

STATE	PROJECT NUMBER	SHEET NO.
MISSISSIPPI	IM-0006-01(096)	1

GENERAL INDEX

INCLUDED THIS PROJECT	BEGIN WITH SHEET
<input checked="" type="checkbox"/> ROADWAY	1
<input type="checkbox"/> PERMANENT SIGNS	1001
<input type="checkbox"/> TRAFFIC SIGNALS	2001
<input type="checkbox"/> ITS COMPONENTS	3001
<input type="checkbox"/> LIGHTING	4001
<input type="checkbox"/> (RESERVED)	5001
<input checked="" type="checkbox"/> ROADWAY STANDARD DWGS	6001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (LRFD)	7001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (STD. SPEC.)	7501
<input type="checkbox"/> BRIDGE	8001
<input type="checkbox"/> CROSS SECTIONS	9001

BRIDGE STRUCTURES REQ'D.
NONE

BOX BRIDGES REQ'D.
NONE

CONVENTIONAL SYMBOLS

COUNTY LINE	
TOWN CORPORATION LINE	
SECTION LINE	
EXISTING ROAD OR TRAVELED WAY	
PROPOSED ROAD OR TRAVELED WAY	
RAILROAD	
SURVEY LINE	
BRIDGES	

LENGTH OF ROADWAY	20,275	FT.	3.791	MI.
LENGTH OF BRIDGES	260	FT.	0.049	MI.
LENGTH OF PROJECT (NET)			3.840	MI.
LENGTH OF EXCEPTIONS	0	FT.	0	MI.
LENGTH OF PROJECT (GROSS)			3.840	MI.

STATE OF MISSISSIPPI

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

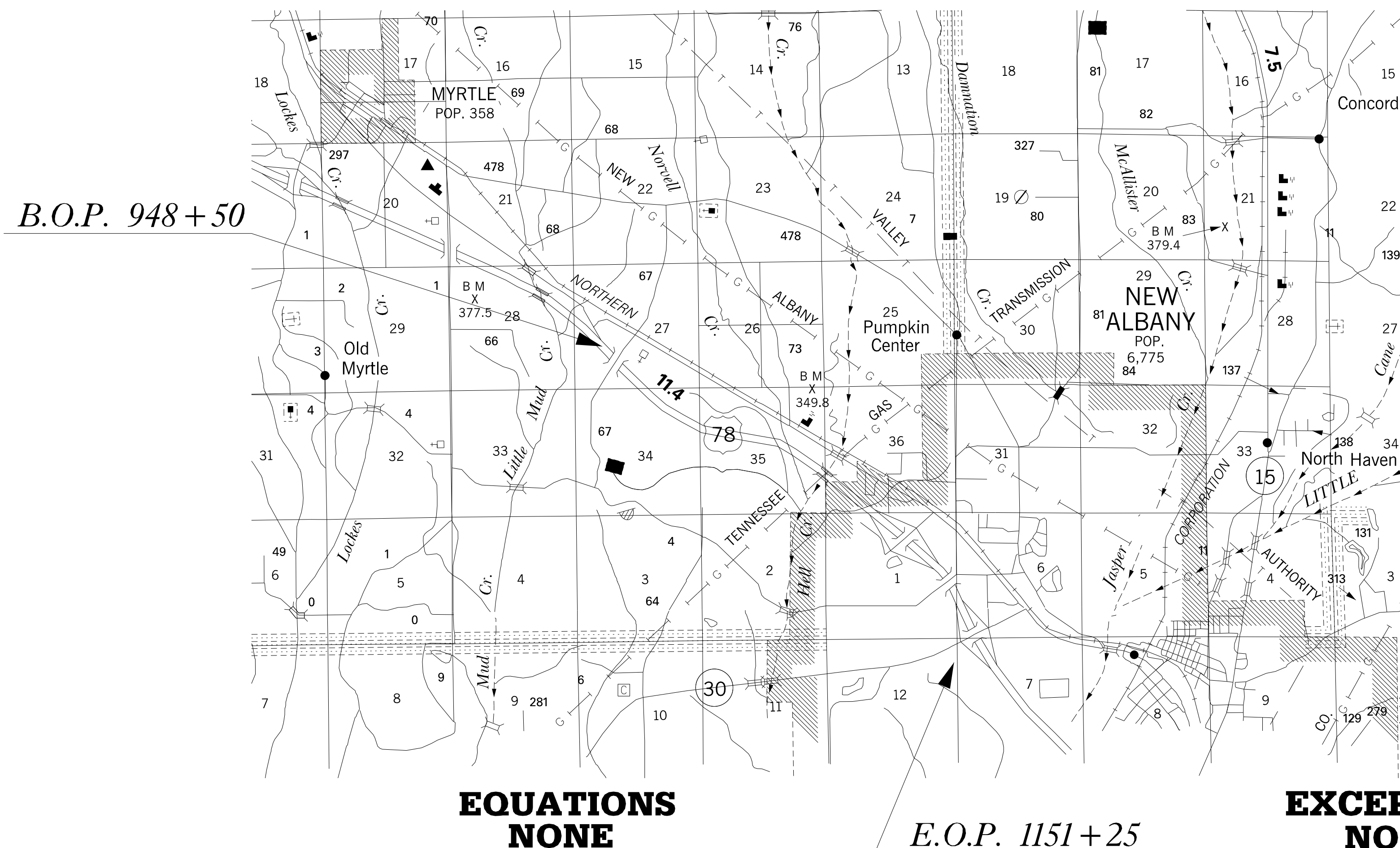
PLAN AND PROFILE OF PROPOSED STATE HIGHWAY FEDERAL AID PROJECT NO. IM-0006-01(096)

I-22 FROM 7.4 MILES EAST OF BENTON COUNTY LINE TO SR 30W IN NEW ALBANY, MS UNION COUNTY

FMS CON. NO. 107826 / 301000

SCALES

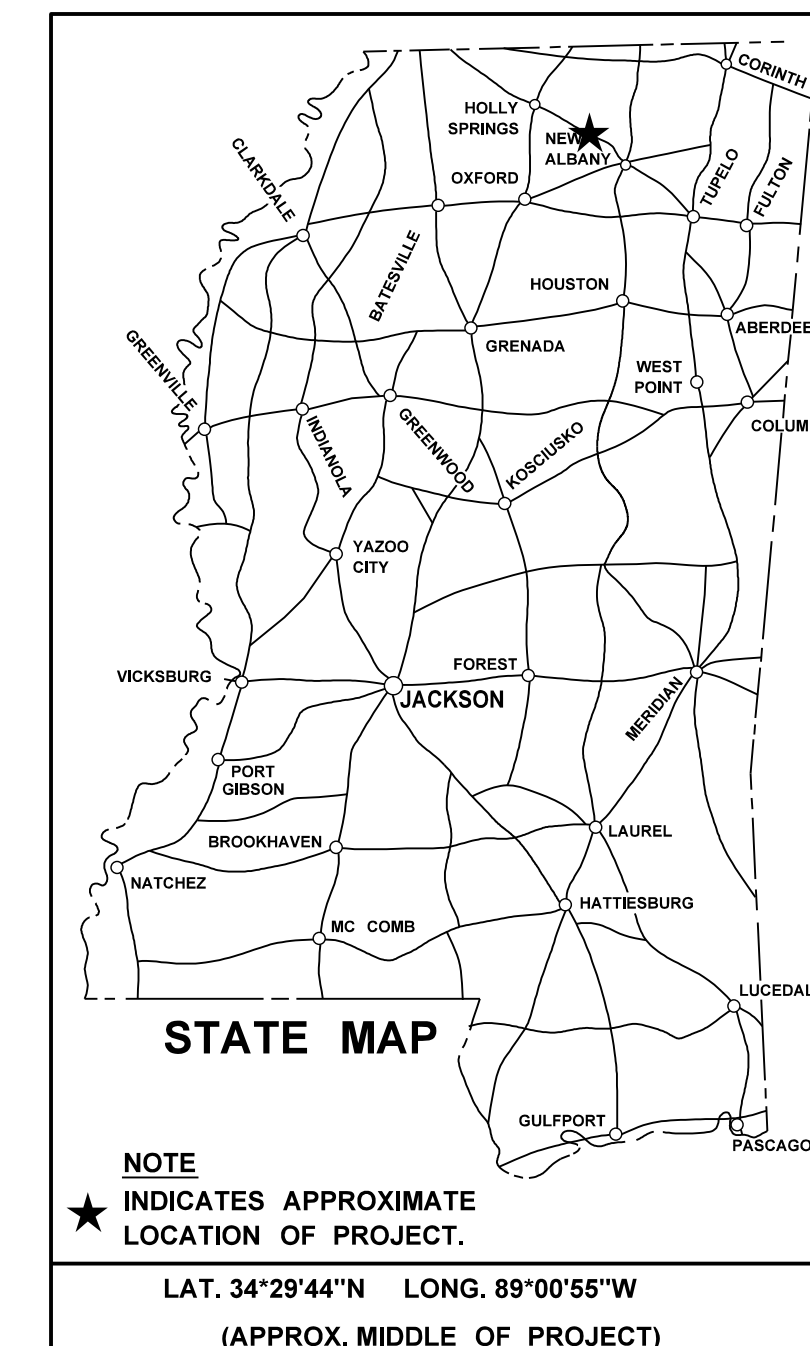
PLAN	1 IN. = 100 FT.
PROFILE {	HOR. 1 IN. = 100 FT.
	VERT. 1 IN. = 10 FT.
LAYOUT	1 IN. = FT.



EQUATIONS
NONE

EXCEPTIONS
NONE

LENGTH DATA



DESIGN CONTROL

MPH = V (SPEED DESIGN)

ADT () = : ADT () =

DHV = : D = % T = %

PERMITS ACQUIRED BY MDOT

WETLANDS AND WATERS PERMITS		
	WATERS	WETLANDS
NATIONWIDE #14	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
NATIONWIDE (OTHER)*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
GENERAL*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
INDIVIDUAL (404)*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
STORMWATER PERMIT <input checked="" type="checkbox"/>		
Y	REQUIRED, CNOI SUBMITTED BY MDOT (DISTURBED AREA > 5 ACRES)	
S	REQUIRED, SCNOI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES)	
N	NO STORMWATER PERMIT REQUIRED (<1 ACRE)	
APPROVED BY: _____		

P S & E DATE: 12/11/19

APPROVED:	_____
DEPUTY EXECUTIVE DIRECTOR / CHIEF ENGINEER	_____
EXECUTIVE DIRECTOR	_____



12/18/2019 10:20:55 AM TLE SHEET

STATE	PROJECT NO.
MISS.	IM-0006-01(096)

DESCRIPTION OF SHEET

WKG.
NO. SH.
NO.

TITLE SHEET		1
DETAILED INDEX & GENERAL NOTES	DI-1	2
DETAILED INDEX & GENERAL NOTES	DI-2	3
TYPICAL SECTION MILLING & OVERLAY	TS-1	4
TYPICAL SECTION SR 30	TS-2	5
SUMMARY OF QUANTITIES	SQ-1	6
SUMMARY OF QUANTITIES	SQ-2	7
DETAIL OF CONSTRUCTION SIGNING	DCS-1	8
DETAIL OF CONSTRUCTION SIGNING	DCS-2	9
TRAFFIC SIGNAL DETAIL	30 M&0	10
TRAFFIC RECORDER CLASSIFICATION PERMANENT SYSTEM - 4 LANE	SPEC-TC	11
SR 30 STRIPING PLAN	30 STRI	12
PAVEMENT MARKING DETAILS FOR 2-LANE AND 4-LANE DIVIDED HIGHWAYS	PM-1	6051
PAVEMENT MARKING DETAILS FOR INTERCHANGE ENTRANCE RAMP (PARALLEL & TAPER)	PM-3	6053
PAVEMENT MARKING DETAILS FOR INTERCHANGE EXIT RAMP (PARALLEL & TAPER)	PM-4	6054
PAVEMENT MARKING LEGEND DETAILS	PM-6	6056
TWO-WAY RAISED PAVEMENT MARKERS AT INTERSECTING ROADS (2-LANE)	PM-11	6061
RUMBLE STRIPES 4-LANE HIGHWAYS (ASPHALT LANES, 2 FT OR WIDER ASPHALT SHOULDER)	RS-2	6065
GUARDRAIL: "W" BEAM (STEEL POSTS)	GR-1B	6203
GUARDRAIL: BRIDGE END SECTION TYPE "D" MODIFIED	GR-2B	6206
GUARDRAIL: BRIDGE END SECTION TYPE "H" (STEEL POSTS)	GR-2D	6208
GUARDRAIL: TYPICAL INSTALLATION AT BRIDGE APPROACHES FOR 2-LANE, 2-WAY HIGHWAY	GR-4A	6215
GUARDRAIL: MISCELLANEOUS HARDWARE	GR-HW	6221
TYPICAL GUARDRAIL DELINEATION	SN-8C	6317
TRAFFIC CONTROL PLAN WITH FLAGGER (ONE LANE CLOSURE OF TWO-WAY TRAFFIC)	TCP-1	6351
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH	TCP-4	6354
SHORT DURATION CLOSING OF TWO-LANE TWO-WAY HIGHWAYS	TCP-6	6356

GENERAL NOTES

FLUORESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED IN PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.

BIDDERS ARE ADVISED THAT HARD COPIES OF ANY ADDENDA FOR THIS PROJECT WILL NO LONGER BE MAILED. ALL ADDENDA FOR THIS PROJECT WILL BE POSTED TO www.mdot.ms.gov UNDER THE PROPOSAL ADDENDA COLUMN. IT IS THE BIDDER'S RESPONSIBILITY TO CHECK AND SEE IF ANY ADDENDA HAVE BEEN POSTED FOR THIS PROJECT. PLEASE CONTACT CONTRACT ADMINISTRATION DIVISION AT 601-359-7700 FOR ANY QUESTIONS REGARDING ELECTRONIC ADDENDA.

DISTRICT 1

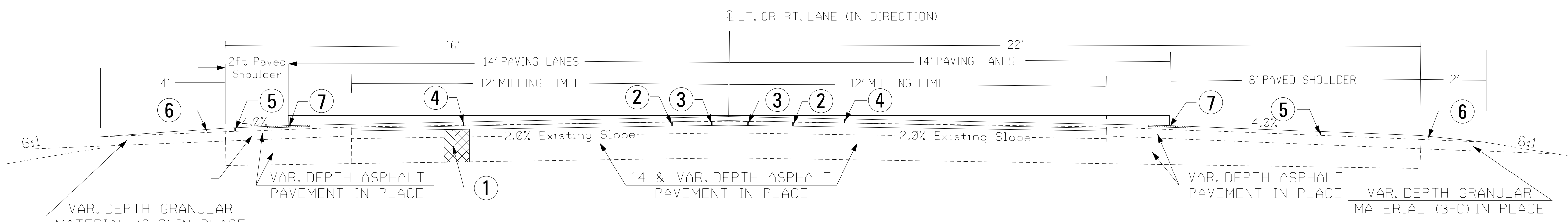
PS & E PLANS-DATE 12/11/19		
FMS CON. # 107826/301000		
REVISIONS		
DATE	SHEET NO.	BY

12/18/2019 10:19:45 AMDEX.DGN



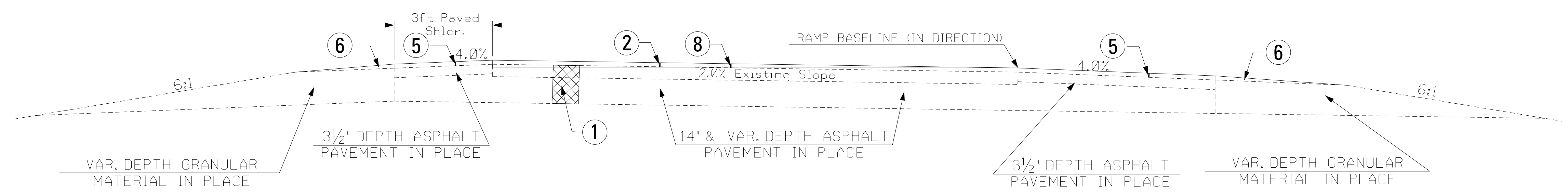
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
DETAILED INDEX	
PROJECT NO. : IM-0006-01(096)	
COUNTY : UNION	
WORKING NUMBER	DI-1
SHEET NUMBER	2
DATE	FILENAME: INDEX.DGN
DESIGN TEAM	CHECKED _____ DATE _____

STATE	PROJECT NO.
MISS.	IM-0006-01(096)



TYPICAL SECTION MAIN FACILITY
(MILL & OVERLAY)
B.O.P. STA. 948+50 TO E.O.P. STA. 1151+25

- 1 REMOVE AND UNDERCUT FAILED AREAS AS DIRECTED BY THE ENGINEER. REMOVAL OF ASPHALT PAVEMENT (ALL DEPTHS) TO BE PAID FOR UNDER PAY ITEM 202-B007, REMOVAL OF ASPHALT PAVEMENT. REMOVAL OF UNDERCUT MATERIAL TO BE PAID FOR UNDER PAY ITEM NO. 203-G001, EXCESS EXCAVATION (F.M.)(A.H.). BACKFILL WITH 9.5-MM ASPHALT PAVEMENT PAY ITEM NO. 403-A013 TO A MAXIMUM DEPTH OF 1 FOOT. IF ADDITIONAL BACKFILL MATERIAL IS REQUIRED, USE 3/4" AND DOWN CRUSHED STONE BASE, PAY ITEM NO. 304-F001 WITH GEOGRID, TYPE II, BIAXIAL PAY ITEM NO. 204-A003 UNDERNEATH.
- 2 1.5" & VAR. FINE MILLING OF BITUMINOUS PAVEMENT (ALL DEPTHS). PAY ITEM NO. 406-D001.
- 3 1.5" STONE MATRIX ASPHALT, 9.5 mm MIXTURE PAY ITEM NO. 405-A002.
- 4 1.5" STONE MATRIX ASPHALT, 9.5 mm MIXTURE PAY ITEM NO. 405-A002.
- 5 1.5" & VAR. 9.5 mm, ST, ASPHALT PAVEMENT ON PAVED SHOULDERS. PAY ITEM NO. 403-A015.
- 6 GRANULAR MATERIAL (CL. 3 GP. C) REQUIRED ON SHOULDER AS DIRECTED BY THE ENGINEER. PAY ITEM NO. 304-B001.
- 7 GROUND IN RUBBLE STRIPS REQUIRED. PAY ITEM 423-A001.
- 8 1.5" & VAR. 9.5 mm, HT, ASPHALT PAVEMENT ON RAMPS & OVERPASSES. PAY ITEM NO. 403-A013.



TYPICAL SECTION MAIN FACILITY
INTERCHANGE RAMPS (OVERLAY)

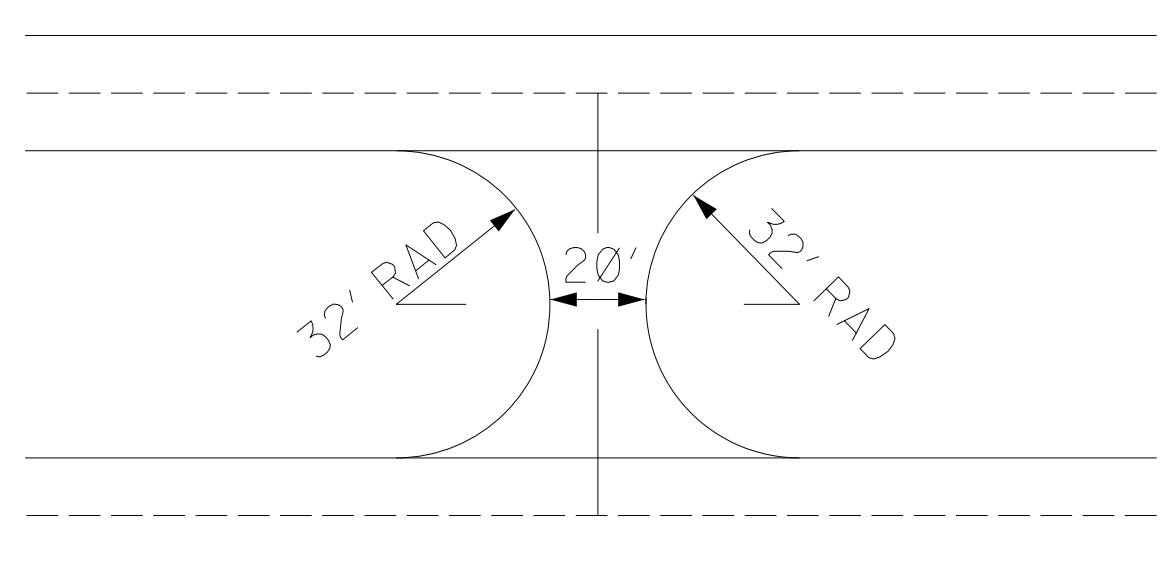
NOTE: RAMP BEGINNING OR ENDING STATION TO BE AT POINT OF NORMAL RAMP WIDTH (16 FT. POINT)

GLENFIELD AND HWY. #30 INTERCHANGE RAMP LIMITS

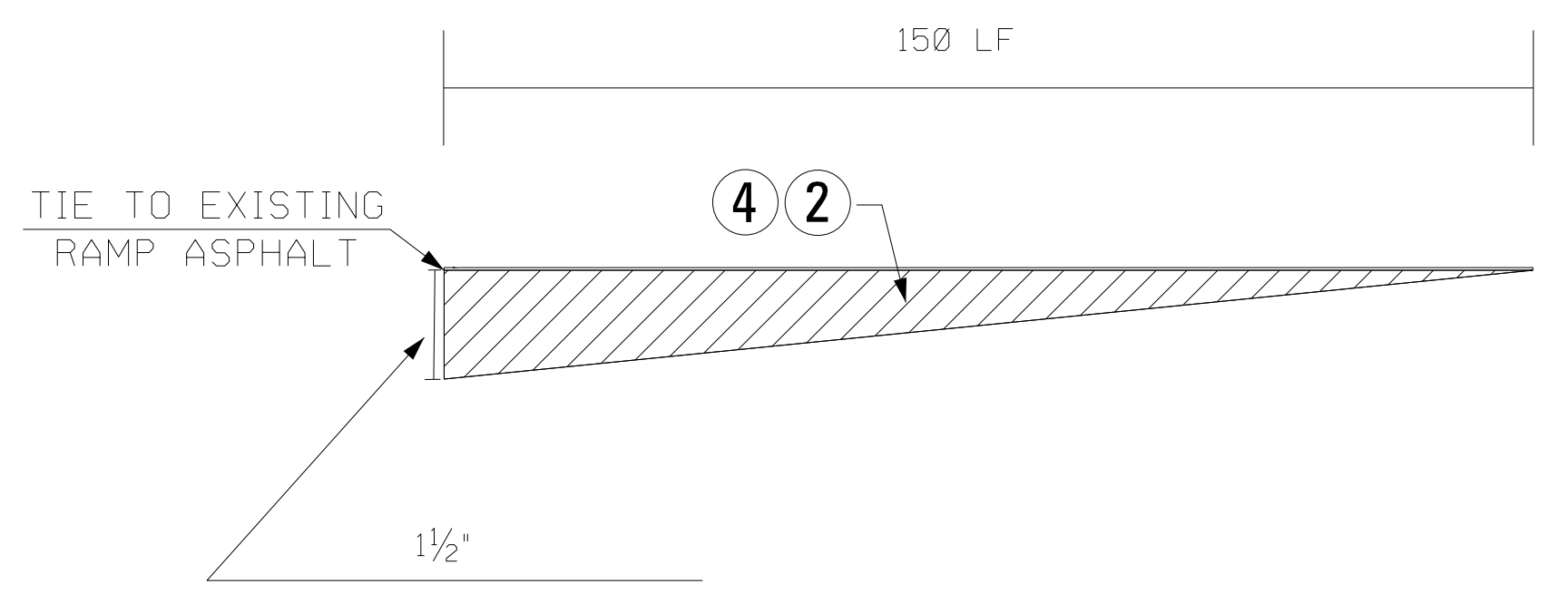
GLENFIELD NW RAMP: 6+94 TO 17+16
GLENFIELD SW RAMP: 10+00 TO 15+25
GLENFIELD NE RAMP: 19+00 TO 24+03
GLENFIELD SE RAMP: 20+10 TO 30+60

HWY. 30 NW RAMP: 10+00 TO 17+30
HWY. 30 SW RAMP: 10+00 TO 15+10

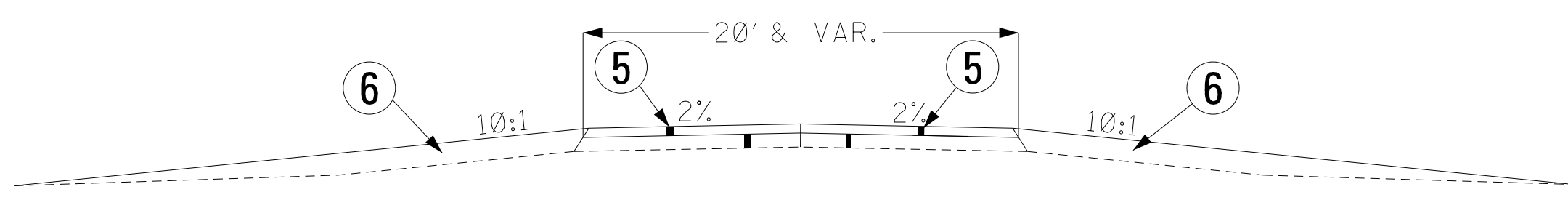
DIG OUT STATIONS	
LEFT SIDE LEFT LANE STA. 950+50 TO STA. 952+50 (14' WIDE)	RIGHT SIDE LEFT LANE STA. 950+50 TO STA. 952+50 (14' WIDE)
LEFT SIDE RIGHT LANE STA. 948+50 TO STA. 950+50 (14' WIDE) STA. 1060+06 TO STA. 1065+06 (14' WIDE)	RIGHT SIDE RIGHT LANE STA. 948+50 TO STA. 950+50 (14' WIDE) STA. 1060+06 TO STA. 1065+06 (14' WIDE)



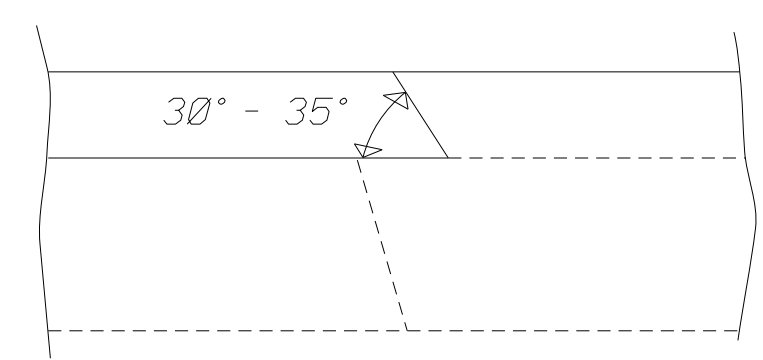
DETAIL OF EMERGENCY CROSSOVER



MILLING TIE-DOWN DETAIL




TYPICAL PAVING SECTION
EMERGENCY CROSSOVERS
STATION 1027+20



SAFETY EDGE REQUIRED
(NOT A PAY ITEM)

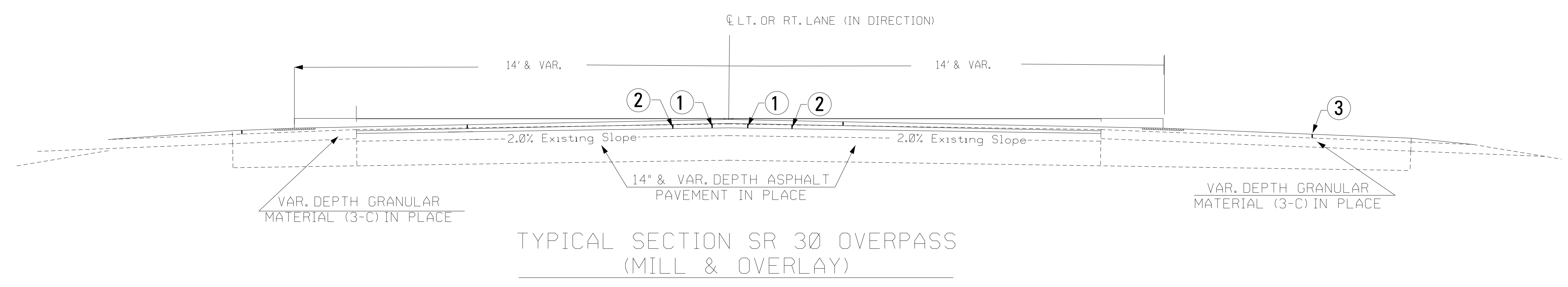
SAFETY EDGE DETAIL

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTIONS MAIN FACILITY, INTERCHANGE RAMPS AND EMERGENCY CROSSOVERS	
	
PROJ. NO.: IM-0006-01(096) COUNTY: UNION	
WORKING NUMBER TS-1	SHEET NUMBER 4
FILE NAME: _____ DESIGN TEAM _____ CHECKED _____ DATE _____	DATE _____

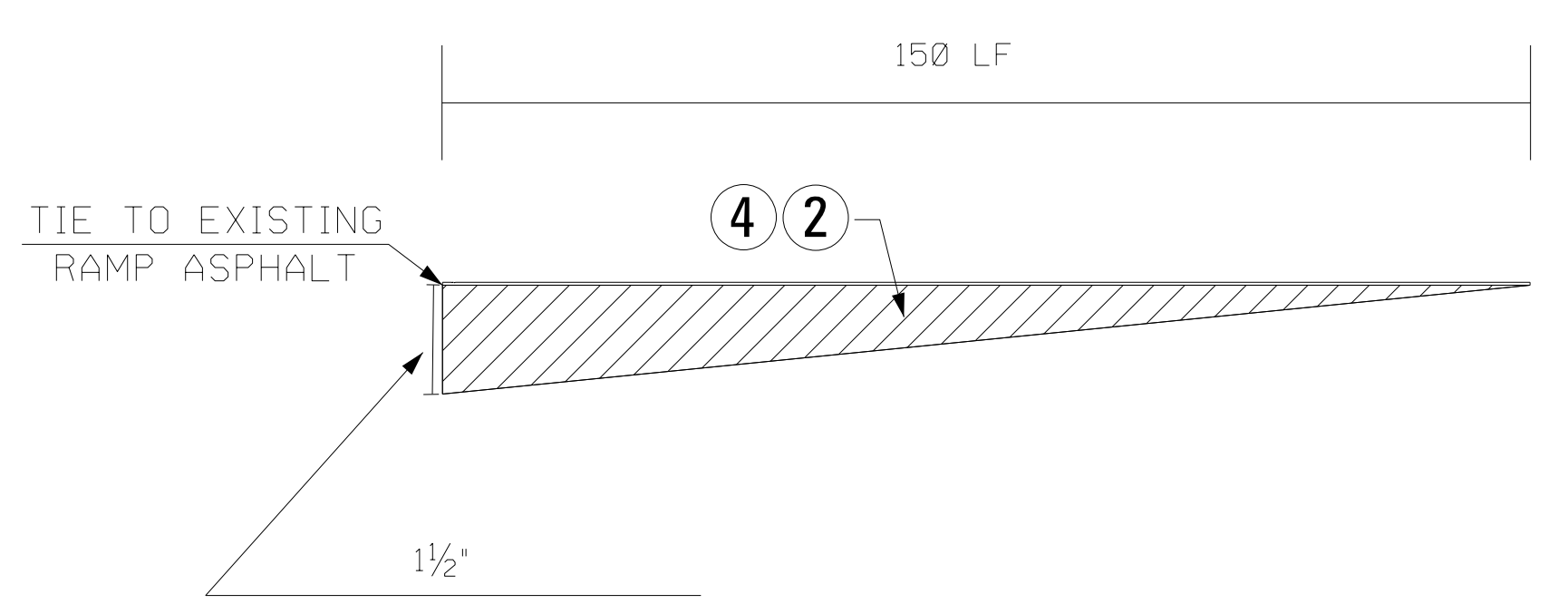
12/5/2019 9:45 AM TS-1

STATE	PROJECT NO.
MISS.	IM-0006-01(096)

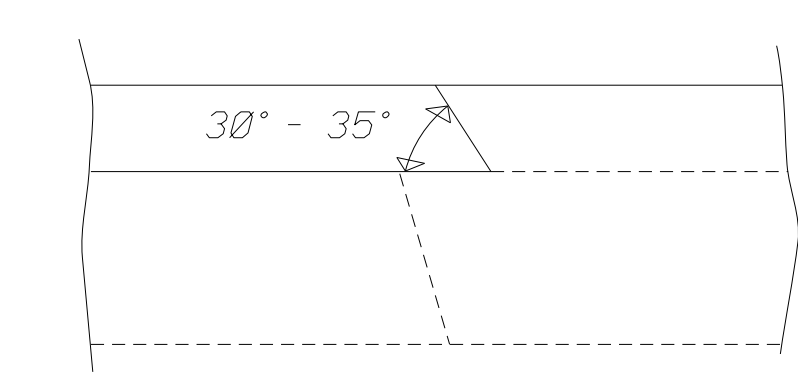
- ① 1.5" & VAR. FINE MILLING OF BITUMINOUS PAVEMENT (ALL DEPTHS). PAY ITEM NO. 406-D001.
- ② 1.5" & VAR. 9.5 mm, HT, ASPHALT PAVEMENT ON PAVED SHOULDERS. PAY ITEM NO. 403-A013.
- ③ GRANULAR MATERIAL (CL. 3 GP. C) REQUIRED ON SHOULDER AS DIRECTED BY THE ENGINEER. PAY ITEM NO. 304-B001.



TYPICAL SECTION SR 30 OVERPASS
(MILL & OVERLAY)



MILLING TIE-DOWN DETAIL



SAFETY EDGE REQUIRED
(NOT A PAY ITEM)

SAFETY EDGE DETAIL

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTION SR 30 OVERPASS	
PROJ. NO.:	IM-0006-01(096)
COUNTY:	UNION
WORKING NUMBER	TS-2
FILE NAME:	_____
DESIGN TEAM	CHECKED _____ DATE _____
DATE	_____
REVISION	_____
BY	_____
SHEET NUMBER 5	




12/5/2019 9:45 AM SR 30 TYPICAL SECTION MISSISSIPPI DEPARTMENT OF TRANSPORTATION

STATE	PROJECT NO.
MISS	IM-0006-01(096)

SUMMARY OF QUANTITIES (SHEET 1)

PAY ITEM NO.	PAY ITEM	UNIT	UNION : 107826-301000	
			Prelim	Final
202-B007	Removal of Asphalt Pavement, All Depths	SY	2,800	
202-B158	Removal of Guard Rail, Including Rails, Posts and Terminal Ends	LF	1,770	
202-B240	Removal of Traffic Stripe	LF	3,456	
203-G001	Excess Excavation, FM, AH	CY	924	
204-A003	Geogrid, Type II, Biaxial	SY	2,800	
209-A005	Geotextile Stabilization, Type V, Non-Woven	SY	2,800	
304-B001	Granular Material, Class 3, Group C	TON	3,146	②
	ALTERNATIVE PAY ITEMS			
304-F001	3/4" and Down Crushed Stone Base	TON	1,202	②
	OR			
304-F002	Size 610 Crushed Stone Base	TON	1,202	②
	OR			
304-F003	Size 825B Crushed Stone Base	TON	1,202	②
403-A013	9.5-mm, HT, Asphalt Pavement	TON	3,628	
403-A015	9.5-mm, ST, Asphalt Pavement	TON	6,115	①
403-S001	Joint Sealant	MI	16	
405-A002	Stone Matrix Asphalt, 9.5 mm Mixture	TON	19,178	
406-D001	Fine Milling of Bituminous Pavement, All Depths	SY	142,017	
407-A001	Asphalt for Tack Coat	GAL	22,914	
423-A001	Rumble Strips, Ground In	MI	15	
	GUARD RAIL			
606-B001	Guard Rail, Class A, Type 1	LF	1,200	
606-D012	Guard Rail, Bridge End Section, Type D Modified	EA	8	
606-D020	Guard Rail, Bridge End Section, Type H, Metal Post	EA	4	
606-E001	Guard Rail, Terminal End Section	EA	12	
630-F006	Delineators, Guard Rail, White	EA	94	
630-F007	Delineators, Guard Rail, Yellow	EA	88	
618-A001	Maintenance of Traffic	LS	1	
618-B001	Additional Construction Signs	SF	1	
620-A001	Mobilization	LS	1	
	TRAFFIC CONTROL			
619-A1001	Temporary Traffic Stripe, Continuous White	MI	28	
619-A2001	Temporary Traffic Stripe, Continuous Yellow	MI	29	
619-A3001	Temporary Traffic Stripe, Skip White	MI	24	
619-A4002	Temporary Traffic Stripe, Skip Yellow	MI	1	
619-A5001	Temporary Traffic Stripe, Detail	LF	17,164	

- ① QUANTITY INCLUDES 1827 TONS FOR DIG OUTS AND 600 TON FOR GUARDRAIL PADS
- ② QUANTITY INCLUDES A 30% SHRINKAGE FACTOR


By		MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
		SUMMARY OF QUANTITIES	
Revision			
Date	Design Team	FILENAME: IM-0006-01(096) Checked	Date 12/04/201 Sheet Number 6

STATE	PROJECT NO.
MISS	IM-0006-01(096)

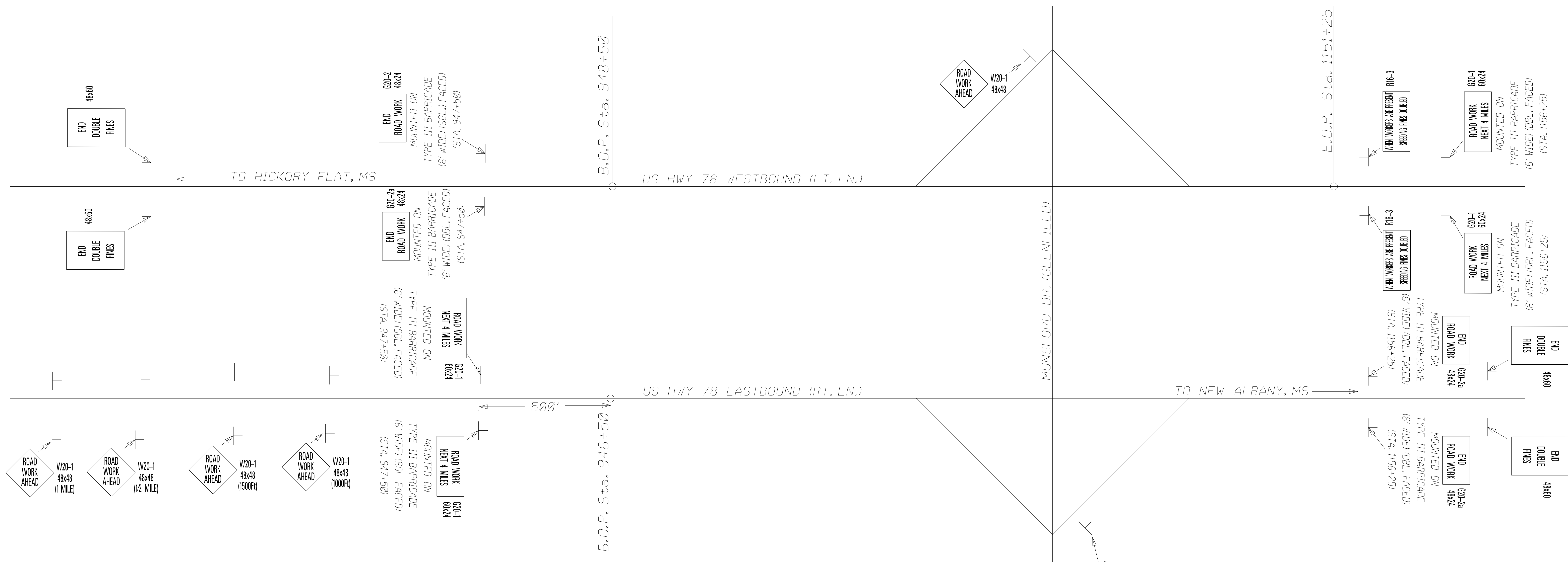
SUMMARY OF QUANTITIES (SHEET 2)

PAY ITEM NO.	PAY ITEM	UNIT	UNION : 107826-301000	
			Prelim	Final
619-A6001	Temporary Traffic Stripe, Legend	SF	607	
619-A6002	Temporary Traffic Stripe, Legend	LF	1,872	
	ALTERNATIVE PAVEMENT MARKING PAY ITEMS			
907-624-A002	6" Inverted Profile Thermoplastic Traffic Stripe, Skip White	LF	520	
907-624-B002	6" Inverted Profile Thermoplastic Traffic Stripe, Continuous White	LF	1,404	
907-624-D002	6" Inverted Profile Thermoplastic Traffic Stripe, Continuous Yellow	LF	2,028	
	OR			
628-G001	6" High Performance Cold Plastic Traffic Stripe, Skip White	LF	520	
628-H001	6" High Performance Cold Plastic Traffic Stripe, Continuous White	LF	1,404	
628-J001	6" High Performance Cold Plastic Traffic Stripe, Continuous Yellow	LF	1,908	
626-A004	6" Thermoplastic Traffic Stripe, Skip White	MI	8	
626-C004	6" Thermoplastic Edge Stripe, Continuous White	MI	11	
626-D004	6" Thermoplastic Traffic Stripe, Skip Yellow	LF	4,160	
626-F003	6" Thermoplastic Edge Stripe, Continuous Yellow	MI	11	
626-G002	Thermoplastic Detail Stripe, White	LF	13,357	
626-G003	Thermoplastic Detail Stripe, Yellow	LF	3,807	
626-H004	Thermoplastic Legend, White	SF	607	
626-H005	Thermoplastic Legend, White	LF	1,872	
627-J001	Two-Way Clear Reflective High Performance Raised Markers	EA	90	
627-K001	Red-Clear Reflective High Performance Raised Markers	EA	1,500	
627-L001	Two-Way Yellow Reflective High Performance Raised Markers	EA	752	
	TRAFFIC SIGNAL			
907-632-C001	Modify Existing Traffic Signal Cabinet Assembly	EA	2	②
907-641-A002	Signal Stop Bar Radar Vehicle Detection Sensor, Type 2	EA	10	②
907-641-D001	Radar Vehicle Detection Cable	LF	2,100	②
907-687-A001	Traffic Recorder Classification Permanent System	EA	1	①

- ① LUMP SUM PAY ITEM, INCLUDES ALL NEW EQUIPMENT ABOVE AND BELOW GROUND AND LABOR NECESSARY TO INSTALL
INSTALL AT STA 1080+00
- ② ALL TRAFFIC SIGNAL WORK SHALL BE PERFORMED BEFORE ANY WORK IS PERFORMED TO DAMAGE EXISTING TRAFFIC DETECTORS

By	MISSISSIPPI DEPARTMENT OF TRANSPORTATION		
	SUMMARY OF QUANTITIES		
Revision	PROJ NO: IM-0006-01(096)		 Working Number SQ-2
	COUNTY: UNION		
Date	FILENAME: IM-0006-01(096)	Checked	Sheet Number 7
	Design Team	Date 12/04/201	

STATE	PROJECT NO.
MISS.	IM-0006-01(096)



GENERAL NOTES

ALL W20-1 (ROAD WORK AHEAD) SIGNS TO BE PLACED APPROXIMATELY 500' FROM CONSTRUCTION OR AS DIRECTED BY THE ENGINEER.

LOCATION OF SIGNS SHOWN ON THIS SHEET ARE APPROXIMATE AND MAY BE ADJUSTED TO FIT FIELD CONDITIONS.

ALL SIGNS, BARRICADES, AND CHANNELIZING DEVICES TO BE PAID FOR UNDER PAY ITEM 618-A001, MAINTENANCE OF TRAFFIC.

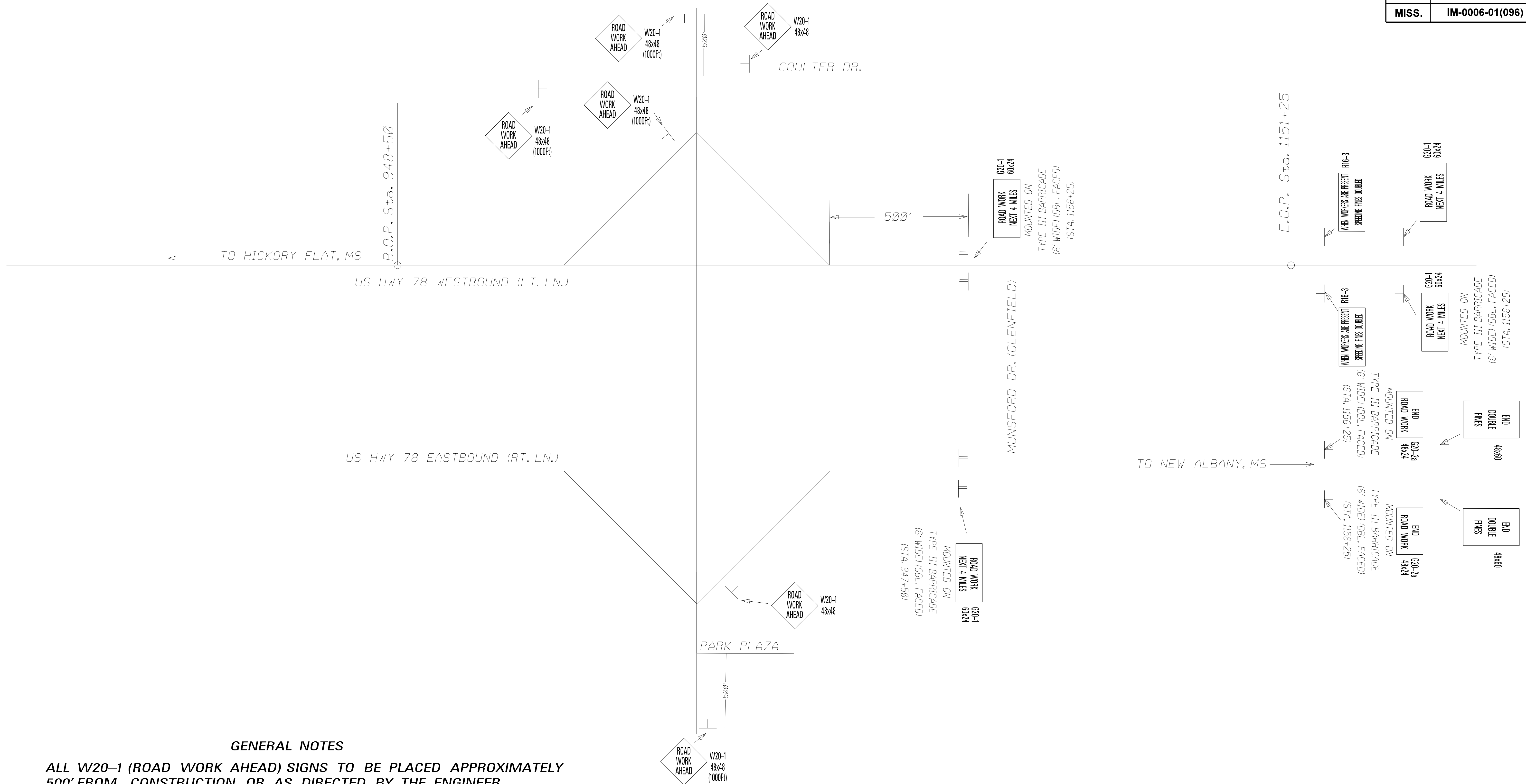
12 W20-1 (ROAD WORK AHEAD) SIGNS REQUIRED ON CITY STREETS OR COUNTY ROADS AS DESIGNATED BY THE ENGINEER.

