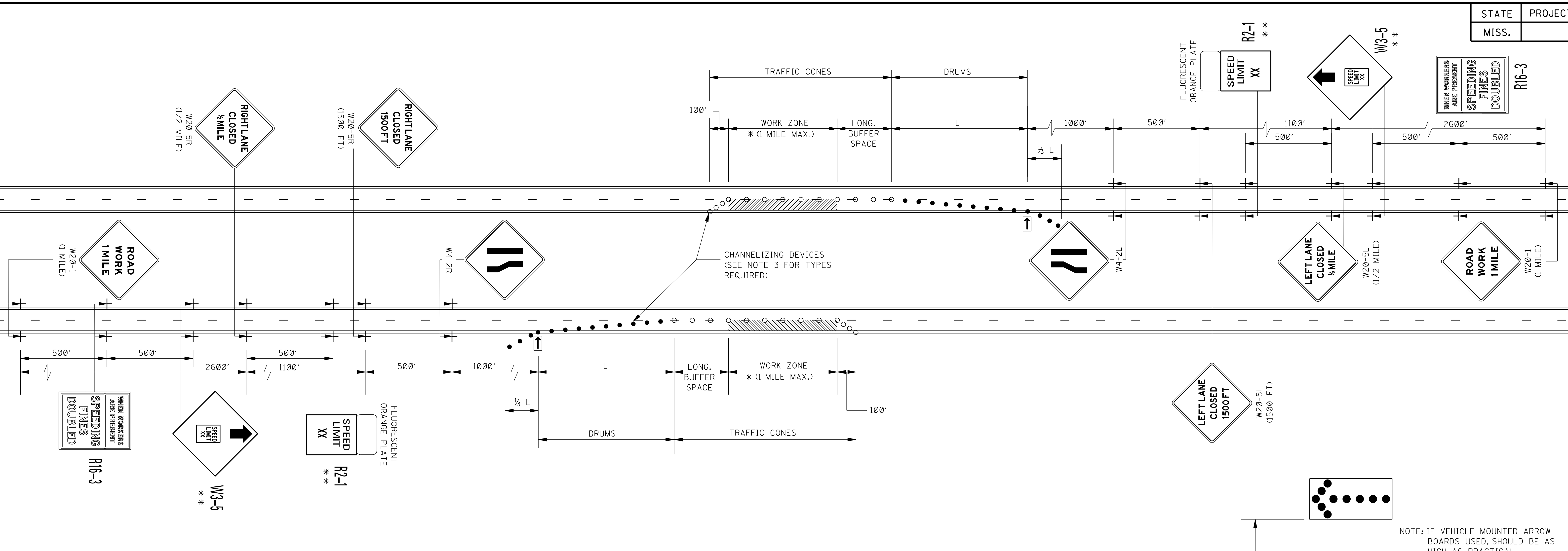
NOTE: ONE-WAY TRAFFIC SHOWN. DELINEATOR SPACING FOR TWO-WAY TRAFFIC SIMILAR. DELINEATOR COLOR WILL BE THE SAME AS THE ADJACENT PAVEMENT EDGE MARKING. THE FIRST THREE (3) MARKERS WILL FACE TRAFFIC IN OFF LANE FOR TWO-WAY TRAFFIC AS SHOWN IN DRAWING FOR OBSTACLE INSTALLATION FOR TWO-WAY TRAFFIC.

GENERAL NOTES:

1. THE UNIT PRICE OF DELINEATOR INCLUDES: COST(S) OF DELINEATOR FACE(S), POST, HARDWARE AND INSTALLATION.
2. DELINEATOR FACE WILL BE ENCAPSULATED LENS REFLECTIVE SHEETING.
3. DELINEATORS FOR GUARDRAIL SHALL BE MOUNTED ON FLEXIBLE POSTS AS FOLLOWS:  
THE DELINEATOR POSTS WILL BE FROM THE DEPARTMENTS "APPROVED SOURCE OF MATERIALS" AND WILL BE FASTENED TO GUARDRAIL POST IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION.



BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		<b>TYPICAL GUARDRAIL DELINEATION</b>	
DATE			
ISSUE DATE:		AUGUST 01, 2017	
		 WORKING NUMBER SN-8C SHEET NUMBER 6317	



GENERAL NOTES:

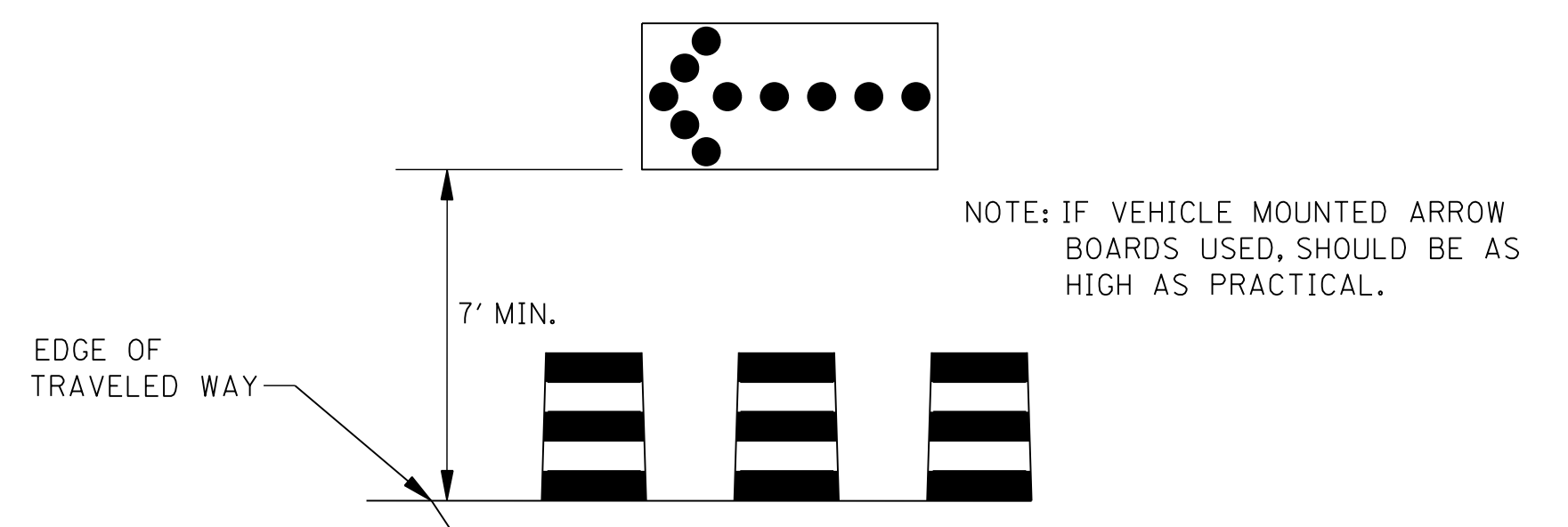
1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft)	TAPER RATES
	TAPER	ALONG LANE LINE & WORK ZONE		
40	40	80	305	27:1
45	45	90	360	45:1
50	50	100	425	50:1
55	55	110	495	55:1
60	60	120	570	60:1
65	65	130	645	65:1
70	70	140	730	70:1

+ NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:  
 $L = WS$  FOR SPEEDS OF 45 mph OR GREATER  
 $L = WS^2/60$  FOR SPEEDS OF 40 mph OR LESS  
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET  
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET  
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

++ NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

- FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
- CHANNELIZING DEVICE TYPES FOR:  
 A. APPROACH TAPER- RETROREFLECTIVE PLASTIC DRUMS  
 B. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT MINIMUM)  
 C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT MINIMUM)
- WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
- FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
- DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48", AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.



