

STATE	PROJECT NUMBER	SHEET NO.
MISSISSIPPI	BR-0020-01(248)	1

GENERAL INDEX

INCLUDED THIS PROJECT	BEGIN WITH SHEET
<input checked="" type="checkbox"/> ROADWAY	1
<input type="checkbox"/> PERMANENT SIGNS	1001
<input type="checkbox"/> TRAFFIC SIGNALS	2001
<input type="checkbox"/> ITS COMPONENTS	3001
<input type="checkbox"/> LIGHTING	4001
<input type="checkbox"/> (RESERVED)	5001
<input checked="" type="checkbox"/> ROADWAY STANDARD DWGS	6001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (LRFD)	7001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (STD. SPEC.)	7501
<input checked="" type="checkbox"/> BRIDGE	8001
<input type="checkbox"/> CROSS SECTIONS	9001

BRIDGE STRUCTURES REQ'D.

NONE

BOX BRIDGES REQ'D.

NONE

CONVENTIONAL SYMBOLS

COUNTY LINE	-----
TOWN CORPORATION LINE	-----
SECTION LINE	-----
EXISTING ROAD OR TRAVELED WAY	-----
PROPOSED ROAD OR TRAVELED WAY	-----
RAILROAD	-----
SURVEY LINE	-----
BRIDGES	-----

STATE OF MISSISSIPPI

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE OF PROPOSED STATE HIGHWAY FEDERAL AID PROJECT NO. BR-0020-01(248)

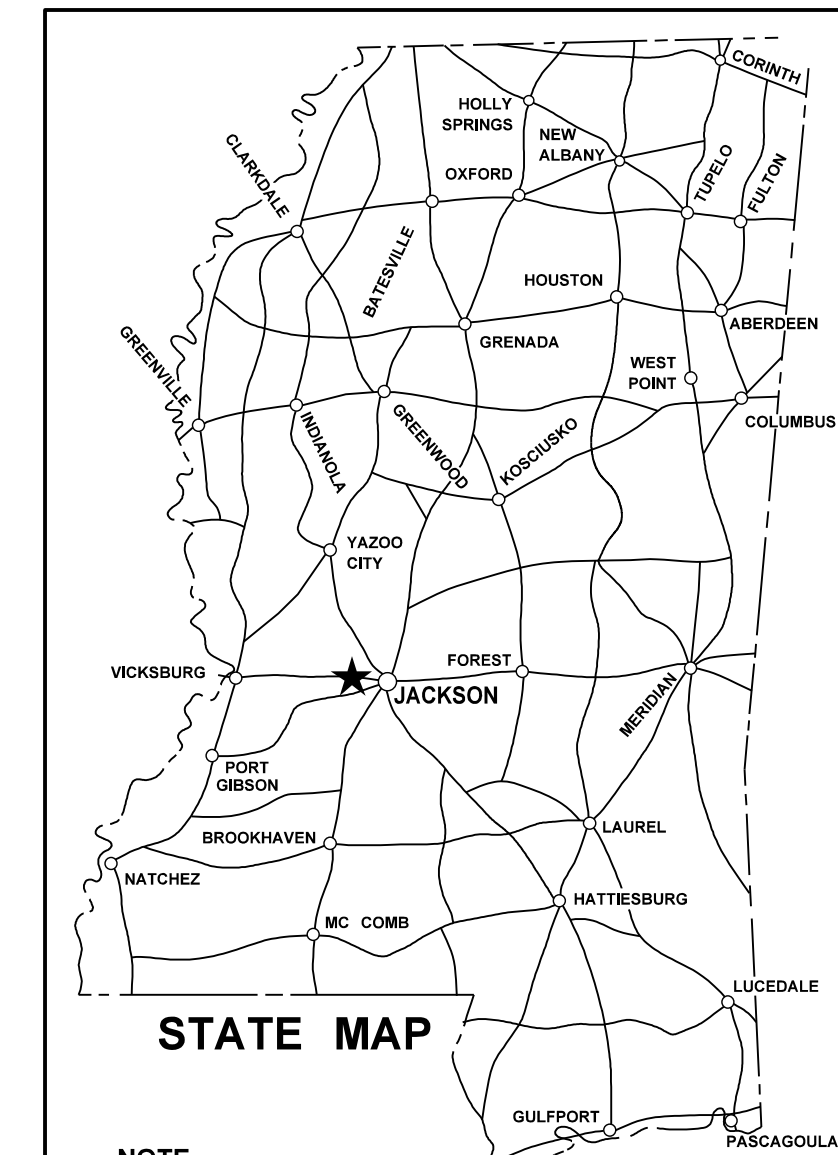
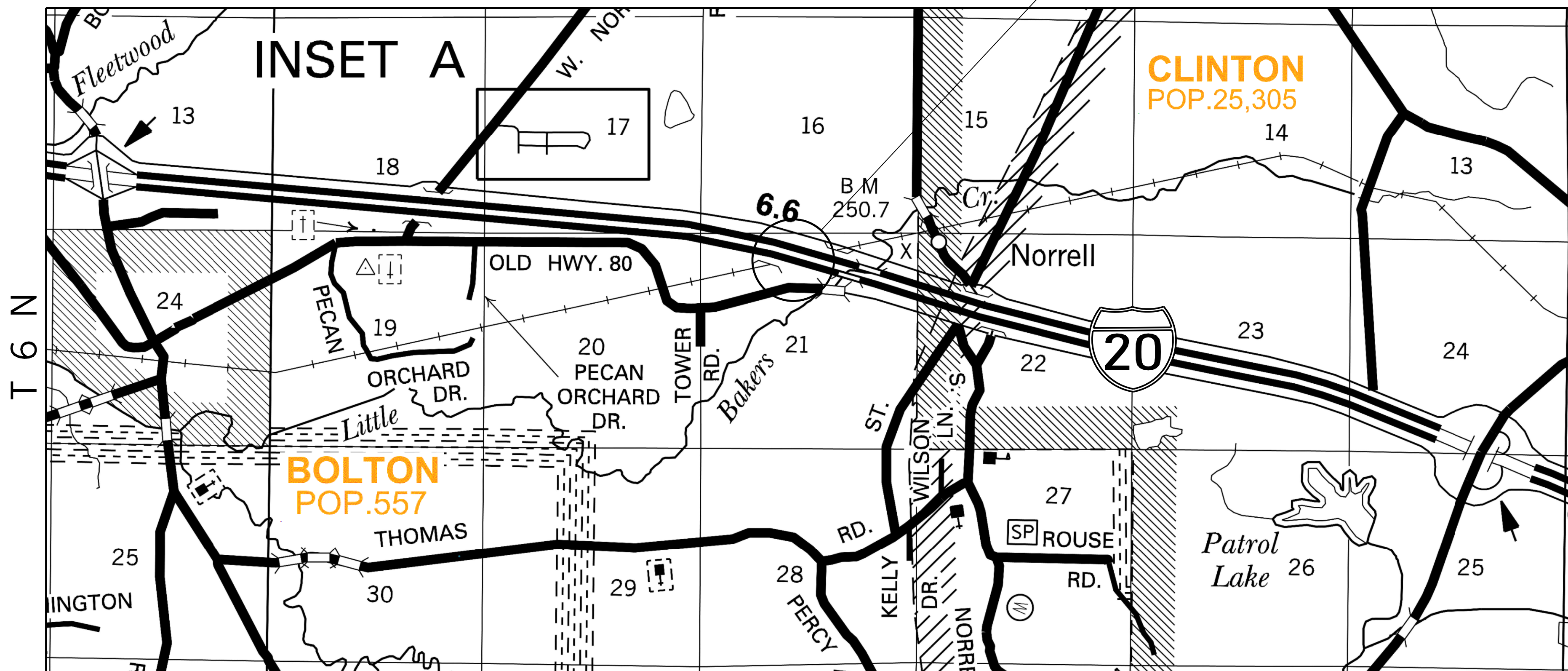
I-20 OVER MERIDIAN SPEEDWAY (BRIDGE NO. 31.5)
HINDS COUNTY

FMS CON. NO. 107878/ 301000

SCALES

PLAN	1 IN. = 100 FT.
PROFILE {	HOR. 1 IN. = 100 FT.
	VERT. 1 IN. = 10 FT.
LAYOUT	1 IN. = 2000 FT.

PROJECT LOCATION



STATE MAP

NOTE
★ INDICATES APPROXIMATE LOCATION OF PROJECT.

LAT. 32°21'20" N LONG. 90°24'28" W
(APPROX. MIDDLE OF PROJECT)

DESIGN CONTROL

70 MPH = V (SPEED DESIGN)

ADT () = : ADT () =

DHV = : D = % T = %

PERMITS ACQUIRED BY MDOT

WETLANDS AND WATERS PERMITS		
	WATERS	WETLANDS
NATIONWIDE #14	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
NATIONWIDE (OTHER)*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
GENERAL*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
INDIVIDUAL (404)*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
STORMWATER PERMIT <input checked="" type="checkbox"/>		
Y	REQUIRED, CNOI SUBMITTED BY MDOT (DISTURBED AREA = 5 ACRES)	
S	REQUIRED, SCNOI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES)	
N	NO STORMWATER PERMIT REQUIRED (<1 ACRE)	
APPROVED BY: _____		

EQUATIONS

NONE

LENGTH DATA

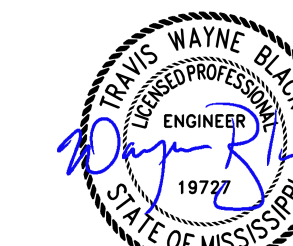
LENGTH OF ROADWAY	FT.	MI.
LENGTH OF BRIDGES	FT.	MI.
LENGTH OF PROJECT (NET)	FT.	MI.
LENGTH OF EXCEPTIONS	FT.	MI.
LENGTH OF PROJECT (GROSS)	FT.	MI.

EXCEPTIONS

NONE



BRIDGE



ROADWAY

P S & E DATE: 12/10/2019

APPROVED:	_____
DEPUTY EXECUTIVE DIRECTOR / CHIEF ENGINEER	_____
EXECUTIVE DIRECTOR	_____
MDOT MISSISSIPPI DEPARTMENT OF TRANSPORTATION	

12/10/2019 09:36:17 TITLE.DGN

STATE	PROJECT NO.
MISS.	BR-0020-01(248)

DESCRIPTION OF SHEET

REVISION DATE WKG. NO. SH. NO.

DESCRIPTION OF SHEET

REVISION DATE WKG. NO. SH. NO.

ROADWAY (11)			
TITLE SHEET (1)			1
DETAILED INDEX & GENERAL NOTES (2)			
DETAILED INDEX	DI-1		2
GENERAL NOTES	GN-1		3
TYPICAL SECTIONS (1)			
TYPICAL SECTION - I-20	TS-1		4
QUANTITY SHEETS (3)			
SUMMARY OF QUANTITIES	SQ-1		5
ESTIMATED QUANTITIES - PAVEMENT MARKINGS AND TRAFFIC CONTROL ITEMS	EQ-1		6
ESTIMATED QUANTITIES - TRAFFIC CONTROL SIGNS	EQ-2		7
SPECIAL DESIGN - ROADWAY ITEMS (4)			
CONSTRUCTION SIGNING DETAIL - I-20	DCS-1		8
TRAFFIC CONTROL PLAN - PHASE 1	TC-1		9
TRAFFIC CONTROL PLAN - PHASE 2	TC-2		10
PAVEMENT MARKING DETAIL - I-20	PMD-1		11

ROADWAY STANDARD DRAWINGS - (11)			
PAVEMENT (1)			
PAVEMENT MARKING DETAILS FOR 2-LANE AND 4-LANE DIVIDED ROADWAYS	PM-1		6051
PROTECTIVE BARRIERS (1)			
CONCRETE MEDIAN BARRIER (PRECAST) (32")	CMB-3		6226
SIGNING (3)			
STANDARD ROADSIDE SIGNS	SN-3A		6304
STANDARD ROADSIDE SIGN ASSEMBLY AND INSTALLATION	SN-4		6306
STANDARD ROADSIDE SIGN ASSEMBLY AND INSTALLATION	SN-4A		6307
TRAFFIC CONTROL PLANS (6)			
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (EXTENDED PERIOD)	TCP-4		6354
TRAFFIC CONTROL PLAN (WORK DAY ONLY)	TCP-5		6355
HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS	TCP-8		6358
TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS	TCP-13		6363
LOCATION OF R16-3 SIGNS (SPEEDING FINES DOUBLED)	TCP-15		6365
TRAFFIC CONTROL DETAILS - DRUM PLACEMENT AND SHOULDER CLOSURE	TCP-16		6366
TOTAL SHEETS (NOT INCLUDING BRIDGE SHEETS) (22)			

12/10/2019 9:36 AM DI_SH.DGN PLAN DIVISION MISSISSIPPI DEPARTMENT OF TRANSPORTATION



GARVER, LLC		
PS & E PLANS - 12/10/2019		
FMS CON. # 107878/301000		
REVISIONS		
DATE	SHEET NO.	BY

REVISION	BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION
		DETAILED INDEX
		I-20
		PROJ. NO.: BR-0020-01(248)
		COUNTY: HINDS
		WORKING NUMBER
		DI-1
		SHEET NUMBER
		2
DATE	FILENAME: TC-1.DGN	
DESIGN TEAM	GARVER	CHECKED TWB
		DATE NOV 2019

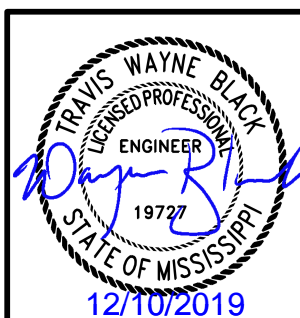


STATE	PROJECT NO.
MISS.	BR-0020-01(248)

GENERAL NOTES

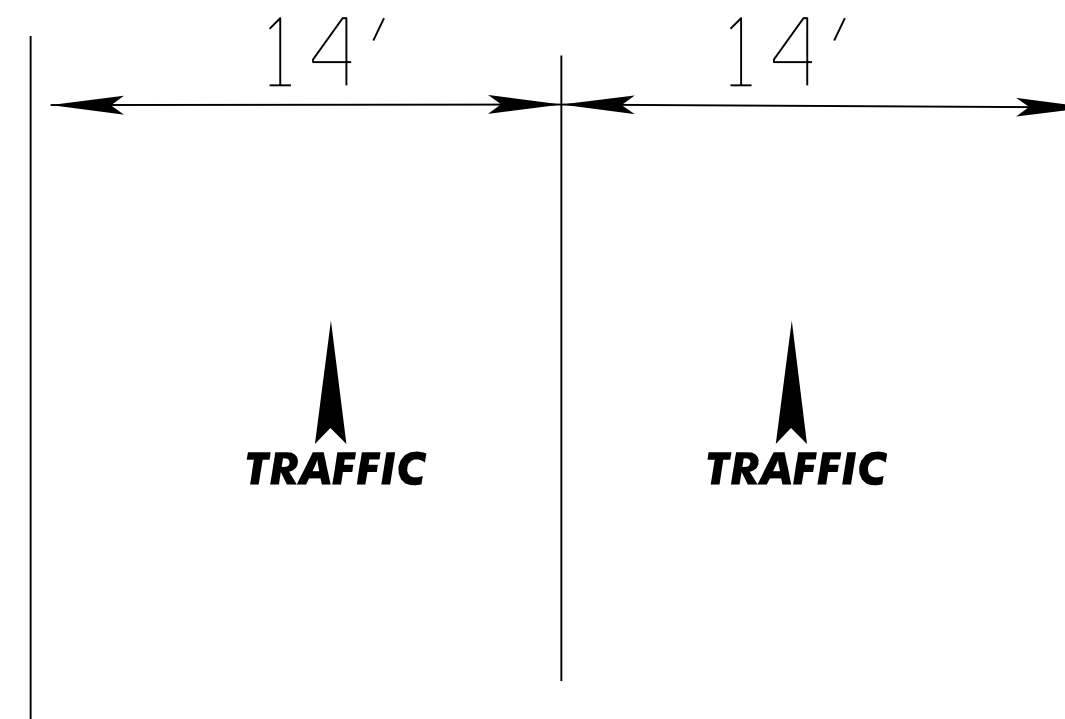
- (1) THE LOCATION AND SPACING OF SIGNS, SHOWN ON THE TRAFFIC CONTROL PLANS, ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.
- (2) ALL TRAFFIC CONTROL DEVICES ON THIS PROJECT SHALL COMPLY WITH PART VI OF THE **MUTCD** (LATEST EDITION).
- (3) ALL PLASTIC DRUMS SHALL HAVE A BALLASTING COLLAR MADE FROM RECYCLED TRUCK TIRES OR OTHER SUITABLE MATERIAL.
- (4) THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING STRUCTURES SUCH AS, BUT NOT LIMITED TO, PIPES, INLETS, APRONS, AND BRIDGES FROM DAMAGE WHICH MIGHT OCCUR DURING CONSTRUCTION. THE CONTRACTOR SHALL REPLACE OR REPAIR, AS DIRECTED BY THE ENGINEER, ANY STRUCTURES DAMAGED DURING THE LIFE OF THE CONTRACT. NO PAYMENT WILL BE MADE FOR REPLACEMENT OR REPAIR OF DAMAGED ITEMS.
- (5) FLUORESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.
- (6) THE CONTRACTOR SHALL COVER OR REMOVE ANY TEMPORARY TRAFFIC CONTROL SIGNS SHOWN IN THE TRAFFIC CONTROL PLAN THAT DO NOT APPLY TO THE CURRENT PHASE.
- (7) REMOVAL OF RAISED PAVEMENT MARKERS THAT ARE IN CONFLICT WITH REQUIRED CONSTRUCTION IS NOT CONSIDERED A SEPARATE PAY ITEM. COST TO BE ABSORBED IN OTHER ITEMS BID.
- (8) TEMPORARY STRIPING SHALL CONFORM TO FINISHED STRIPE SPECIFICATIONS FOR ALIGNMENT, NEATNESS, AND STRAIGHTNESS.
- (9) SEE BRIDGE PLANS FOR DETAILED INDEX SHEET(S), ESTIMATED AND SUMMARY OF QUANTITY SHEETS, AND EROSION CONTROL SHEETS.
- (10) ALL ADDENDA TO THESE PLANS WILL BE POSTED TO WWW.MDOT.MS.GOV UNDER THE PROPOSAL ADDENDA COLUMN. BIDDERS ARE ADVISED THAT HARD COPIES OF ANY ADDENDA FOR THIS PROJECT WILL NOT BE MAILED. IT IS THE BIDDER'S RESPONSIBILITY TO CHECK AND SEE IF ANY ADDENDA HAVE BEEN POSTED FOR THIS PROJECT.
- (11) THE BRIDGE DECKS SHALL BE GROOVED AND ALL BRIDGE JOINTS SHALL BE SEALED PRIOR TO OPENING THE BRIDGES TO TRAFFIC.
- (12) STORAGE OF FLAMMABLE MATERIALS WILL NOT BE ALLOWED UNDER ANY BRIDGE STRUCTURES.
- (13) SOME WORK IS REQUIRED OUTSIDE THE PROJECT LIMITS. NO ADDITIONAL COMPENSATION WILL BE MADE FOR SUCH WORK EXCEPT AS PROVIDED BY SPECIFIC PAY ITEMS INCLUDED IN THE PLANS.
- (14) THE CONTRACTOR SHALL COORDINATE WITH THE CONTRACTOR FROM ADJACENT PROJECT(S) IN IMPLEMENTING THE TRAFFIC CONTROL PLAN AS DIRECTED BY THE ENGINEER. ALL CONFLICTING SIGNS SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

12/10/2019 9:36 AM GN-1.DGN MISSISSIPPI DEPARTMENT OF TRANSPORTATION

		MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
		GENERAL NOTES	
	REVISION	I-20	
	DATE	PROJ. NO.: BR-0020-01(248) COUNTY: HINDS	
	BY	 WORKING NUMBER GN-1	
	DATE	FILENAME: TC-1.DGN	
	DESIGN TEAM	CHECKED	DATE
	GARVER	TWB	NOV 2019
			SHEET NUMBER
			3

STATE	PROJECT NO.
MISS.	BR-0020-01(248)

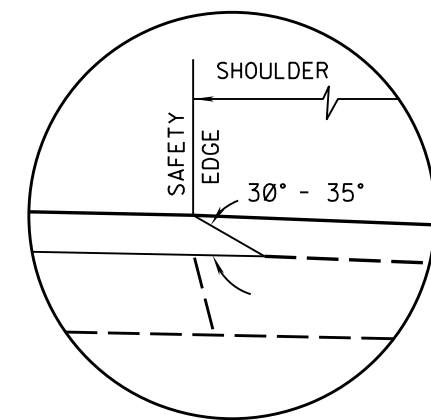
CL EB
LANES



2

1

- ① 1.00" OGFC
- ② 1.00" FINE MILLING



SAFETY EDGE REQ'D
(NOT A PAY ITEM)
OVERLAY 11' OR 12' PAV'MT.
WITHOUT TRENCH

*** OGFC PAVEMENT LAYER ON TOP OF BRIDGE END
SLAB WILL BE WIDENED 10' TO EACH SIDE TO
COVER THE ENTIRE CONCRETE SLAB.

TYPICAL SECTION - MILLING/OVERLAY

I-20 EASTBOUND
STA. 963+84 - STA. 977+52

ROADWAY PLAN DIVISION
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

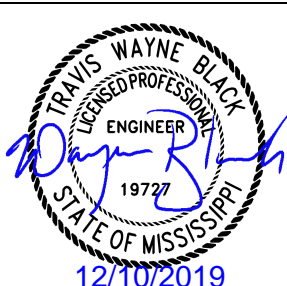
12/10/2019 9:36 AM TS.DGN

MISSISSIPPI DEPARTMENT OF TRANSPORTATION TYPICAL SECTION		
REVISION	BY	
	I-20	WORKING NUMBER TS-1
DATE	FILENAME: TS.DGN	SHEET NUMBER 4
DESIGN TEAM	GARVER	CHECKED TWB
		DATE NOV 2019

STATE	PROJECT NO.
MISS	BR-0020-01(248)

SUMMARY OF QUANTITIES (SHEET 1)

PAY ITEM NO.	PAY ITEM	UNIT	HINDS : 107878-301000	
			Prelim	Final
202-B240	Removal of Traffic Stripe	LF	1,954	
402-A002	Open Graded Friction Course, 9.5-mm Mixture	TON	253	
402-B001	Bituminous Tack Coat	GAL	646	
406-D001	Fine Milling of Bituminous Pavement, All Depths	SY	5,379	
618-A001	Maintenance of Traffic	LS	1	
907-618-Q002	Traffic Queue Protection	LS	1	
619-A1002	Temporary Traffic Stripe, Continuous White	LF	8,236	
619-A1005	Temporary Traffic Stripe, Continuous White, Type 1 Tape	LF	2,134	
619-A2002	Temporary Traffic Stripe, Continuous Yellow	LF	8,236	
619-A2006	Temporary Traffic Stripe, Continuous Yellow, Type 1 Tape	LF	2,134	
619-A3002	Temporary Traffic Stripe, Skip White	LF	6,504	
619-D1001	Standard Roadside Construction Signs, Less than 10 Square Feet	SF	37	
619-D2001	Standard Roadside Construction Signs, 10 Square Feet or More	SF	312	
907-619-E3001	Changeable Message Sign	EA	2	
619-F1001	Concrete Median Barrier, Precast	LF	1,153	
619-F2001	Remove and Reset Concrete Median Barrier, Precast	LF	1,153	
619-G4005	Barricades, Type III, Single Faced	LF	24	
619-G5001	Free Standing Plastic Drums	EA	32	
619-G7001	Warning Lights, Type "B"	EA	2	
619-J1004	Impact Attenuator, 60 MPH	EA	1	
619-J2004	Impact Attenuator, 60 MPH, Replacement Package	EA	1	
620-A001	Mobilization	LS	1	
626-A002	6" Thermoplastic Double Drop Traffic Stripe, Skip White	LF	3,406	
626-C001	6" Thermoplastic Double Drop Edge Stripe, Continuous White	LF	3,406	
626-F002	6" Thermoplastic Double Drop Edge Stripe, Continuous Yellow	LF	3,406	
627-K001	Red-Clear Reflective High Performance Raised Markers	EA	85	
699-A001	Roadway Construction Stakes	LS	1	

Revision	By	MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
		SUMMARY OF QUANTITIES	
Date	Date		Working Number
		PROJ NO: BR-0020-01(248) COUNTY: HINDS	SQ-1
FILENAME: SUMMARY OF QUANTITIES Design Team GARVER Checked TWB Date DEC. 2019		Sheet Number 5	

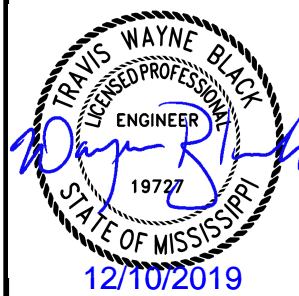
STATE	PROJECT NO.
MISS.	BR-0020-01(248)

SUMMARY OF PAVEMENT MARKINGS														
WK. SH. NO.	STATION TO STATION		DOUBLE DROP THERMOPLASTIC					MARKERS				REMARKS		
			CONTINUOUS		EDGE		SKIP		RED-CLEAR	YELLOW			CLEAR	
			WHITE	YELLOW	WHITE	YELLOW	WHITE	YELLOW		1 WAY	2 WAY		1 WAY	2 WAY
PMD-1	960+24	994+30			3406	3406	3406		85					
	UNITS		L. F.	L. F.	L. F.	L. F.	L. F.	L. F.	EACH	EACH	EACH	EACH	EACH	
	TOTALS				3406	3406	3406		85					

SUMMARY OF TRAFFIC CONTROL ITEMS REQUIRED																															
WORKING NUMBER	PHASE OF CONST.	PAINT				TAPE				BARRICADES		WARNING LIGHTS			FLASHING ARROW PANEL	FREE STANDING PLASTIC DRUMS	CHANGEABLE MESSAGE SIGN	TRAFFIC CONTROL SIGNS										REMARKS			
		CONTINUOUS		SKIP		CONTINUOUS		SKIP		TYPE III		TYPE A	TYPE B	TYPE C				G20 - 1	G20 - 2	R2 - 1	R16 - 3	W3 - 5	W4 - 2L	W4 - 2R	W20 - 1	W20 - 5L	W20 - 5R		ORANGE PLATE		
		WHITE	YELLOW	WHITE	YELLOW	WHITE	YELLOW	WHITE	YELLOW	SINGLE FACED	DOUBLE FACED																				
DCS-1										24		2				2		2	2	4	2	2			10			4			
	UNITS	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH			
	TOTAL									24		2				2		2	2	4	2	2			10			4			
TC-1	1	1370	1730	1730		977	977								24																
	UNITS	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH		
	TOTAL	1370	1730	1730		977	977								24																
TC-2	2	1730	1730			1157	1157								32																
	UNITS	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	EACH	LN. FT.	LN. FT.	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH		
	TOTAL	1730	1730			1157	1157								32																
PRE- OVERLAY		1730	1370	1368																											
	UNITS	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	EACH	LN. FT.	LN. FT.	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	
	TOTAL	1730	1370	1368																											
PRE- THERMO		3406	3406	3406																											
	UNITS	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	EACH	LN. FT.	LN. FT.	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	
	TOTAL	3406	3406	3406																											
	UNITS	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	LN. FT.	EACH	LN. FT.	LN. FT.	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	
	TOTAL	8236	8236	6504		2134	2134			24			2		32	2		2	4	2	2				10			4			
REMOVE & RESET															24																

12/10/2019 9:36 AM EQ-SH.DGN MISSISSIPPI DEPARTMENT OF TRANSPORTATION

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ESTIMATED QUANTITIES	
PAVEMENT MARKINGS AND TRAFFIC CONTROL ITEMS	
PROJ. NO.:	BR-0020-01(248)
COUNTY:	HINDS
FILENAME:	TC-1.DGN
DESIGN TEAM:	GARVER
CHECKED:	TWB
DATE:	NOV 2019



12/10/2019

WORKING NUMBER
EQ-1

SHEET NUMBER
6

SIGNS REQUIRED

SIGN NO.	SIZE	UNIT AREA SQ.FT.	QUAN. REQ'D.	TOTAL SIGN AREA SQ.FT.	REMARKS
G20 - 1	60" X 24"	10.00			ROAD WORK NEXT X X MILES
G20 - 2	48" X 24"	8.00	2	16.00	END ROAD WORK
G20 - 4	36" X 18"	4.50			PILOT CAR FOLLOW ME
1 M1 - 1	24" X 24"	4.00			1 OR 2 DIGIT
1 M1 - 1	30" X 24"	5.00			3 DIGIT
2 M1 - 4	24" X 24"	4.00			1 OR 2 DIGIT
2 M1 - 4	30" X 24"	5.00			3 DIGIT
3 M1 - 5	24" X 24"	4.00			1 OR 2 DIGIT
3 M1 - 5	30" X 24"	5.00			3 DIGIT
4 M3 - 1	24" X 12"	2.00			NORTH- 1 OR 2 DIGIT RTE. MARKER
4 M3 - 1	30" X 15"	3.13			NORTH- 3 DIGIT RTE. MARKER
4 M3 - 2	24" X 12"	2.00			EAST- 1 OR 2 DIGIT RTE. MARKER
4 M3 - 2	30" X 15"	3.13			EAST- 3 DIGIT RTE. MARKER
4 M3 - 3	24" X 12"	2.00			SOUTH- 1 OR 2 DIGIT RTE. MARKER
4 M3 - 3	30" X 15"	3.13			SOUTH- 3 DIGIT RTE. MARKER
4 M3 - 4	24" X 12"	2.00			WEST- 1 OR 2 DIGIT RTE. MARKER
4 M3 - 4	30" X 15"	3.13			WEST- 3 DIGIT RTE. MARKER
M4 - 8	24" X 12"	2.00			DETOUR- 1 OR 2 DIGIT RTE. MARKER
M4 - 8	30" X 15"	3.13			DETOUR- 3 DIGIT RTE. MARKER
M4 - 9	48" X 36"	12.00			DETOUR
M4 - 9L	48" X 36"	12.00			DETOUR
M4 - 9BL	48" X 36"	12.00			DETOUR
M4 - 9SL	48" X 36"	12.00			DETOUR
M4 - 9BSL	48" X 36"	12.00			DETOUR
M4 - 9R	48" X 36"	12.00			DETOUR
M4 - 9BR	48" X 36"	12.00			DETOUR
M4 - 9SR	48" X 36"	12.00			DETOUR
M4 - 9BSR	48" X 36"	12.00			DETOUR
M4 - 10L	48" X 18"	6.00			DETOUR
M4 - 10R	48" X 18"	6.00			DETOUR
4 M4 - 5	24" X 12"	2.00			TO
4 M5 - 1L	21" X 15"	2.19			
4 M5 - 1R	21" X 15"	2.19			
4 M5 - 2L	21" X 15"	2.19			
4 M5 - 2R	21" X 15"	2.19			
4 M6 - 1L	21" X 15"	2.19			
4 M6 - 1R	21" X 15"	2.19			
4 M6 - 2L	21" X 15"	2.19			
4 M6 - 2R	21" X 15"	2.19			
4 M6 - 3	21" X 15"	2.19			
R1 - 1	36" OCTAGON	7.46			STOP
R1 - 1	48" OCTAGON	13.25			
R1 - 2	48" X 48" X 48"	6.93			YIELD
R1 - 2	60" X 60" X 60"	10.83			

SIGNS REQUIRED (CONT'D)

SIGN NO.	SIZE	UNIT AREA SQ.FT.	QUAN. REQ'D.	TOTAL SIGN AREA SQ.FT.	REMARKS
R1 - 3	18" X 9"	1.13			3-WAY, 4 WAY ETC.
R1 - 3	24" X 12"	2.00			
R2 - 1	24" X 30"	5.00			
R2 - 1	36" X 48"	12.00			SPEED LIMIT
R2 - 1	48" X 60"	20.00	4	80.00	
R3 - 1	36" X 36"	9.00			
R3 - 1	48" X 48"	16.00			
R3 - 2	36" X 36"	9.00			
R3 - 2	48" X 48"	16.00			
R3 - 4	36" X 36"	9.00			
R3 - 4	48" X 48"	16.00			
R3 - 5L	30" X 36"	7.50			
R3 - 5R	30" X 36"	7.50			
R3 - 6L	30" X 36"	7.50			
R3 - 6R	30" X 36"	7.50			
R3 - 7L	30" X 30"	6.25			LEFT LANE MUST TURN LEFT
R3 - 7R	30" X 30"	6.25			RIGHT LANE MUST TURN RIGHT
R4 - 1	24" X 30"	5.00			DO NOT PASS
R4 - 1	48" X 60"	20.00			
R4 - 2	24" X 30"	5.00			PASS WITH CARE
R4 - 2	48" X 60"	20.00			
R4 - 7	48" X 60"	20.00			
R4 - 8	48" X 60"	20.00			
R5 - 1	48" X 48"	16.00			DO NOT ENTER
R5 - 1a	42" X 30"	8.75			WRONG WAY
R6 - 1L	36" X 12"	3.00			ONE WAY
R6 - 1R	36" X 12"	3.00			ONE WAY
R6 - 2L	24" X 30"	5.00			ONE WAY
R6 - 2R	24" X 30"	5.00			ONE WAY
R11 - 2	48" X 30"	10.00			ROAD CLOSED
R11 - 3a	60" X 30"	12.50			ROAD CLOSED XX MILES AHEAD
R11 - 3b	60" X 30"	12.50			BRIDGE OUT XX MILES AHEAD
R11 - 4	60" X 30"	12.50			ROAD CLOSED TO THRU TRAFFIC
R12 - 1	36" X 48"	12.00			WEIGHT LIMIT XX TONS
R16 - 3	36" X 48"	12.00			WHEN WORKERS ARE PRESENT SPEEDING FINES DOUBLED
R16 - 3	48" X 60"	20.00	2	40.00	
W1 - 1L	48" X 48"	16.00			
W1 - 1R	48" X 48"	16.00			
W1 - 2L	48" X 48"	16.00			
W1 - 2R	48" X 48"	16.00			
W1 - 3L	48" X 48"	16.00			
W1 - 3R	48" X 48"	16.00			
W1 - 4aL	48" X 48"	16.00			
W1 - 4aR	48" X 48"	16.00			
W1 - 5L	48" X 48"	16.00			
W1 - 5R	48" X 48"	16.00			
W1 - 6L	48" X 24"	8.00			
W1 - 6L	60" X 30"	12.50			
W1 - 6R	48" X 24"	8.00			
W1 - 6R	60" X 30"	12.50			
W1 - 7	48" X 24"	8.00			

SIGNS REQUIRED (CONT'D)

SIGN NO.	SIZE	UNIT AREA SQ.FT.	QUAN. REQ'D.	TOTAL SIGN AREA SQ.FT.	REMARKS
W1 - 7	60" X 30"	12.50			
W1 - 8L	18" X 24"	3.00			
W1 - 8L	36" X 48"	12.00			
W1 - 8R	18" X 24"	3.00			
W1 - 8R	36" X 48"	12.00			
W1 - 9L	48" X 48"	16.00			
W1 - 9R	48" X 48"	16.00			
W3 - 1a	48" X 48"	16.00			
W3 - 2a	48" X 48"	16.00			
W3 - 3	48" X 48"	16.00			
W3 - 5	48" X 48"	16.00	2	32.00	SPEED REDUCTION
W4 - 1L	48" X 48"	16.00			
W4 - 1R	48" X 48"	16.00			
W4 - 2L	48" X 48"	16.00			
W4 - 2R	48" X 48"	16.00			
W5 - 1a	48" X 48"	16.00			PAVEMENT NARROWS
W6 - 1	48" X 48"	16.00			
W6 - 2	48" X 48"	16.00			
W6 - 3	48" X 48"	16.00			
W8 - 1	48" X 48"	16.00			BUMP
W8 - 4	48" X 48"	16.00			SOFT SHOULDER
W8 - 6	48" X 48"	16.00			TRUCK CROSSING
W8 - 7	48" X 48"	16.00			LOOSE GRAVEL
W8 - 9	48" X 48"	16.00			LOW SHOULDER
W8 - 11	36" X 36"	9.00			UNEVEN LANES
W8 - 12	48" X 48"	16.00			NO CENTER STRIPE
W10 - 1	36" DIA.	7.07			
W10 - 1	48" DIA.	12.56			
W13 - 1	24" X 24"	4.00			XX MPH
W14 - 3	36"X48"X48"	5.56			NO PASSING ZONE
W14 - 3	48"X64"X64"	9.89			
W16-2	24" X 18"	3.00			XXX FEET
W19 - 2	48" X 48"	16.00			BRIDGE MAY ICE IN COLD WEATHER
W20 - 1	48" X 48"	16.00	10	160.00	ADVANCE ROAD WORK
W20 - 1	36" X 36"	9.00			
W20 - 2	48" X 48"	16.00			ADVANCE DETOUR
W20 - 3	48" X 48"	16.00			ADVANCE ROAD CLOSED
W20 - 4	48" X 48"	16.00			ADVANCE ONE-LN. RD.
W20 - 4B	48" X 48"	16.00			ADVANCE ONE-LN. BR.
W20 - 5L	48" X 48"	16.00			ADVANCE LT. LN. CLOSED
W20 - 5R	48" X 48"	16.00			ADVANCE RT. LN. CLOSED
W20 - 7a	48" X 48"	16.00			
W21 - 1	36" X 36"	9.00			WORKERS
W21 - 1a	36" X 36"	9.00			

SIGNS REQUIRED (CONT'D)

SIGN NO.	SIZE	UNIT AREA SQ.FT.	QUAN. REQ'D.	TOTAL SIGN AREA SQ.FT.	REMARKS
W21 - 2	36" X 36"	9.00			FRESH OIL (TAR)
W21 - 3	48" X 48"	16.00			ADVANCE ROAD MACHINERY
W21 - 5	48" X 48"	16.00			SHOULDER WORK
W21 - 6	36" X 36"	9.00			SURVEY CREW
W24 - 1L	48" X 48"	16.00			
W24 - 1R	48" X 48"	16.00			
W24 - 1AL	48" X 48"	16.00			
W24 - 1AR	48" X 48"	16.00			
W24 - 1BL	48" X 48"	16.00			
W24 - 1BR	48" X 48"	16.00			
VP - 1L	12" X 36"	3.00			
VP - 1R	12" X 36"	3.00			
	40" X 16"	5.33	4	21.32	ORANGE PLATE
OM - 3L	12" X 36"	3.00			
OM - 3R	12" X 36"	3.00			
TOTAL SIGN AREA LESS THAN 10 SQ. FT.					37.32 SQ. FT.
TOTAL SIGN AREA 10 SQ. FT. OR MORE					312.00 SQ. FT.

- ① STANDARD
- ② SPECIAL (USE WHERE WARRANTED)

NOTES

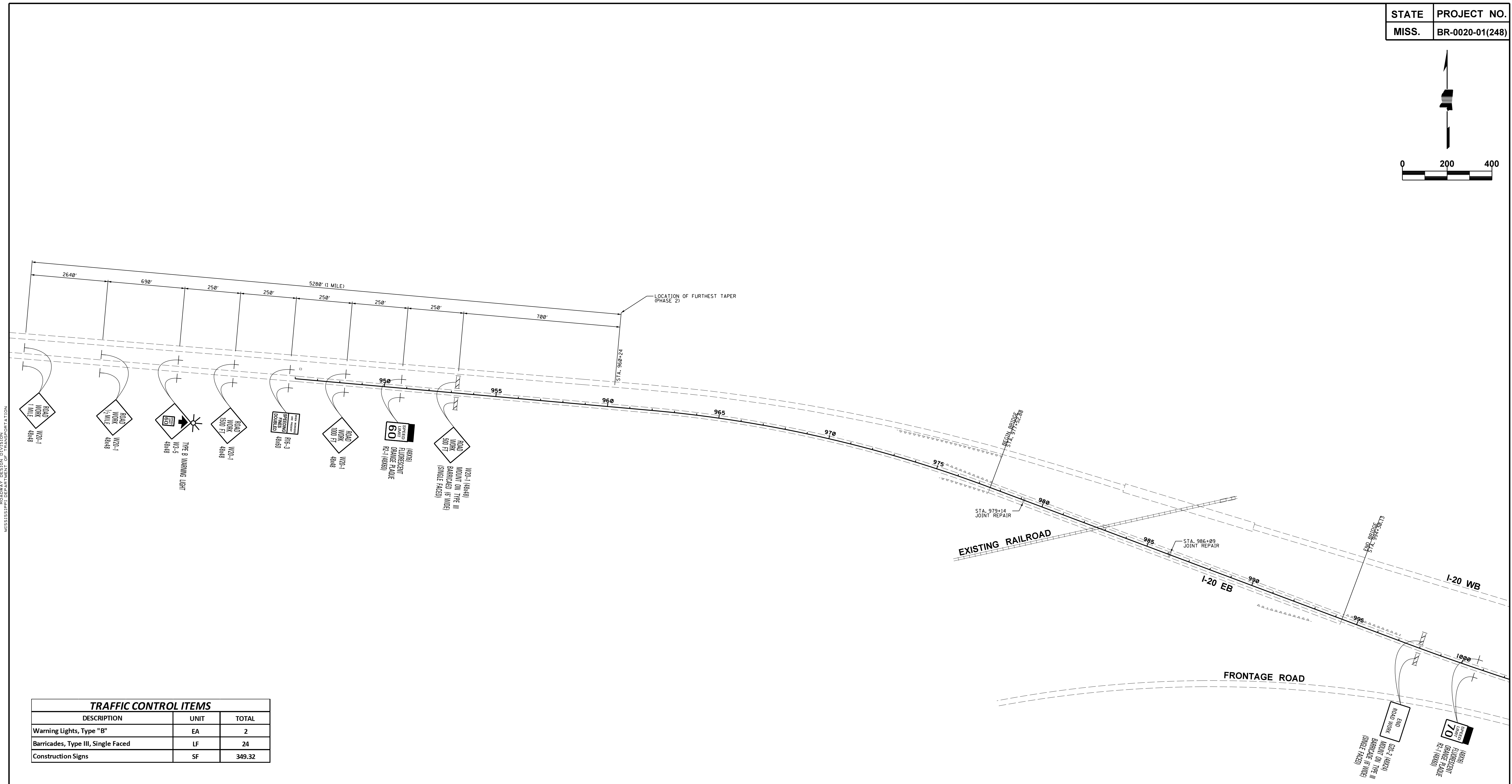
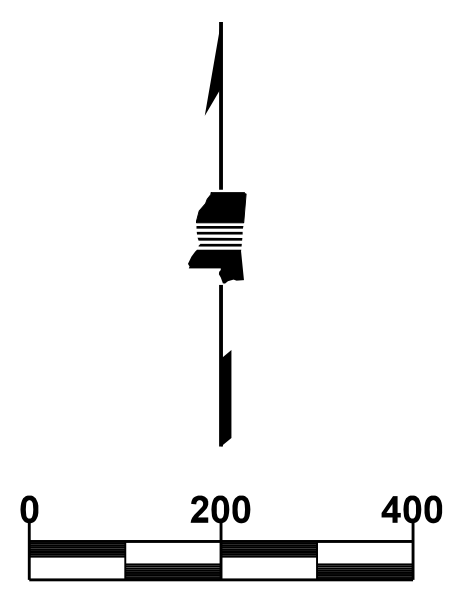
- ① INTERSTATE ROUTE MARKER
- ② UNITED STATES ROUTE MARKER
- ③ STATE ROUTE MARKER
- ④ COLORS OF CARDINAL DIRECTION MARKERS AND DIRECTIONAL ARROWS SHALL BE APPROPRIATE TO MATCH ACCOMPANYING ROUTE MARKERS.
- ⑤ BLACK STRIPES ON YELLOW BACKGROUND
- ⑥ INTERSTATE USE ONLY
- ⑦ TOP OF SIGN - BLACK LETTERING ON ORANGE BACKGROUND, BOTTOM OF SIGN - BLACK LETTERING ON WHITE BACKGROUND

THE BACKGROUND OF ALL WARNING SIGNS ("W" SERIES) EXCEPT W10-1 SHALL BE ORANGE. THE W10-1 BACKGROUND SHALL BE YELLOW IN ALL CASES.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ESTIMATED QUANTITIES	
TRAFFIC CONTROL SIGNS	
PROJ. NO.: BR-0020-01(248)	
COUNTY: HINDS	
DATE	FILENAME: TC-1.DGN
DESIGN TEAM	GARVER
CHECKED	TWB
DATE	DEC 2019

WORKING NUMBER
EQ-2
SHEET NUMBER
7

STATE	PROJECT NO.
MISS.	BR-0020-01(248)



TRAFFIC CONTROL ITEMS		
DESCRIPTION	UNIT	TOTAL
Warning Lights, Type "B"	EA	2
Barricades, Type III, Single Faced	LF	24
Construction Signs	SF	349.32

CONSTRUCTION SIGNS					
DESCRIPTION	MUTCD. NO.	SIZE	SQ. FT.	QUANTITY	TOTAL SQ. FT.
END ROAD WORK	G20 - 2	48" X 24"	8	2	16
SPEED LIMIT	R2 - 1	48" X 60"	20	4	80
SPEEDING FINES DOUBLED	R16 - 3	48" X 60"	20	2	40
SPEED REDUCTION SIGN	W3 - 5	48" X 48"	16	2	32
ADVANCE ROAD WORK	W20 - 1	48" X 48"	16	10	160
ORANGE PLATE	ORANGE PLATE	48" X 16"	5.33	4	21.32
			TOTAL	24	349.32

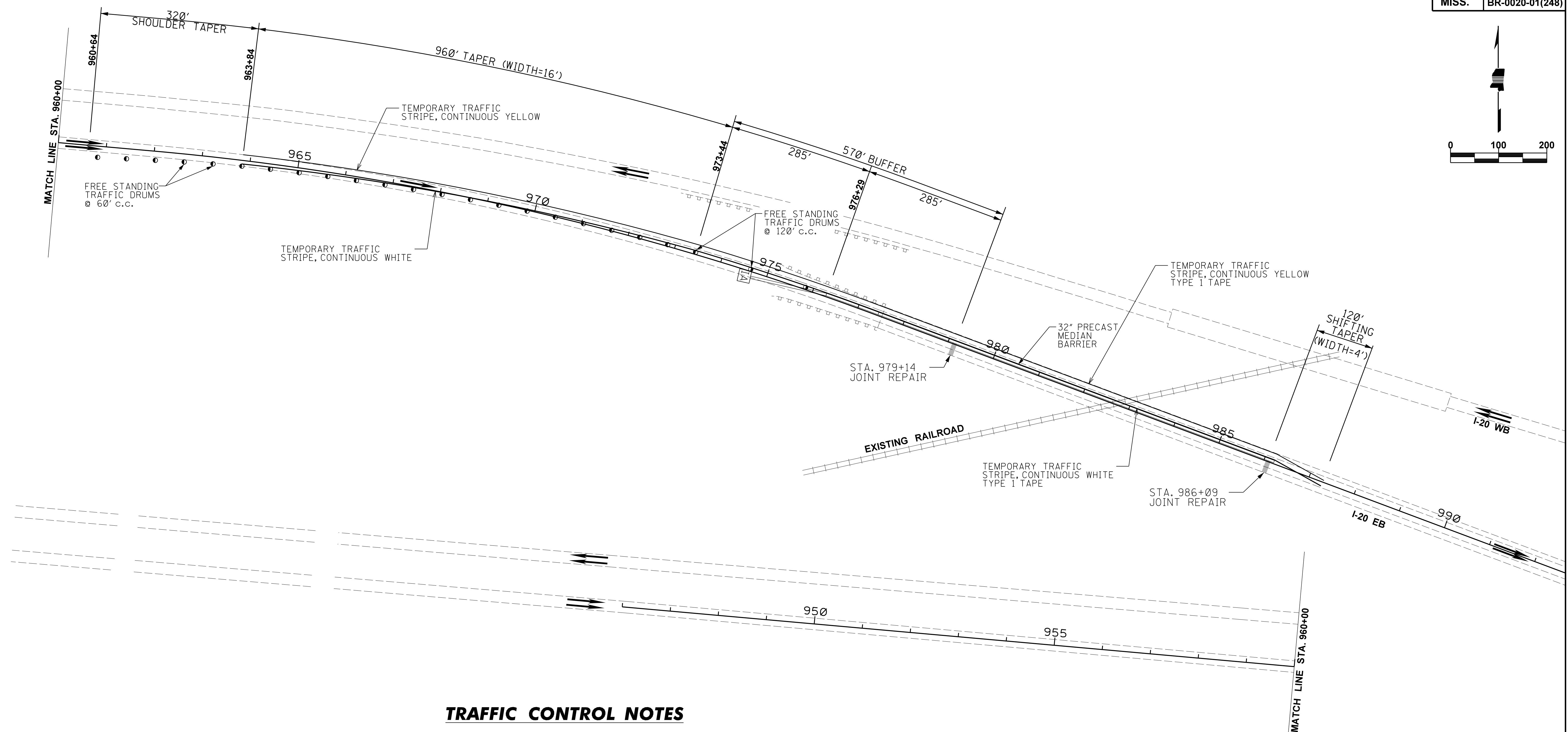
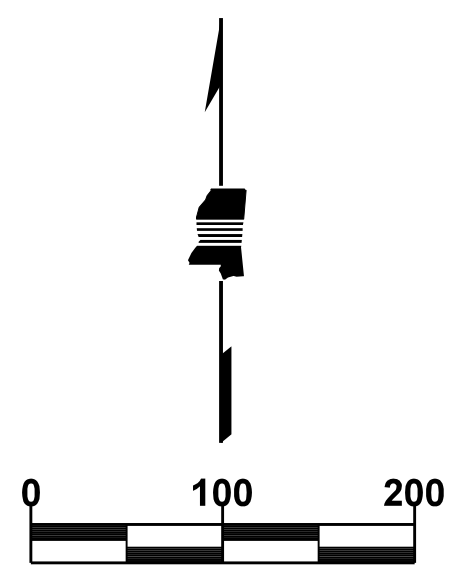
CONSTRUCTION AREA

MISSISSIPPI DEPARTMENT OF TRANSPORTATION CONSTRUCTION SIGNING DETAIL	
REVISION	BY
DATE	DATE
I-20	
PROJ. NO.: BR-0020-01(248)	
COUNTY: HINDS	
FILENAME: DCS-1.DGN	
DESIGN TEAM	CHECKED
GARVER	TWB
DATE NOV 2019	

WORKING NUMBER
DCS-1
SHEET NUMBER
8

12/10/2019 9:36 AM DCS-1.DGN

STATE	PROJECT NO.
MISS.	BR-0020-01(248)

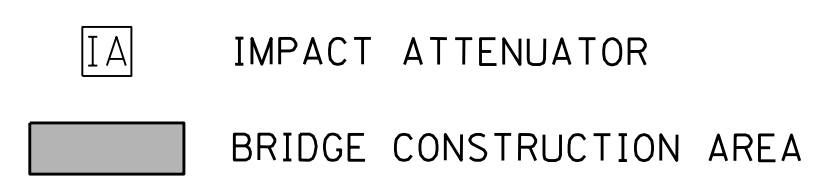


TRAFFIC CONTROL NOTES

- SEE STANDARD DRAWINGS, TCP-4 AND TCP-5 FOR SIGN LAYOUT DURING LANE CLOSURE OPERATIONS.
- SEE BRIDGE SHEET 8007 FOR PHASE 1 TYPICAL SECTIONS.