12/5/2019 9:45 AM DCS-1

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TRAFFIC CONTROL PLAN DETAIL OF CONSTRUCTION SIGNING	
PROJ. NO.: IM-0006-01(096) COUNTY: UNION	
DATE: _____ DESIGN TEAM: _____	FILE NAME: DCS-1.dgn CHECKED: _____ DATE: _____
REVISION: _____ BY: _____	WORKING NUMBER: DCS-1 SHEET NUMBER: 8



STATE	PROJECT NO.
MISS.	IM-0006-01(096)



GENERAL NOTES

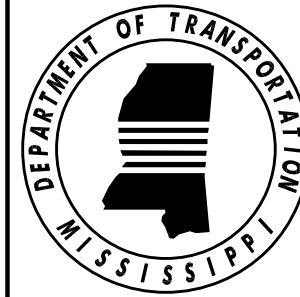
ALL W20-1 (ROAD WORK AHEAD) SIGNS TO BE PLACED APPROXIMATELY 500' FROM CONSTRUCTION OR AS DIRECTED BY THE ENGINEER.

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ALL SIGNS, BARRICADES, AND CHANNELIZING DEVICES TO BE PAID FOR UNDER PAY ITEM 618-A001, MAINTENANCE OF TRAFFIC.

12 W20-1 (ROAD WORK AHEAD) SIGNS REQUIRED ON CITY STREETS OR COUNTY ROADS AS DESIGNATED BY THE ENGINEER.

12/5/2019 9:45 AM DCS-2

MISSISSIPPI DEPARTMENT OF TRANSPORTATION																					
TRAFFIC CONTROL PLAN DETAIL OF CONSTRUCTION SIGNING																					
PROJ. NO.: IM-0006-01(096) COUNTY: UNION																					
<table border="1"> <tr><th>NO.</th><th>DATE</th><th>REVISION</th><th>BY</th></tr> <tr><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td></tr> </table>	NO.	DATE	REVISION	BY													<table border="1"> <tr><td>WORKING NUMBER</td><td>DCS-2</td></tr> <tr><td>SHEET NUMBER</td><td>9</td></tr> </table>	WORKING NUMBER	DCS-2	SHEET NUMBER	9
NO.	DATE	REVISION	BY																		
WORKING NUMBER	DCS-2																				
SHEET NUMBER	9																				
FILE NAME: DCS-1.dgn DESIGN TEAM: UPDATE CHECKED: UPDATE DATE: UPDATE																					

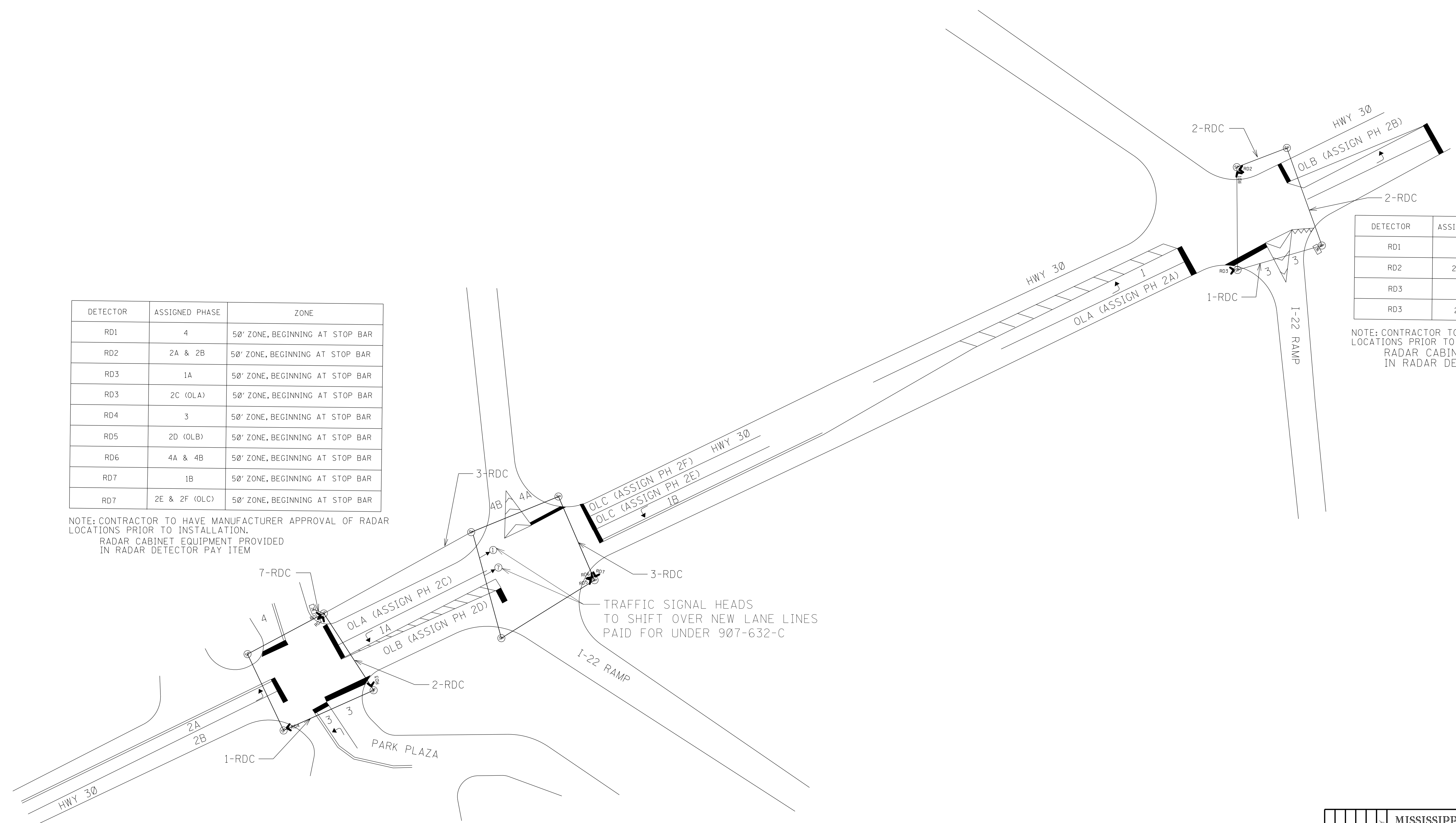
STATE	PROJECT NO.
MISS.	IM-0006-01(096)

DETECTOR	ASSIGNED PHASE	ZONE
RD1	4	50' ZONE, BEGINNING AT STOP BAR
RD2	2A & 2B	50' ZONE, BEGINNING AT STOP BAR
RD3	1A	50' ZONE, BEGINNING AT STOP BAR
RD3	2C (OLA)	50' ZONE, BEGINNING AT STOP BAR
RD4	3	50' ZONE, BEGINNING AT STOP BAR
RD5	2D (OLB)	50' ZONE, BEGINNING AT STOP BAR
RD6	4A & 4B	50' ZONE, BEGINNING AT STOP BAR
RD7	1B	50' ZONE, BEGINNING AT STOP BAR
RD7	2E & 2F (OLC)	50' ZONE, BEGINNING AT STOP BAR

NOTE: CONTRACTOR TO HAVE MANUFACTURER APPROVAL OF RADAR LOCATIONS PRIOR TO INSTALLATION.
 RADAR CABINET EQUIPMENT PROVIDED IN RADAR DETECTOR PAY ITEM

DETECTOR	ASSIGNED PHASE	ZONE
RD1	3	50' ZONE, BEGINNING AT STOP BAR
RD2	2B (OLB)	50' ZONE, BEGINNING AT STOP BAR
RD3	1	50' ZONE, BEGINNING AT STOP BAR
RD3	2A (OLA)	50' ZONE, BEGINNING AT STOP BAR

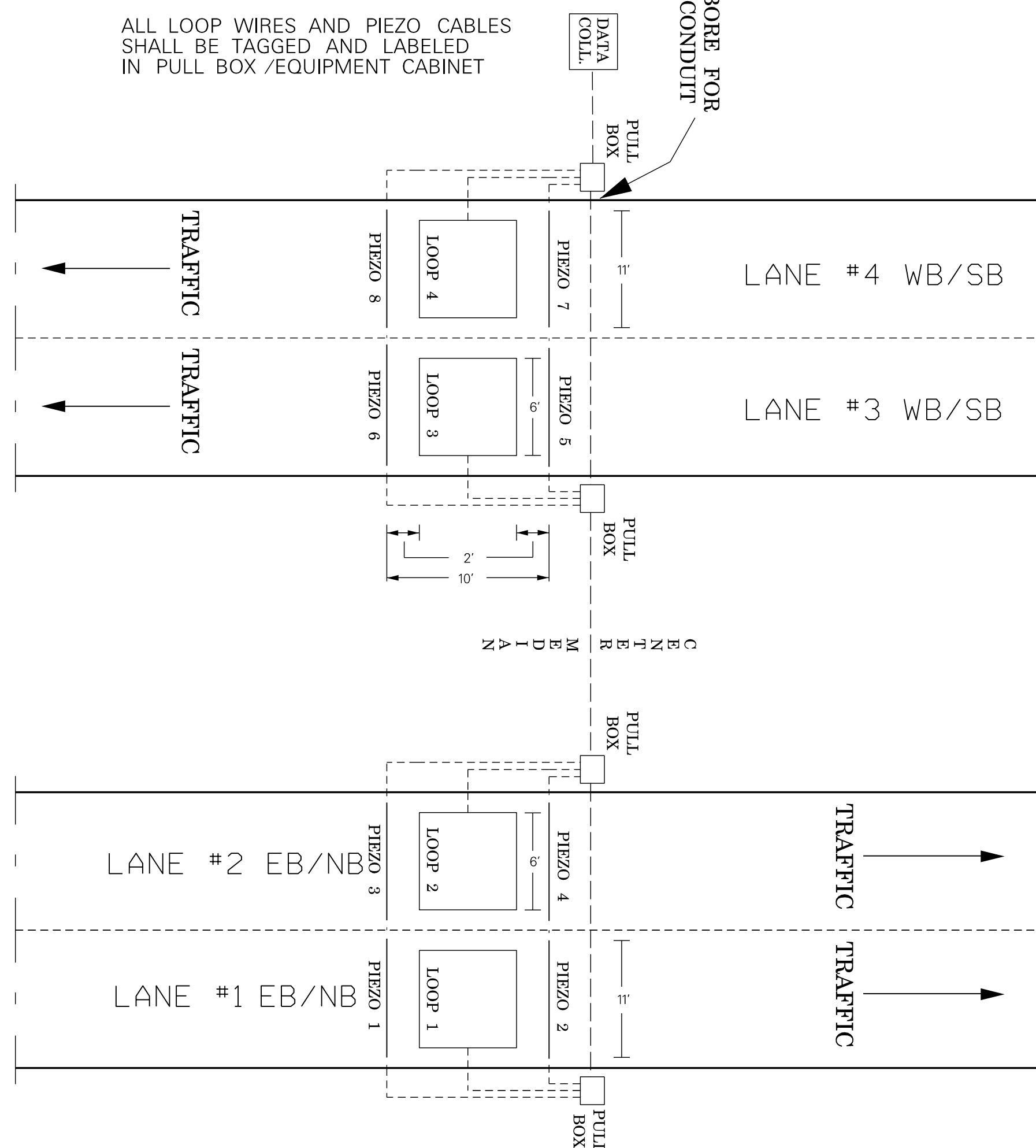
NOTE: CONTRACTOR TO HAVE MANUFACTURER APPROVAL OF RADAR LOCATIONS PRIOR TO INSTALLATION.
 RADAR CABINET EQUIPMENT PROVIDED IN RADAR DETECTOR PAY ITEM



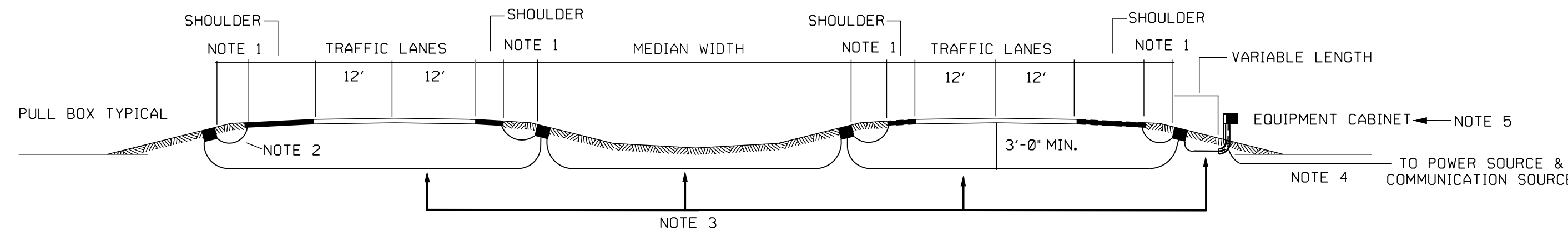
1.2/5/2019 9:45 AM SR 30 M & O MISSISSIPPI DEPARTMENT OF TRANSPORTATION

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TRAFFIC SIGNAL DETAILS	
PROJ. NO.: IM-0006-01(096) COUNTY: UNION	
FILE NAME: 30 M & O RADAR DESIGN TEAM UPDATE CHECKED DATE UPDATE	WORKING NUMBER 30 M&O SHEET NUMBER 10

STATE	PROJECT NO.
MISS.	IM-006-01(096)

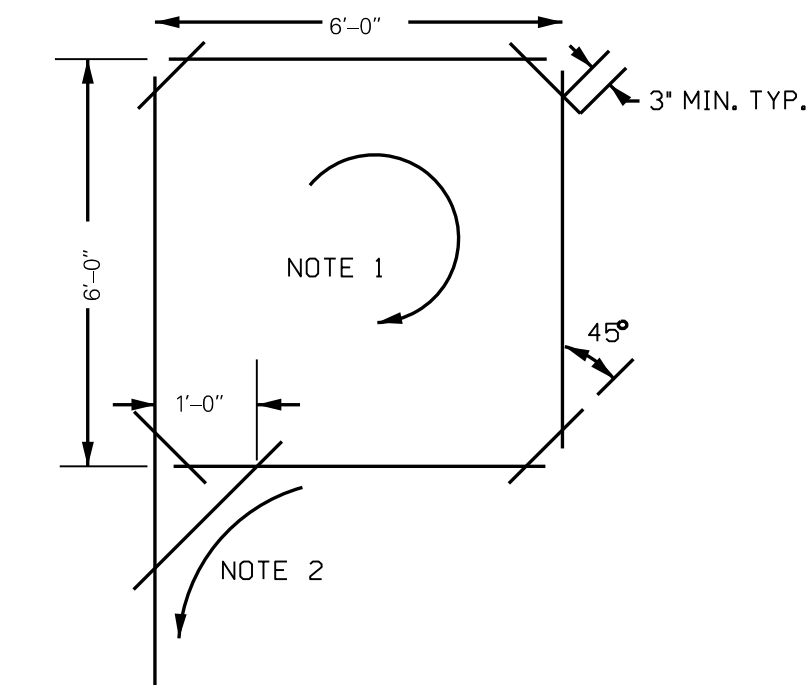


SCHEMATIC 4 LANE DIVIDED ROADWAY LAYOUT PLAN
NOT TO SCALE



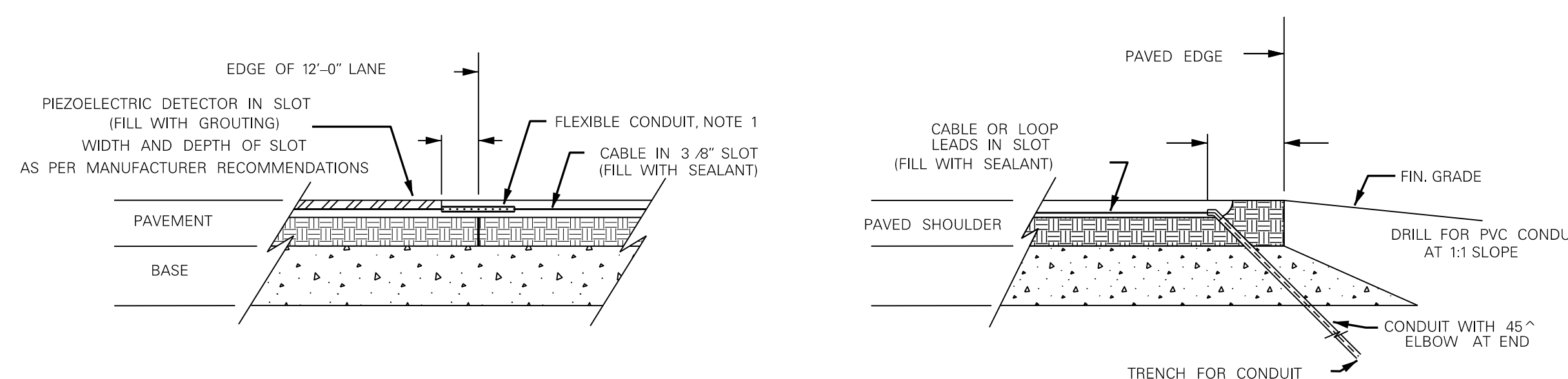
TYPICAL SECTION FOR CONDUIT PLACEMENT
NOT TO SCALE

1. LOCATE PULL BOXES 15'-0" MIN. FROM SHOULDER UNLESS UNUSUAL SITE CONDITIONS EXIST. THE PULL BOX LOCATION & EQUIPMENT CABINET LOCATION MUST BE APPROVED BY PROJECT ENGINEER.
2. 3/4" DIA. SCH 40 PVC CONDUIT FOR EACH LOOP OR SENSOR TO PULL BOX CONDUIT BURIED AT 3'-0" AT THE SHOULDER.
3. 2" DIA. SCH 80 PVC.
4. BURIED CABLE TO POWER SOURCE AND COMMUNICATION SOURCE.
5. LOCATION OF EQUIPMENT CABINET AND LANE WIDTH WILL VARY DEPENDING ON SITE.
6. ALL SENSOR WIRE AND LOOP WIRE MUST BE PLACED IN CONDUIT FROM PAVEMENT TO EQUIPMENT CABINET. SENSOR WIRE SHALL BE A CONTINUOUS RUN WITH OUT ANY SPLICING FROM THE SENSOR TO THE EQUIPMENT CABINET..



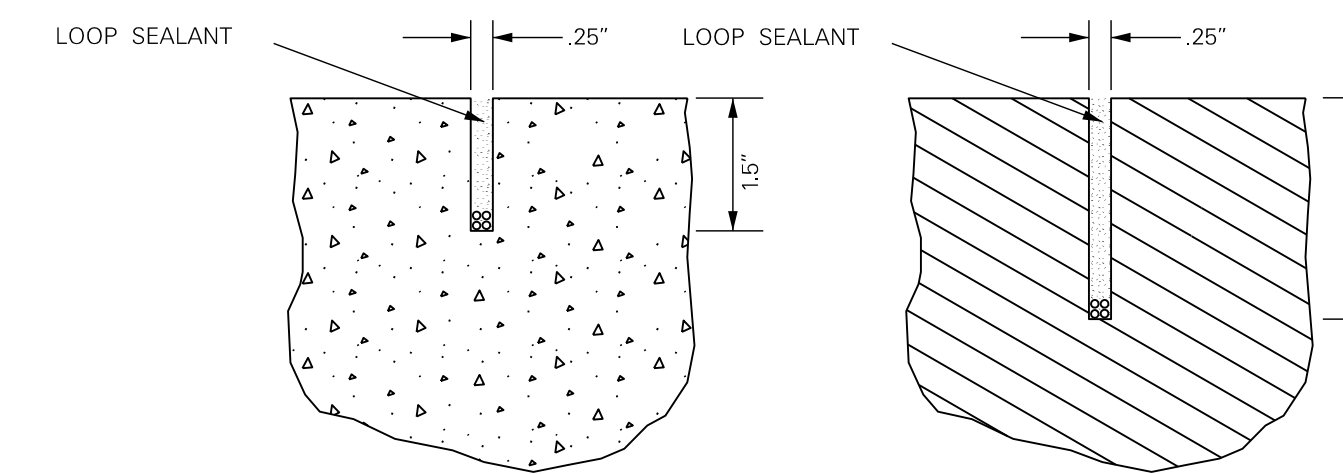
DETAIL PLAN OF LOOP SLOTS
NOT TO SCALE

1. INSTALL WIRE TURNS THE SAME (CLOCKWISE) DIRECTION.
2. LOOP LEAD WIRE EXIT FROM LAST TURN
3. ALL LOOPS TO INCLUDE 4 TURNS OF #14AWG WIRE.
4. DIMENSIONAL TOLERANCE FOR LOOP LENGTH, SPACING AND DETECTOR LOCATION: 1/2".



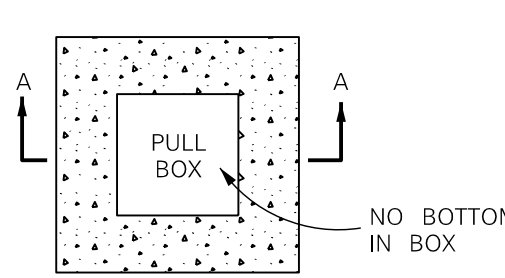
DETAIL AT PIEZOELECTRIC DETECTOR
NOT TO SCALE

DETAIL AT CONDUIT END
NOT TO SCALE

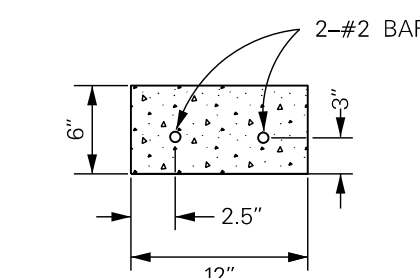


DETAIL OF CABLE SLOT
NOT TO SCALE

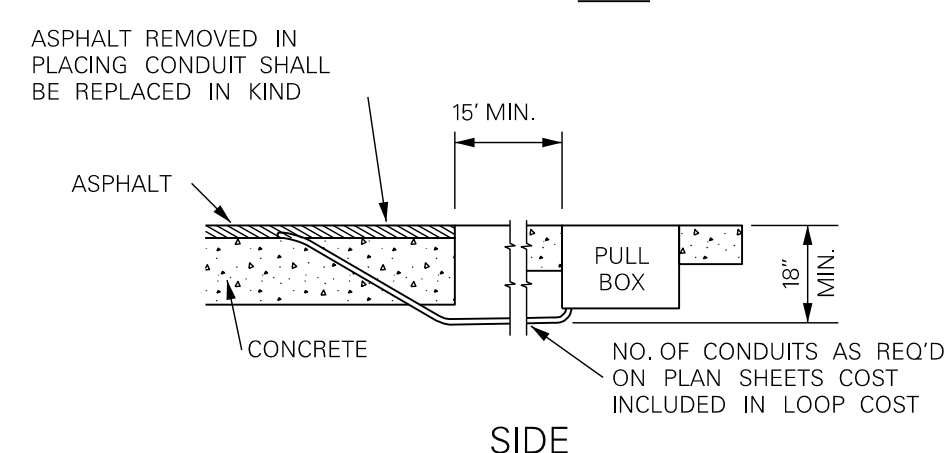
1. PROVIDE 3/4" DIA. X 1'-0" FLEXIBLE PVC CONDUIT SLEEVE AT DETECTOR ENDS. PROVIDE SIMILAR DETAIL FOR DETECTOR CABLE AND LOOP LEADS AT PAVEMENT JOINTS BETWEEN LANES AND JOINTS BETWEEN PAVEMENT AND SHOULDER.



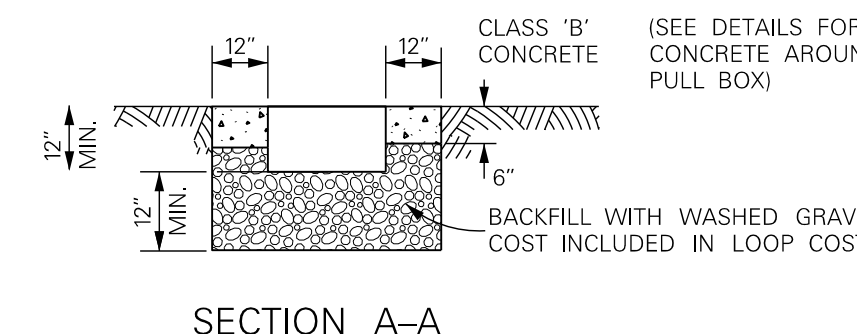
TOP



DETAIL OF CONCRETE AROUND PULL BOX
NOT TO SCALE



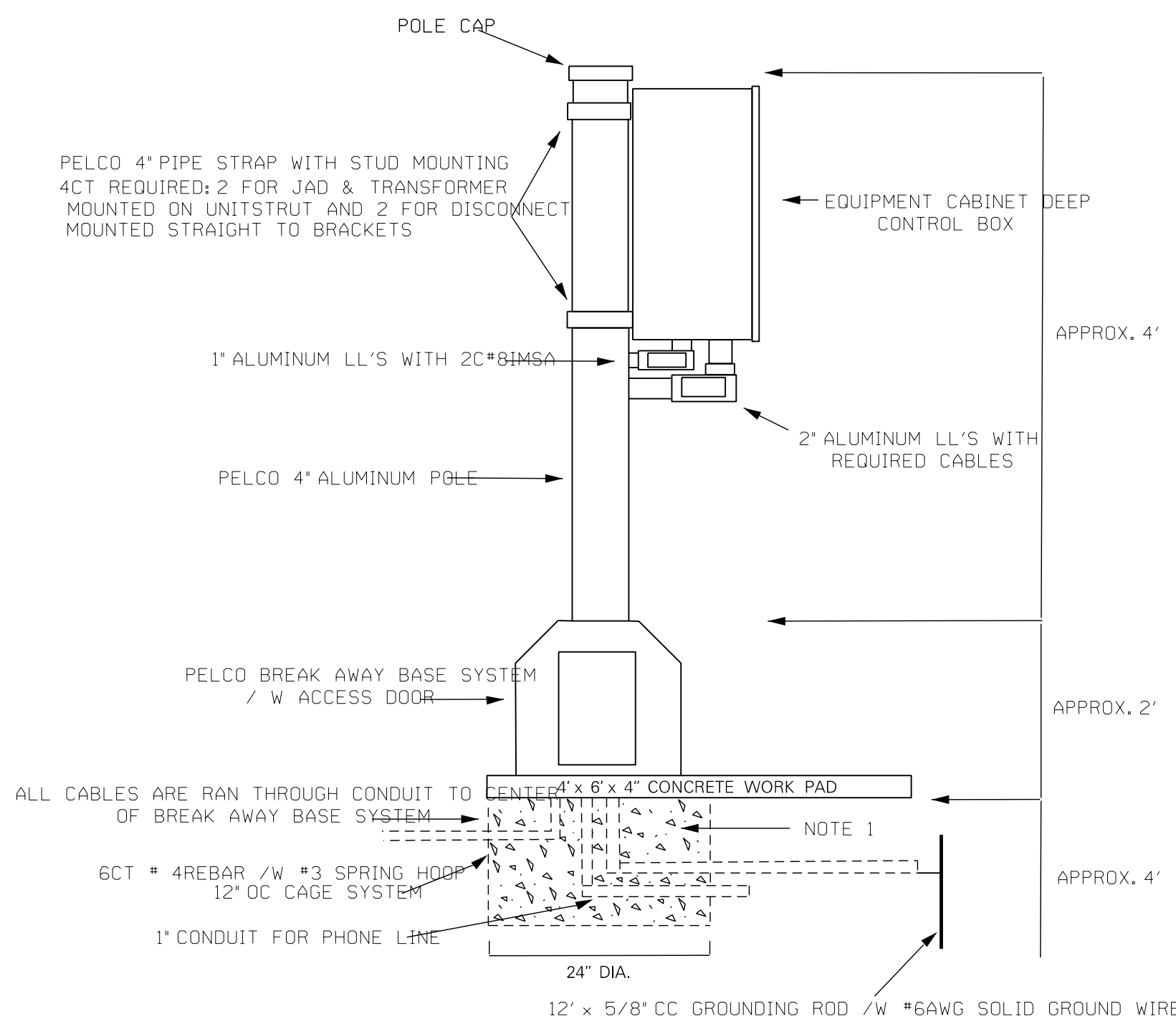
SIDE



SECTION A-A


PULL BOX DETAILS
NOT TO SCALE

PULL BOXES SHALL CONFORM TO 722.06 (e) TYPE 2 OF THE STANDARD SPECIFICATIONS



DETAIL OF POST & EQUIPMENT CABINET
NOT TO SCALE

1. BACKFILL WITH CONCRETE AS APPROVED BY PROJECT ENGINEER.

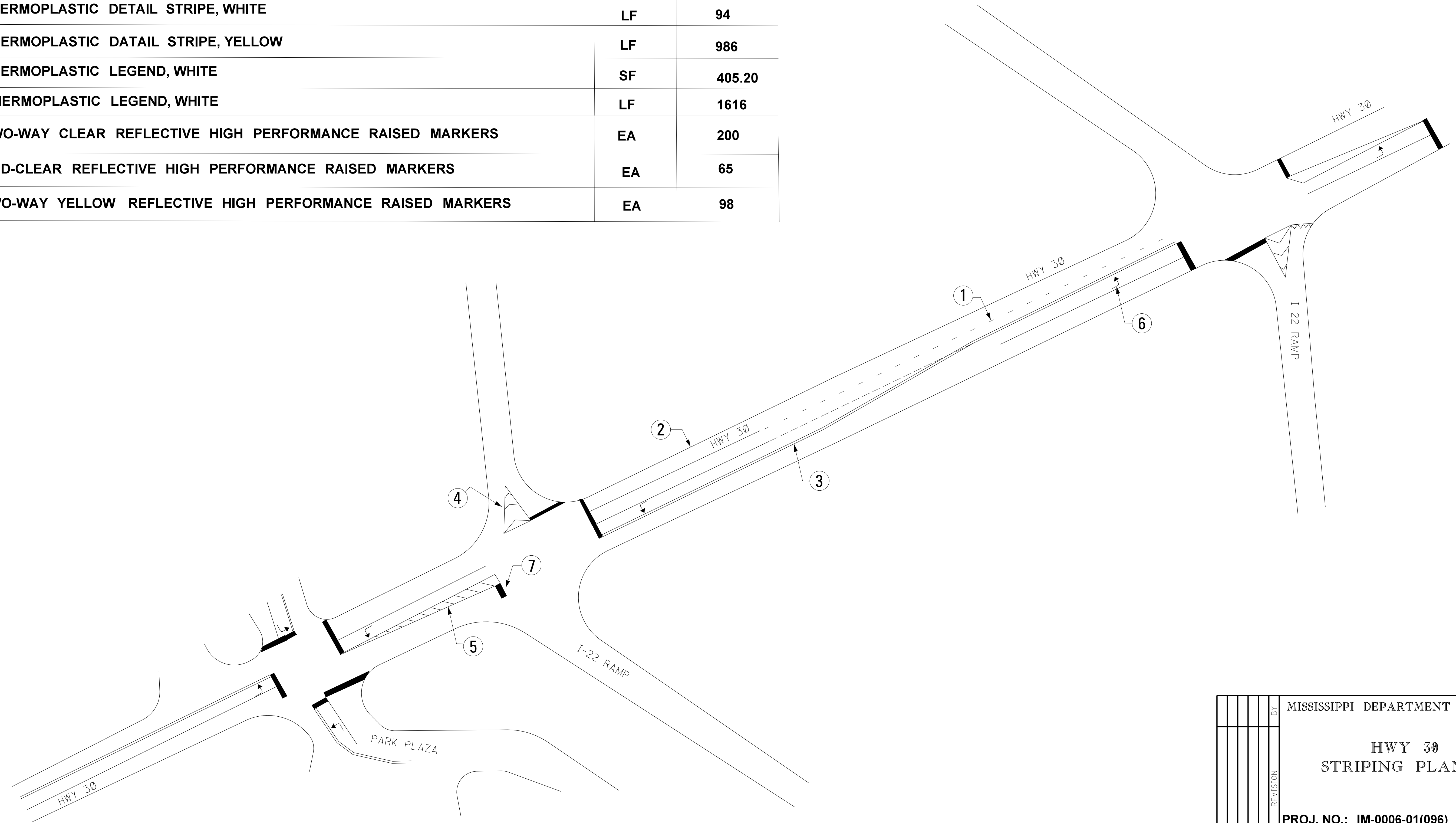
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TRAFFIC RECORDER CLASSIFICATION PERMANENT SYSTEM	
4 LANE DIVIDED ROADWAY LAYOUT PLAN	
WORKING NUMBER SPEC-TC	
SHEET NUMBER 11	
DATE	FILENAME: _____ .dgn
DESIGN TEAM	CHECKED _____ DATE _____

12/18/2019 10:23:51 ARRAFFIC RECORDER CLASSIFICATION PERMANENT SYSTEM-4 LN STANDARD WITH BREAKAWAY BASE SYSTEM DEPARTMENT OF TRANSPORTATION

STATE	PROJECT NO.
MISS.	IM-0006-01(096)

PAVEMENT MARKING

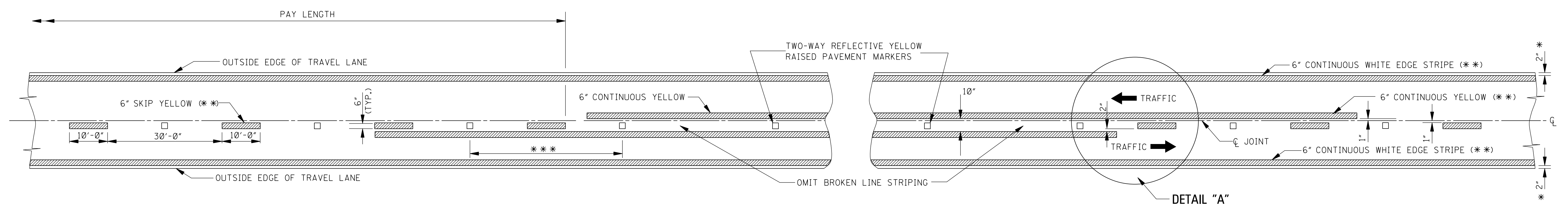
SYMBOL	DESCRIPTION	UNIT	QUANTITY
①	6" THERMOPLASTIC TRAFFIC STRIPE, SKIP WHITE	MI	0.045
②	6" THERMOPLASTIC EDGE STRIPE, CONTINUOUS WHITE	MI	0.362
③	6" THERMOPLASTIC EDGE STRIPE, CONTINUOUS YELLOW	MI	0.291
④	THERMOPLASTIC DETAIL STRIPE, WHITE	LF	94
⑤	THERMOPLASTIC DATAIL STRIPE, YELLOW	LF	986
⑥	THERMOPLASTIC LEGEND, WHITE	SF	405.20
⑦	THERMOPLASTIC LEGEND, WHITE	LF	1616
⑧	TWO-WAY CLEAR REFLECTIVE HIGH PERFORMANCE RAISED MARKERS	EA	200
⑨	RED-CLEAR REFLECTIVE HIGH PERFORMANCE RAISED MARKERS	EA	65
⑩	TWO-WAY YELLOW REFLECTIVE HIGH PERFORMANCE RAISED MARKERS	EA	98



12/5/2019 9:45 AM SR 30 STRIPING PLAN

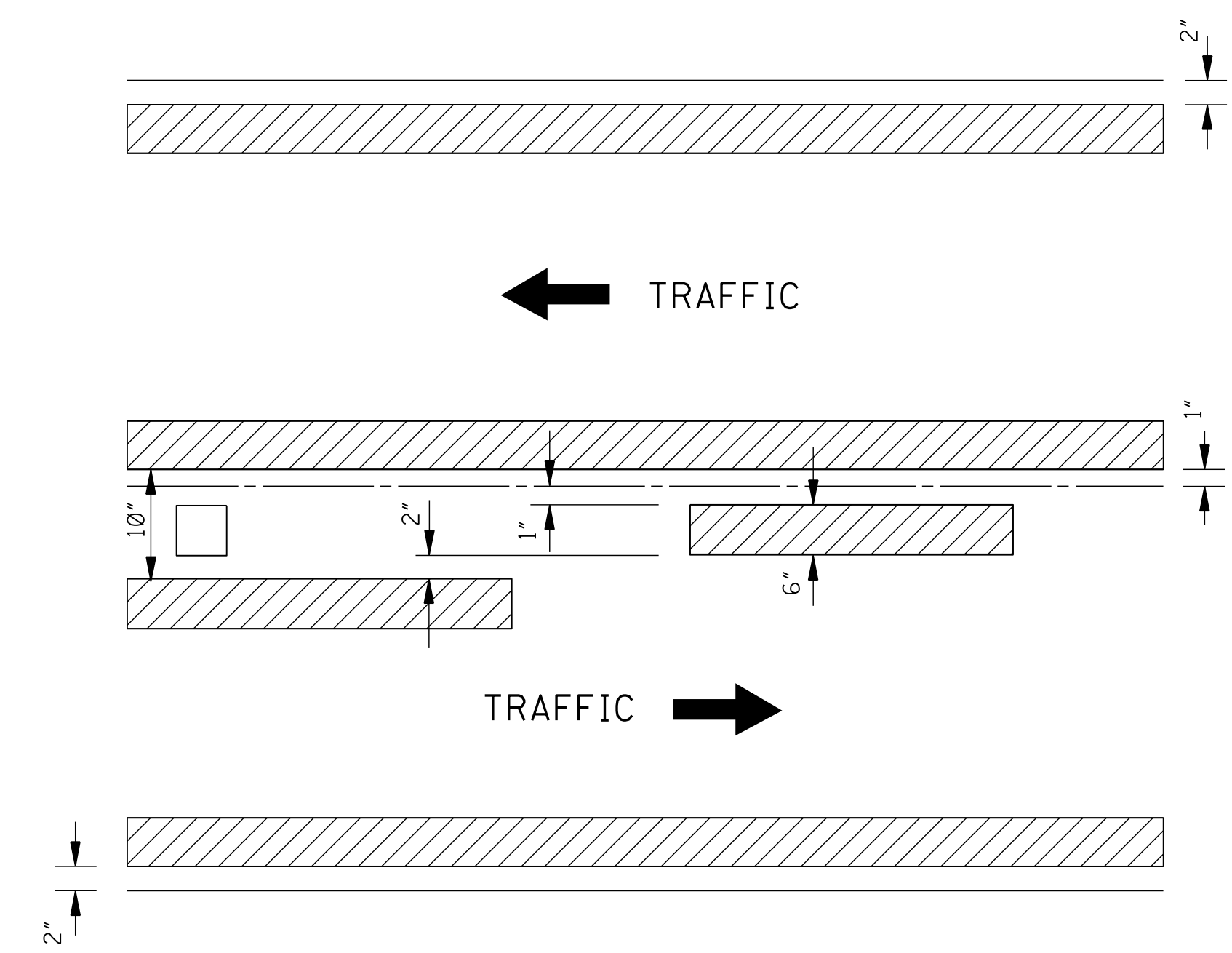
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
HWY 30 STRIPING PLAN	
PROJ. NO.: IM-0006-01(096) COUNTY: UNION	
WORKING NUMBER 30 STRI	SHEET NUMBER 12
FILE NAME: _____ DESIGN TEAM _____ CHECKED _____ DATE _____	DATE _____





TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)

NOTE: THE CRITERIA FOR NO-PASSING ZONES CAN BE FOUND IN THE MDT ROADWAY DESIGN MANUAL, SECTION 11-1.01.



DETAIL "A"

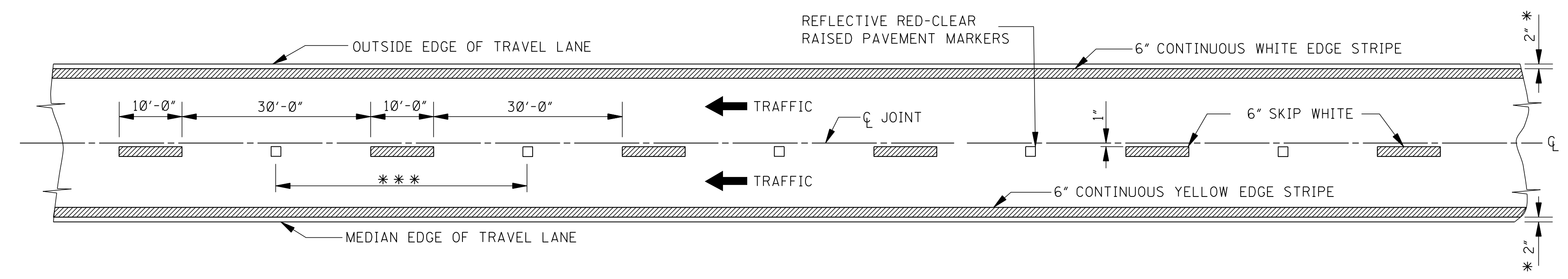
GENERAL NOTES:

- * 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS. FOR STRIPING ON RUMBLE STRIP SECTIONS REFER TO WK. SHEETS RS-1, RS-2, AND RS-3.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
- *** 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

	URBAN AREA (ft-in)	RURAL AREA (ft-in)
TANGENT SECTIONS	40'-0"	80'-0"
HORIZONTAL CURVES	40'-0"	40'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

† NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDT "APPROVED SOURCES OF MATERIALS."

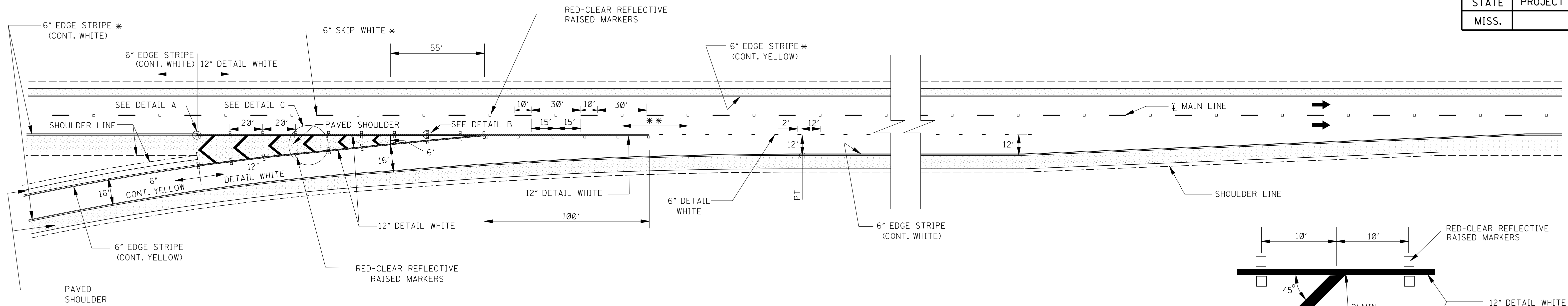


4-LANE WITH ONE-WAY TRAFFIC

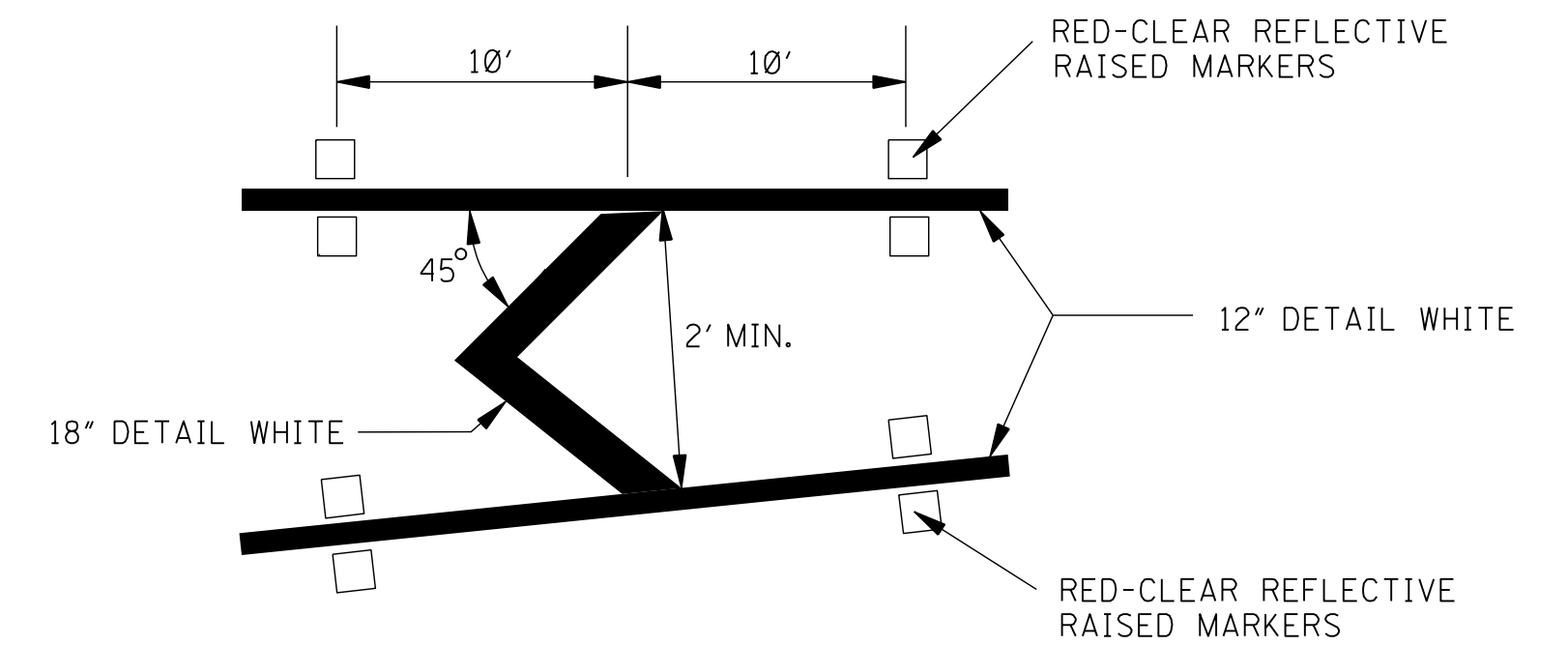
BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
REVISION	PAVEMENT MARKING DETAILS FOR 2-LANE AND 4-LANE DIVIDED ROADWAYS
DATE	ISSUE DATE: AUGUST 01, 2017



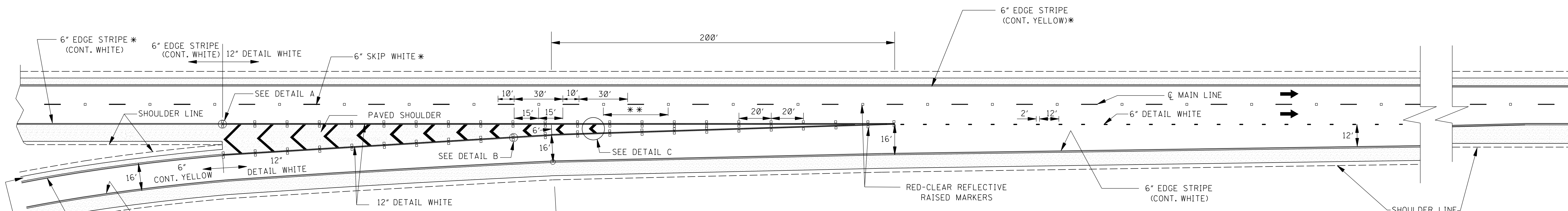
WORKING NUMBER
PM-1
SHEET NUMBER
6051



PARALLEL ENTRANCE RAMP

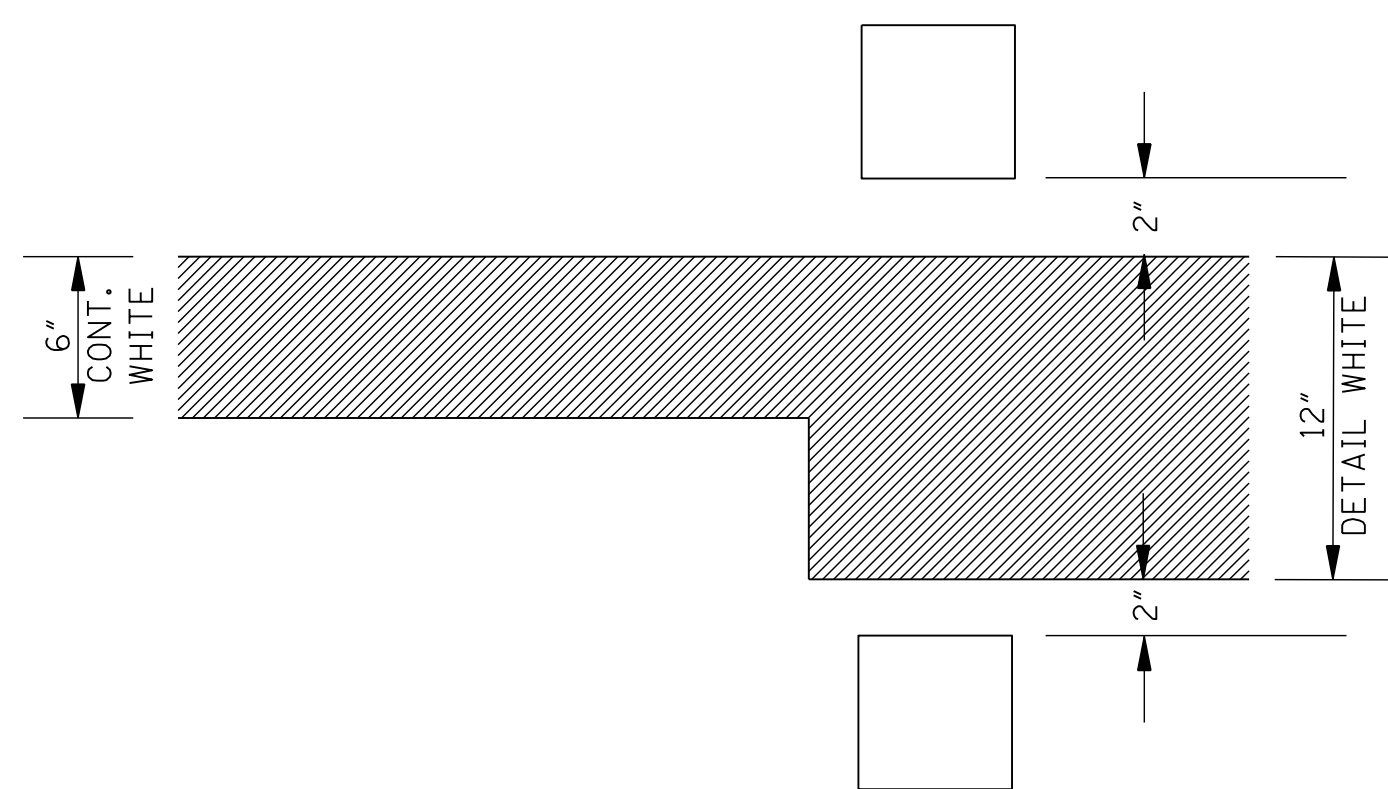


DETAIL C

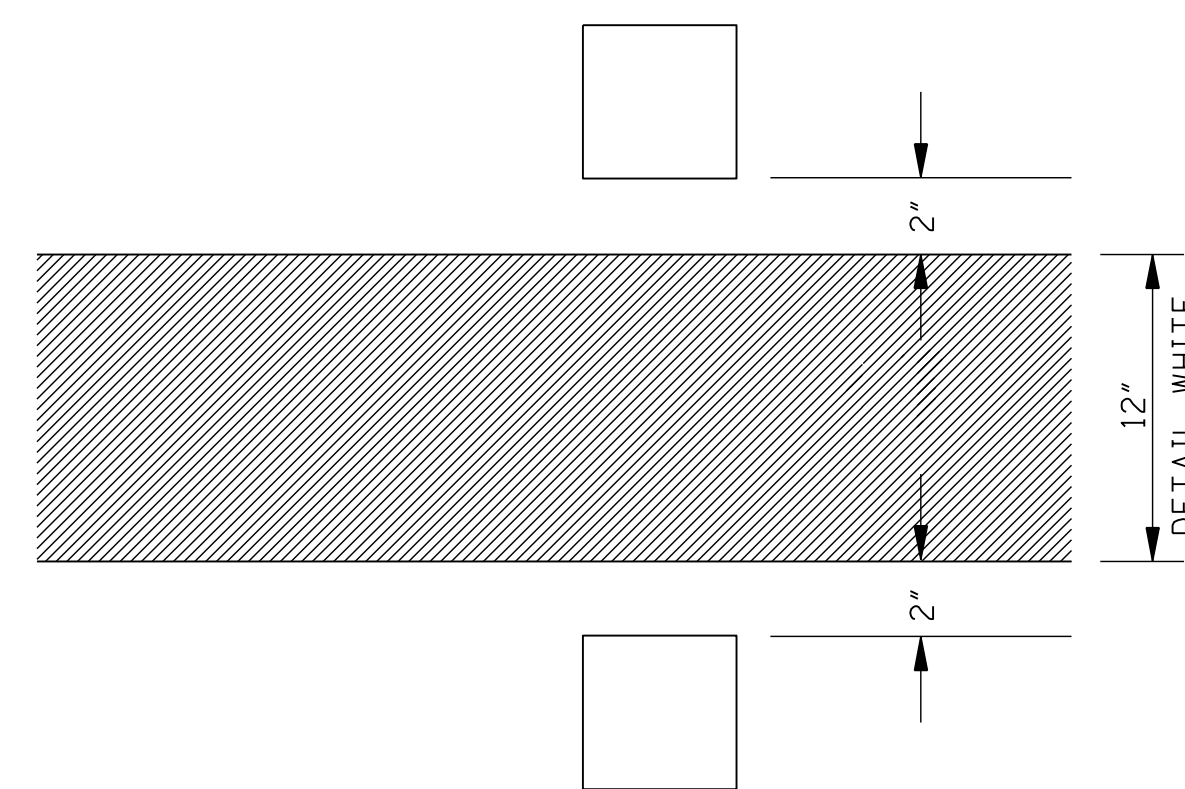


TAPER ENTRANCE RAMP

NOTE: 2'-12' SKIP WHITE EXTENDS TO THE TERMINATION POINT OF THE ACCELERATION LANE.