LEGEND

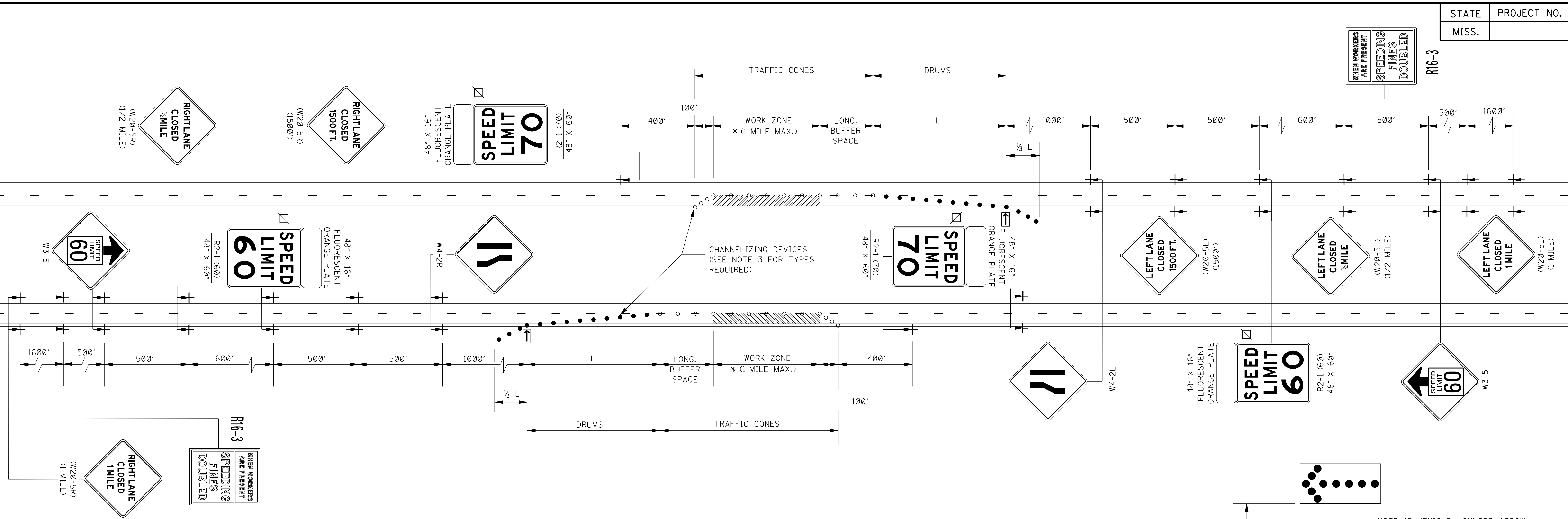
- \* OR AS SHOWN ELSEWHERE ON THE PLANS.
- \*\* THE SPEED ON R2-1 AND W3-5 SIGNS SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT UNLESS OTHERWISE DIRECTED BY COMMISSION ORDER.
- FLASHING ARROW PANEL (TYPE "C")
- RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
- TRAFFIC CONES (28" HEIGHT MINIMUM)

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
REVISION	<b>TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT LESS THAN 65 MPH (4-LANE: MEDIAN LANE OR OUTSIDE LANE CLOSURE) (WORK DAY ONLY)</b>
DATE	ISSUE DATE: AUGUST 01, 2017

**MDOT**  
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

WORKING NUMBER  
TCP-2

SHEET NUMBER  
6352



GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

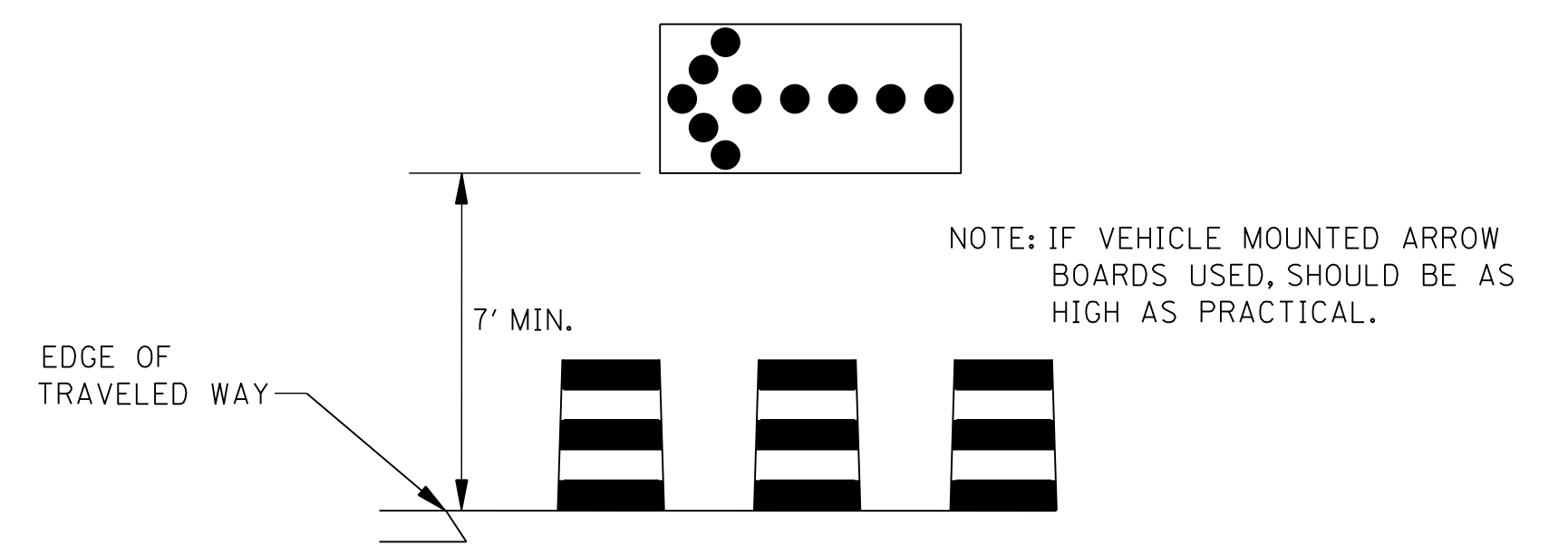
POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft)	TAPER RATES
	TAPER	ALONG LANE LINE & WORK ZONE		
40	40	80	305	27:1
45	45	90	360	45:1
50	50	100	425	50:1
55	55	110	495	55:1
60	60	120	570	60:1
65	65	130	645	65:1
70	70	140	730	70:1

+ NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:  
 L = WS FOR SPEEDS OF 45 mph OR GREATER  
 L = WS<sup>2</sup>/60 FOR SPEEDS OF 40 mph OR LESS  
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET  
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET  
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

++ NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.