SEQUENCE OF CONSTRUCTION NOTES

- PRIOR TO SHIFTING TRAFFIC, FINE MILL ALL OGFC PAVEMENT (1" LIFT THICKNESS) BETWEEN STATION 963+84 AND BEGINNING OF BRIDGE STATION 977+52.
- PRIOR TO OPENING MILLED LANE TO TRAFFIC, TEMPORARY PAVEMENT JOINTS ARE TO BE PLACED AT MILLED JOINTS. TEMPORARY PAVEMENT JOINTS SHALL BE A MINIMUM OF 3 PAPER-WIDTHS LONG AND SHALL BE ADEQUATELY MAINTAINED.



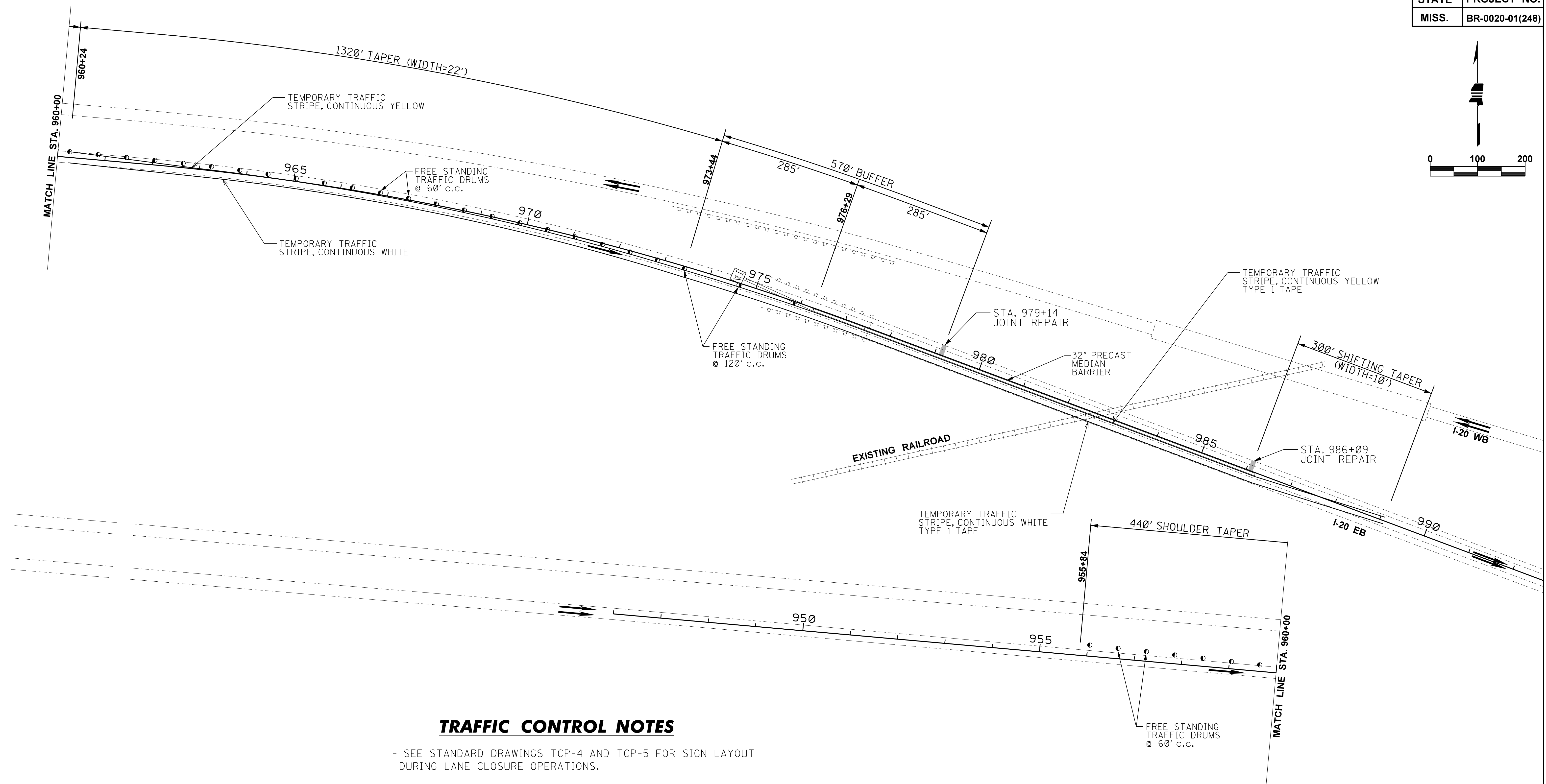
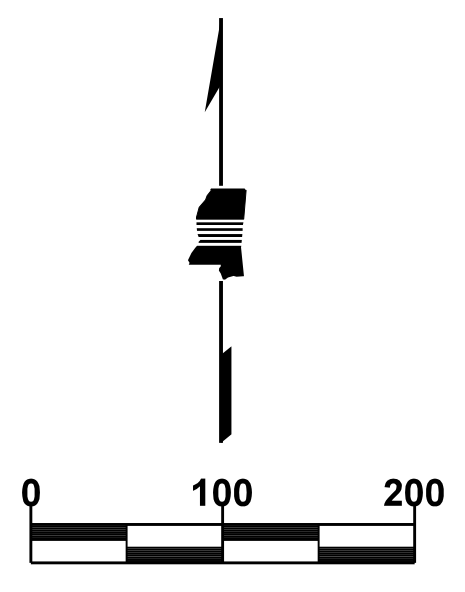
TRAFFIC CONTROL ITEMS		
DESCRIPTION	UNIT	TOTAL
Free Standing Plastic Drums	EA	24
Impact Attenuator, 60 MPH	EA	1
Concrete Median Barrier, Precast	LF	1153

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TRAFFIC CONTROL PLAN	
PHASE 1	
PROJ. NO.: BR-0020-01(248)	
COUNTY: HINDS	
DATE	FILENAME: TC-1.DGN
DESIGN TEAM	GARVER
CHECKED	TWB
DATE	DEC 2019

WORKING NUMBER
TC-1
SHEET NUMBER
9

12/10/2019 9:36 AM TC-1.DGN

STATE	PROJECT NO.
MISS.	BR-0020-01(248)



TRAFFIC CONTROL NOTES

- SEE STANDARD DRAWINGS TCP-4 AND TCP-5 FOR SIGN LAYOUT DURING LANE CLOSURE OPERATIONS.
- SEE BRIDGE SHEET 8008 FOR PHASE 2 TYPICAL SECTIONS.

SEQUENCE OF CONSTRUCTION NOTES

- AFTER JOINT REPAIR IS COMPLETE, REPLACE 1" LIFT OF OGFC BETWEEN STATIONS 963+84 AND 977+52 UNDER TRAFFIC, UTILIZING STANDARD LANE CLOSURES.

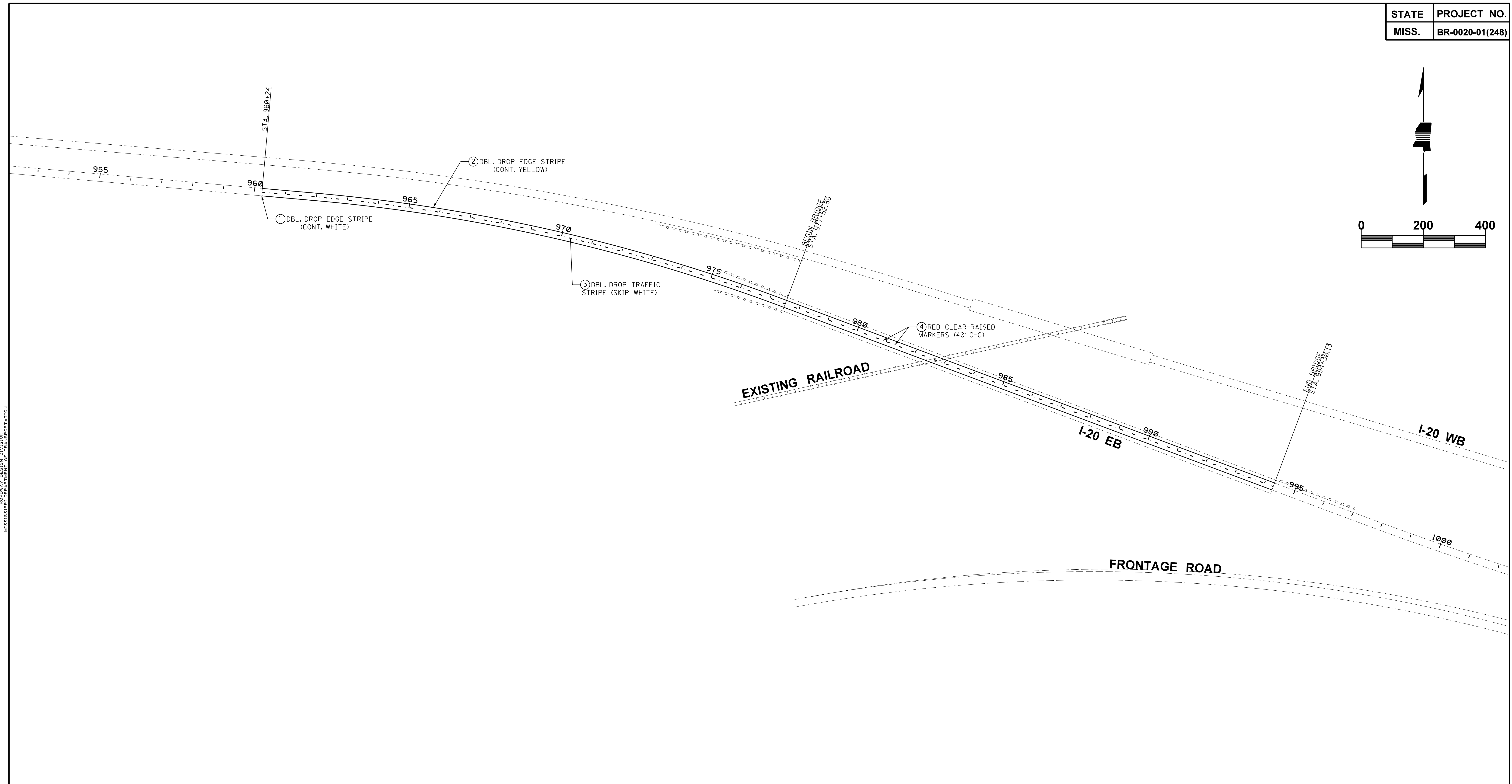
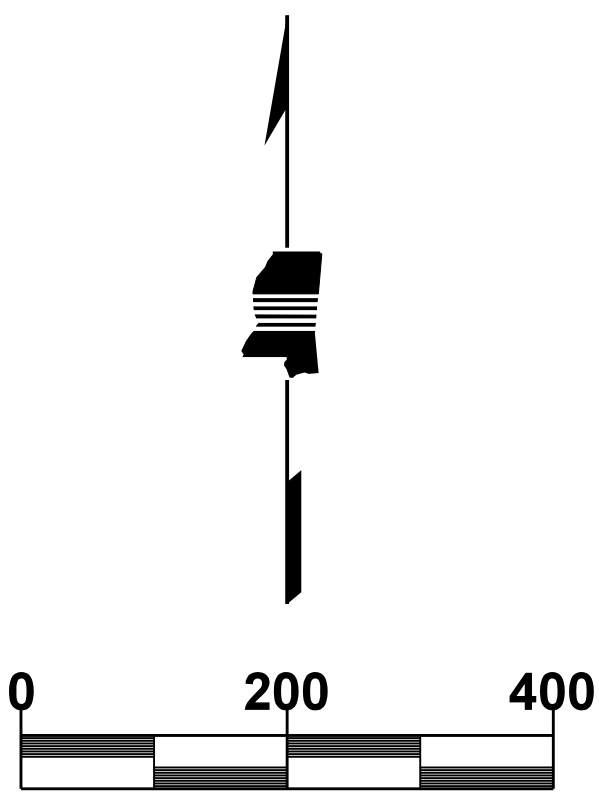
TRAFFIC CONTROL ITEMS		
DESCRIPTION	UNIT	TOTAL
Free Standing Plastic Drums	EA	32
Impact Attenuator, 60 MPH	EA	1
Concrete Median Barrier, Precast	LF	1153

- [IA] IMPACT ATTENUATOR
- [] BRIDGE CONSTRUCTION AREA

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TRAFFIC CONTROL PLAN	
PHASE 2	
PROJ. NO.: BR-0020-01(248)	
COUNTY: HINDS	
FILENAME: TC-2.DGN	WORKING NUMBER
DESIGN TEAM: GARVER	TC-2
CHECKED: TWB	SHEET NUMBER
DATE: NOV 2019	10

12/10/2019 9:36 AM TC-2.DGN

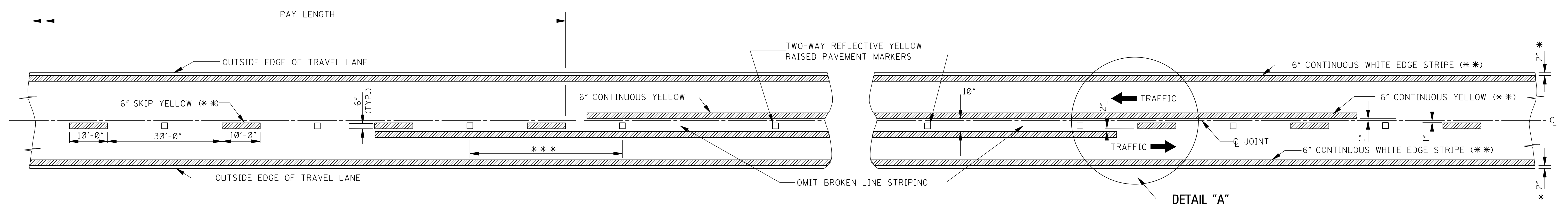
STATE	PROJECT NO.
MISS.	BR-0020-01(248)



1.2/10/2019 9:36 AM PMD-1.DGN

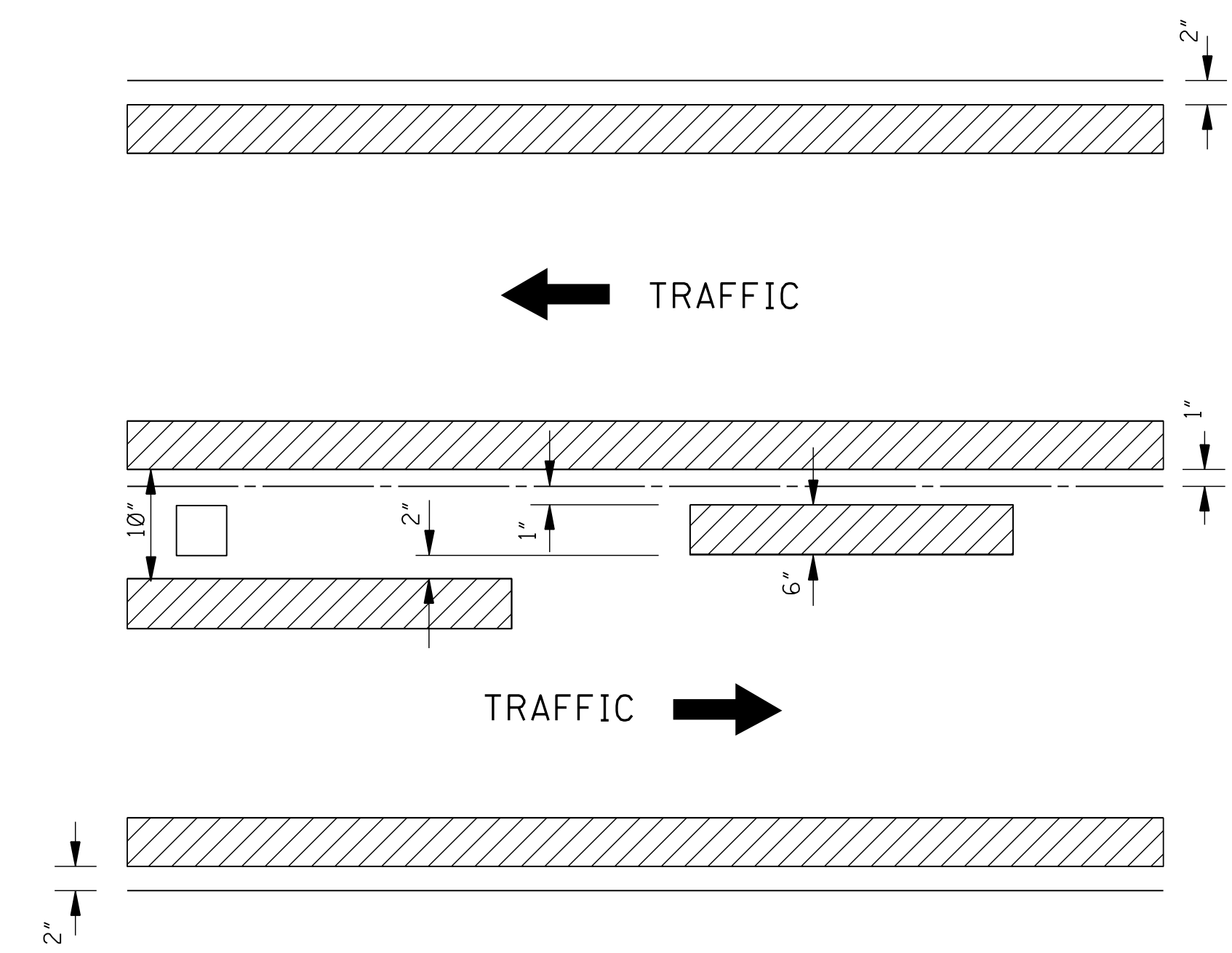
PMD-1				
I-20 STA. 960+24 to STA. 994+30				
PAVEMENT MARKINGS				
SYMBOL	DESCRIPTION	TYPE	QUANTITY	UNIT
①	6" THERMOPLASTIC DOUBLE DROP EDGE STRIPE CONTINUOUS WHITE	PLASTIC	3406	LF
②	6" THERMOPLASTIC DOUBLE DROP EDGE STRIPE CONTINUOUS YELLOW	PLASTIC	3406	LF
③	6" THERMOPLASTIC DOUBLE DROP TRAFFIC STRIPE, SKIP WHITE	PLASTIC	3406	LF
④	RED-CLEAR RAISED MARKERS	HIGH PERF	85	EA

MISSISSIPPI DEPARTMENT OF TRANSPORTATION PAVEMENT MARKING DETAIL	
REVISION	BY
I-20	
PROJ. NO.: BR-0020-01(248) COUNTY: HINDS	
DATE	FILENAME: PMD-1
DESIGN TEAM	GARVER
CHECKED	TWB
DATE	NOV 2019
WAYNE BLICK LICENSED PROFESSIONAL ENGINEER 19797 STATE OF MISSISSIPPI 12/10/2019	
WORKING NUMBER	PMD-1
SHEET NUMBER	11



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)

NOTE: THE CRITERIA FOR NO-PASSING ZONES CAN BE FOUND IN THE MDT ROADWAY DESIGN MANUAL, SECTION 11-1.01.



DETAIL "A"

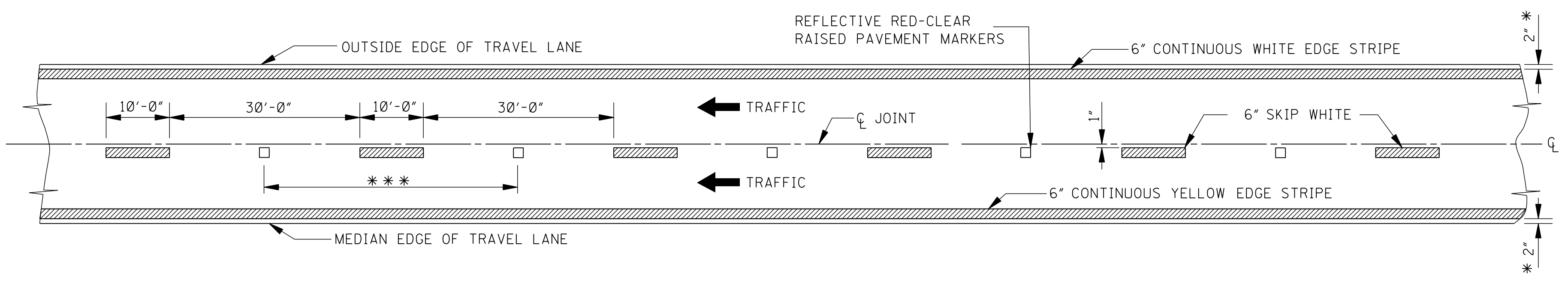
GENERAL NOTES:

- * 1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS. FOR STRIPING ON RUMBLE STRIP SECTIONS REFER TO WK. SHEETS RS-1, RS-2, AND RS-3.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
- *** 3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

	URBAN AREA (ft-in)	RURAL AREA (ft-in)
TANGENT SECTIONS	40'-0"	80'-0"
HORIZONTAL CURVES	40'-0"	40'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

† NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDT "APPROVED SOURCES OF MATERIALS."

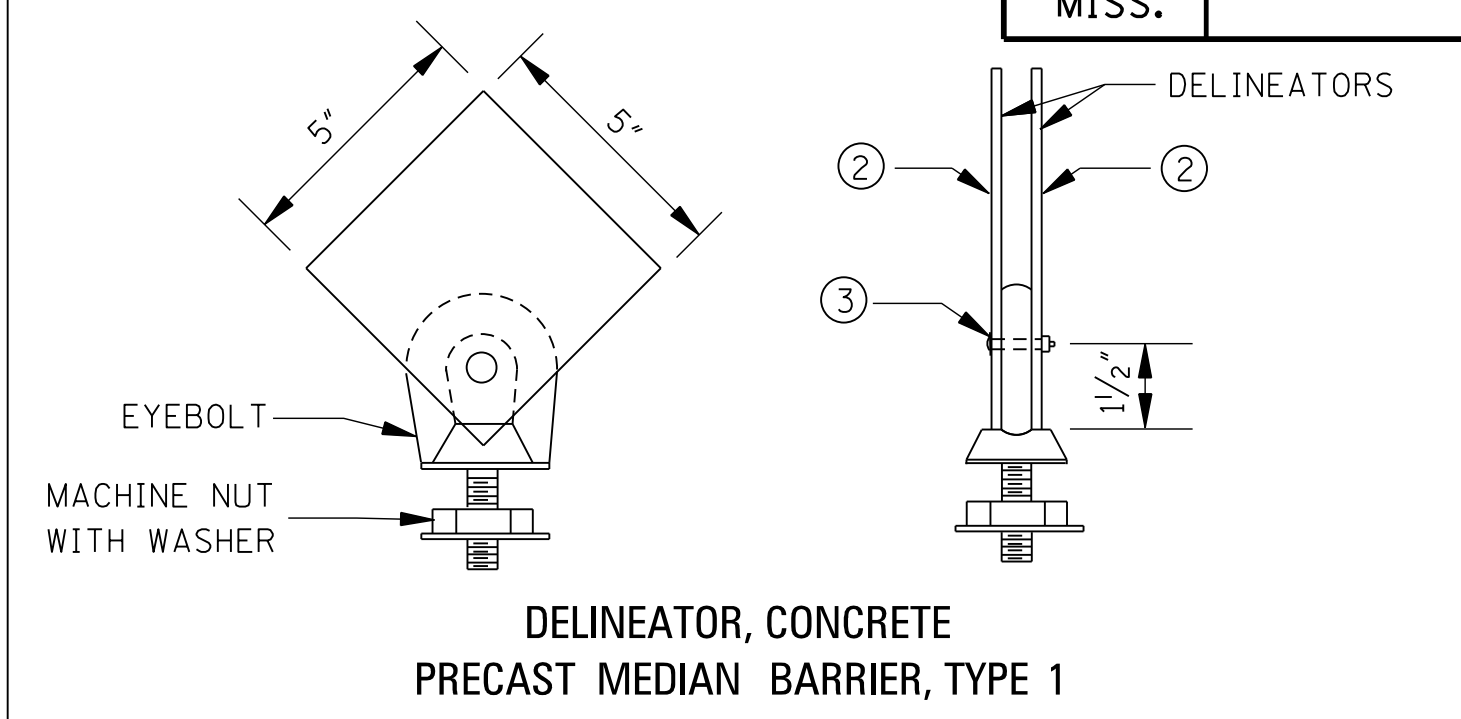
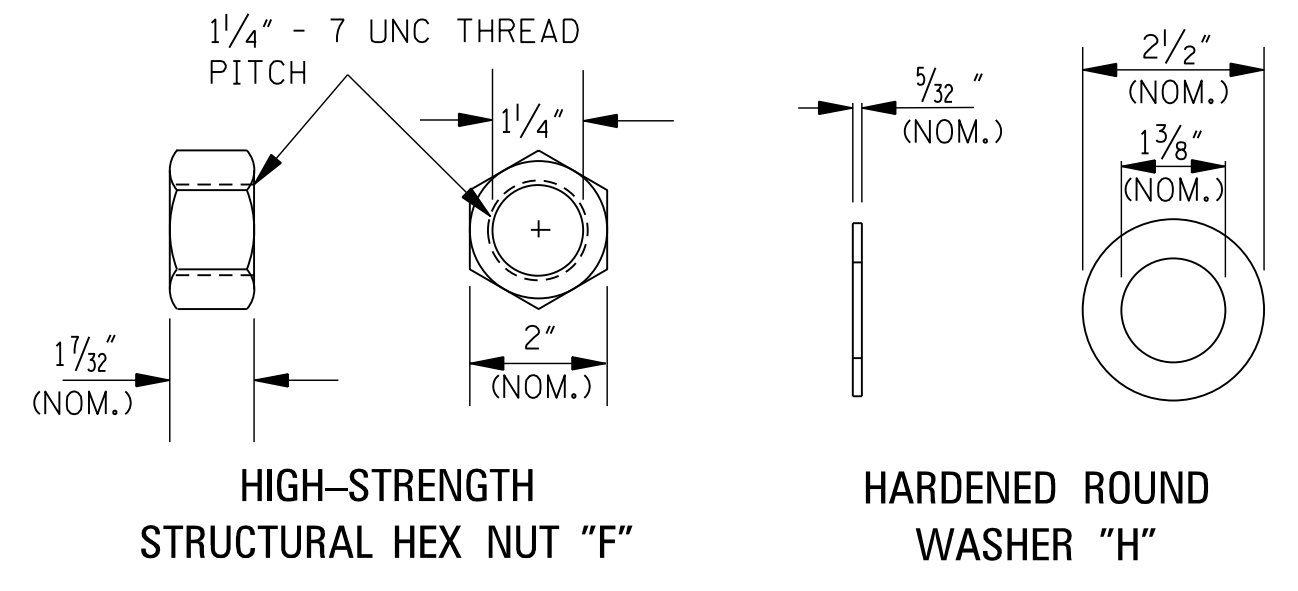
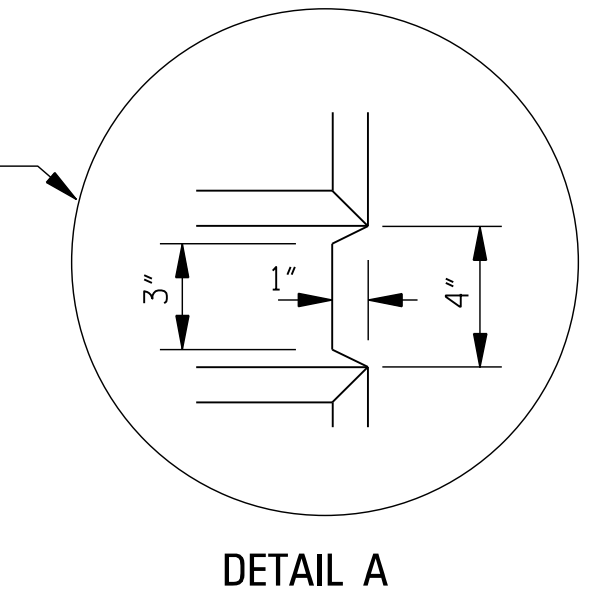
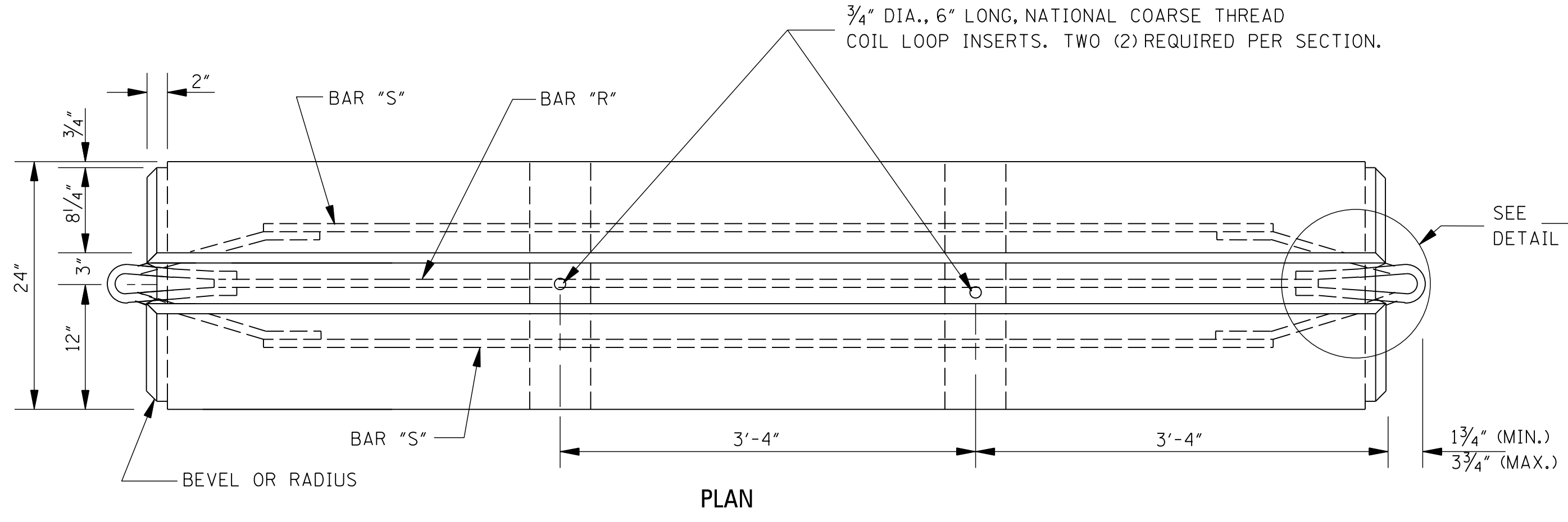


4-LANE WITH ONE-WAY TRAFFIC

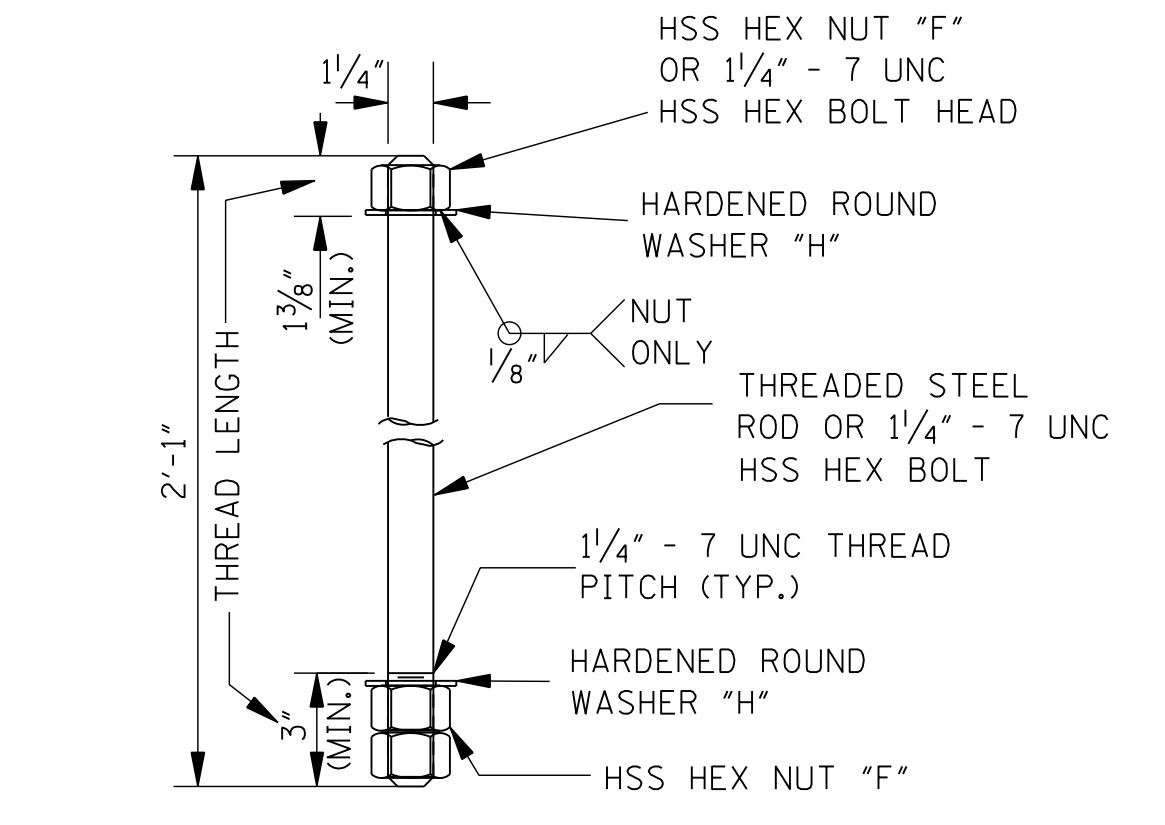
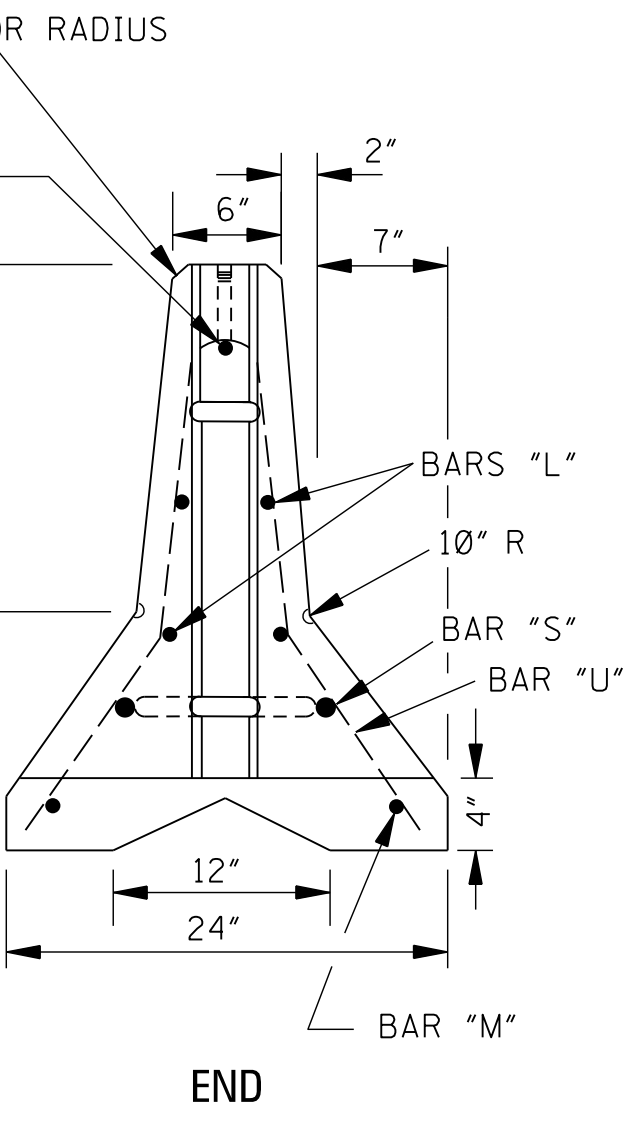
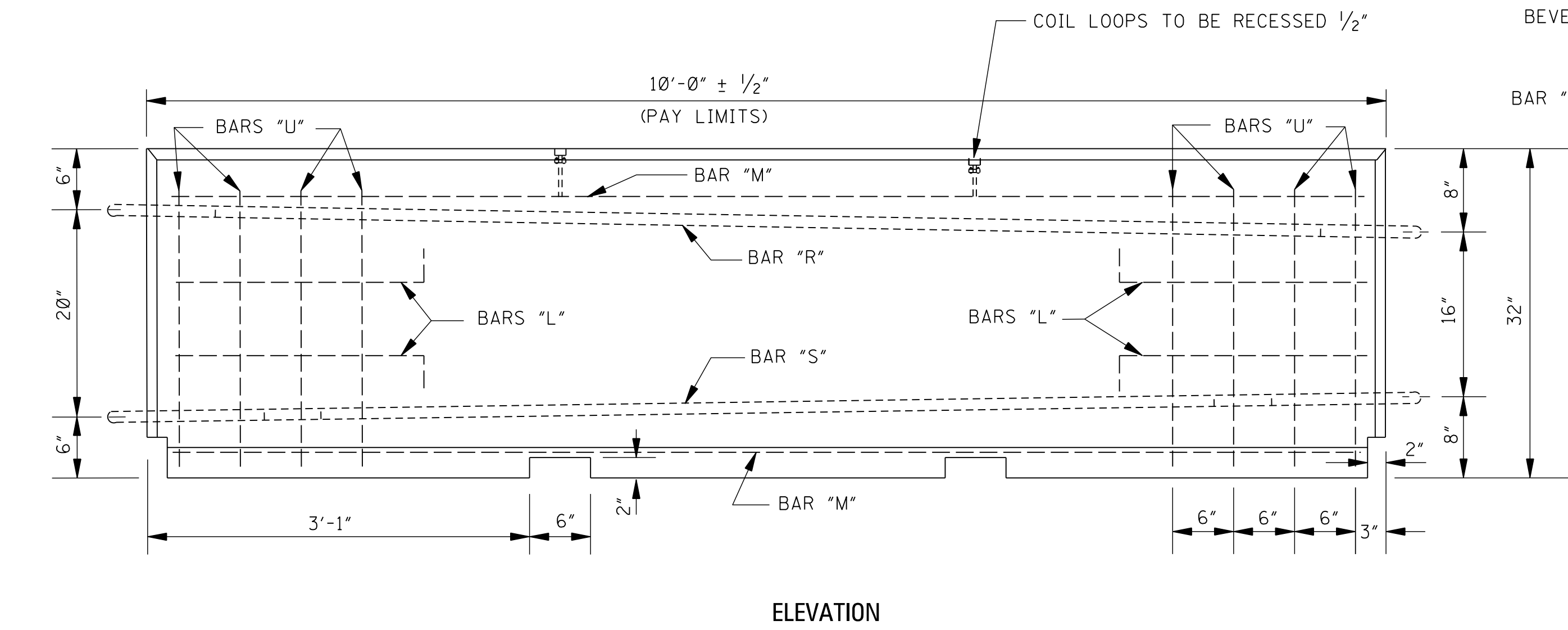
BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
REVISION	PAVEMENT MARKING DETAILS FOR 2-LANE AND 4-LANE DIVIDED ROADWAYS
DATE	ISSUE DATE: AUGUST 01, 2017



WORKING NUMBER
PM-1
SHEET NUMBER
6051

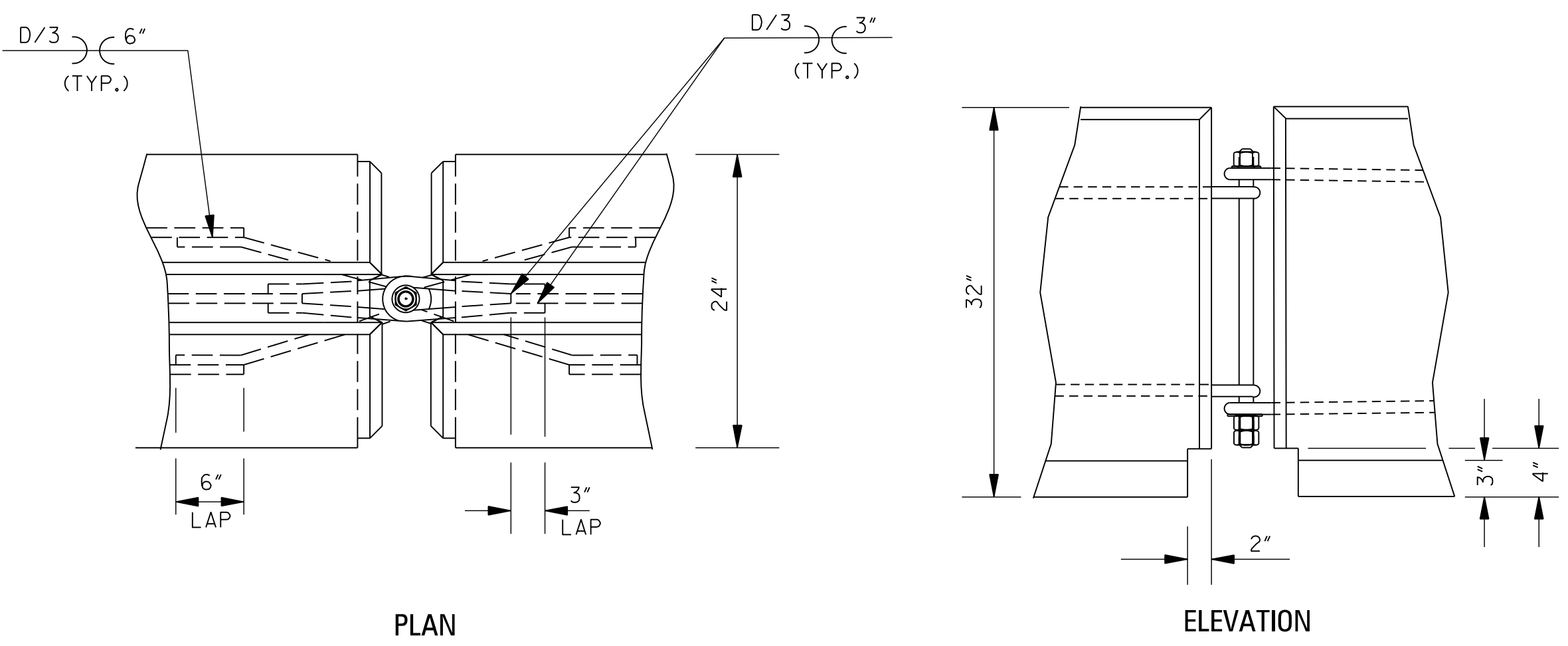
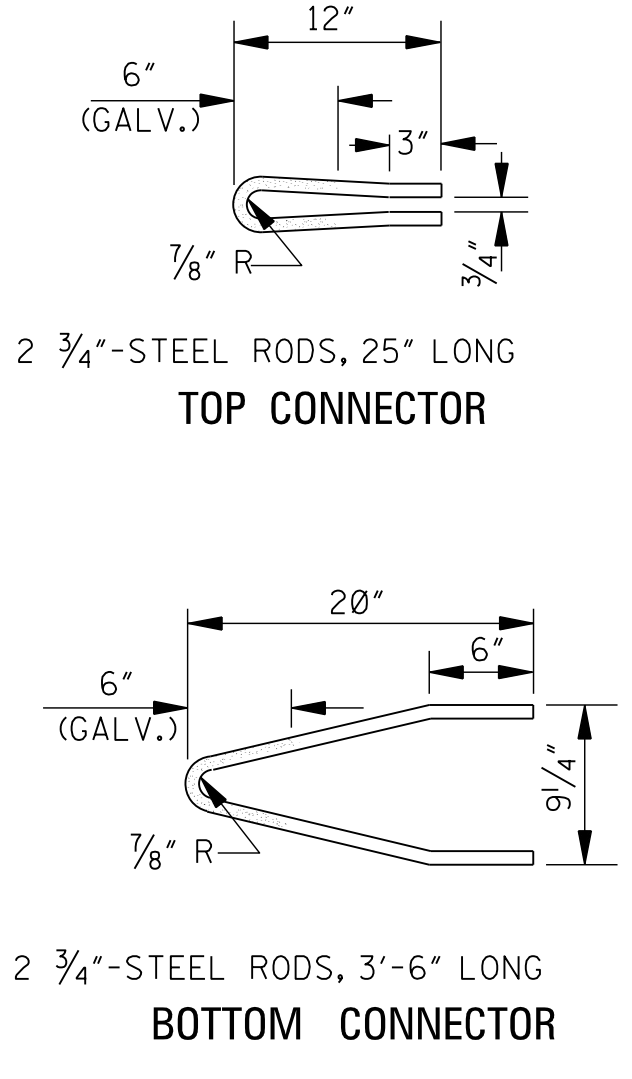
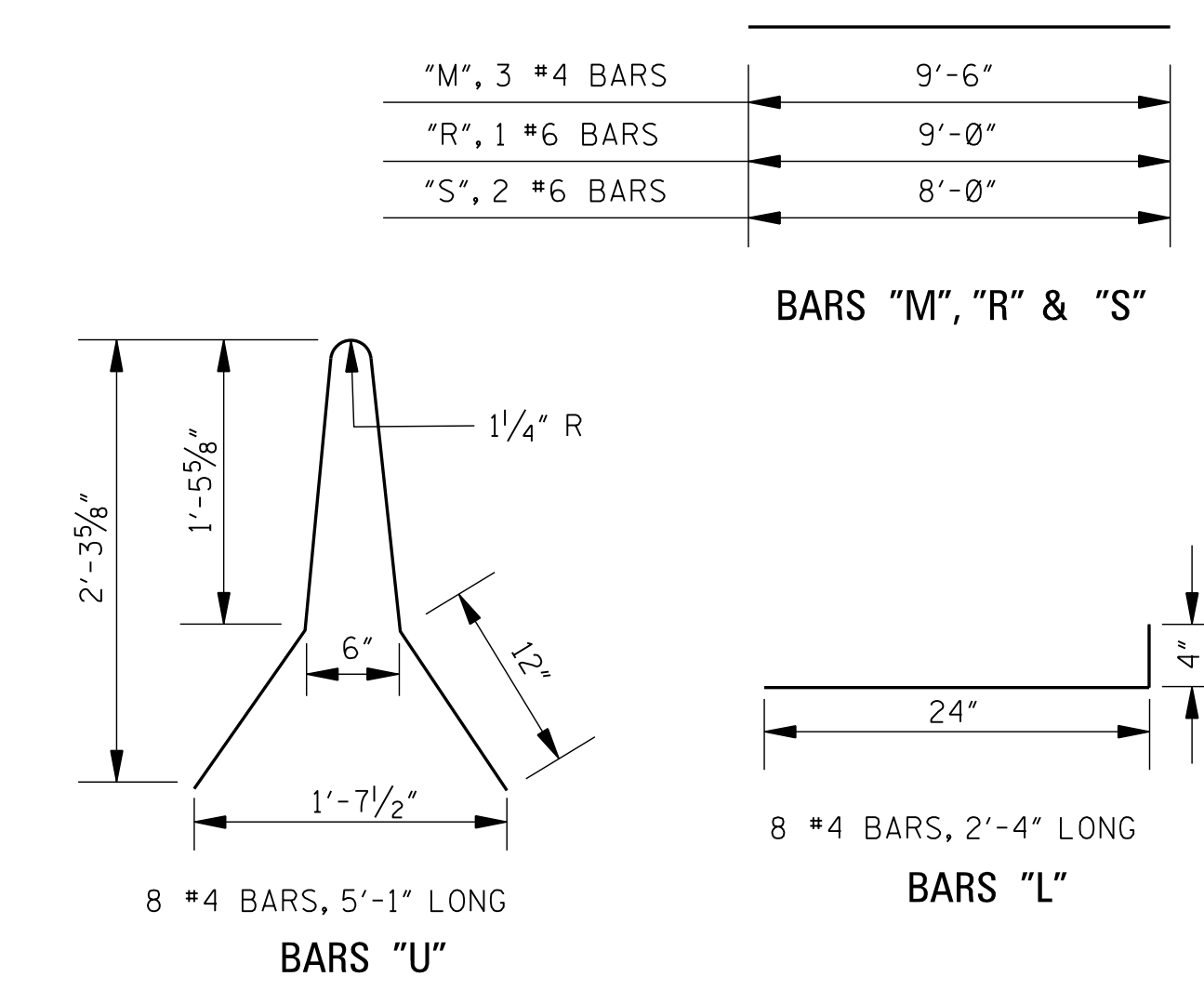


- DELINATORS SHALL BE REQUIRED UNLESS INDICATED OTHERWISE ON THE PLANS. THIS ITEM WILL NOT BE MEASURED SEPARATELY, BUT WILL BE INCLUDED UNDER PAYMENT FOR PRECAST CONCRETE BARRIER.
- DELINATORS SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING ON ALUMINUM SHEET, 0.080" THICK, OR SHEET STEEL, 14 GAGE, WHICH IS GALVANIZED.
- ALUMINUM OR STAINLESS STEEL SLOTTED ROUND HEAD MACHINE SCREW, NO. 10, 1 1/2" LONG, 2-WASHERS AND 1-HEX HEAD NUT (COMMERCIAL QUALITY)
- THE DELINATORS SHALL BE INSTALLED FACING TRAFFIC WITH YELLOW ON THE LEFT AND WHITE ON THE RIGHT, UNLESS OTHERWISE SPECIFIED.
- SPACINGS OF DELINATORS: TANGENT SECTION - 20'-0". CURVED SECTION - 10'-0".
- OPTIONAL DELINATORS, WHICH ARE ON THE MISSISSIPPI DEPARTMENT OF TRANSPORTATION "LIST OF APPROVED MATERIALS", WILL BE ACCEPTED.



NOTE: ALTERNATE METHODS OF CONNECTING PRECAST BARRIERS, SUCH AS J-HOOKS, MAY BE SUBSTITUTED IF APPROVED BY THE ENGINEER.

- GENERAL NOTES:
- LIFTING DEVICES AND ATTACHMENTS TO BARRIER SECTIONS SHALL BE AS APPROVED BY THE ENGINEER.
 - PLACE ALL STEEL REINFORCEMENT 2" MINIMUM FROM OUTSIDE FACE OF WALL, EXCEPT AS OTHERWISE SHOWN.
 - THE ANCHOR STUD CONNECTOR SHALL CONFORM TO AASHTO M 314, GRADE 55. THE HSS HEX NUTS AND THE HARDENED ROUND WASHERS SHALL MEET THE REQUIREMENTS OF ASTM A 325.
 - CONCRETE SHALL BE CLASS "B" (CLASS 1 FINISH). REINFORCING STEEL SHALL MEET THE REQUIREMENTS OF AASHTO M 31.
 - STEEL RODS SHALL MEET THE REQUIREMENTS OF ASTM A 36.
 - CONNECTOR RODS, CONNECTOR PINS, NUTS AND WASHERS SHALL BE GALVANIZED MEETING THE REQUIREMENTS OF AASHTO M 111.



APPROXIMATE QUANTITIES FOR 10' BARRIER			
WEIGHT (lbs.)	REINF. STEEL (lbs.)	STEEL RODS (lbs.)	CONCRETE (yd ³)
3875	104	18	0.931

BAR AND ROD DETAILS
NOTE: WHERE STEEL ROD GALVANIZATION IS SHOWN ABOVE, GALVANIZE AFTER BENDING.