DETAIL A



DETAIL B

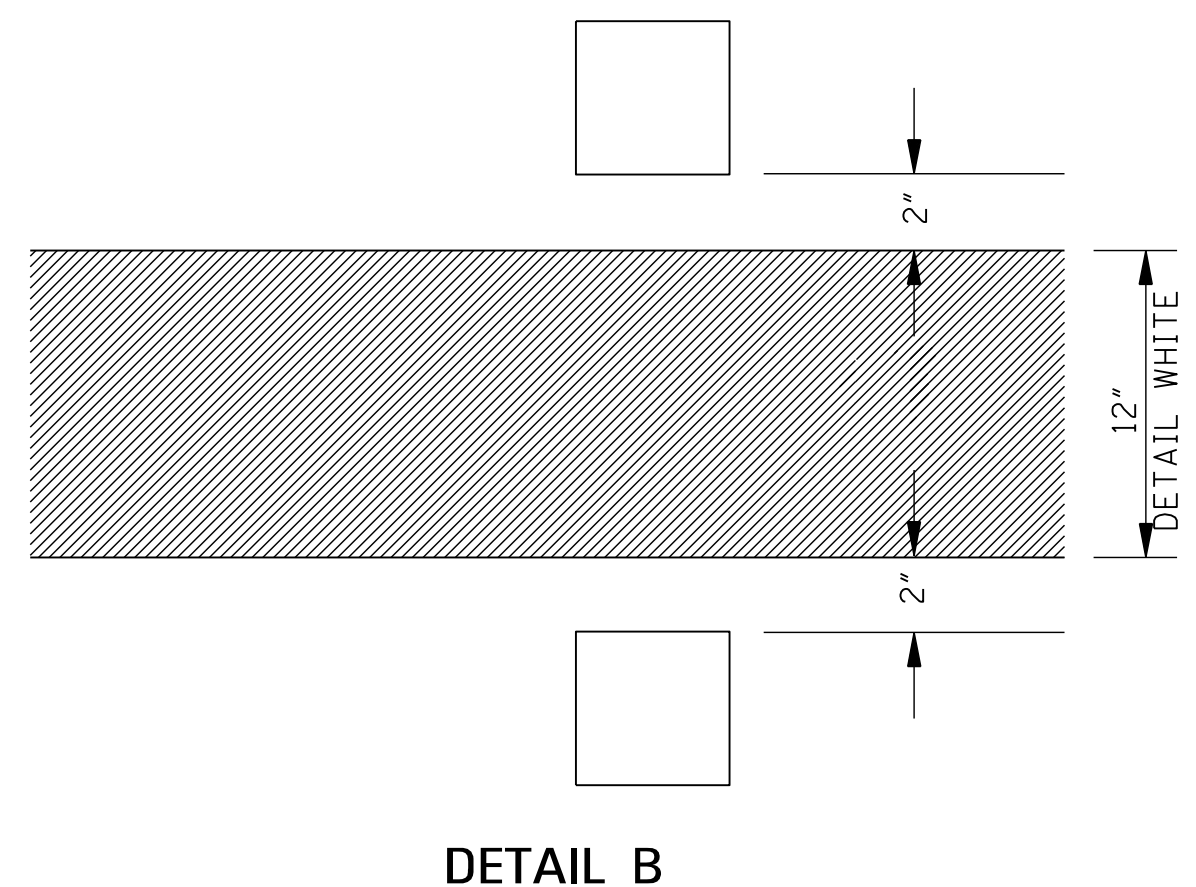
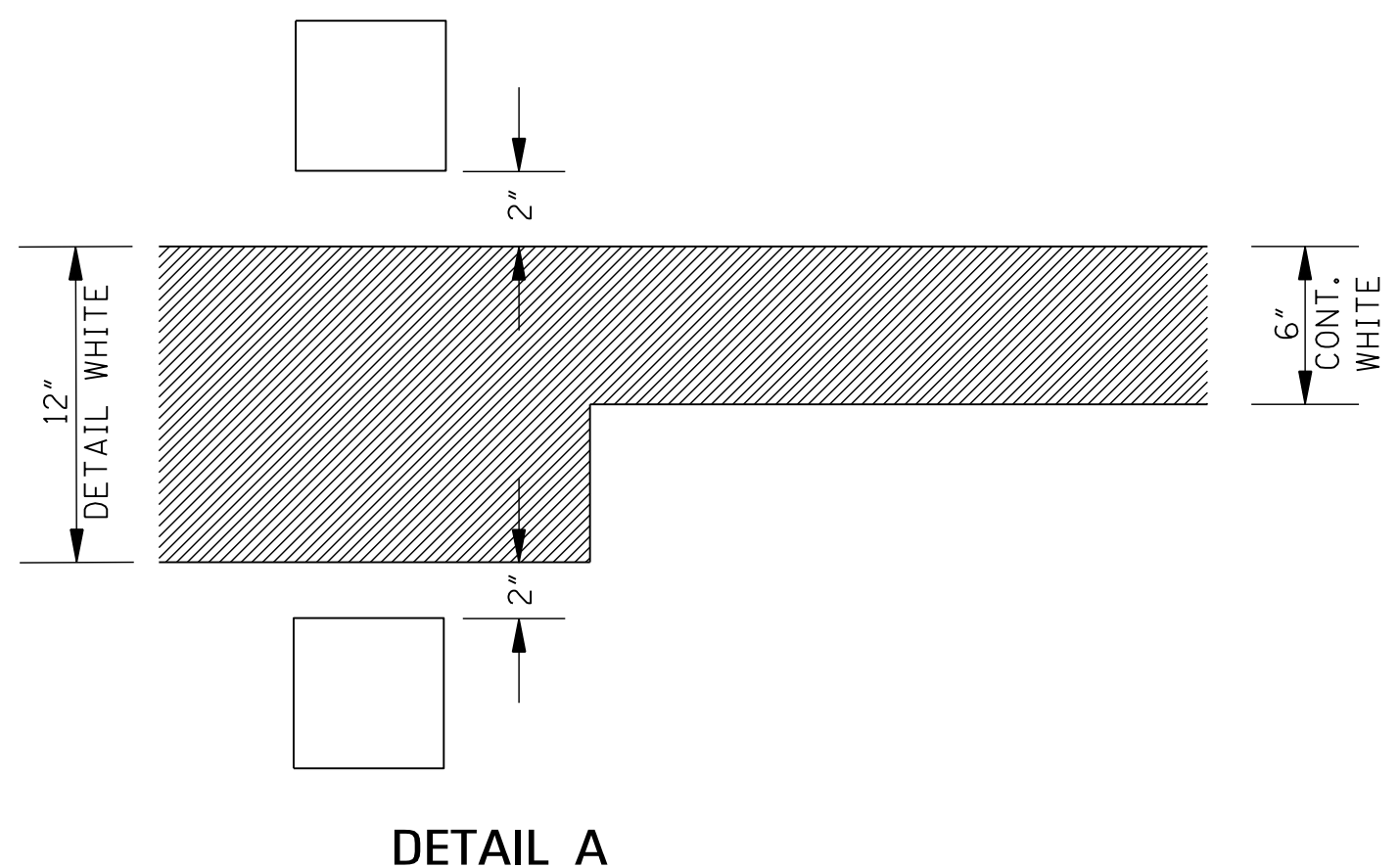
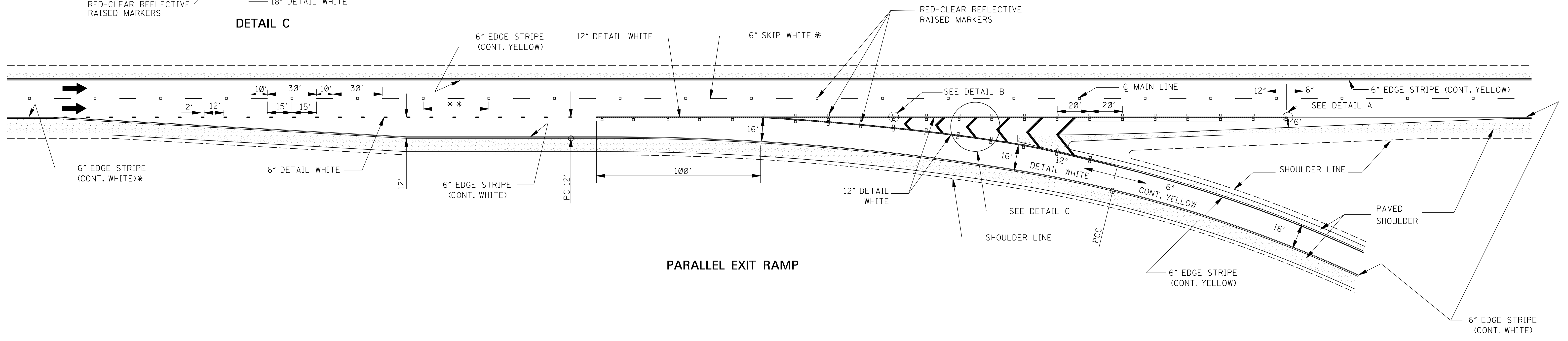
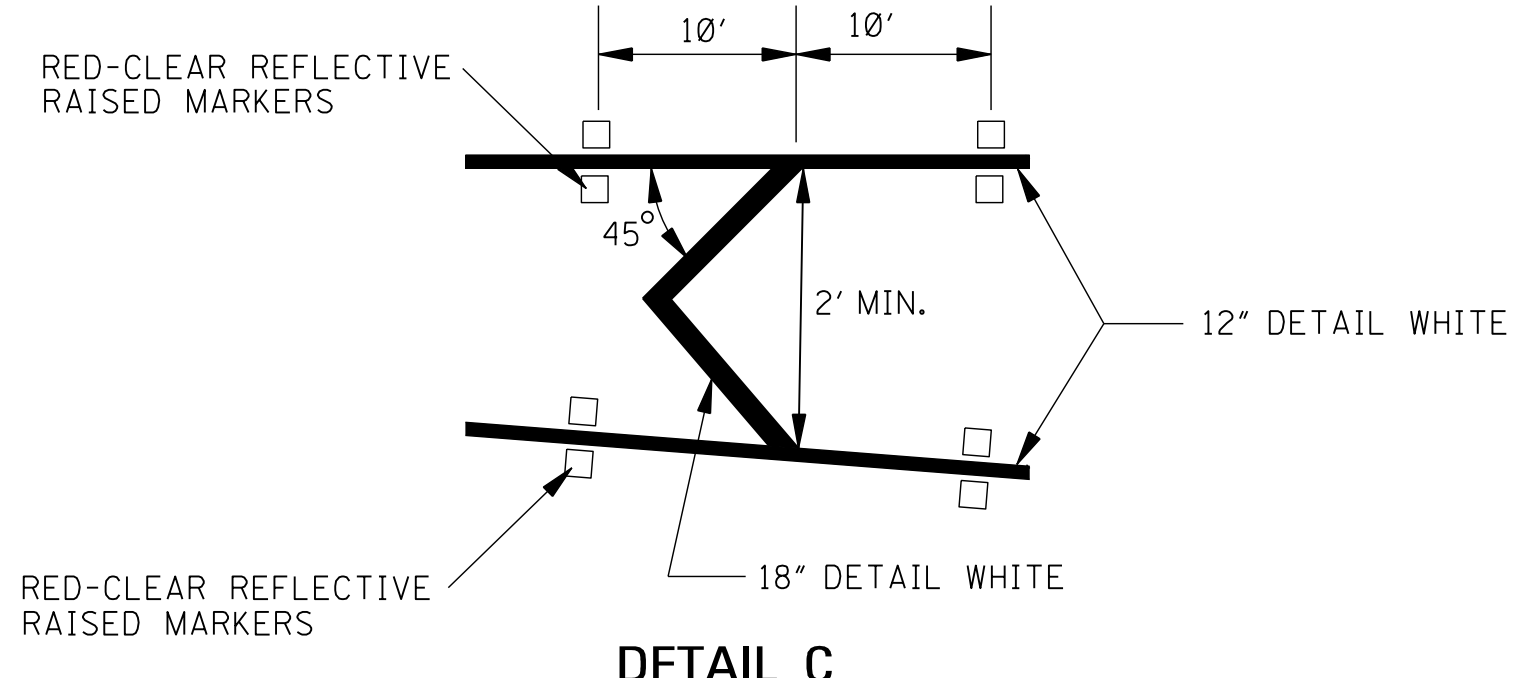
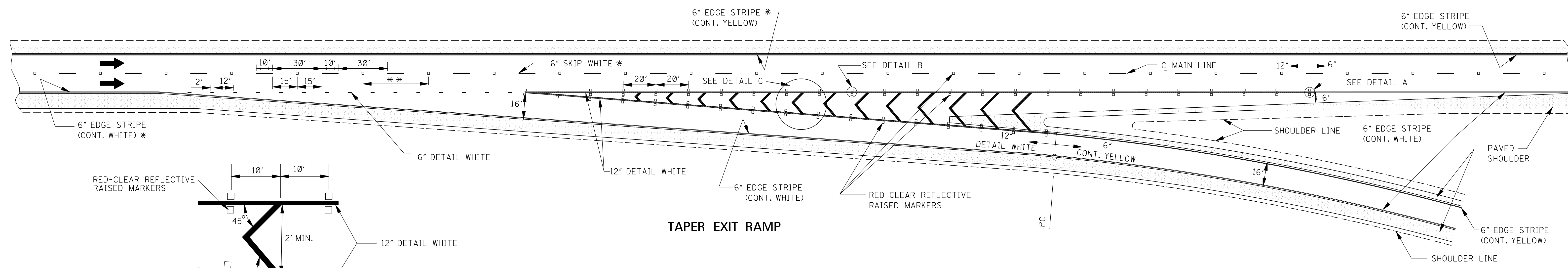
GENERAL NOTES:

- * 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELED WAY.
- ** 2. ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
- 3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED SOURCES OF MATERIALS."

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		PAVEMENT MARKING DETAILS FOR INTERCHANGE ENTRANCE RAMPS (PARALLEL AND TAPER)	
DATE			
ISSUE DATE:		AUGUST 01, 2017	



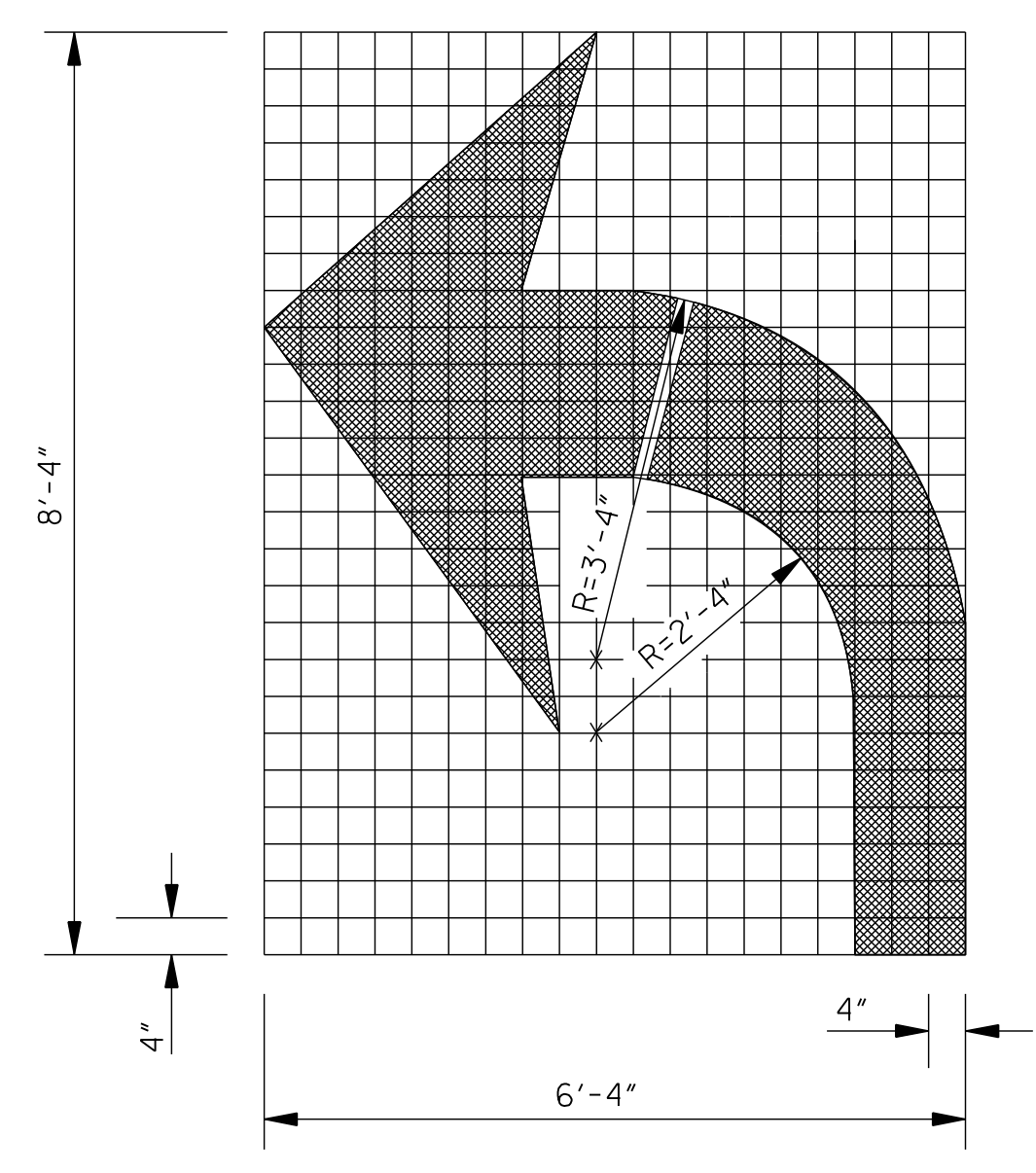
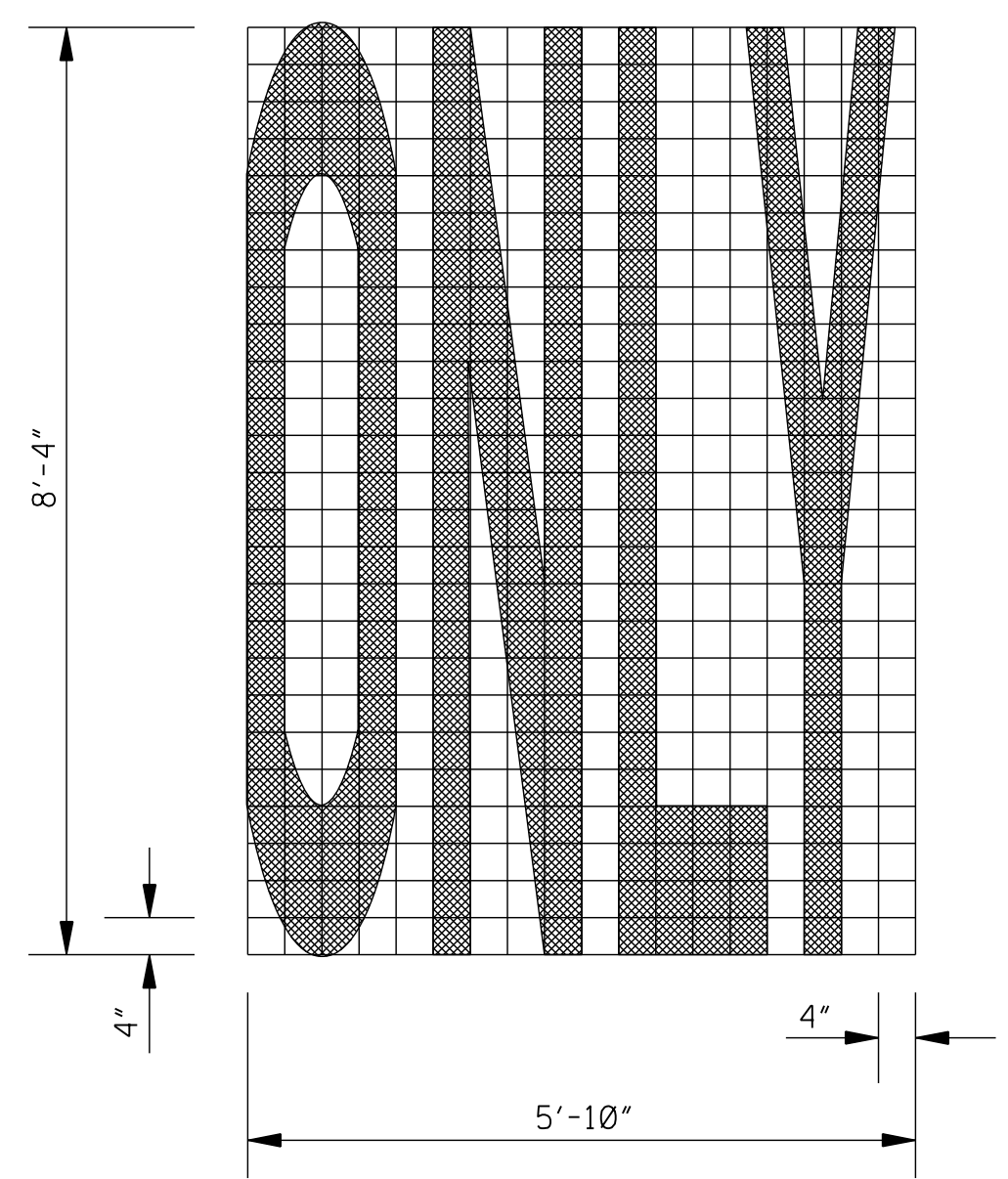
WORKING NUMBER
PM-3
SHEET NUMBER
6053



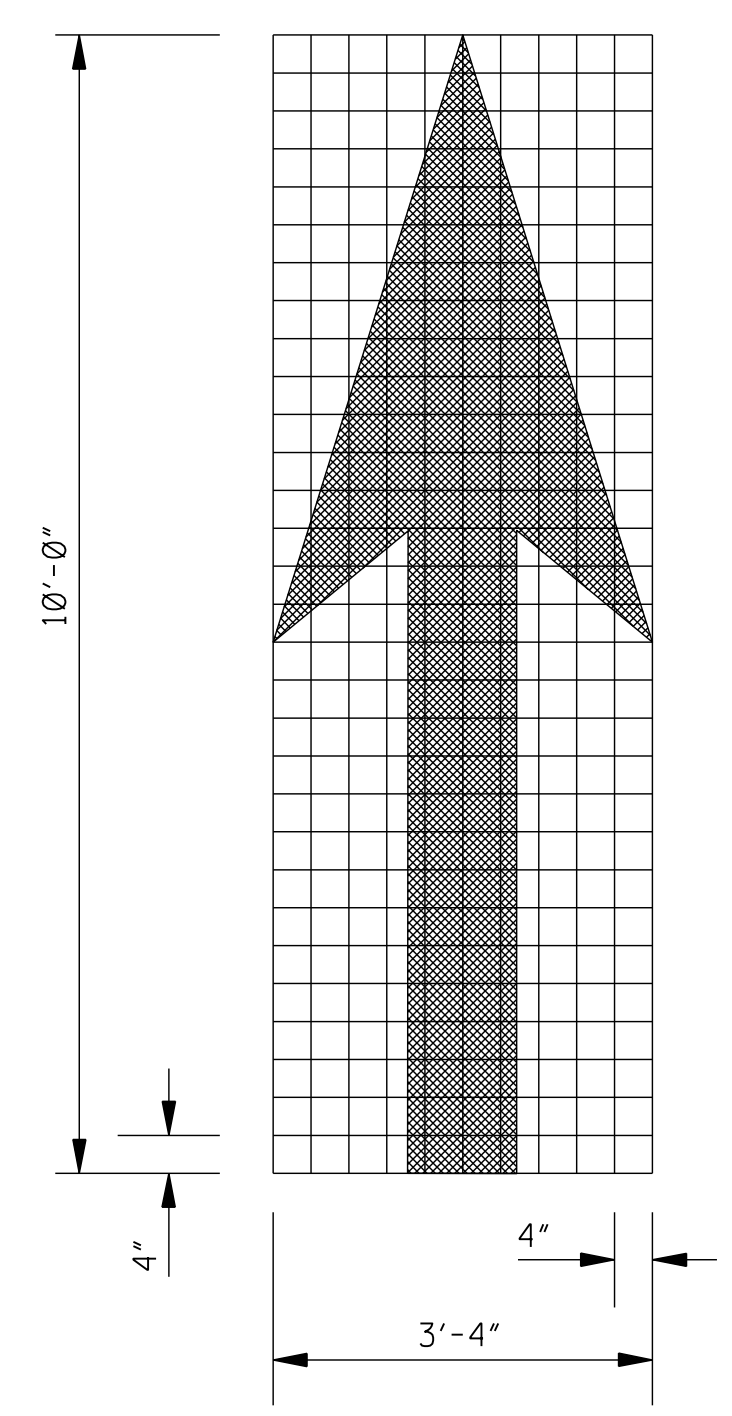
- GENERAL NOTES:
- * 1. SEE SHEET PM-1 FOR THE PLACEMENT OF LANE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF THE EDGE LINE WITH RESPECT TO THE OUTSIDE EDGE OF THE TRAVELED WAY.
 - ** 2. ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - 3. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDOT "APPROVED SOURCES OF MATERIALS."

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
PAVEMENT MARKING DETAILS FOR INTERCHANGE EXIT RAMP (PARALLEL AND TAPER)	
BY	
REVISION	
DATE	ISSUE DATE: AUGUST 01, 2017

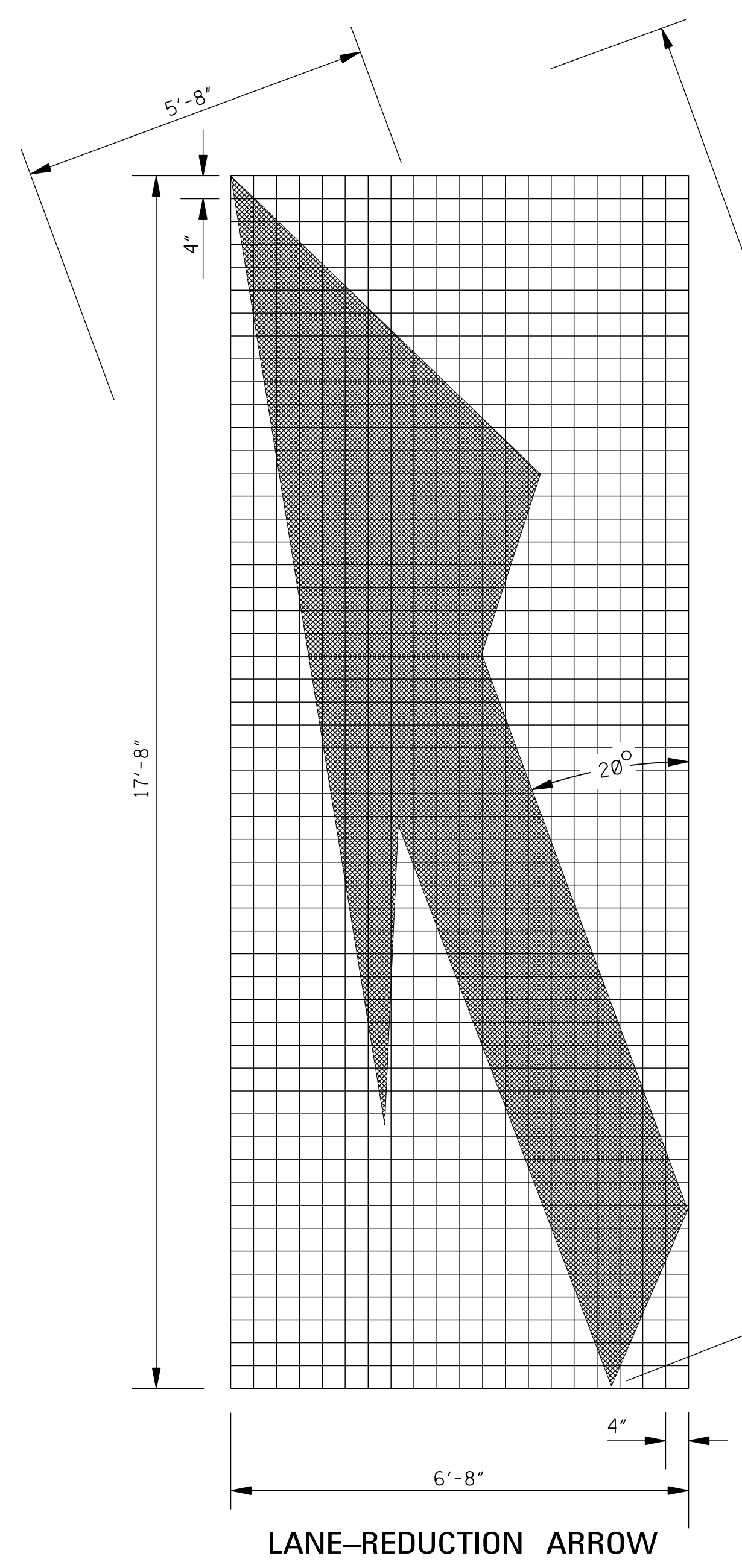

 WORKING NUMBER
 PM-4
 SHEET NUMBER
 6054



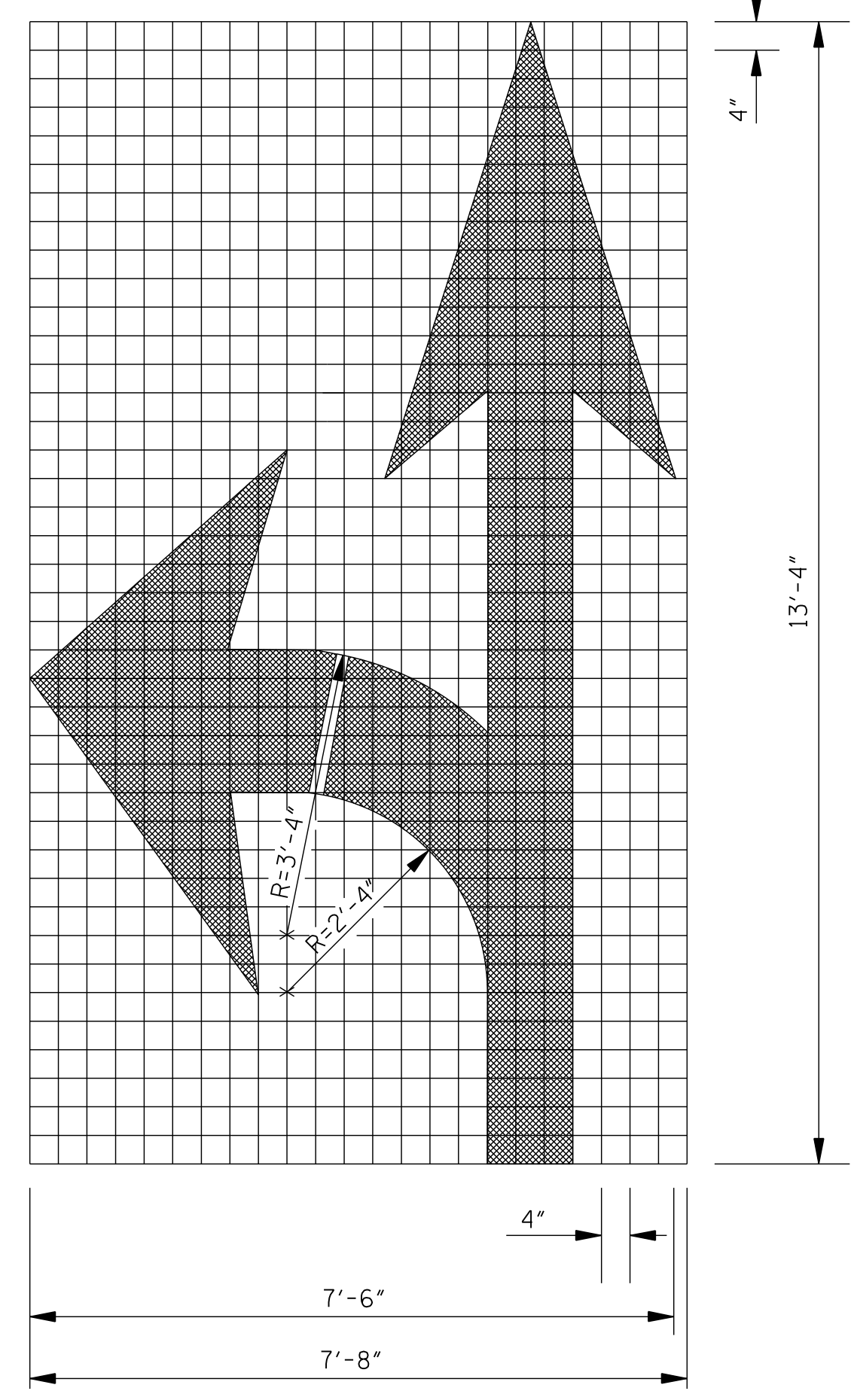
TURN ARROW



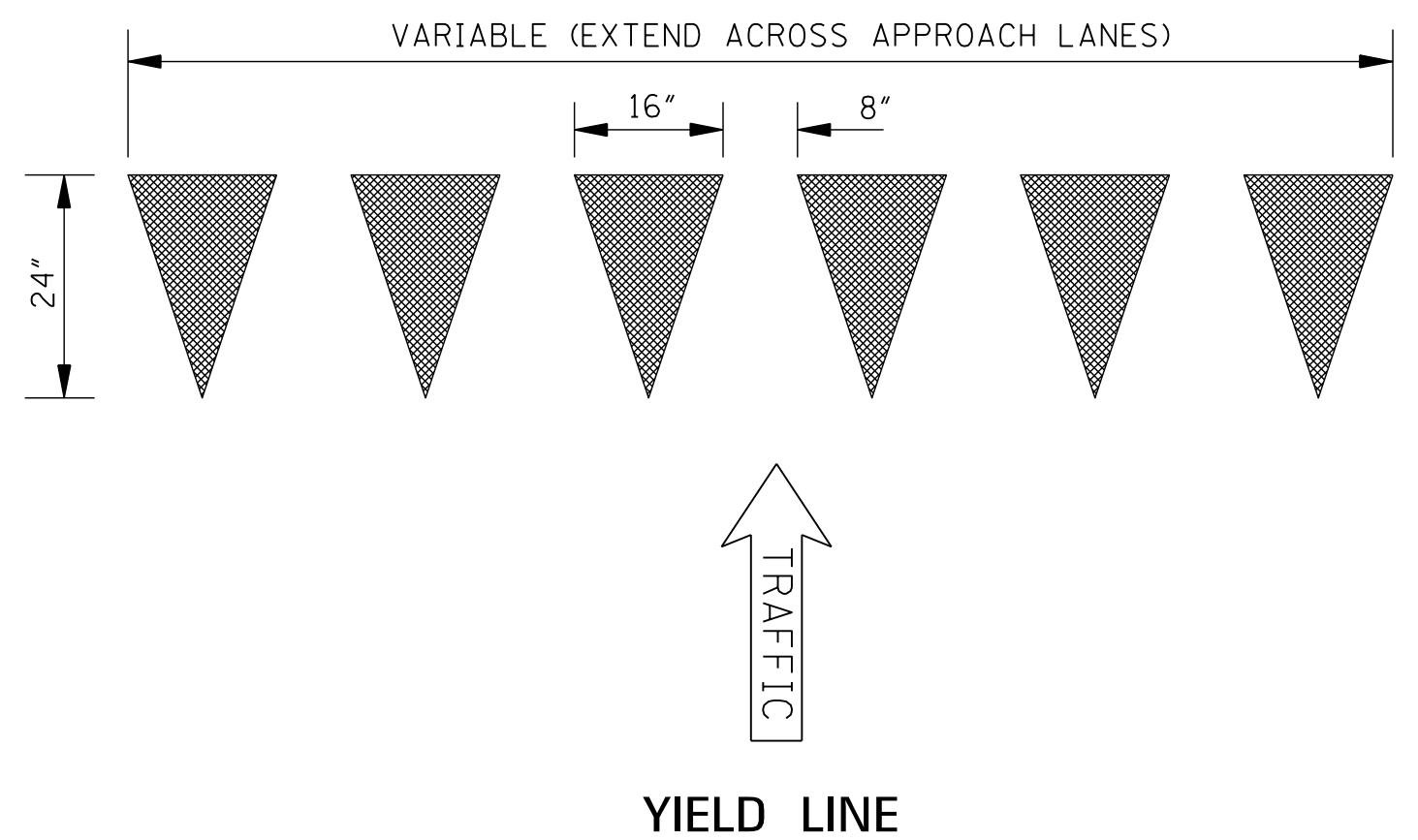
THRU ARROW



LANE-REDUCTION ARROW



COMBINATION ARROW

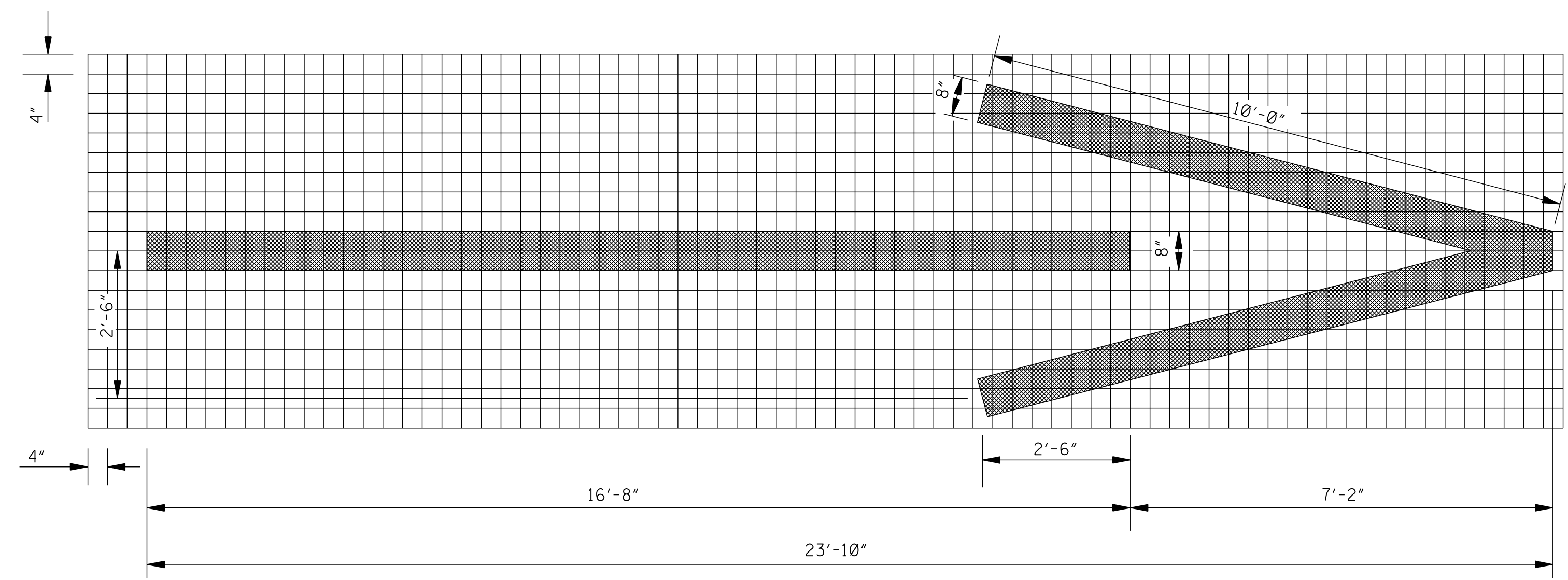


YIELD LINE

GENERAL NOTES:

1. TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTORS) OF 1/2" OR LESS AND EXTENDING THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
2. FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
3. DIMENSIONS OF THE YIELD LINE MAY VARY WITH APPROVAL OF THE ENGINEER. SEE MUTCD, LATEST EDITION, FOR ALLOWABLE DIMENSIONS.
4. PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

PAY QUANTITIES	
LEGEND/SYMBOL	AREA (ft ²)
ONLY	22.0
TURN ARROW	16.4
THRU ARROW	12.3
COMB. ARROW	27.5
1-WAY ARROW	24.3
LANE REDUCTION ARROW	40.0