- CHANNELIZING DEVICE TYPES FOR:
  - APPROACH TAPER- RETROREFLECTIVE PLASTIC DRUMS
  - ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT MINIMUM)
  - EXIT TAPER- TRAFFIC CONES (28" HEIGHT MINIMUM)
- WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
- FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING AND REGULATORY SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
- DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48". AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
- ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON THE FACE OF SIGN.
- ADDITIONAL REDUCED REGULATORY SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. TWO (2) WILL BE REQUIRED FOR EACH RAMP AND LOCATION WILL BE DETERMINED BY THE ENGINEER.
- THIS TRAFFIC CONTROL PLAN, WITH SPEED ZONE, MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
- LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.
- A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS AND "REDUCED SPEED AHEAD" SIGNS REQUIRED FOR LANE CLOSURE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

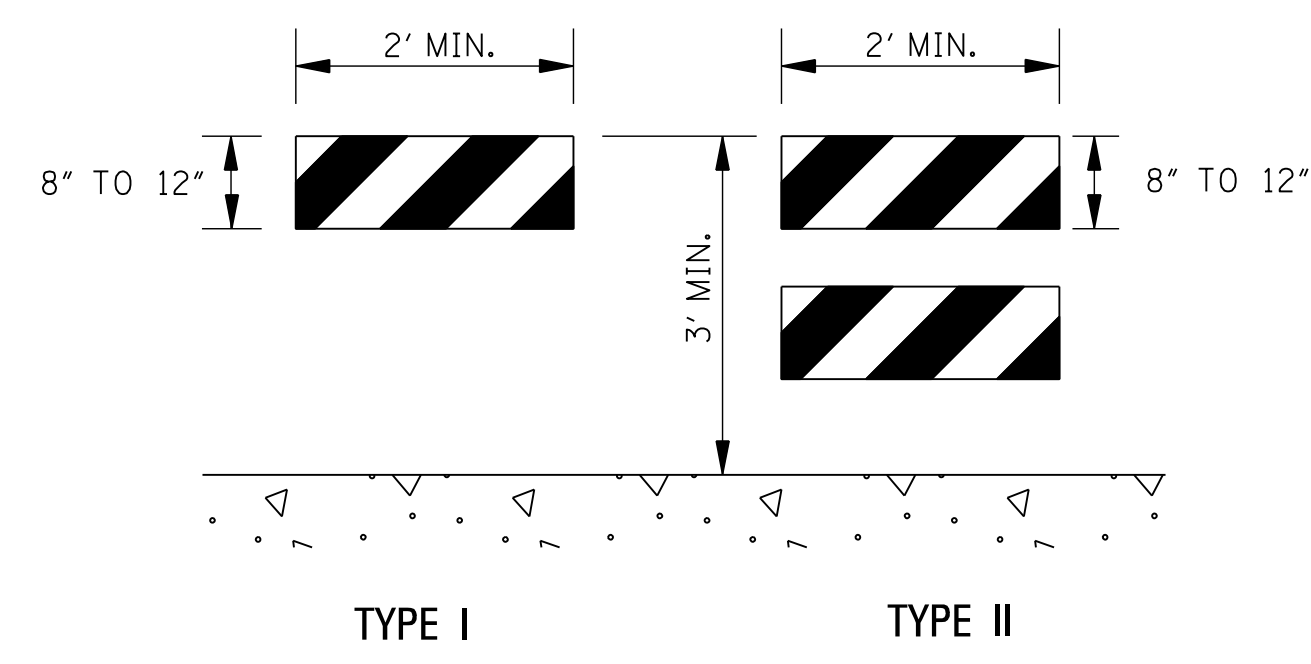


LEGEND

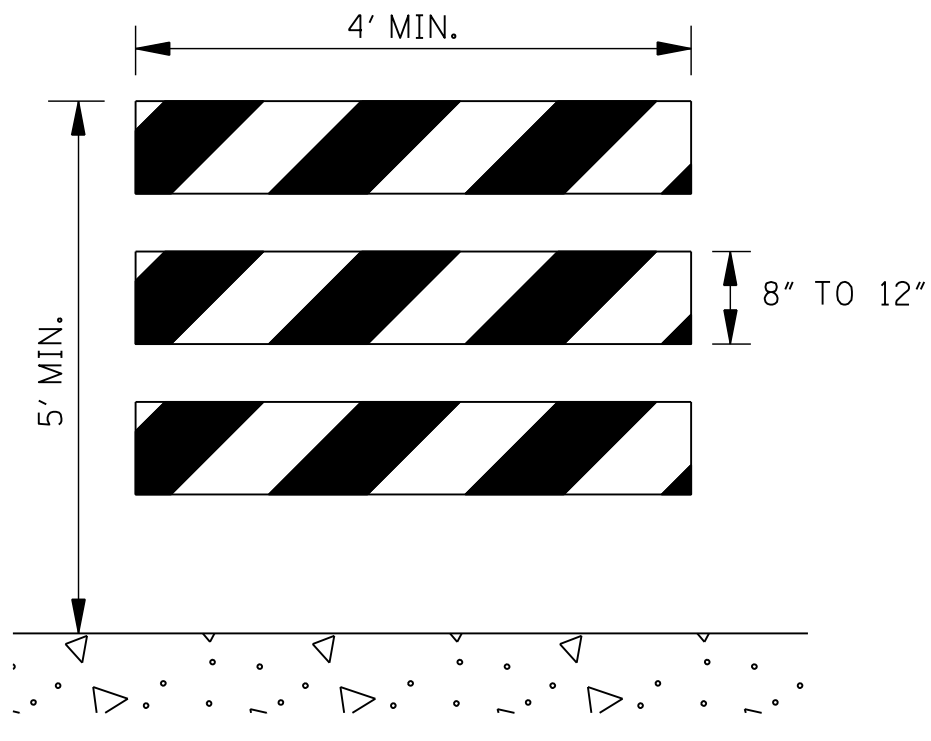
- \* OR AS SHOWN ELSEWHERE ON THE PLANS.
- FLASHING ARROW PANEL (TYPE "C")
- BLACK LEGEND AND BORDER ON WHITE BACKGROUND
- RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
- TRAFFIC CONES (28" HEIGHT)

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION
REVISION	STANDARD PLAN <b>TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (WORK DAY ONLY)</b>
DATE	ISSUE DATE: AUGUST 01, 2017