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		<p align="center">CONCRETE MEDIAN BARRIER (PRECAST) (32")</p>	
DATE			
ISSUE DATE:		AUGUST 01, 2017	
WORKING NUMBER		CMB-3	
SHEET NUMBER		6226	

SIGN NUMBER	R1-1		R1-2			R2-1			R2-4a	R3-1		R3-2		R3-4		R3-5	R4-3	
ALUMINUM (6061-T6) SIGN BLANK THICKNESS	0.100"	0.125"	0.080"	0.100"	0.125"	0.080"	0.100"	0.125"	0.125"	0.080"	0.100"	0.080"	0.100"	0.080"	0.100"	0.080"	0.100"	0.125"
LEGEND																		
LETTER & NUMERAL SERIES	12" SERIES "C"	16" SERIES "C"	3" SERIES "C"			4" SERIES "E"			8" SERIES "E"	8" SERIES "E"		8" SERIES "E"		8" SERIES "E"		6" SERIES "D"	8" SERIES "D"	
WIDTH OF BORDER INSIDE	7/8" WHITE	1/4" WHITE	5" RED			5/8" BLACK			7/8" BLACK	5/8" BLACK		5/8" BLACK		5/8" BLACK		3/4" BLACK	7/8" BLACK	
WIDTH OF BORDER OUTSIDE	1/4" WHITE	1/4" WHITE	3/4" WHITE			3/8" WHITE			3/4" WHITE	3/8" WHITE		3/8" WHITE		3/8" WHITE		1/2" WHITE	1/4" BLACK	
SIZE (WIDTH X HEIGHT)	36" OCTAGON	48" OCTAGON	36" EQUIL. TRIANGLE			24" X 30"			48" X 96"	24" X 24"		24" X 24"		24" X 24"		30" X 36"	36" X 48"	
COLORS COPY BACKGROUND	WHITE RED	WHITE RED	RED WHITE			BLACK WHITE			BLACK WHITE	BLACK & RED WHITE		BLACK & RED WHITE		BLACK & RED WHITE		BLACK WHITE	BLACK WHITE	
REFLECTORIZATION	ALL	ALL	ALL			B'GROUND			BACKGROUND	BACKGROUND, CIRCLE, & DIAG.		BACKGROUND, CIRCLE, & DIAG.		BACKGROUND, CIRCLE, & DIAG.		BACKGROUND	BACKGROUND	
NUMBER OF POSTS FOR MOUNTING	1	1	1			1			1	1		1		1		1	1	
NUMBER OF HOLES TO BE PUNCHED (3/8" DIA.)	2	4	2			2			10	2		2		2		2	6	
PUNCHING DISTANCE FROM EACH VERTICAL EDGE	18" (VERT. CENTER)	9"	18" (VERT. CENTER)			12" (VERT. CENTER)			9"	12" (VERT. CENTER)		12" (VERT. CENTER)		12" (VERT. CENTER)		15" (VERT. CENTER)	9"	
PUNCHING DISTANCE FROM TOP EDGE	3"; 33"	9"; 39"	3"; 27"			3"; 21"			4"; 28"; 52"; 60"; 92"	3"; 21"		3"; 21"		3"; 21"		6"; 30"	4"; 30"; 56"	

SIGN NUMBER	R4-7			R6-3		R5-1		R5-1a	R6-1L, R6-1R		R6-2L, R6-2R		R8-4		R11-1	
ALUMINUM (6061-T6) SIGN BLANK THICKNESS	0.080"			0.125"		0.080"		0.100"	0.080"		0.080"		0.080"		0.125"	0.125"
LEGEND																
LETTER & NUMERAL SERIES	5/8" BLACK			1/4" BLACK		4" SERIES "D"		8" SERIES "D"	4" SERIES "D"		6" SERIES "D"		4" SERIES "D"		10" SERIES "C"	
WIDTH OF BORDER INSIDE	3/8" WHITE			3/4" WHITE		WHITE OUT-SIDE BORDER		1" WHITE	1/2" WHITE		5/8" BLACK		5/8" BLACK		1/4" BLACK	
WIDTH OF BORDER OUTSIDE	3/8" WHITE			3/4" WHITE		WHITE OUT-SIDE BORDER		1" WHITE	1/2" WHITE		3/8" WHITE		3/8" WHITE		3/4" WHITE	
SIZE (WIDTH X HEIGHT)	24" X 30"			30" X 24"		36" X 36"		42" X 30"	36" X 12"		24" X 30"		30" X 24"		48" X 60"	
COLORS COPY BACKGROUND	BLACK WHITE			BLACK WHITE		WHITE RED		WHITE RED	BLACK (WHITE ARROW) BLACK		BLACK WHITE		BLACK WHITE		BLACK WHITE	
REFLECTORIZATION	BACKGROUND			BACKGROUND		ALL		ALL	ARROW & BORDER		BACKGROUND		BACKGROUND		BACKGROUND	
NUMBER OF POSTS FOR MOUNTING	1			1		1		1	1		1		1		1	
NUMBER OF HOLES TO BE PUNCHED (3/8" DIA.)	2			4		2		4	2		2		4		6	
PUNCHING DISTANCE FROM EACH VERTICAL EDGE	12" (VERT. CENTER)			3"		9"		15" (VERT. CENTER)	18" (VERT. CENTER)		12" (VERT. CENTER)		15" (VERT. CENTER)		9"	
PUNCHING DISTANCE FROM TOP EDGE	3"; 27"			9"; 39"		4"; 30"; 56"		3"; 21"	1/2"; 10/2"		3"; 27"		3"; 21"		4"; 30"; 56"	

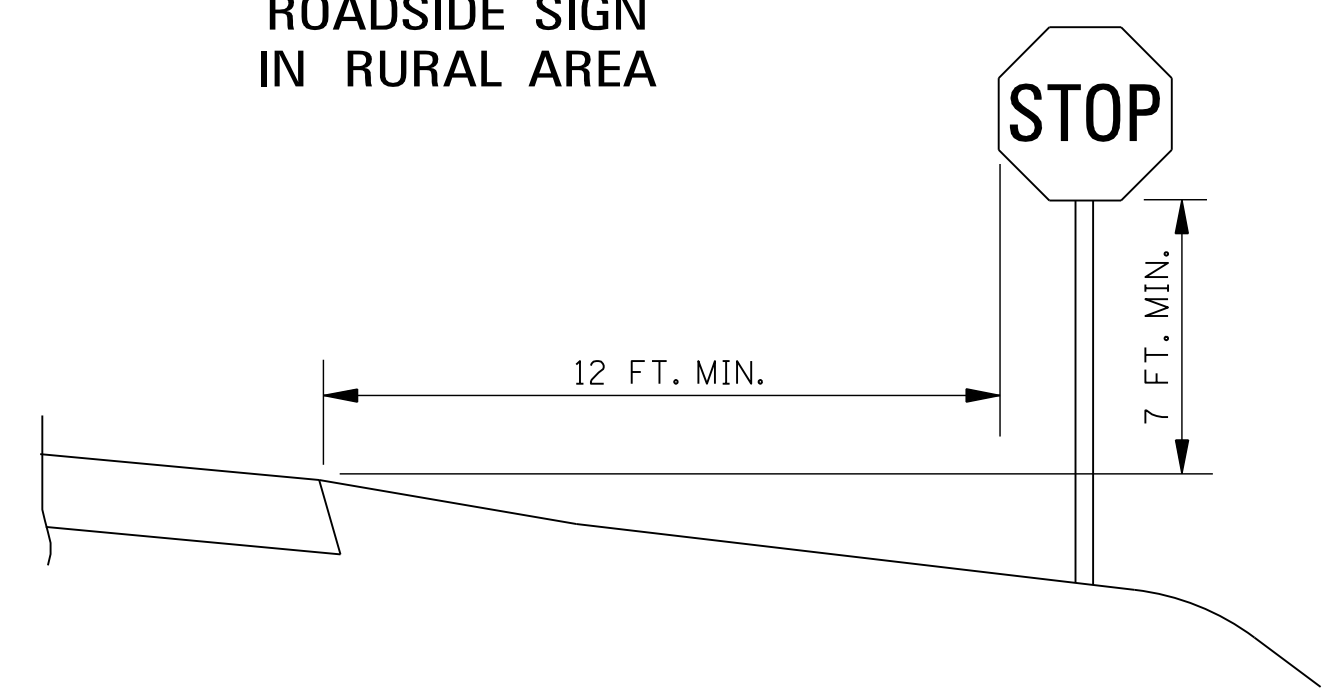
GENERAL NOTES:

- THE QUANTITIES LISTED ON THE SUMMARY OF QUANTITIES SHEET FOR THE SIGNS SHOWN ON THIS SHEET WILL BE USED AS THE BASIS FOR FINAL PAYMENT, EXCEPT WHERE SIGNS ARE MODIFIED FROM THAT SHOWN.
- THE SPEED LIMITS REQUIRED ON SIGNS R2-1 AND R2-4a WILL BE SHOWN ON INDIVIDUAL PLAN SHEETS.

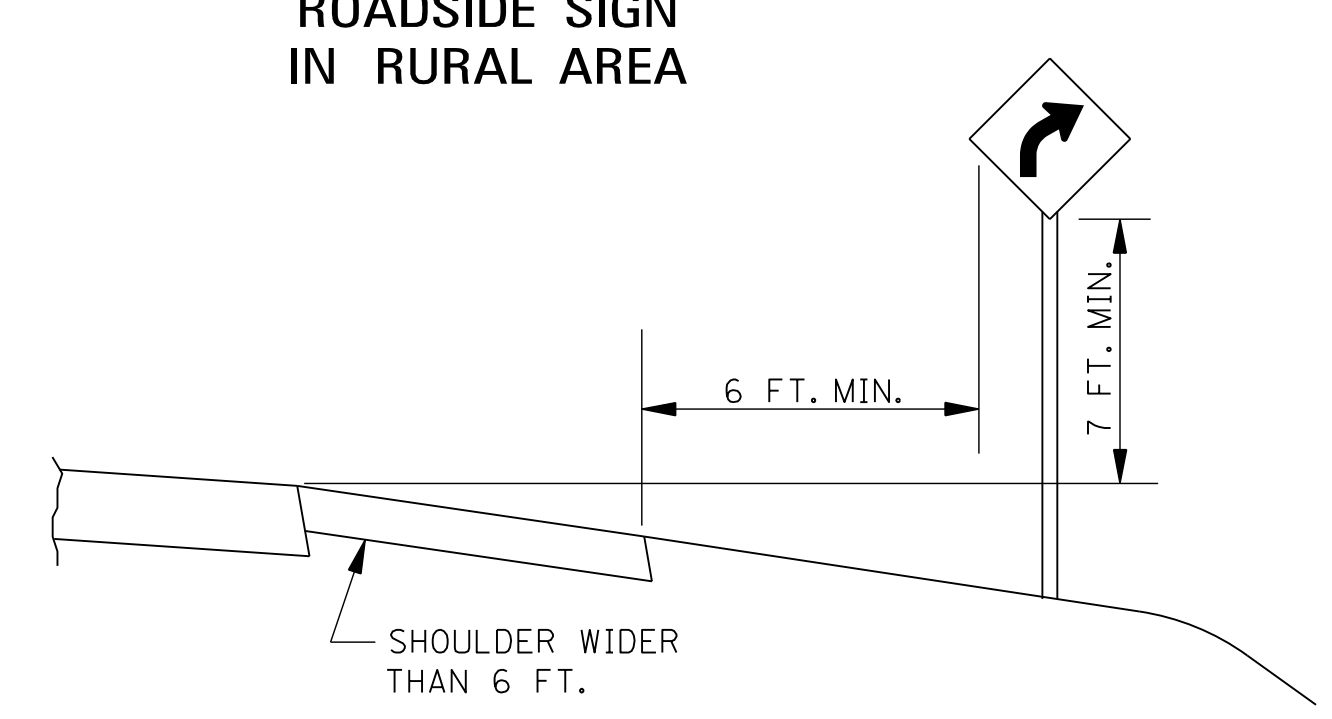
BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
REVISION	STANDARD ROADSIDE SIGNS
DATE	ISSUE DATE: AUGUST 01, 2017

WORKING NUMBER SN-3A
 SHEET NUMBER 6304

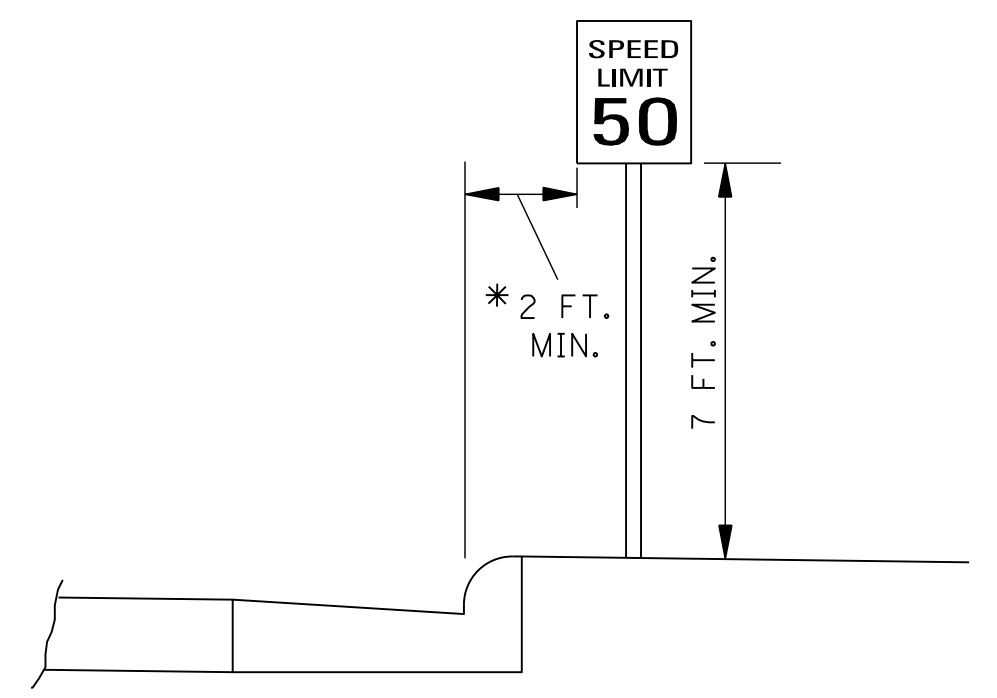
ROADSIDE SIGN IN RURAL AREA



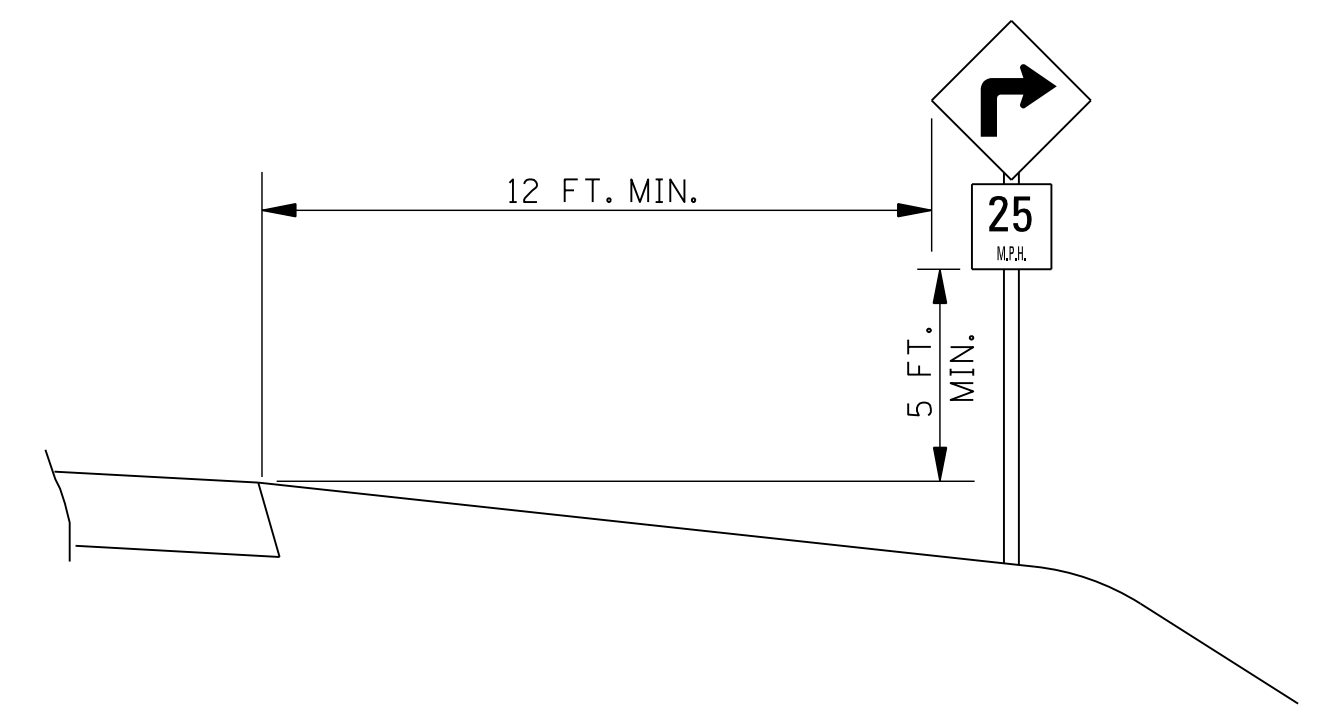
ROADSIDE SIGN IN RURAL AREA



ROADSIDE SIGN IN BUSINESS, COMMERCIAL, OR RESIDENTIAL AREA

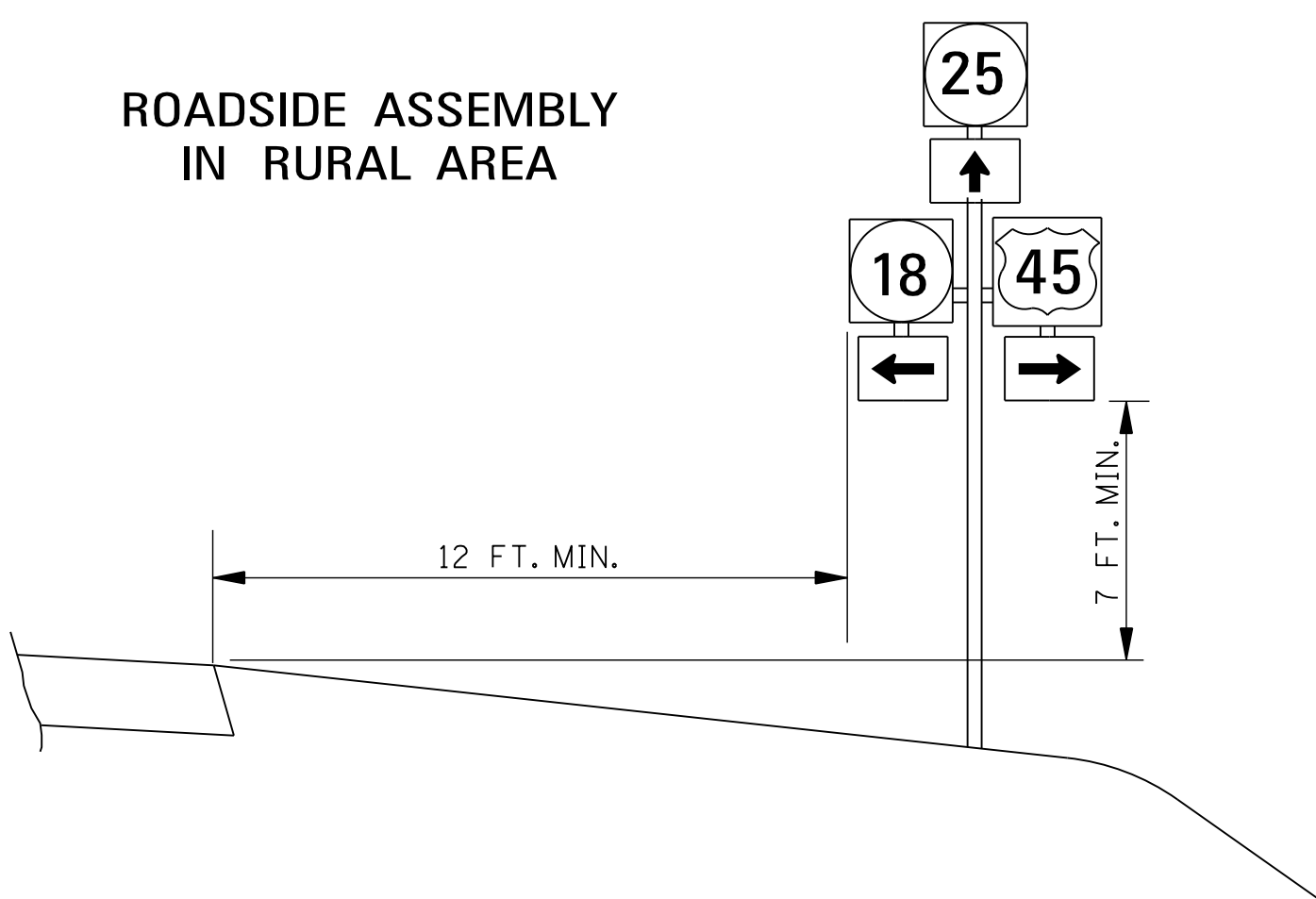


WARNING SIGN WITH ADVISORY SPEED PLAQUE IN RURAL AREA

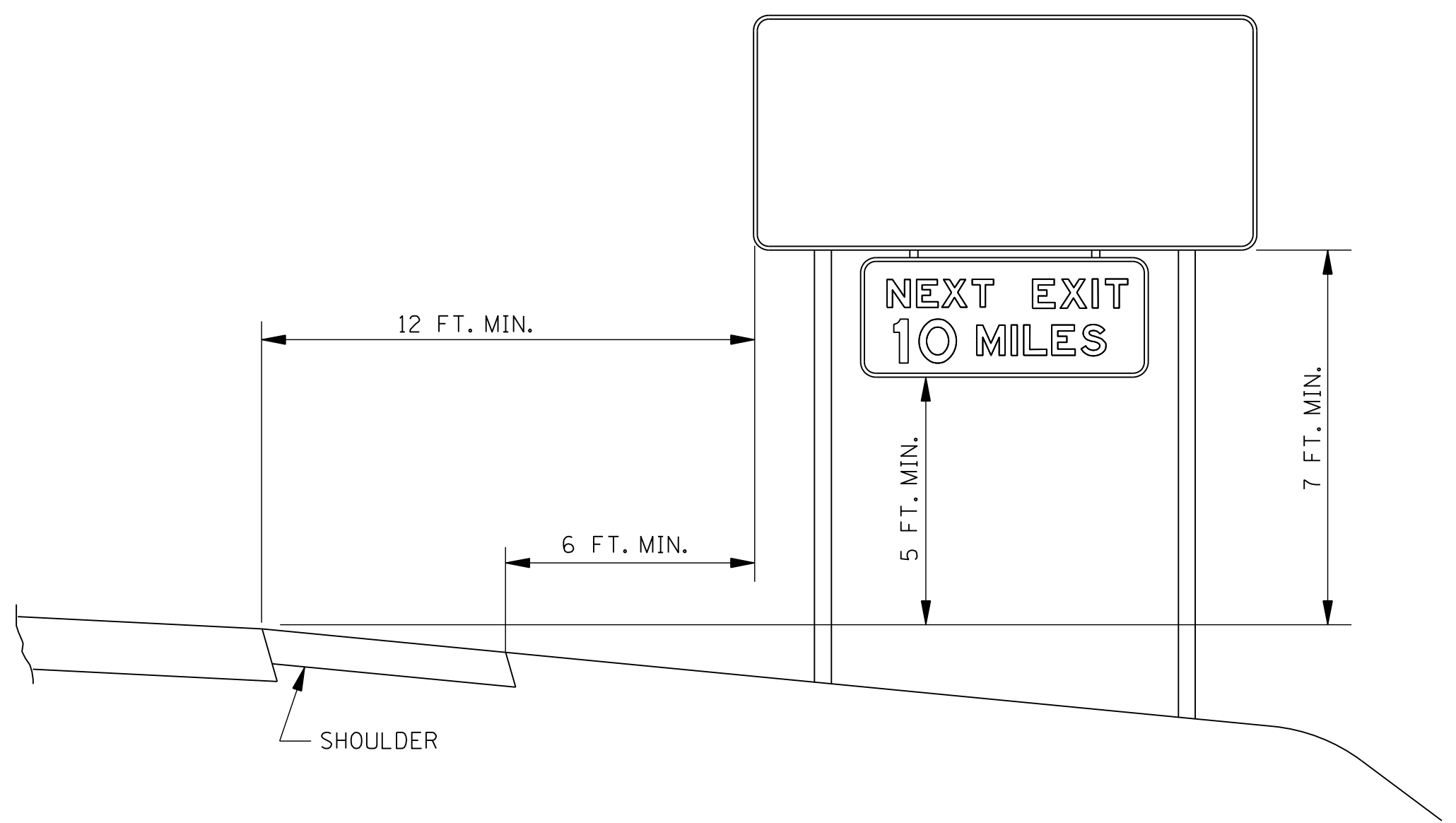


* THE 2 FT. MINIMUM OFFSET APPLIES ONLY TO STANDARD SIGNS MOUNTED ON U-POSTS. ALL STANDARD SIGNS MOUNTED ON PIPE WILL BE OFFSET A MINIMUM OF 4 FT.. RAMP DESTINATION SIGNS WILL BE OFFSET 4 FT. FROM THE SHOULDERS.

ROADSIDE ASSEMBLY IN RURAL AREA



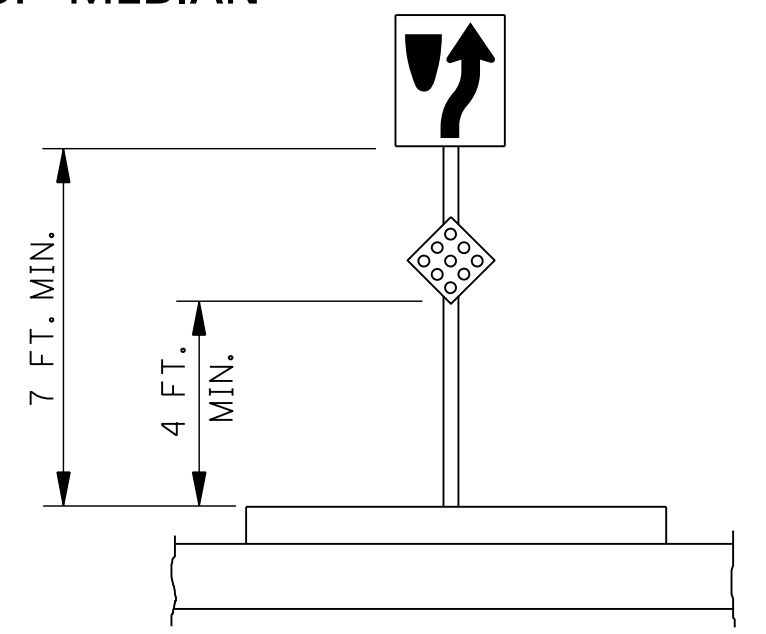
INTERSTATE OR FREEWAY SIGN WITH SECONDARY SIGN



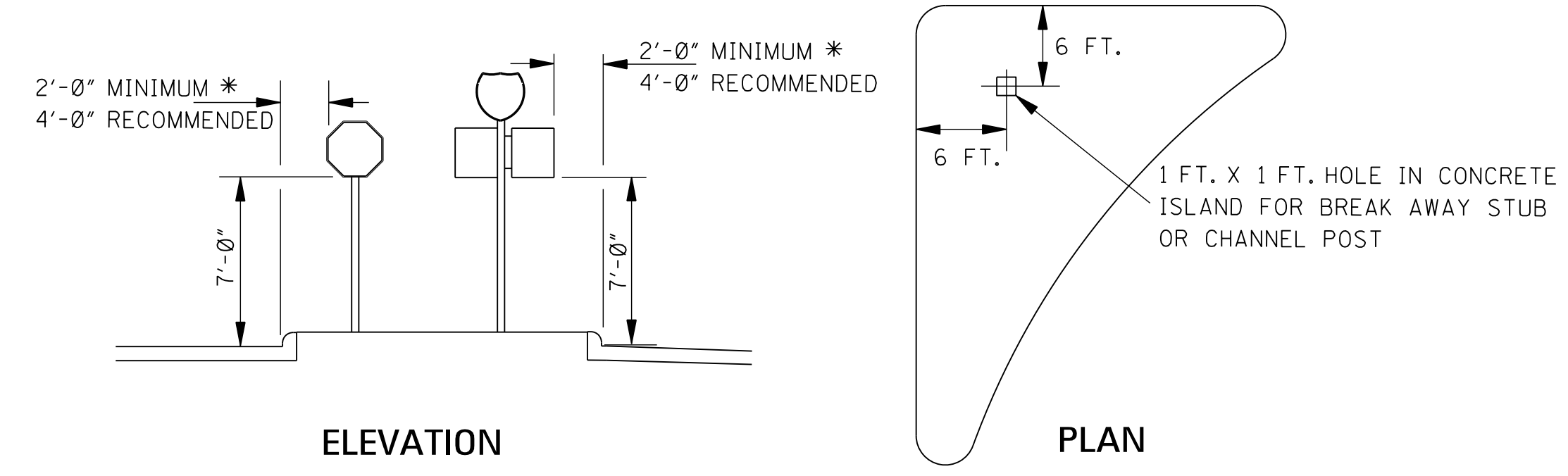
OVERHEAD SIGN



SIGN ON NOSE OF MEDIAN



SIGNS IN ISLANDS OR BEHIND CURB USING U-POSTS OR PIPE POSTS

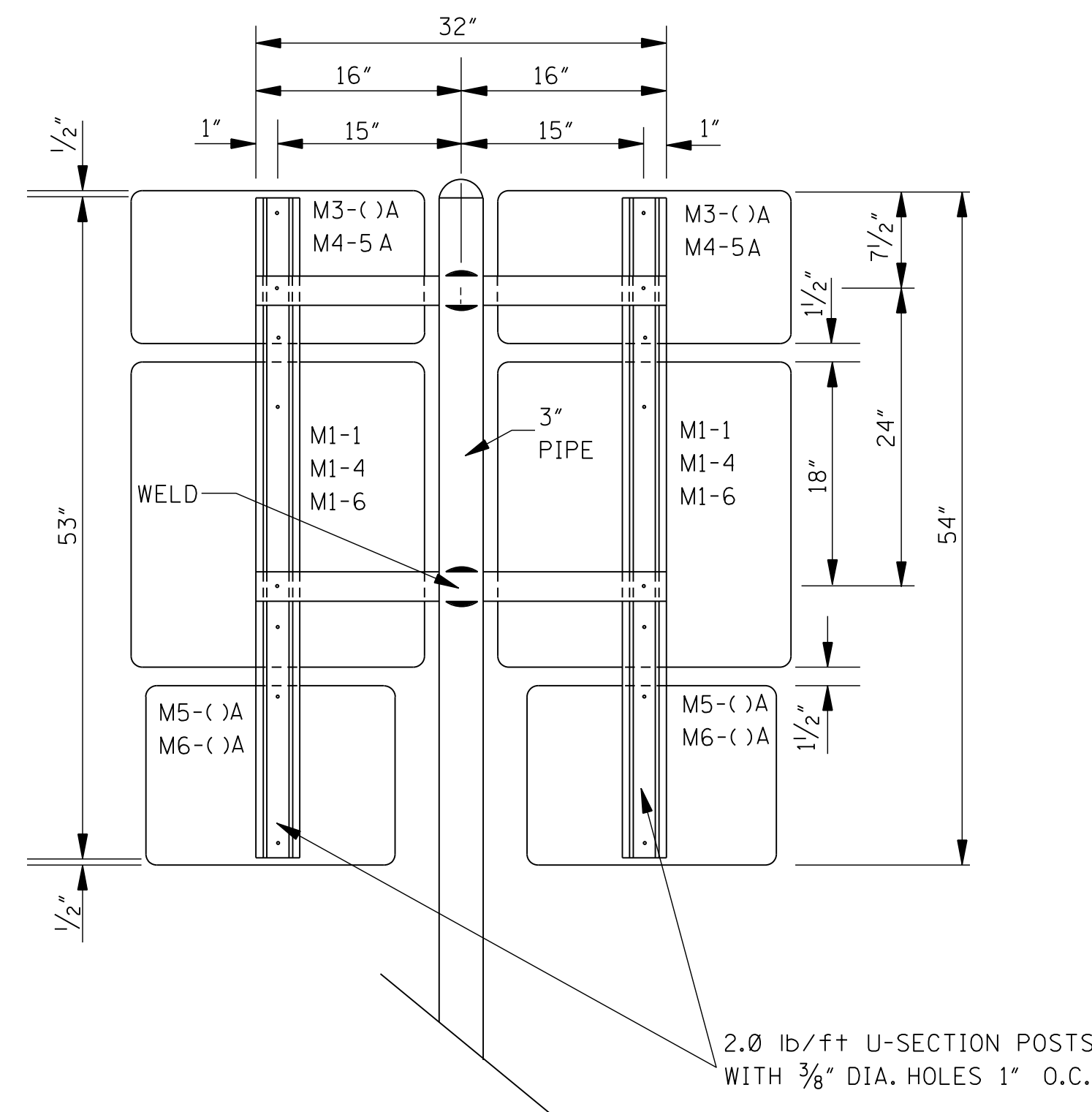
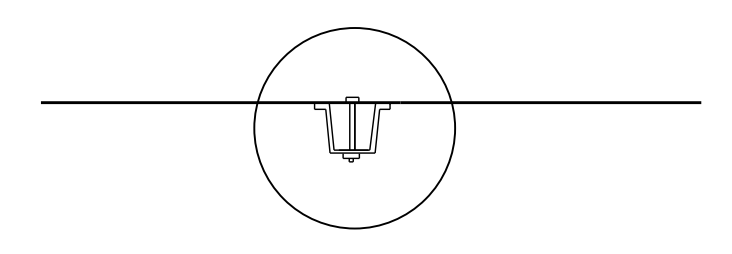
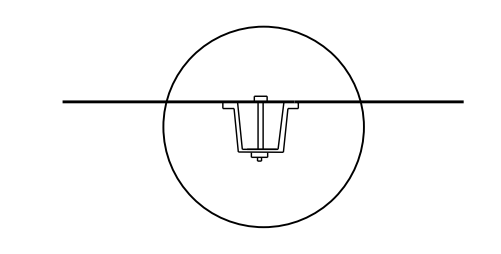
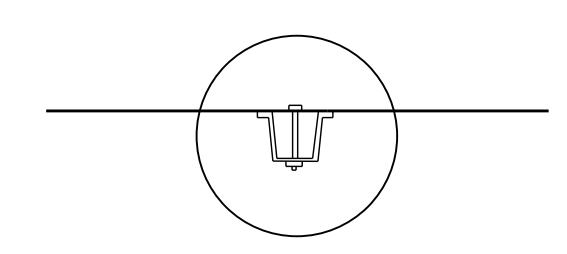
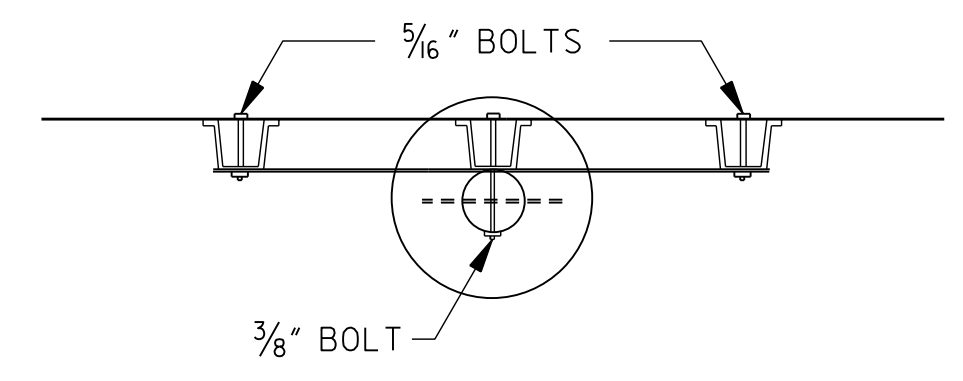
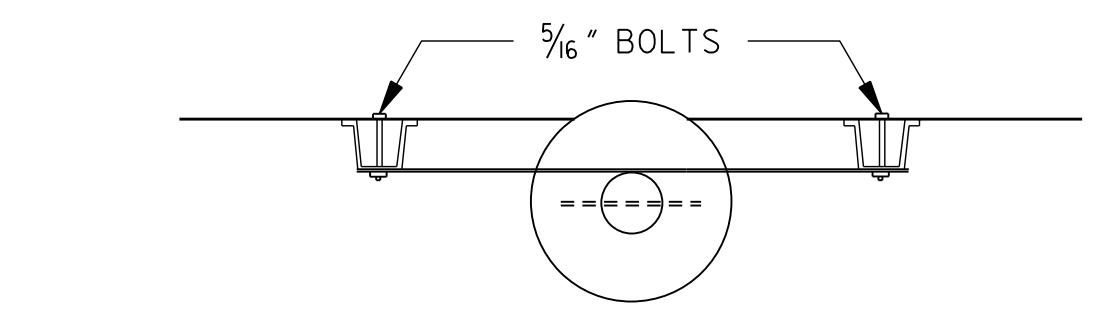


GENERAL NOTES:

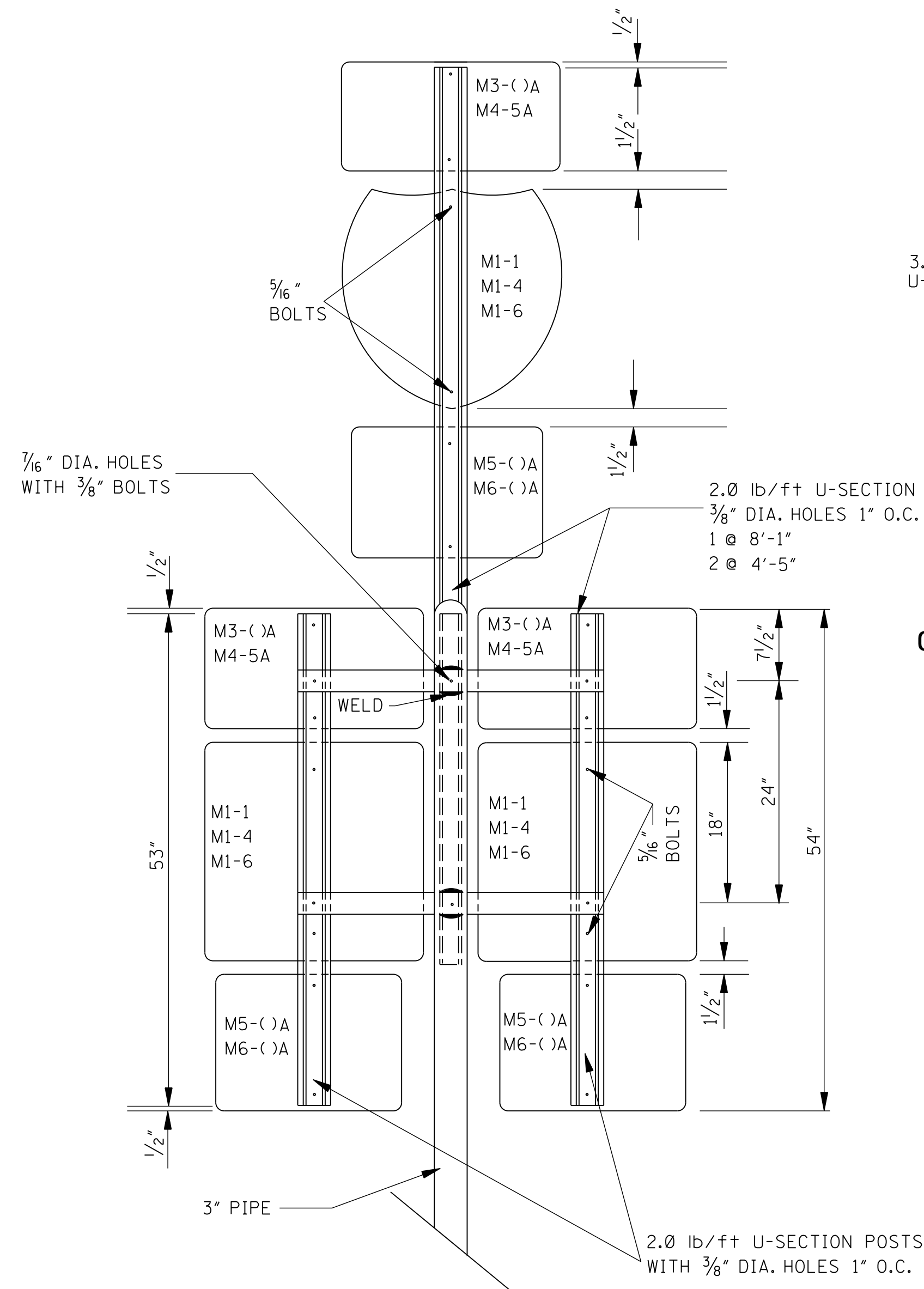
- SEE SECTION 2A-19 OF THE MUTCD FOR REDUCED LATERAL OFFSET DISTANCES THAT MAY BE USED IN AREAS WHERE LATERAL OFFSETS ARE LIMITED, AND IN BUSINESS, COMMERCIAL, OR RESIDENTIAL AREAS WHERE SIDEWALK WIDTH IS LIMITED OR WHERE EXISTING POLES ARE CLOSE TO THE CURB.
- SIGNS SHALL BE LOCATED OUTSIDE THE CLEAR ZONE UNLESS PLACED ON A BREAKAWAY OR YIELDING SUPPORT.

* THE 2 FT. MINIMUM OFFSET APPLIES ONLY TO STANDARD SIGNS MOUNTED ON U-POSTS. ALL STANDARD SIGNS MOUNTED ON PIPE WILL BE OFFSET A MINIMUM OF 4 FT.. RAMP DESTINATION SIGNS WILL BE OFFSET 4 FT. FROM THE SHOULDERS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
STANDARD ROADSIDE SIGN ASSEMBLY AND INSTALLATION	
BY	
REVISION	
DATE	ISSUE DATE: AUGUST 01, 2017
WORKING NUMBER	SN-4
SHEET NUMBER	6306

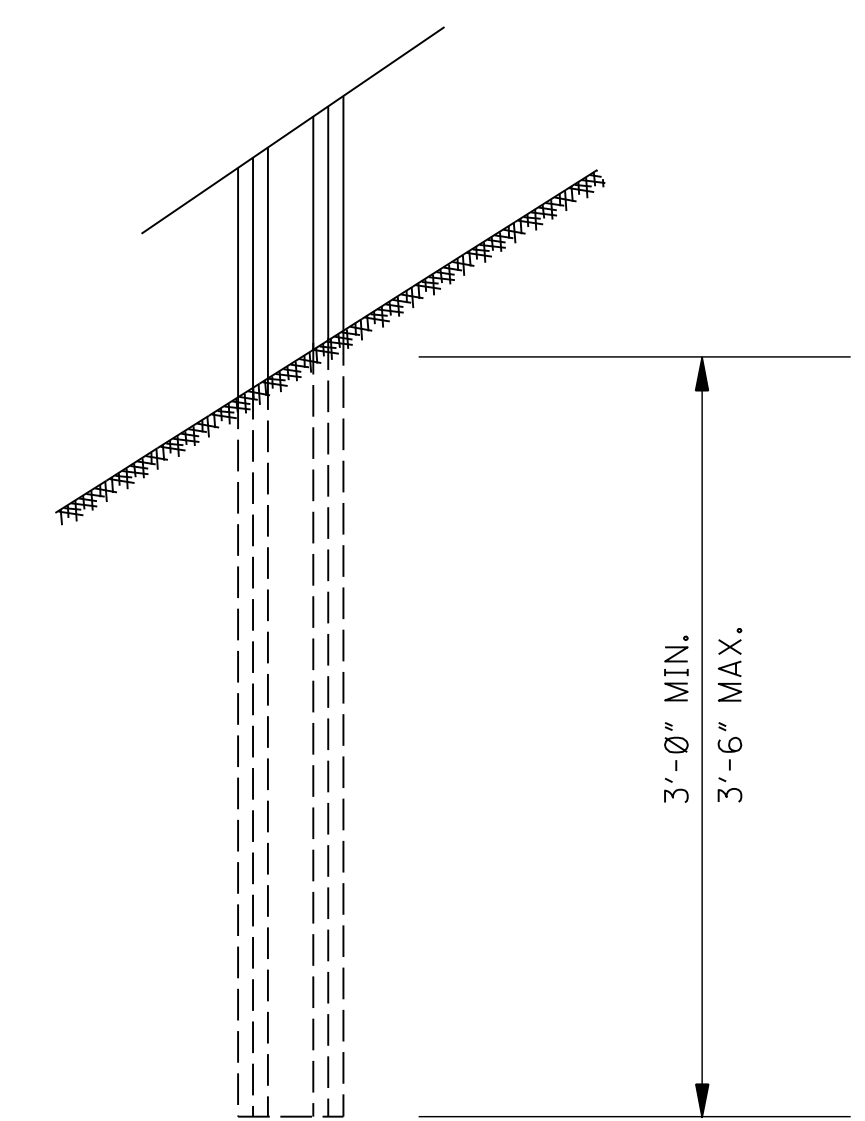
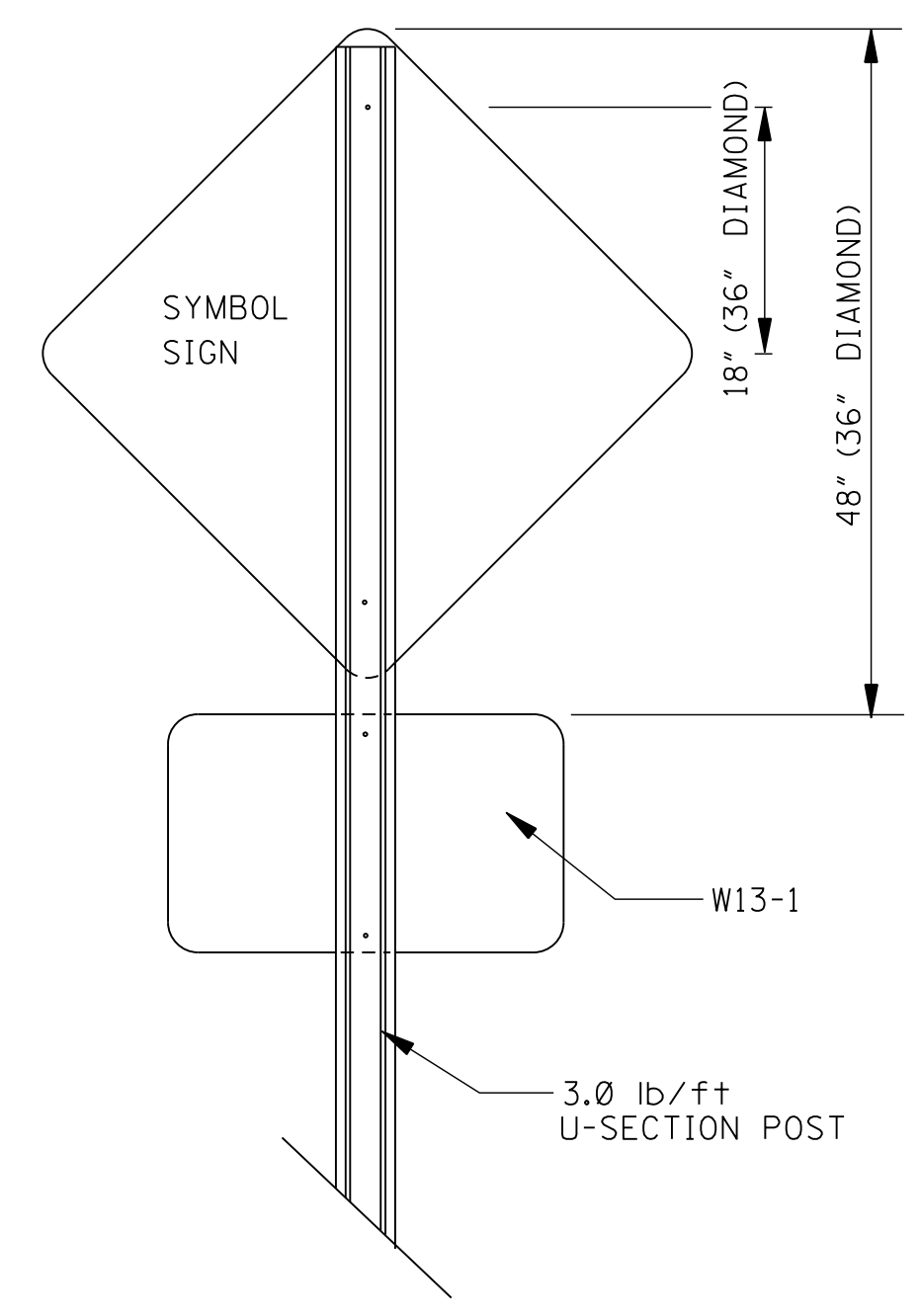
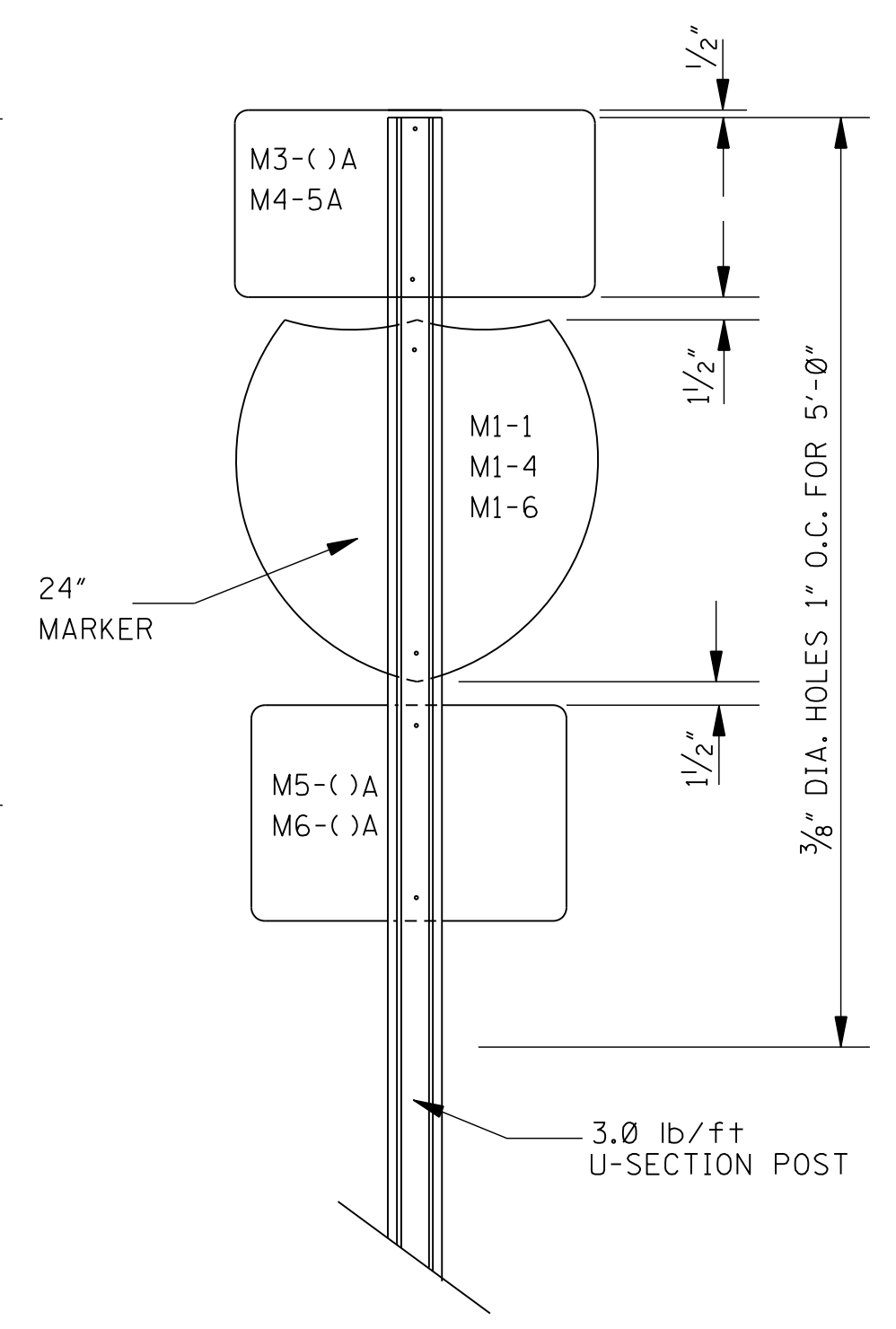
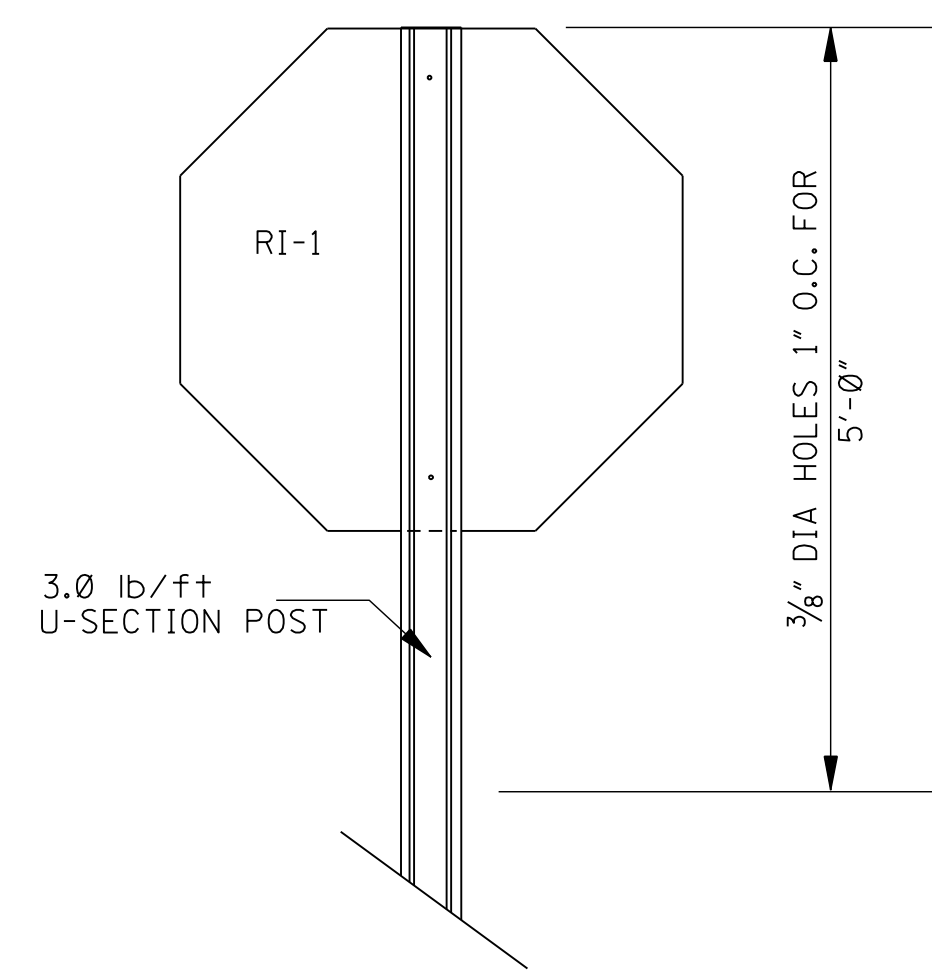