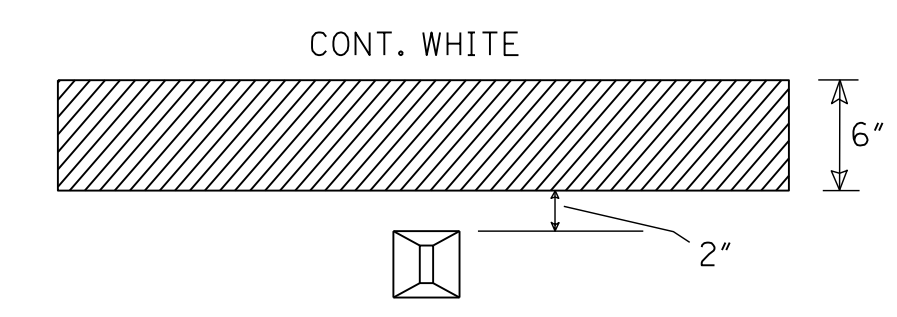
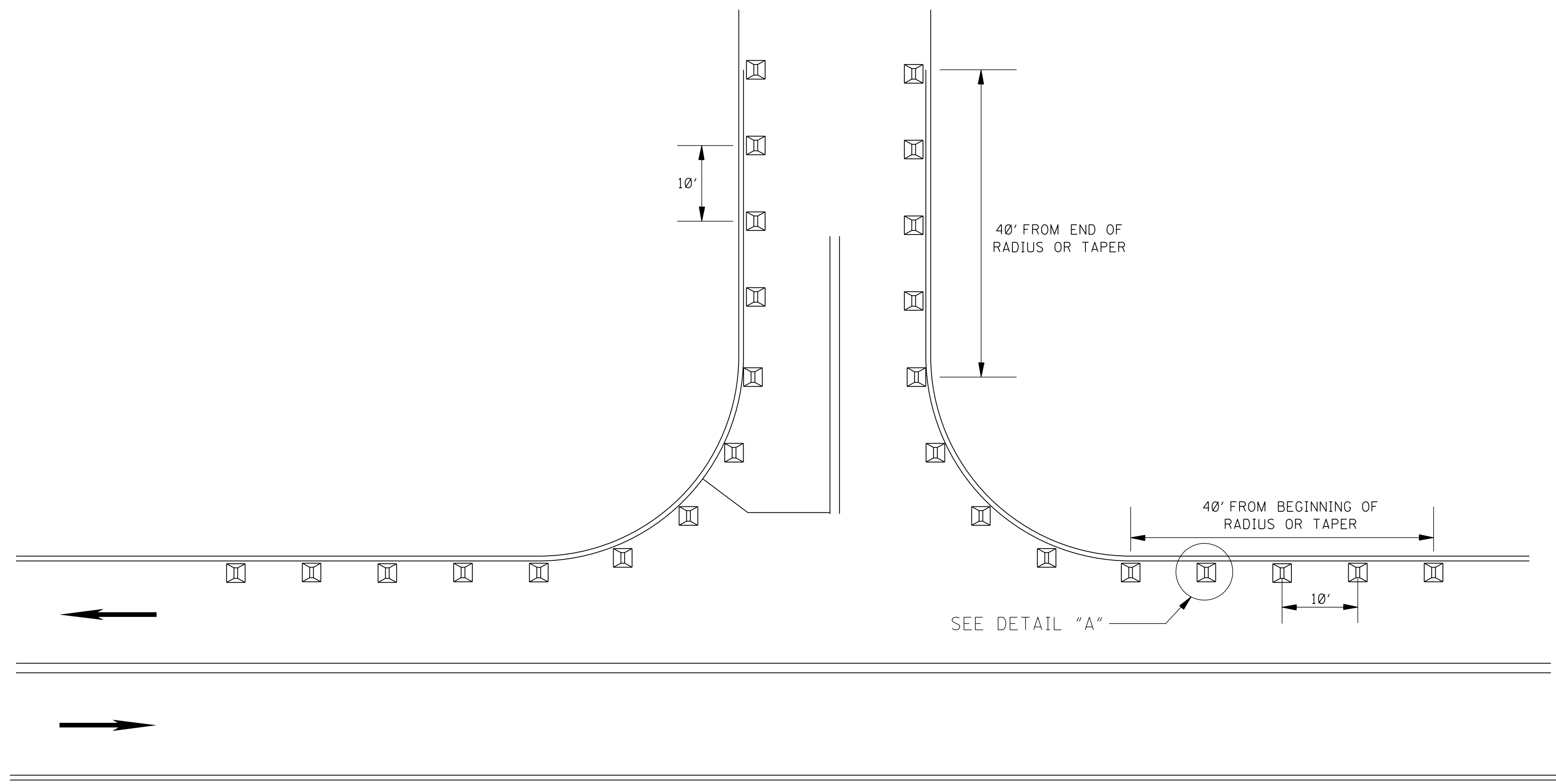
1-WAY ARROW

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
REVISION	
DATE	ISSUE DATE: AUGUST 01, 2017

**PAVEMENT MARKING
LEGEND DETAILS**


 WORKING NUMBER
 PM-6
 SHEET NUMBER
 6056

TYPICAL PLACEMENT OF RAISED PAVEMENT MARKERS ON SIDE ROAD RADIUS
2-LANE, TWO WAY TRAFFIC



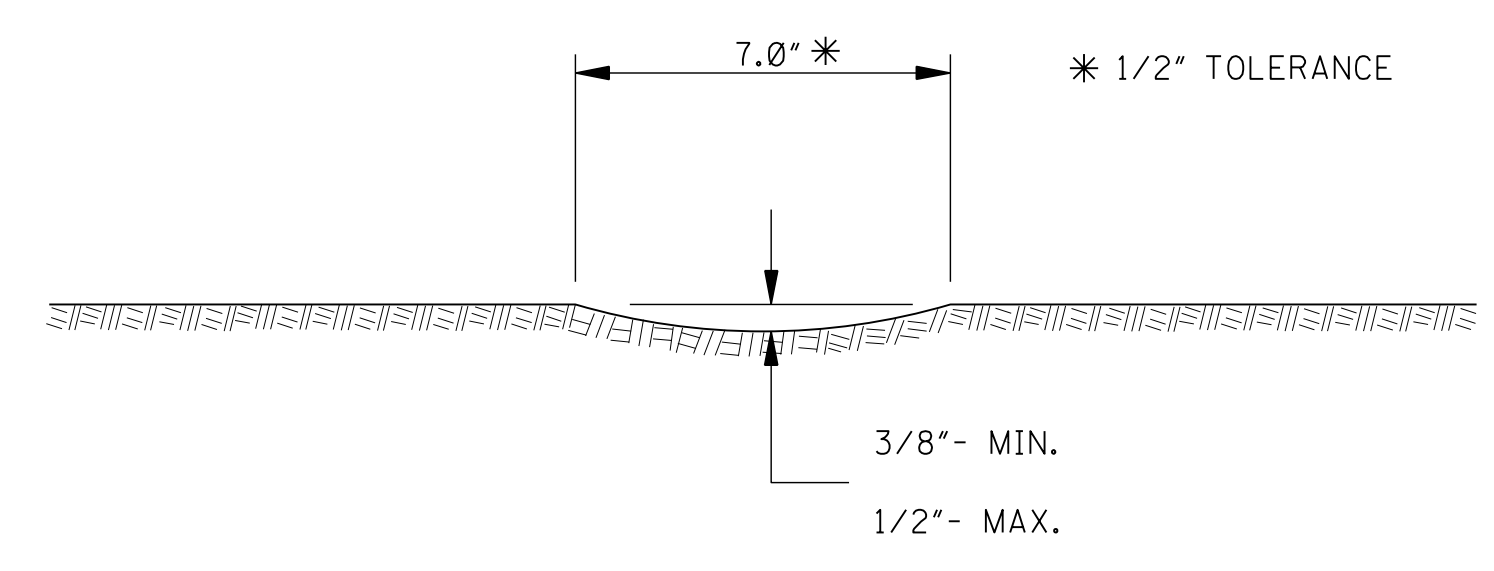
DETAIL A

➔ DIRECTION OF TRAFFIC

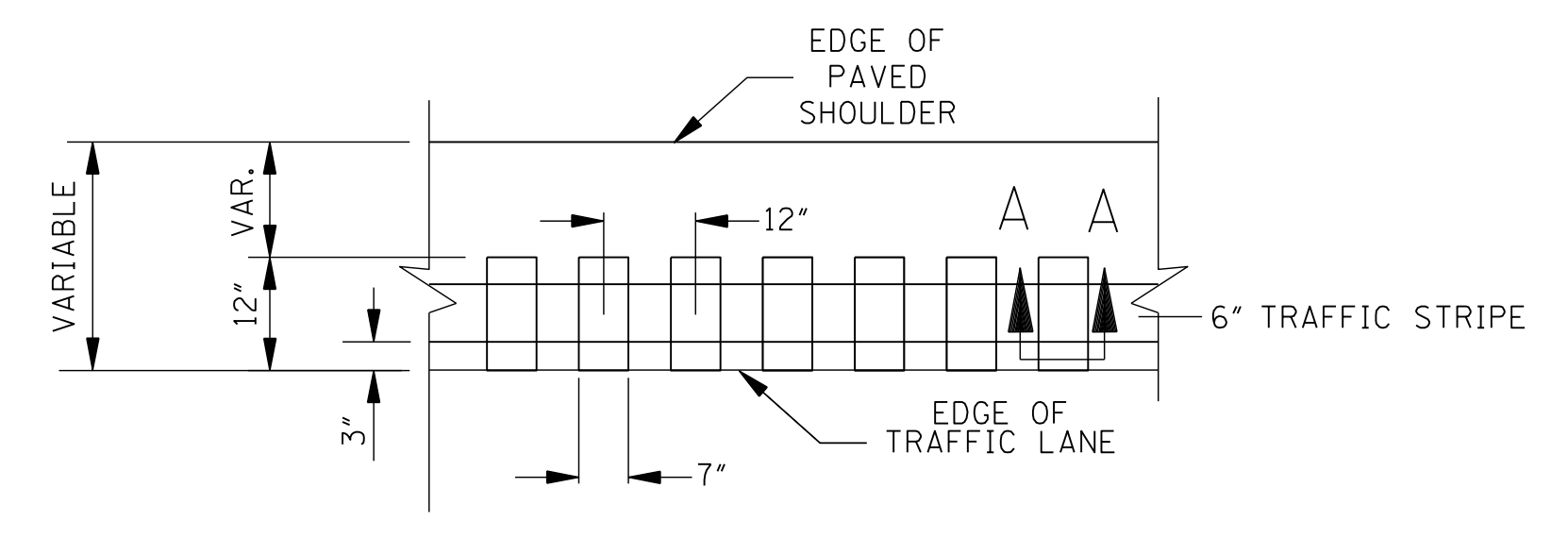
GENERAL NOTES:

1. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
2. MARKERS SHALL BE HIGH PERFORMANCE TWO-WAY CLEAR.
3. MARKERS SHALL NOT BE ROTATED WHEN BEING PLACED ALONG RADIUS AND TANGENT SECTION OF LOCAL ROAD.
4. MARKERS SHALL BE INSTALLED AT SIMPLE AND CHANNELIZED INTERSECTIONS TO THE LIMITS SHOWN ABOVE.

		BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
		REVISION		2-WAY RAISED PAVEMENT MARKERS AT INTERSECTING ROADS (2-LANE)	
		DATE			
		ISSUE DATE: AUGUST 01, 2017		 WORKING NUMBER PM-11 SHEET NUMBER 6061	

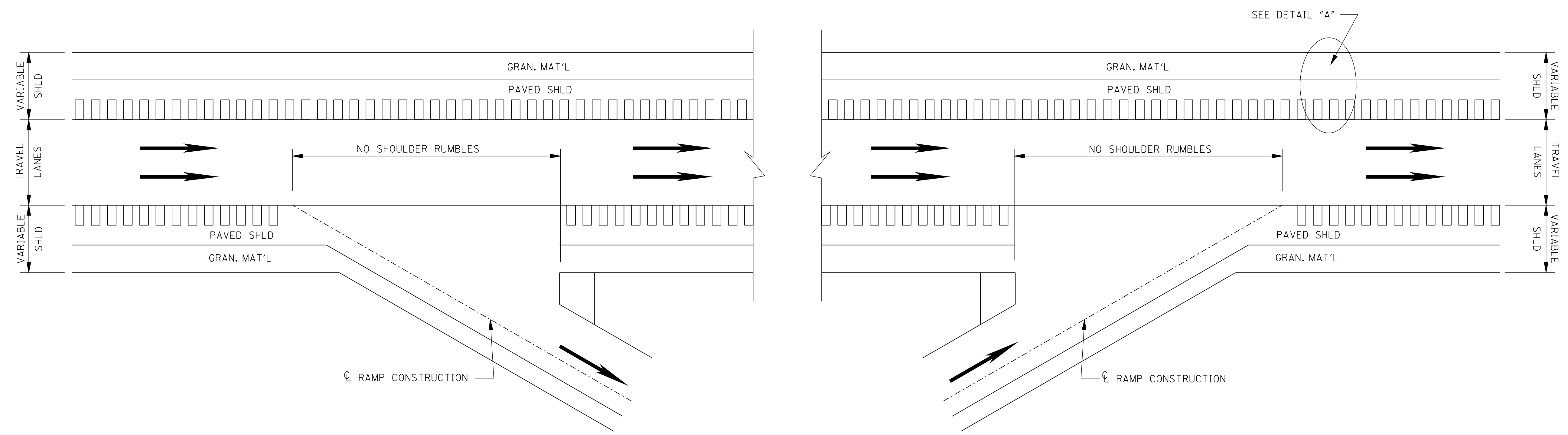


SECTION "A-A"



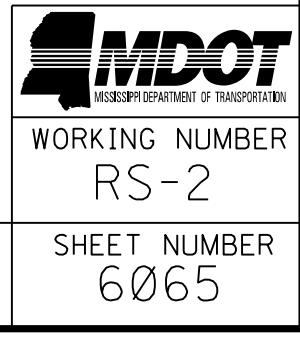
DETAIL "A"

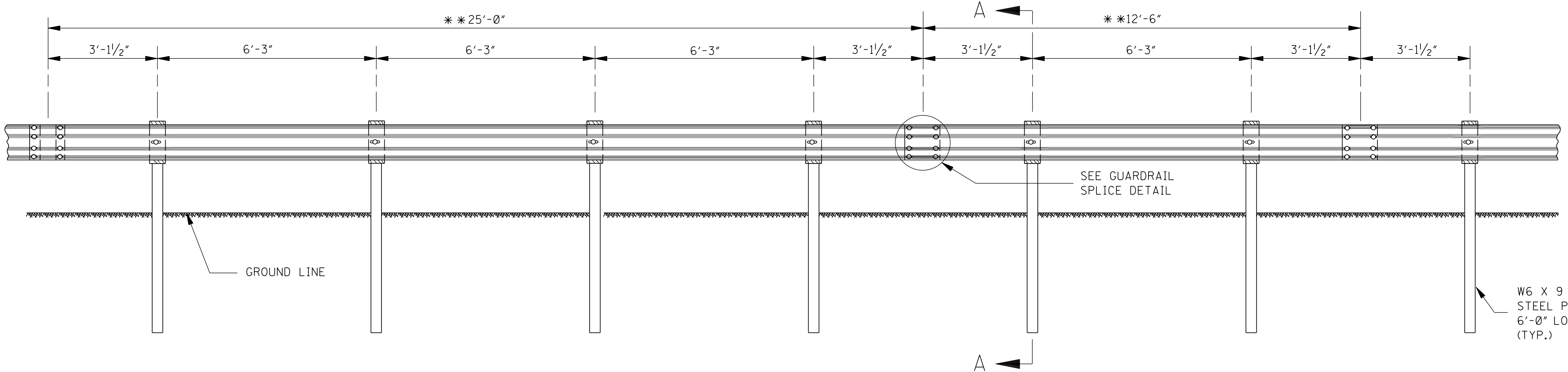
- GENERAL NOTES
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
 - GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PUBLIC INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 - COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
 - MAINLINE
 - INTERSECTING ROADWAY IF OVERLAID OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.W.
 - ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.



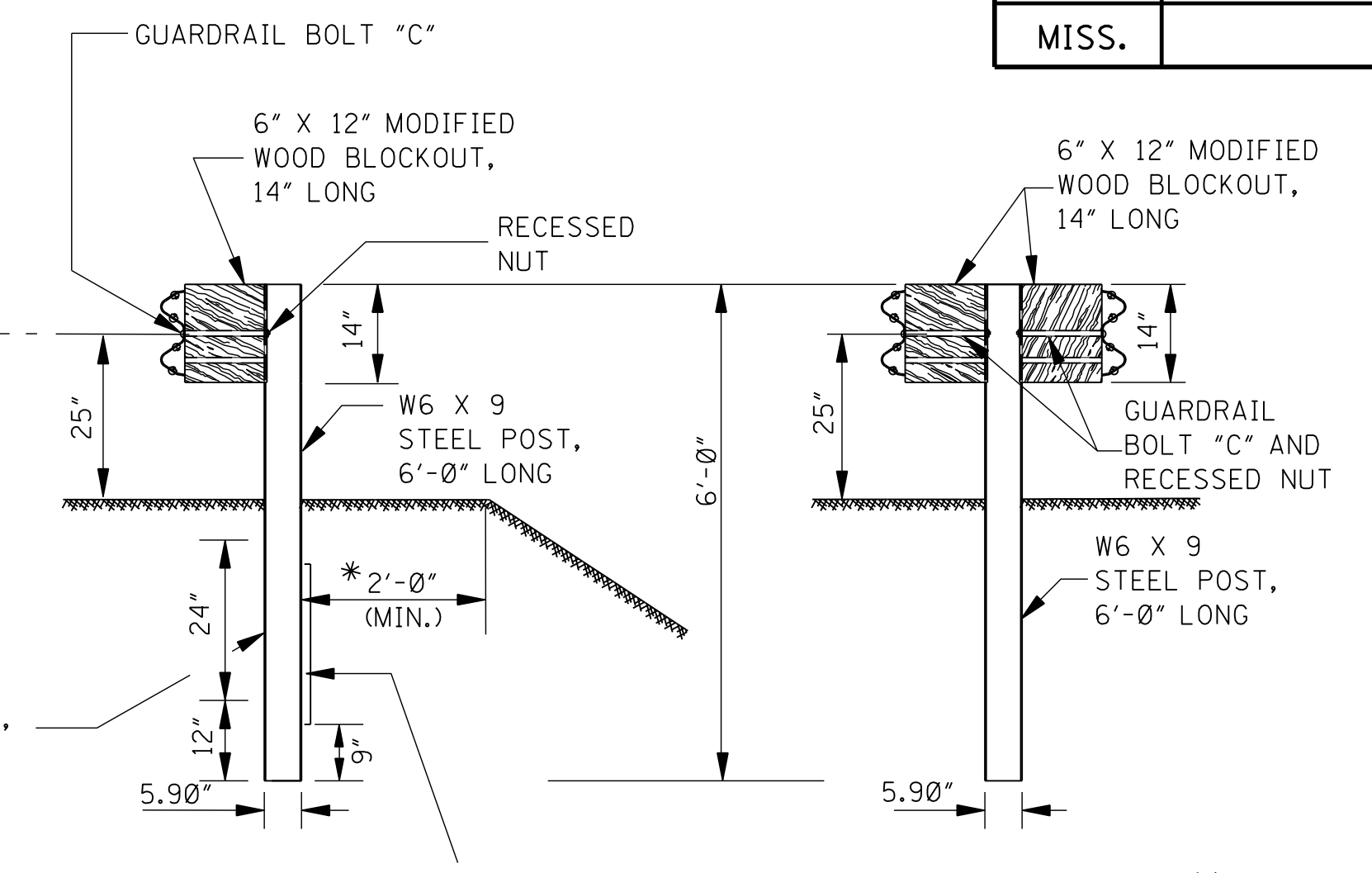
PLAN
NOT TO SCALE
DETAILS OF
RUMBLE STRIPS

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		RUMBLE STRIPES 4-LANE HIGHWAYS (ASPHALT LANES, 2-FT OR WIDER ASPHALT SHOULDERS)	
DATE			
ISSUE DATE:		AUGUST 01, 2017	
WORKING NUMBER		RS-2	
SHEET NUMBER		6065	

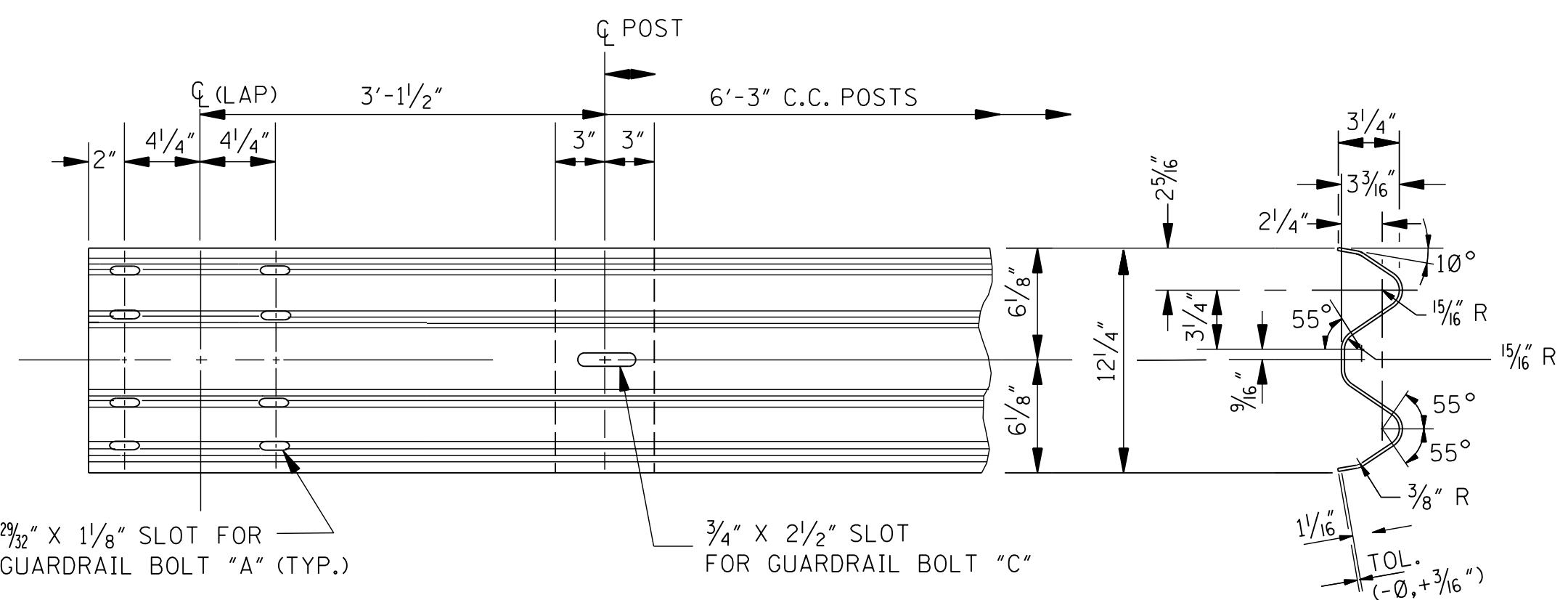




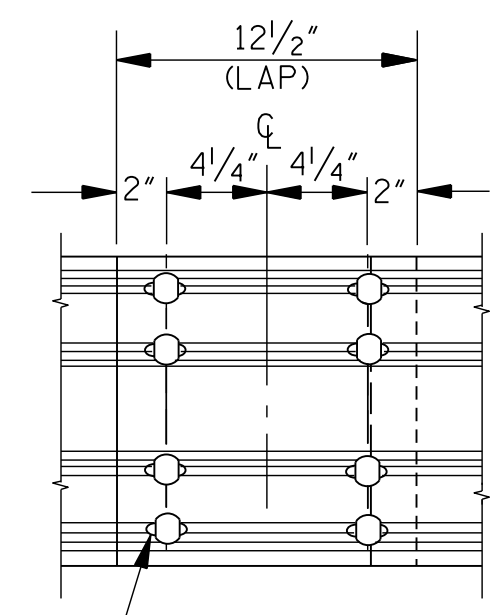
ELEVATION FROM ϕ ROADWAY
 ** NOTE: OPTIONAL BEAM LENGTHS.



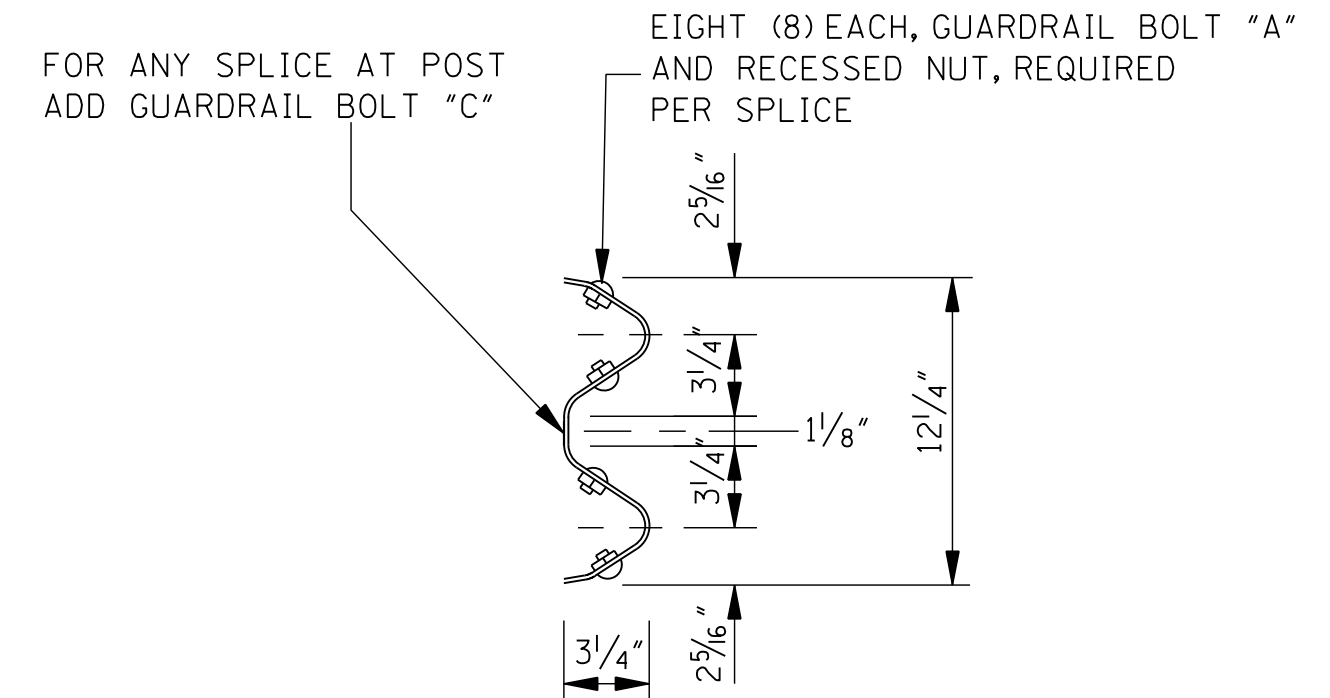
SINGLE-FACED BARRIER DOUBLE-FACED BARRIER
 SECTION A-A
 * NOTE: UNLESS SPECIFIED OTHERWISE ON THE PLANS.



TYPICAL GUARDRAIL SECTION

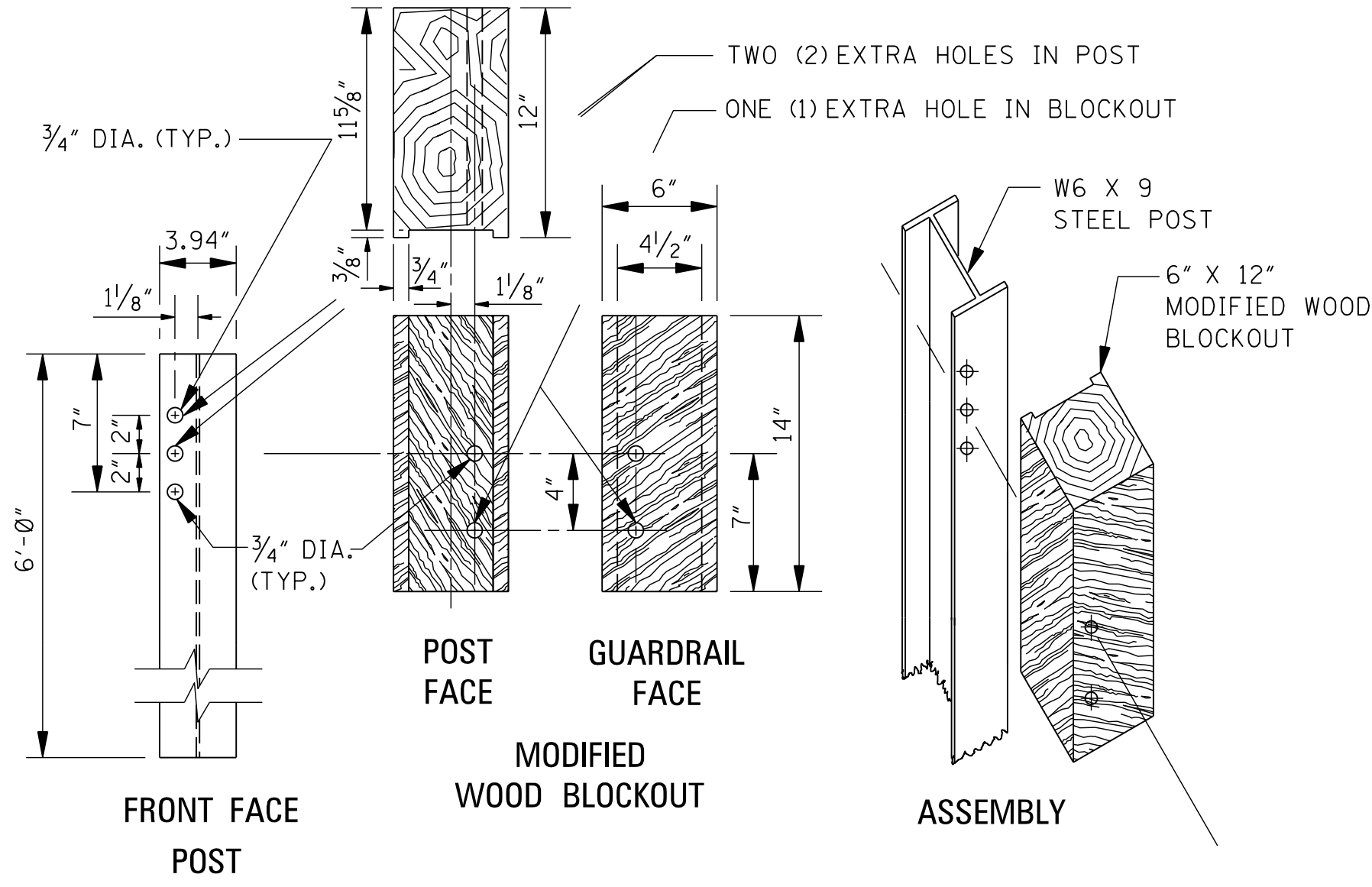


ELEVATION

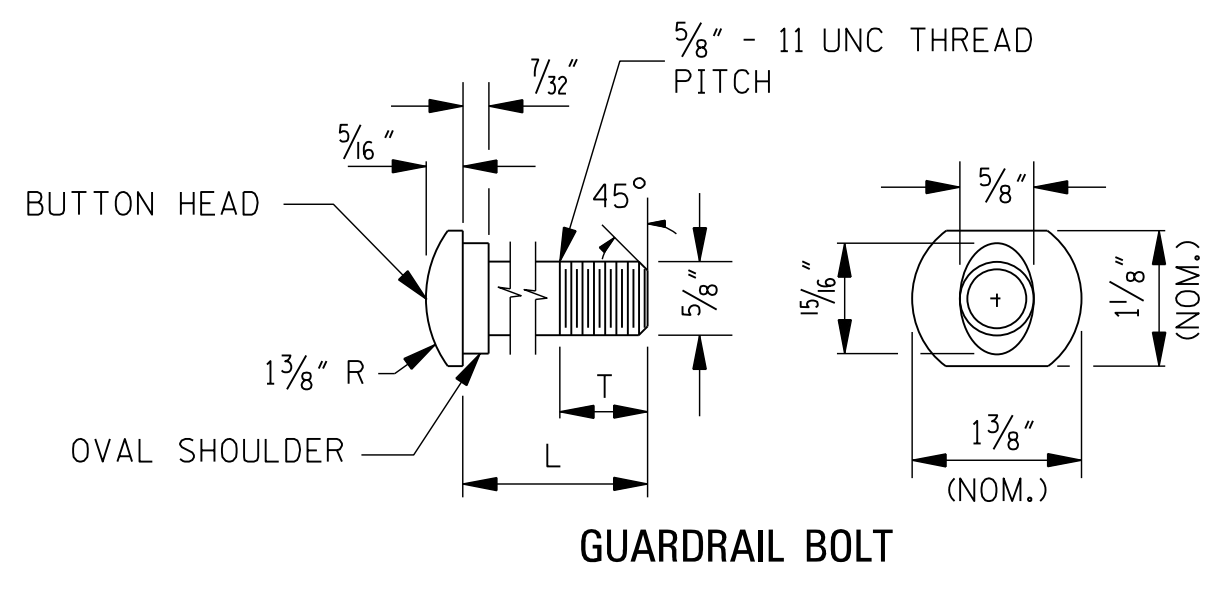


SECTION

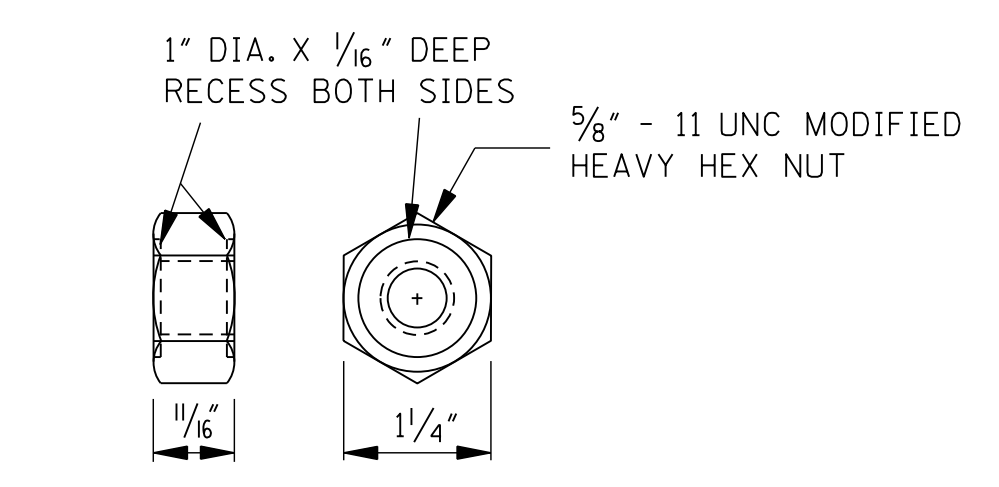
GUARDRAIL SPLICE DETAIL



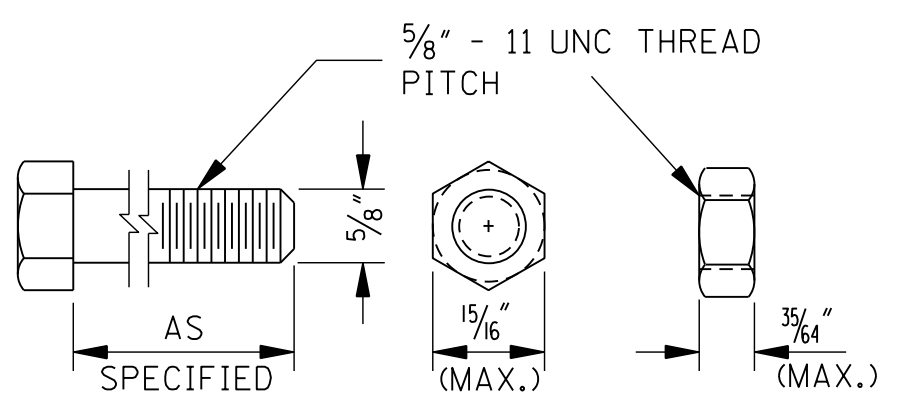
DETAILS OF ADJUSTABLE HEIGHT BLOCKOUT ASSEMBLY



GUARDRAIL BOLT



RECESSED NUT



HEX NUT AND BOLT "F"

GUARDRAIL BOLTS		
BOLT	L	T (MIN.)
"A"	1 1/4"	1"
"B"	12"	4"
"C"	14"	4"

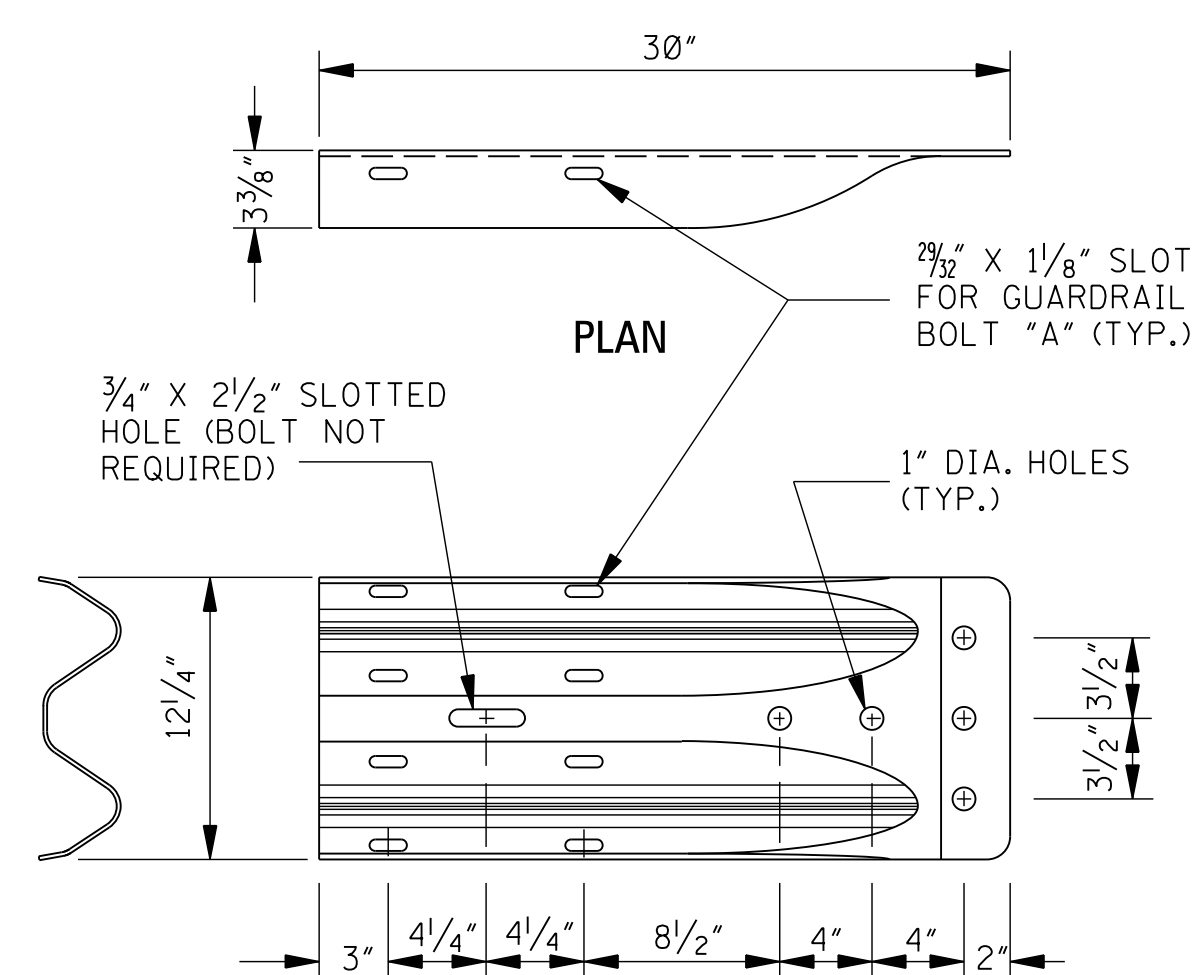
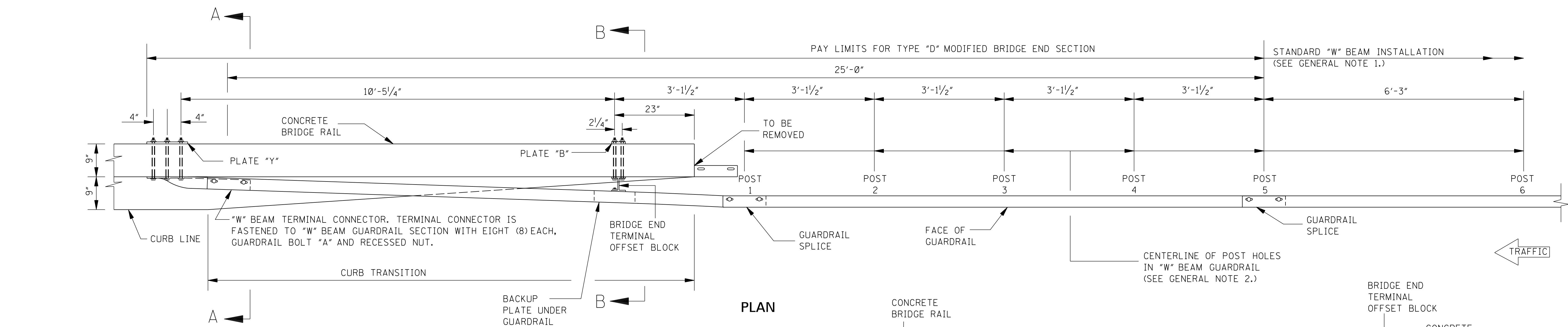
- NOTES:
- ALL GUARDRAIL BOLTS ARE 5/8" - 11 UNC THREAD PITCH.
 - IF ANY BOLT EXTENDS MORE THAN 1/4" FROM THE NUT, THE BOLT SHOULD BE TRIMMED BACK.

FASTENER DETAILS

- NOTES:
- ON INITIAL INSTALLATION, THE MODIFIED WOOD BLOCKOUT SHALL BE FASTENED TO THE BOTTOM HOLE IN THE STEEL POST. OTHER HOLES IN THE STEEL POST AND THE MODIFIED WOOD BLOCKOUT ARE FOR FUTURE 2" HEIGHT ADJUSTMENTS WHEN THE ROADWAY IS RESURFACED.
 - AN ADDITIONAL GUARDRAIL BOLT "C" AND RECESSED NUT IS REQUIRED FOR THE SECOND HEIGHT ADJUSTMENT.
 - HOLE DETAILS ARE REQUIRED ON ALL STEEL POSTS AND MODIFIED WOOD BLOCKOUTS.
 - STEEL POSTS ARE FABRICATED FROM W6 X 9 STRUCTURAL STEEL SHAPES.
 - MODIFIED WOOD BLOCKOUTS ARE FABRICATED FROM 6" X 12" TREATED TIMBER UNLESS SPECIFIED OTHERWISE ON THE PLANS.
 - ALL HOLES IN BOTH STEEL POSTS AND MODIFIED WOOD BLOCKOUTS ARE 3/4" IN DIAMETER.

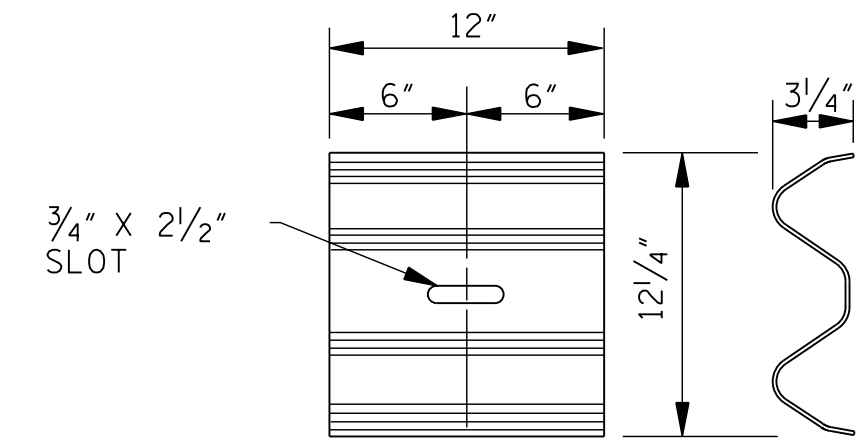
- GENERAL NOTES:
- GUARDRAIL SHALL MEET THE REQUIREMENTS OF AASHTO M 180, CLASS A, TYPE 1 UNLESS OTHERWISE DESIGNATED.
 - GUARDRAIL SHALL BE SINGLE FACED UNLESS OTHERWISE DESIGNATED.
 - GUARDRAIL SECTIONS SHALL BE LAPPED IN THE DIRECTION OF TRAFFIC FLOW FOR THE LANE NEAREST THE GUARDRAIL. THE ONLY EXCEPTION NOTED IS THAT GUARDRAIL SHALL BE LAPPED FOR APPROACHING TRAFFIC ON A BRIDGE WITH 2-WAY TRAFFIC.
 - STEEL POSTS SHALL CONFORM TO ASTM A36 (EXCEPT ULTRASONIC TESTING). THEY SHALL BE GALVANIZED IN ACCORDANCE WITH AASHTO M 111 EXCEPT WHEN CORROSION RESISTANT STEEL IS REQUIRED IN WHICH CASE POSTS SHALL CONFORM TO AASHTO SPECIFICATIONS FOR CORROSION RESISTANCE AND SHALL NOT BE PAINTED OR GALVANIZED. NO PUNCHING, DRILLING OR CUTTING WILL BE PERMITTED AFTER GALVANIZING EXCEPT FOR HOLES TO MOUNT GUARDRAIL DELINEATORS.
 - ALL MODIFIED WOOD BLOCKOUTS SHALL BE TREATED TIMBER IN ACCORDANCE WITH MISSISSIPPI DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS.
 - FOR OTHER DETAILS OF POSTS, POST ACCESSORIES, FASTENERS & RAIL ELEMENTS, SEE AASHTO-AGC-ARTBA JOINT TASK FORCE NO. 13, TITLED "A GUIDE TO STANDARDIZED HIGHWAY BARRIER HARDWARE," LATEST EDITION.

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		<p>GUARDRAIL: "W" BEAM (STEEL POSTS)</p>	
DATE			
ISSUE DATE: AUGUST 01, 2017		WORKING NUMBER GR-1B	SHEET NUMBER 6203



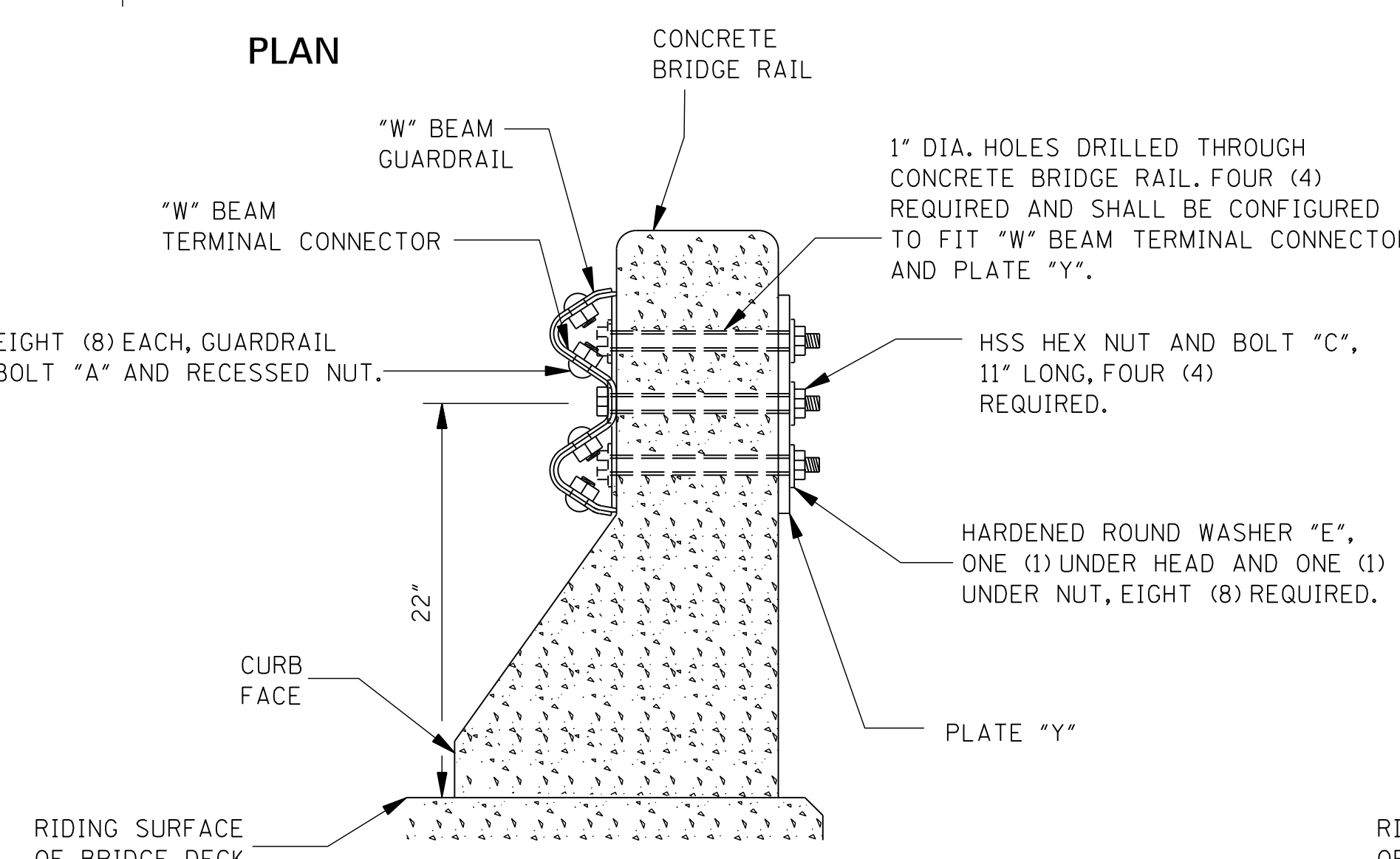
W-BEAM TERMINAL CONNECTOR

NOTE: THE "W" BEAM TERMINAL CONNECTOR IS USED WITH THE TYPE "D" MODIFIED BRIDGE END SECTION. THE CROSS-SECTIONAL DIMENSIONS OF THIS PART ARE IDENTICAL TO THE STANDARD "W" BEAM SECTION (AASHTO M 180 CLASS B, TYPE II).