  
 WORKING NUMBER  
 TCP-5  
 SHEET NUMBER  
 6355



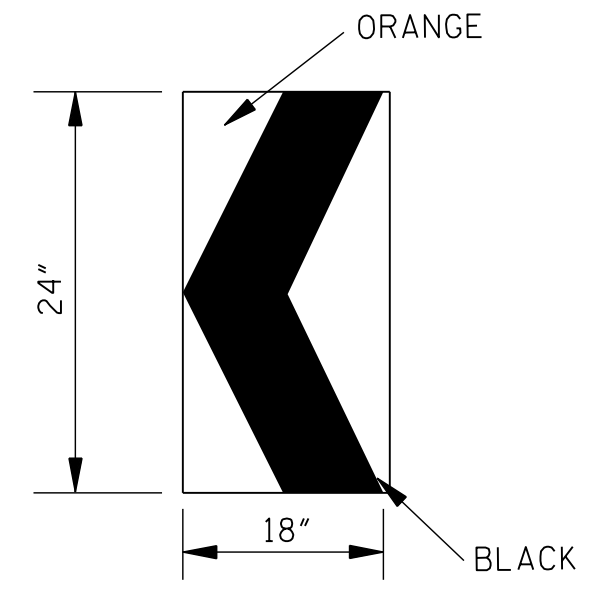
TYPE I                      TYPE II



TYPE III

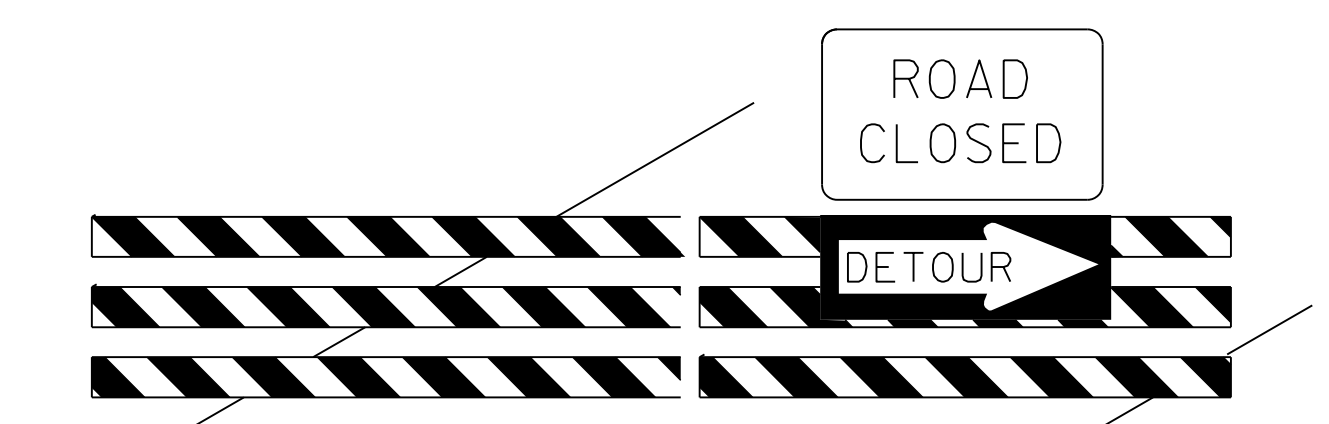
**STANDARD BARRICADES**

1. THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).
2. RAIL STRIPE SHOULD BE 6 INCHES, EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.
3. DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
4. FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.
5. BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES WHICH REQUIRE CRASHWORTHINESS ACCEPTANCE LETTERS. TO DATE, 2-IN. THICK TIMBER RAILS HAVE NOT BEEN SUCCESSFULLY CRASH TESTED. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE:  
[http://safety.fhwa.dot.gov/roadway\\_dept/policy\\_guide/road\\_hardware/cat2.cfm](http://safety.fhwa.dot.gov/roadway_dept/policy_guide/road_hardware/cat2.cfm)



**CHEVRON SIGN  
DETAIL**

1. A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
2. THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.
3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.

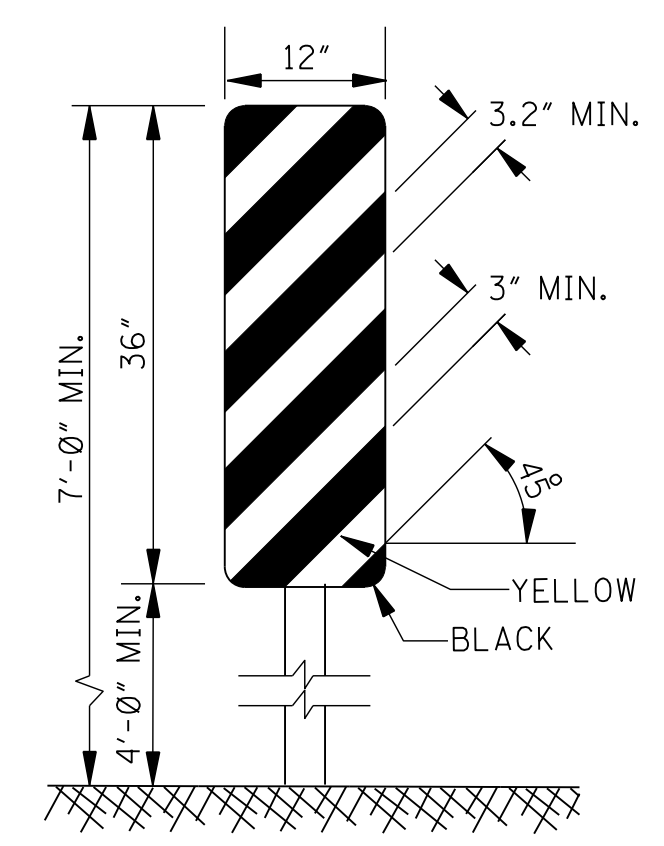


**BARRICADE CLOSING A ROAD**

**BARRICADE CHARACTERISTICS**

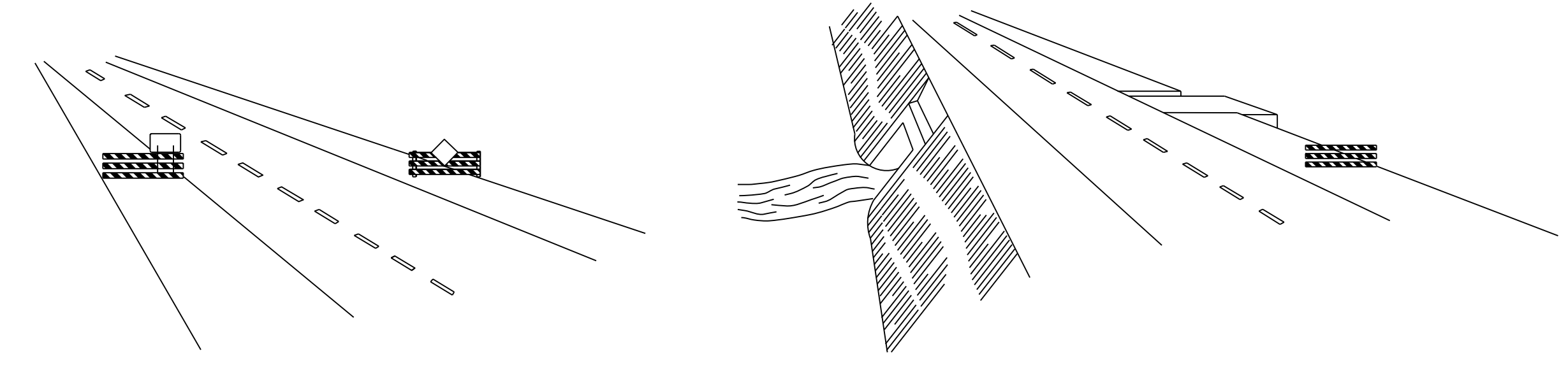
	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF RETROREFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS

- \* 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- \*\* 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 in<sup>2</sup> OF REFLECTIVE AREA FACING TRAFFIC.