TYPICAL ROUTE ASSEMBLY



TYPICAL STACKED ROUTE ASSEMBLY

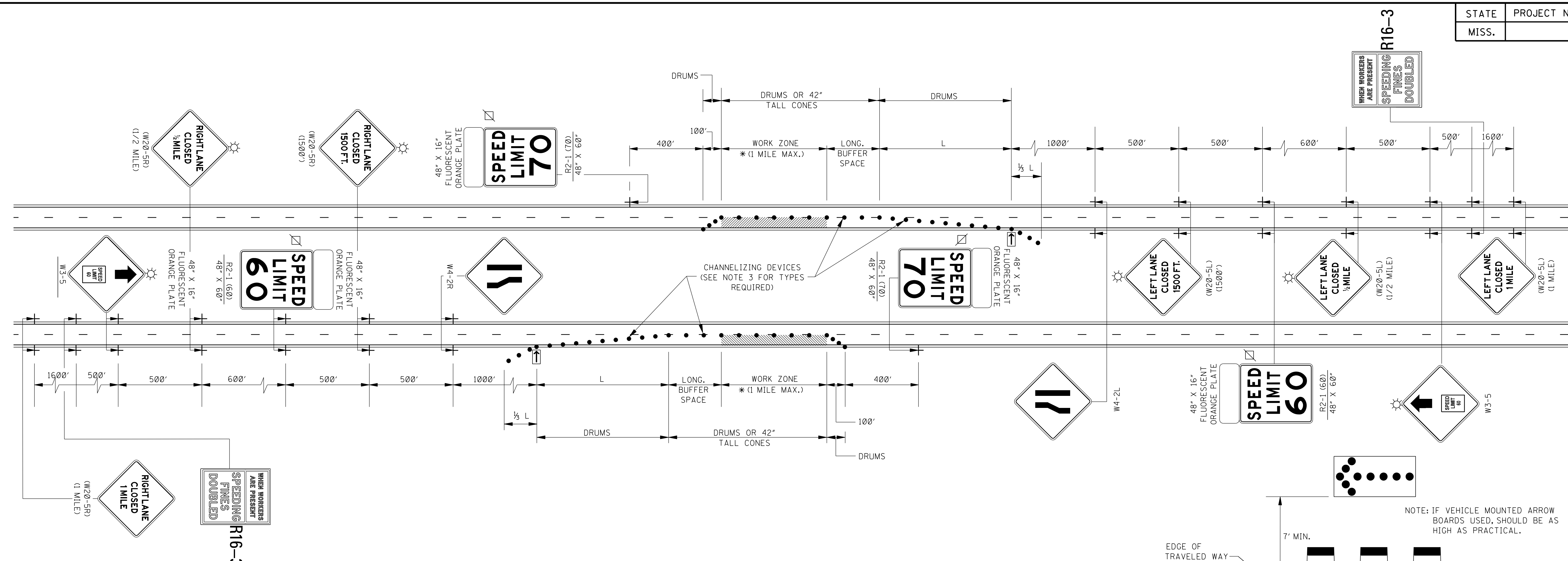
TYPICAL ASSEMBLY OF "FLAT TOPPED" REGULATORY OR WARNING SIGN MOUNTED ON A SINGLE U-SECTION POST



FOOTING DETAIL FOR U-SECTION POSTS

- GENERAL NOTES:
- UNLESS OTHERWISE SPECIFIED, HORIZONTAL BRACES ARE 3/16" X 2 1/2" X VARIABLE LENGTH FLAT STEEL BARS. BARS ARE WELDED TO PIPE AS SHOWN. WHEN FABRICATION IS COMPLETE, POST SHALL BE GALVANIZED AS PER SECTION 630 OF THE STANDARD SPECIFICATION.
 - HOLES IN FLAT BARS ARE 3/8" DIAMETER.
 - SIGNS ARE FASTENED TO FLAT BARS AND U-SECTION POST WITH 5/16" BOLTS, WITH FLAT WASHER AND LOCK-NUTS.
 - GROUND PLATE NOT REQUIRED ON U-SECTION POST.
 - SEE WK. NO. SN-4B FOR DETAIL OF 3" PIPE FOOTING DETAIL.

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		<p align="center">STANDARD ROADSIDE SIGN ASSEMBLY AND INSTALLATION</p> 	
DATE			
ISSUE DATE:		AUGUST 01, 2017	
WORKING NUMBER		SN-4A	
SHEET NUMBER		6307	



GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft)	TAPER RATES
	TAPER	ALONG BUFFER SPACE & WORK ZONE		
≤40	40	80	305	27:1
45	45	90	360	45:1
50	50	100	425	50:1
55	55	110	495	55:1
60	60	120	570	60:1
65	65	130	645	65:1
70	70	140	730	70:1

+ NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 $L = WS$ FOR SPEEDS OF 45 mph OR GREATER
 $L = WS^2/60$ FOR SPEEDS OF 40 mph OR LESS
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

++ NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.

3. CHANNELIZING DEVICES:

- A. ALL CHANNELIZING DEVICES IN TAPERS SHALL BE RETROREFLECTIVE FREE STANDING PLASTIC DRUMS.
- B. CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER RETROREFLECTIVE FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.
- C. ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE.
- D. RETROREFLECTORIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE M.U.T.C.D.

4. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48", AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.

5. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON FACE OF SIGN.

6. ADDITIONAL REDUCED REGULATORY SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. TWO (2) WILL BE REQUIRED FOR EACH RAMP AND LOCATION WILL BE DETERMINED BY THE ENGINEER.

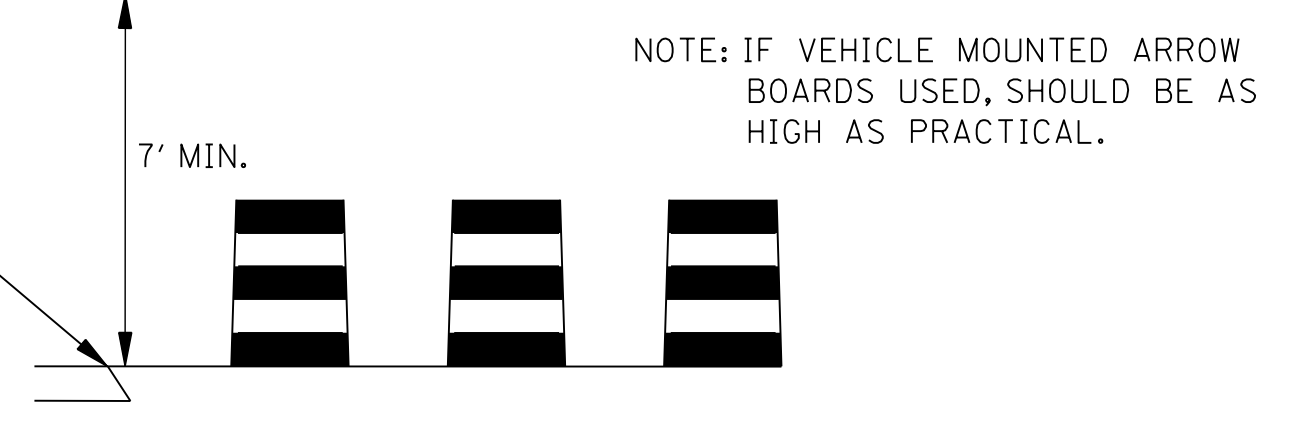
7. THIS TRAFFIC CONTROL PLAN, WITH SPEED ZONE, MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.

8. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.

9. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS REQUIRED FOR LANE CLOSURE.

10. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

EDGE OF TRAVELED WAY



LEGEND

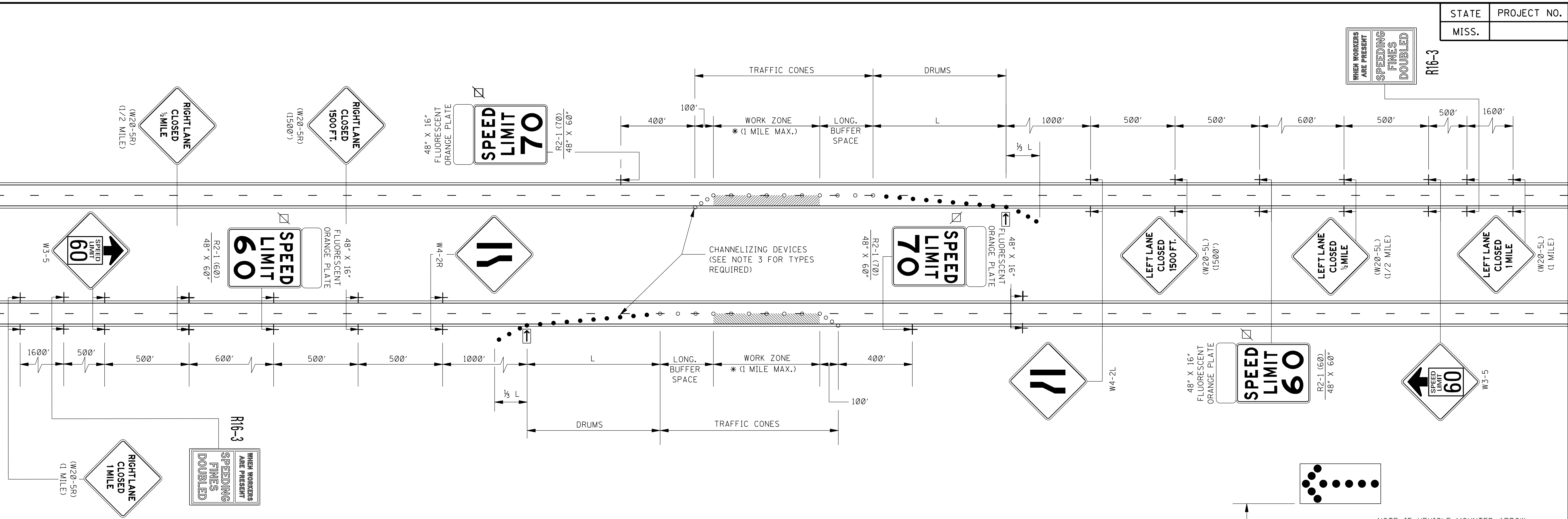
- * OR AS SHOWN ELSEWHERE ON THE PLANS.
- FLASHING ARROW PANEL (TYPE "C")
- BLACK LEGEND AND BORDER ON WHITE BACKGROUND
- ☼ TYPE "B" WARNING LIGHTS
- RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (EXTENDED PERIOD)	
BY	
REVISION	
DATE	ISSUE DATE: AUGUST 01, 2017

MDOT
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

WORKING NUMBER
TCP-4

SHEET NUMBER
6354



GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

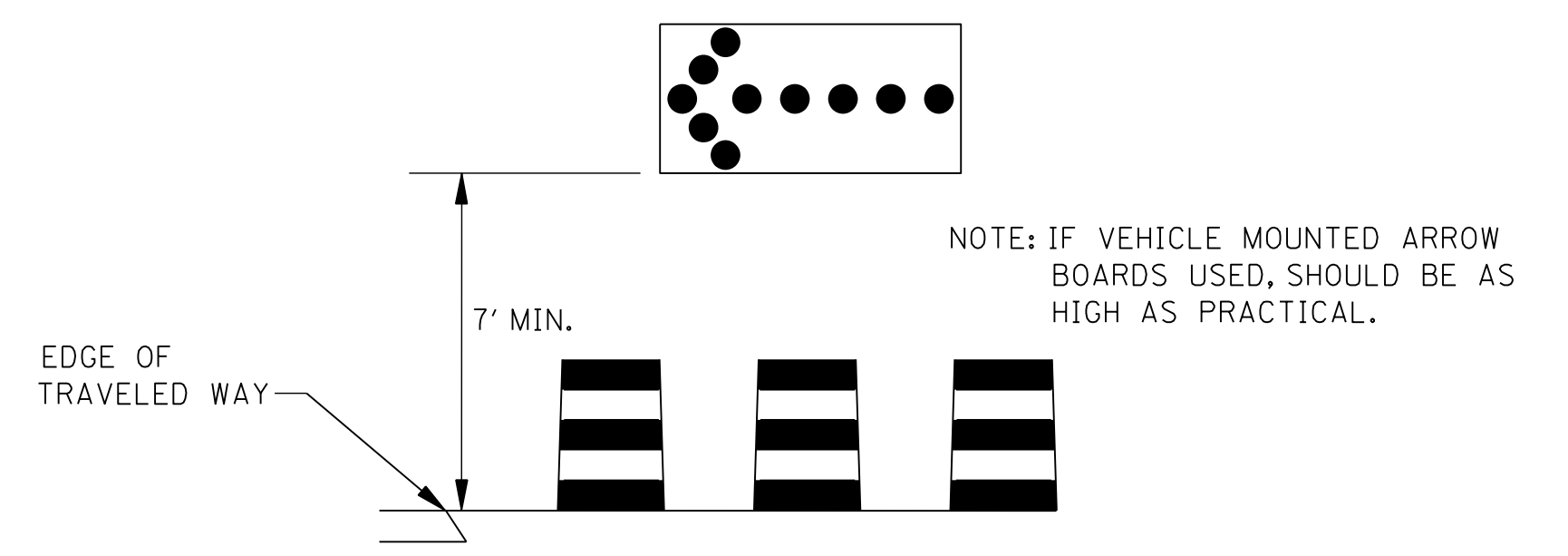
POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft)	TAPER RATES
	TAPER	ALONG LANE LINE & WORK ZONE		
40	40	80	305	27:1
45	45	90	360	45:1
50	50	100	425	50:1
55	55	110	495	55:1
60	60	120	570	60:1
65	65	130	645	65:1
70	70	140	730	70:1

+ NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 L = WS FOR SPEEDS OF 45 mph OR GREATER
 L = WS²/60 FOR SPEEDS OF 40 mph OR LESS
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

++ NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.


- CHANNELIZING DEVICE TYPES FOR:
 - APPROACH TAPER- RETROREFLECTIVE PLASTIC DRUMS
 - ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT MINIMUM)
 - EXIT TAPER- TRAFFIC CONES (28" HEIGHT MINIMUM)
- WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
- FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING AND REGULATORY SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
- DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48". AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
- ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON THE FACE OF SIGN.
- ADDITIONAL REDUCED REGULATORY SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. TWO (2) WILL BE REQUIRED FOR EACH RAMP AND LOCATION WILL BE DETERMINED BY THE ENGINEER.
- THIS TRAFFIC CONTROL PLAN, WITH SPEED ZONE, MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
- LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.
- A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS AND "REDUCED SPEED AHEAD" SIGNS REQUIRED FOR LANE CLOSURE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

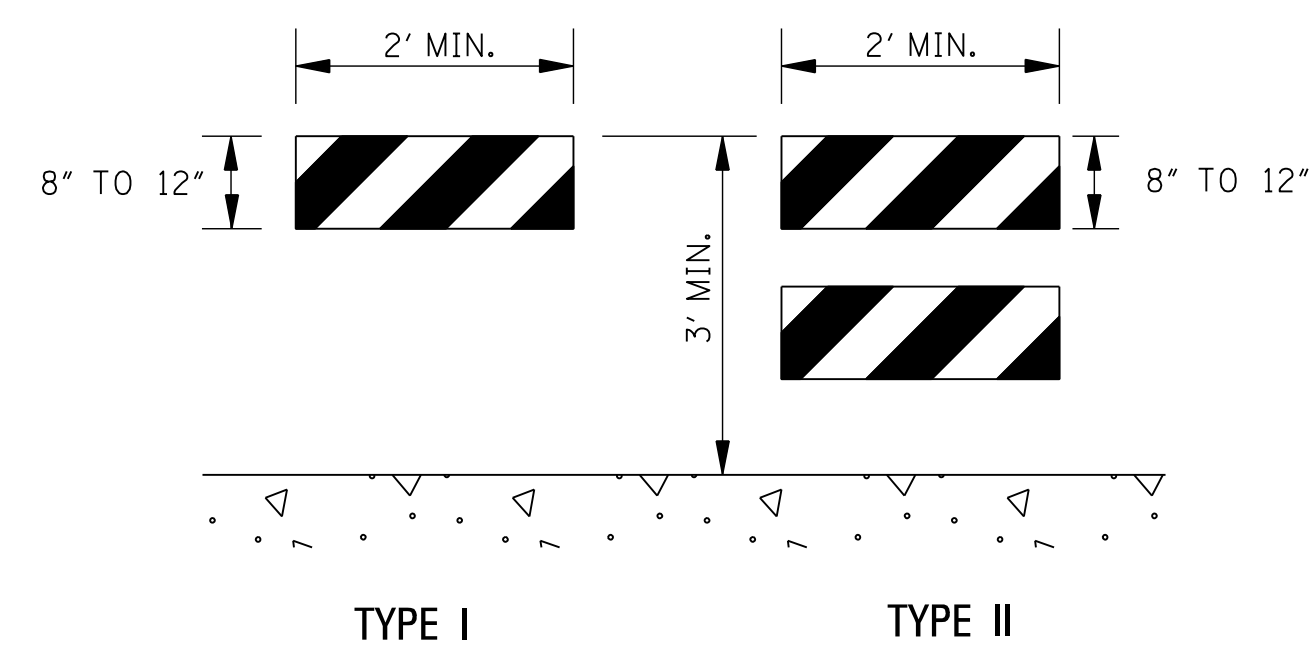


LEGEND

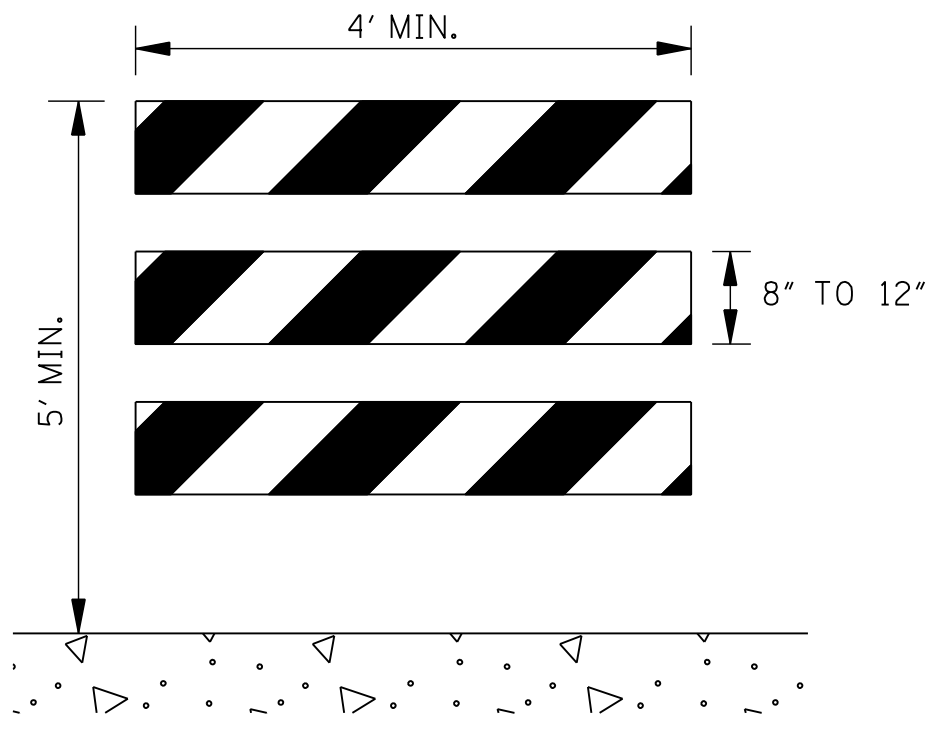
- * OR AS SHOWN ELSEWHERE ON THE PLANS.
- FLASHING ARROW PANEL (TYPE "C")
- BLACK LEGEND AND BORDER ON WHITE BACKGROUND
- RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
- TRAFFIC CONES (28" HEIGHT)

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION
REVISION	STANDARD PLAN TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (WORK DAY ONLY)
DATE	ISSUE DATE: AUGUST 01, 2017


 WORKING NUMBER
TCP-5
 SHEET NUMBER
6355



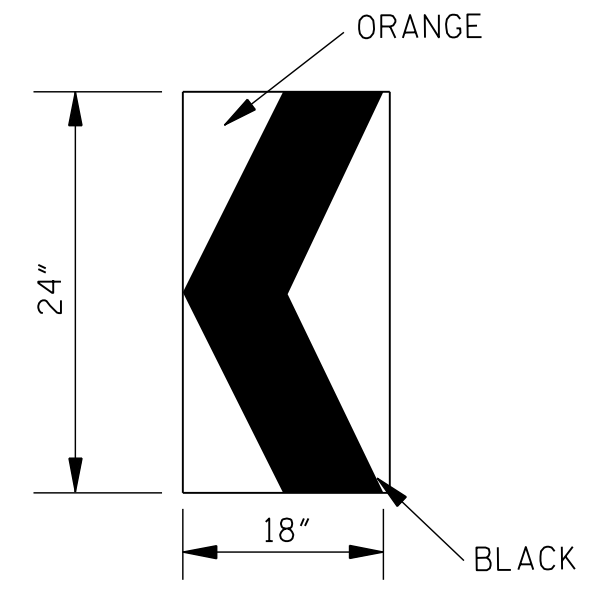
TYPE I TYPE II



TYPE III

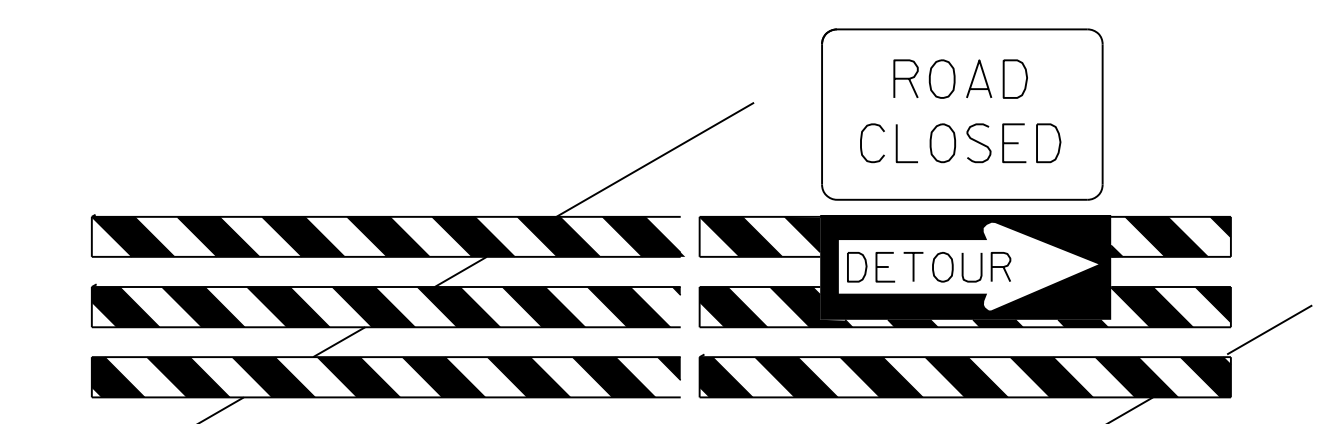
STANDARD BARRICADES

1. THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).
2. RAIL STRIPE SHOULD BE 6 INCHES, EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.
3. DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
4. FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.
5. BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES WHICH REQUIRE CRASHWORTHINESS ACCEPTANCE LETTERS. TO DATE, 2-IN. THICK TIMBER RAILS HAVE NOT BEEN SUCCESSFULLY CRASH TESTED. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE:
http://safety.fhwa.dot.gov/roadway_dept/policy_guide/road_hardware/cat2.cfm



**CHEVRON SIGN
DETAIL**

1. A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
2. THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.
3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.

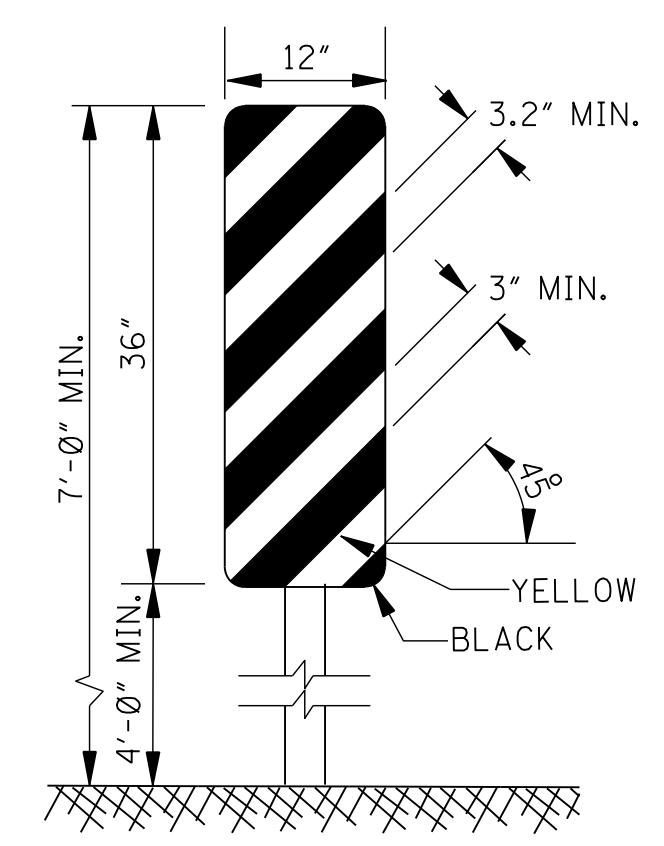


BARRICADE CLOSING A ROAD

BARRICADE CHARACTERISTICS

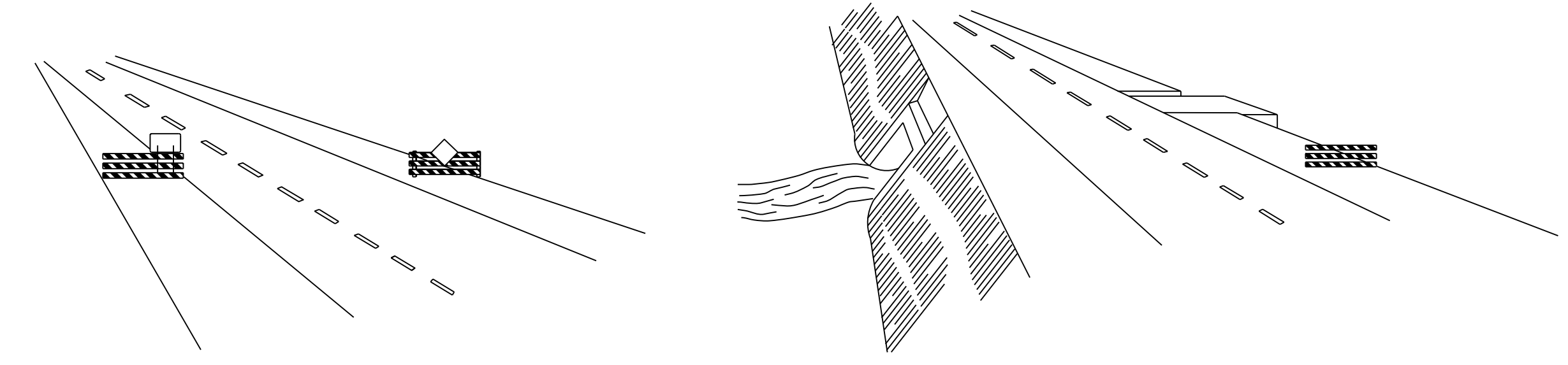
	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF RETROREFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS

- * 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- ** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 in² OF REFLECTIVE AREA FACING TRAFFIC.



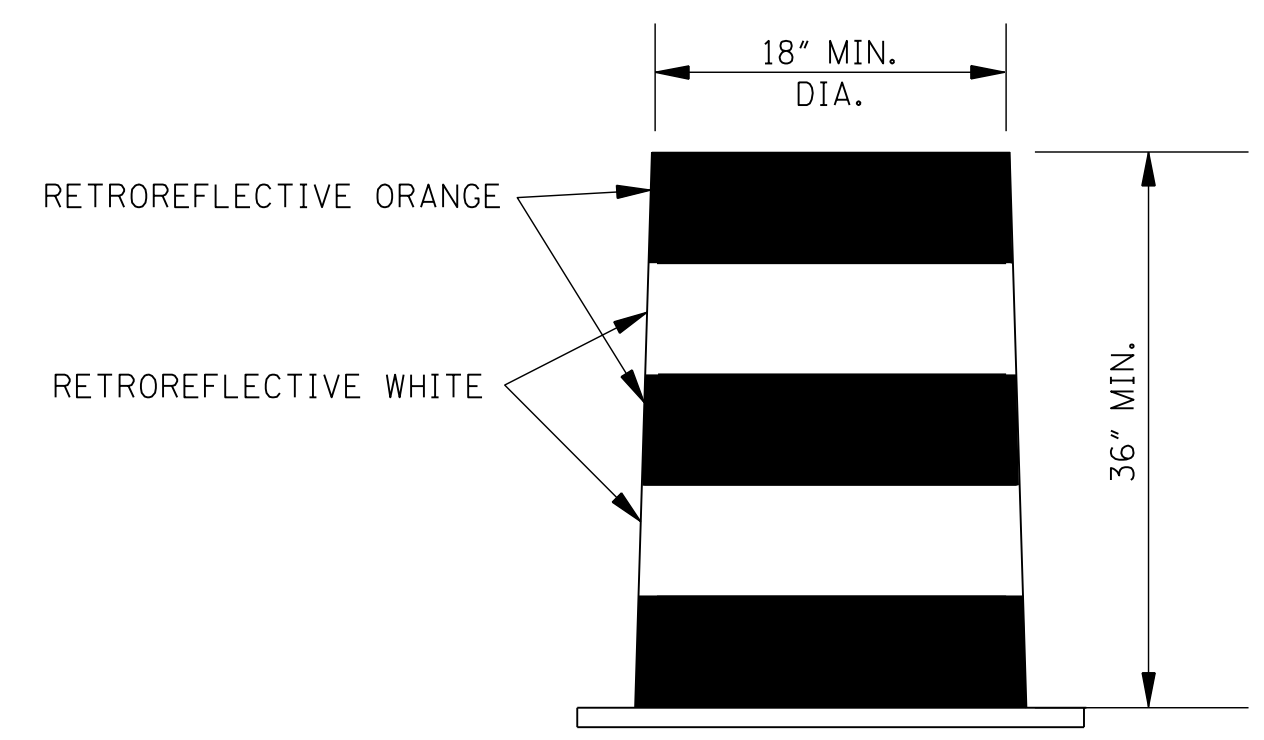
**TYPE 3 OBJECT MARKER
(OM-3R)**

1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
2. THE OM-3R IS SHOWN. THE OM-3L IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
3. THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



WING BARRICADES

1. WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
2. WING BARRICADES SHOULD BE USED:
 - A. IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - B. IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.



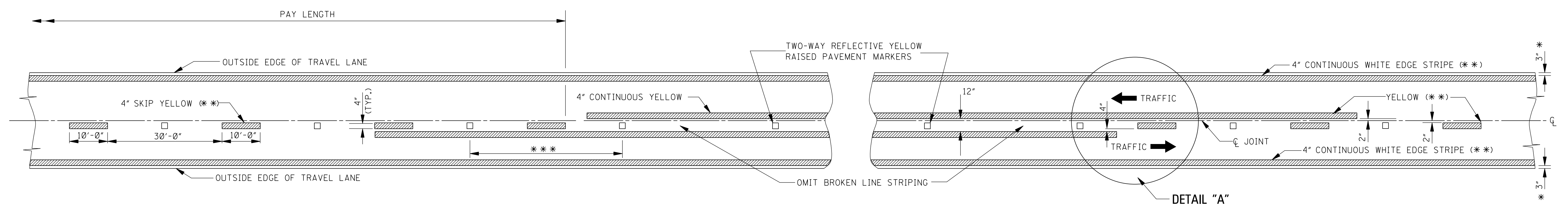
PLASTIC DRUM STRIPING DETAIL

1. PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT COLOR ON DRUMS SHALL BE ORANGE WITH FOUR (4) RETROREFLECTIVE, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
2. DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
3. WHERE PRACTICAL PLASTIC DRUMS SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

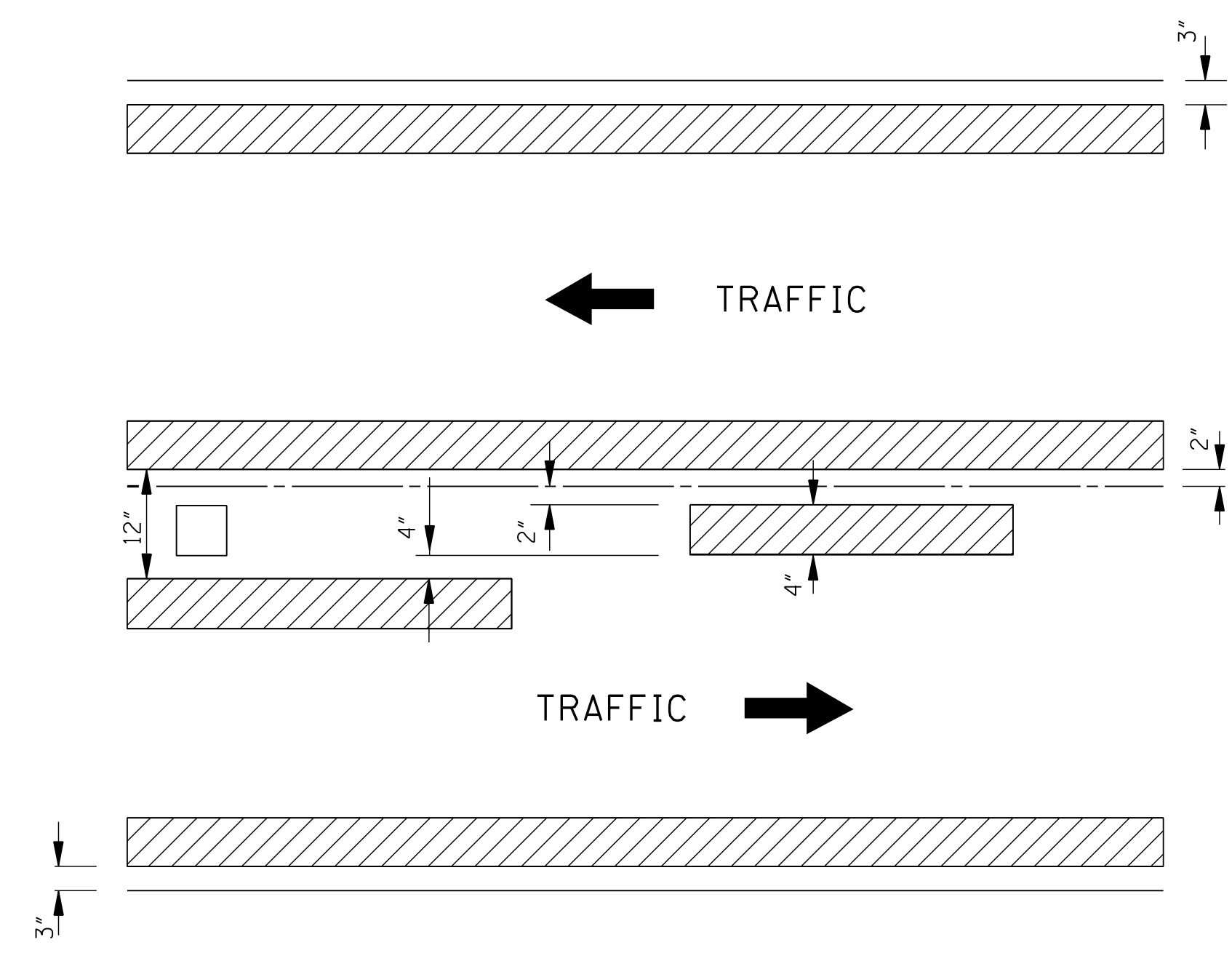
BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
REVISION		<p>HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS</p>	
DATE			
ISSUE DATE:		AUGUST 01, 2017	



WORKING NUMBER
TCP-8
SHEET NUMBER
6358



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



DETAIL "A"



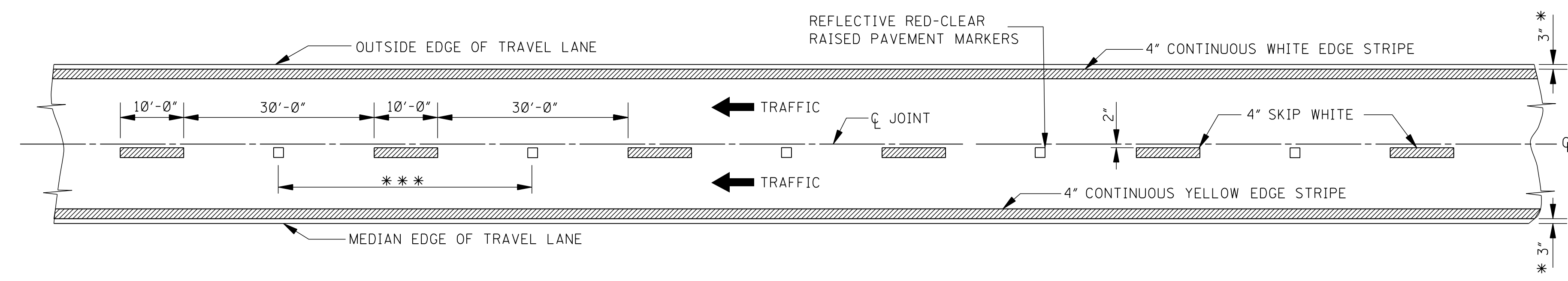
GENERAL NOTES:

- * 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
- 3. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS
- *** 4. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

	URBAN AREA (ft-in)	RURAL AREA (ft-in)
TANGENT SECTIONS	40'-0"	80'-0"
HORIZONTAL CURVES	40'-0"	40'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

† NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON LANE-LINE(S) THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.

5. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MDT "APPROVED SOURCES OF MATERIALS."

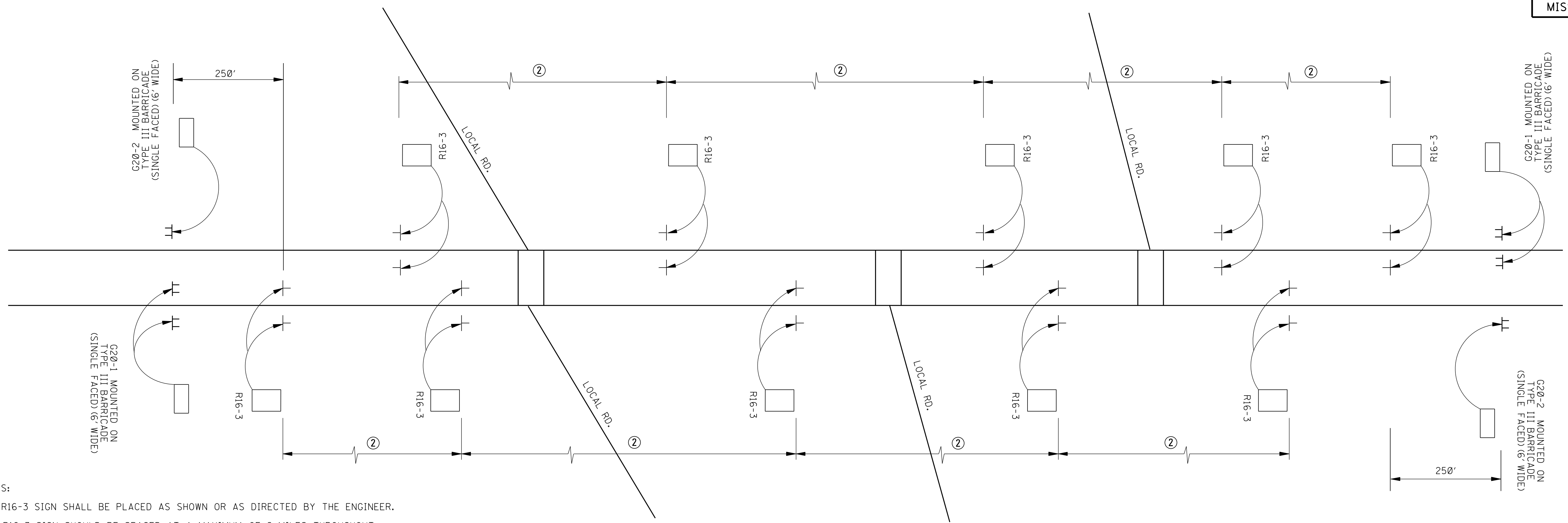


4-LANE WITH ONE-WAY TRAFFIC

BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
REVISION	TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS
DATE	ISSUE DATE: AUGUST 01, 2017



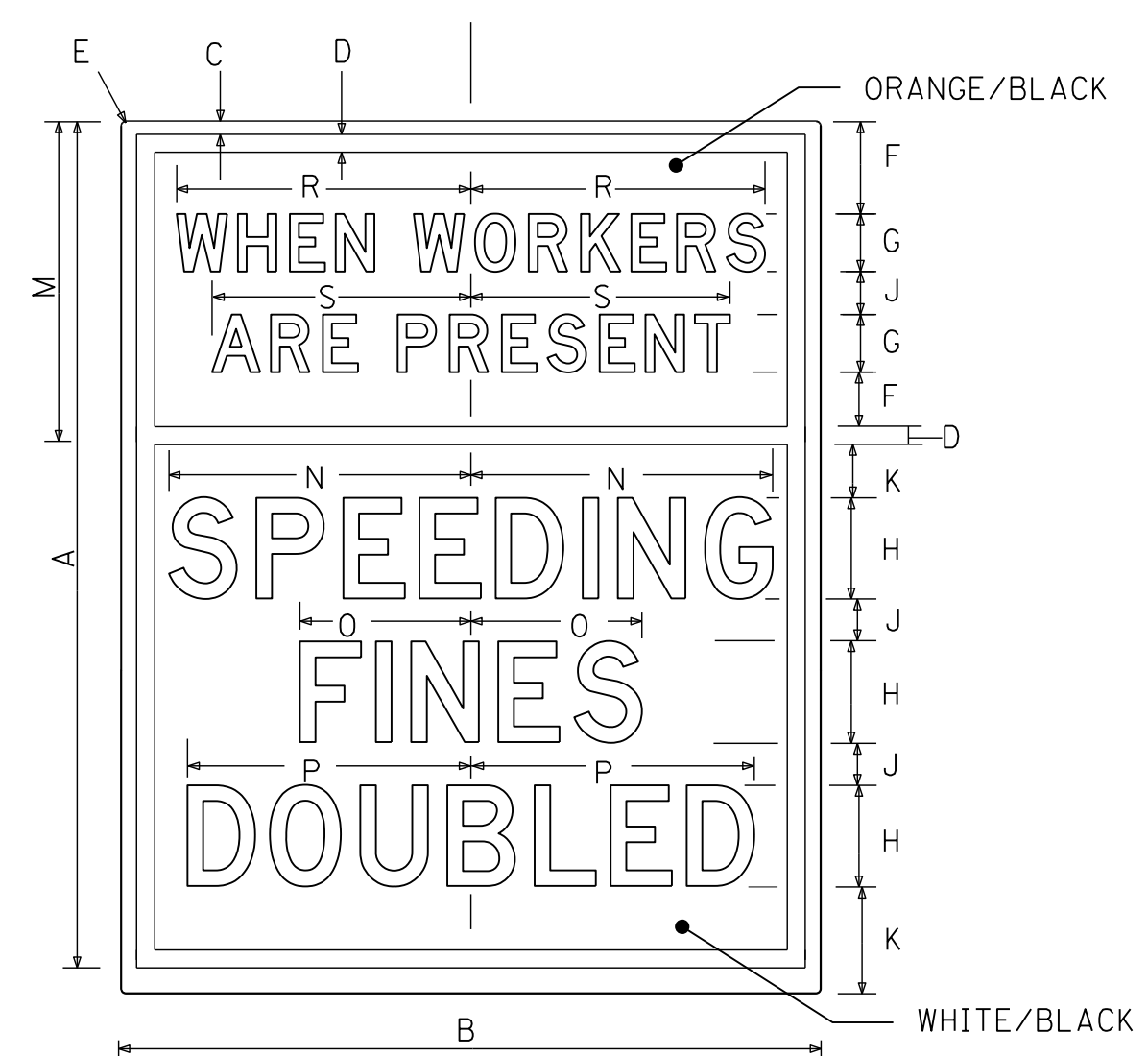
WORKING NUMBER
TCP-13
SHEET NUMBER
6363



NOTES:

1. R16-3 SIGN SHALL BE PLACED AS SHOWN OR AS DIRECTED BY THE ENGINEER.
2. R16-3 SIGN SHOULD BE SPACED AT A MAXIMUM OF 2 MILES THROUGHOUT LENGTH OF PROJECT.
3. THIS SHEET WILL ONLY APPLY TO SPEED REDUCTION SECTIONS.