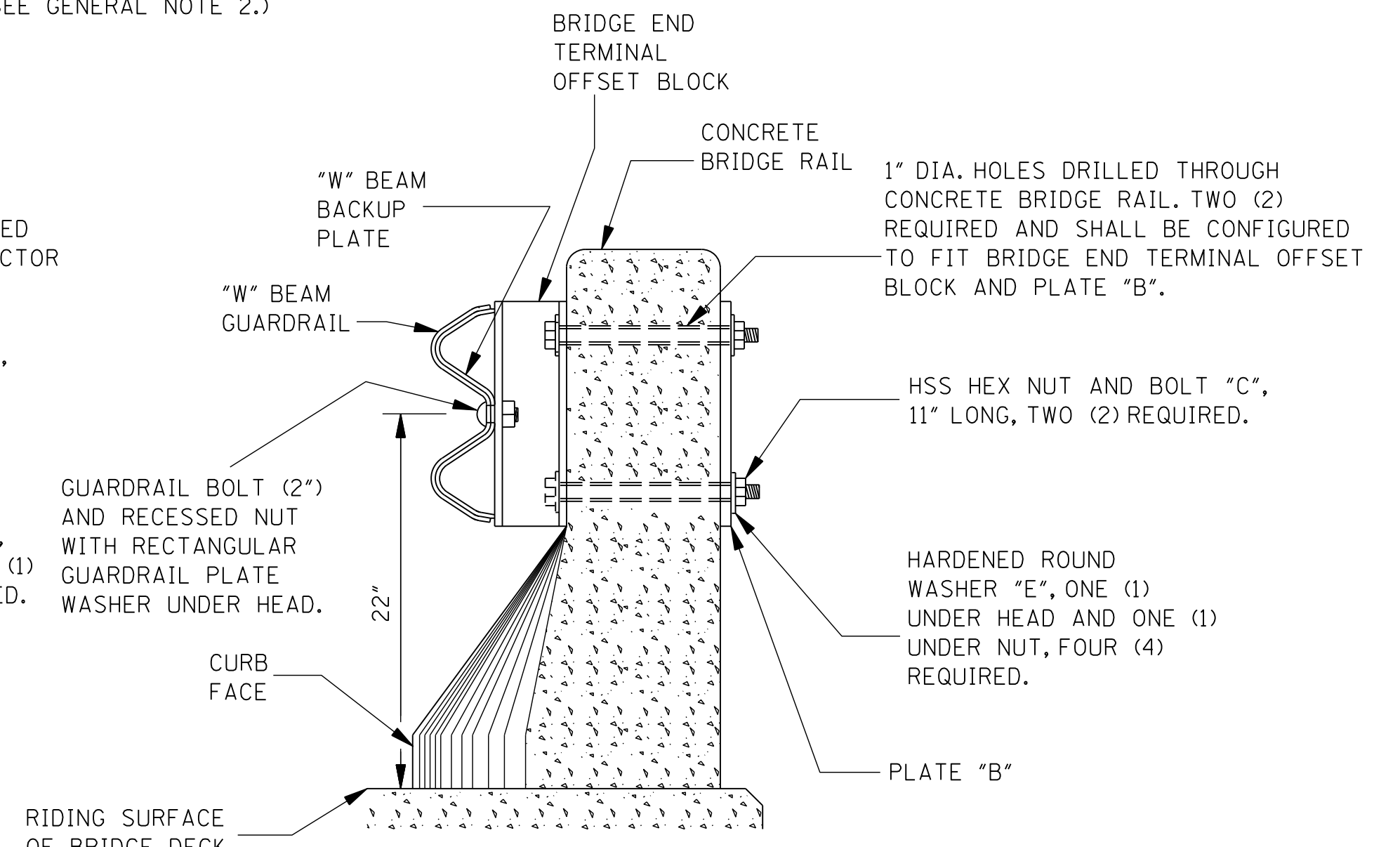


W-BEAM BACKUP PLATE

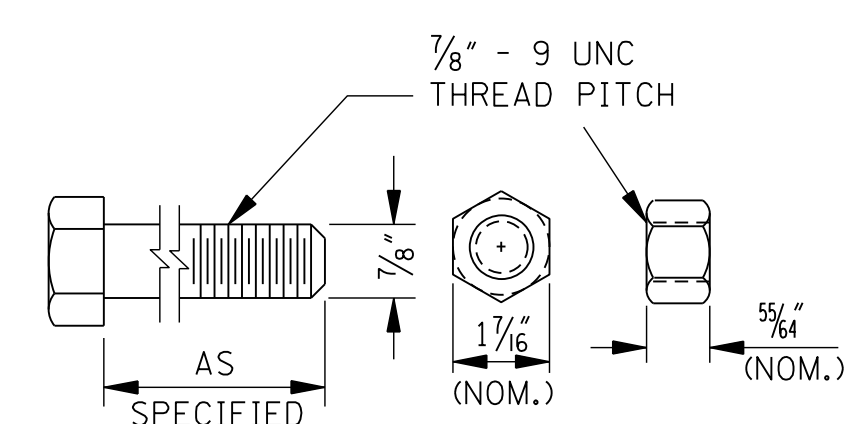
NOTE: THE "W" BEAM BACK-UP PLATE IS USED UNDER THE "W" BEAM RAIL SECTION WHERE INDICATED ON THIS SHEET. THE CROSS-SECTIONAL DIMENSIONS OF THIS PART ARE IDENTICAL TO THOSE OF THE STANDARD "W" BEAM RAIL SECTION.



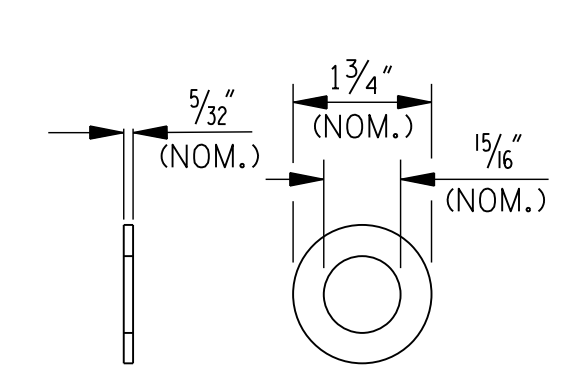
SECTION A-A



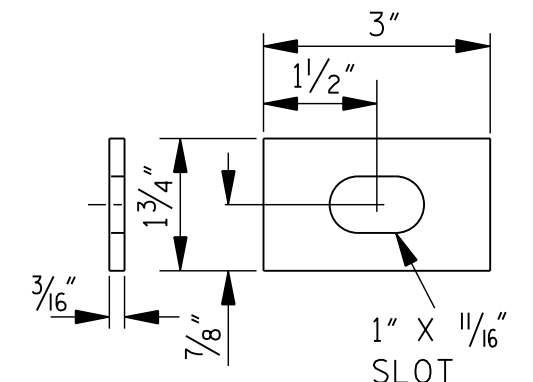
SECTION B-B



HIGH-STRENGTH STRUCTURAL HEX NUT AND BOLT "C"
(ASTM A 325)

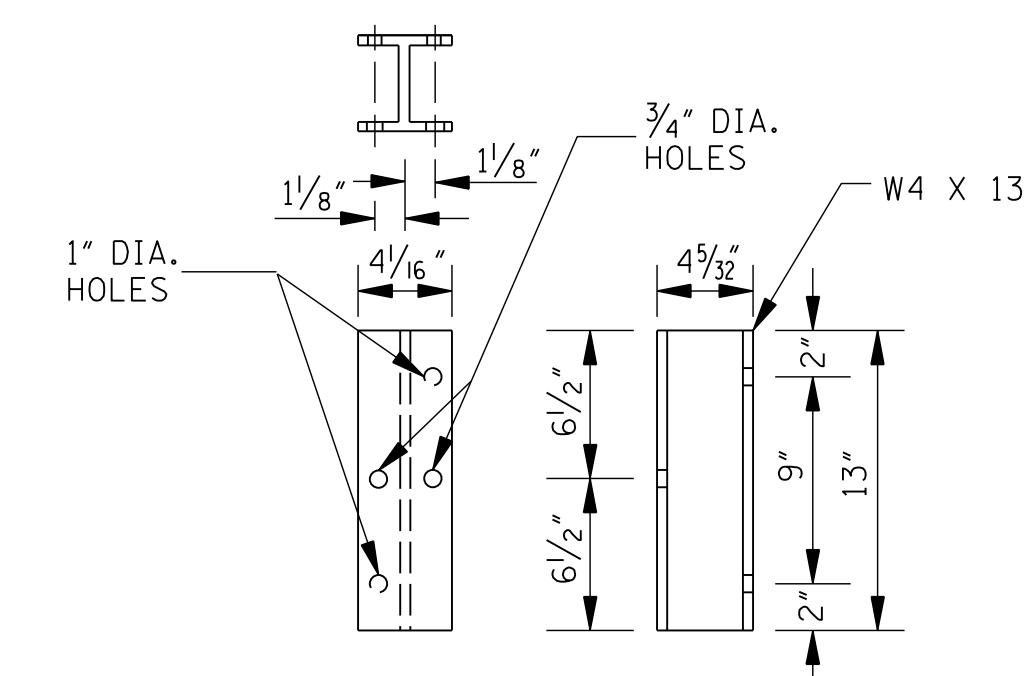


HARDENED ROUND WASHER "E"

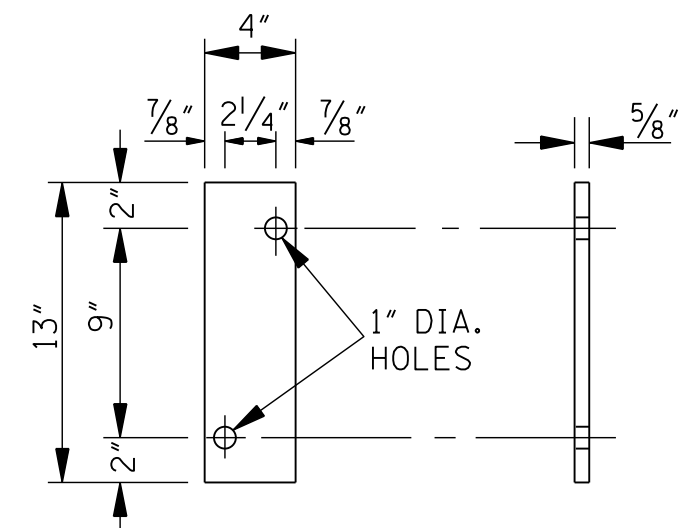


RECTANGULAR GUARDRAIL PLATE WASHER

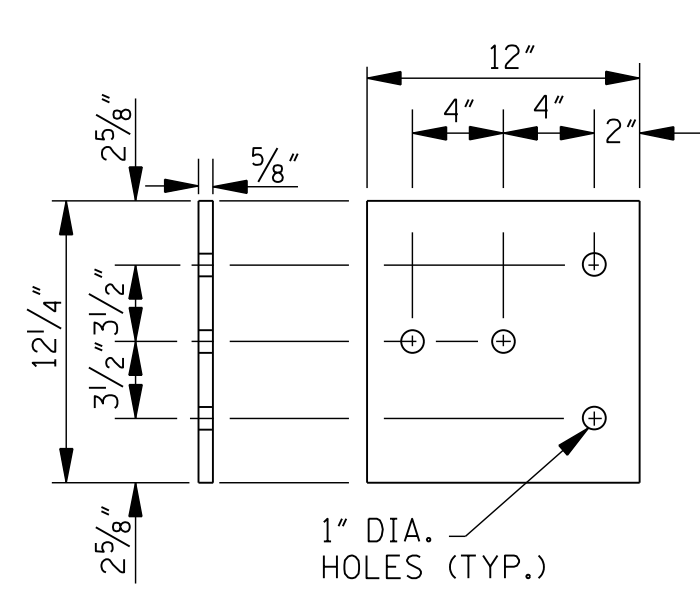
FASTENER DETAILS



DETAIL OF BRIDGE END TERMINAL OFFSET BLOCK



DETAIL OF PLATE "B"



DETAIL OF PLATE "Y"

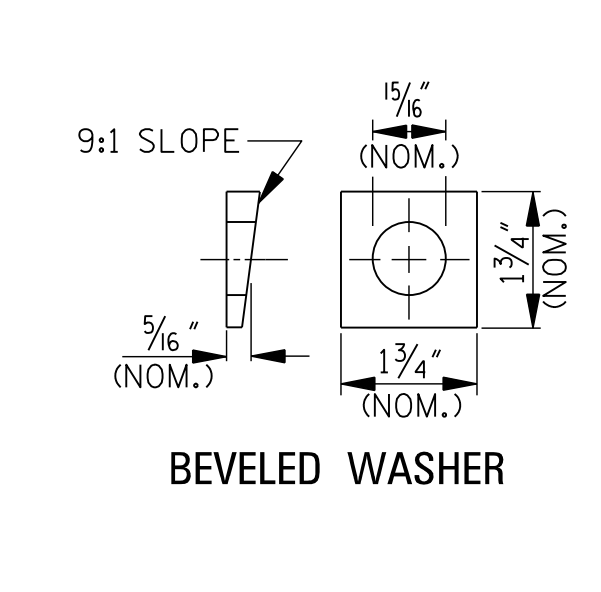
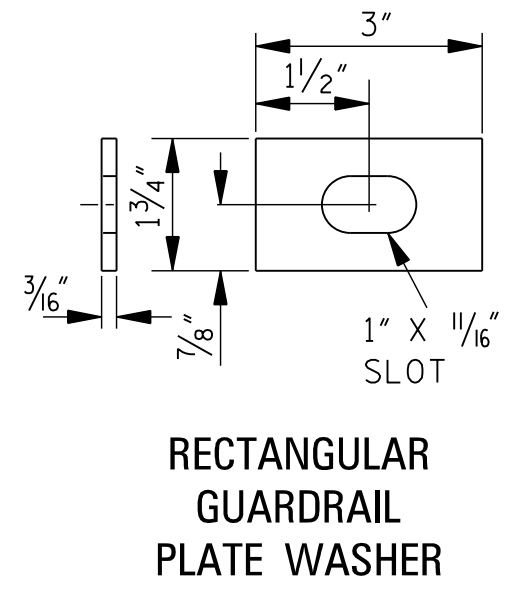
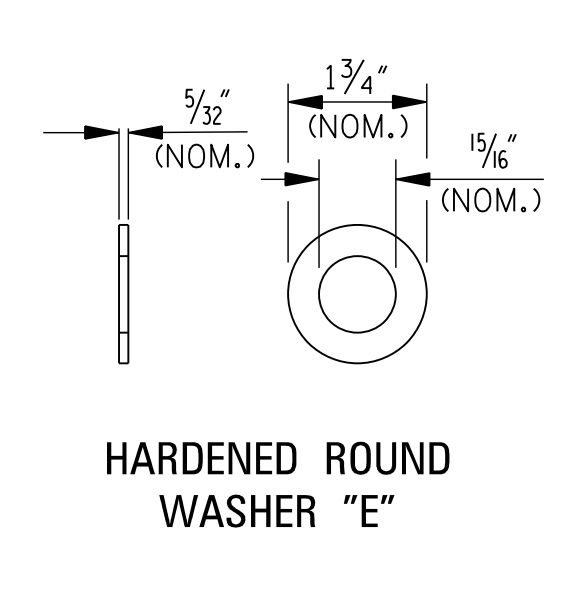
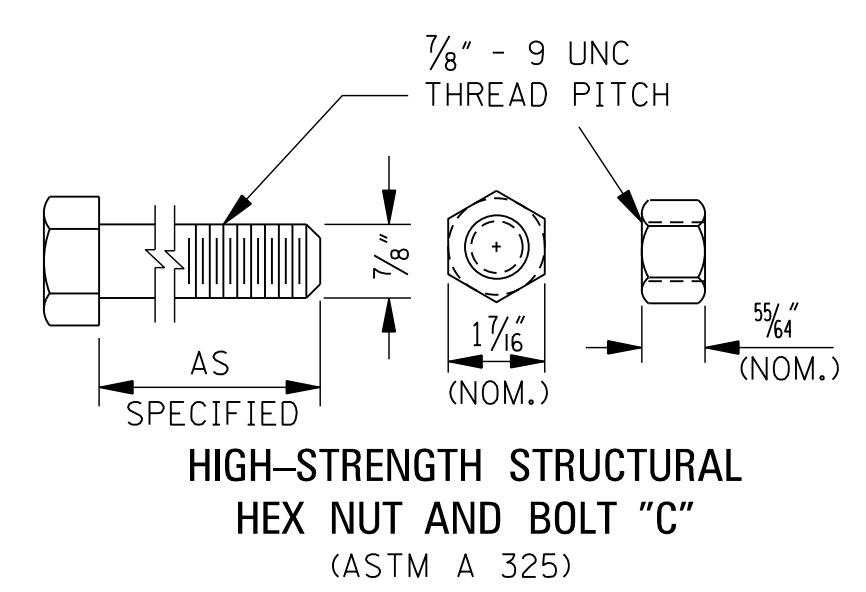
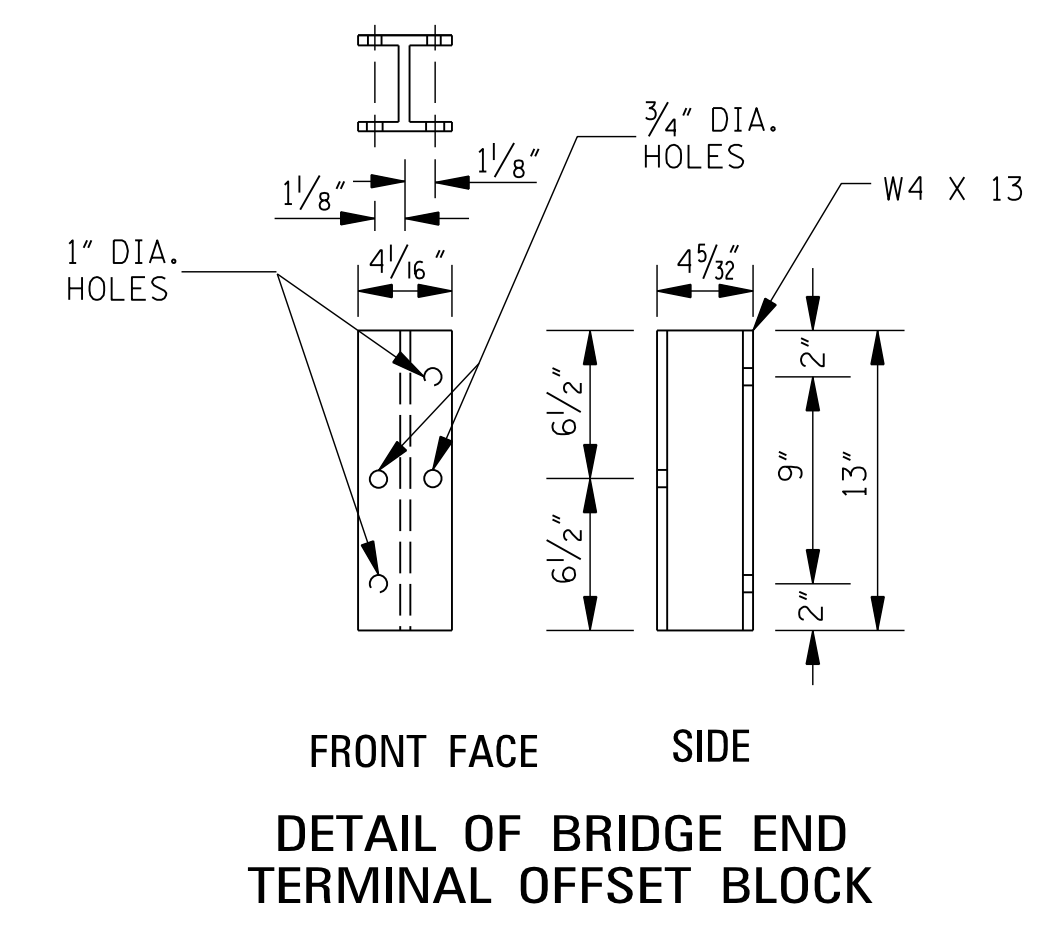
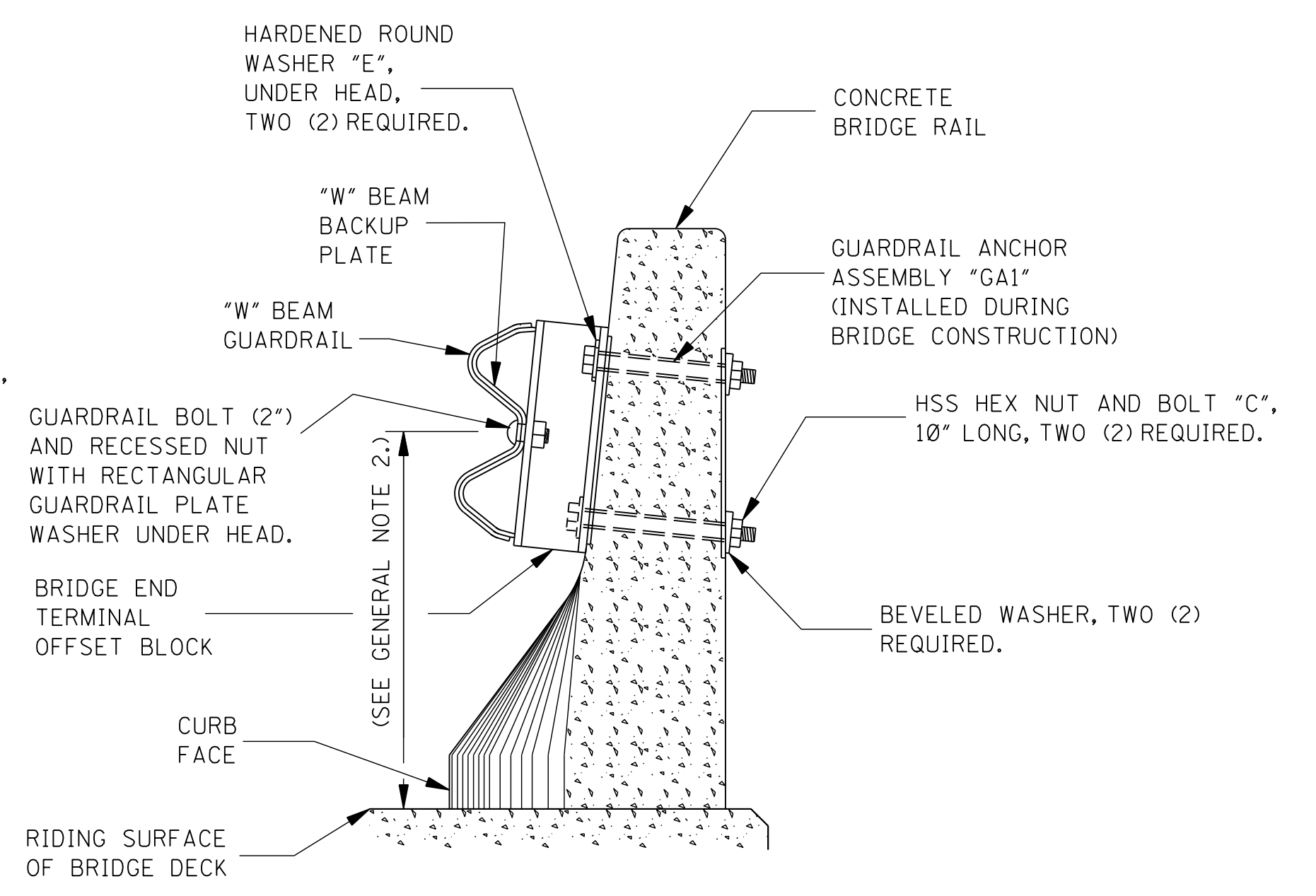
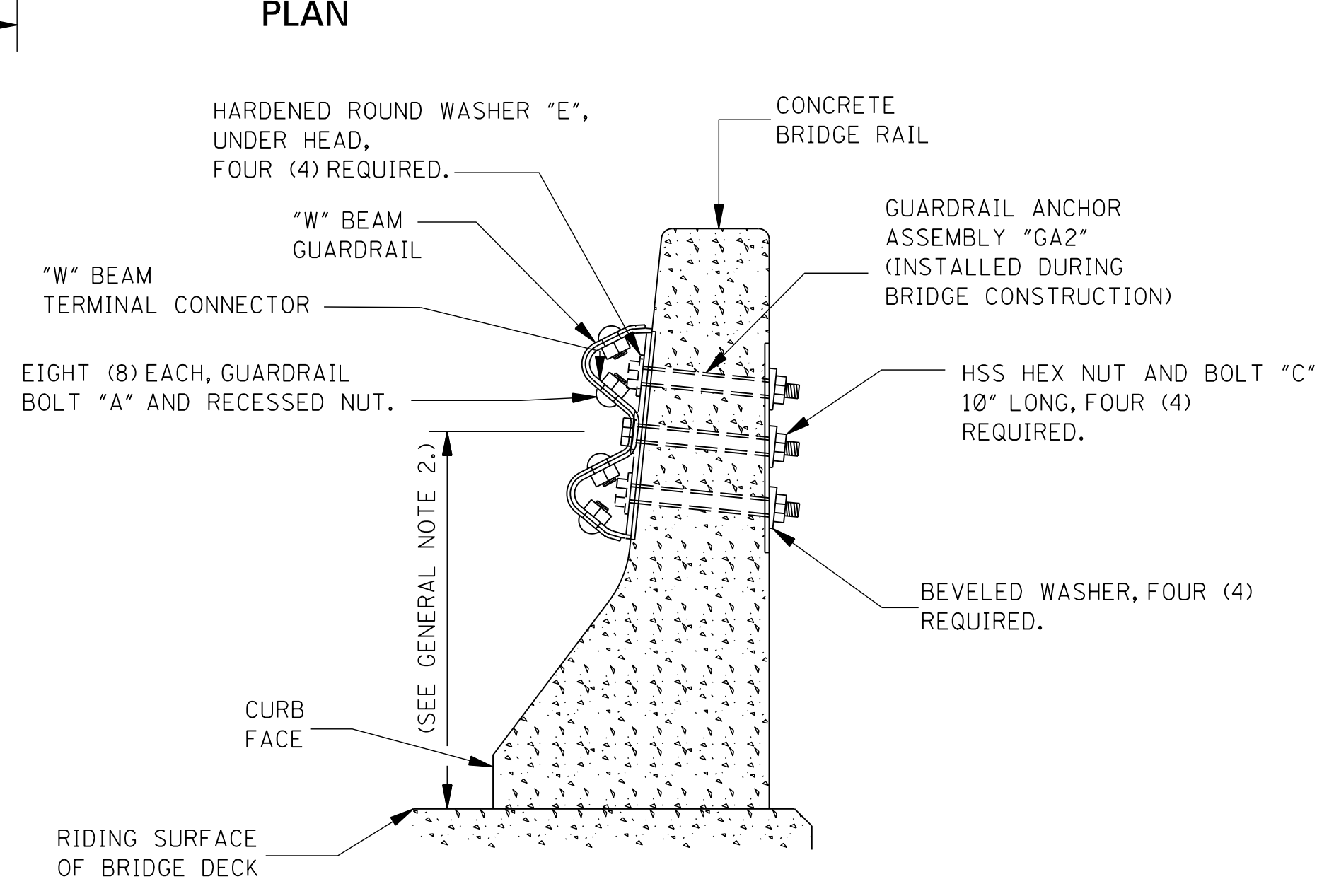
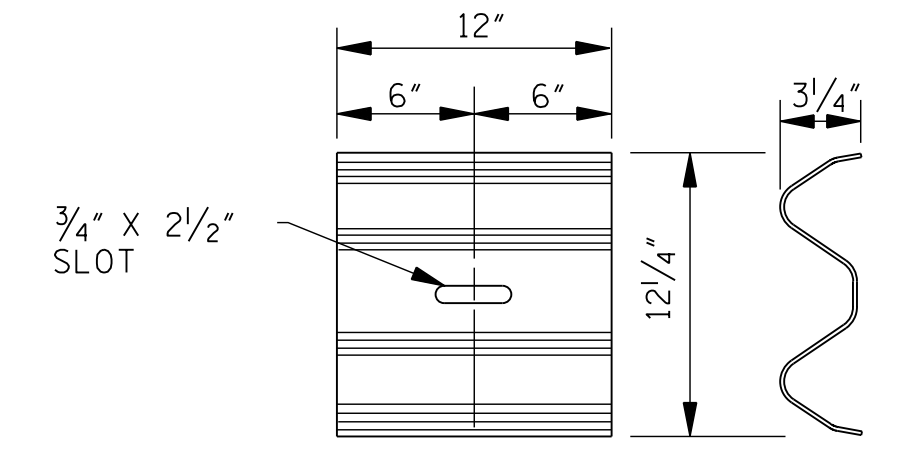
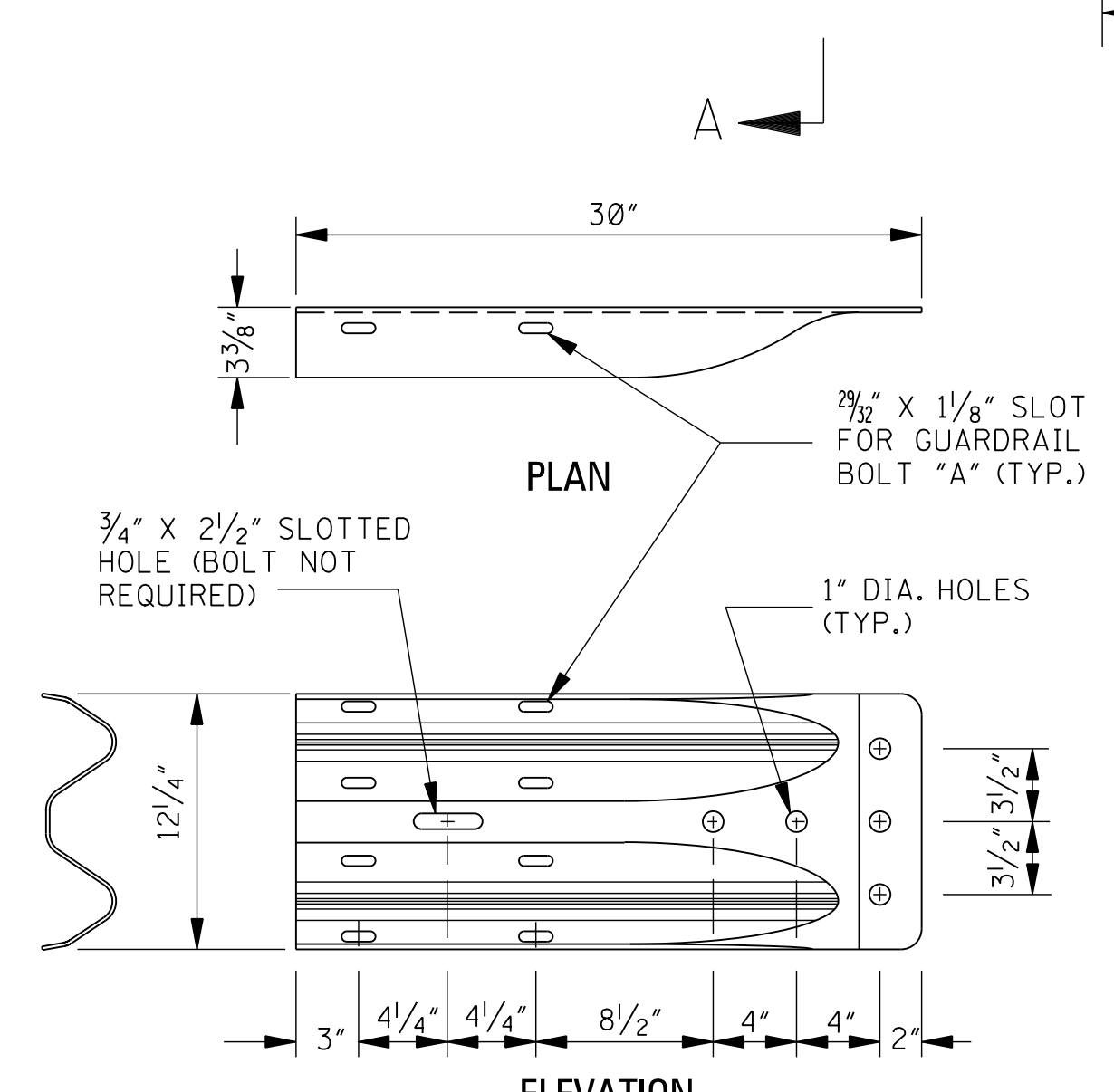
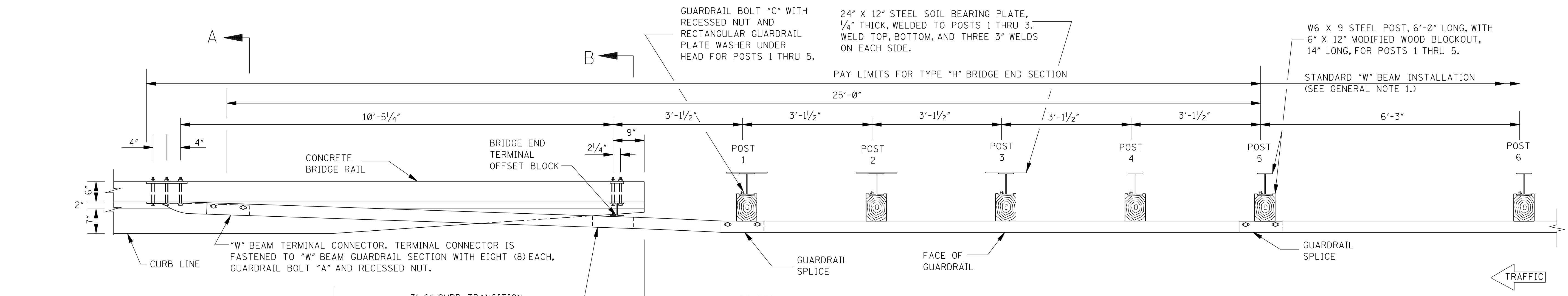
GENERAL NOTES:

- ALL NOTES AND DETAILS PERTAINING TO NORMAL "W" BEAM GUARDRAIL INSTALLATION NOT SPECIFICALLY MODIFIED ON THIS SHEET WILL BE FOUND ON EITHER SHEET GR-1 (WOOD POSTS) OR GR-1B (STEEL POSTS).
- INSTALLATION DETAILS FOR BRIDGE END SECTION GUARDRAIL POSTS WILL BE FOUND ON EITHER SHEET GR-2C (WOOD POSTS) OR GR-2D (STEEL POSTS).
- GUARDRAIL ELEMENTS SHALL BE LAPPED IN THE DIRECTION OF TRAFFIC. THE ONLY EXCEPTION NOTED IS THAT GUARDRAIL SHALL BE LAPPED FOR APPROACHING TRAFFIC ON A BRIDGE WITH 2-WAY TRAFFIC.
- THE HEIGHT OF RAIL AT THE BRIDGE END IS 22" AND WILL BE TRANSITIONED TO 25" AT POST 5.

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
REVISION	
DATE	ISSUE DATE: AUGUST 01, 2017

**GUARDRAIL:
BRIDGE END SECTION
TYPE "D" MODIFIED**

WORKING NUMBER GR-2B
SHEET NUMBER 6206



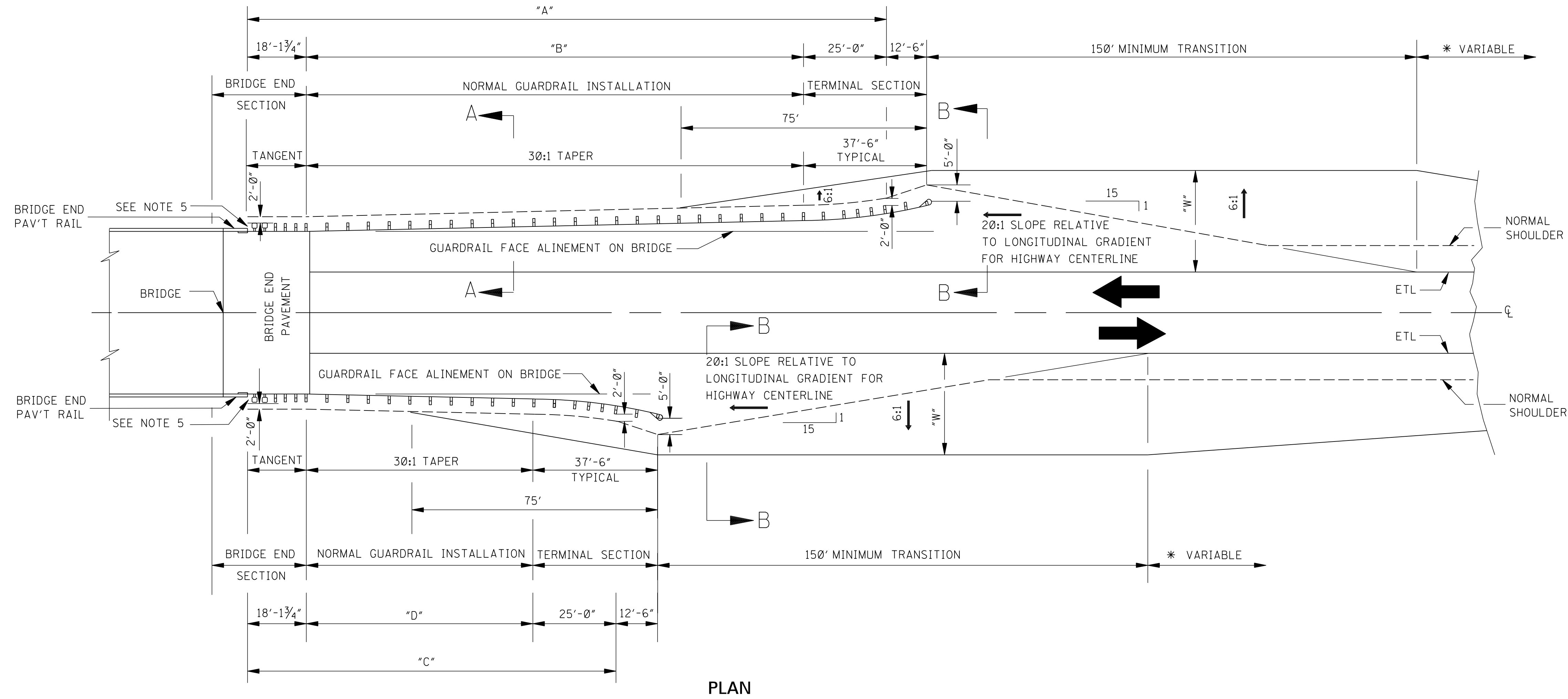
- GENERAL NOTES:**
- ALL NOTES AND DETAILS PERTAINING TO NORMAL "W" BEAM GUARDRAIL INSTALLATION NOT SPECIFICALLY MODIFIED ON THIS SHEET WILL BE FOUND ON SHEET GR-1B (STEEL POSTS).
 - THE HEIGHT OF RAIL AT THE BRIDGE END IS 21" AND WILL BE TRANSITIONED TO 25" AT POST 5.
 - GUARDRAIL ELEMENTS SHALL BE LAPPED IN THE DIRECTION OF TRAFFIC. THE ONLY EXCEPTION NOTED IS THAT GUARDRAIL SHALL BE LAPPED FOR APPROACHING TRAFFIC ON A BRIDGE WITH 2-WAY TRAFFIC.

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN		
REVISION			
DATE	ISSUE DATE: AUGUST 01, 2017		

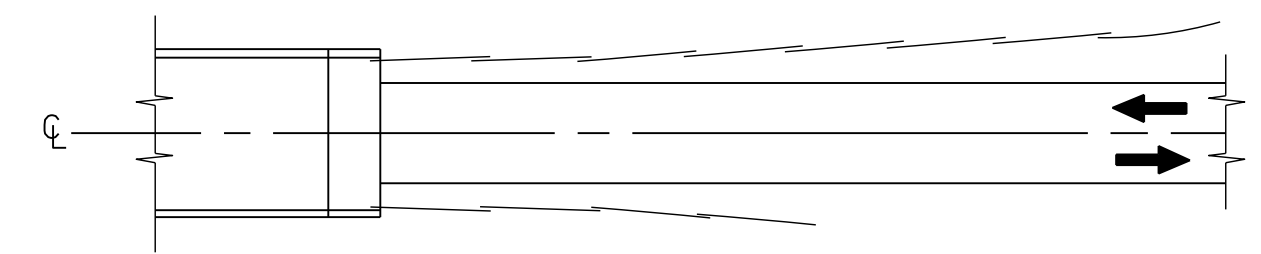
**GUARDRAIL:
BRIDGE END SECTION
TYPE "H"
(STEEL POSTS)**

WORKING NUMBER	GR-2D
SHEET NUMBER	6208

* NOTE: IF FORESLOPE, SHOWN ELSEWHERE ON PLANS, IS OTHER THAN 6:1, TRANSITION WILL OCCUR IN AREA SHOWN.



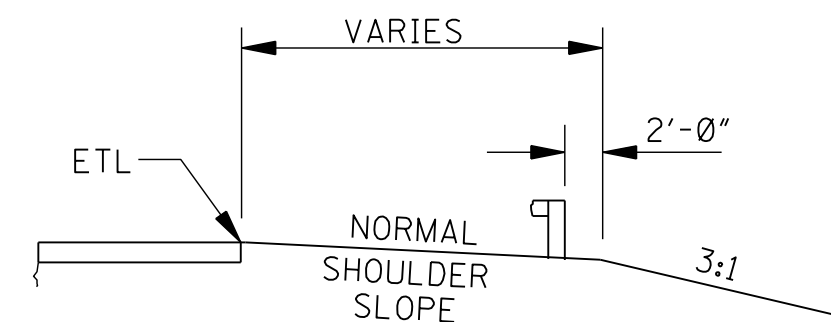
PLAN



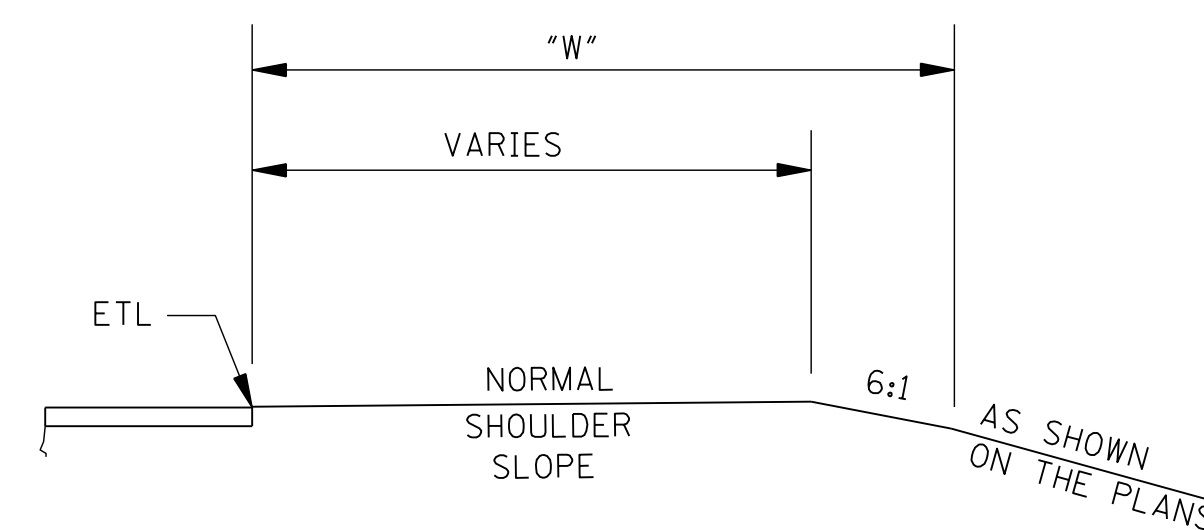
DETAIL OF GUARDRAIL SECTION LAPS

GENERAL NOTES:

- VALUES FOR "A", "B", "C" AND "D" WILL BE SHOWN ELSEWHERE ON THE PLANS.
- FOR DETAILS PERTINENT TO INSTALLATION OF THE TERMINAL SECTION, SEE MANUFACTURER'S SPECIFICATIONS AND DRAWINGS OR ELSEWHERE ON PLANS
- GUARDRAIL SECTIONS SHALL BE LAPPED IN THE DIRECTION OF TRAFFIC APPROACHING THE BRIDGE.
- THE OVERALL LENGTH OF GUARDRAIL IS MEASURED FROM THE CONNECTING END ON THE BRIDGE.
- IN THE ABSENCE OF A BRIDGE END PAVEMENT RAIL, CONNECT THE BRIDGE END SECTION TO THE BRIDGE RAIL (SEE WK. NOS. GR-2 THRU GR-2C). THE SHOULDER WIDTH AT THE BRIDGE END PAVEMENT RAIL OR BRIDGE END RAIL SHOULD BE SUFFICIENTLY WIDE TO PROVIDE A MINIMUM OF 2'-0" BEHIND THE BACK OF POST BEFORE THE SLOPE BREAK (HINGEPOINT).
- TYPE, DETAILS AND LIMITS OF GUARDRAIL BRIDGE END SECTION WILL BE SHOWN ELSEWHERE ON THE PLANS.
- W = SHOULDER WIDTH + FORESLOPE WIDTH. DIMENSIONS FOUND ELSEWHERE ON THE PLANS.