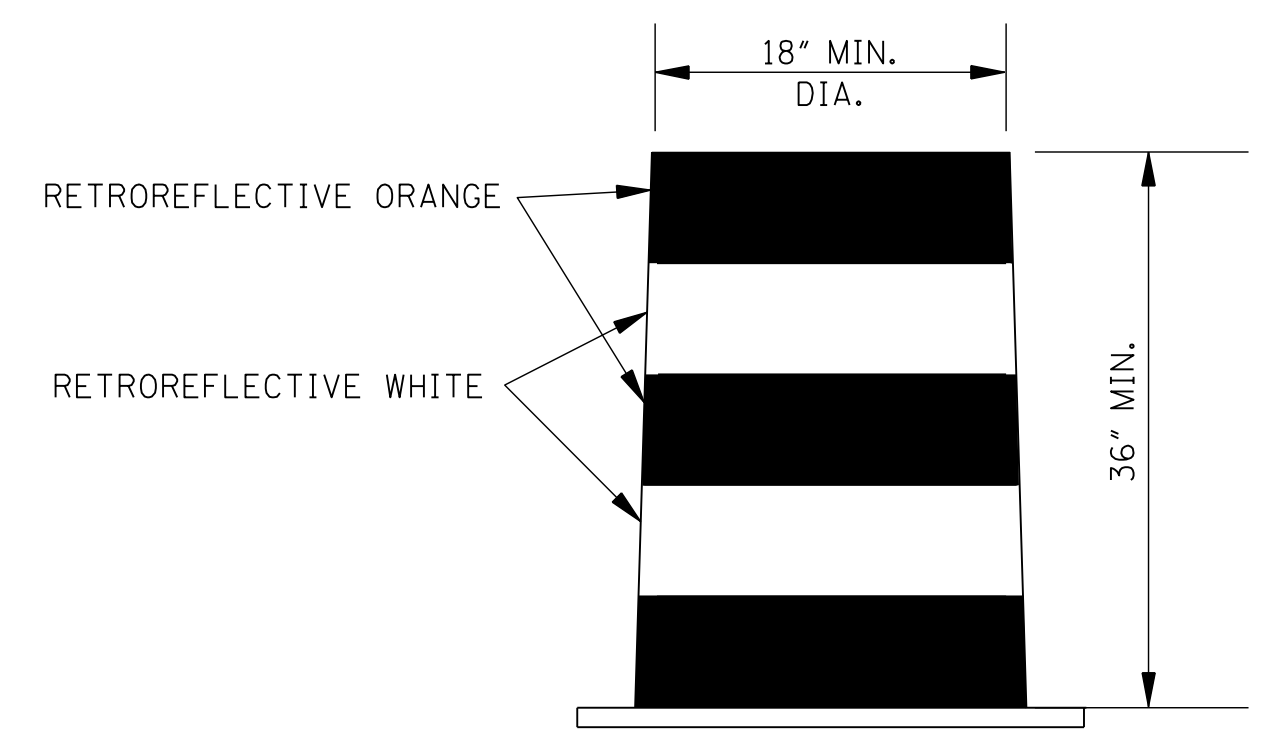
**TYPE 3 OBJECT MARKER  
(OM-3R)**

1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
2. THE OM-3R IS SHOWN. THE OM-3L IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
3. THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



**WING BARRICADES**

1. WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
2. WING BARRICADES SHOULD BE USED:
  - A. IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
  - B. IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.



**PLASTIC DRUM STRIPING DETAIL**

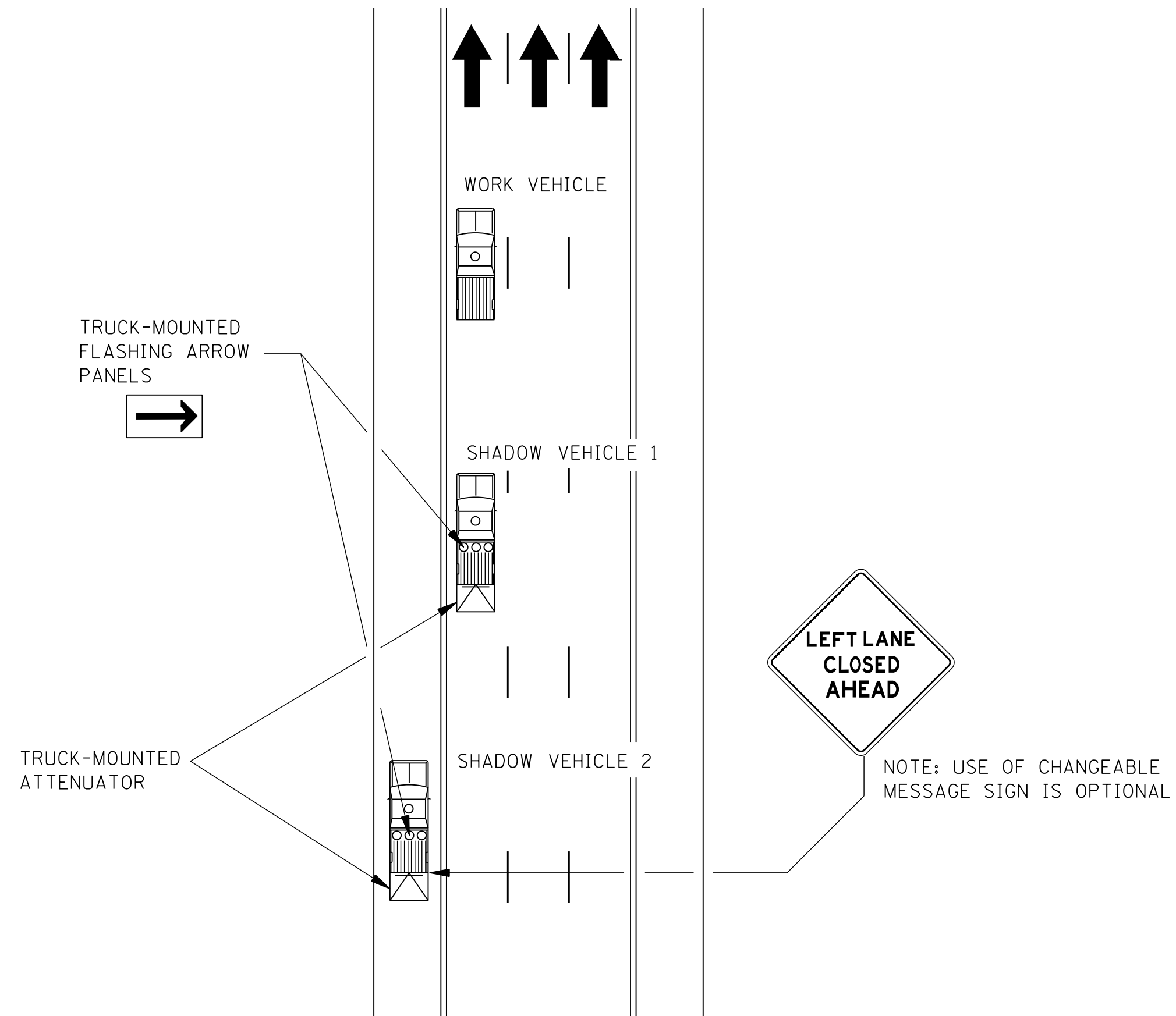
1. PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT COLOR ON DRUMS SHALL BE ORANGE WITH FOUR (4) RETROREFLECTIVE, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
2. DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
3. WHERE PRACTICAL PLASTIC DRUMS SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		<p><b>HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS</b></p>	
DATE			
ISSUE DATE:		AUGUST 01, 2017	