**DIVIDED HIGHWAY SHOWN
(2 LANE – 2 WAY ROADWAY SIMILAR)
(PROJECT MORE THAN 1 MILE LENGTH)**



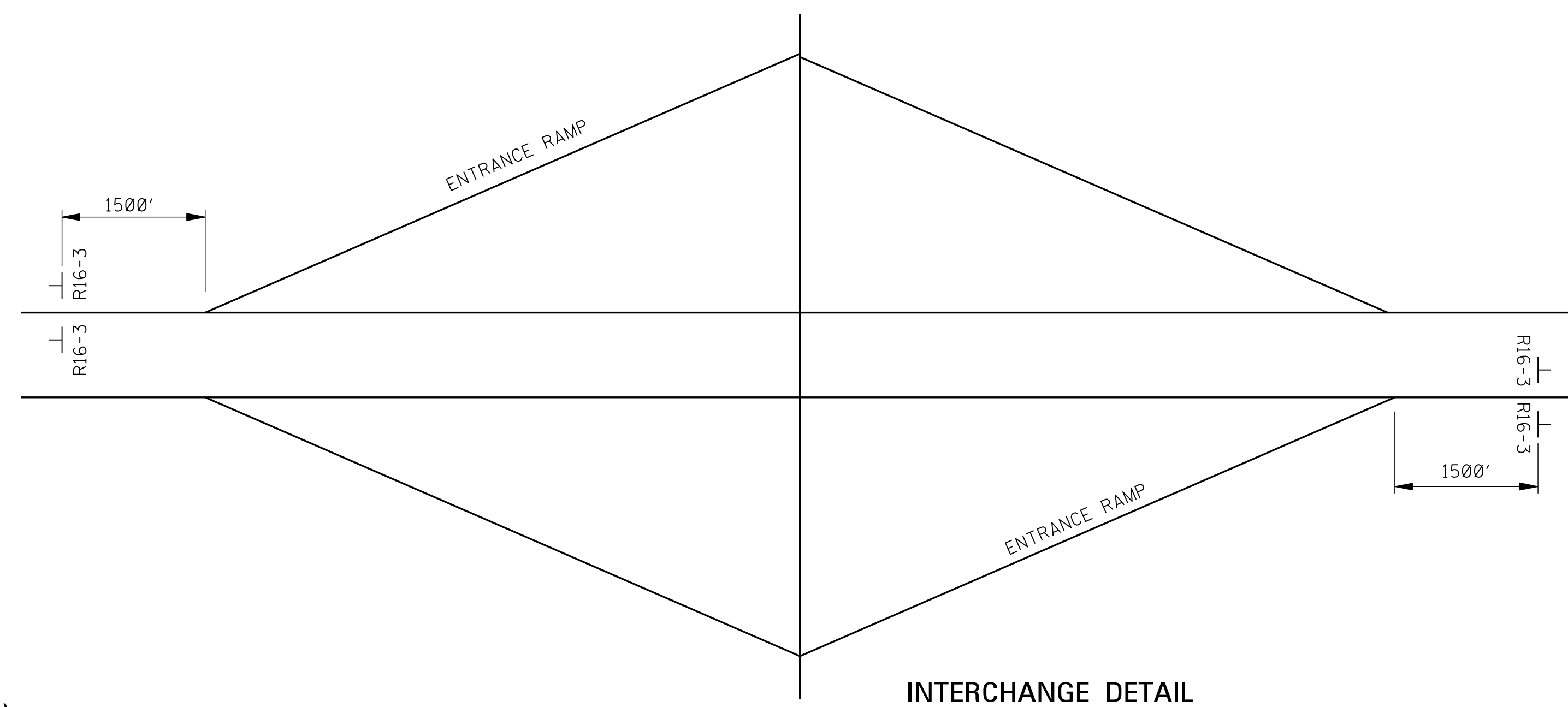
SIGN	DIMENSIONS (INCHES)							
	A	B	C	D	E	F	G	H
STD.	60	48	3/4	1 1/4	3	3 3/4	4 Dm	7 D
STD.	3	6 5/8	22 1/8	21	11 1/8	19 2 1/2	20 5/32	18

**48" x 60"
(INTERSTATE USE)**

SIGN	DIMENSIONS (INCHES)							
	A	B	C	D	E	F	G	H
STD.	48	36	3/4	1 1/4	3	2 3/4	3 Dm	6 D
STD.	3	4 1/8	14 3/4	14	7 1/8	13 1/8	13 5/8	12

**36" x 48"
(ALL OTHER HIGHWAYS)**

R16-3

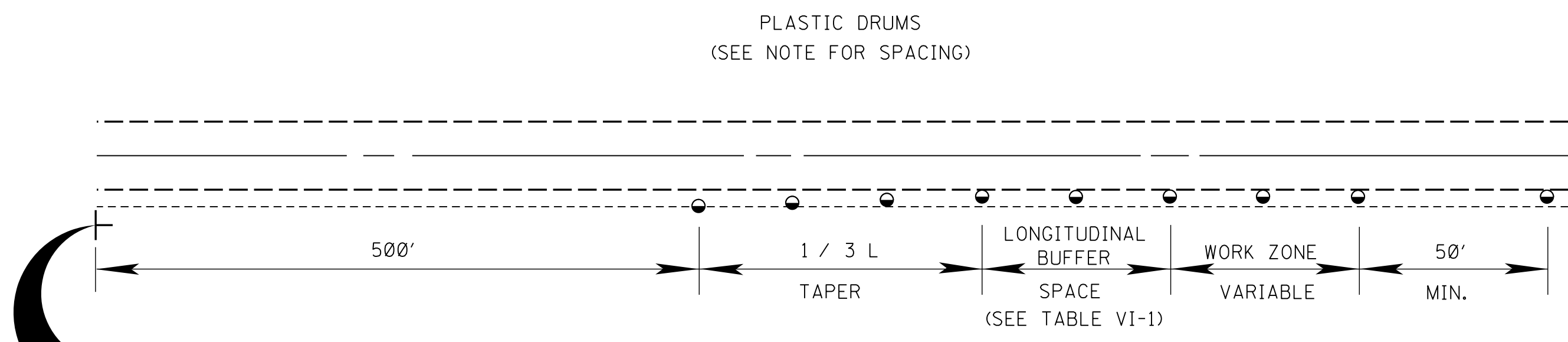


BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN		
REVISION	<p>LOCATION OF R16-3 SIGNS (SPEEDING FINES DOUBLED)</p>		
DATE			
ISSUE DATE:	AUGUST 01, 2017		

MDOT
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

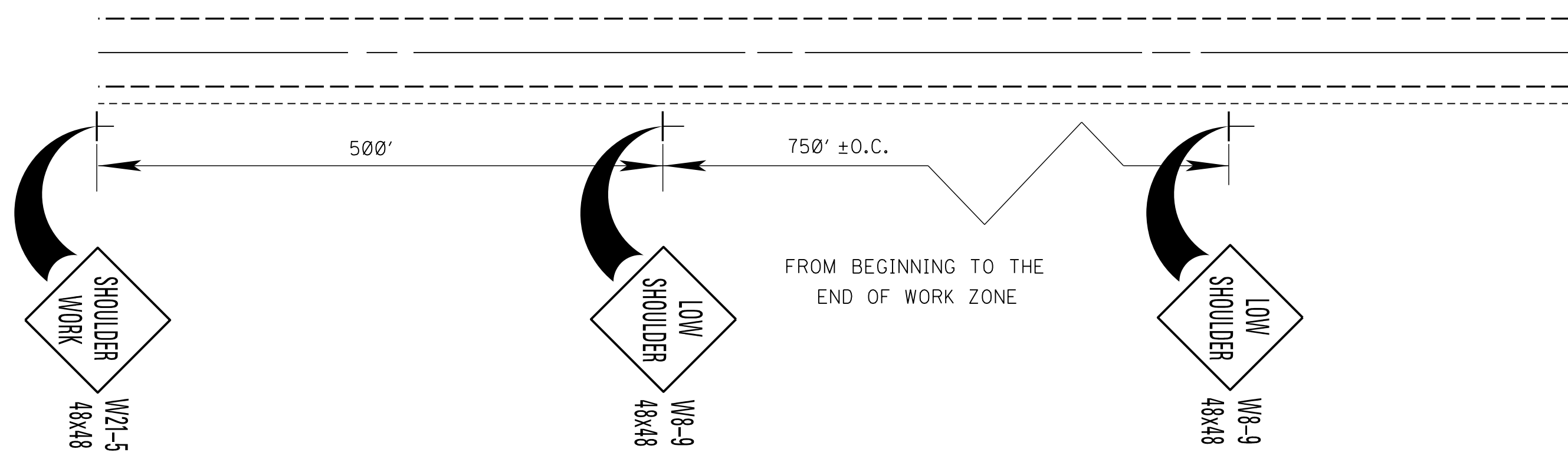
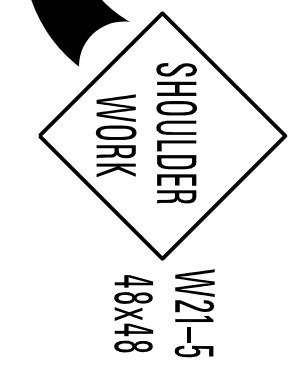
WORKING NUMBER
TCP-15

SHEET NUMBER
6365

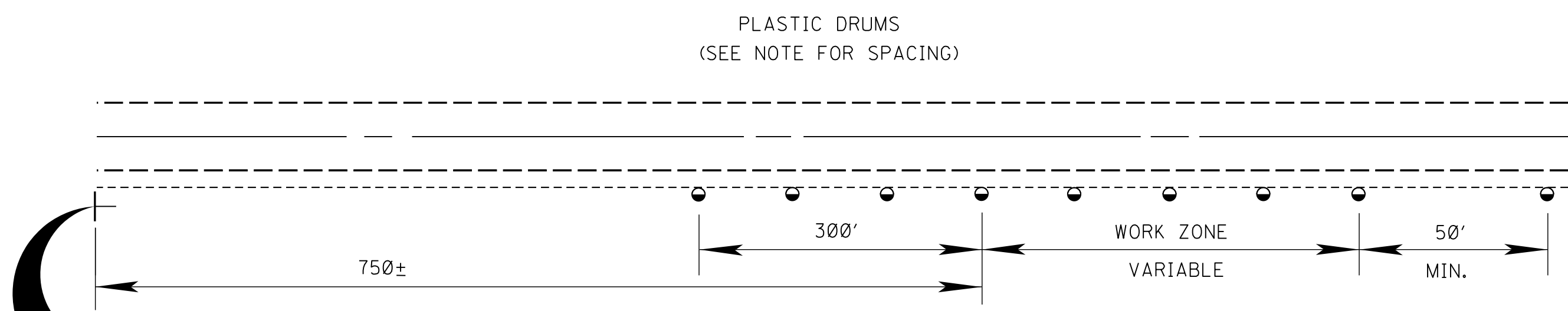
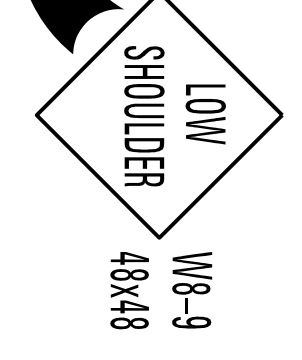
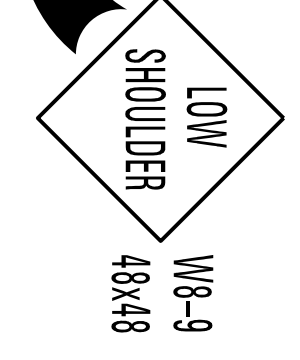


TYPICAL SHOULDER CLOSURE

- (1) TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
- (2) TO BE USED WHEN CONSTRUCTION VEHICLES (EQUIPMENT) ENCROACHES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.

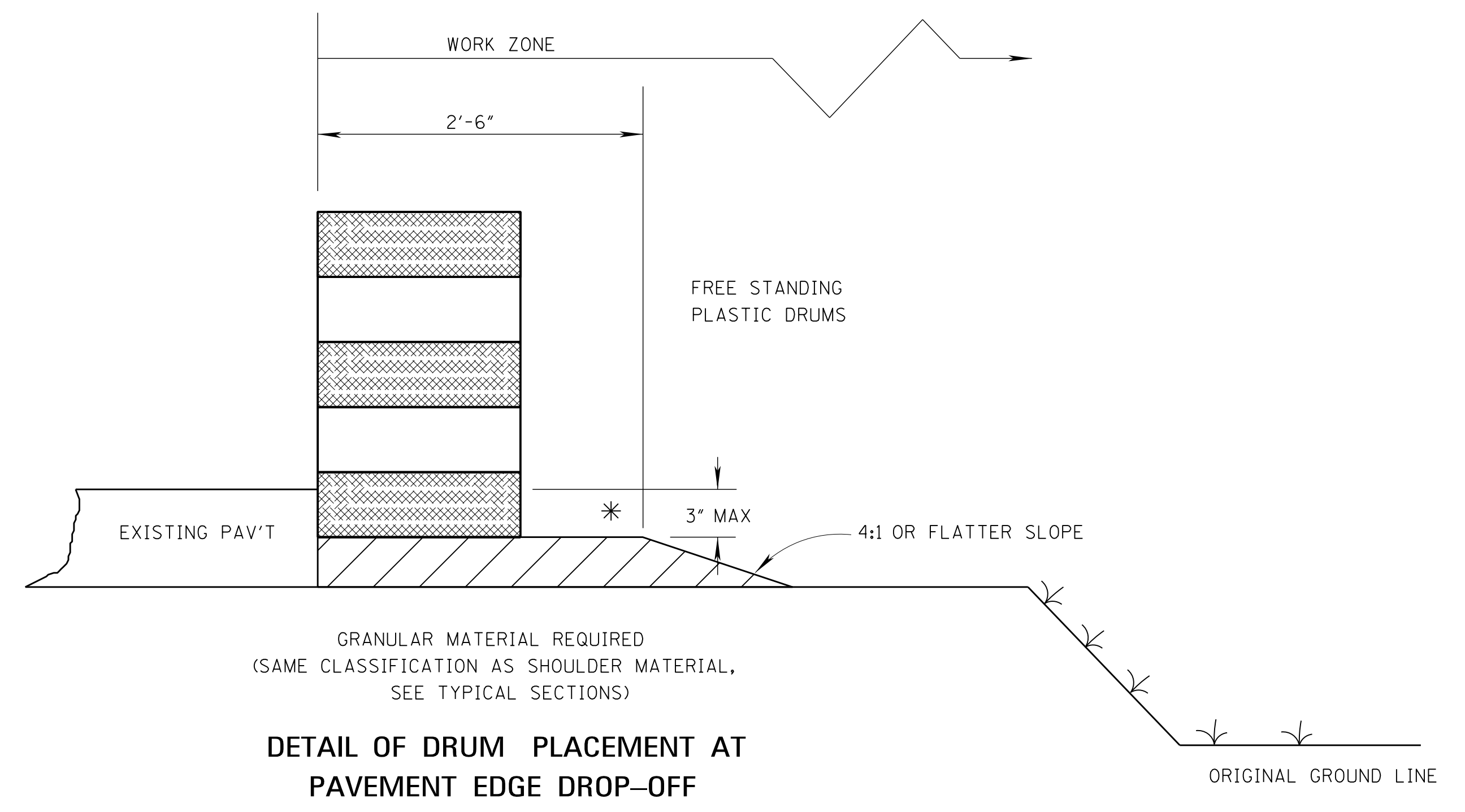
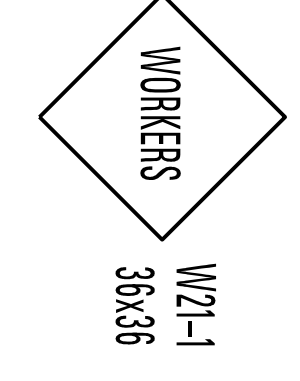


**TYPICAL SHOULDER WORK #1
(SEE NOTE A-1 THIS SHEET)**



TYPICAL SHOULDER WORK #2

NOTE:
WORK OUTSIDE TWO (2) FOOT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA, OR SEE NOTE A-3 THIS SHEET.



DETAIL OF DRUM PLACEMENT AT PAVEMENT EDGE DROP-OFF

NOTES:

- * A. PAVEMENT EDGE DROP-OFF
 1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750'±O.C.).
 2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. CONES MAY BE USED IN PLACE OF DRUMS, PANELS, AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE M.U.T.C.D. (1 / 3 L, WHERE L IS THE TAPER LENGTH IN FEET.)
 3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 4:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED.
 4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
 5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.
- B. DRUM SPACING
 1. TANGENTS = 2 X S
 2. TAPERS = L / 3

WHERE L = S X W
L = TAPER LENGTH IN FEET
S = SPEED IN MPH (POSTED OR 85 PERCENTILE)
W = WIDTH OF OFFSET IN FEET
- C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

TABLE VI-1. GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

** SPEED (MPH)	LENGTH (FEET)
20	35
25	55
30	85
35	120
40	170
45	220
50	280
55	335
60	415
65	485

** POSTED SPEED, OFF-PEAK 85 PERCENTILE SPEED PRIOR TO WORK STARTING, OR THE ANTICIPATED OPERATING SPEED IN MPH.

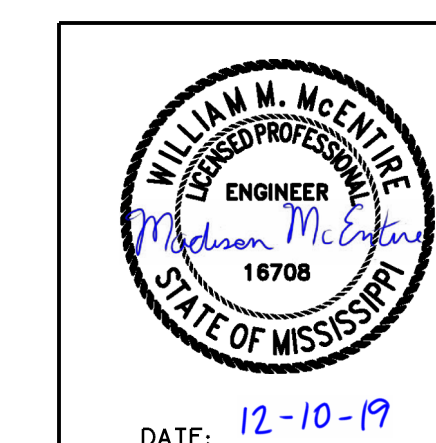
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TRAFFIC CONTROL DETAILS DRUM PLACEMENT AND SHOULDER CLOSURE	
BY	
REVISION	
DATE	ISSUE DATE: AUGUST 01, 2017
WORKING NUMBER TCP-16 SHEET NUMBER 6366	

STATE	PROJECT NO.
MISS.	BR-0020-01(248)

DESCRIPTION OF SHEETS	WORKING NO(S).	SHEET NO(S).
DETAIL INDEX (BRIDGE)	D1-BR-1	8001
I-20 EASTBOUND LANES OVER MERIDIAN SPEEDWAY RAILROAD	1	8002
I-20 EASTBOUND LANES OVER MERIDIAN SPEEDWAY RAILROAD	2	8003
I-20 EASTBOUND LANES OVER MERIDIAN SPEEDWAY RAILROAD	3	8004
I-20 EASTBOUND LANES OVER MERIDIAN SPEEDWAY RAILROAD	4	8005
FINGER JOINT REPAIR	5	8006
FINGER JOINT REPAIR	6	8007
FINGER JOINT REPAIR	7	8008
FINGER JOINT REPAIR	8	8009
FINGER JOINT REPAIR	9	8010
FINGER JOINT REPAIR	10	8011
FINGER JOINT REPAIR	11	8012
FINGER JOINT REPAIR	12	8013
FINGER JOINT REPAIR	13	8014
FINGER JOINT REPAIR	14	8015
FINGER JOINT REPAIR	15	8016
INFORMATION PLANS - BR-0020-01(192)	-	8017-8019

BRIDGE DIVISION		
REVISIONS		
DATE	SHEET NO.	BY

10:55:53 AM 12/10/2019 L:\2019\19T05003 - I-20 EB Finger Joint Repair\Drawings\Final Plans\I-20-EB-S002-QT.dgn



BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 977+52.88
REVISIONS		DETAILED INDEX (BRIDGE)
DATE		PROJECT BR-0020-01(248) 107878-301000
DESIGNER - Madison McEntire		CHECKER - Amjed Elkhairi
DETAILER - Hugh E. Williams		ISSUE DATE
DATE: 12-10-19		HINDS COUNTY
DIRECTOR OF STRUCTURES, STATE BRIDGE ENGINEER - JUSTIN WALKER PE.		WORKING NUMBER DI-BR-1
DEP. DIR. OF STRUCTURES, ASSIST. STATE BRIDGE ENGINEER - SCOTT WESTERFIELD PE.		SHEET NUMBER 8001

*** MAINTENANCE OF TRAFFIC**

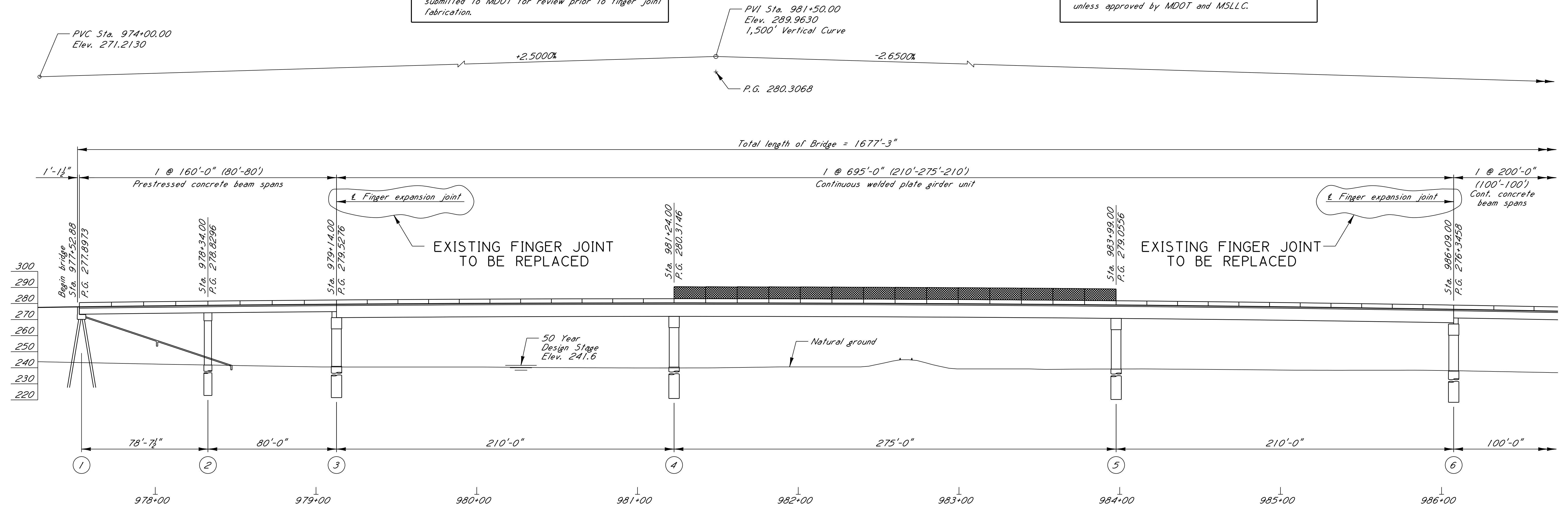
Maintain traffic in accordance with Section 618 of the 2017 Edition of the Standard Specifications for Road and Bridge Construction and the "Manual On Uniform Traffic Devices" and Traffic Control Plans included in these plans.

NOTE

The Contractor is required to survey the existing bridge deck to ensure the fabricator has accurate information with which to fabricate the finger joint for proper fit. This survey information shall be submitted to MDOT for review prior to finger joint fabrication.

NOTE

The railroad on this project is part of the Meridian Speedway, LLC (MSLLC) and is operated by Kansas City Southern Railway Company (KCS). The Contractor shall not cross or enter MSLLC ROW unless approved by MDOT and MSLLC.



ELEVATION WITH PROFILE ALONG I-20 EASTBOUND
Scale: 1" = 30'-0"

GENERAL NOTES

Specifications: Mississippi Standard Specifications For Road And Bridge Construction, 2017.

No change of plans will be permitted except by written approval of the Director of Structures, State Bridge Engineer. Minor changes in details of design or construction procedure may be authorized by the Director of Structures, State Bridge Engineer, provided such changes will not be cause for contract price adjustment.

The final surface texture of the new portion of the bridge deck shall be mechanically transverse grooved in accordance with sections 501 of the Specifications and Special Provision 907-804 of the specifications. See Sheet No. 13 for limits of transverse grooving on bridge deck.

Bar bending details shall be in accordance with "Manual Of Standard Practice For Detailing Reinforced Concrete Structures" (ACI 315R-94).

Reinforcement order lists and required placing plans shall be furnished in accordance with section 805 of the Mississippi Standard Specifications. Partial submittals are not acceptable.

All concrete shall be Class "AA".

New concrete surfaces shall receive a Class 2 rubbed or spray finish in accordance with the Specifications.

Reinforcing steel shall be A.S.T.M. A615, Grade 60, unless otherwise noted.

Work for which no pay items are provided in the proposal will not be paid for directly and compensation therefore will be included in the prices and payments for bid items.

Shop drawings of the finger joint assembly shall be submitted to the Bridge Engineer for approval prior to fabrication.

All concrete, reinforcing steel, joint hardware and other material to be removed from the existing structure shall be disposed of off the bridge site at the Contractor's expense.

Any structural steel painted surfaces damaged or effected during the course of the work shall be cleaned and painted in accordance with Section 816 of the Specifications. Any such cleaning and painting is not a separate pay item and shall be at the Contractor's expense.

At least one line of traffic shall be maintained at all times during construction.

All details on these plans are based on dimensions shown on the original plans for the existing structure. The Contractor shall be responsible for adjusting the elements of the new construction to ensure a proper fit with the existing structure.

The sequence of work shall be submitted by the Contractor and approved by the Director of Structures, State Bridge Engineer two weeks prior to the beginning of any work.

All of the work on Phase I new construction shall be completed before the work on Phase II new construction can begin.

All new steel for finger expansion joint shall be A.S.T.M. 709, Grade 50. Mill test reports will be required.

Any damage that occurs to the existing structure or the reinforcement to remain in place during the duration of the project shall be repaired by a method approved and to the satisfaction of the Director of Structures, State Bridge Engineer by the Contractor at no additional cost to the State.

All existing concrete surfaces that will be in contact with new concrete shall be blasted clean and painted with an approved concrete bonding adhesive designed to bond new concrete to old. Concrete bonding adhesive shall be applied according to the Manufacturer's recommendations.

Chamfer all edges 3/4" unless otherwise noted.

Prior to construction, dimensions and elevations of the existing structure shall be field verified by Contractor. The Contractor shall be responsible for adjusting the elements of the new construction to ensure proper fit with existing structure.

Two weeks prior to beginning work, the Contractor shall submit a proposed demolition plan with hammer information. No work may begin until proposed demolition plan is approved by the Director of Structures, State Bridge Engineer.

INFORMATION PLANS

- 1-20 Eastbound Lanes over Meridian Speedway Railroad
Original Project No. 105022/301000 BR-0020-01(192)
- Shop drawings of the steel spans are available for viewing in the Bridge Division of the Department of Transportation.

DECK CRACK AND SURFACE SEALING NOTES:

- Bridge deck surface shall be sealed with one of the following products:
 - Sikadur-55 SLV shall be as manufactured by Sika Corp, <https://usa.sika.com/en/construction/department-of-transportation/multi-purpose-epoxies/cracks/sikadur-55-slv.html>
 - DeckSeal shall be as manufactured by SSI Commercial & Highway Construction Materials, http://ssicm.com/ssi_sds_sheets.php.
 - Bridge Seal 75K shall be as manufactured by Dayton Superior, <http://www.daytonsuperior.com>.
- Cost of all labor and miscellaneous materials necessary to complete the deck crack sealing shall be paid for on a square foot basis as Bridge Repair, Bridge Deck Crack Sealing.

SPECIAL PROVISIONS REQUIRED

Concrete Bridges and Structures.....No. 907-804-6

DESIGN DATA

Specifications.....A.A.S.H.T.O. L.R.F.D. 2012
And Current Interims

Loading.....HL-93

Roadway Width.....42'-0" (Gutter To Gutter)

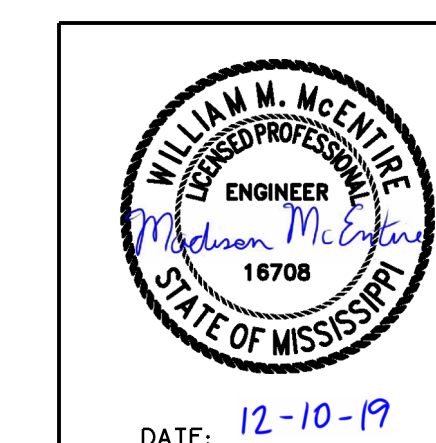
Concrete.....Class "AA" (4000 psi)

Seismic Performance Zone.....1 (S₁ = 0.050, F_v = 2.40)

Site Class.....D

Operational Class.....Essential Bridge

Structural Steel.....ASTM A709, Grade 50



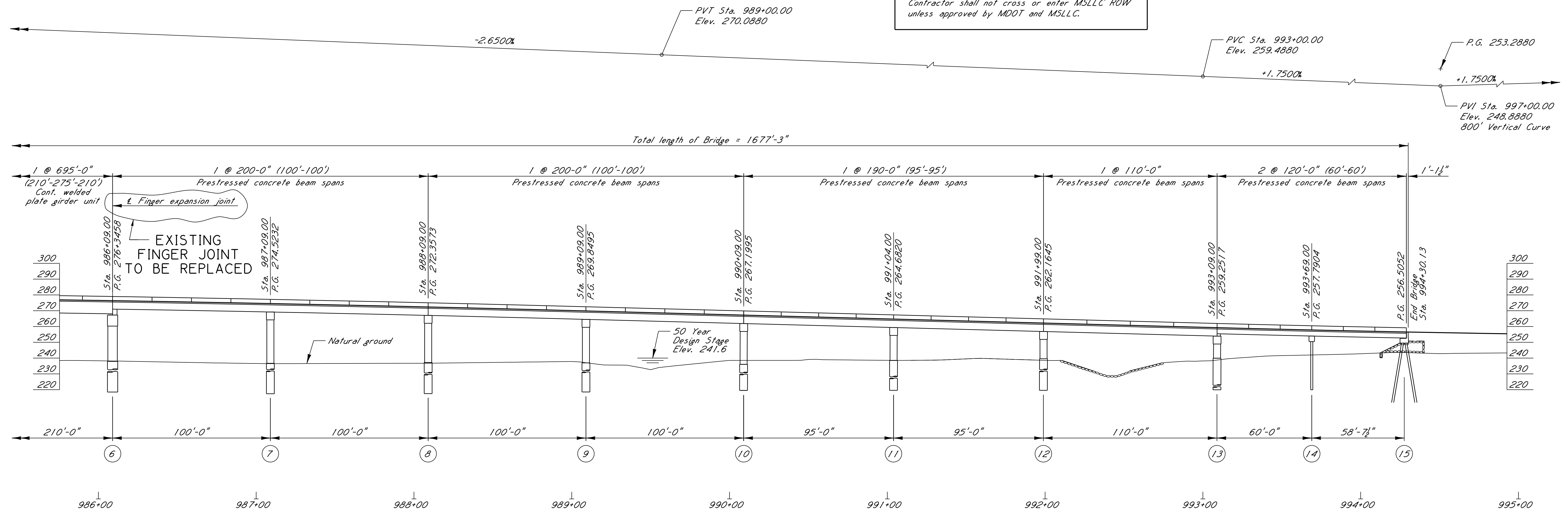
DATE: 12-10-19

SCOPE OF WORK

- Furnish and install new steel finger joint at Bents 3 and 6.
- Clean the bridge deck and drains.
- Seal the entire bridge deck per the specifications.
- Replace portions of existing downspout saddle at each side of Bents 3 and 6. See Sheet 15.
- Maintain traffic in accordance with the plans and specifications.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 977+52.88 I-20 EASTBOUND LANES OVER MERIDIAN SPEEDWAY RAILROAD FINGER JOINT REPAIR PROJECT BR-0020-01(248) 107878-301000 HINDS COUNTY		WORKING NUMBER 1 of 15
DESIGNER: Madison McEntire DETAILER: Hugh E. Williams	CHECKER: Amjad Elkhairi ISSUE DATE:	SHEET NUMBER 8002

NOTE
 The railroad on this project is part of the Meridian Speedway, LLC (MSLLC) and is operated by Kansas City Southern Railway Company (KCS). The Contractor shall not cross or enter MSLLC ROW unless approved by MDOT and MSLLC.

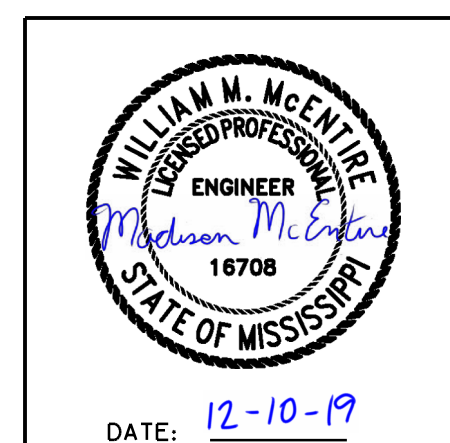


ELEVATION WITH PROFILE ALONG I-20 EASTBOUND
 Scale: 1" = 30'-0"

NOTE
 The Contractor is required to survey the existing bridge deck to ensure the fabricator has accurate information with which to fabricate the finger joint for proper fit. This survey information shall be submitted to MDOT for review prior to finger joint fabrication.

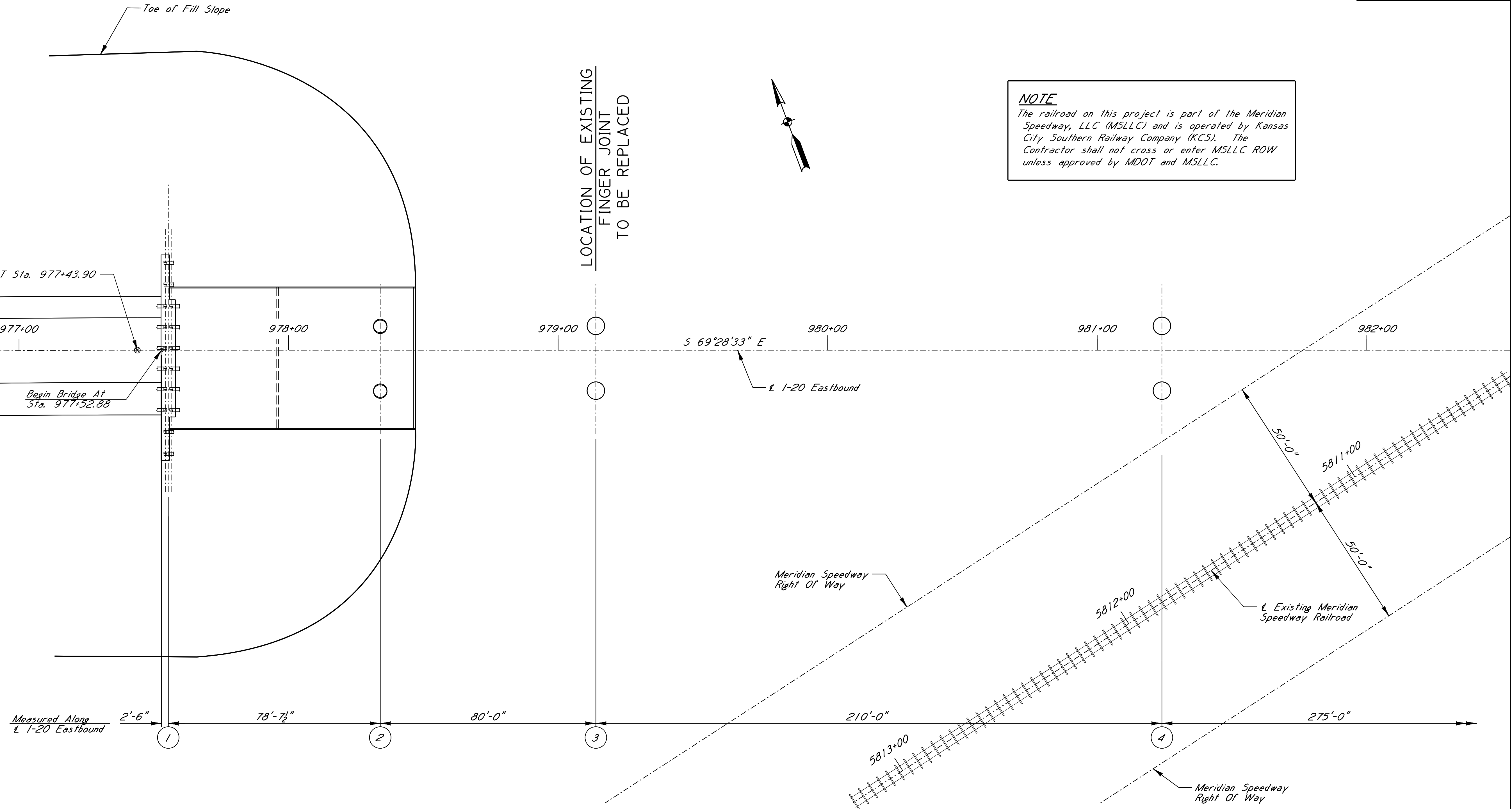
ESTIMATED BRIDGE QUANTITIES			
PAY ITEM CODE	DESCRIPTION	UNIT	TOTAL
501-K001	Transverse Grooving	SY	48
503-C001	Saw Cut, 1-inch	LF	246
805-A001	Reinforcement	LBS	2716
810-A006	Structural Steel, A709, Grade 50	LBS	18693
813-A002	Concrete Railing 32"	LF	28
907-804-8002	Bridge Concrete, Class "AA"	CY	24
907-824-PP003	Bridge Repair, Bridge Deck Crack Sealing, Per Plans	SF	70445
907-824-PP004	Bridge Repair, Cleaning of Bridge, Per Plans	LS	1
* 907-824-PP008	Bridge Repair, Removal of Existing Finger Joint, Per Plans	LF	90

* Includes removal and placement of existing finger plate and components, portion of existing concrete rail, and drain trough and downspout saddles as shown in the plans.



DATE: 12-10-19

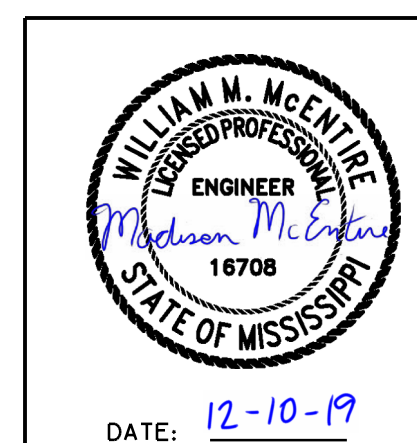
MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 977+52.88 I-20 EASTBOUND LANES OVER MERIDIAN SPEEDWAY RAILROAD FINGER JOINT REPAIR PROJECT BR-0020-01(248) 107878-301000 HINDS COUNTY		WORKING NUMBER 2 of 15 SHEET NUMBER 8003
DESIGNER: Madison McEntire DETAILER: Hugh E. Williams	CHECKER: Amjad Elkhairi ISSUE DATE:	DIRECTOR OF STRUCTURES, STATE BRIDGE ENGINEER - JUSTIN WALKER PE. DEP. DIR. OF STRUCTURES, ASSIST. STATE BRIDGE ENGINEER - SCOTT WESTERFIELD PE.



NOTE
 The railroad on this project is part of the Meridian Speedway, LLC (MSLLC) and is operated by Kansas City Southern Railway Company (KCS). The Contractor shall not cross or enter MSLLC ROW unless approved by MDOT and MSLLC.

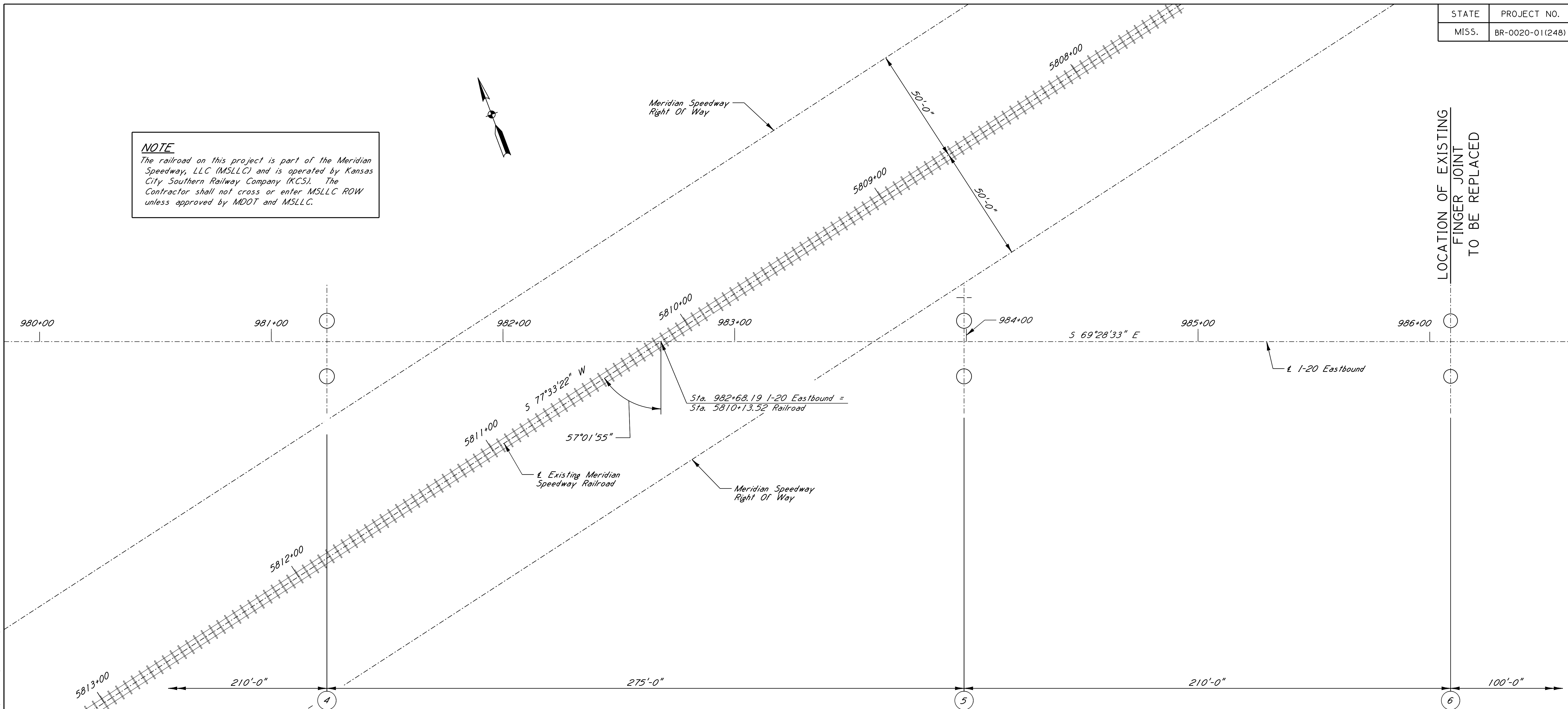
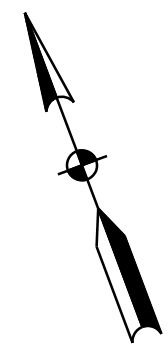
FOUNDATION PLAN
 Scale: 1" = 20'-0"

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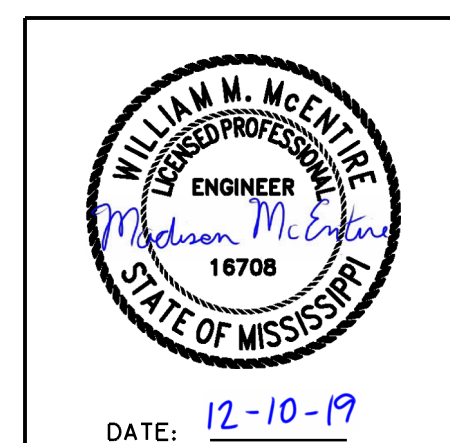
BY MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 977+52.88 I-20 EASTBOUND LANES OVER MERIDIAN SPEEDWAY RAILROAD FINGER JOINT REPAIR PROJECT BR-0020-01(248) 107878-301000 HINDS COUNTY		WORKING NUMBER 3 of 15
DESIGNER: Madison McEntire CHECKER: Amjad Elkhairi DETAILER: Hugh E. Williams ISSUE DATE: _____ DATE: 12-10-19	DIRECTOR OF STRUCTURES, STATE BRIDGE ENGINEER - JUSTIN WALKER PE. DEP. DIR. OF STRUCTURES, ASSIST. STATE BRIDGE ENGINEER - SCOTT WESTERFIELD PE.	SHEET NUMBER 8004

NOTE
 The railroad on this project is part of the Meridian Speedway, LLC (MSLLC) and is operated by Kansas City Southern Railway Company (KCS). The Contractor shall not cross or enter MSLLC ROW unless approved by MDOT and MSLLC.

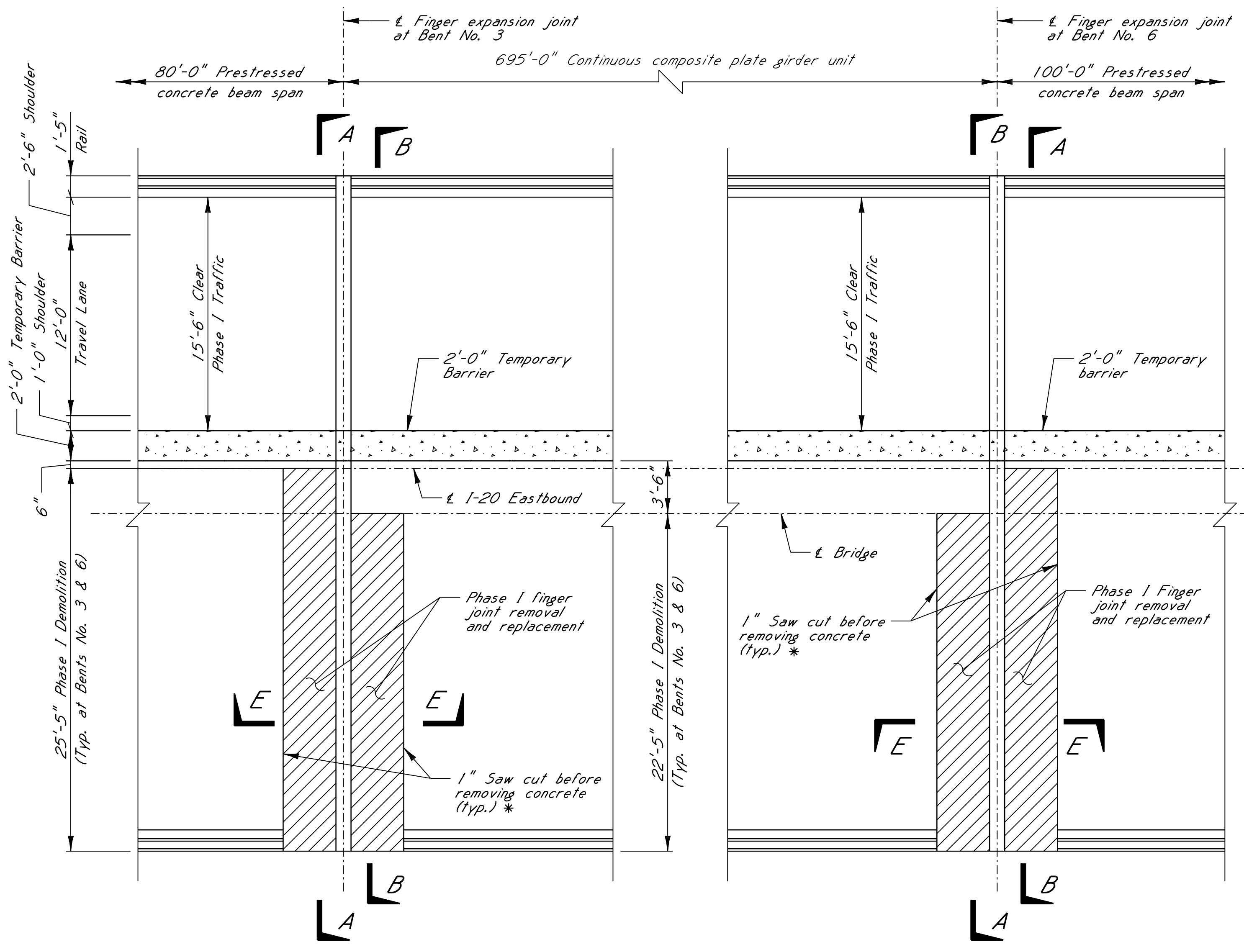


FOUNDATION PLAN
 Scale: 1" = 20'-0"

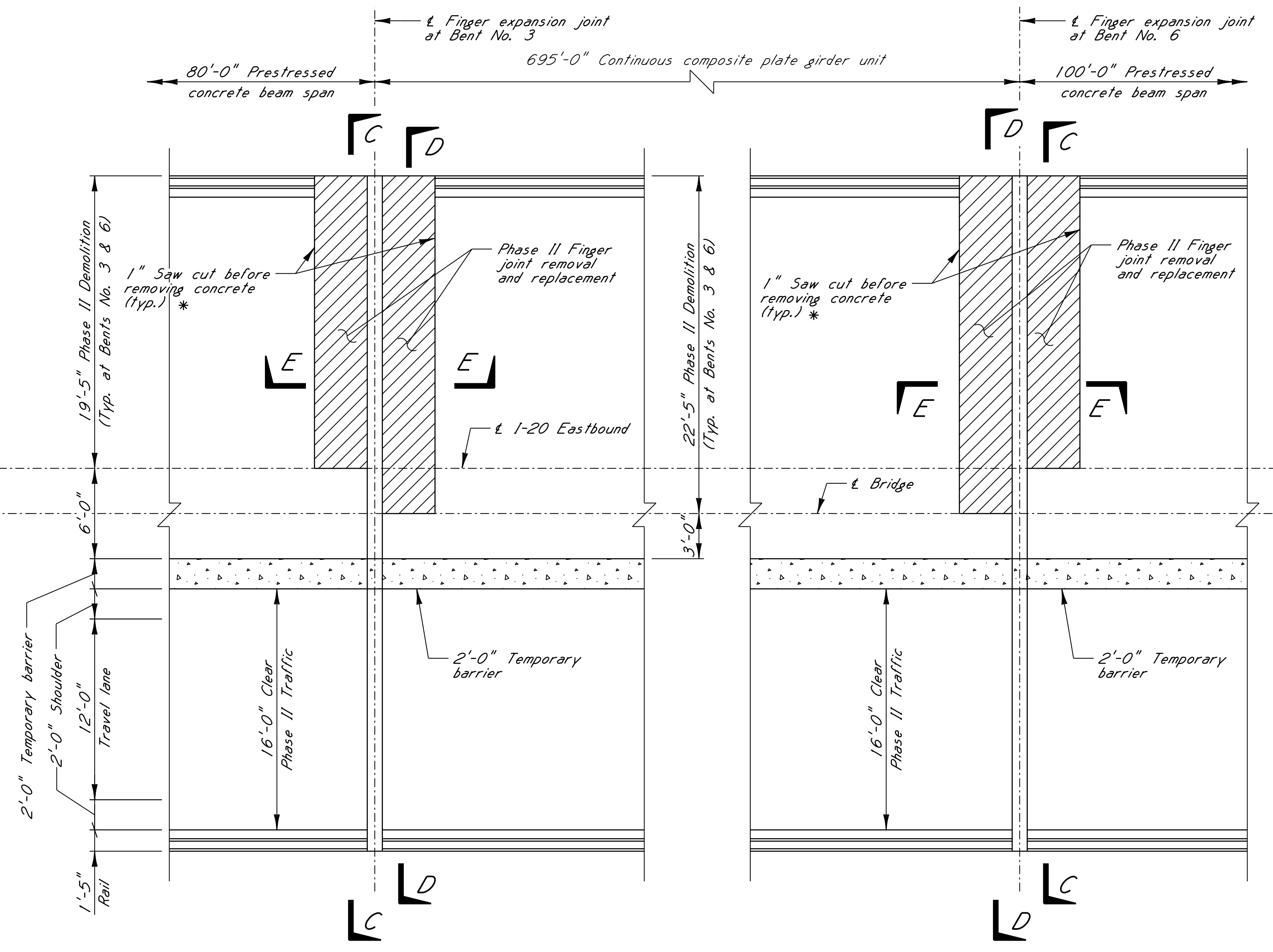
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BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 977+52.88 I-20 EASTBOUND LANES OVER MERIDIAN SPEEDWAY RAILROAD FINGER JOINT REPAIR PROJECT BR-0020-01(248) 107878-301000 HINDS COUNTY		WORKING NUMBER 4 of 15
DATE	DESIGNER Madison McEntire	CHECKER Amjad Elkhairi	ISSUE DATE	SHEET NUMBER 8005
	DIRECTOR OF STRUCTURES, STATE BRIDGE ENGINEER - JUSTIN WALKER PE. DEP. DIR. OF STRUCTURES, ASSIST. STATE BRIDGE ENGINEER - SCOTT WESTERFIELD PE.			



PLAN VIEW - PHASE I
Scale: 3/8" = 1'-0"

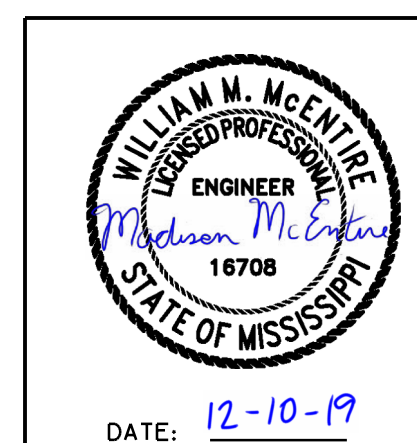


PLAN VIEW - PHASE II
Scale: 3/8" = 1'-0"

* See "SAW CUT NOTE" and "EPOXY BONDING NOTE" on Sheet No. 9.