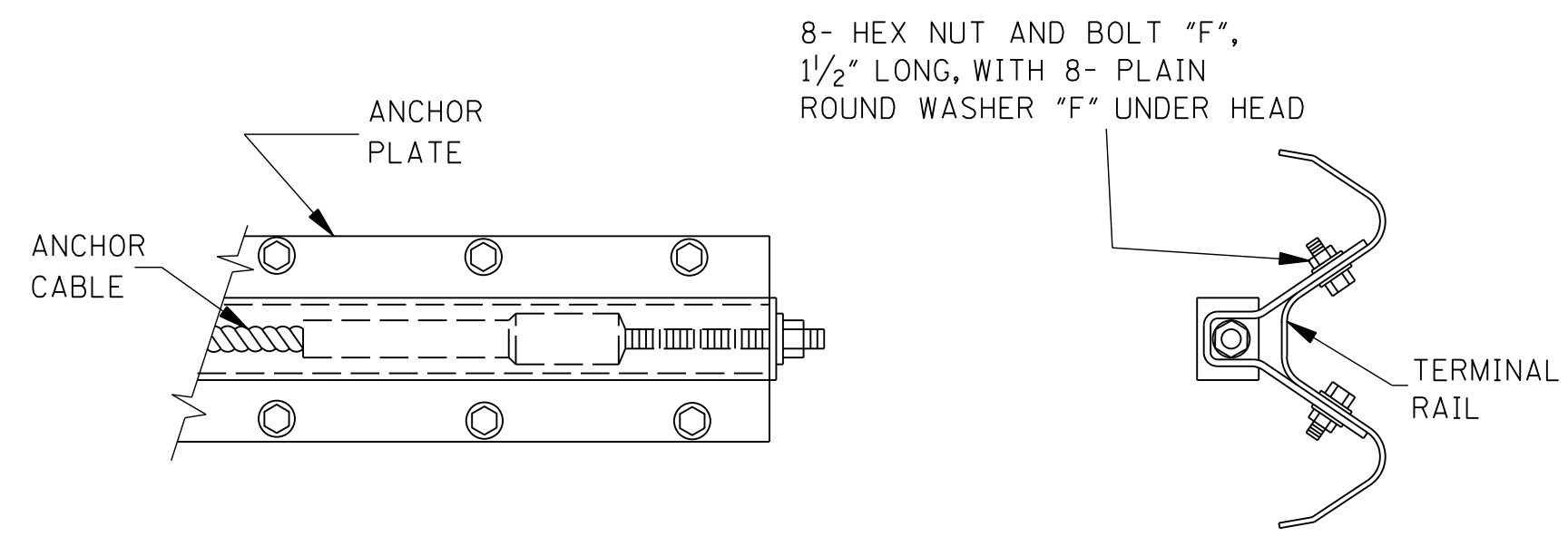
SECTION A-A



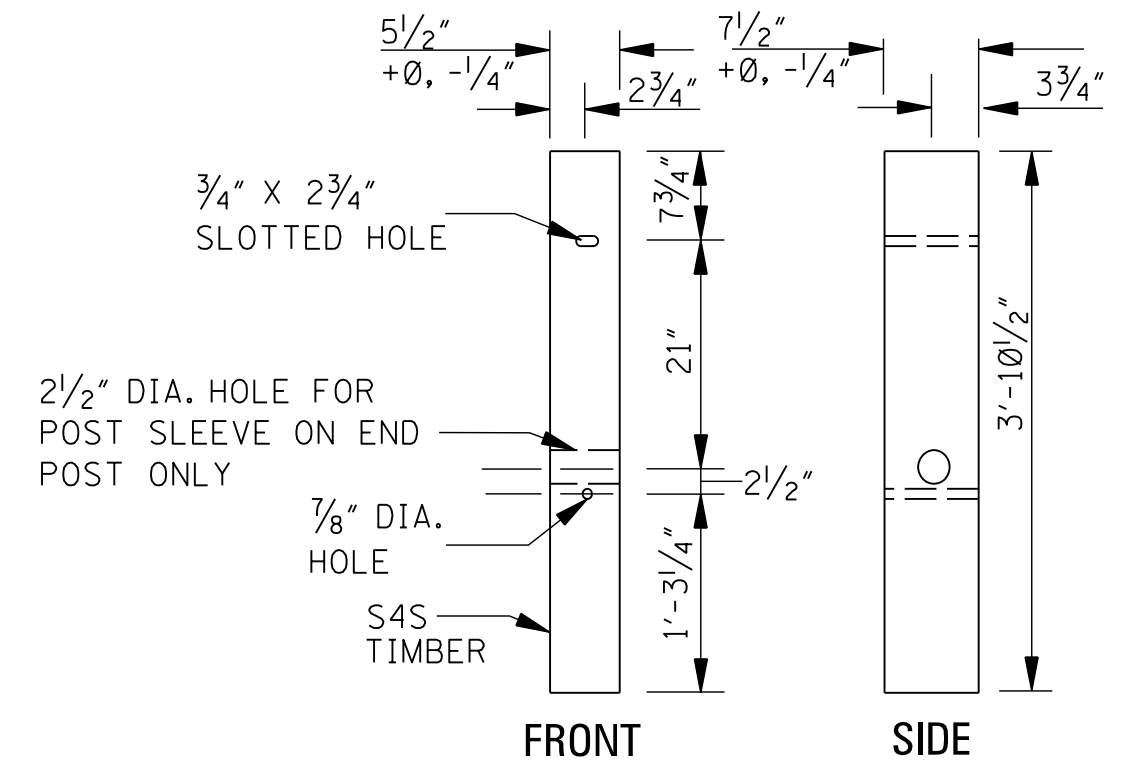
SECTION B-B

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		GUARDRAIL: TYPICAL INSTALLATION AT BRIDGE APPROACHES FOR 2-LANE, 2-WAY HIGHWAY	
DATE			
ISSUE DATE:		AUGUST 01, 2017	
WORKING NUMBER		GR-4A	
SHEET NUMBER		6215	



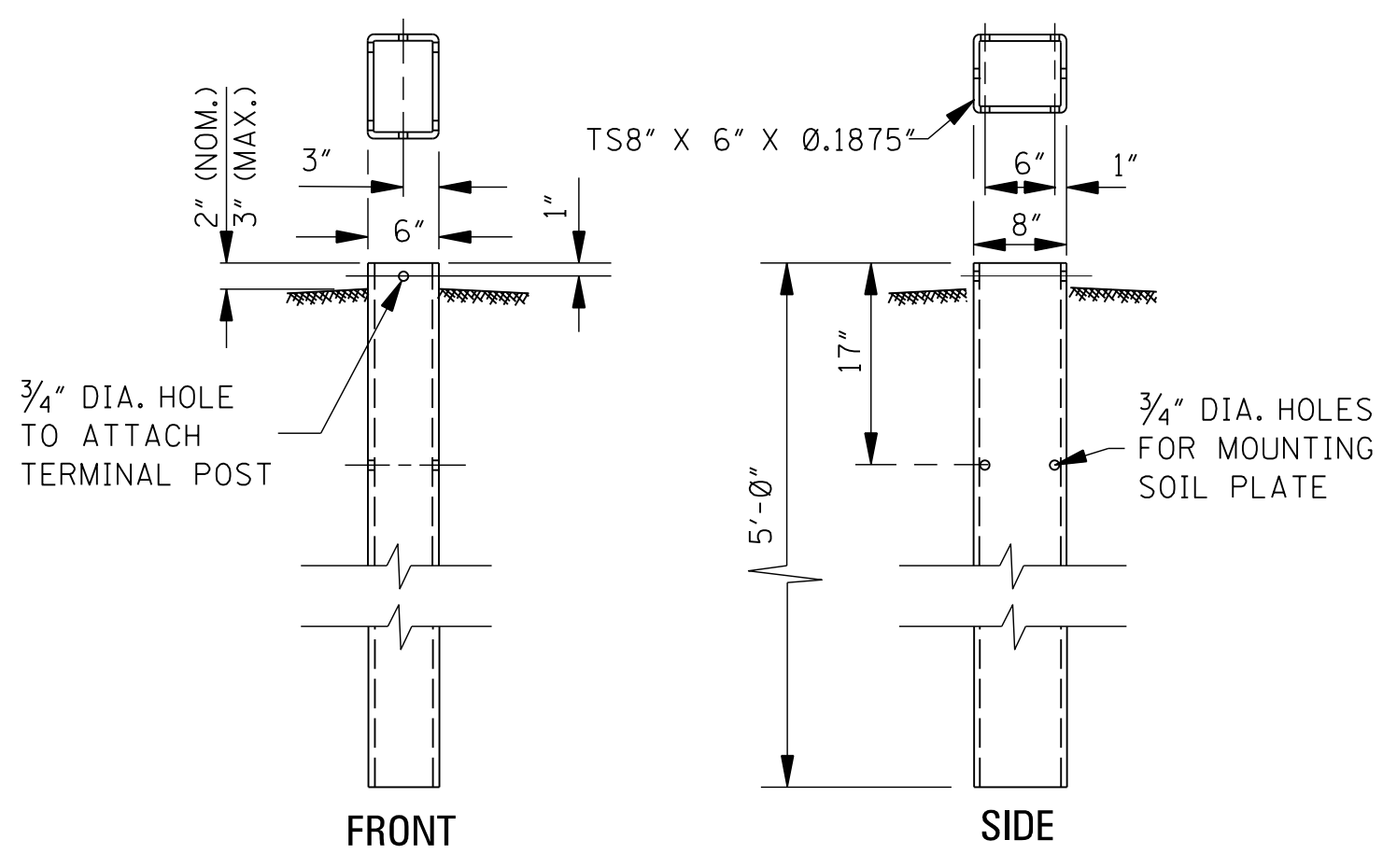


ANCHOR PLATE ASSEMBLY DETAILS



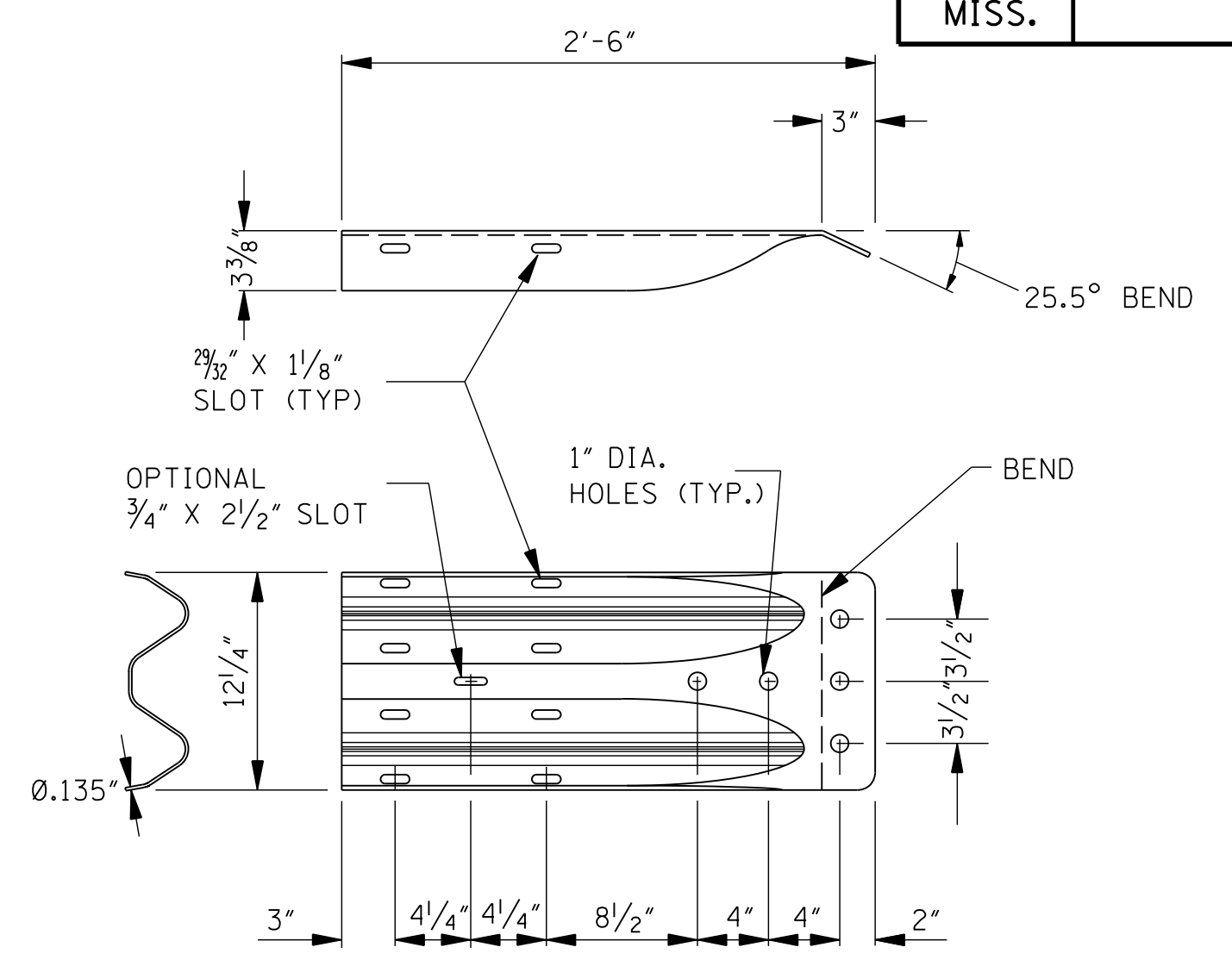
TERMINAL POST FOR FOUNDATION TUBE INSTALLATION

NOTE: TERMINAL POST SHALL BE MADE OF S4S TIMBER WITH 2 STRESS GRADE OF 1200 lbs/in.



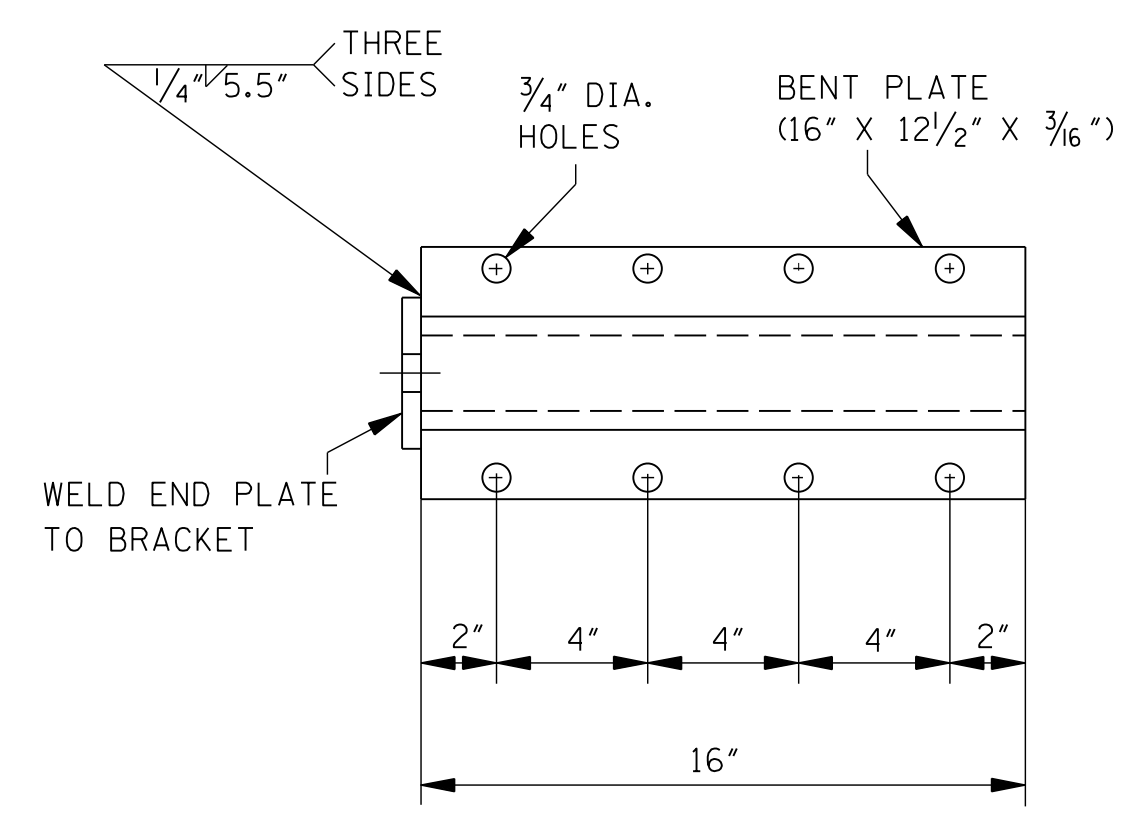
STEEL TUBE ANCHOR

NOTE: TERMINAL POST SHALL BE ABLE TO SLIDE INTO THE TOP OF THIS SECTION SO THE ACTUAL INSIDE DIMENSIONS OF THIS GALVANIZED TUBE CANNOT BE LESS THAN 7 1/2" X 5 1/2".

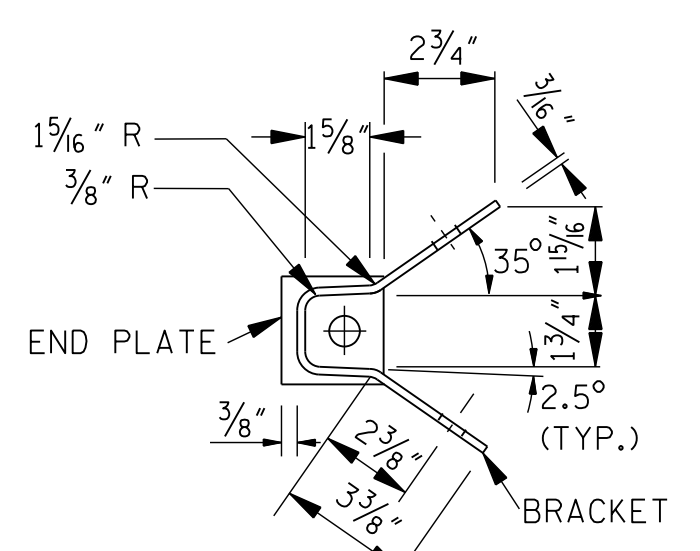


"W" BEAM TERMINAL CONNECTOR PLATE

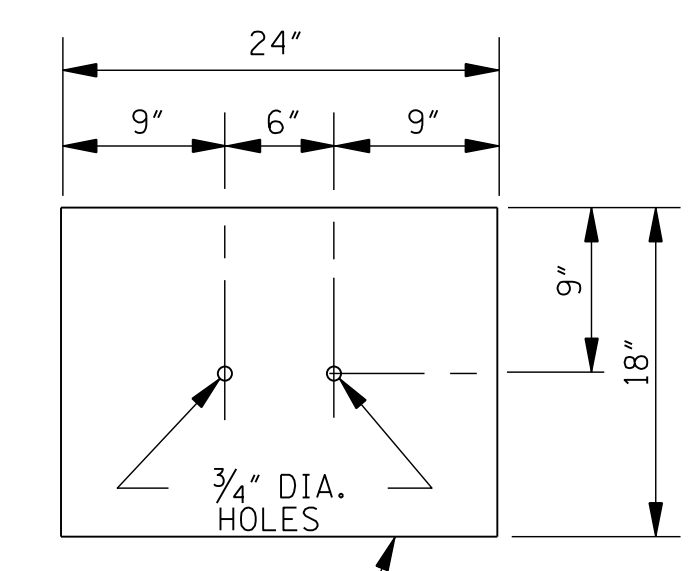
- NOTES:
1. THE "W" BEAM TERMINAL CONNECTOR SHALL BE AASHTO M 180 CORRUGATED SHEET STEEL, CLASS B, TYPE 1.
 2. SPLICE-BOLT SLOTS MAY ALSO BE ORIENTED AT 50° (ON THE FLAT) INSTEAD OF 0° AS SHOWN.



ANCHOR PLATE

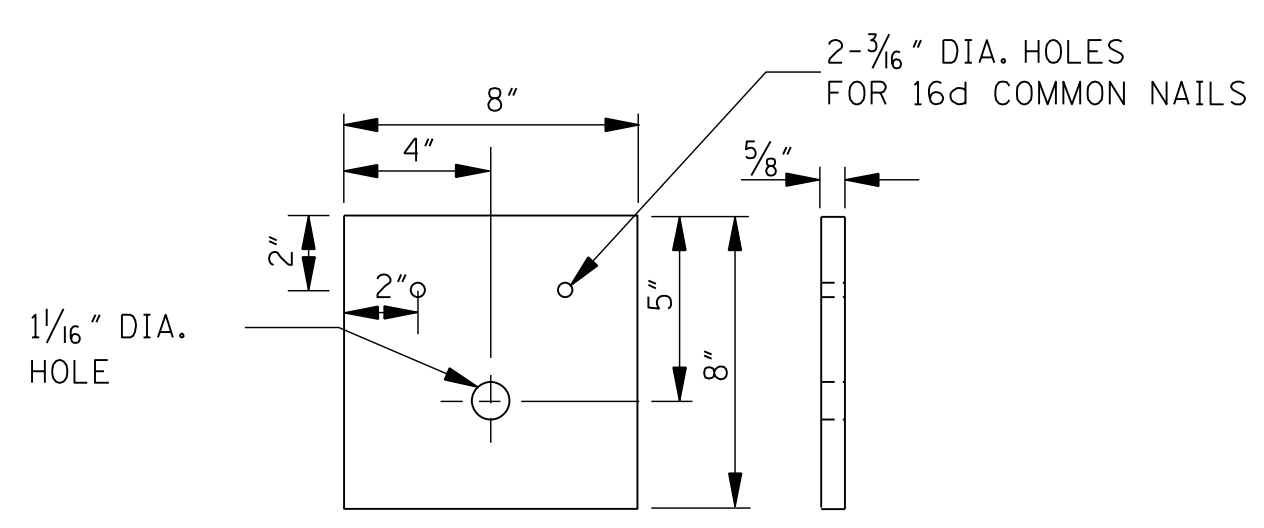


BRACKET

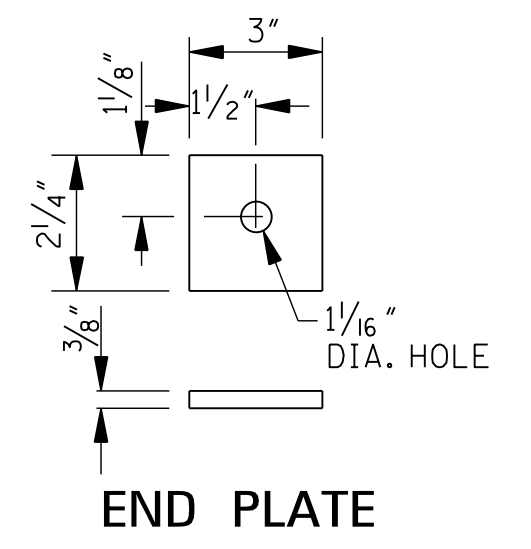


SOIL PLATE

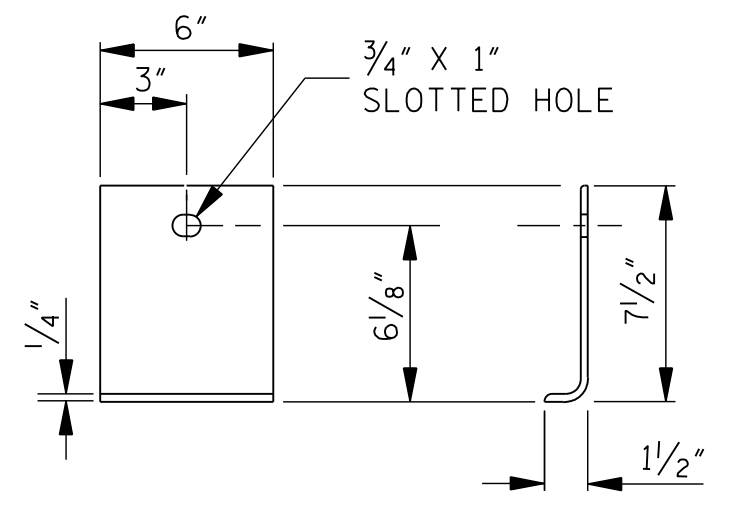
NOTE: 2 REQUIRED



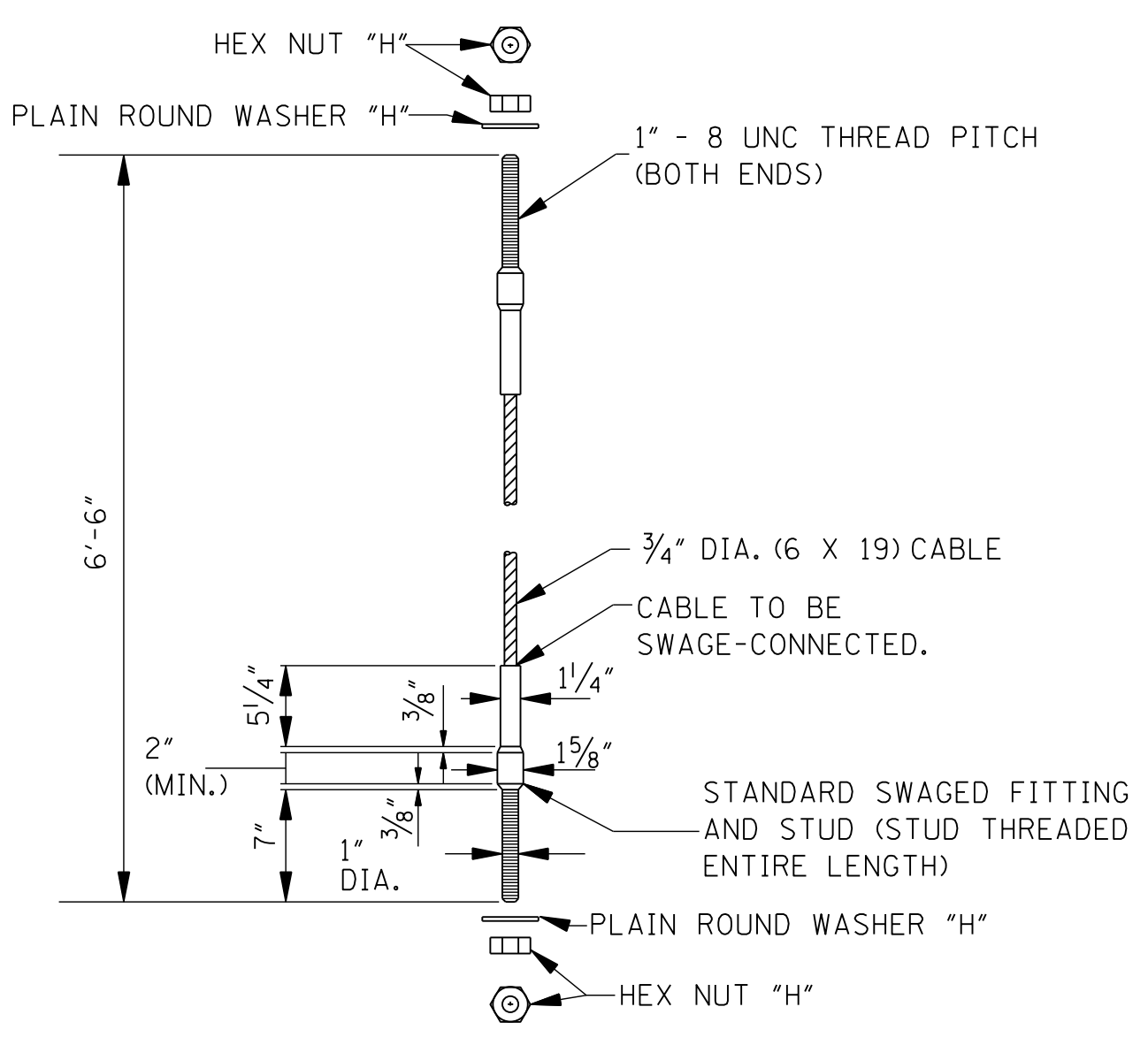
BEARING PLATE



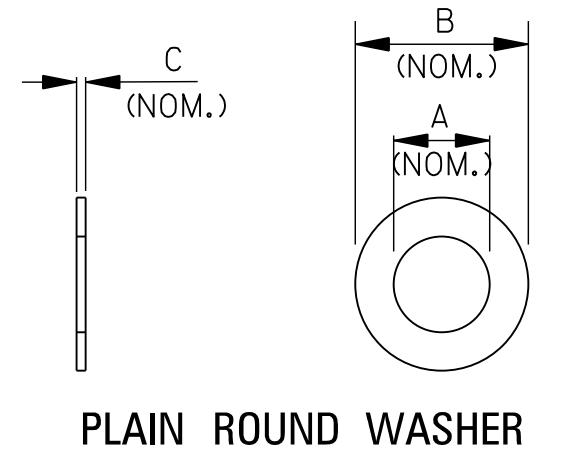
END PLATE



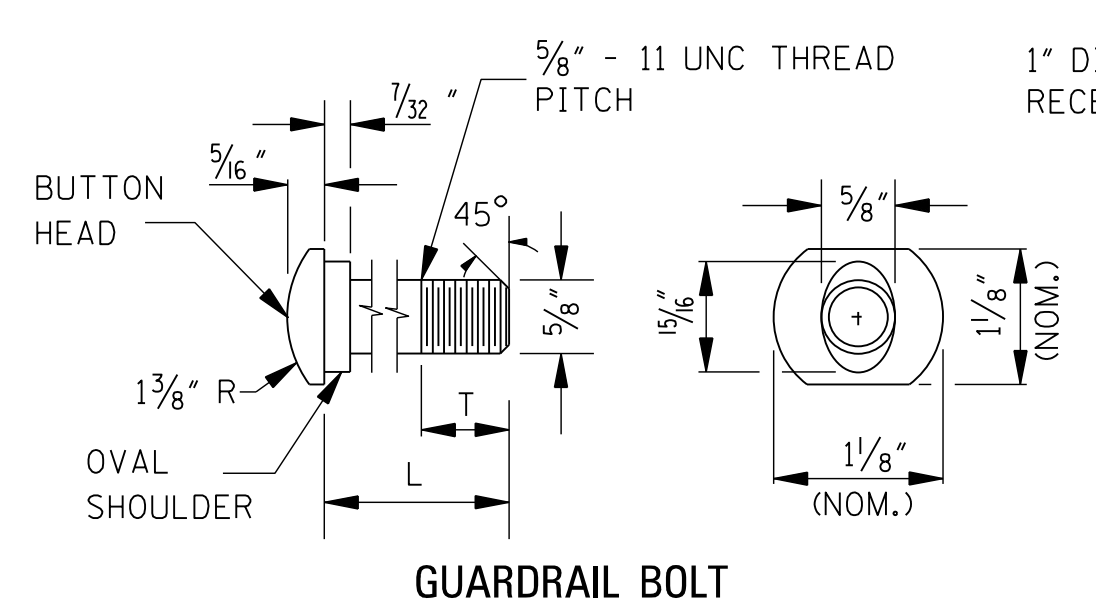
SHELF ANGLE BRACKET



CABLE ANCHOR ASSEMBLY



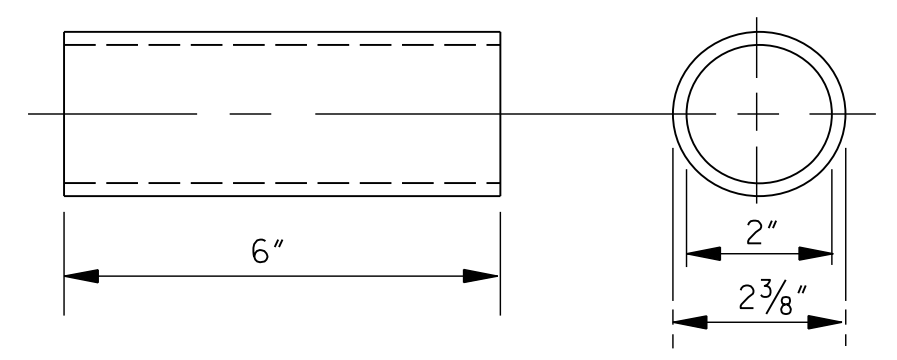
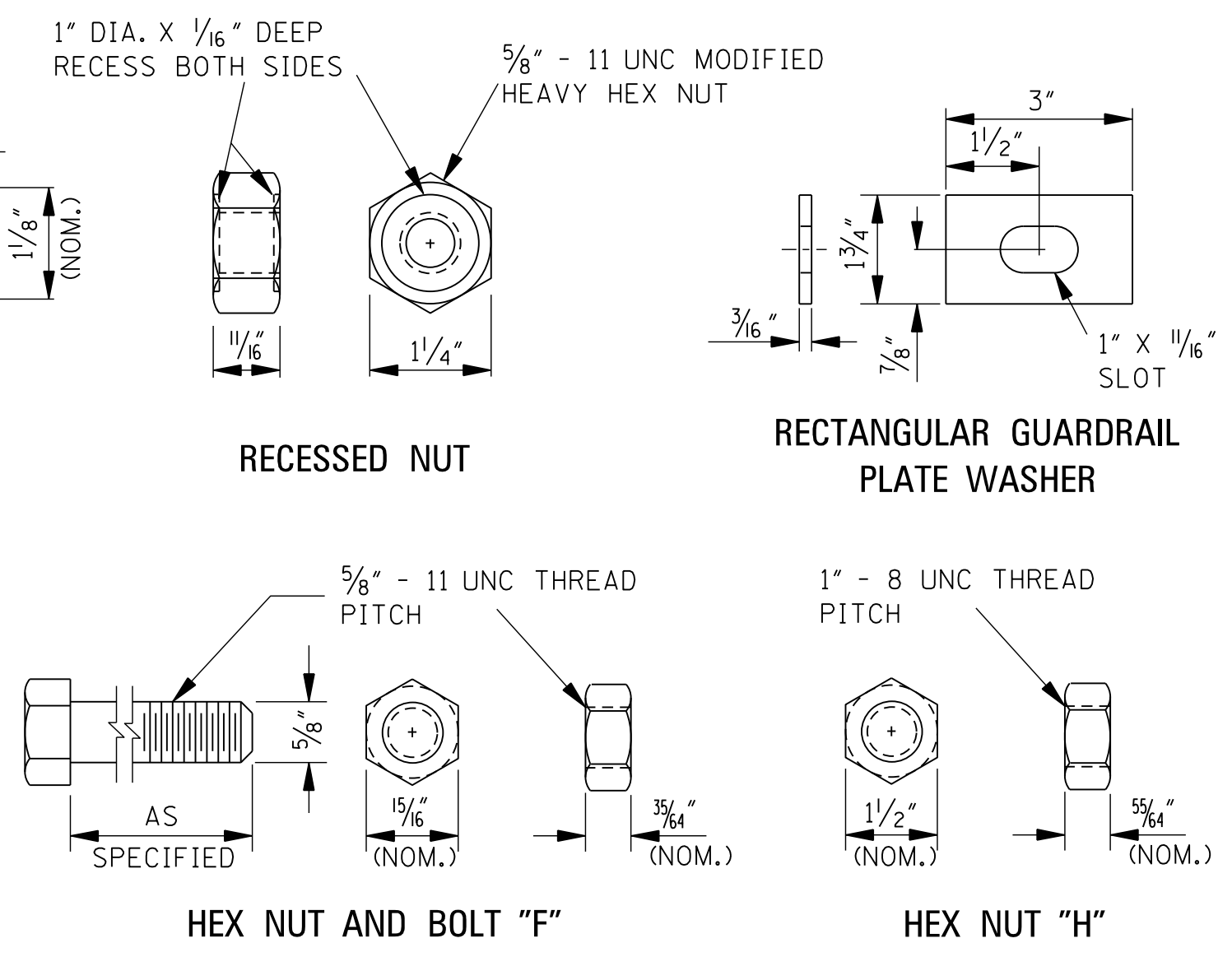
WASHER	A (NOM.)	B (NOM.)	C (NOM.)
"F"	1 1/16"	1 3/4"	9/64"
"H"	1 1/16"	2"	3/64"



BOLT	L	T (MIN.)
"A"	1 1/4"	1"
"B"	12"	4"
"C"	14"	4"
"D"	22"	4"
"E"	24"	4"
"F"	33"	4"
	2"	1 3/4"
	10"	4"
	25"	4"

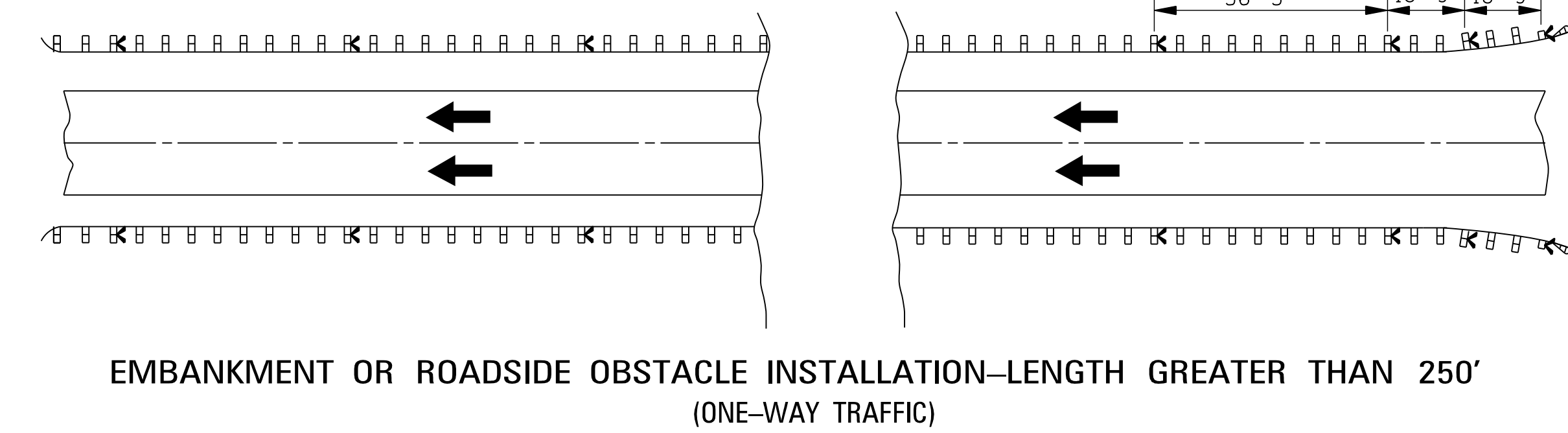
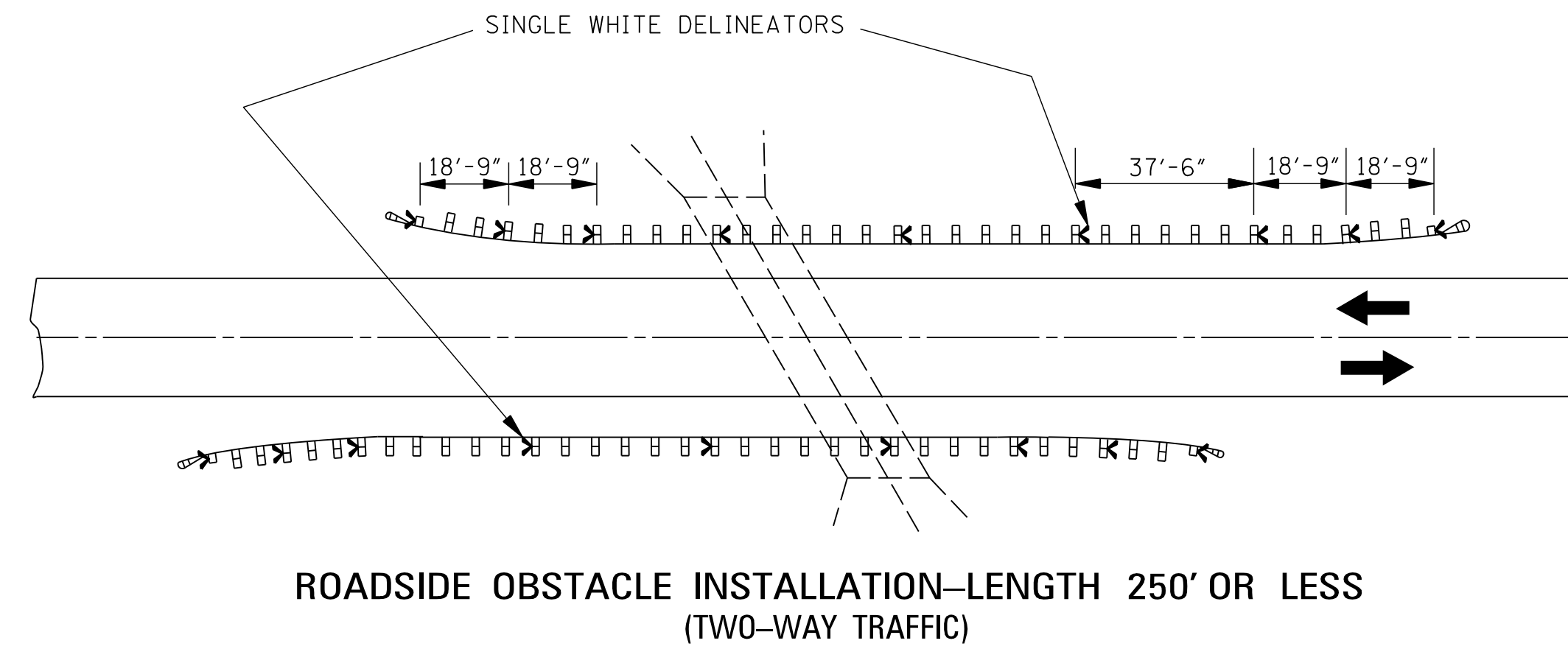
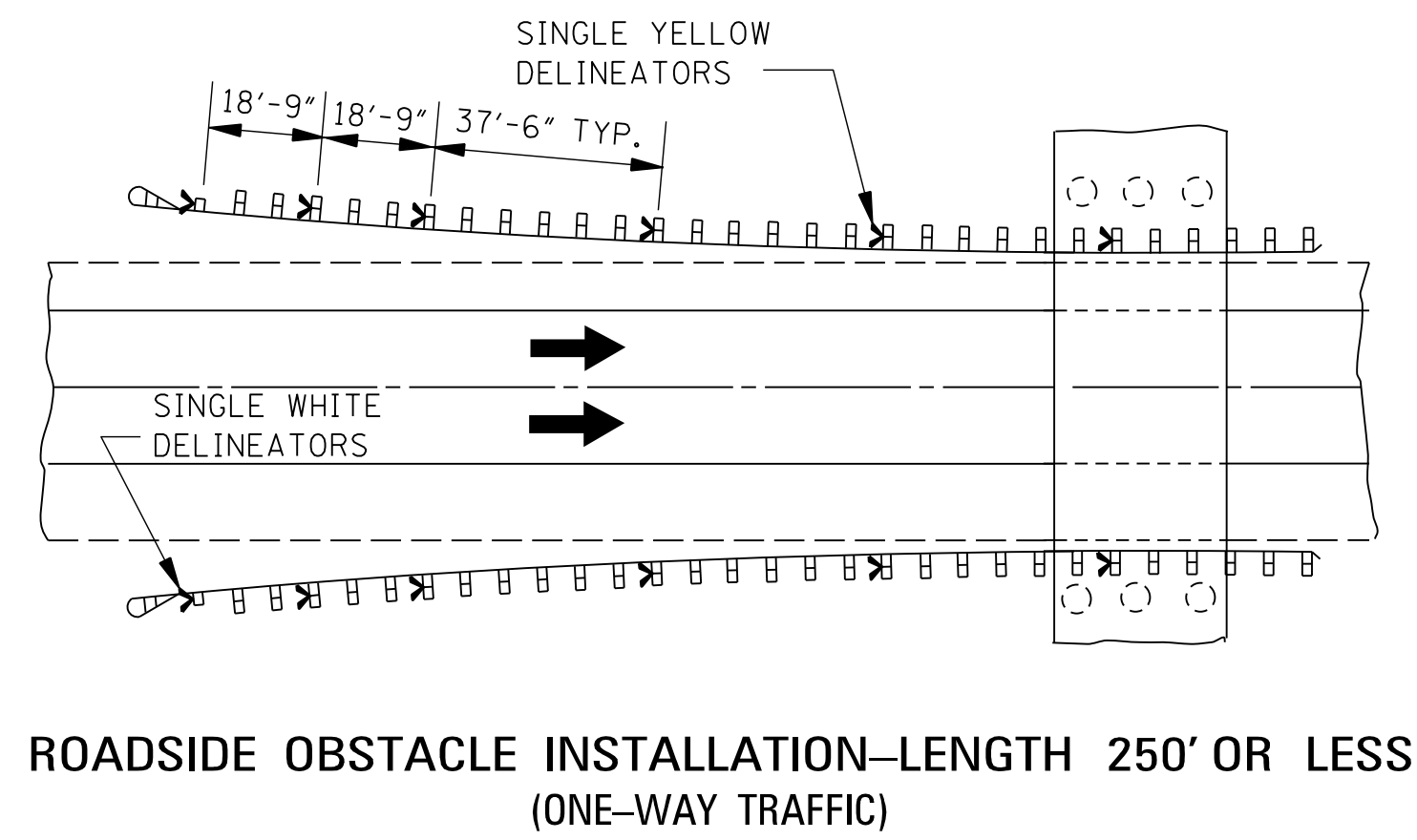
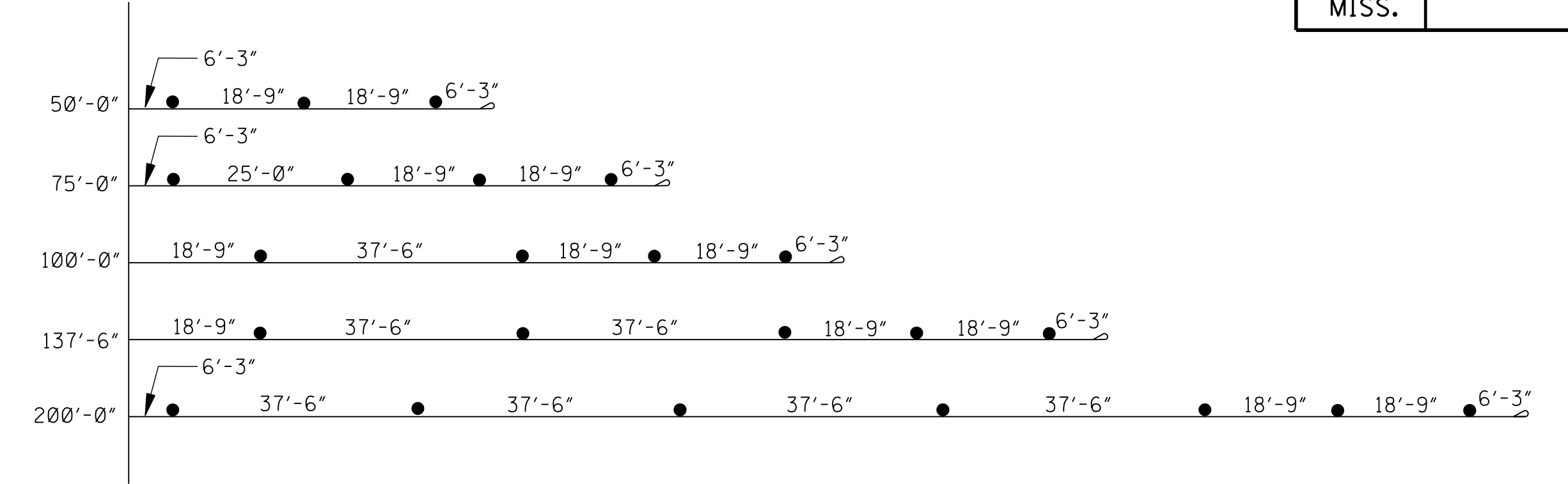
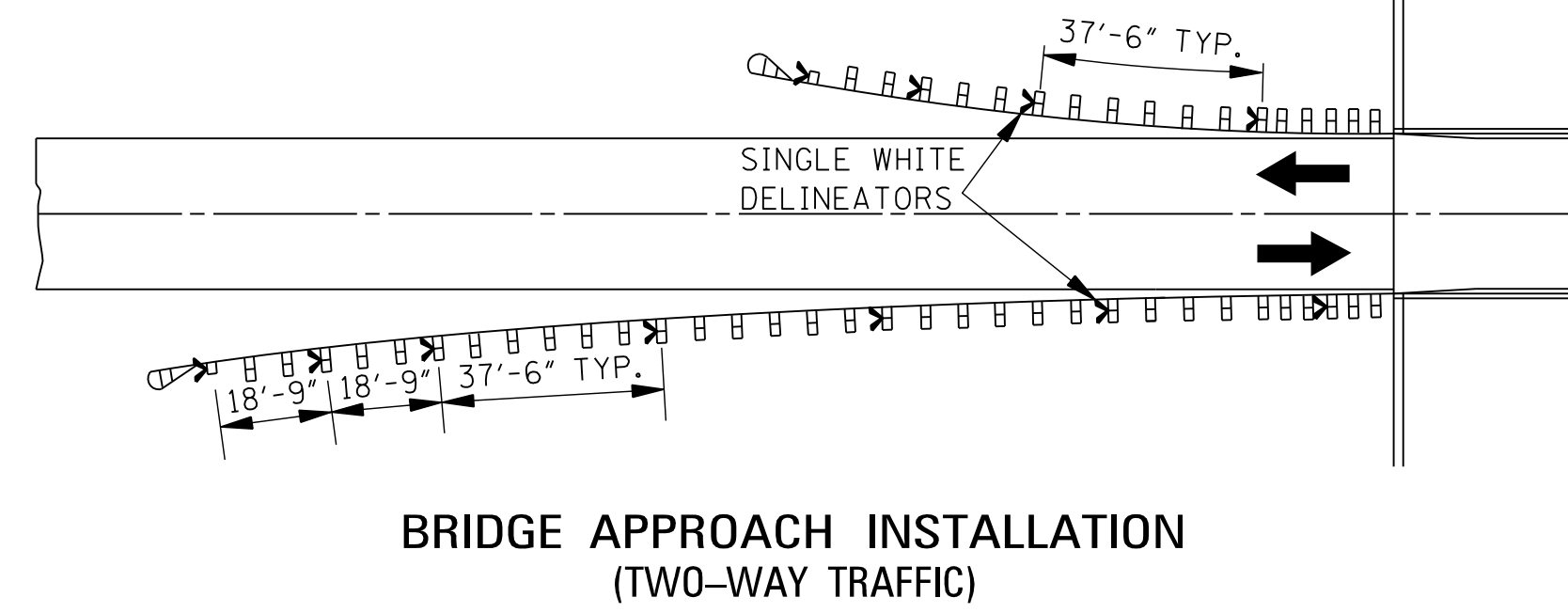
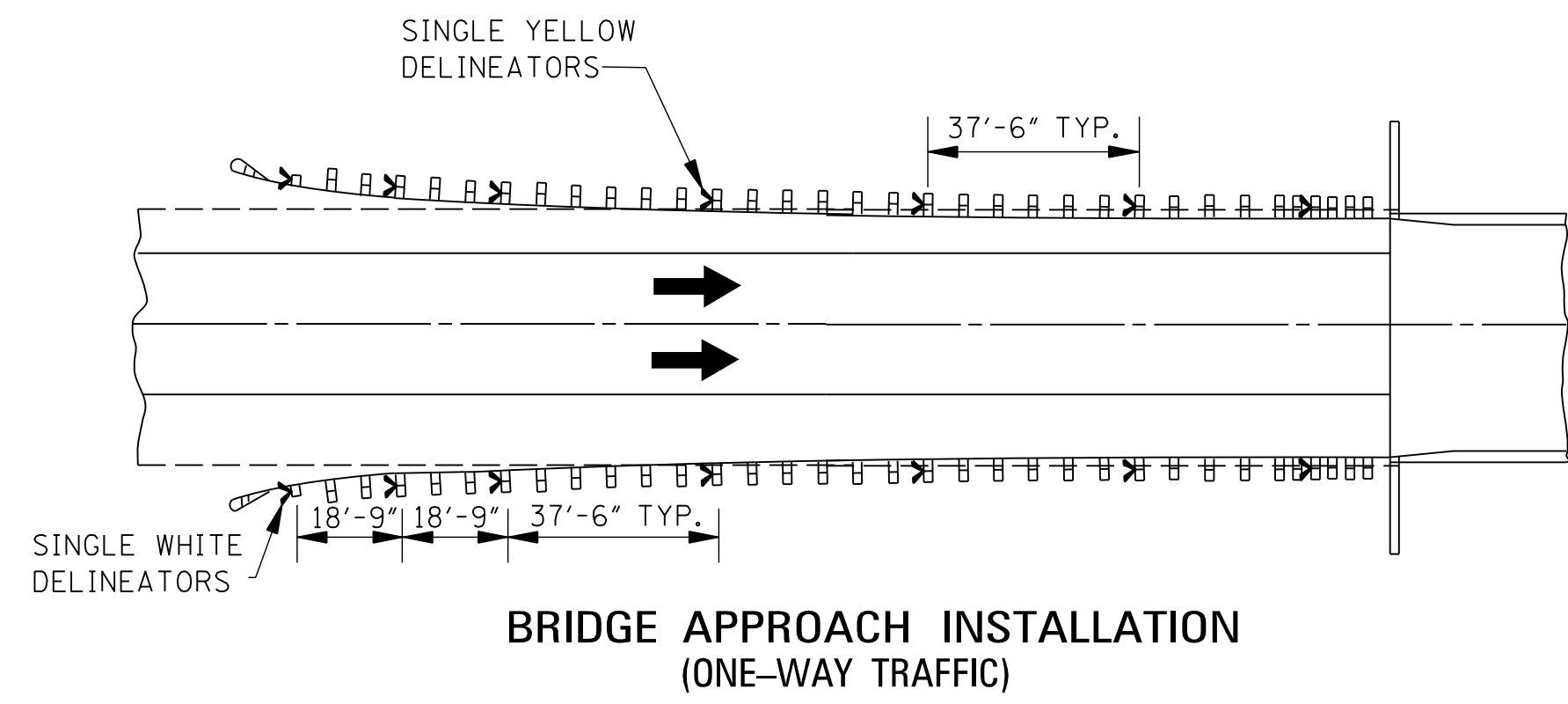
- NOTES:
1. ALL GUARDRAIL BOLTS ARE 5/8" - 11 UNC THREAD PITCH.
 2. IF ANY BOLT EXTENDS MORE THAN 1/4" FROM THE NUT, THE BOLT SHOULD BE TRIMMED BACK.