WORKING NUMBER  
TCP-8  
SHEET NUMBER  
6358

### MOBILE OPERATIONS ON MULTILANE ROAD

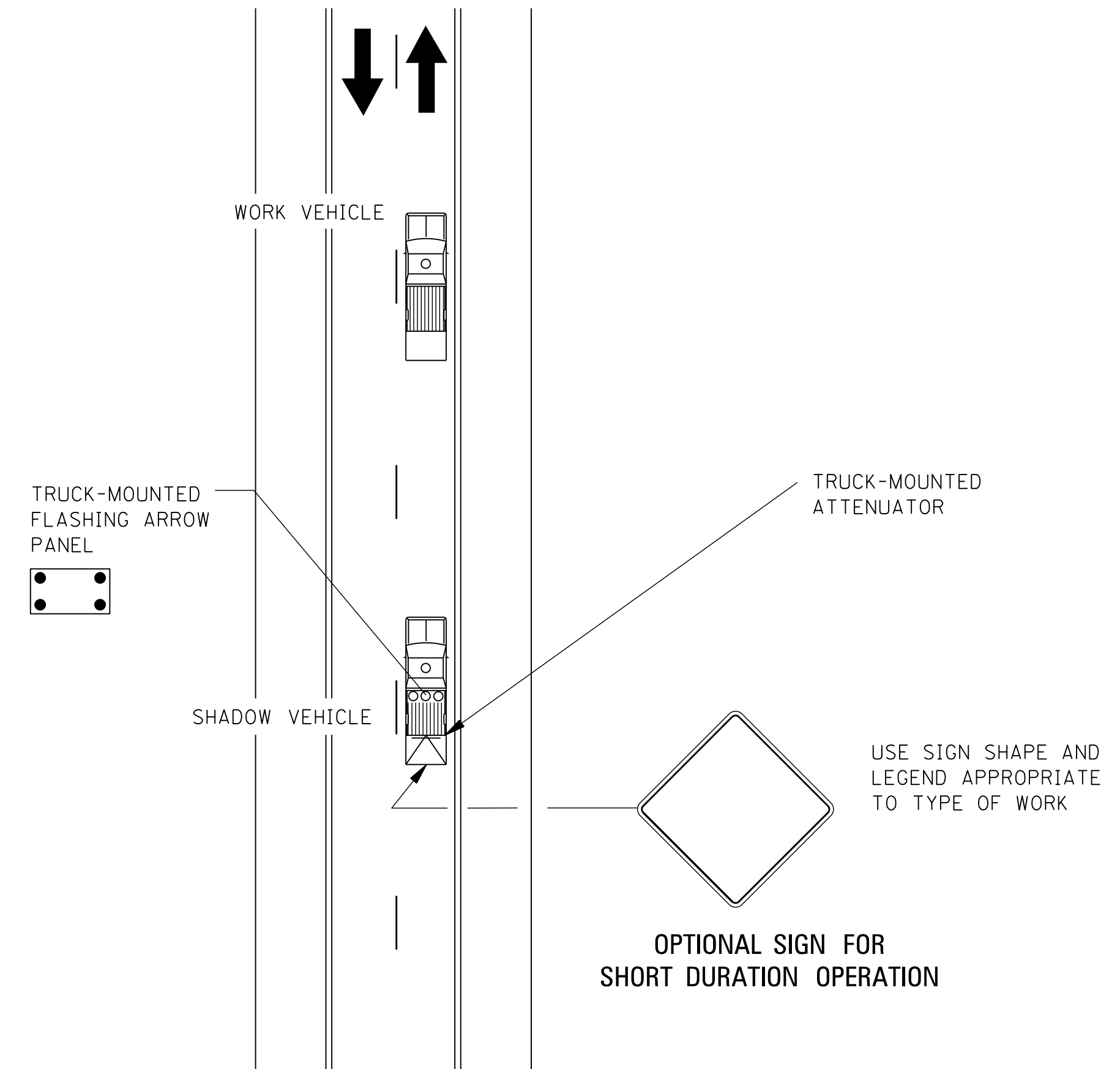


### MOBILE OPERATIONS ON MULTILANE ROAD

NOTES FOR MULTILANE LANE OPERATION:

1. VEHICLES USED FOR THESE OPERATIONS SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT, SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLAGS, SIGNS, OR ARROW PANELS.
2. SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK MOUNTED ATTENUATOR (TMA). AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON SHADOW VEHICLE 2 SO AS NOT TO OBSCURE THE ARROW PANEL.
3. SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK-MOUNTED ATTENUATOR (TMA).
4. SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
5. WHEN ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, SHADOW VEHICLE 2 SHOULD BE ELIMINATED.
6. ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE SHOULD BE USED (i.e., VEHICLE 3 ON THE SHOULDER (IF PRACTICAL), VEHICLE 2 IN THE CLOSED LANE, AND VEHICLE 1 IN THE CLOSED LANE).
7. ARROW PANELS SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
8. WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
9. VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
10. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