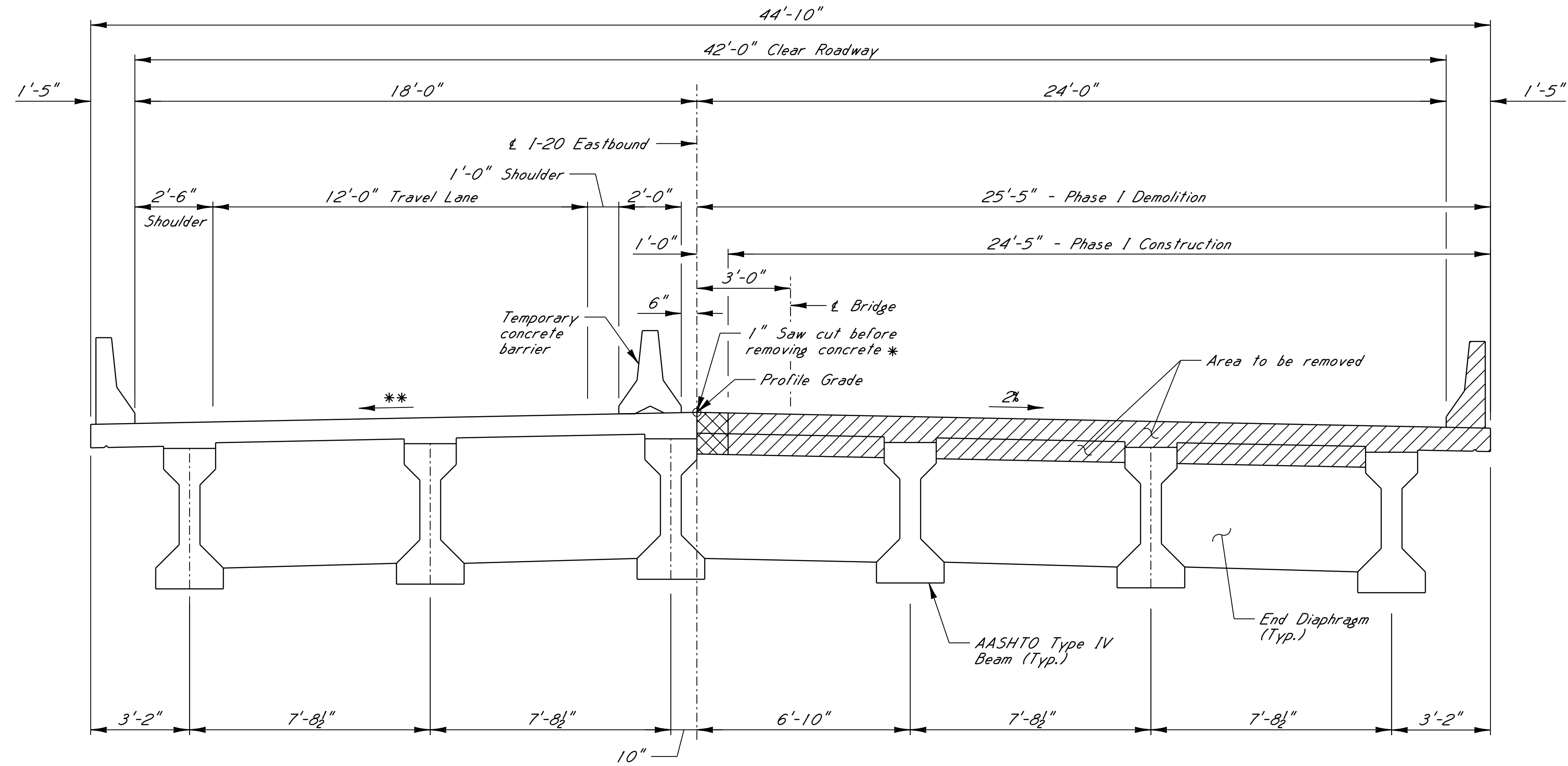
NOTE:
For Sections A-A and B-B, See Sheet No. 6.
For Sections C-C and D-D, See Sheet No. 7.
For Section E-E, See Sheet No. 9.

10:56:00 AM 12/10/2019 L:\2019\19T05003 - L-20 EB Finger Joint Repair\Drawings\Final Plans\1-20-EB-S105-Phasing.dgn

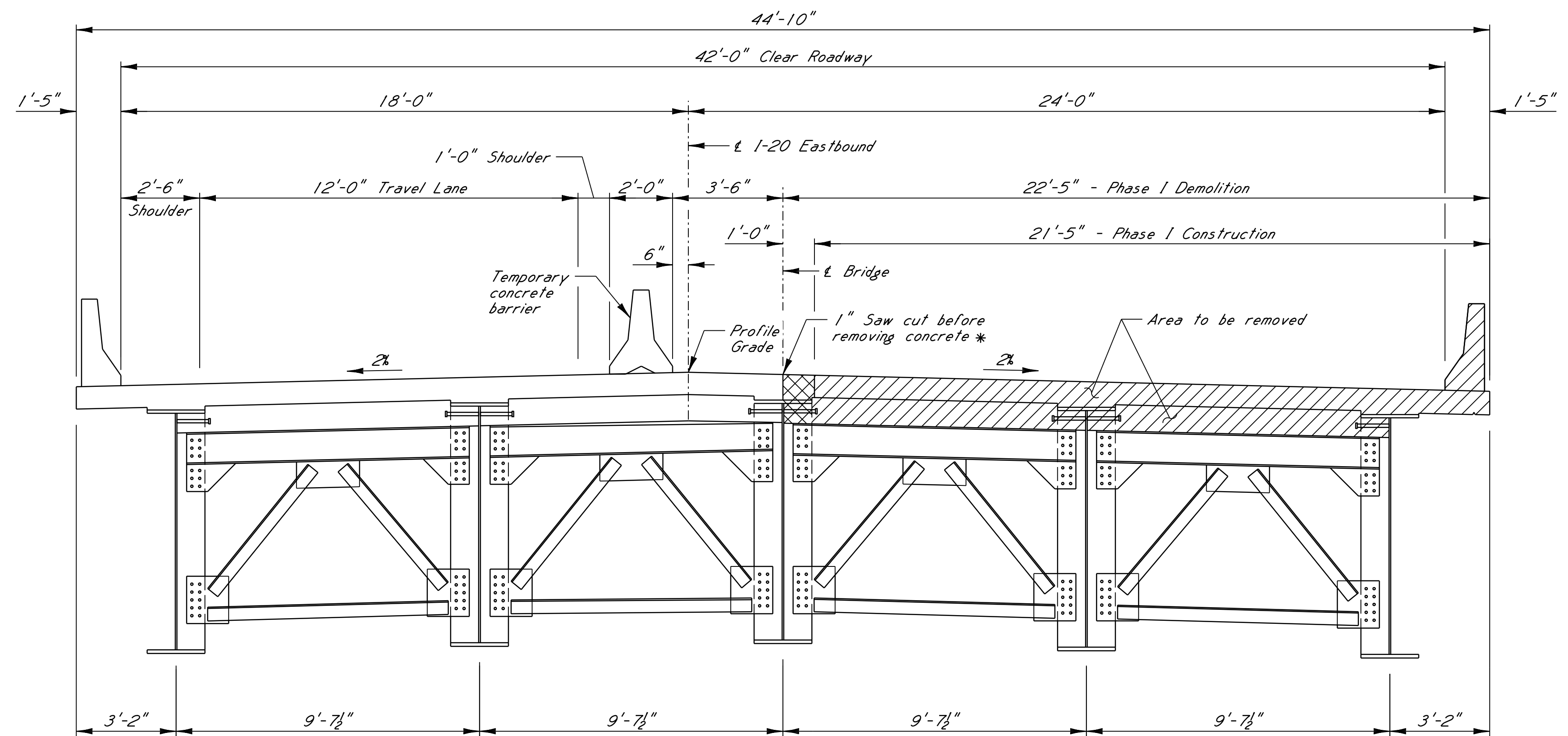


DATE: 12-10-19

BY MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 977+52.88	
FINGER JOINT REPAIR	
PROJECT BR-0020-01(248) 107878-301000	WORKING NUMBER 5 of 15
HINDS COUNTY	SHEET NUMBER 8006
DESIGNER: Madison McEntire CHECKER: Amjad Elkhairi	DATE: 12-10-19
DETAILER: Hugh E. Williams ISSUE DATE:	
DIRECTOR OF STRUCTURES, STATE BRIDGE ENGINEER - JUSTIN WALKER PE. DEP. DIR. OF STRUCTURES, ASSIST. STATE BRIDGE ENGINEER - SCOTT WESTERFIELD PE.	



SECTION A-A
 (Showing Phase I for Prestressed Concrete Span)
 (Looking Forward)
 Scale: $\frac{3}{8}$ " = 1'-0"



SECTION B-B
 (Showing Phase I for Composite Plate Girder Unit)
 (Looking Forward)
 Scale: $\frac{3}{8}$ " = 1'-0"

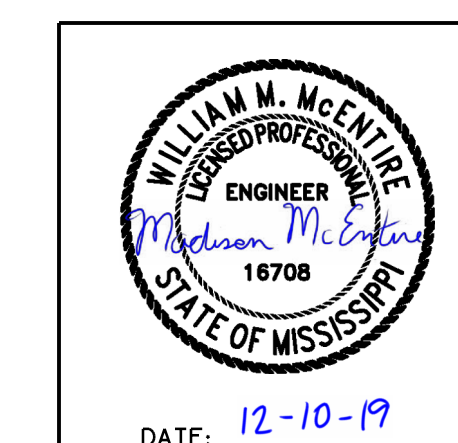
* See "SAW CUT NOTE" and "EPOXY BONDING NOTE" on Sheet No. 9.
 ** Span 2 - Varies from 0% at Sta. 978+34.00 to 2% at Sta. 979+14.00
 Span 6 - 2%

NOTE
 The Contractor is required to survey the existing bridge deck to ensure the fabricator has accurate information with which to fabricate the finger joint for proper fit. This survey information shall be submitted to MDOT for review prior to finger joint fabrication.

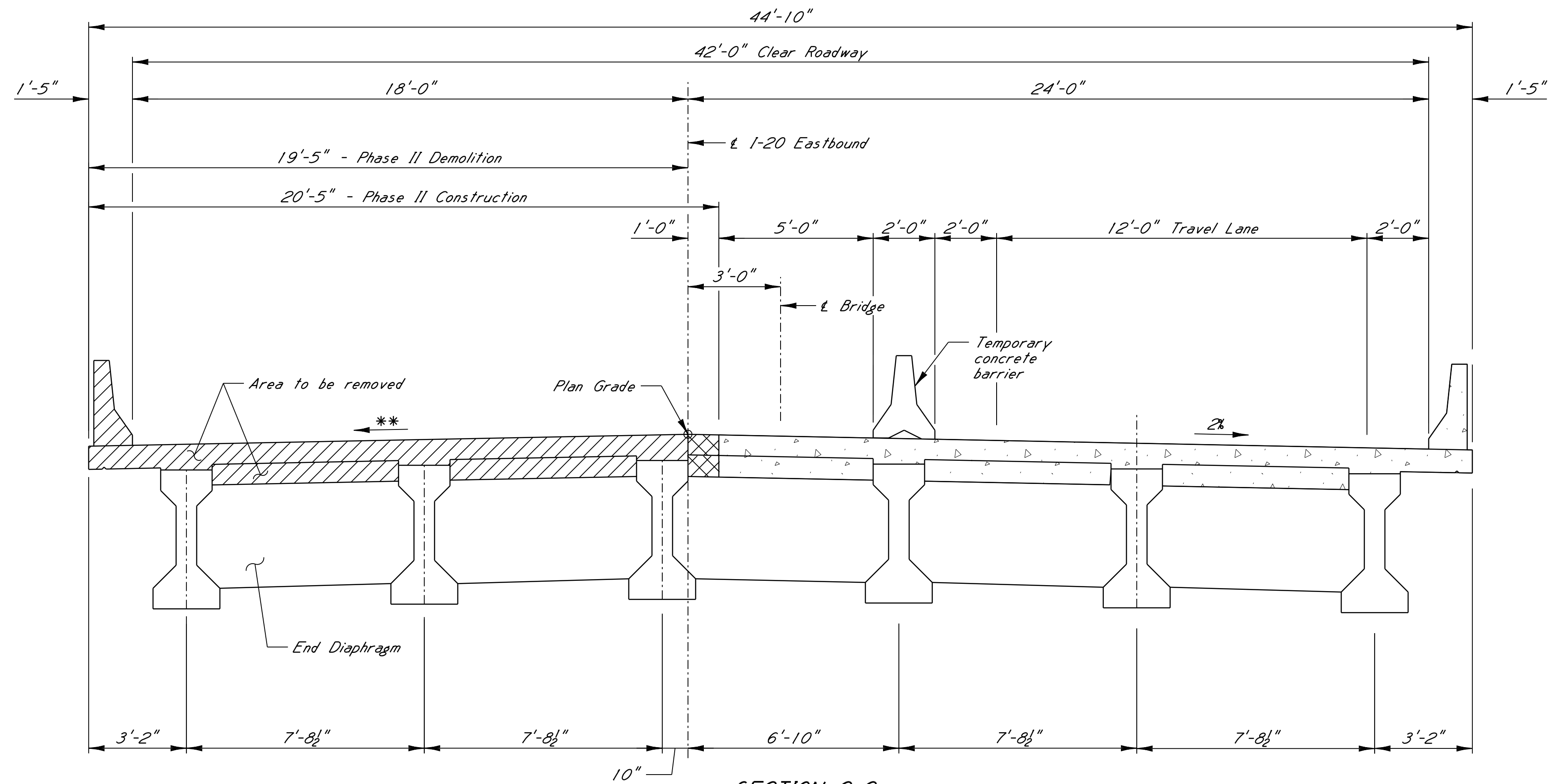
LEGEND

Phase I Demolition and Construction

Phase I Demolition Only



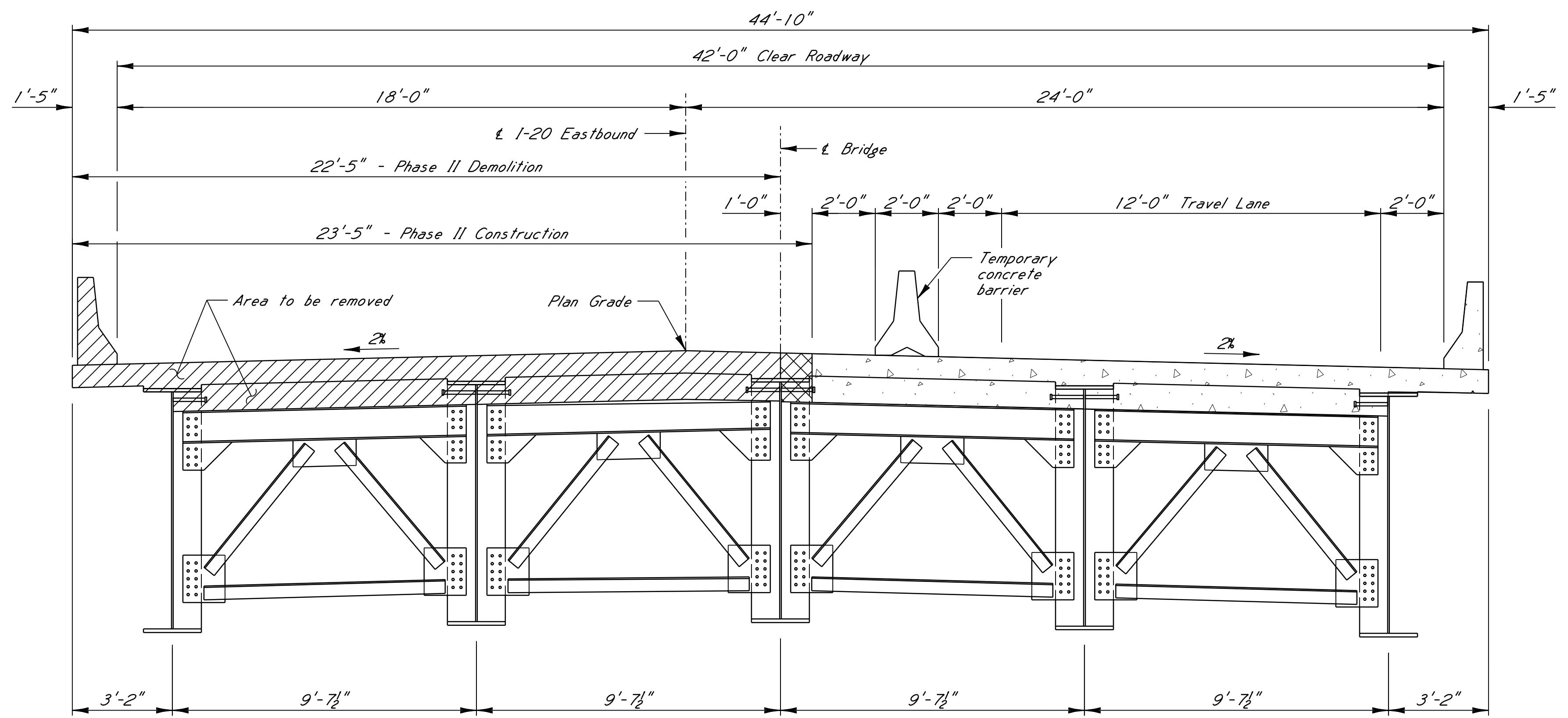
BY MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 977+52.88	
FINGER JOINT REPAIR	
PROJECT BR-0020-01(248) 107878-301000	WORKING NUMBER 6 of 15
HINDS COUNTY	SHEET NUMBER 8007
DESIGNER: Madison McEntire CHECKER: Amjad Elkhaiir	ISSUE DATE
DIRECTOR OF STRUCTURES, STATE BRIDGE ENGINEER - JUSTIN WALKER PE. DEP. DIR. OF STRUCTURES, ASSIST. STATE BRIDGE ENGINEER - SCOTT WESTERFIELD PE.	



SECTION C-C
(Showing Phase II for Prestressed Concrete Span)
(Looking Forward)
Scale: 3/8" = 1'-0"

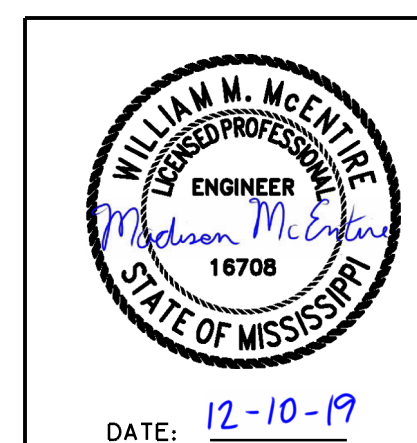
** Span 2 - Varies from 0% at Sta. 978+34.00 to 2% at Sta. 979+14.00
Span 6 - 2%

NOTE
The Contractor is required to survey the existing bridge deck to ensure the fabricator has accurate information with which to fabricate the finger joint for proper fit. This survey information shall be submitted to MDOT for review prior to finger joint fabrication.

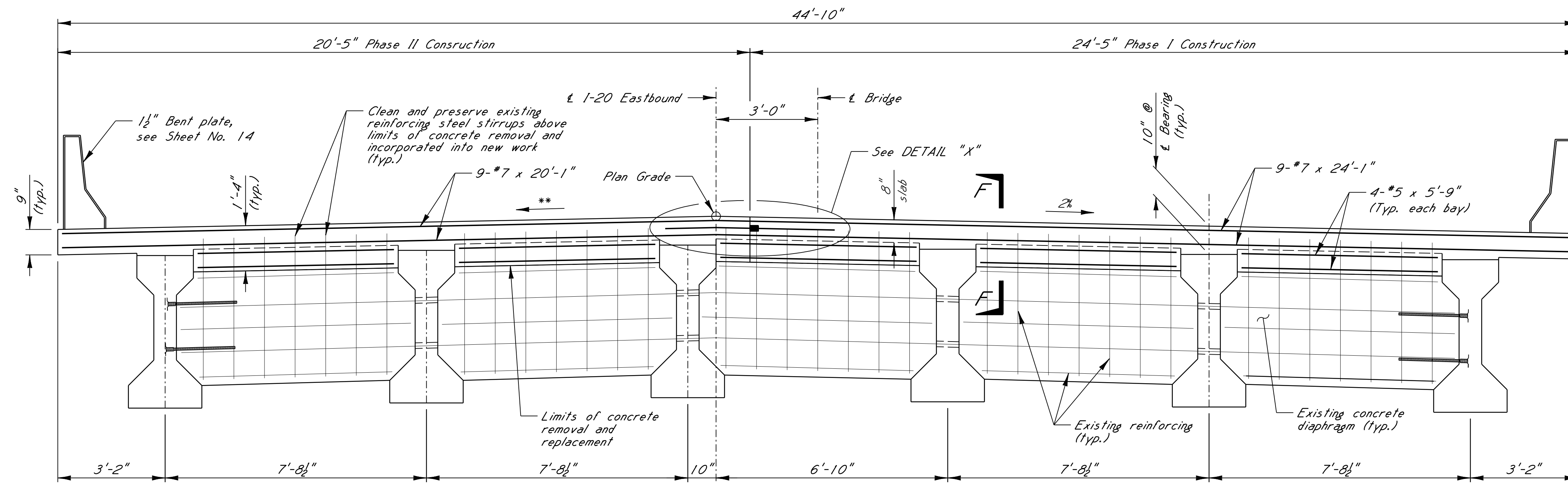


SECTION D-D
(Showing Phase II for Composite Plate Girder Unit)
(Looking Forward)
Scale: 3/8" = 1'-0"

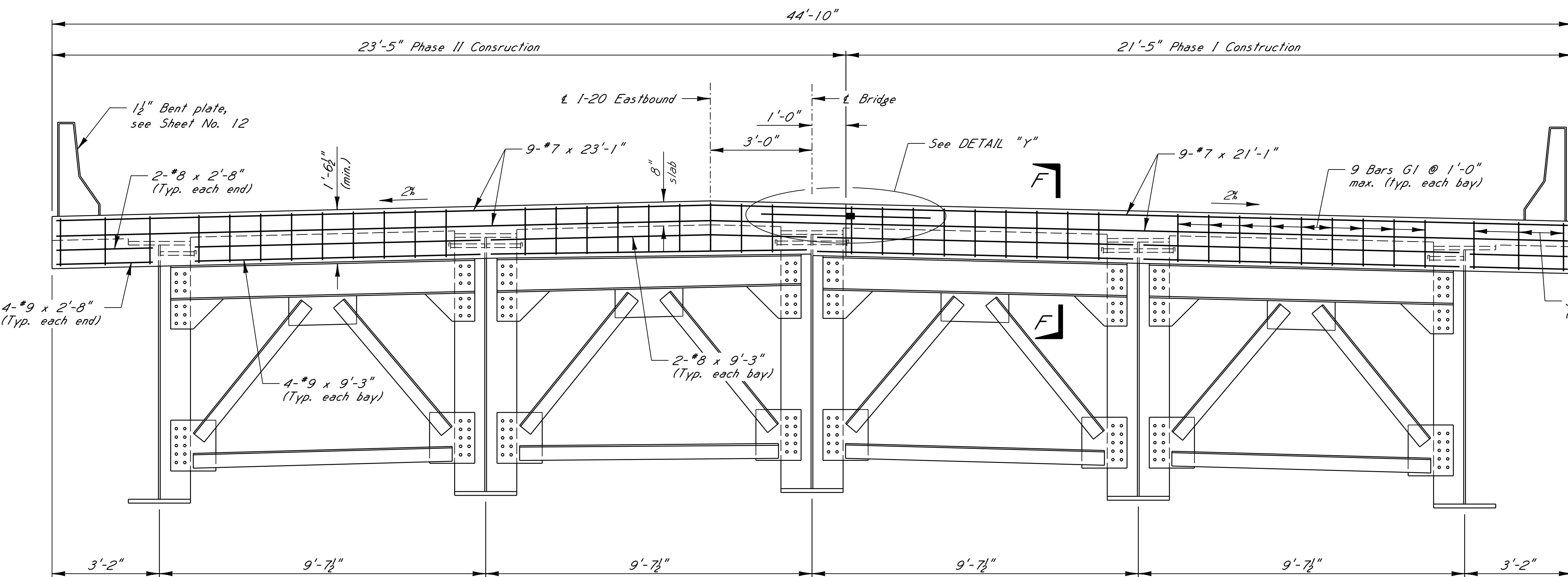
LEGEND
 Phase II Demolition and Construction
 Phase II Construction Only



BY MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 977+52.88	
FINGER JOINT REPAIR	
PROJECT BR-0020-01(248) 107878-301000	WORKING NUMBER 7 of 15
HINDS COUNTY	SHEET NUMBER 8008
DESIGNER: Madison McEntire CHECKER: Amjed Elkhaiir DETAILER: Hugh E. Williams ISSUE DATE: _____	DIR. OF STRUCTURES, STATE BRIDGE ENGINEER - JUSTIN WALKER PE. DEP. DIR. OF STRUCTURES, ASSIST. STATE BRIDGE ENGINEER - SCOTT WESTERFIELD PE.



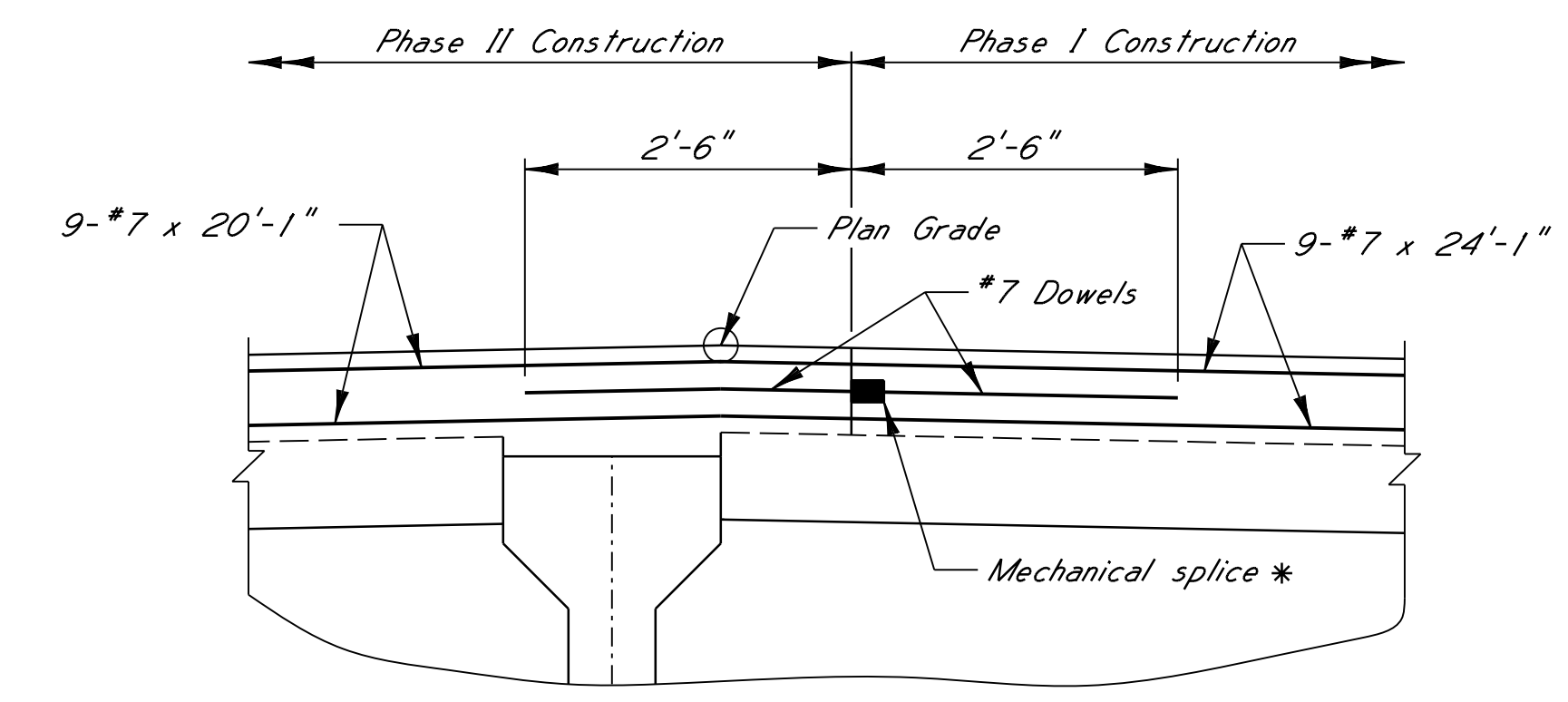
TYPICAL SECTION AT END DIAPHRAGM
 (Showing reinforcing at prestressed concrete spans)
 (Looking forward)
 Scale: $\frac{3}{8}$ " = 1'-0"



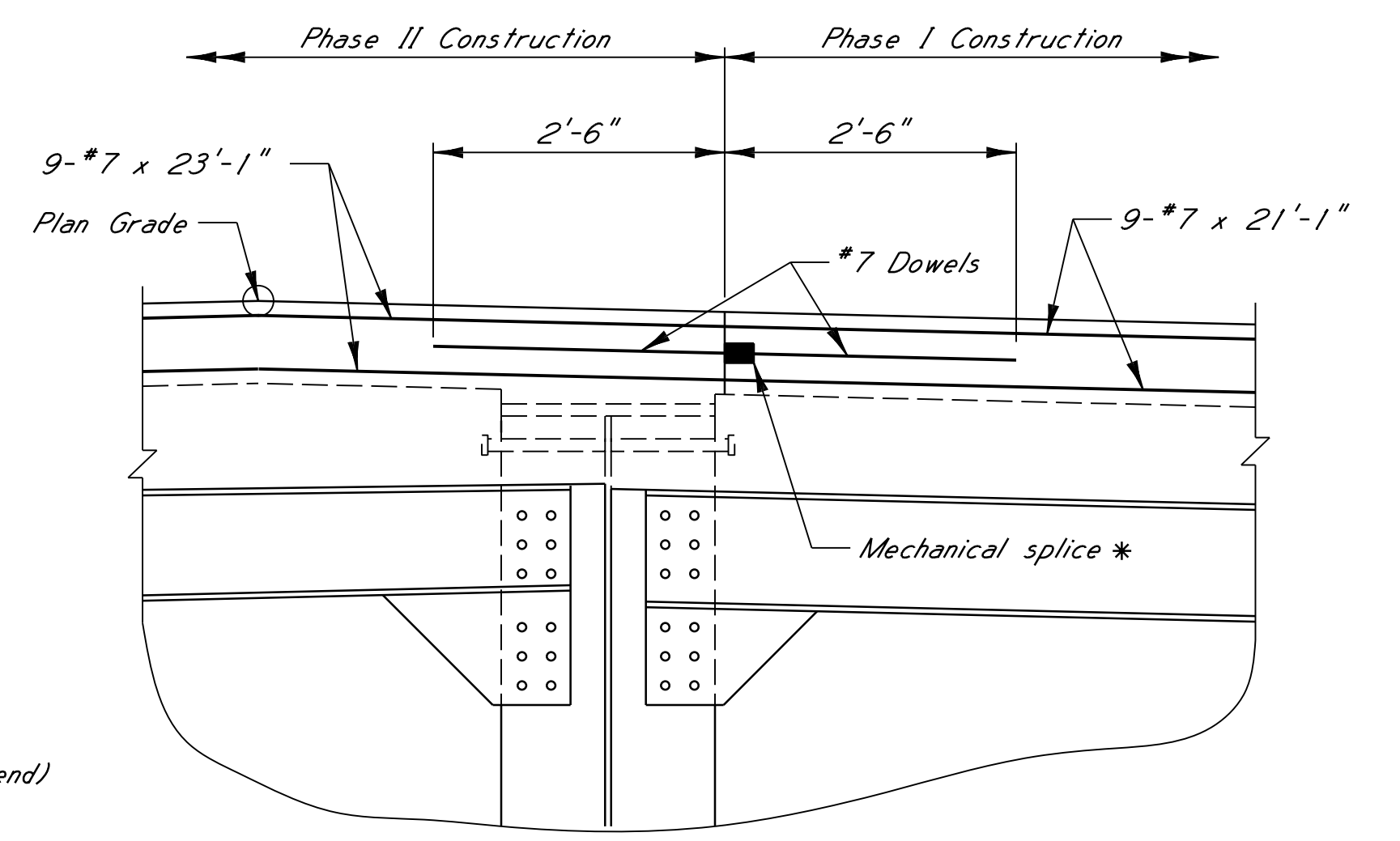
TYPICAL SECTION AT DROP SLAB
 (Showing reinforcing at composite plate girder span)
 (Looking forward)
 Scale: $\frac{3}{8}$ " = 1'-0"

* See "MECHANICAL SPLICE NOTE" on Sheet No. 9.

** Span 2 - Varies from 0k at Sta. 978+34.00 to 2k at Sta. 979+14.00
 Span 6 - 2k

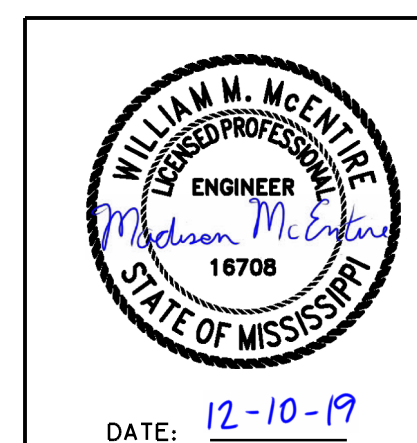


DETAIL "X"
 Scale: $\frac{3}{4}$ " = 1'-0"

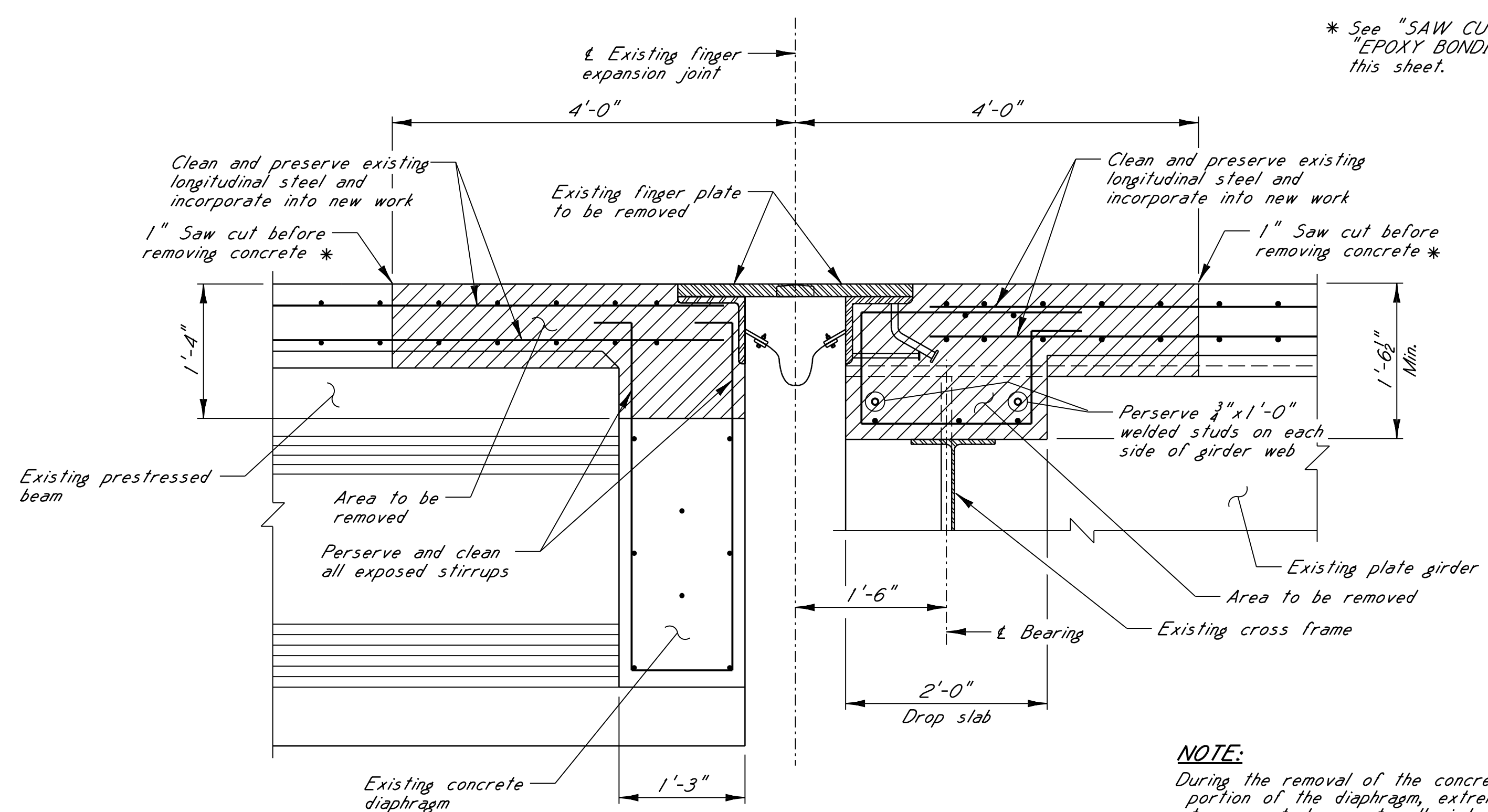


DETAIL "Y"
 Scale: $\frac{3}{4}$ " = 1'-0"

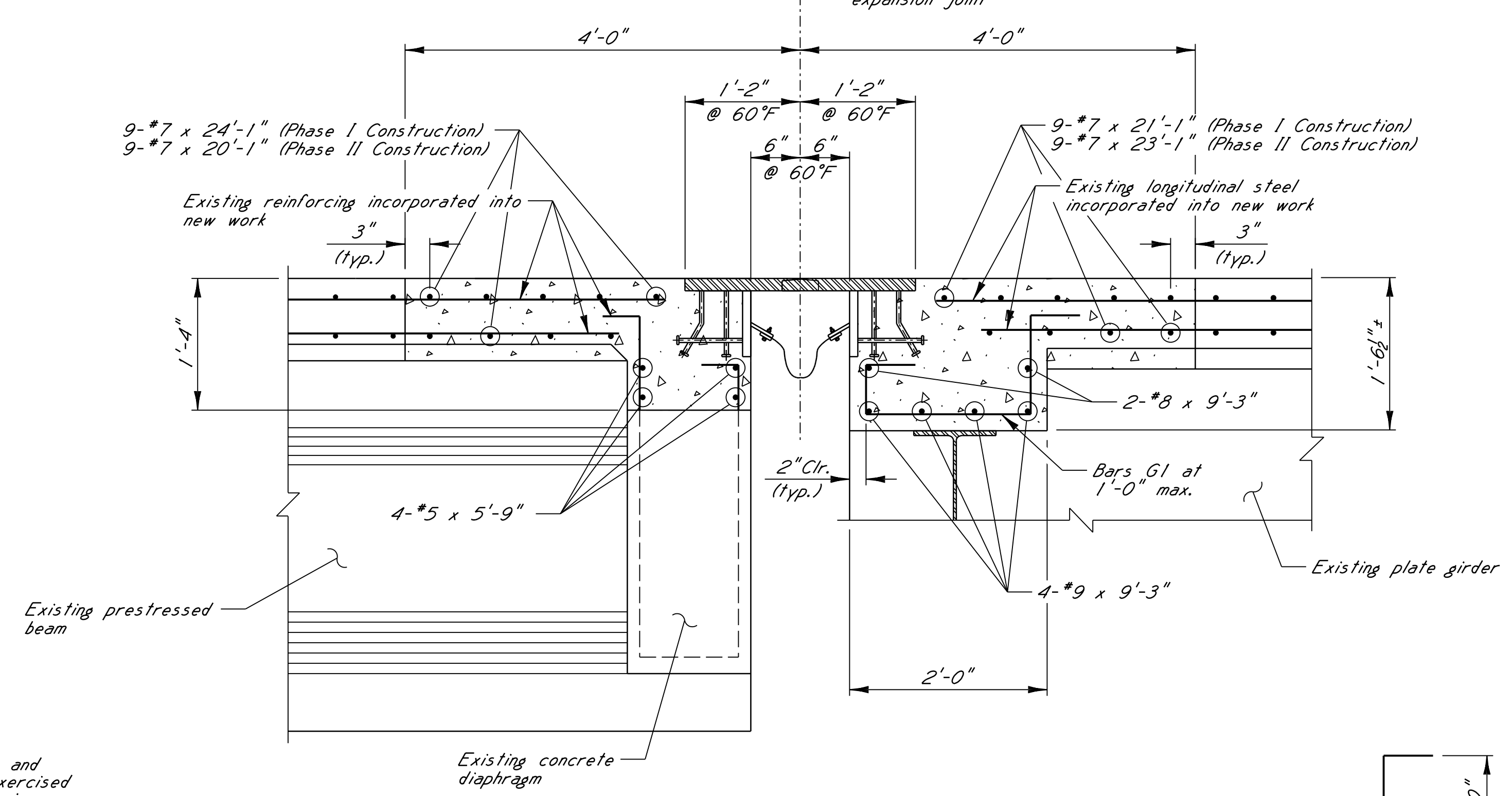
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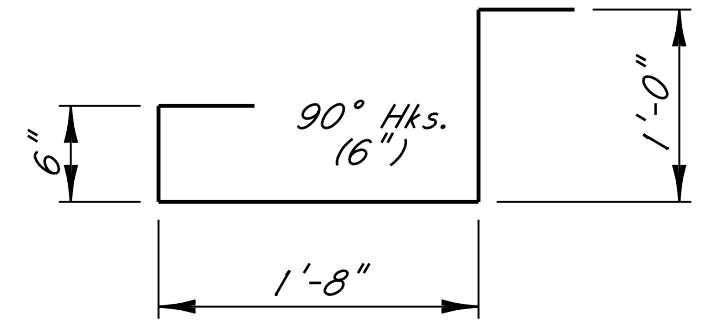
BY MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 977+52.88	
FINGER JOINT REPAIR	
PROJECT BR-0020-01(248)	WORKING NUMBER 8 of 15
107878-301000	SHEET NUMBER 8009
HINDS COUNTY	
DESIGNER: Madison McEntire	CHECKER: Amjad Elkhaiir
DETAILER: Hugh E. Williams	ISSUE DATE:
DATE: 12-10-19	



SECTION E-E
(Showing limits of concrete removal)
Scale: 1" = 1'-0"



SECTION F-F
Scale: 1" = 1'-0"



Bars G1 - #5
BAR BENDING DETAILS
(Dimensions are out-to-out)

NOTE:
During the removal of the concrete deck, drop slab and portion of the diaphragm, extreme care shall be exercised to prevent damage to all girders, stringers and their various components.

ANCHOR NOTE:

1. Rebar Anchoring System shall be one of the following products:
 - A. "HIT-RE 500 V3" shall be as Manufactured By Hilti, Inc., https://www.hilti.com/c/CLS_FASTENER_7135/CLS_INJECTABLE_ADHESIVE_ANCHORS_7135/4929903
 - B. "Ultra Bond 1300" shall be as Manufactured By Adhesives Technology Corp., <https://atcepoxy.com/high-strength-non-sag-doweling-epoxy/>
 - C. "Red Head C6+" shall be as Manufactured By ITW Ramset/Red Head, <https://www.itwredhead.com/products/adhesive-anchors/c6>
2. Installation of the anchoring system shall be performed in strict accordance with the Manufacturer's recommendations.
3. A representative of the anchoring system's manufacturer must be present for sufficient time to assure that the contractor is properly schooled in the installation of anchoring system.

SAWCUT NOTE:

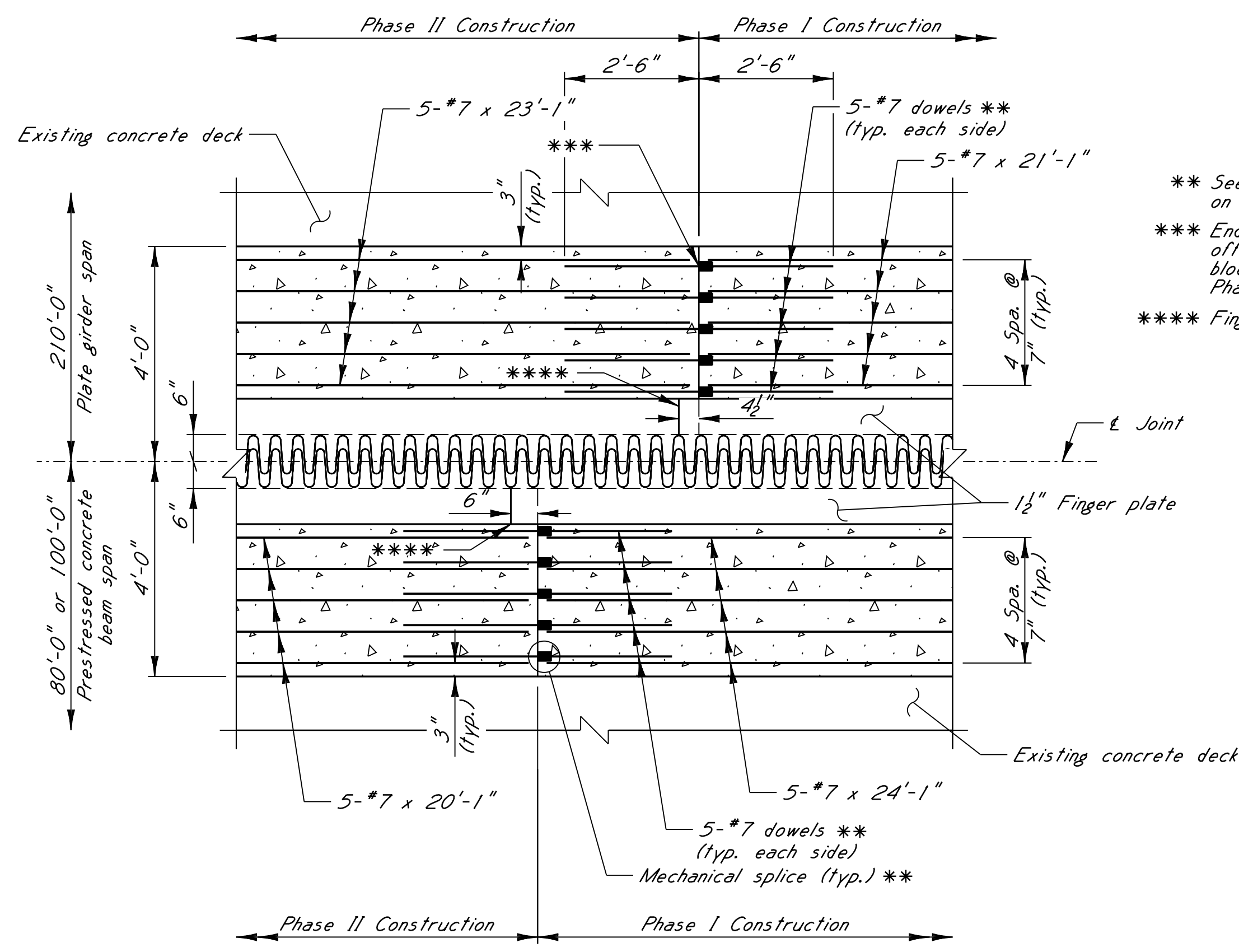
Saw cut 1"± before removing existing slab. Reinforcing bars shall remain in place. Blast clean existing steel & contact surfaces immediately prior to applying epoxy & pouring concrete for new construction. See "EPOXY BONDING NOTE" On This Sheet.

EPOXY BONDING NOTE: (Not A Separate Pay Item)

Contact areas where new concrete is placed against old concrete shall be blasted clean and then coated with an approved epoxy binder designed to bond the new concrete to old concrete. The binder shall be applied in accordance with manufacturer's directions.

MECHANICAL SPLICE NOTE: (Not A Separate Pay Item)

1. Mechanical reinforcing bar splice shall be one of the finishing products or approved equal.
 - A. "LENTON LOCK" shall be as manufactured by ERICO Products, Inc., <https://www.ericoproducts.com>
 - B. "Zip Screwlok" shall be as manufactured by Bar Splice Products, Inc., <https://www.barsplice.com>
 - C. "Bar Lock" shall be as manufactured by DAYTON SUPERIOR, <https://www.daytonsuperior.com>
2. Installation of mechanical splicing system shall be performed in strict accordance with the Manufacturer's recommendations.
3. A representative of the Manufacturer must be present for sufficient time to assure that the contractor is properly schooled in the installation of mechanical splicing system.
4. Mechanical couplers shall develop at least 125% of the specified yield strength of the reinforcing steel.
5. It is the Contractor's responsibility to adjust the dimensions of reinforcing to the proper length according to what mechanical splice is used during construction.



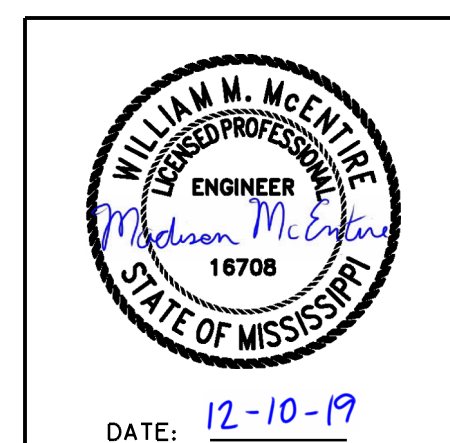
PLAN OF PHASING SPLICE
(Showing transverse reinforcing in top of deck)
Scale: 1" = 1'-0"

NOTE:
Existing deck reinforcing to be incorporated into new work not shown for clarity.

- ** See "MECHANICAL SPLICE NOTE" on this sheet.
- *** End of mechanical splices shall be topped off during Phase I Construction to avoid blockage for splicing use during Phase II Construction.
- **** Finger plate weld. See Sheet No. 14

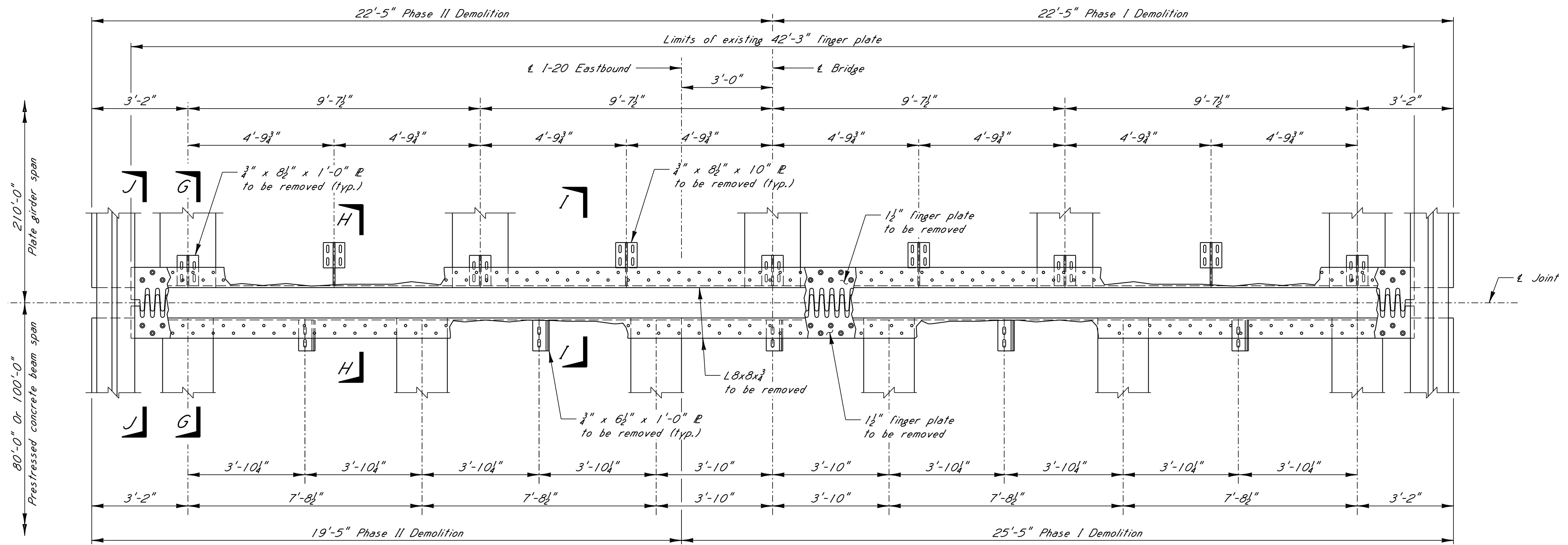
GENERAL NOTES:

All concrete in span and railing shall be Class "AA". chamfer all edges 3/4", unless otherwise noted. See layout sheet for finishing of concrete surfaces. Placing dimensions for reinforcing steel to concrete surfaces are clear distances.