FASTENER DETAILS



BREAKAWAY TERMINAL POST SLEEVE

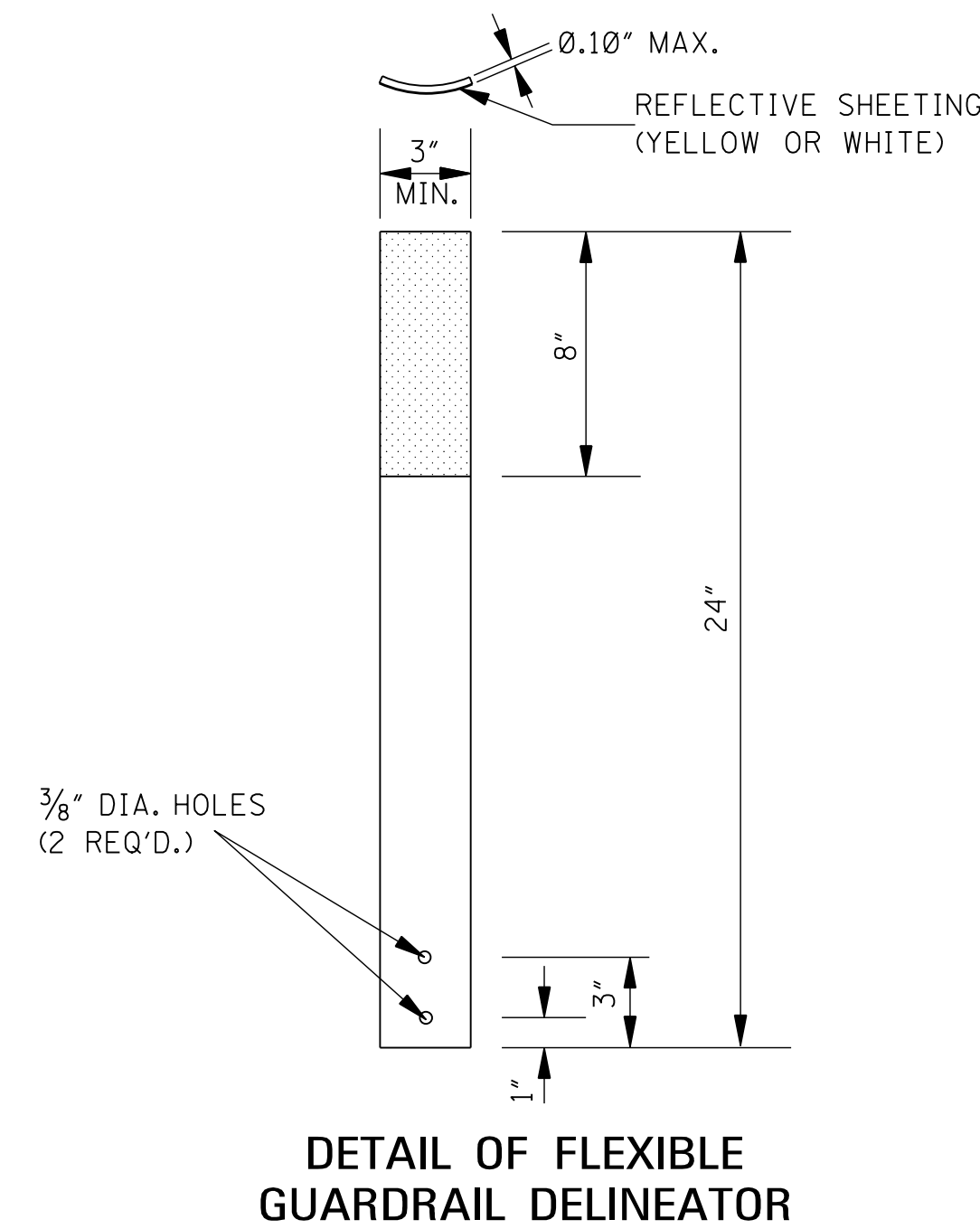
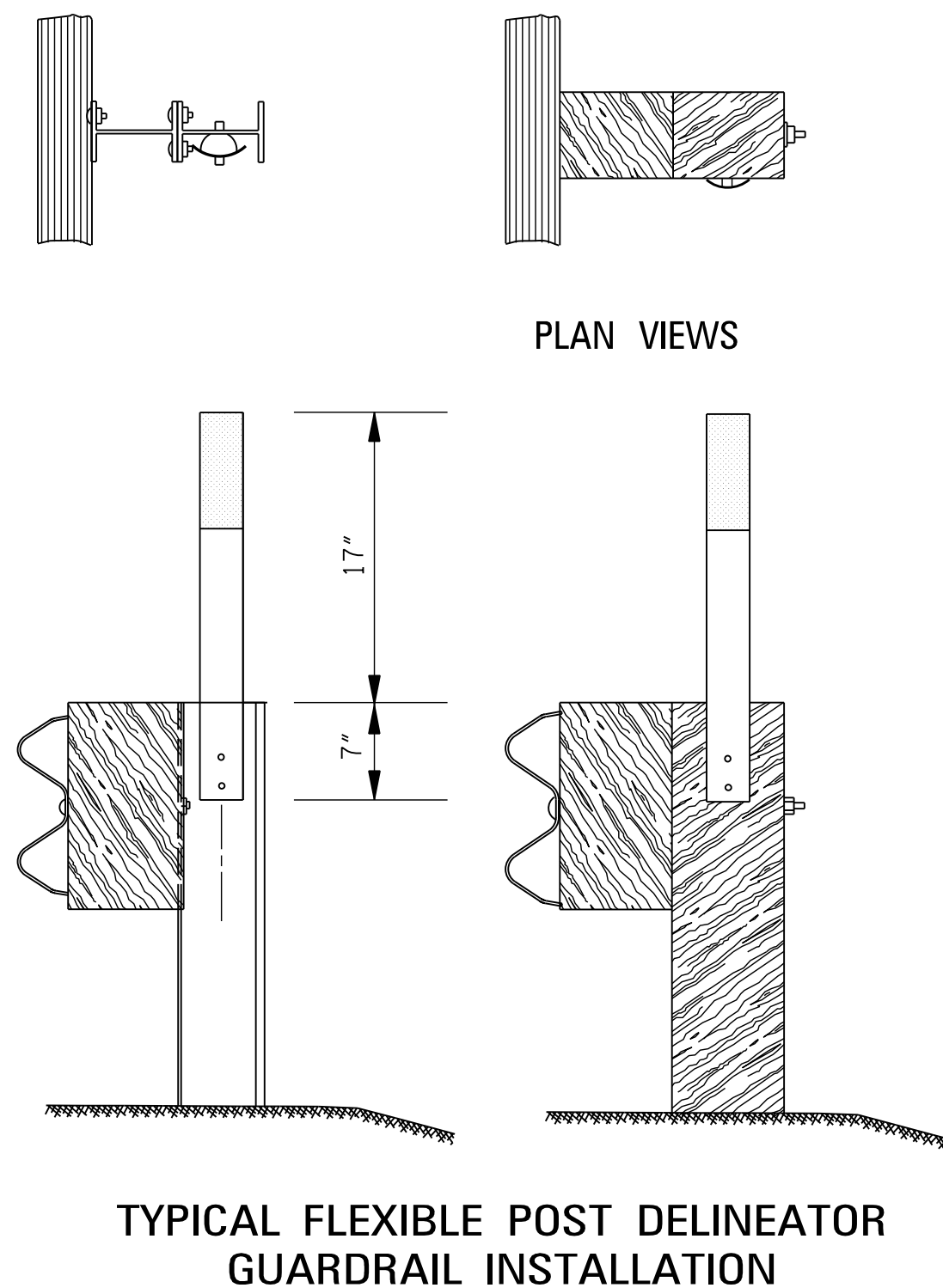
BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		GUARDRAIL: MISCELLANEOUS HARDWARE	
DATE		ISSUE DATE: AUGUST 01, 2017	
REVISION		MDOT WORKING NUMBER GR-HW SHEET NUMBER 6221	



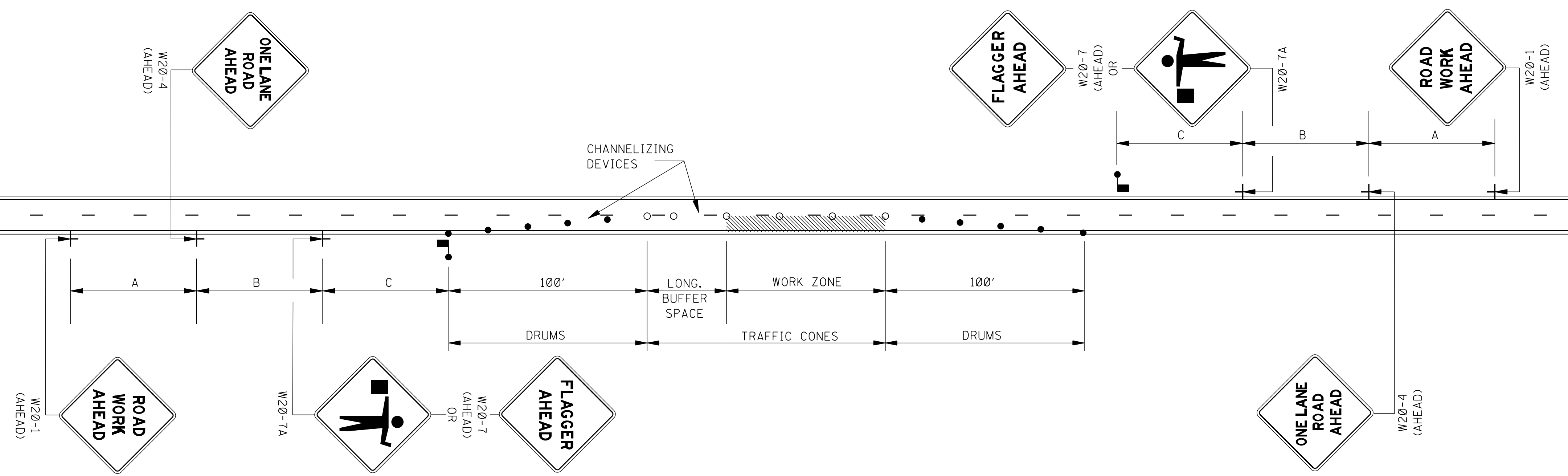
NOTE: ONE-WAY TRAFFIC SHOWN. DELINEATOR SPACING FOR TWO-WAY TRAFFIC SIMILAR. DELINEATOR COLOR WILL BE THE SAME AS THE ADJACENT PAVEMENT EDGE MARKING. THE FIRST THREE (3) MARKERS WILL FACE TRAFFIC IN OFF LANE FOR TWO-WAY TRAFFIC AS SHOWN IN DRAWING FOR OBSTACLE INSTALLATION FOR TWO-WAY TRAFFIC.

GENERAL NOTES:

1. THE UNIT PRICE OF DELINEATOR INCLUDES: COST(S) OF DELINEATOR FACE(S), POST, HARDWARE AND INSTALLATION.
2. DELINEATOR FACE WILL BE ENCAPSULATED LENS REFLECTIVE SHEETING.
3. DELINEATORS FOR GUARDRAIL SHALL BE MOUNTED ON FLEXIBLE POSTS AS FOLLOWS: THE DELINEATOR POSTS WILL BE FROM THE DEPARTMENTS "APPROVED SOURCE OF MATERIALS" AND WILL BE FASTENED TO GUARDRAIL POST IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION.



BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		TYPICAL GUARDRAIL DELINEATION	
DATE			
ISSUE DATE:		AUGUST 01, 2017	
		 WORKING NUMBER SN-8C SHEET NUMBER 6317	



- LEGEND**
- FLAGGER
 - RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
 - TRAFFIC CONES (28" HEIGHT MINIMUM)

GENERAL NOTES:


1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE. FLAGGER STATIONS SHALL BE LOCATED SUCH THAT APPROACHING VEHICLES WILL HAVE SUFFICIENT DISTANCE TO STOP. VALUES IN STOPPING SIGHT DISTANCE COLUMN MAY BE USED AS A MINIMUM FOR THIS DISTANCE.

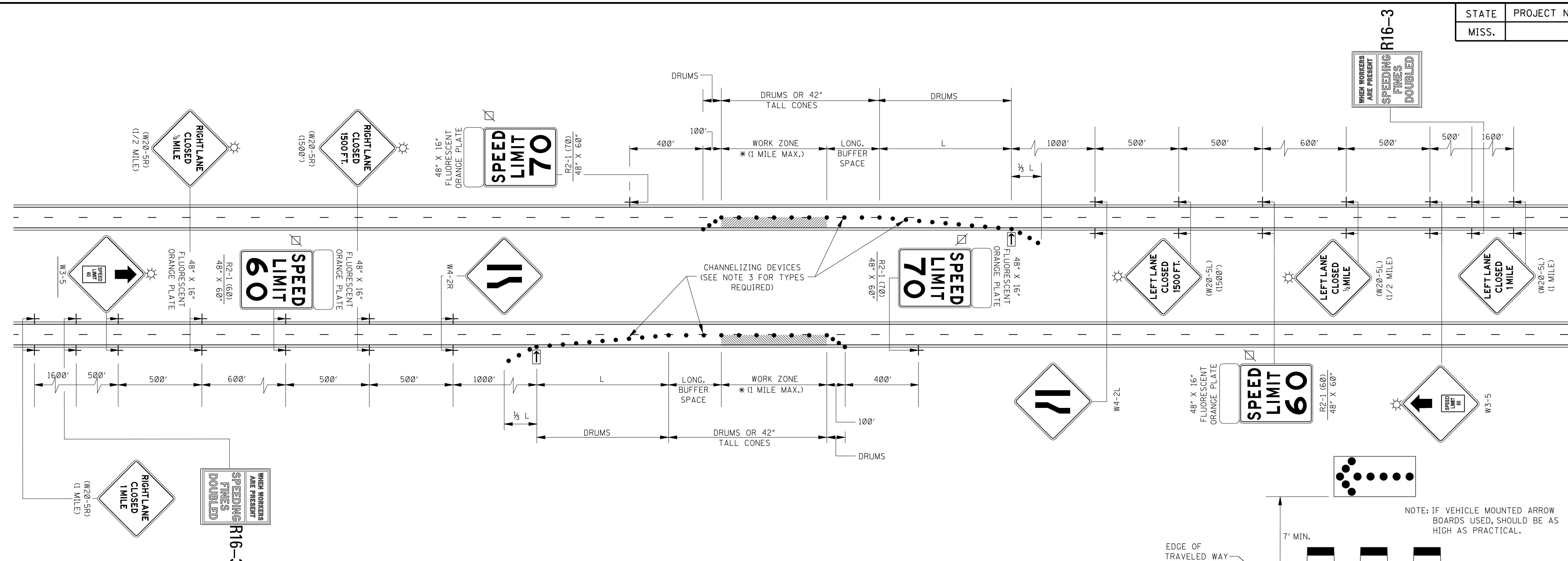
POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft) †	STOPPING SIGHT DISTANCE
	TAPER	ALONG LANE LINE & WORK ZONE		
mph				
25	20	50	55	155
30	20	60	85	200
35	20	70	120	250
40	20	80	170	305
45	20	90	220	360
50	20	100	280	425
55	20	110	335	495
60	20	120	415	570
65	20	130	485	645

† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

2. ALL CHANNELIZING DEVICES SHALL BE A MINIMUM OF 28" IN HEIGHT.
3. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 36" x 36" AND BLACK COPY ON FLUORESCENT ORANGE SHEETING.
4. WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND ALL CHANNELIZING DEVICES SHALL BE MOVED TO THE SHOULDER EDGE.
5. ADDITIONAL FLAGGERS MAY BE NEEDED AS DIRECTED BY THE ENGINEER.
6. WHEN WORK IS REQUIRED AT NIGHT, FLAGGER STATIONS SHALL BE ILLUMINATED.
7. CHANNELIZING DEVICE TYPES FOR:
 - A. APPROACH AND EXIT TAPERS- RETROREFLECTIVE PLASTIC DRUMS
 - B. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT)
8. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

ROAD TYPE	A	B	C
URBAN (35 MPH OR LESS)	100 FT.	100 FT.	100 FT.
URBAN (40 - 70 MPH)	350 FT.	350 FT.	350 FT.
RURAL	500 FT.	500 FT.	500 FT.
EXPRESSWAY / FREEWAY	1000 FT.	1500 FT.	2640 FT.

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		<p align="center">TRAFFIC CONTROL PLAN WITH FLAGGER (ONE-LANE CLOSURE OF TWO-WAY TRAFFIC)</p>	
DATE			
ISSUE DATE:		AUGUST 01, 2017	
		<p align="right">  WORKING NUMBER TCP-1 SHEET NUMBER 6351 </p>	



GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft)	TAPER RATES
	TAPER	ALONG BUFFER SPACE & WORK ZONE		
≤40	40	80	305	27:1
45	45	90	360	45:1
50	50	100	425	50:1
55	55	110	495	55:1
60	60	120	570	60:1
65	65	130	645	65:1
70	70	140	730	70:1

+ NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 $L = WS$ FOR SPEEDS OF 45 mph OR GREATER
 $L = WS^2/60$ FOR SPEEDS OF 40 mph OR LESS
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

++ NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.

3. CHANNELIZING DEVICES:

- A. ALL CHANNELIZING DEVICES IN TAPERS SHALL BE RETROREFLECTIVE FREE STANDING PLASTIC DRUMS.
- B. CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER RETROREFLECTIVE FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.
- C. ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE.
- D. RETROREFLECTORIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE M.U.T.C.D.

4. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48", AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.

5. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON FACE OF SIGN.

6. ADDITIONAL REDUCED REGULATORY SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. TWO (2) WILL BE REQUIRED FOR EACH RAMP AND LOCATION WILL BE DETERMINED BY THE ENGINEER.

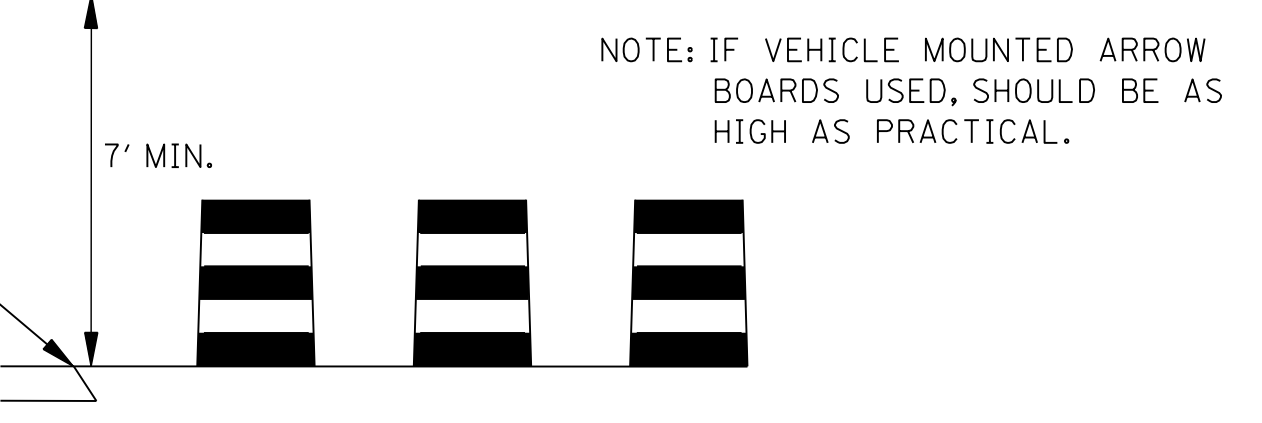
7. THIS TRAFFIC CONTROL PLAN, WITH SPEED ZONE, MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.

8. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.

9. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS REQUIRED FOR LANE CLOSURE.

10. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

EDGE OF TRAVELED WAY



LEGEND

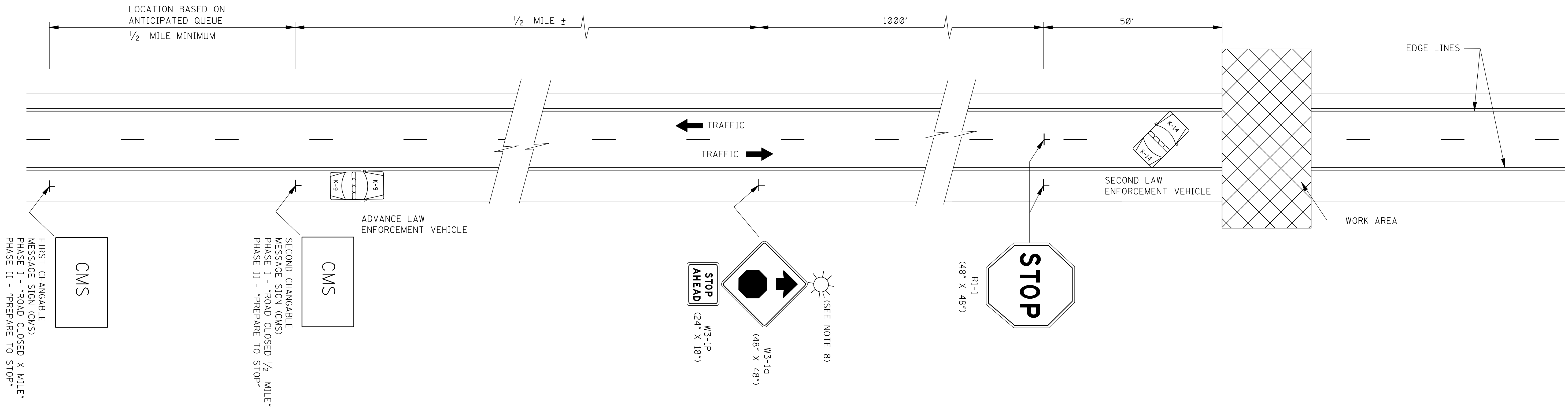
- * OR AS SHOWN ELSEWHERE ON THE PLANS.
- FLASHING ARROW PANEL (TYPE "C")
- BLACK LEGEND AND BORDER ON WHITE BACKGROUND
- ☼ TYPE "B" WARNING LIGHTS
- RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (EXTENDED PERIOD)	
BY	
REVISION	
DATE	ISSUE DATE: AUGUST 01, 2017

MDOT
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

WORKING NUMBER
TCP-4

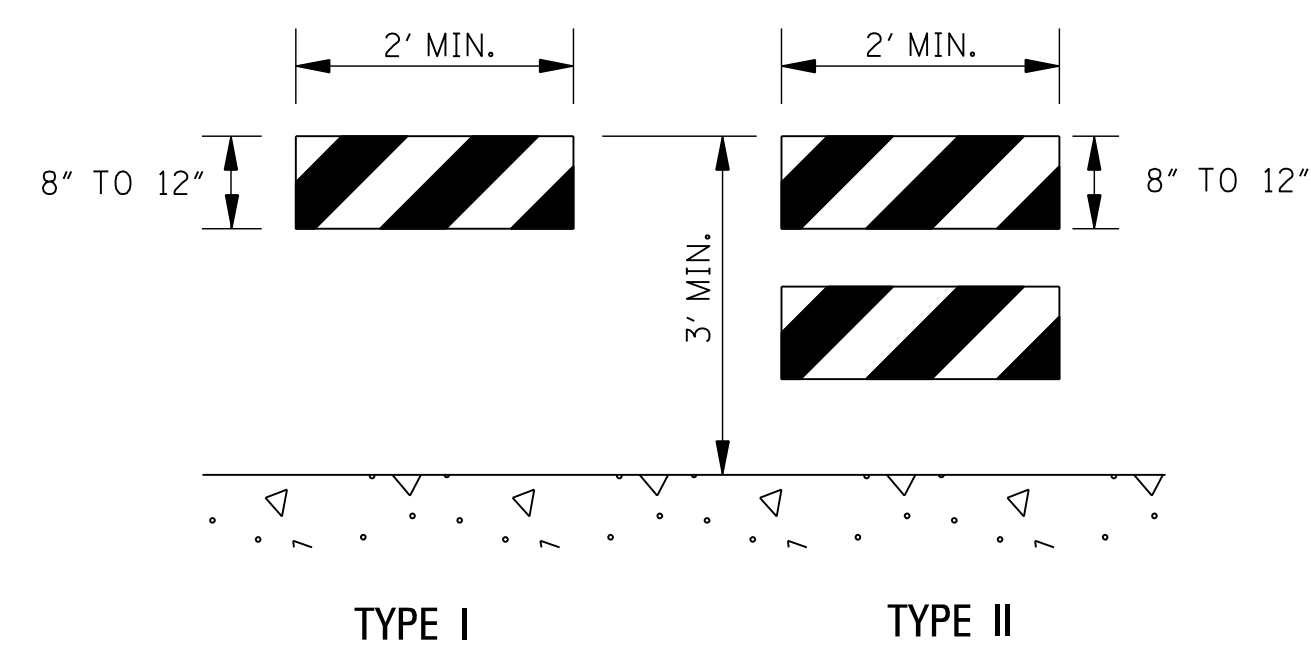
SHEET NUMBER
6354



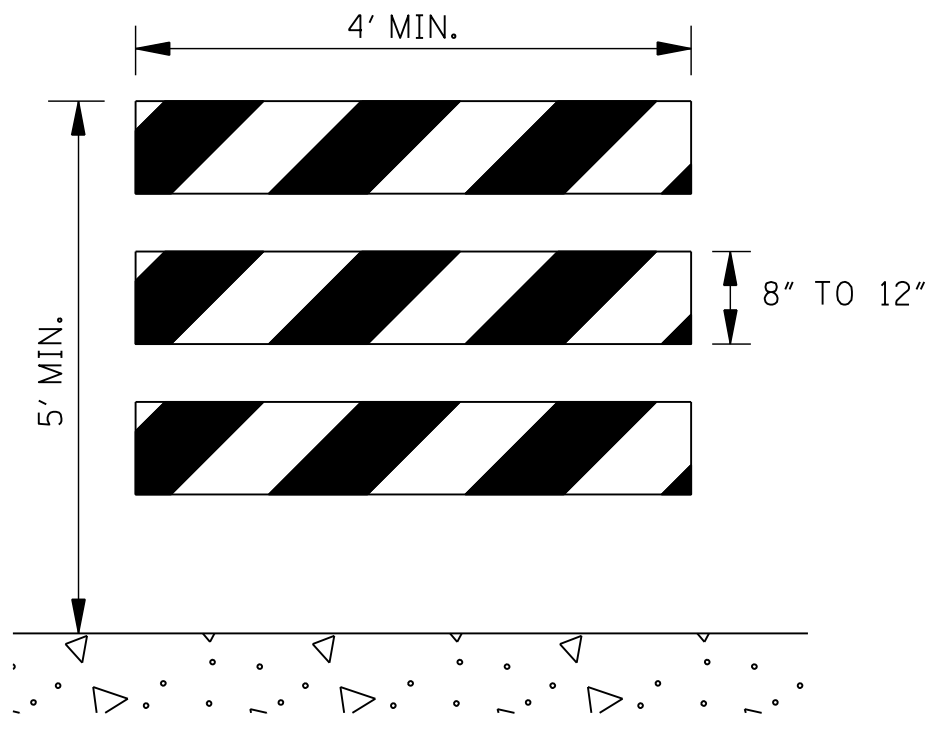
GENERAL NOTES:

- THIS TYPE OF HIGHWAY CLOSURE SHOULD ONLY BE USED FOR CONSTRUCTION OPERATIONS WHEN THE DURATION OF CLOSURE WILL NOT EXCEED 30 MINUTES. AFTER THE HIGHWAY HAS BEEN CLOSED AND REOPENED VIA THIS PROCEDURE, A MINIMUM PERIOD OF 30 MINUTES SHOULD ELAPSE BEFORE ANOTHER SHORT DURATION CLOSURE, EXCEPT WITH THE APPROVAL OF THE ENGINEER.
- AT LEAST TWO LAW ENFORCEMENT OFFICERS AND TWO LAW ENFORCEMENT VEHICLES SHOULD BE PROVIDED ON EACH APPROACH TO THE CLOSURE. EACH LAW ENFORCEMENT VEHICLE SHOULD HAVE A ROOF MOUNTED FLASHING BLUE LIGHT OR LIGHT BAR.
- RESTRICTIONS ON ROAD CLOSURES ARE SPECIFIED IN THE CONTRACT DOCUMENT.
- THE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD BE MOVED BACK AS REQUIRED BY THE QUEUING OF STOPPED VEHICLES.
- IF QUEUE EXCEEDS THE FIRST CHANGABLE MESSAGE SIGN (CMS) AT ANYTIME DURING A CLOSURE; THE TRAFFIC CONTROL PLAN SHOULD BE ADJUSTED AS NECESSARY, WITH APPROVAL OF THE ENGINEER.
- TRAFFIC CONTROL FOR THE CLOSURE SHOULD BE ACCOMPLISHED IN THE FOLLOWING ORDER:
 - FIRST CHANGABLE MESSAGE SIGN (CMS)
 - SECOND CHANGEABLE MESSAGE SIGN (CMS)
 - ADVANCE LAW ENFORCEMENT VEHICLE, LIGHTS AND FLASHERS ON.
 - "W3-1a (48" X 48")" AND "W3-1P (24" X 18")" SIGNS ERECTED.
 - "R1-1 (48" X 48")" SIGNS ERECTED TO STOP TRAFFIC. THE ORDER OF ERECTION SHOULD BE IN THE FOLLOWING ORDER: RIGHT SHOULDER THEN CENTER.
 - SECOND LAW ENFORCEMENT VEHICLE, LIGHTS AND FLASHERS ON.
- TRAFFIC CONTROL SHOULD BE REMOVED IN THE FOLLOWING ORDER:
 - WITH TRAFFIC STOPPED REMOVE THE "R1-1 (48" X 48")" SIGNS TOWARD THE RIGHT SHOULDER IN THE FOLLOWING ORDER: CENTER THEN SIGN ON THE RIGHT SHOULDER. SECOND LAW ENFORCEMENT VEHICLE LEADS TRAFFIC THROUGH WORK AREA.
 - AFTER ALL STOPPED VEHICLES HAVE STARTED MOVING, THE "W3-1a (48" X 48")" AND "W3-1P (24" X 18")" SIGNS SHOULD BE REMOVED. THESE SIGNS MAY BE COVERED IF RE-USE IS IMMINENT.
 - AFTER ALL VEHICLES HAVE RESUMED APPROXIMATELY NORMAL SPEED, THE CHANGABLE MESSAGE SIGNS TURNED OFF.
- UNILLUMINATED SECTIONS OF HIGHWAYS SHOULD NOT BE CLOSED DURING HOURS OF DARKNESS EXCEPT FOR EMERGENCIES OR WITH THE APPROVAL OF THE ENGINEER. WHEN THE HIGHWAY MUST BE CLOSED DURING HOURS OF DARKNESS, A TYPE B HIGH INTENSITY FLASHING BARRICADE WARNING LIGHT SHALL BE USED ON EACH W3-1a SIGN.
- IF AN ENTRANCE RAMP IS LOCATED BETWEEN THE SECOND CMS AND R1-1, THE CMS, "W3-1a (48" X 48")", AND "W3-1P (24" X 18")" SIGNS SHOULD ALSO BE ERECTED ON THE RAMP SHOULDER.
- THE ABOVE DURATION WILL APPLY TO EACH APPROACH TO THE CLOSURE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC, INCLUDING SECURING LAW ENFORCEMENT SERVICES.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
SHORT DURATION CLOSING OF TWO-LANE TWO-WAY HIGHWAYS	
WORKING NUMBER TCP-6	SHEET NUMBER 6356
ISSUE DATE: AUGUST 01, 2017	



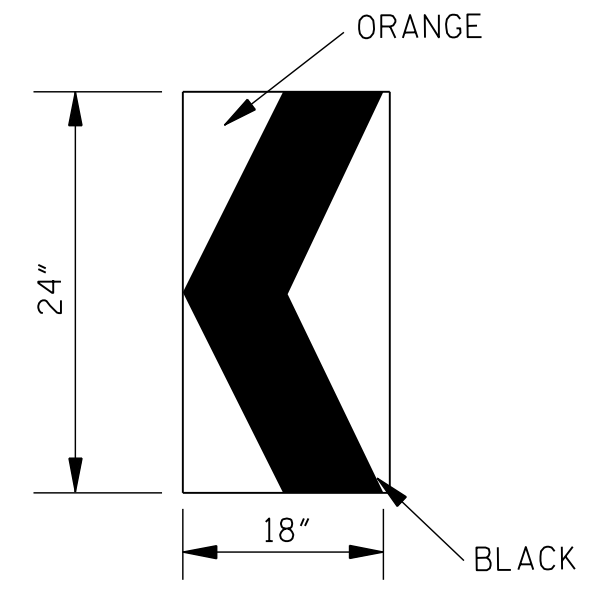
TYPE I TYPE II



TYPE III

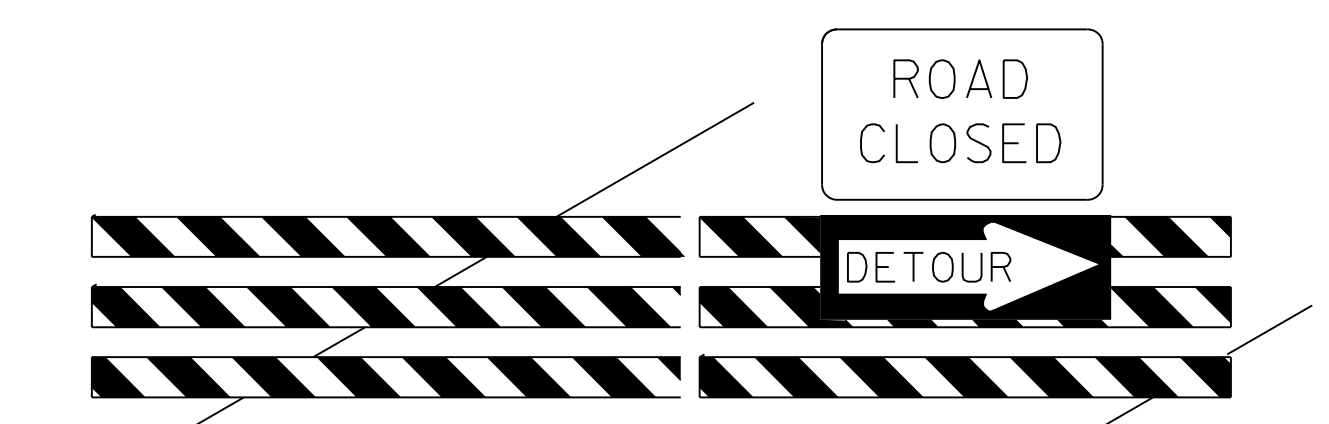
STANDARD BARRICADES

1. THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).
2. RAIL STRIPE SHOULD BE 6 INCHES, EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.
3. DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
4. FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.
5. BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES WHICH REQUIRE CRASHWORTHINESS ACCEPTANCE LETTERS. TO DATE, 2-IN. THICK TIMBER RAILS HAVE NOT BEEN SUCCESSFULLY CRASH TESTED. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE:
http://safety.fhwa.dot.gov/roadway_dept/policy_guide/road_hardware/cat2.cfm



**CHEVRON SIGN
DETAIL**

1. A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
2. THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.
3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.

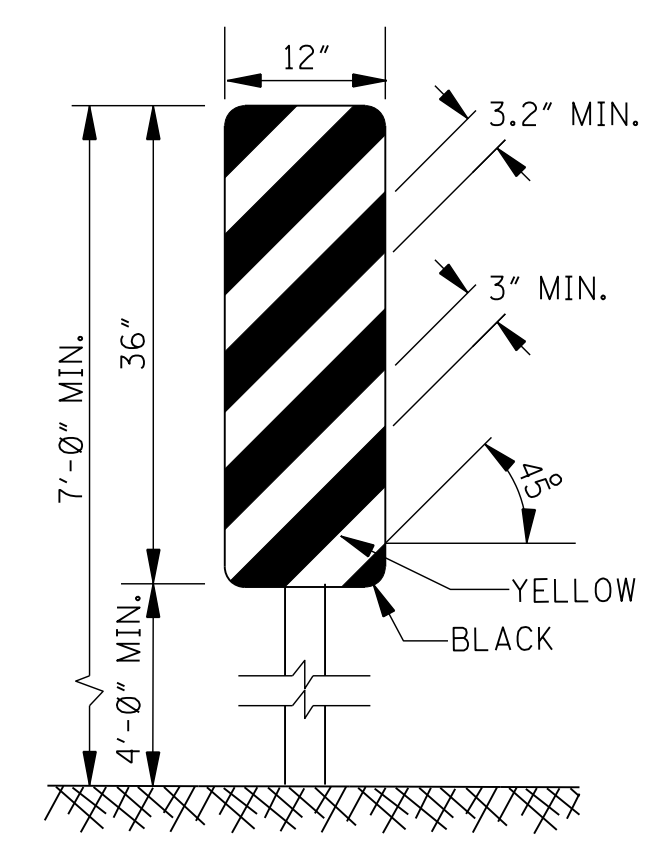


BARRICADE CLOSING A ROAD

BARRICADE CHARACTERISTICS

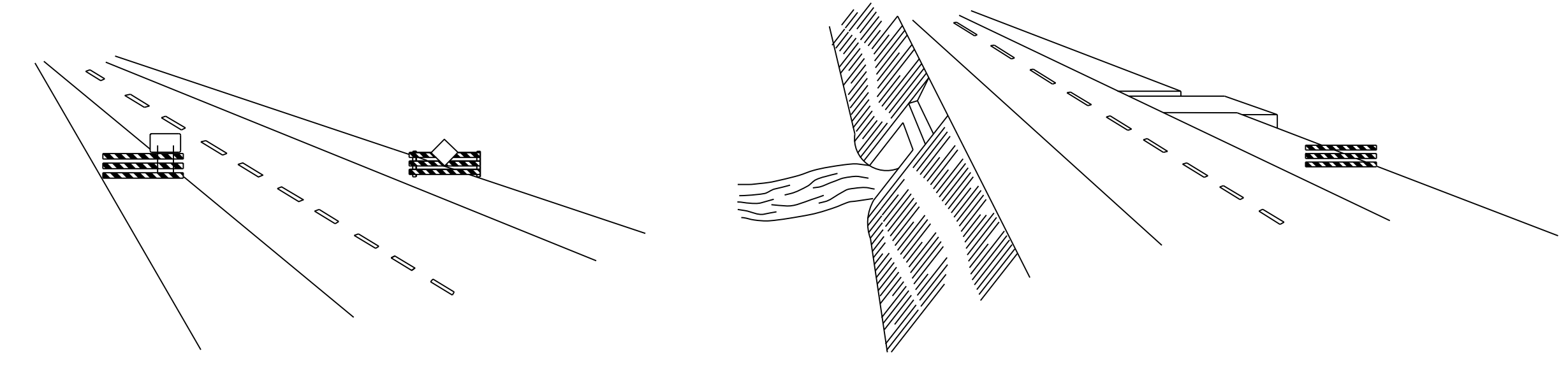
	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF RETROREFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS

- * 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- ** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 in² OF REFLECTIVE AREA FACING TRAFFIC.



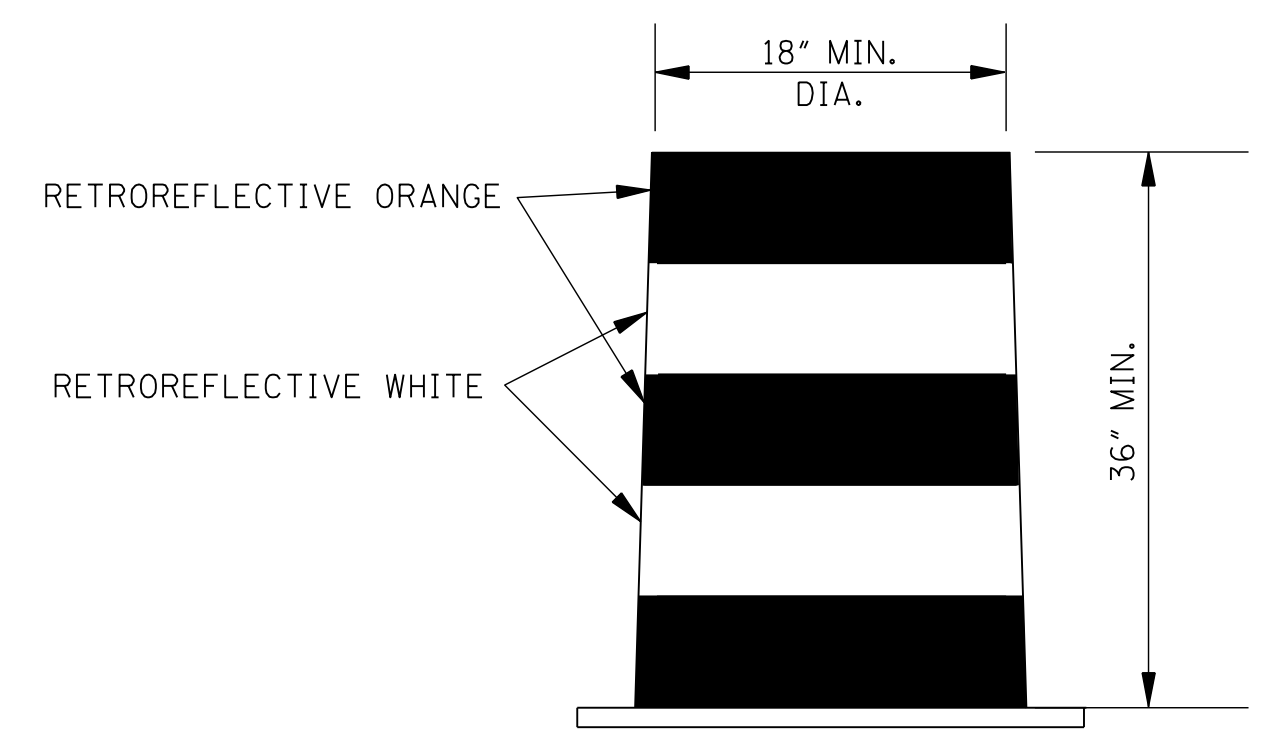
**TYPE 3 OBJECT MARKER
(OM-3R)**

1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
2. THE OM-3R IS SHOWN. THE OM-3L IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
3. THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



WING BARRICADES

1. WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
2. WING BARRICADES SHOULD BE USED:
 - A. IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - B. IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.