### MOBILE OPERATIONS ON TWO-LANE ROAD

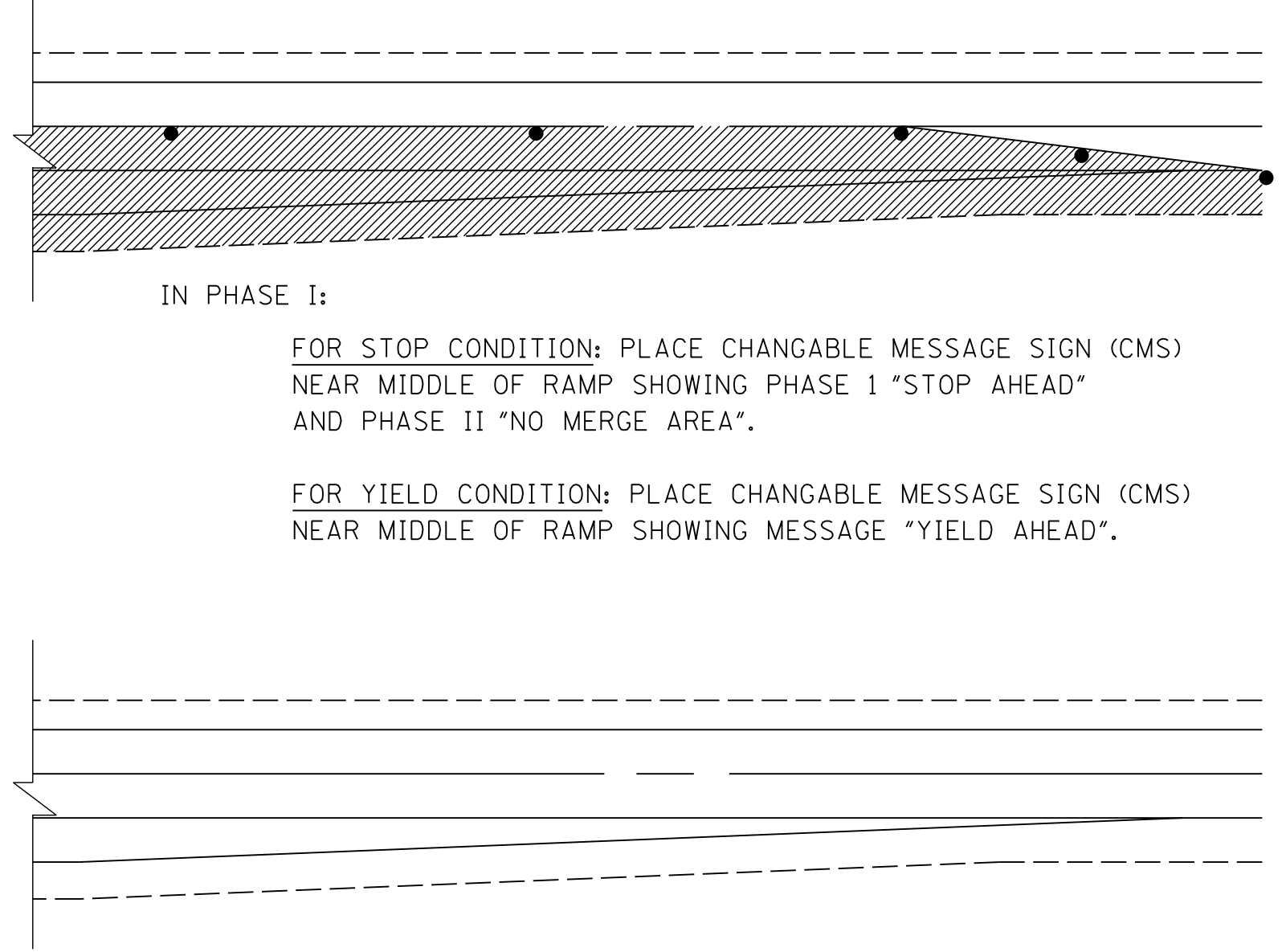
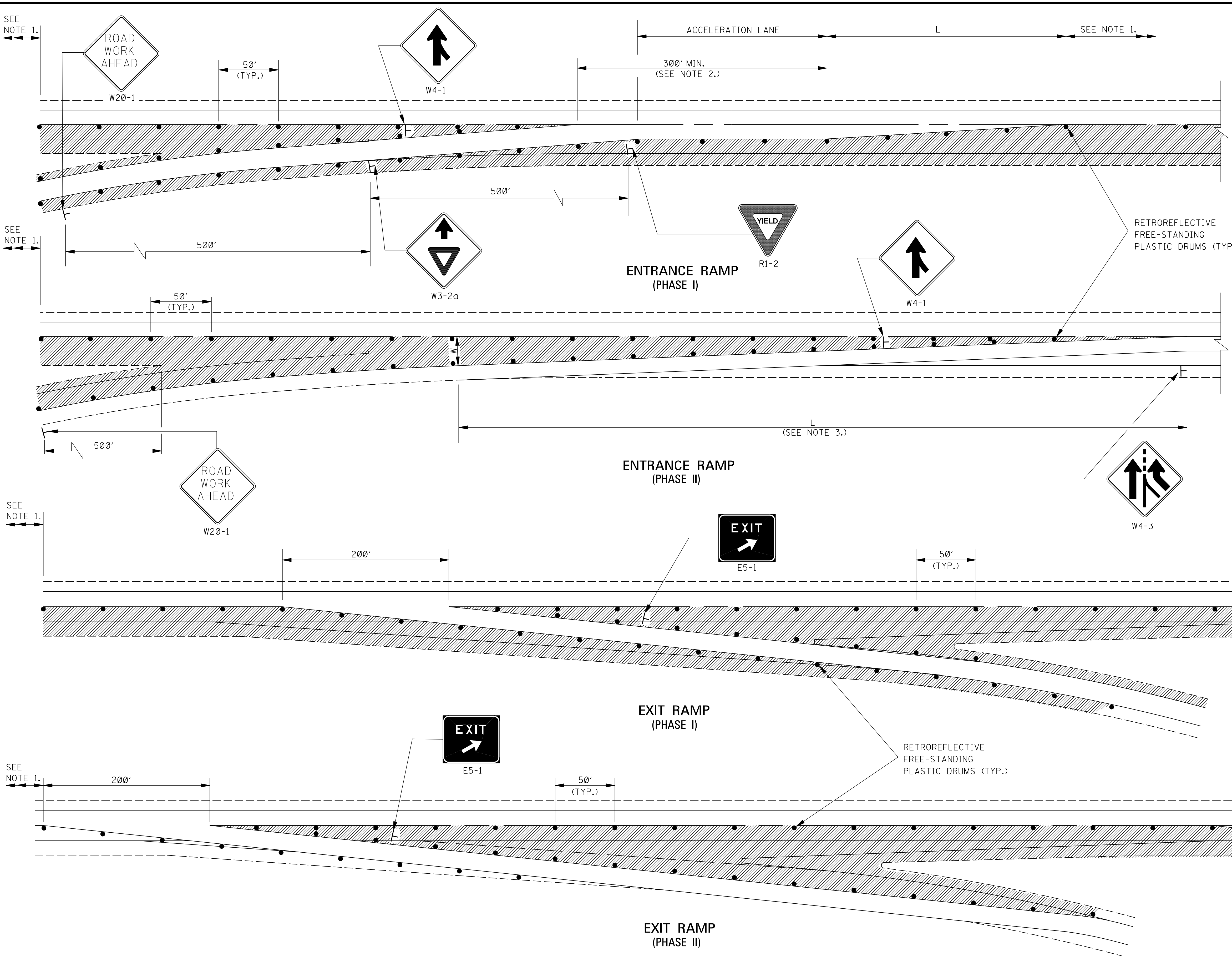


### MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES FOR TWO-LANE OPERATION:

1. WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY, AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
2. THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. SHADOW VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, THE SHADOW VEHICLE SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
3. ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
4. A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
5. THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW VEHICLES SHALL BE EQUIPPED WITH TWO HIGH-INTENSITY FLASHING LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN. SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
6. VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
7. ARROW BOARD TO BE USED IN CAUTION MODE.
8. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

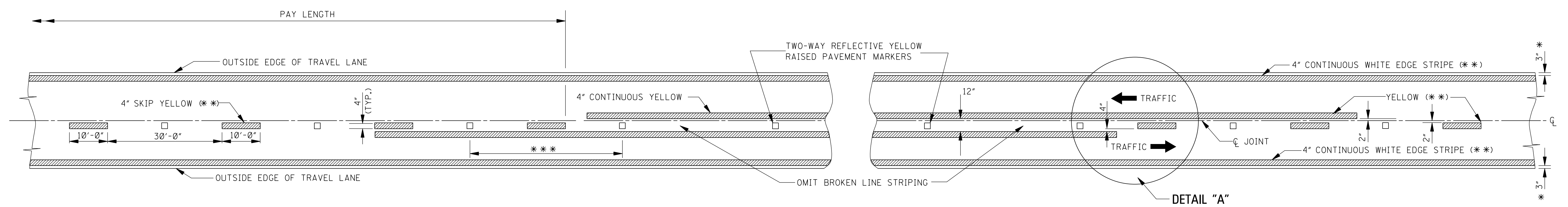
BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		<b>TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO-LANE ROADS</b>	
DATE			
ISSUE DATE:		AUGUST 01, 2017	
		 WORKING NUMBER TCP-9 SHEET NUMBER 6359	