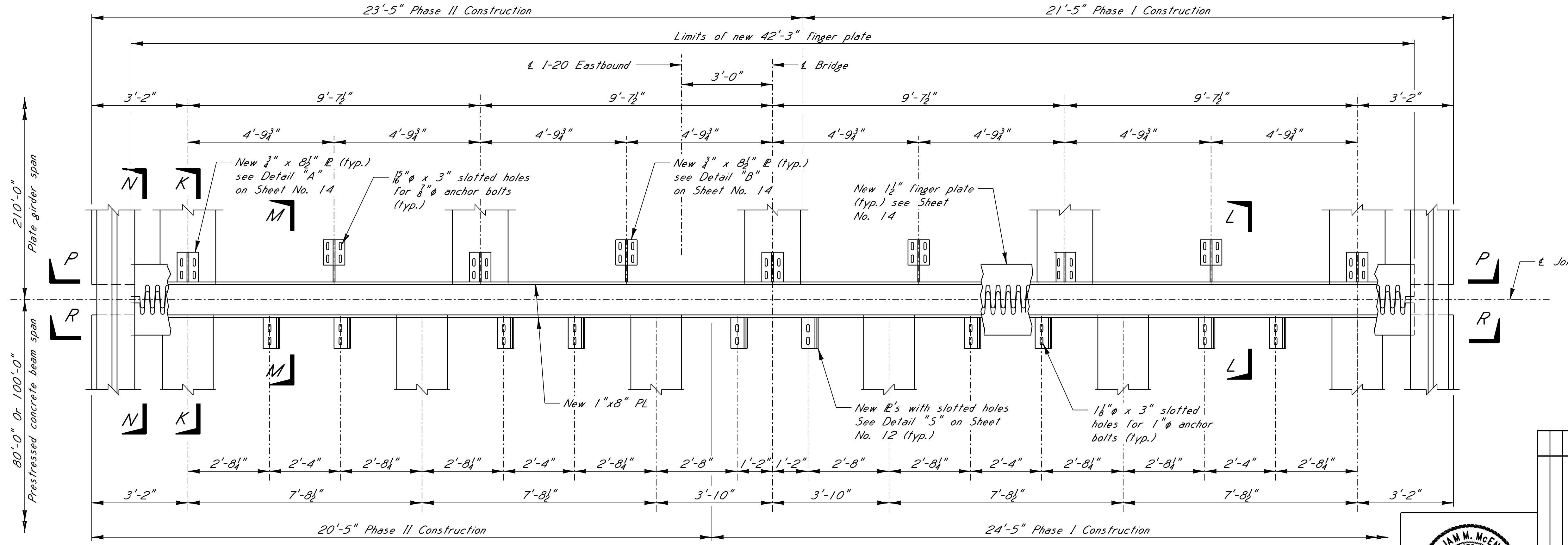
DATE: 12-10-19

BY MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
BRIDGE AT STA. 977+52.88	
FINGER JOINT REPAIR	
PROJECT BR-0020-01(248)	WORKING NUMBER 9 of 15
107878-301000	SHEET NUMBER 8010
HINDS COUNTY	
DESIGNER: Madison McEntire	CHECKER: Amjad Elkhairi
DETAILER: Hugh E. Williams	ISSUE DATE:
DIRECTOR OF STRUCTURES, STATE BRIDGE ENGINEER - JUSTIN WALKER PE.	
DEP. DIR. OF STRUCTURES, ASSIST. STATE BRIDGE ENGINEER - SCOTT WESTERFIELD PE.	

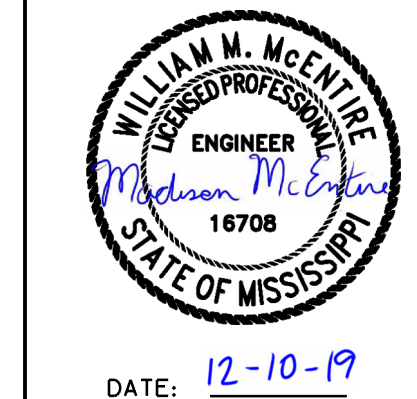


PLAN OF EXISTING FINGER JOINT AT BENT NOS. 3 & 6
 (Showing finger joint items to be removed as noted above)
 Scale: 1/2" = 1'-0"

NOTE:
 For Sections G-G, H-H, I-I and J-J,
 See Sheet No. 11.
 For Sections K-K, L-L and M-M,
 See Sheet No. 12.
 For Sections P-P and R-R,
 See Sheet No. 13.
 For Section N-N, See Sheet No. 14.

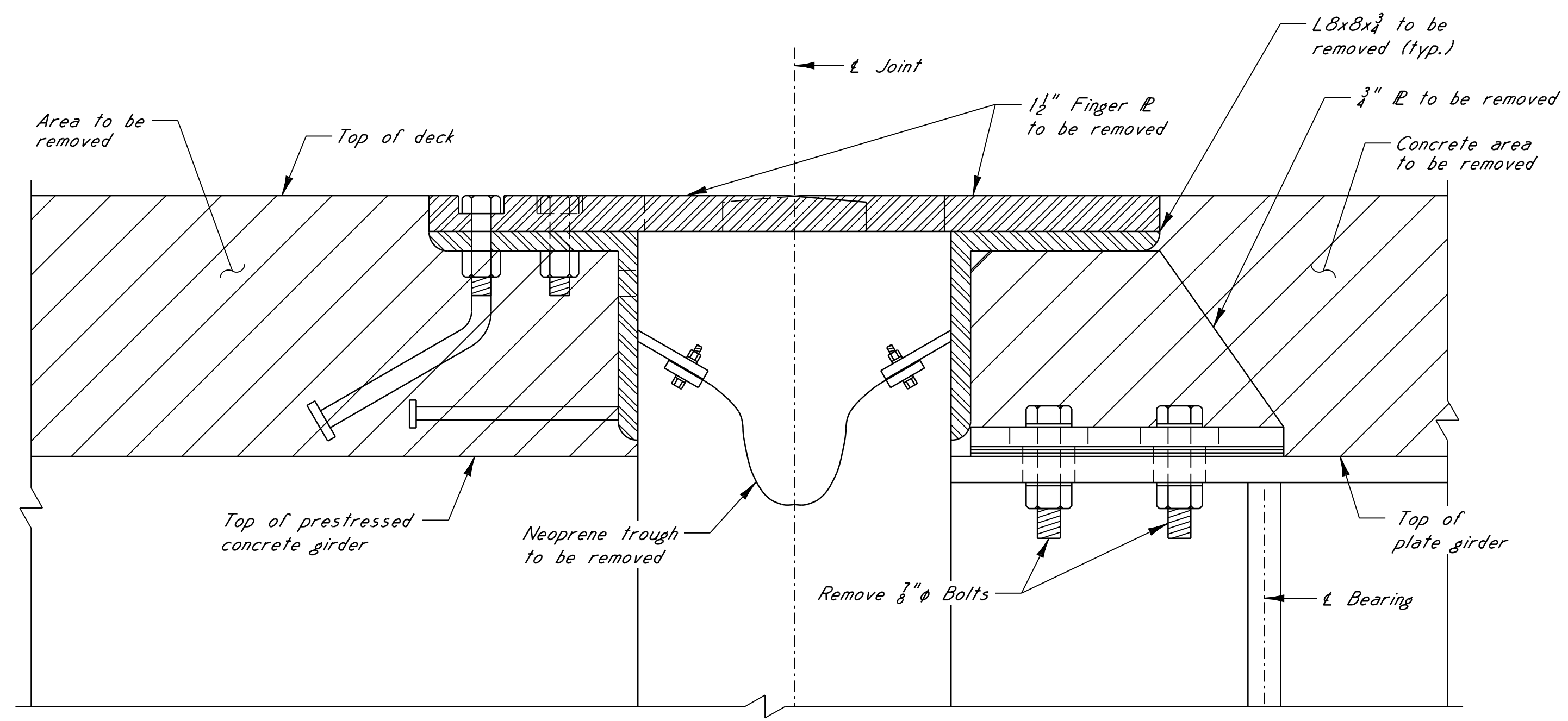


PLAN OF NEW FINGER JOINT AT BENT NOS. 3 & 6
 (Showing finger joint items to be installed as noted above)
 Scale: 1/2" = 1'-0"

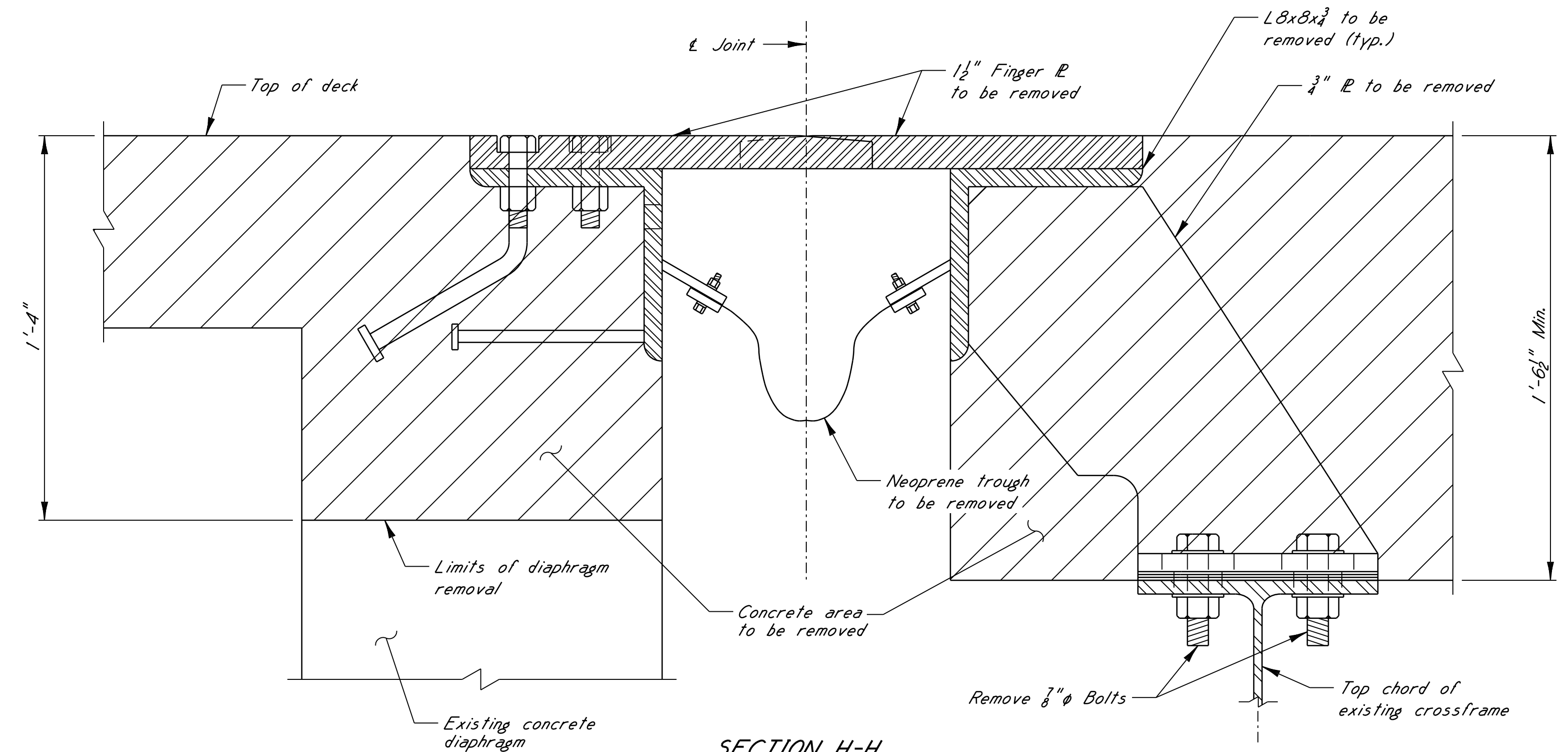


DATE: 12-10-19

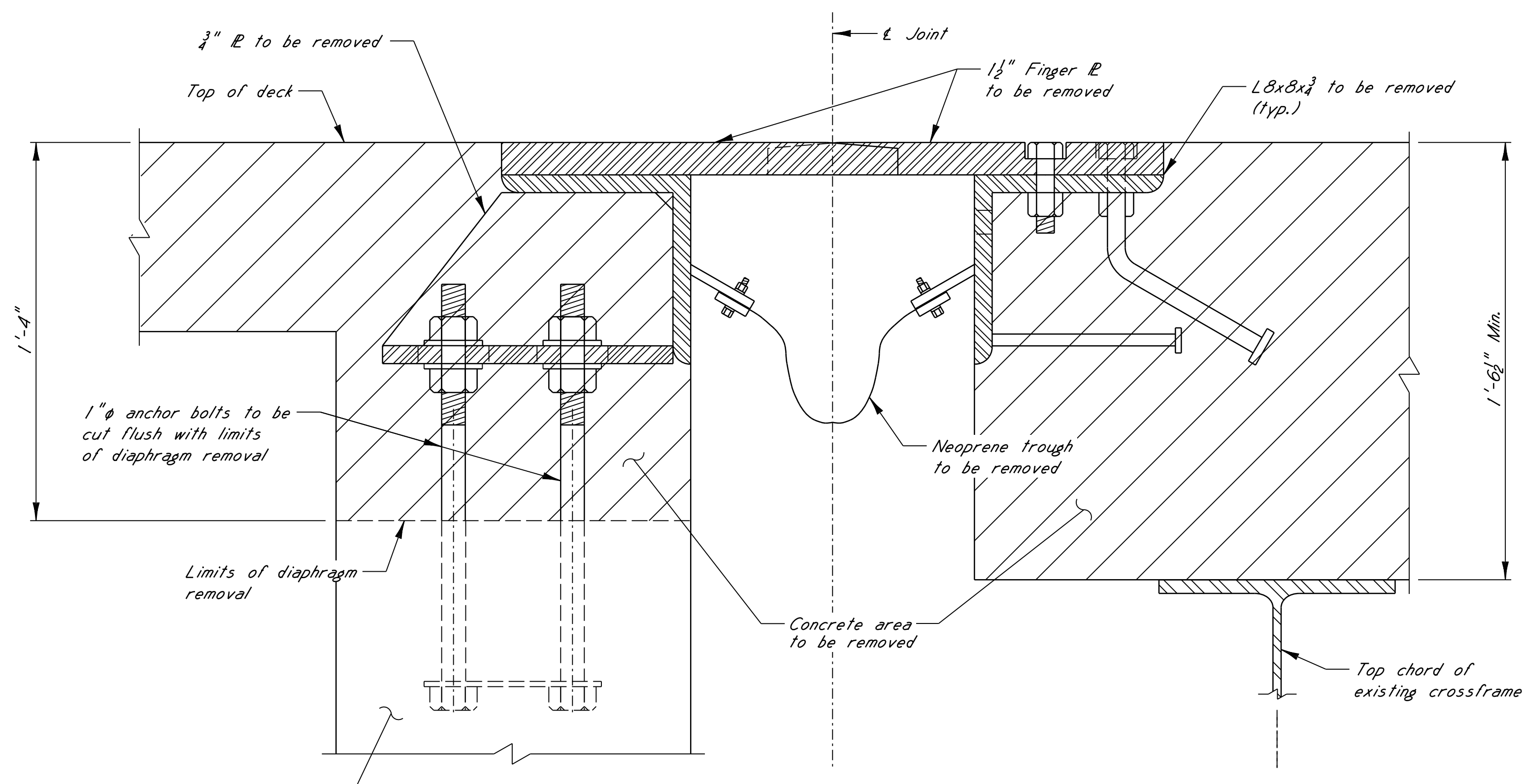
BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 977+52.88	
REVISIONS		FINGER JOINT REPAIR	
PROJECT		BR-0020-01(248) 107878-301000	
HINDS COUNTY		WORKING NUMBER 10 of 15	
DESIGNER	Madison McEntire	CHECKER	Amjad Elkhaiir
DATE		ISSUE DATE	
DIRECTOR OF STRUCTURES, STATE BRIDGE ENGINEER - JUSTIN WALKER PE.		SHEET NUMBER 8011	
DEP. DIR. OF STRUCTURES, ASSIST. STATE BRIDGE ENGINEER - SCOTT WESTERFIELD PE.			



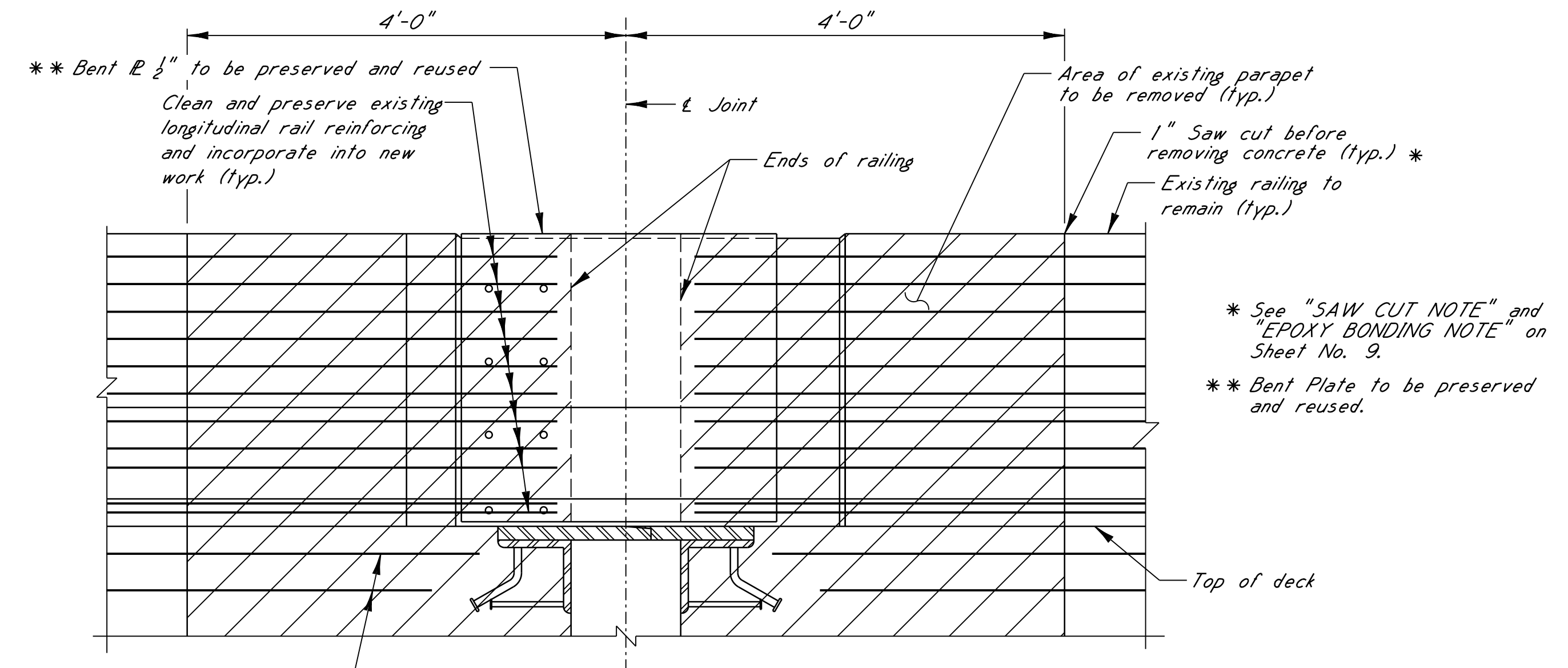
SECTION G-G
Scale: 3" = 1'-0"



SECTION H-H
Scale: 3" = 1'-0"

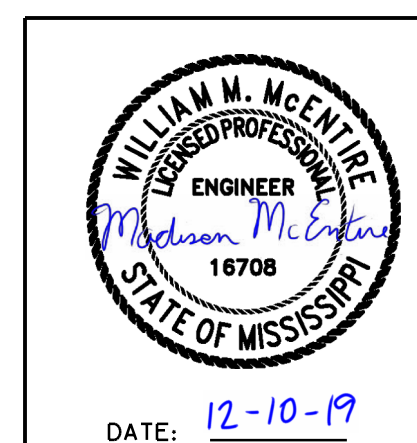


SECTION I-I
Scale: 3" = 1'-0"

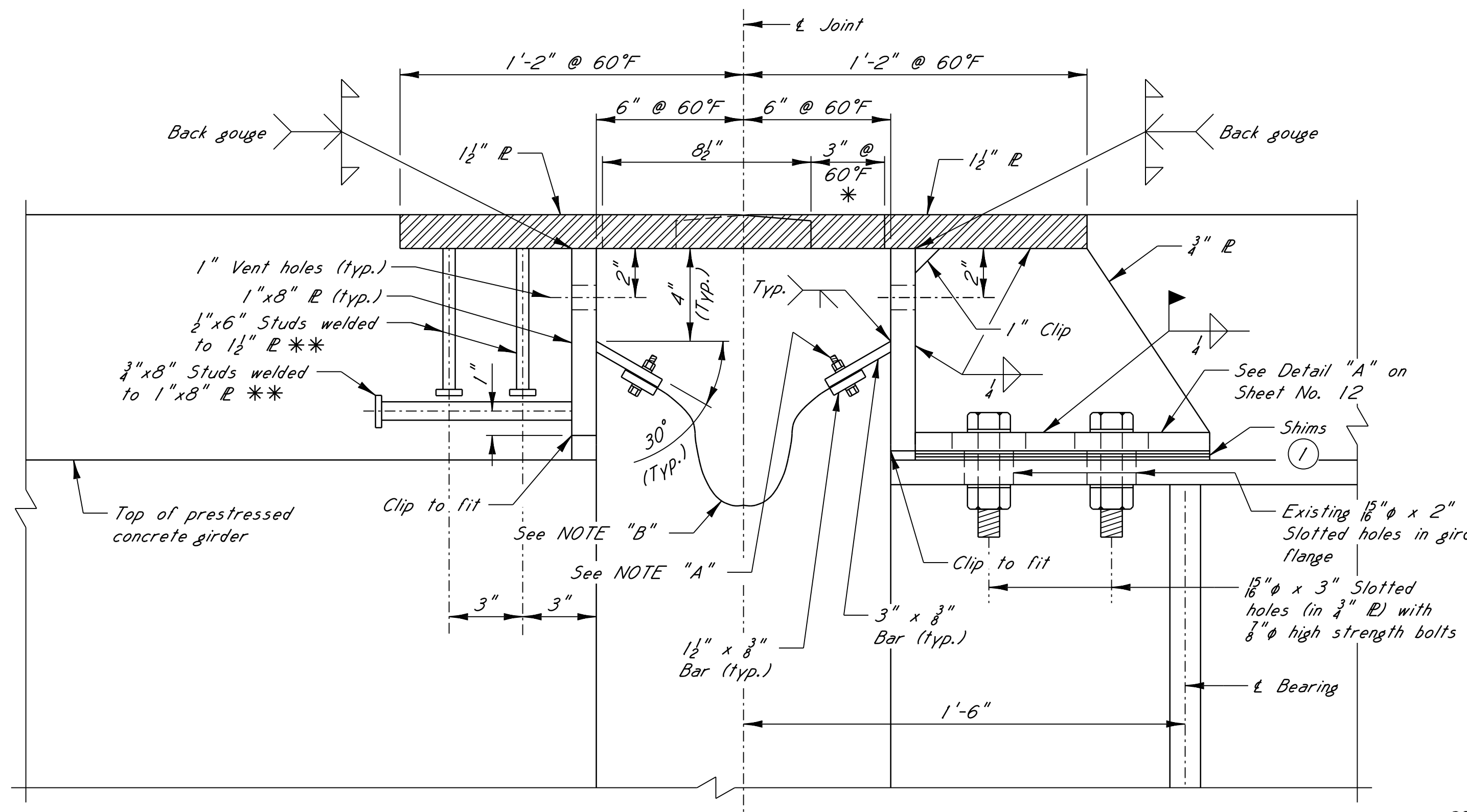


SECTION J-J
(Showing limits of rail removal)
Scale: 1" = 1'-0"

NOTE:
Remove concrete in railing with small hand tools to protect reinforcing steel. Existing longitudinal reinforcing steel shall be left intact, only cutting what is necessary for the placement of the new joint cover. Handheld chipping hammer shall be limited to 40 lbs. in size.



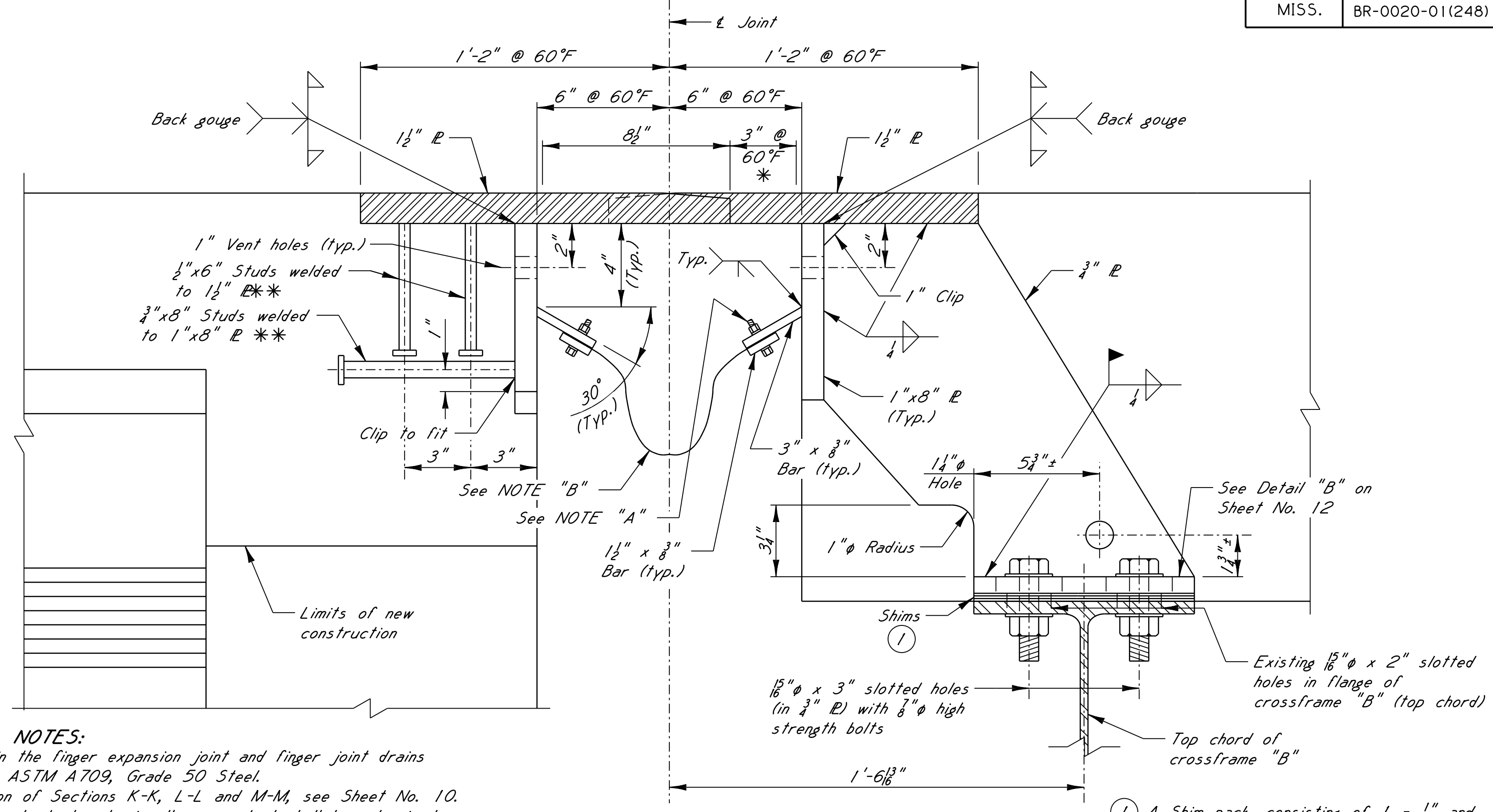
BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 977+52.88	
PROJECT		BR-0020-01(248) 107878-301000	
HINDS COUNTY		WORKING NUMBER 11 of 15	
DESIGNER	Medison McEntire	CHECKER	Amjad Elkhairi
DATE	12-10-19	ISSUE DATE	
DIRECTOR OF STRUCTURES, STATE BRIDGE ENGINEER - JUSTIN WALKER PE.		SHEET NUMBER 8012	
DEP. DIR. OF STRUCTURES, ASSIST. STATE BRIDGE ENGINEER - SCOTT WESTERFIELD PE.			



SECTION K-K
Scale: 3" = 1'-0"

*** NOTE:**
If temperature is different from 60°F at time of erection of finger joint, this dimension shall be as shown in the erection dimensions table on sheet no. 14.

**** See Section P-P and R-R On Sheet No. 13 for spacing.**



SECTION L-L
Scale: 3" = 1'-0"

① A Shim pack, consisting of 1 - 1/4" and 2 - 3/8" plates, shall be provided at each location where shims are required.

GENERAL NOTES:
All steel in the finger expansion joint and finger joint drains shall be ASTM A709, Grade 50 Steel.
For location of Sections K-K, L-L and M-M, see Sheet No. 10.
All structural steel and miscellaneous steel shall be galvanized per ASTM A123 after fabrication unless noted otherwise.
All bolts, hardened washers, lock washers and nuts shall be galvanized in accordance with ASTM A153.
All material for finger expansion joints and finger joint drains, including structural steel, miscellaneous steel, bolts, washers, studs, downspouts, neoprene troughs and supports, shall be included in the pay item "Structural Steel, A709, Grade 50".

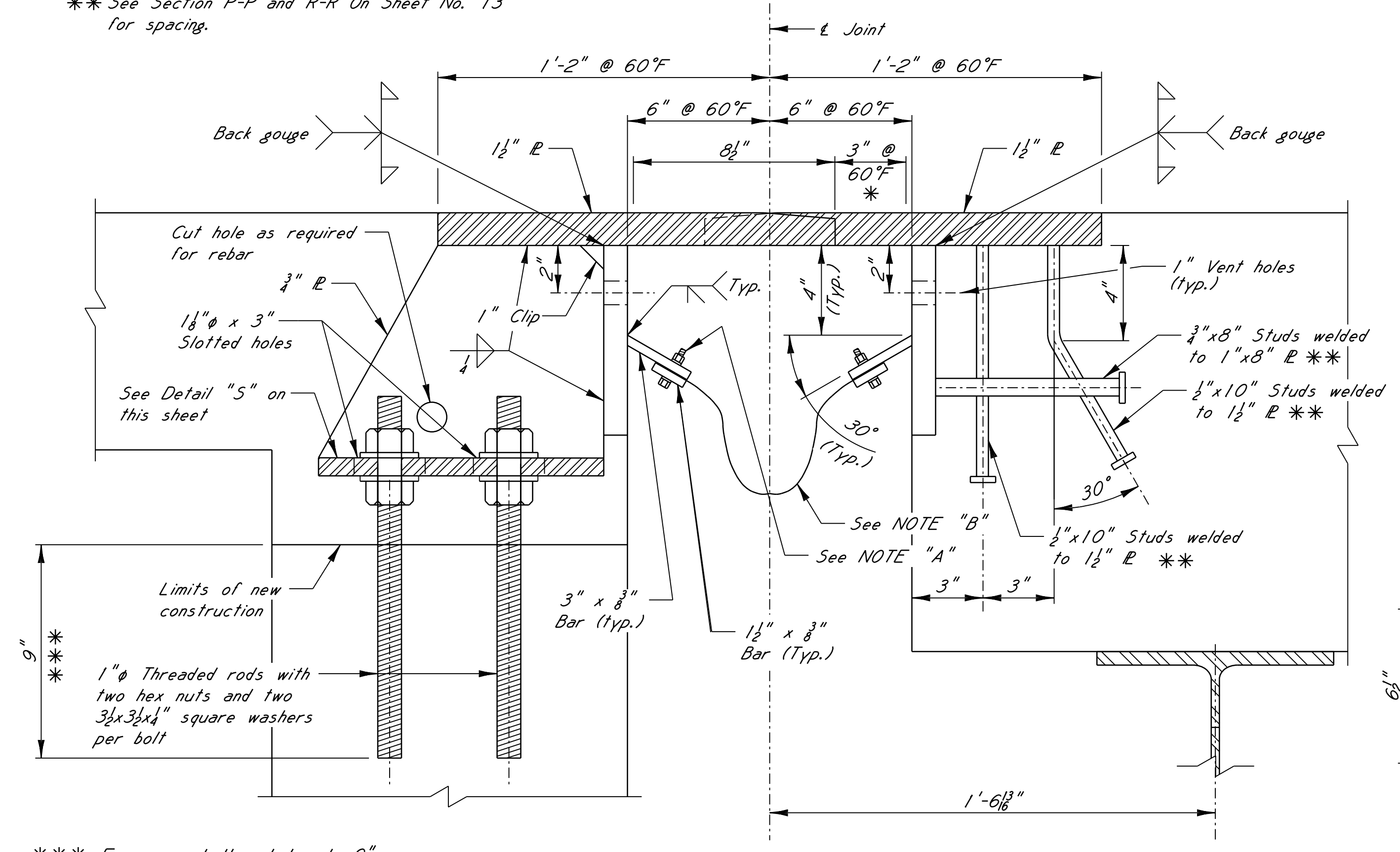
FABRICATION NOTES:
Any bolt, stud or vent hole that falls directly on or within 2" of an intermediate or girder support brace may be eliminated on the shop drawings.
Portion of Finger Joint to be constructed in each phase shall be shipped fully assembled.

NOTE "A":
3/8" Galvanized bolts at 12" o.c.; tack weld nuts to 3/8" x 3" bar.

NOTE "B":
A 3/8" x 20" Neoprene trough (not a separate pay item) is required at all finger joint locations. The neoprene material for drain trough shall be in accordance with ASTM D-2000 "Line Call-Out" 2 BC615 A14 F17 and is to be integrally molded with two layers of fabric reinforcement. Fabric shall be of good quality and equal to fabric used by manufacturers of sheet rubber products designed for use in highway bridge expansion joint applications. Immediately after placement of concrete in blackout and it has been vibrated through air holes in angles, the neoprene trough shall be thoroughly flushed out to remove all concrete debris.

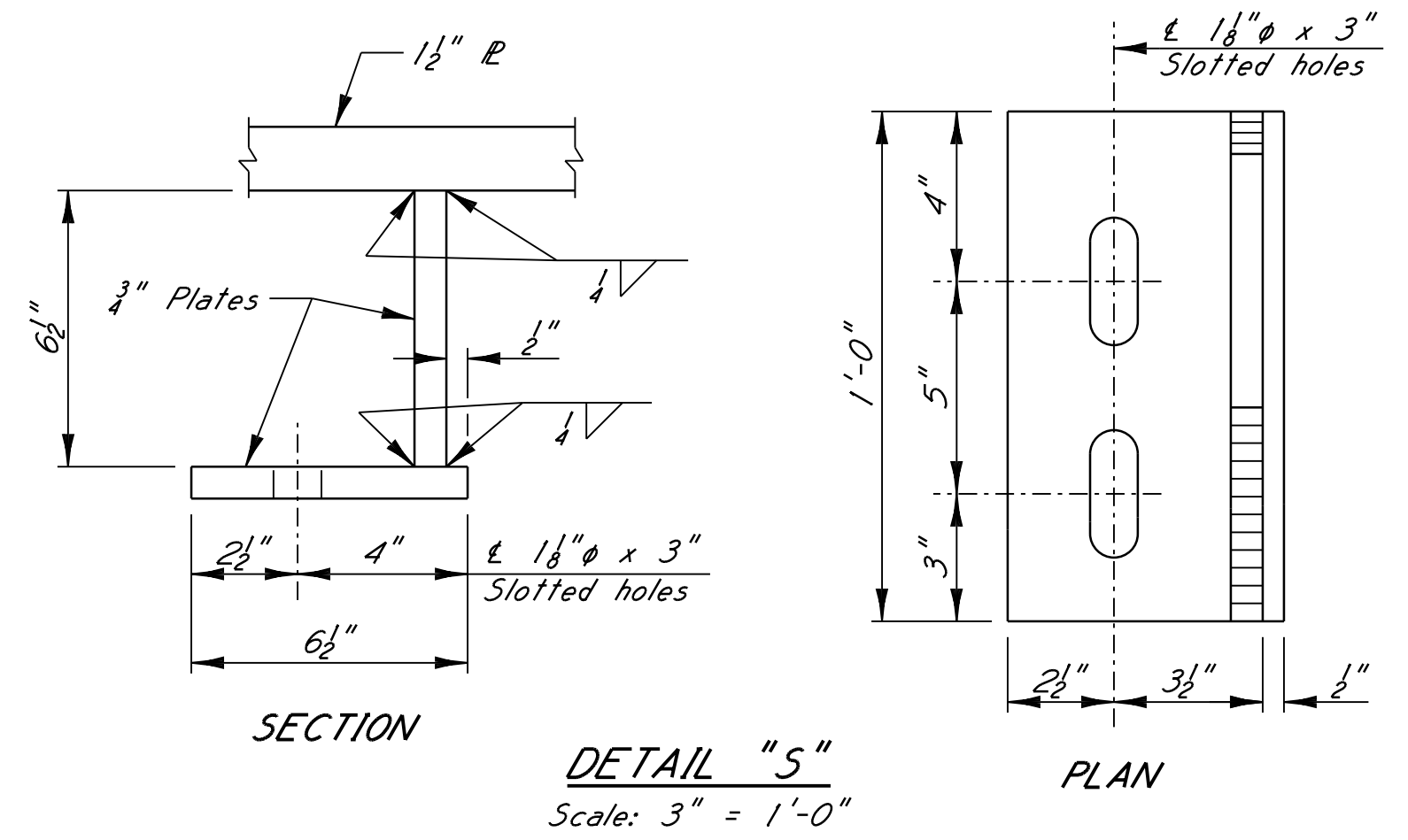
STRUCTURAL STEEL NOTES:
Special attention is called to Section 810 of the Mississippi Standard Specifications concerning shop drawings, assembly and erection of steel structures.
Structural steel plates shall conform to A.S.T.M. Designation A709, Grade 50. All structural steel shall be new.
All welding shall be done by the electric arc process and shall conform to the ANSI/AASHTO/AWS D1.5 BRIDGE WELDING CODE.
Certification for all welders to be used on this project shall be submitted to the Director of Structures, State Bridge Engineer through the Shop Inspector. Prior to the fabrication, the Fabricator shall have shop drawings, welding procedures and a procedure for storage and handling of welding electrodes, wire and flux which have been approved by the Director of Structures, State Bridge Engineer.
Structural steel shall be hot dip galvanized in accordance with the requirements of ASTM A123, Class 50 coating or galvanized by the mechanical process in accordance with the requirements of ASTM B695, Class 50 coating.

SPECIAL NOTES ON BOLTS, NUTS & WASHERS:
High strength bolts shall meet the requirements of ASTM F3125, Gr. A325, Type 1, and shall be hot dip galvanized in accordance with the requirements of ASTM A153, Class C coating, or galvanized by the mechanical process in accordance with the requirements of ASTM B695, Class 50 coating maximum hardness for high strength bolts shall be 33 Rockwell C (Rc). Nuts for high strength bolts shall be heavy hex and meet the requirements of ASTM A563, Grade DH galvanized. Hardened steel washers shall meet the requirements of ASTM F436, galvanized.
High strength bolts and nuts shall not be reused after tightening. Mill test reports, certified test reports, and certificates of compliance are required for high strength bolts, nuts, and hardened washers.

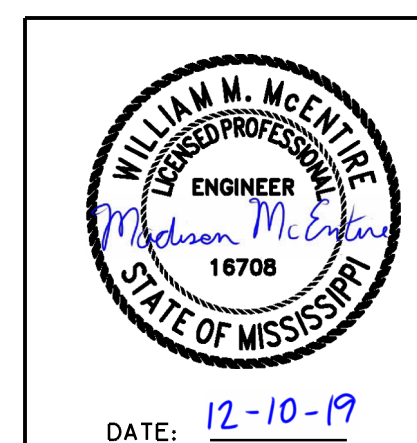


SECTION M-M
Scale: 3" = 1'-0"

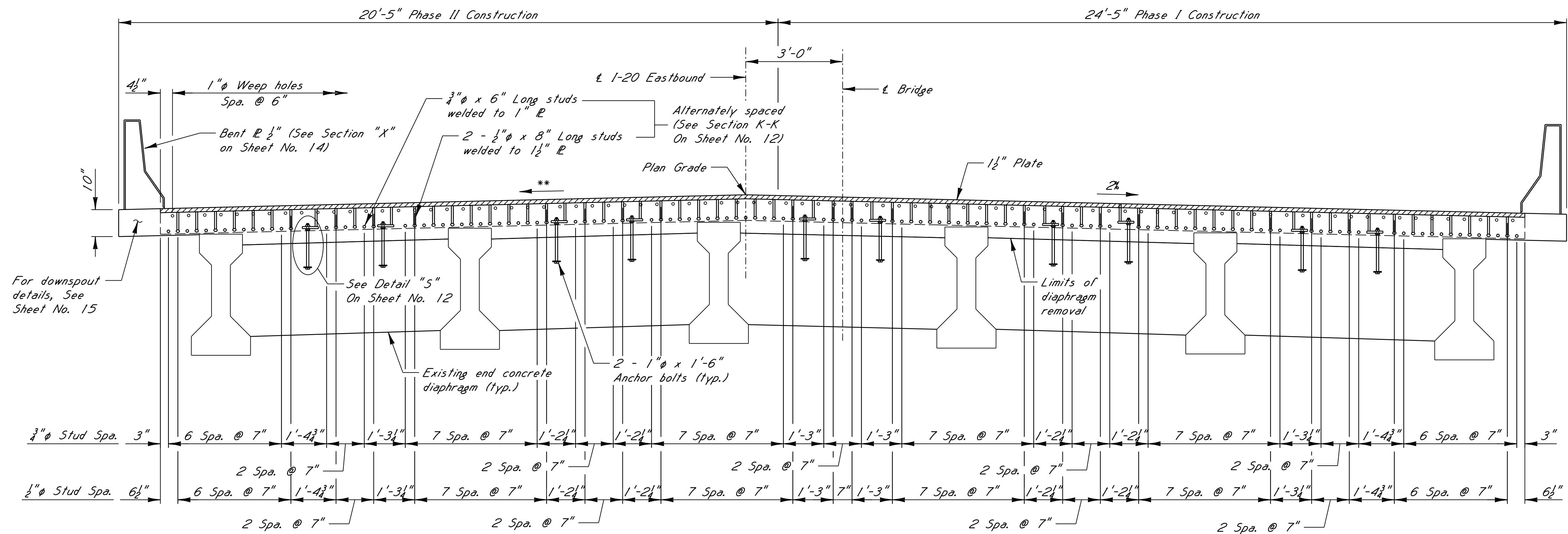
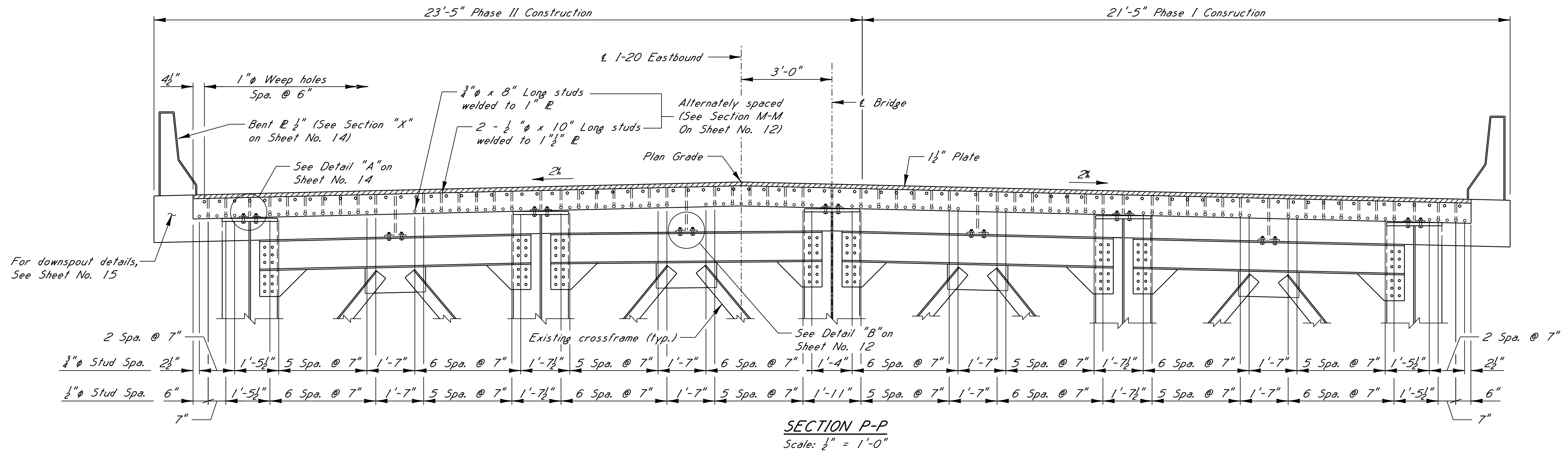
***** Epoxy grout threaded rods 9" into existing end diaphragm. See "ANCHOR NOTE" on Sheet No. 9.**



SECTION DETAIL "5"
Scale: 3" = 1'-0"



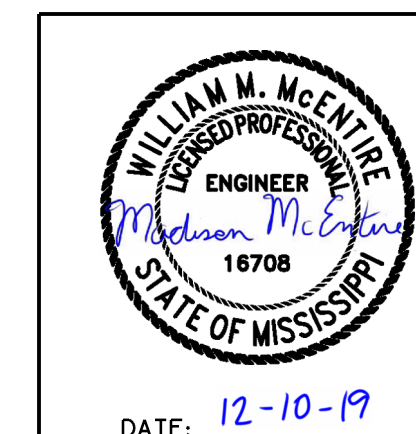
DESIGNED BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
DRAWN BY		BRIDGE AT STA. 977+52.88	
CHECKED BY		FINGER JOINT REPAIR	
DATE		PROJECT BR-0020-01(248)	
ISSUE DATE		107878-301000	
REVISIONS		HINDS COUNTY	
DESIGNER		WORKING NUMBER	
DETAILER		12 of 15	
CHECKER		SHEET NUMBER	
ISSUE DATE		8013	
DIRECTOR OF STRUCTURES, STATE BRIDGE ENGINEER - JUSTIN WALKER PE.		DEP. DIR. OF STRUCTURES, ASSIST. STATE BRIDGE ENGINEER - SCOTT WESTERFIELD PE.	