PLASTIC DRUM STRIPING DETAIL

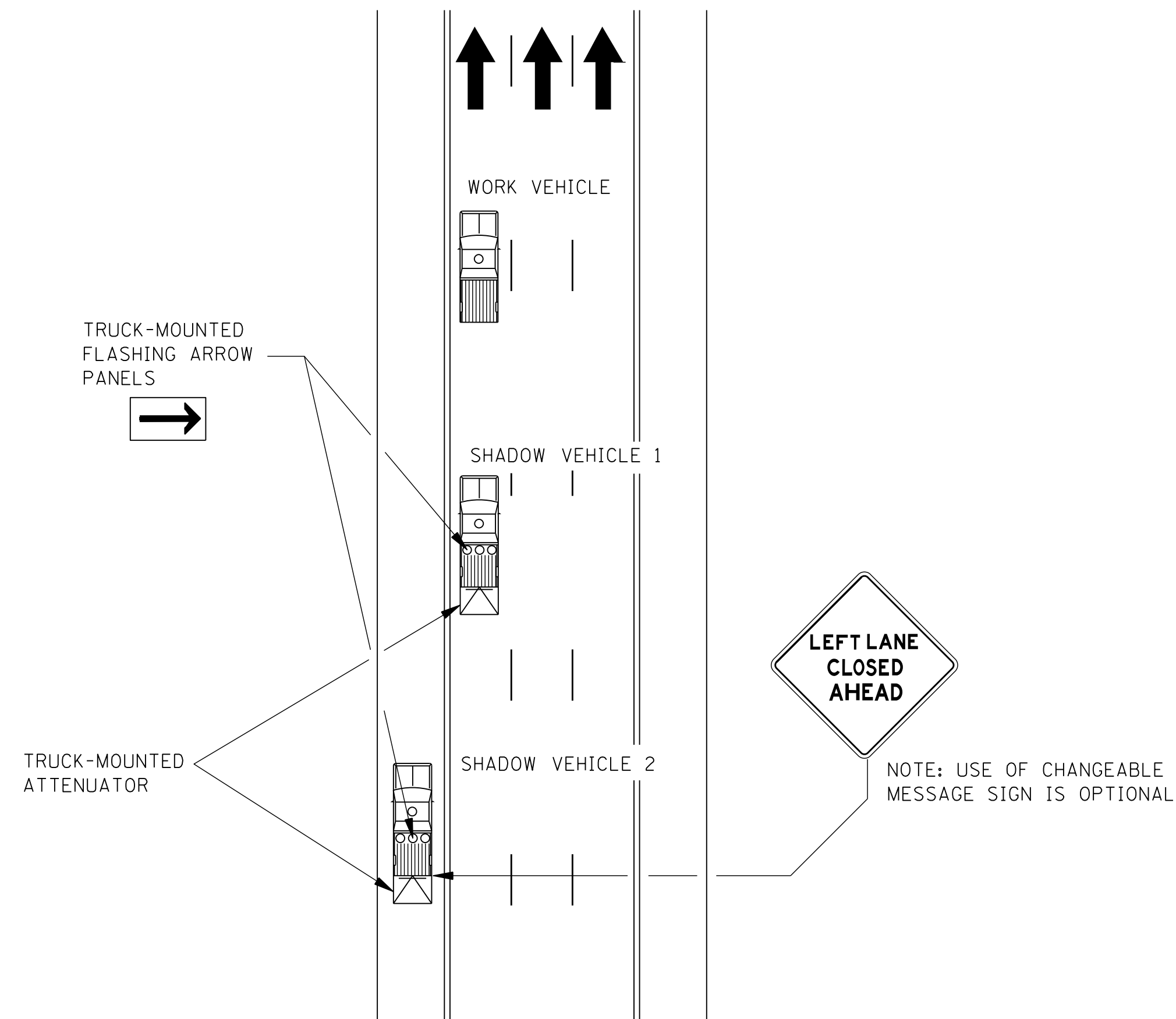
1. PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT COLOR ON DRUMS SHALL BE ORANGE WITH FOUR (4) RETROREFLECTIVE, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
2. DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
3. WHERE PRACTICAL PLASTIC DRUMS SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		<p>HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS</p>	
DATE			
ISSUE DATE:		AUGUST 01, 2017	



WORKING NUMBER
TCP-8
SHEET NUMBER
6358

MOBILE OPERATIONS ON MULTILANE ROAD

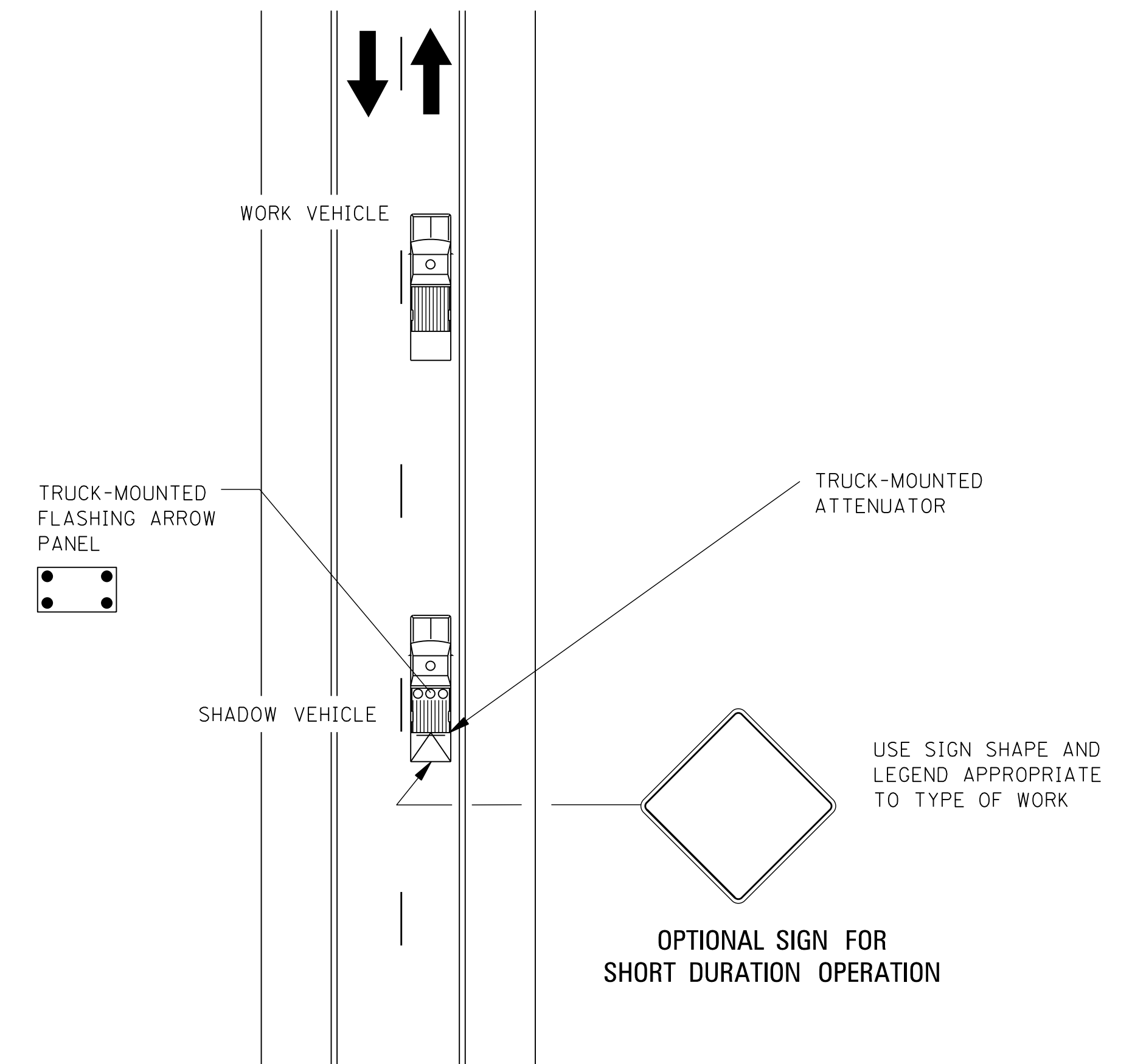


MOBILE OPERATIONS ON MULTILANE ROAD

NOTES FOR MULTILANE LANE OPERATION:

- VEHICLES USED FOR THESE OPERATIONS SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT, SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLAGS, SIGNS, OR ARROW PANELS.
- SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK MOUNTED ATTENUATOR (TMA). AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON SHADOW VEHICLE 2 SO AS NOT TO OBSCURE THE ARROW PANEL.
- SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK-MOUNTED ATTENUATOR (TMA).
- SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
- WHEN ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, SHADOW VEHICLE 2 SHOULD BE ELIMINATED.
- ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE SHOULD BE USED (i.e., VEHICLE 3 ON THE SHOULDER (IF PRACTICAL), VEHICLE 2 IN THE CLOSED LANE, AND VEHICLE 1 IN THE CLOSED LANE).
- ARROW PANELS SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
- WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MOBILE OPERATIONS ON TWO-LANE ROAD

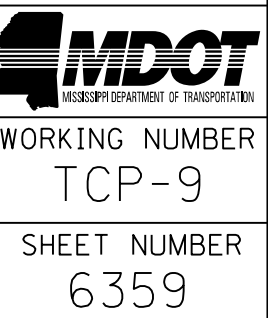


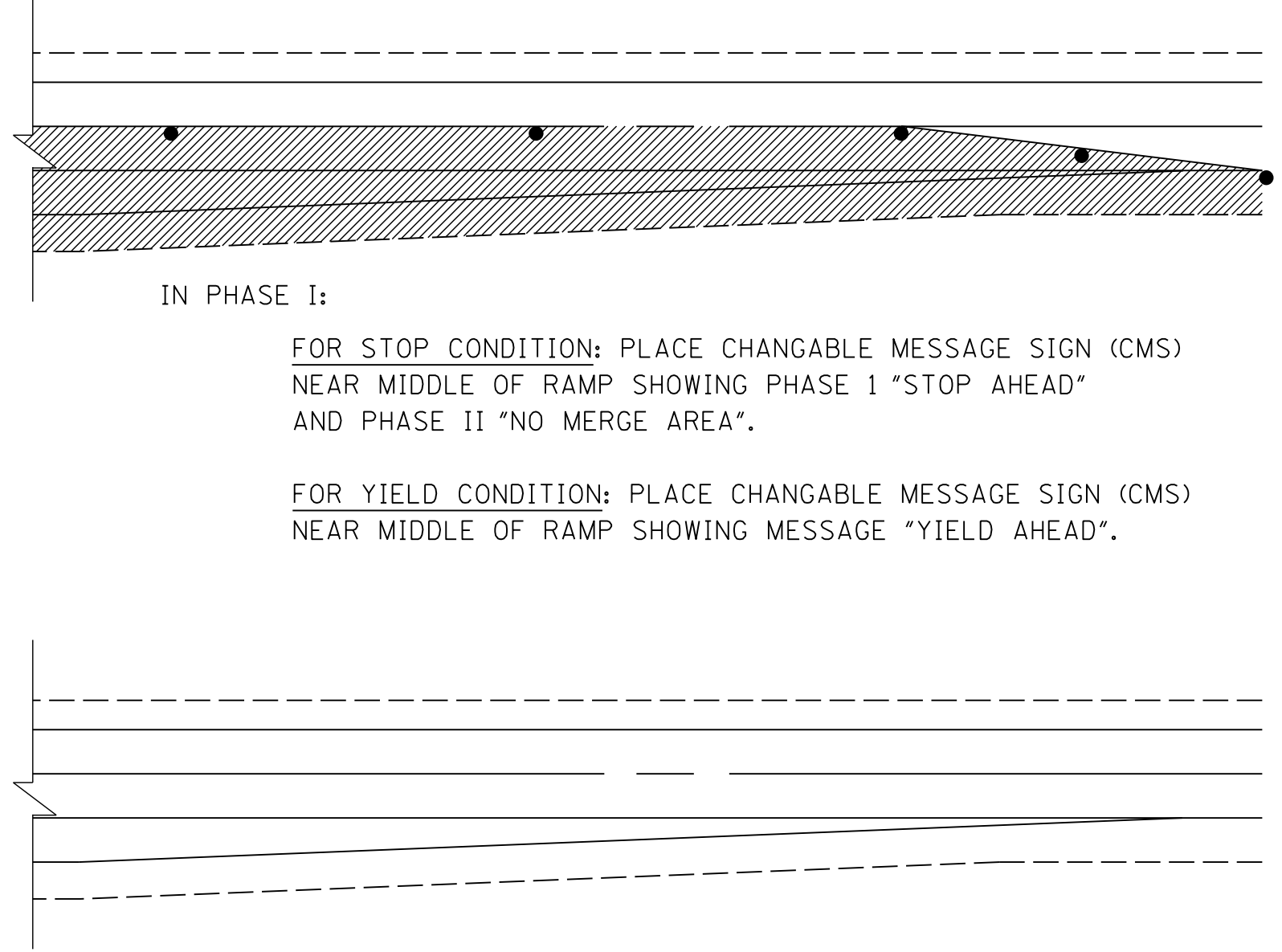
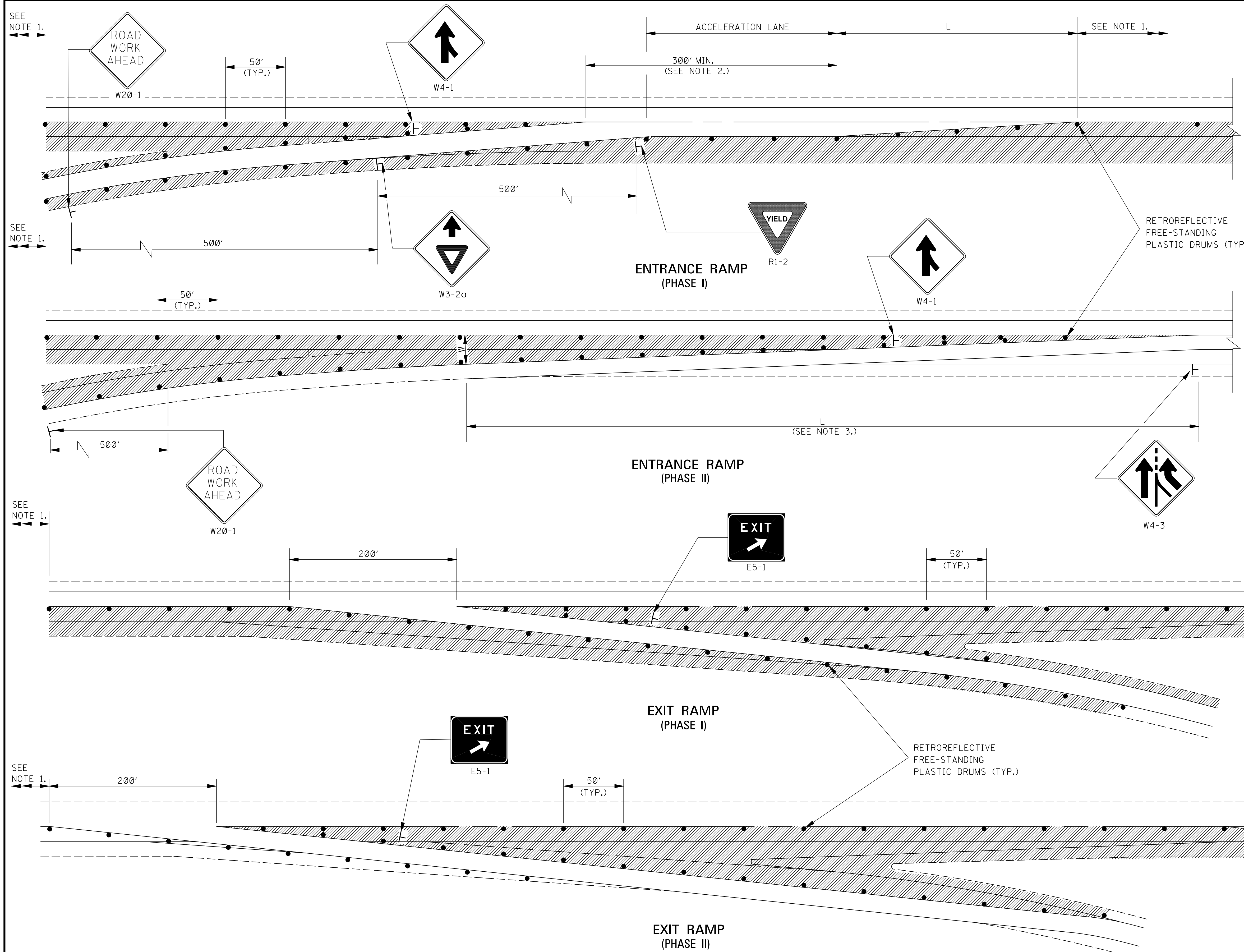
MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES FOR TWO-LANE OPERATION:

- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY, AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. SHADOW VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, THE SHADOW VEHICLE SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW VEHICLES SHALL BE EQUIPPED WITH TWO HIGH-INTENSITY FLASHING LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN. SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ARROW BOARD TO BE USED IN CAUTION MODE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN		
REVISION	TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO-LANE ROADS		
DATE	ISSUE DATE:	AUGUST 01, 2017	
	WORKING NUMBER	TCP-9	
	SHEET NUMBER	6359	

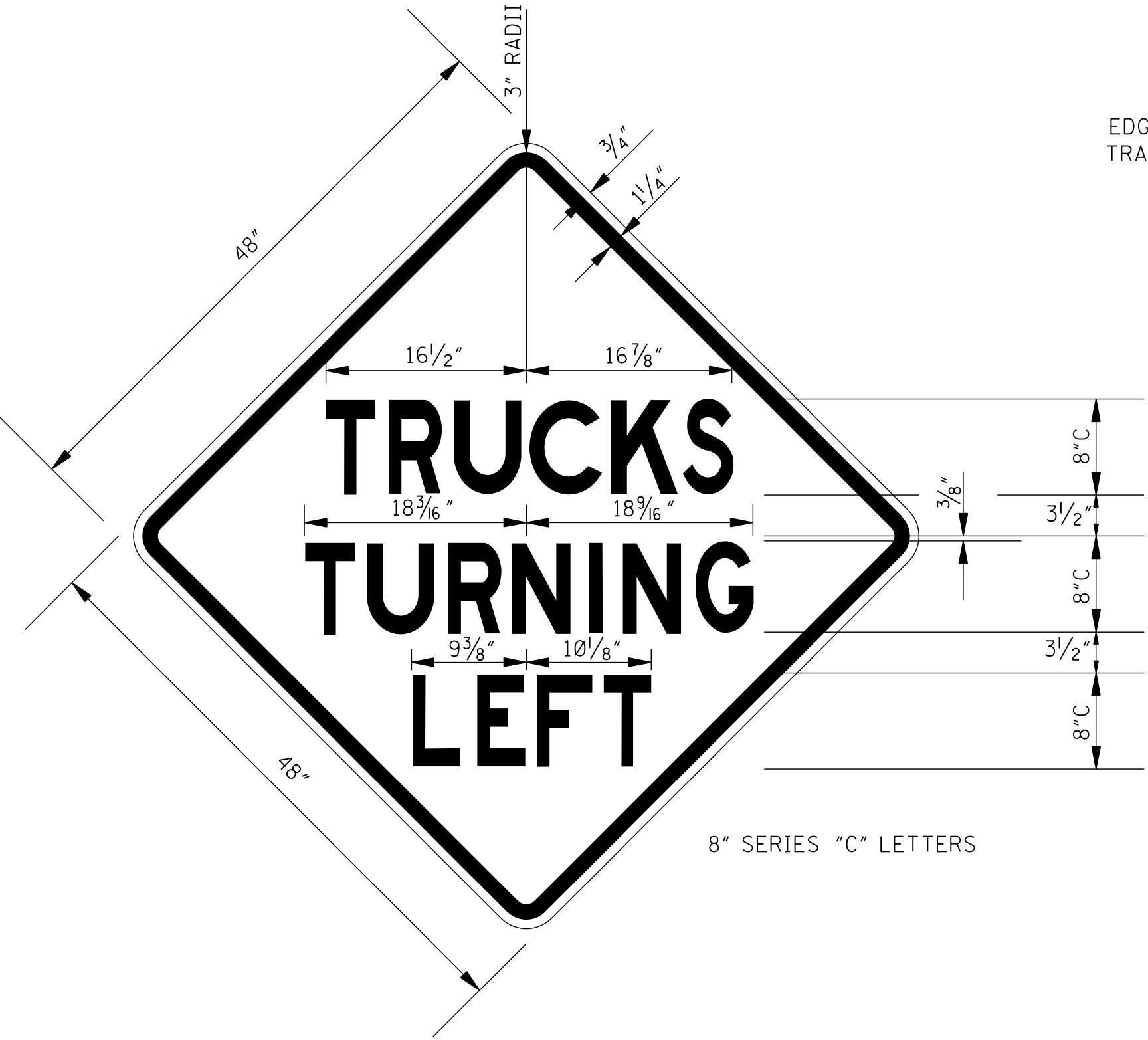
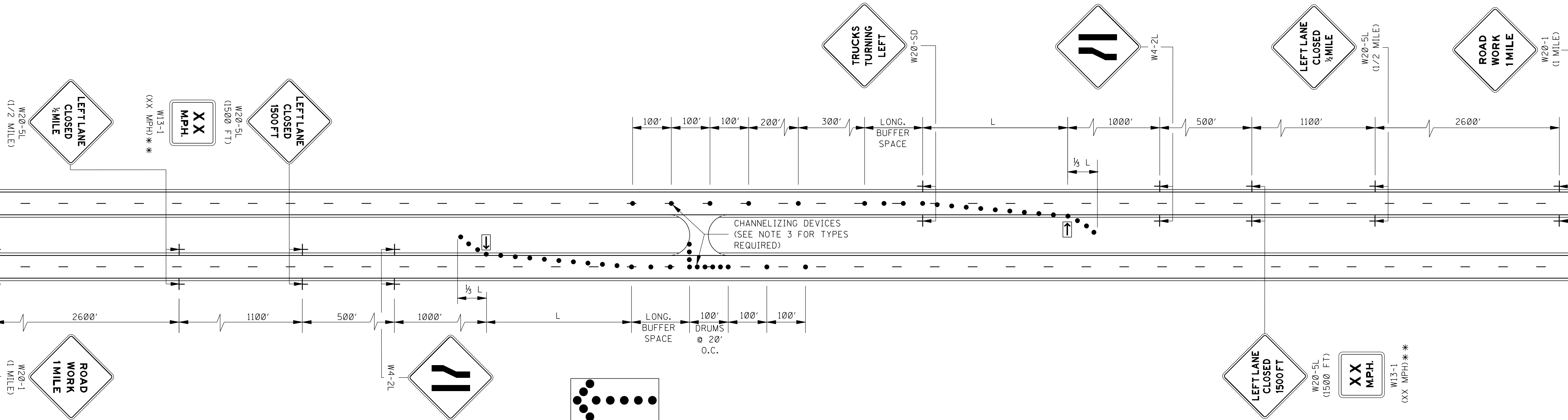




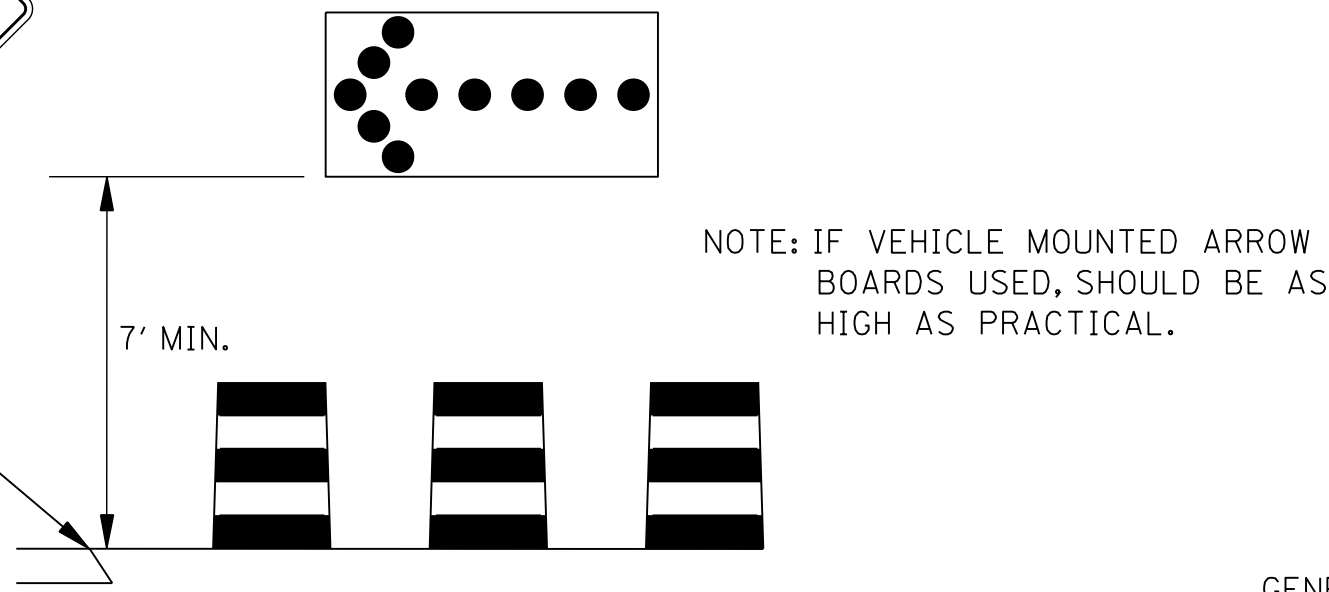
IN PHASE I:
 FOR STOP CONDITION: PLACE CHANGABLE MESSAGE SIGN (CMS) NEAR MIDDLE OF RAMP SHOWING PHASE I "STOP AHEAD" AND PHASE II "NO MERGE AREA".
 FOR YIELD CONDITION: PLACE CHANGABLE MESSAGE SIGN (CMS) NEAR MIDDLE OF RAMP SHOWING MESSAGE "YIELD AHEAD".

- GENERAL NOTES:
- FOR SIGNING, FLASHING ARROW PANEL, AND DRUM SPACING CRITERIA, SEE TYPICAL TRAFFIC CONTROL PLANS FOR LANE CLOSURES ON SHEETS TCP-2, TCP-3, TCP-4, AND TCP-5, AS APPROPRIATE
 - IF NO ACCELERATION LANE EXISTS FOR THE TEMPORARY ENTRANCE, THE YIELD SIGN (R1-2) AND THE YIELD AHEAD SIGN (W3-2A) SHALL BE REPLACED WITH A STOP SIGN (R1-1) AND A STOP AHEAD SIGN (W3-1a), ONE ON EACH SIDE OF APPROACH. WHERE STOP SIGNS ARE USED, A TEMPORARY STOP LINE SHOULD BE PLACED ACROSS THE RAMP AT THE DESIRED STOP LOCATION. IF INSUFFICIENT GAPS ARE AVAILABLE IN THE TRAFFIC STREAM, CONSIDERATION SHOULD BE GIVEN TO CLOSING THE RAMP.
 - DETERMINE TAPER LAYOUTS USING THE FOLLOWING EQUATIONS:
 $L = WS$ FOR SPEED OF 45 mph OR GREATER
 $L = WS^2/60$ FOR SPEEDS OF 40 mph OR LESS
 WHERE:
 L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
 - ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

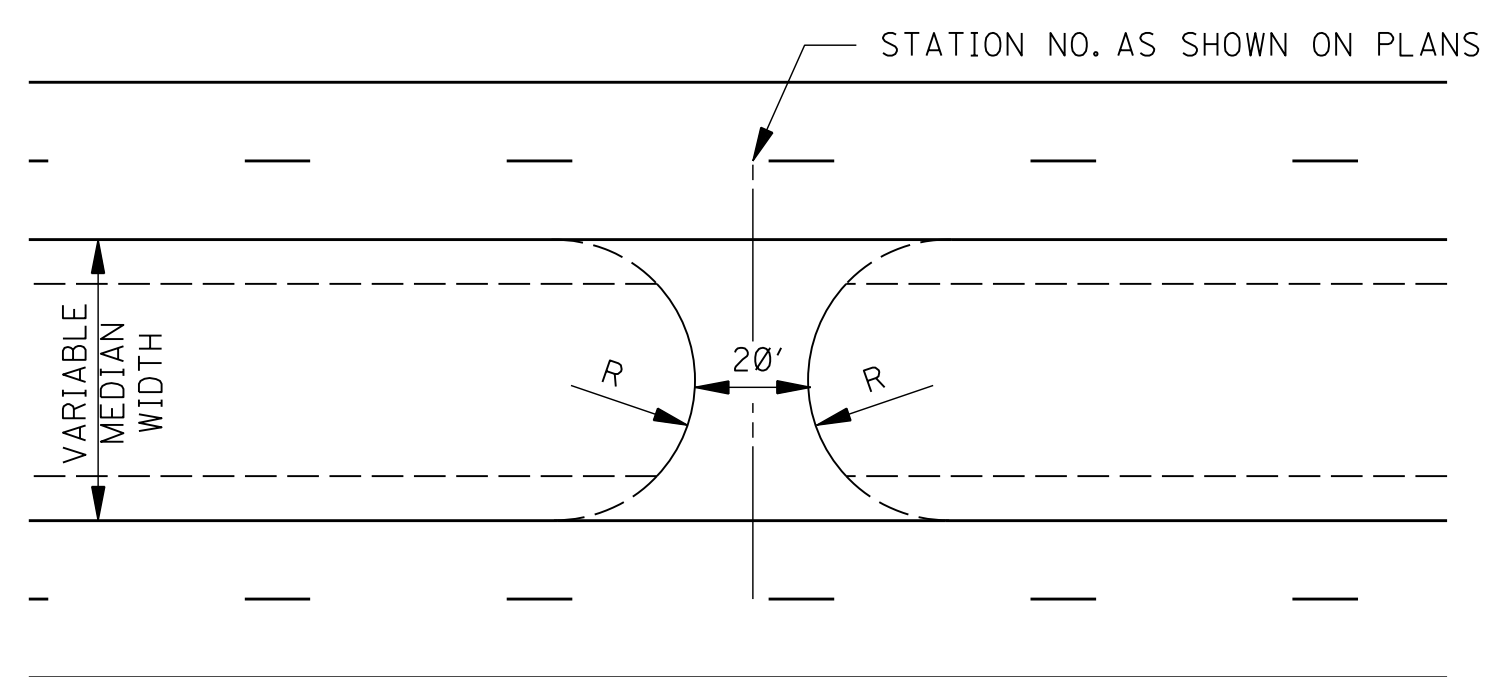
BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		DETAILS OF OUTSIDE LANE CLOSURE AT EXIT AND ENTRANCE RAMPS	
DATE		ISSUE DATE: AUGUST 01, 2017	
		 WORKING NUMBER TCP-10 SHEET NUMBER 6360	



DETAIL OF W20-SD



- LEGEND**
- ** THE SPEED ON W13-1 SIGN SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT UNLESS OTHERWISE DIRECTED BY COMMISSION ORDER.
 - ☒ FLASHING ARROW PANEL (TYPE "C")
 - RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
 - ☼ TYPE "B" WARNING LIGHTS



DETAIL OF TEMPORARY CROSSOVER
NOTE: R = 0.5 (MEDIAN WIDTH)

GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

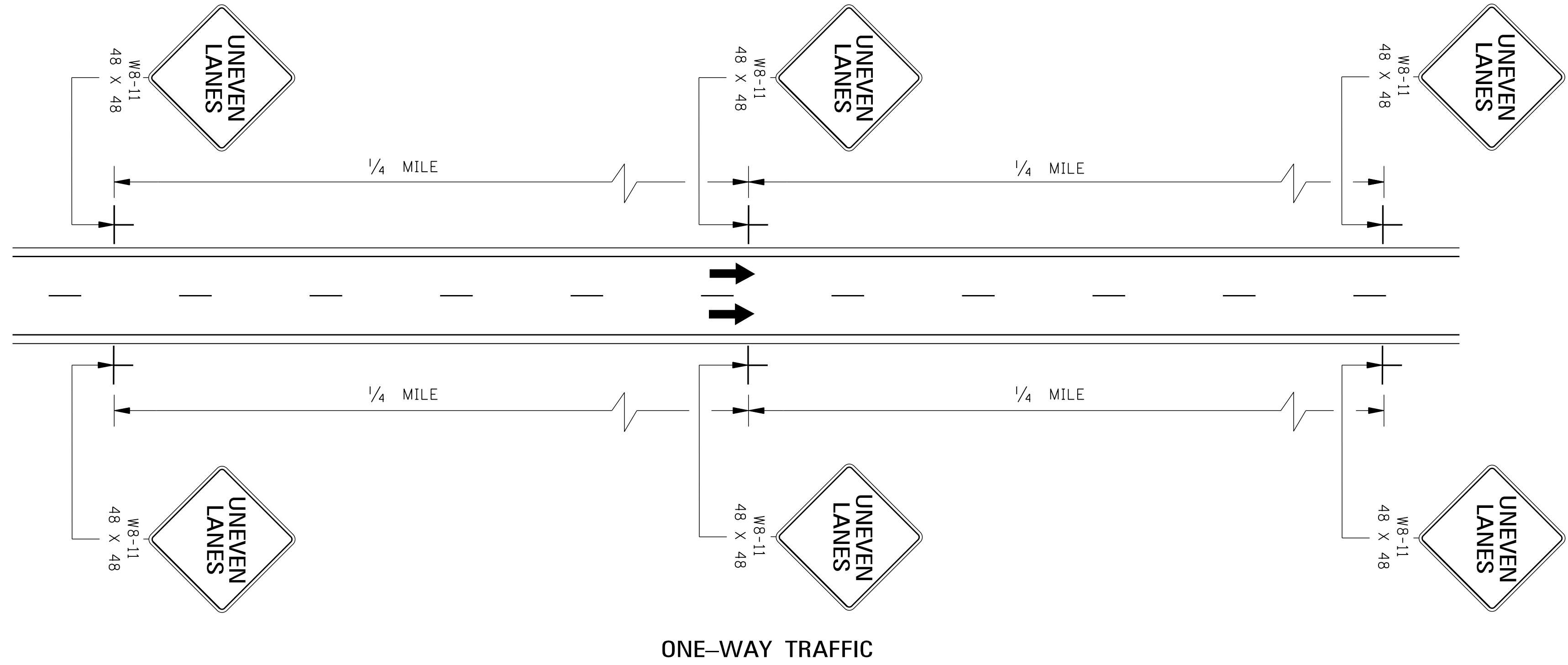
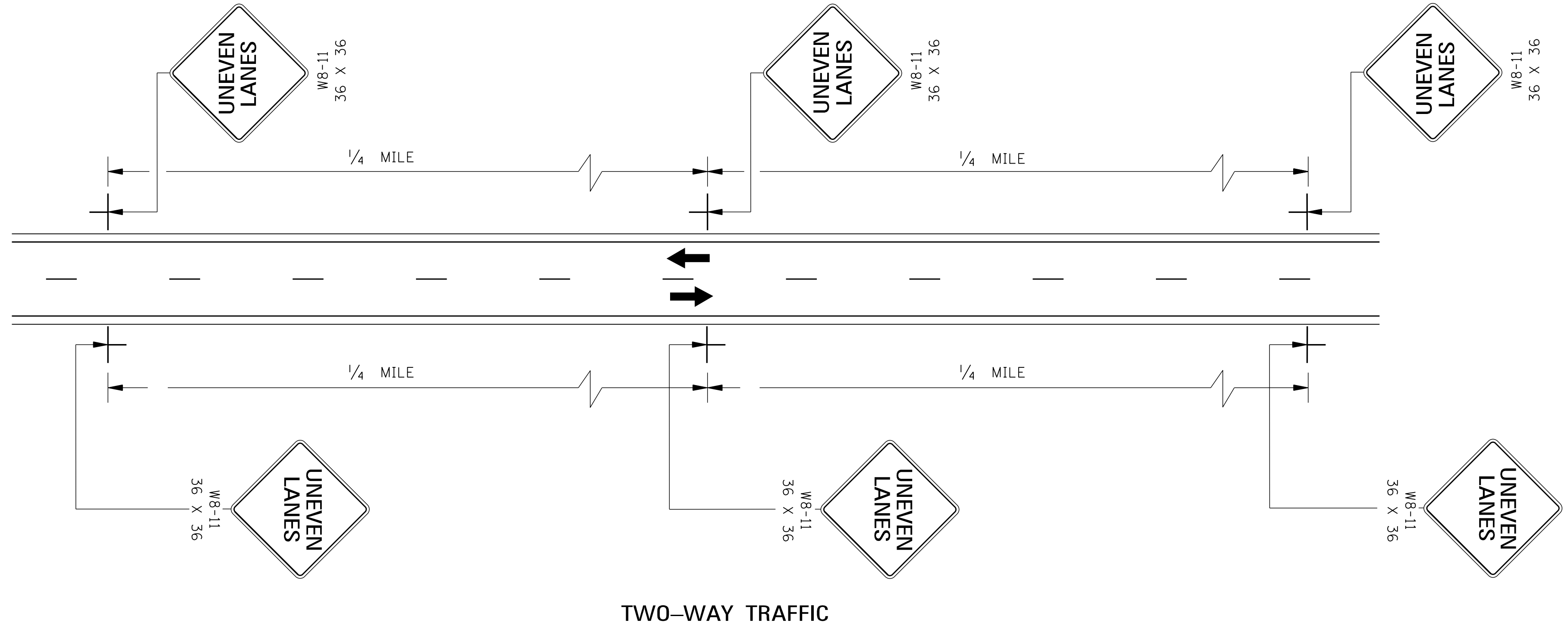
POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft) ⁺⁺	TAPER [†] RATES
	TAPER	ALONG LANE LINE & WORK ZONE		
mph				
≤40	40	80	305	27:1
45	45	90	360	45:1
50	50	100	425	50:1
55	55	110	495	55:1
60	60	120	570	60:1
65	65	130	645	65:1
70	70	140	730	70:1

[†] NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 $L = WS$ FOR SPEEDS OF 45 mph OR GREATER
 $L = WS^2/60$ FOR SPEEDS OF 40 mph OR LESS
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

⁺⁺ NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE FREE STANDING PLASTIC DRUMS.
4. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48" AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
5. LOCATION OF CROSSOVERS WILL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
6. USE OF CROSSOVER SHALL BE LIMITED TO TRUCKS APPROACHING FROM ONE DIRECTION ONLY.
7. REVERSE LAYOUT OF TAPERS, DRUMS AND SIGNS TO ACCOMMODATE TRUCKS APPROACHING FROM OPPOSITE DIRECTION.
8. EACH CONSTRUCTION CROSSOVER SHOULD BE CLOSED AS DIRECTED BY THE ENGINEER OR WHEN THE CONSTRUCTION OPERATION, ZONE OR SIGNS ENCROACH ON OR INTERFERES WITH THE OPERATION OF THE CONSTRUCTION CROSSOVER. THE CROSSOVER SHOULD BE CLOSED USING 24' OF TYPE III BARRICADE (DOUBLE FACED).
9. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

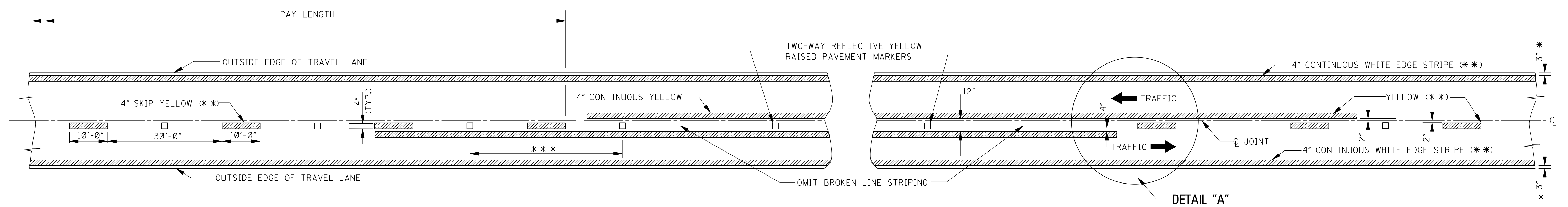
BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		TRAFFIC CONTROL PLAN FOR TEMPORARY CONSTRUCTION CROSSOVER (WORK DAY ONLY)	
DATE	ISSUE DATE: AUGUST 01, 2017		
		WORKING NUMBER TCP-11 SHEET NUMBER 6361	



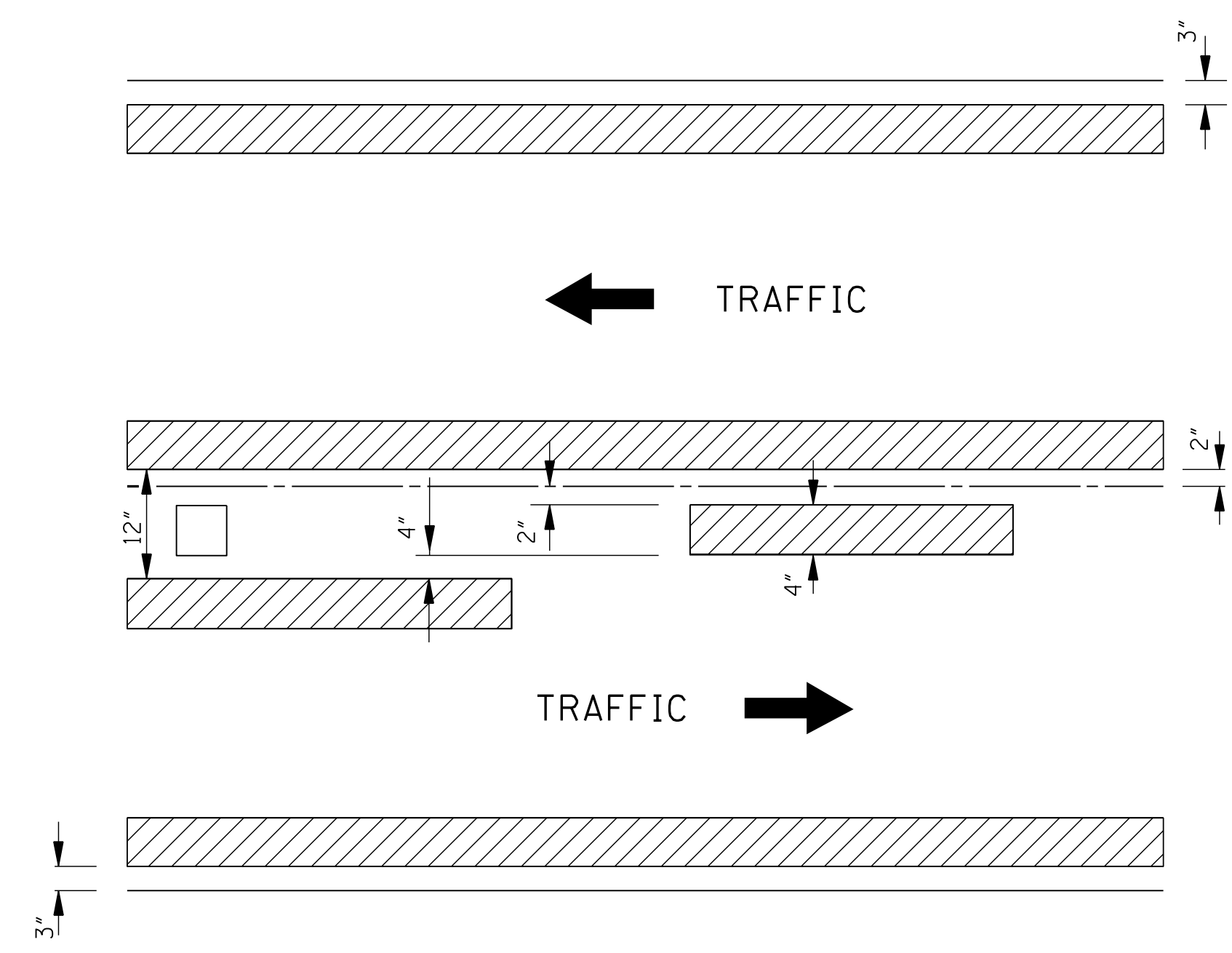
GENERAL NOTES:

1. UNEVEN LANE LINE:
 - A. IF LESS THAN OR EQUAL TO 1/2", NO SIGNS REQUIRED.
 - B. IF GREATER THAN 1/2" AND LESS THAN OR EQUAL TO 2/4", PLACE SIGNS AS SHOWN ON THIS SHEET.
 - C. IF GREATER THAN 2/4", TRAFFIC SHOULD NOT BE ALLOWED TO CROSS UNEVEN LANE LINE.
2. THE W8-11 SIGNS SHOULD BE SPACED AT 1/4-MILE INTERVALS THROUGHOUT UNEVEN LANE LINE LIMITS.
3. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
		TRAFFIC CONTROL PLANS UNEVEN PAVEMENT DETAILS	
			
		WORKING NUMBER TCP-12	
		SHEET NUMBER 6362	
BY	REVISION	DATE	
		ISSUE DATE: AUGUST 01, 2017	



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



DETAIL "A"



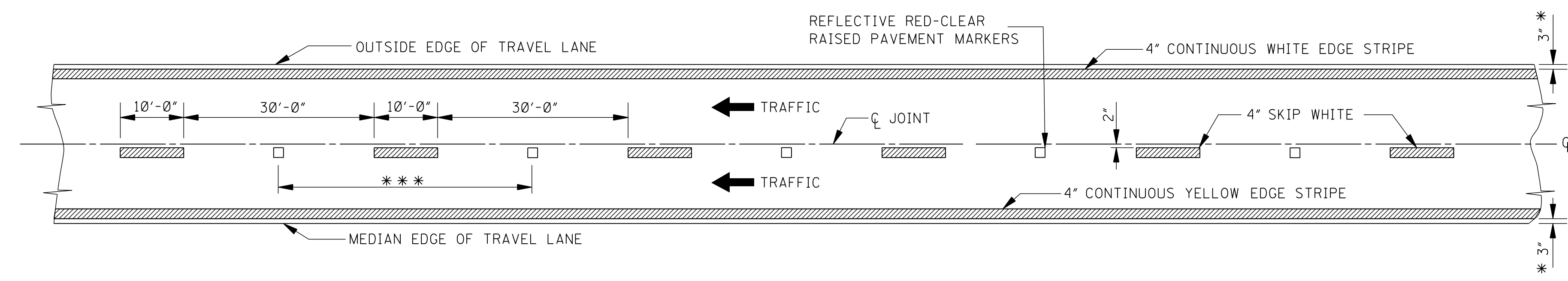
GENERAL NOTES:

- * 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
- 3. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS
- *** 4. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

	URBAN AREA (ft-in)	RURAL AREA (ft-in)
TANGENT SECTIONS	40'-0"	80'-0"
HORIZONTAL CURVES	40'-0"	40'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

† NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

5. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDT "APPROVED SOURCES OF MATERIALS."



4-LANE WITH ONE-WAY TRAFFIC

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
REVISION	TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS
DATE	ISSUE DATE: AUGUST 01, 2017



WORKING NUMBER
TCP-13
SHEET NUMBER
6363