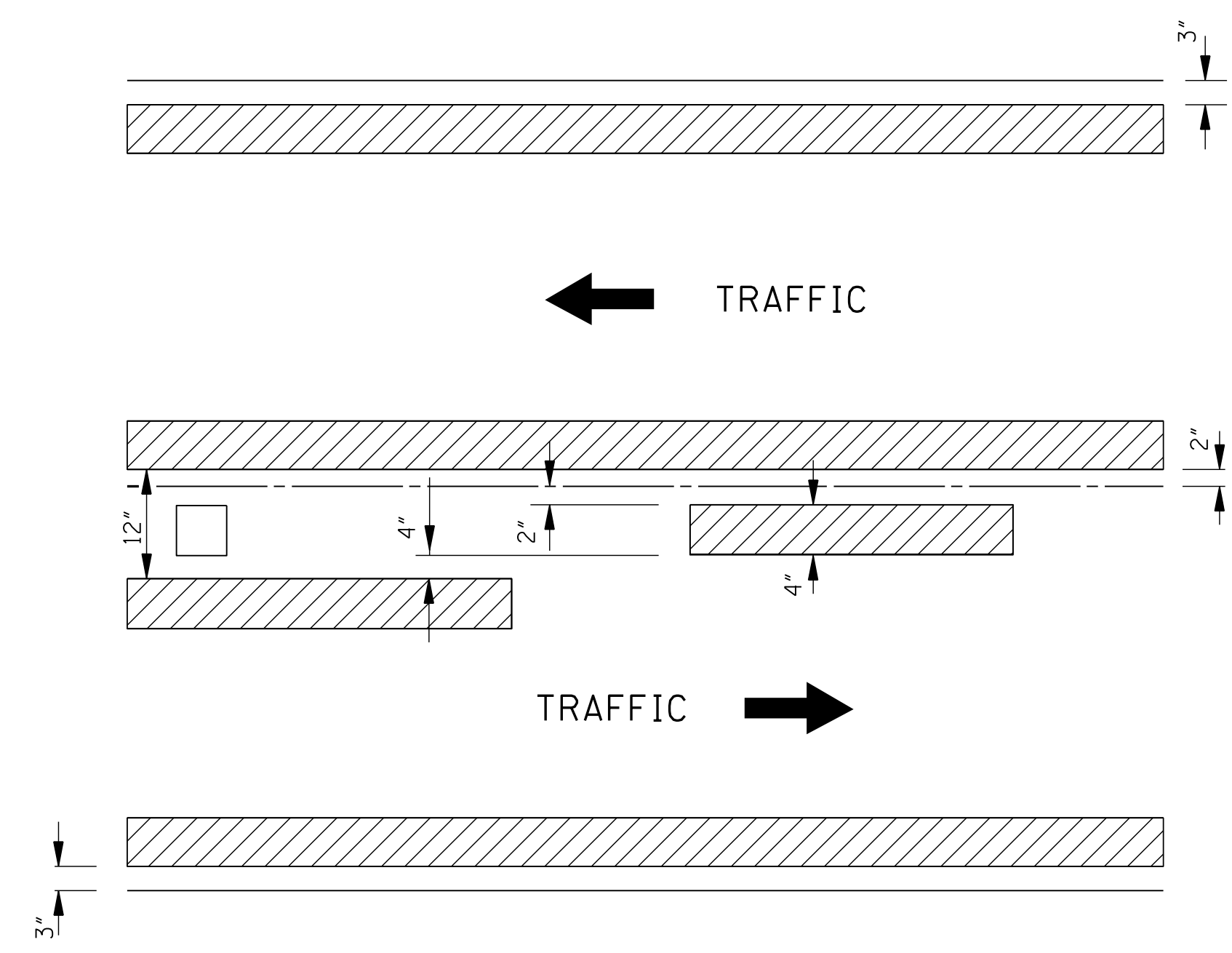
IN PHASE I:  
 FOR STOP CONDITION: PLACE CHANGABLE MESSAGE SIGN (CMS) NEAR MIDDLE OF RAMP SHOWING PHASE I "STOP AHEAD" AND PHASE II "NO MERGE AREA".  
 FOR YIELD CONDITION: PLACE CHANGABLE MESSAGE SIGN (CMS) NEAR MIDDLE OF RAMP SHOWING MESSAGE "YIELD AHEAD".

- GENERAL NOTES:
1. FOR SIGNING, FLASHING ARROW PANEL, AND DRUM SPACING CRITERIA, SEE TYPICAL TRAFFIC CONTROL PLANS FOR LANE CLOSURES ON SHEETS TCP-2, TCP-3, TCP-4, AND TCP-5, AS APPROPRIATE
  2. IF NO ACCELERATION LANE EXISTS FOR THE TEMPORARY ENTRANCE, THE YIELD SIGN (R1-2) AND THE YIELD AHEAD SIGN (W3-2A) SHALL BE REPLACED WITH A STOP SIGN (R1-1) AND A STOP AHEAD SIGN (W3-1a), ONE ON EACH SIDE OF APPROACH. WHERE STOP SIGNS ARE USED, A TEMPORARY STOP LINE SHOULD BE PLACED ACROSS THE RAMP AT THE DESIRED STOP LOCATION. IF INSUFFICIENT GAPS ARE AVAILABLE IN THE TRAFFIC STREAM, CONSIDERATION SHOULD BE GIVEN TO CLOSING THE RAMP.
  3. DETERMINE TAPER LAYOUTS USING THE FOLLOWING EQUATIONS:  
 $L = WS$  FOR SPEED OF 45 mph OR GREATER  
 $L = WS^2/60$  FOR SPEEDS OF 40 mph OR LESS  
 WHERE:  
 L = MINIMUM LENGTH OF TAPER IN FEET  
 W = WIDTH OF OFFSET IN FEET  
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
  4. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		<b>DETAILS OF OUTSIDE LANE CLOSURE AT EXIT AND ENTRANCE RAMPS</b>	
DATE		ISSUE DATE: AUGUST 01, 2017	
		 WORKING NUMBER TCP-10 SHEET NUMBER 6360	



**TWO-WAY TRAFFIC**  
(ASPHALT OR CONCRETE PAVEMENT)



**DETAIL "A"**



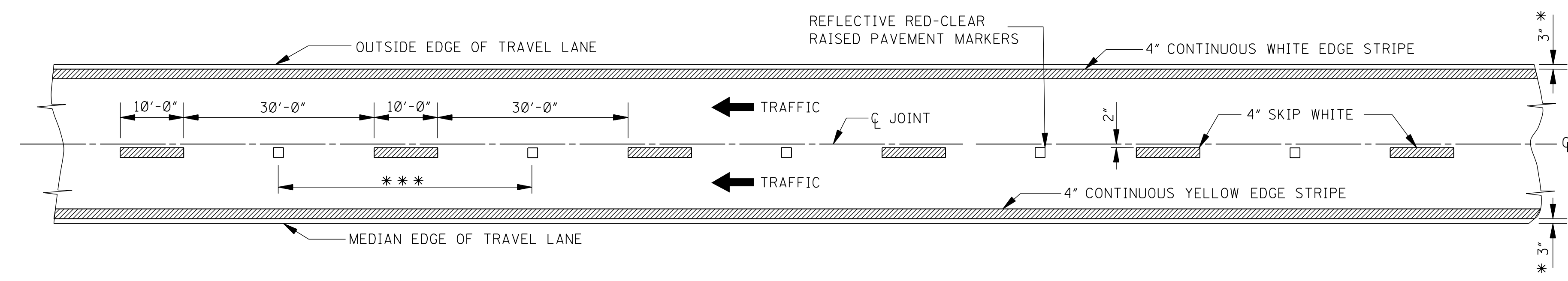
GENERAL NOTES:

- \* 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- \*\* 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
- 3. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS
- \*\*\* 4. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

	URBAN AREA (ft-in)	RURAL AREA (ft-in)
TANGENT SECTIONS	40'-0"	80'-0"
HORIZONTAL CURVES	40'-0"	40'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

† NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

5. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED SOURCES OF MATERIALS."



**4-LANE WITH ONE-WAY TRAFFIC**

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
REVISION	<b>TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS</b>
DATE	ISSUE DATE: AUGUST 01, 2017



WORKING NUMBER  
TCP-13  
SHEET NUMBER  
6363