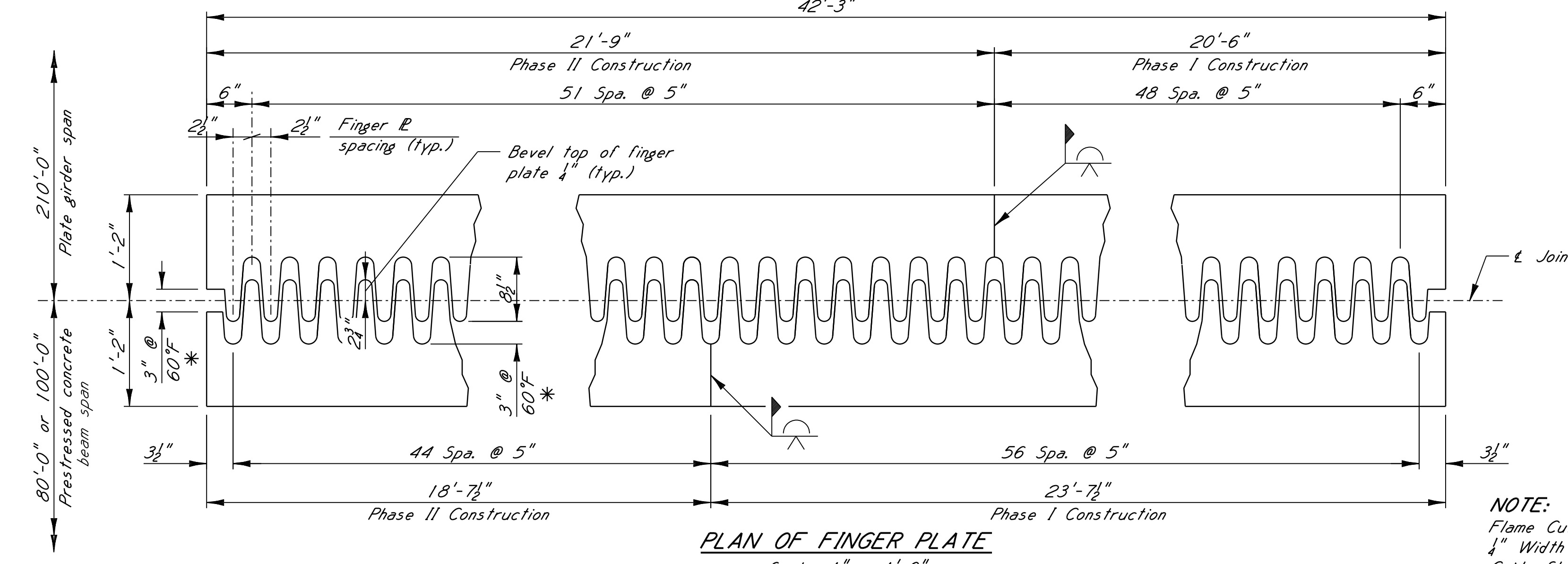
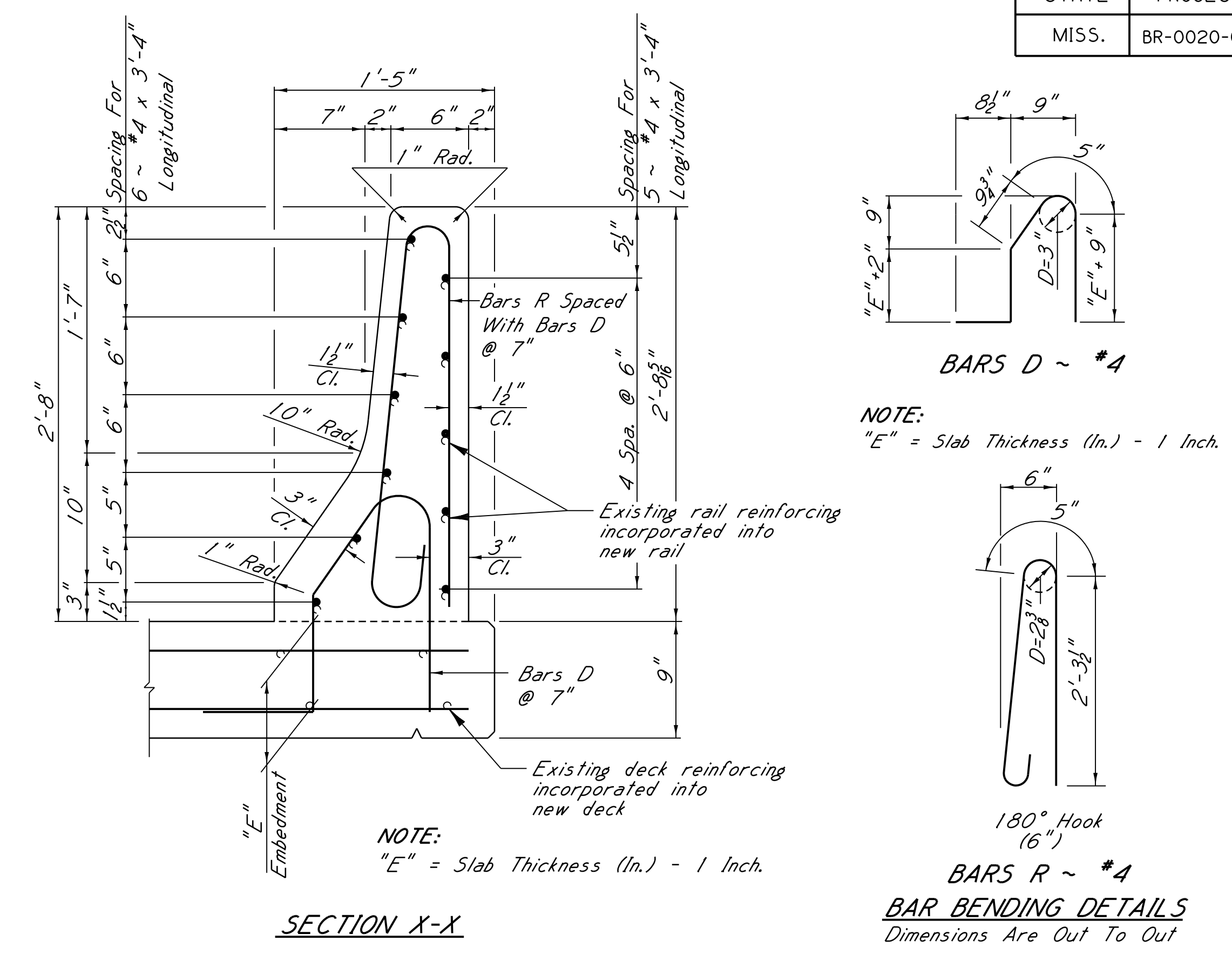
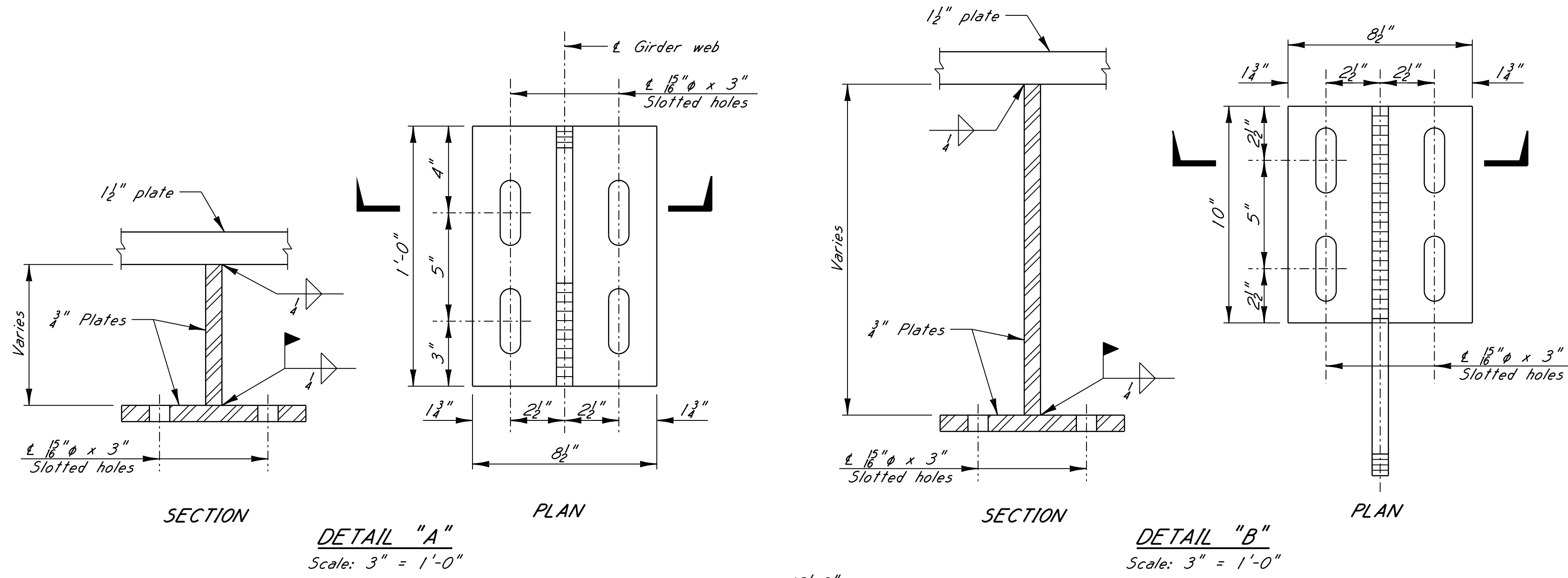
** Span 2 - Varies from 0% at Sta. 978+34.00 to 2% at Sta. 979+14.00
Span 6 - 2%

NOTE
The Contractor is required to survey the existing bridge deck to ensure the fabricator has accurate information with which to fabricate the finger joint for proper fit. This survey information shall be submitted to MDOT for review prior to finger joint fabrication.

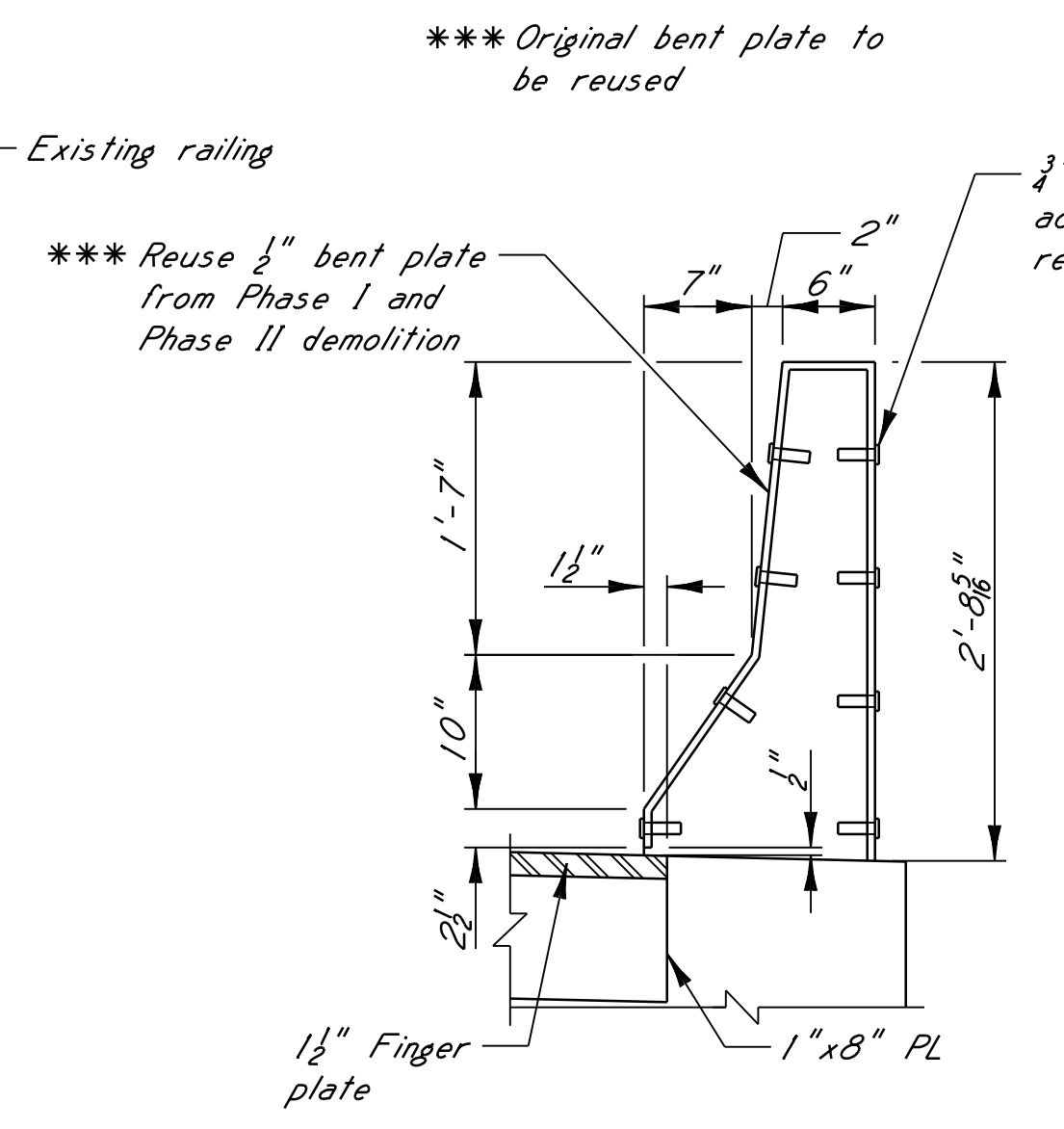
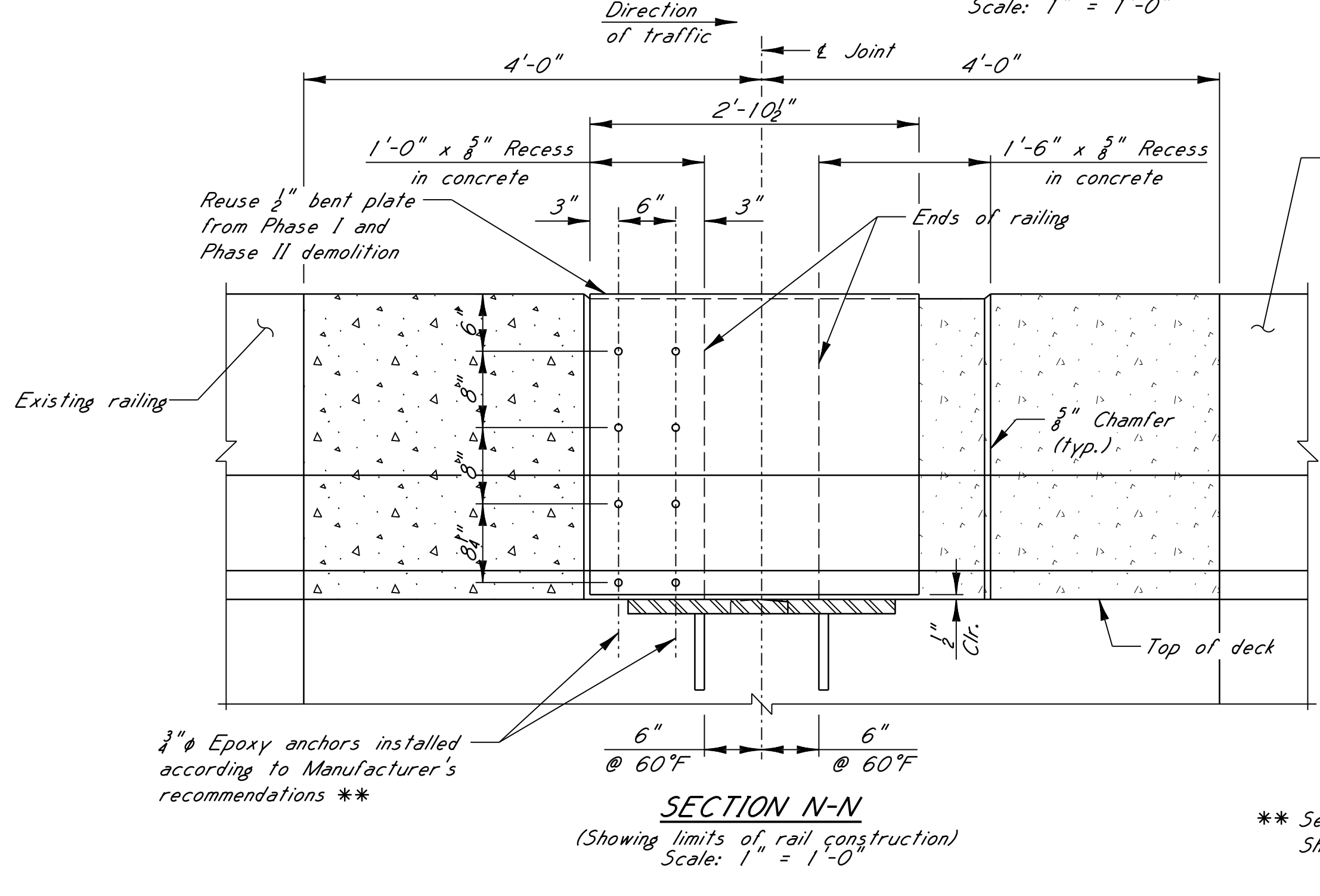
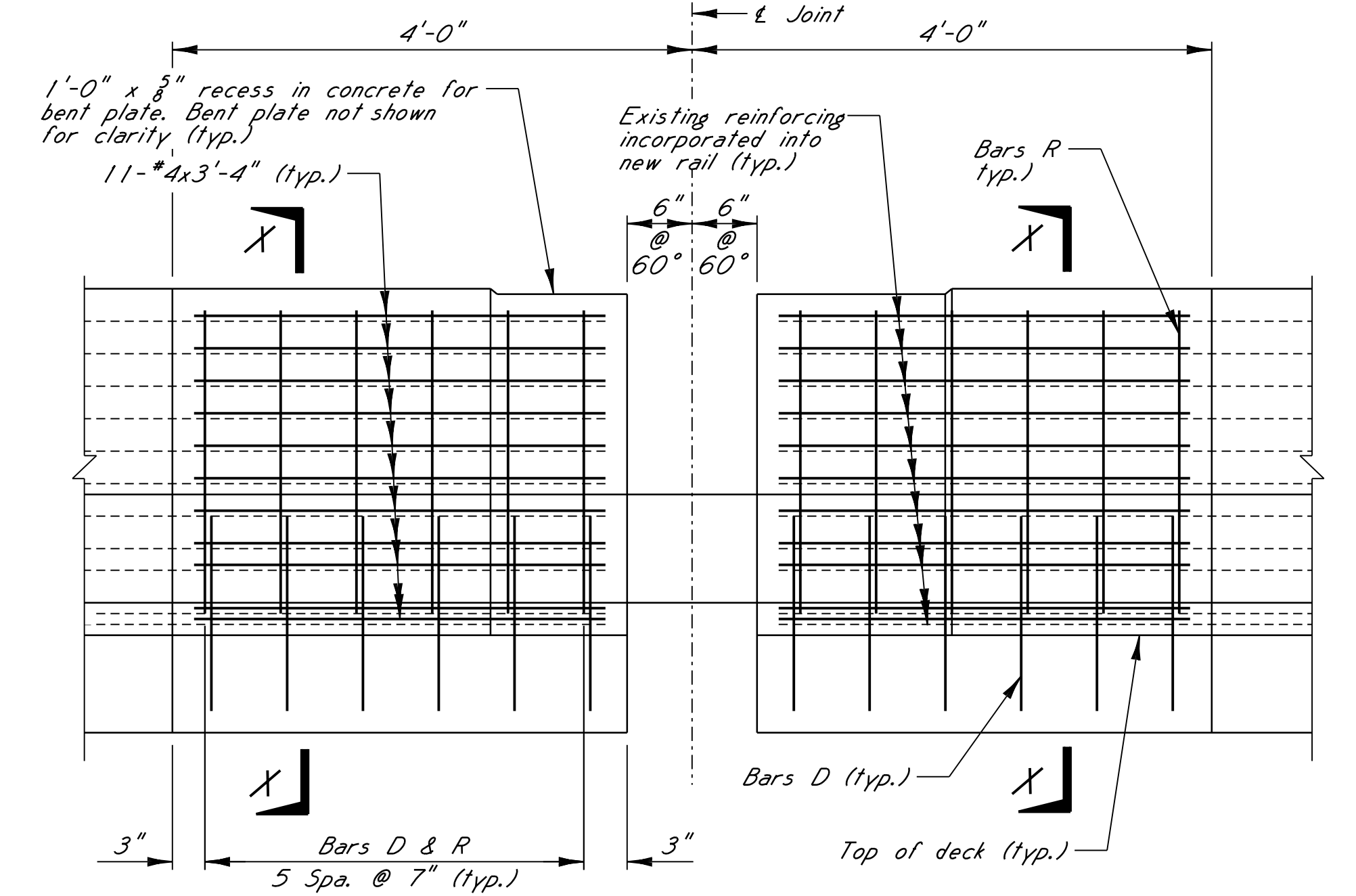
NOTE:
The location of studs shall be adjusted as necessary to avoid conflicts with other finger joint components.



BY MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 977+52.88	
FINGER JOINT REPAIR	
PROJECT BR-0020-01(248) 107878-301000	WORKING NUMBER 13 of 15
HINDS COUNTY	SHEET NUMBER 8014
DESIGNER: Madison McEntire CHECKER: Amjad Elkhairi	ISSUE DATE
DIRECTOR OF STRUCTURES, STATE BRIDGE ENGINEER - JUSTIN WALKER PE. DEP. DIR. OF STRUCTURES, ASSIST. STATE BRIDGE ENGINEER - SCOTT WESTERFIELD PE.	

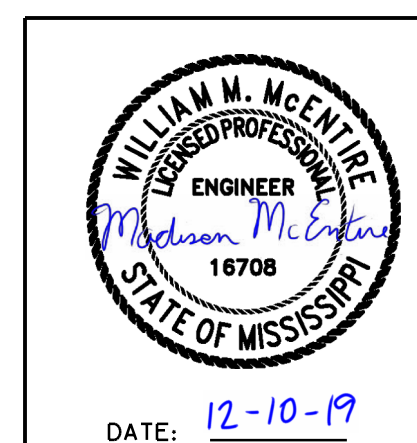


NOTE:
Flame Cutting Of The Finger Plate Shall Allow A 1/4" Width Between Edges Of Teeth. A Mechanical Guide Shall Be Used To Guide The Cutting Torch. All Burrs Shall Be Ground Smooth.

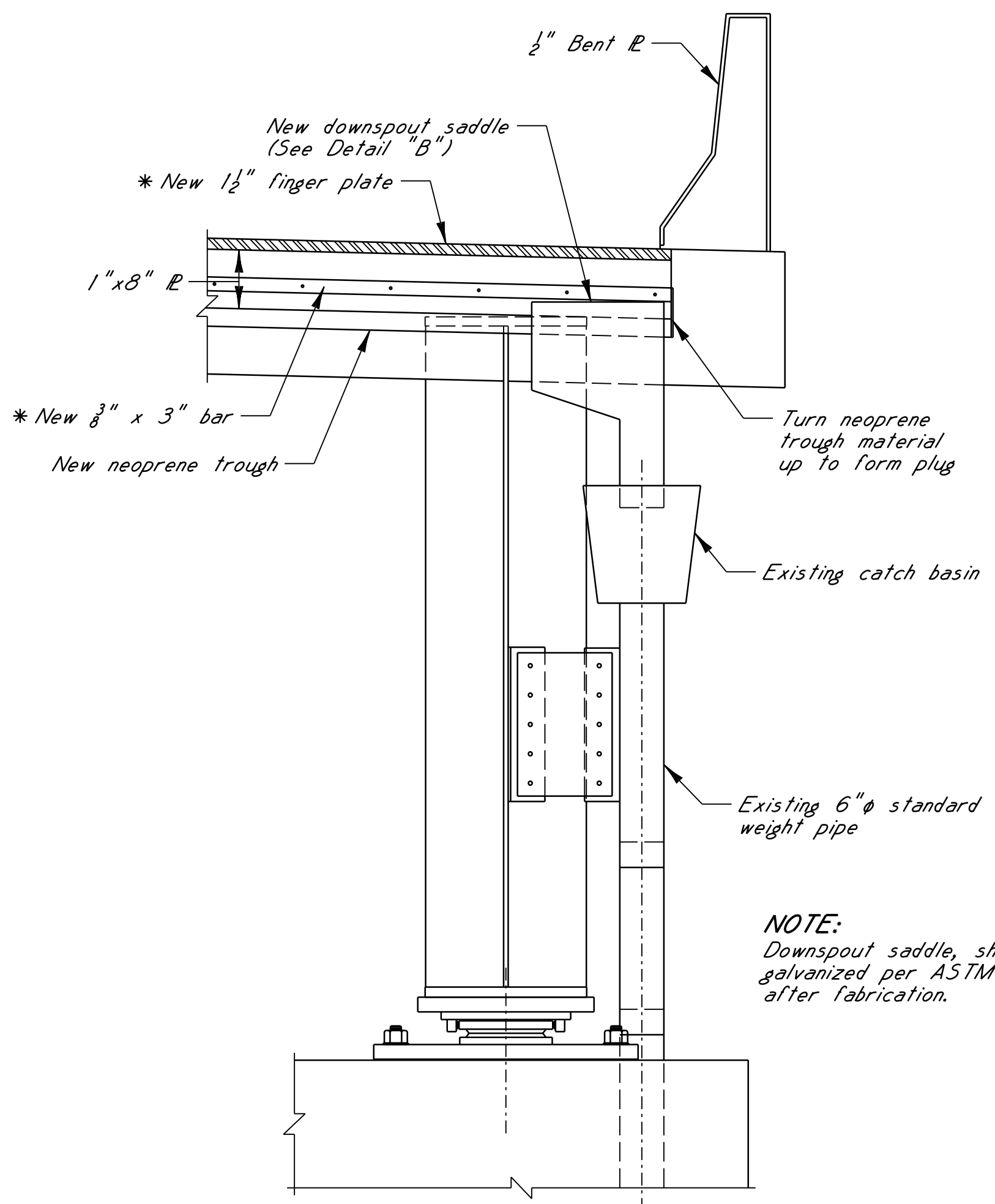


TEMP. °F	JOINT OPENING
20°F	4 3/8"
30°F	4"
40°F	3 7/8"
50°F	3 5/8"
60°F	3"
70°F	2 7/8"
80°F	2 5/8"
90°F	2"
100°F	1 5/8"

* NOTE:
If Temperature Is Different From 60°F At Time Of Erection Of Finger Joint, This Dimension Shall Be As Shown In The Erection Dimensions Table.

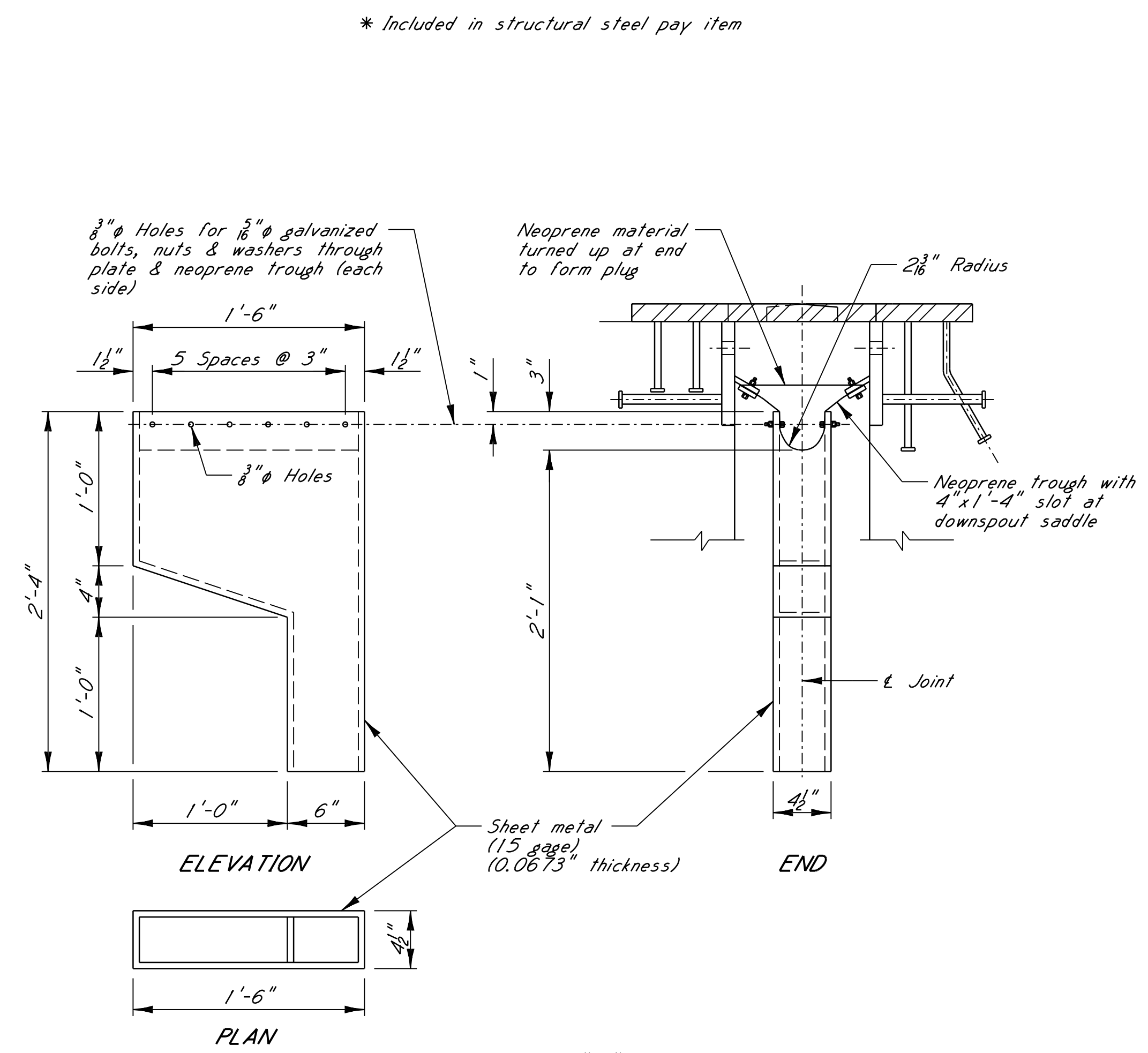


BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 977+52.88	
PROJECT		BR-0020-01(248) 107878-301000	
HINDS COUNTY		FINGER JOINT REPAIR	
DESIGNER		CHECKER	
DETAILER		ISSUE DATE	
DATE		12-10-19	
WORKING NUMBER		14 of 15	
SHEET NUMBER		8015	

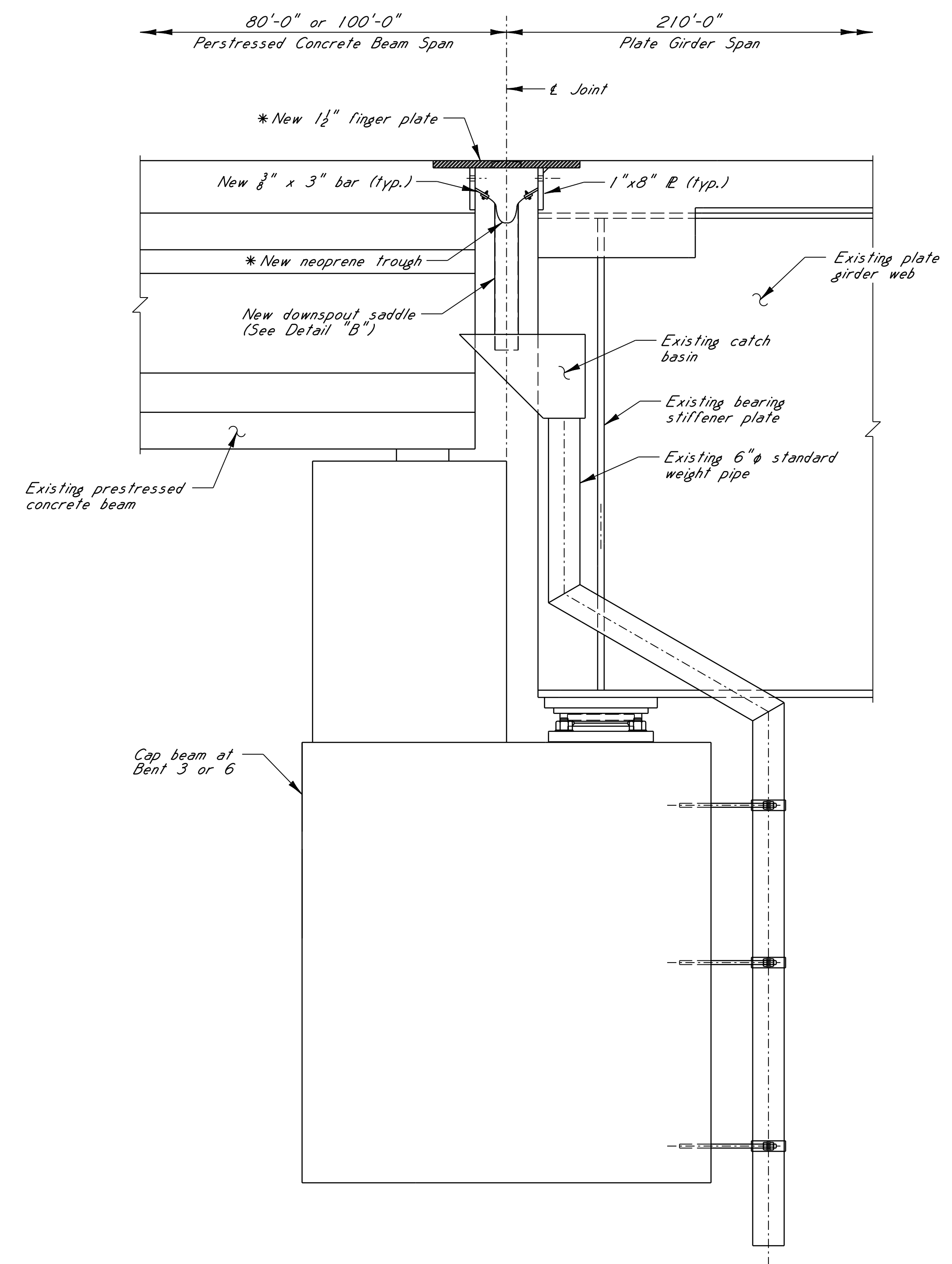


DRAIN DETAIL - ELEVATION
Scale: 3/4" = 1'-0"

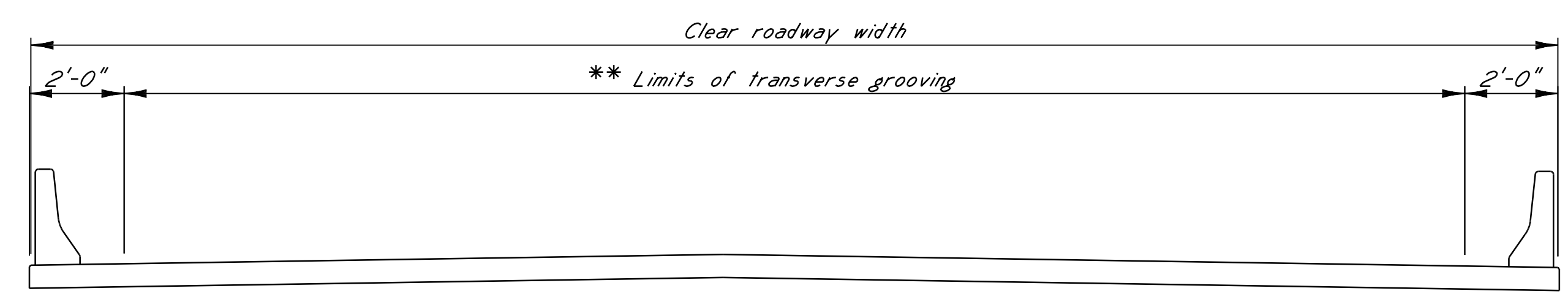
NOTE:
Downspout saddle, shall be galvanized per ASTM A123 after fabrication.



DETAIL "B"
Scale: 1/2" = 1'-0"



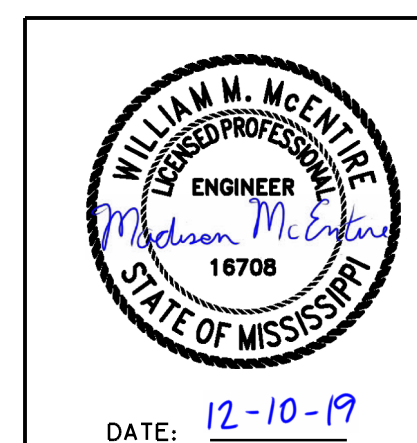
DRAIN DETAIL - END ELEVATION
Scale: 3/4" = 1'-0"



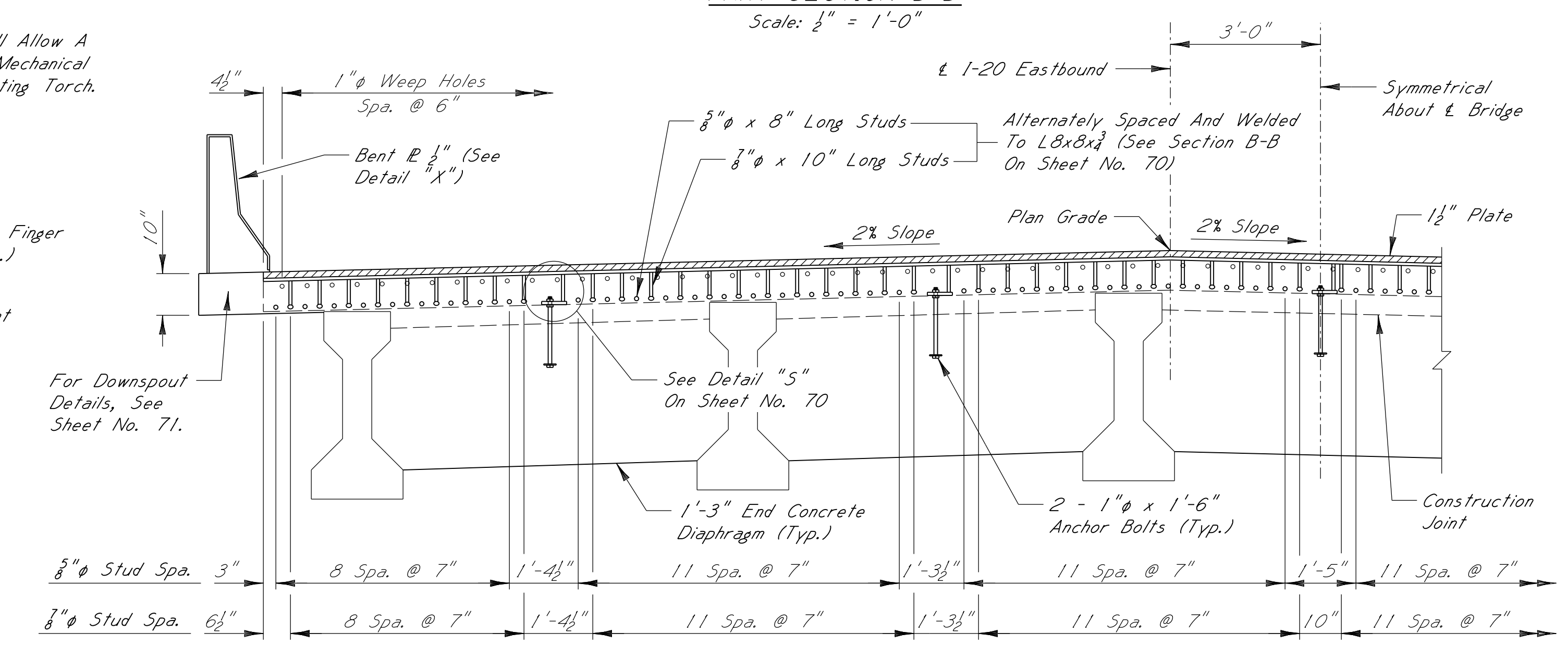
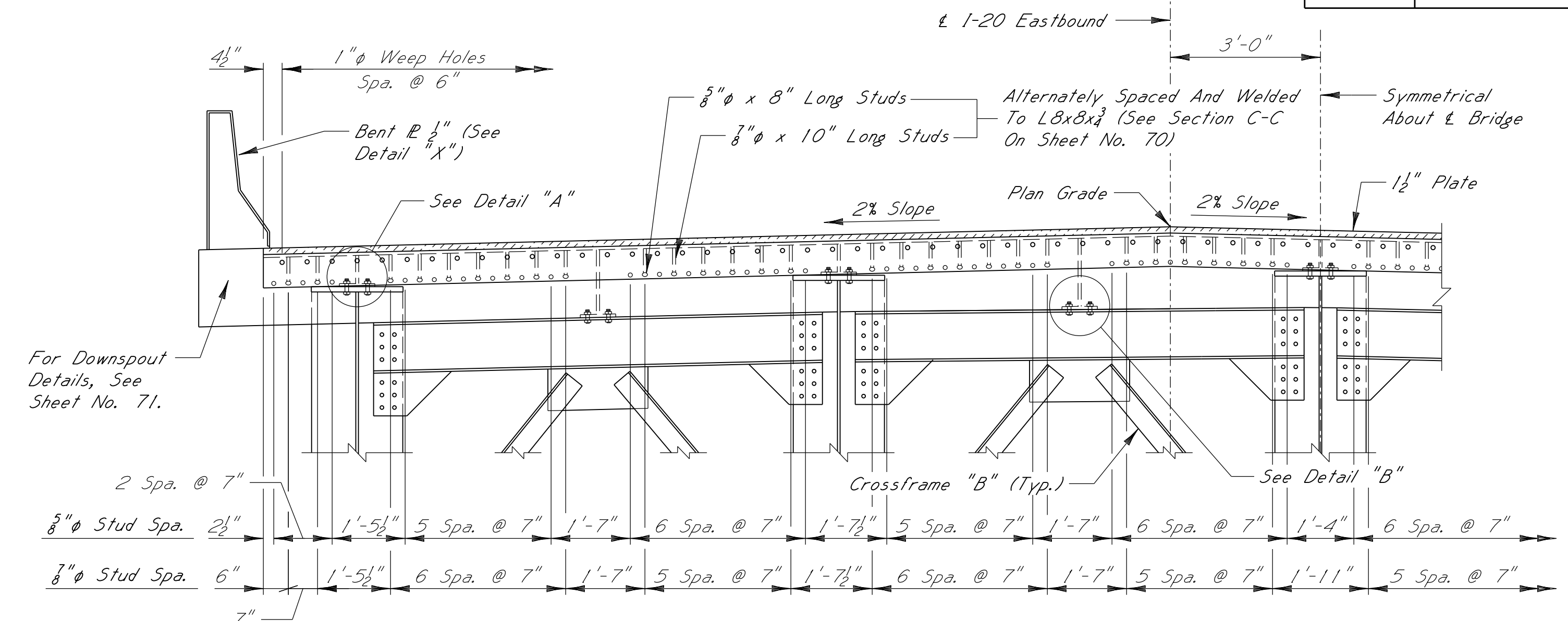
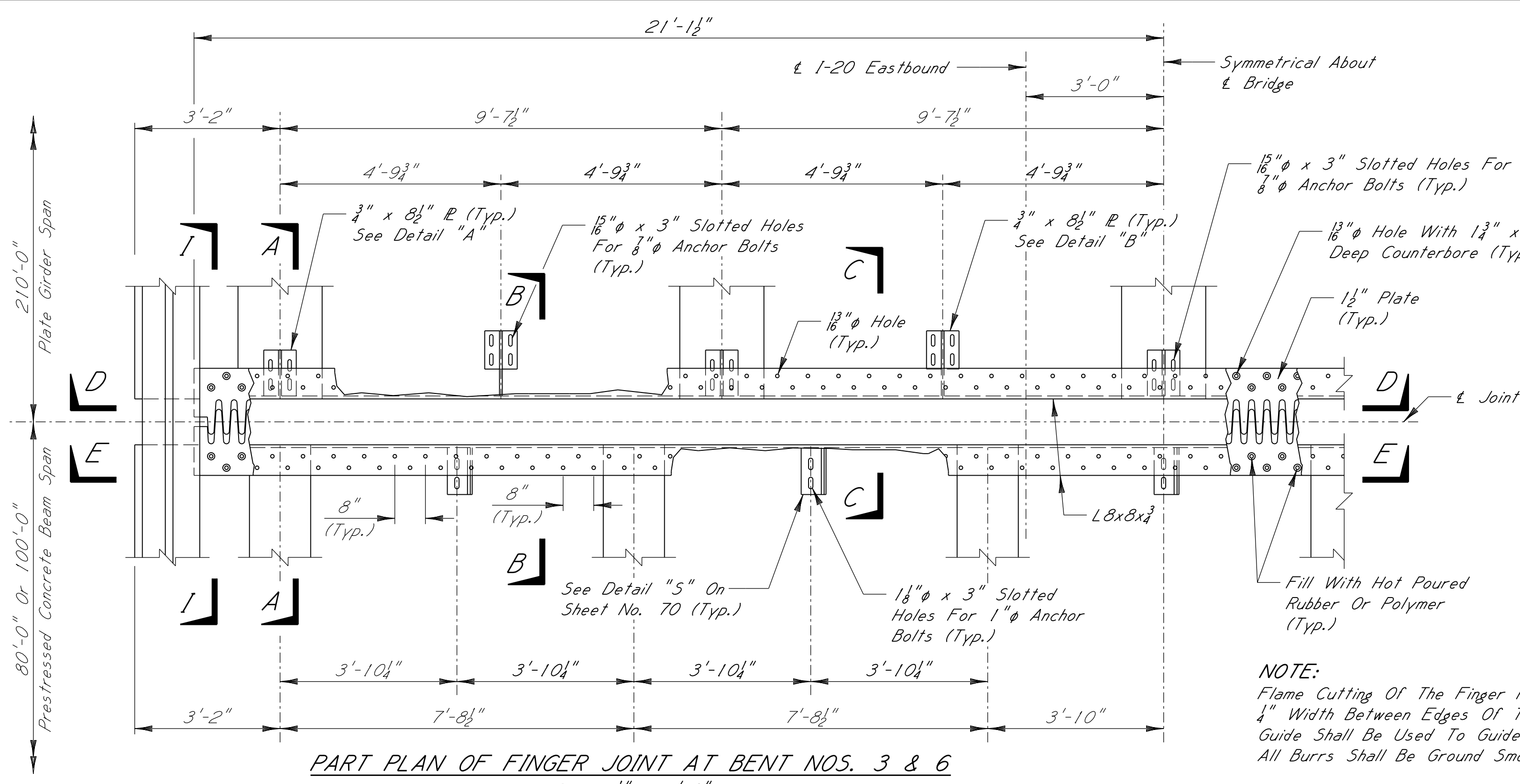
LIMITS OF TRANSVERSE GROOVING

** The Contractor shall groove all new concrete 4 ft. each side of the new finger joint at Bents 3 & 6

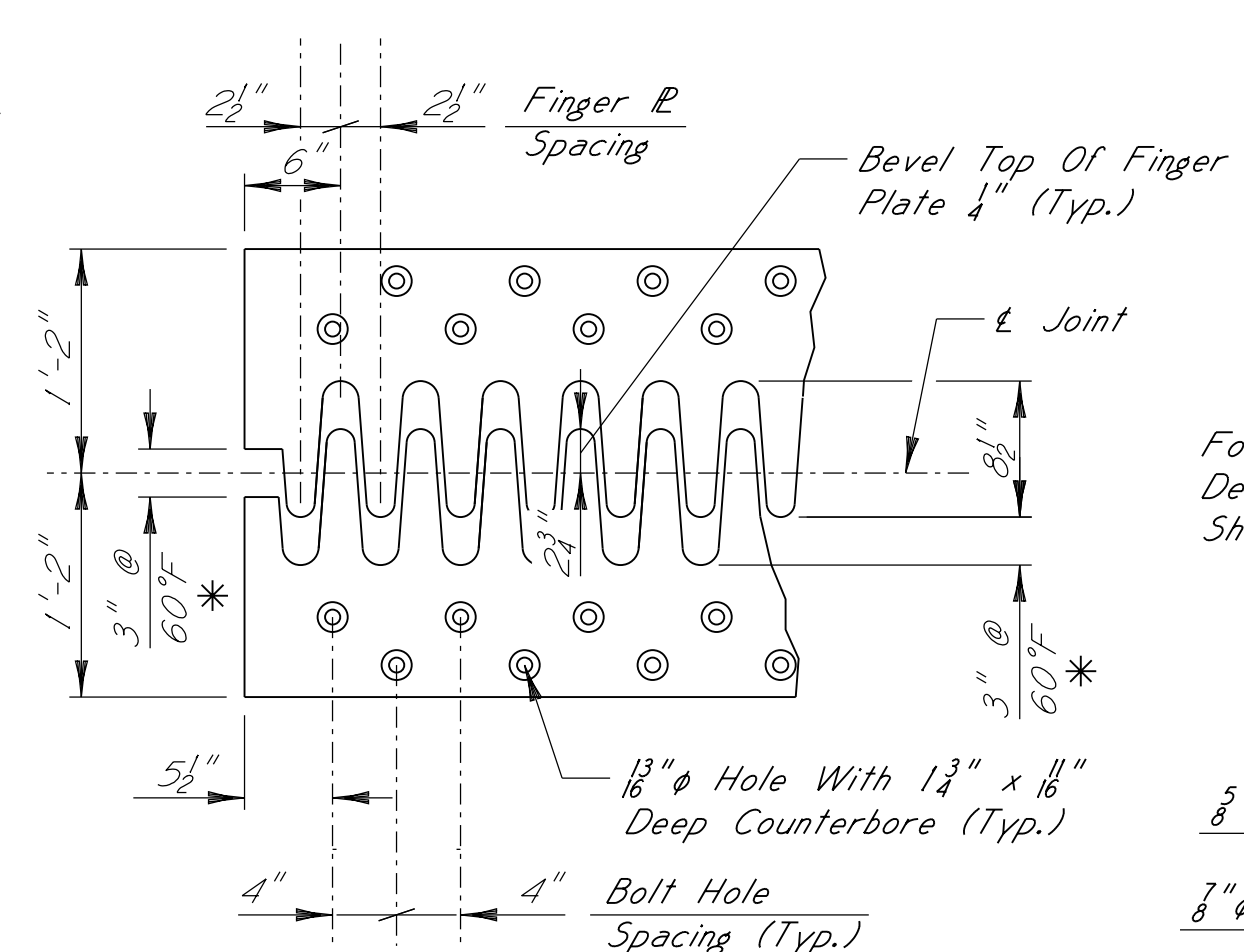
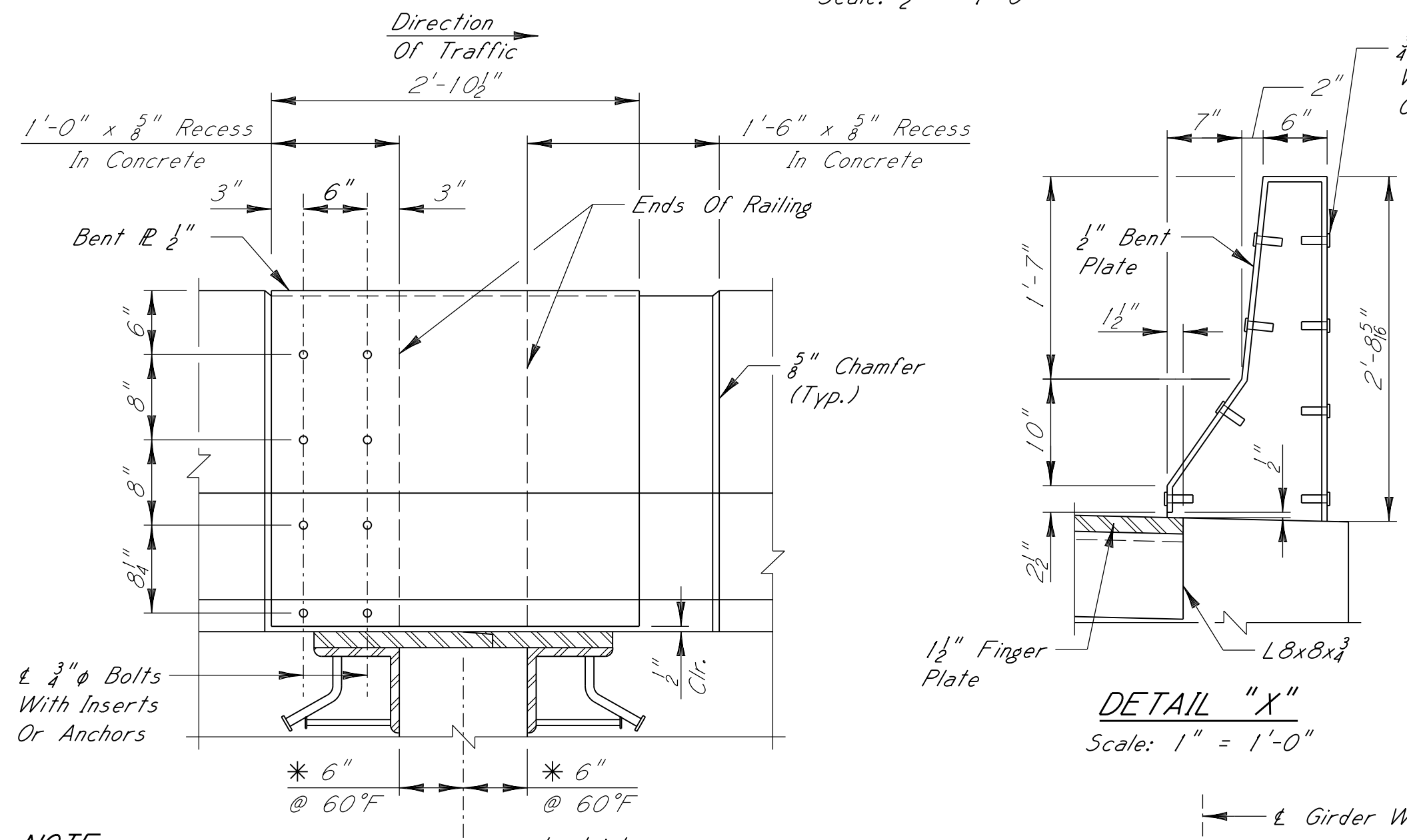
* Included in structural steel pay item



BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 977+52.88	
REVISIONS		FINGER JOINT REPAIR	
PROJECT		BR-0020-01(248) 107878-301000	
HINDS COUNTY		WORKING NUMBER 15 of 15	
DESIGNER	Medison McEntire	CHECKER	Amjad Elkhairi
DATE	12-10-19	ISSUE DATE	
DIRECTOR OF STRUCTURES, STATE BRIDGE ENGINEER - JUSTIN WALKER PE.		SHEET NUMBER 8016	
DEP. DIR. OF STRUCTURES, ASSIST. STATE BRIDGE ENGINEER - SCOTT WESTERFIELD PE.			

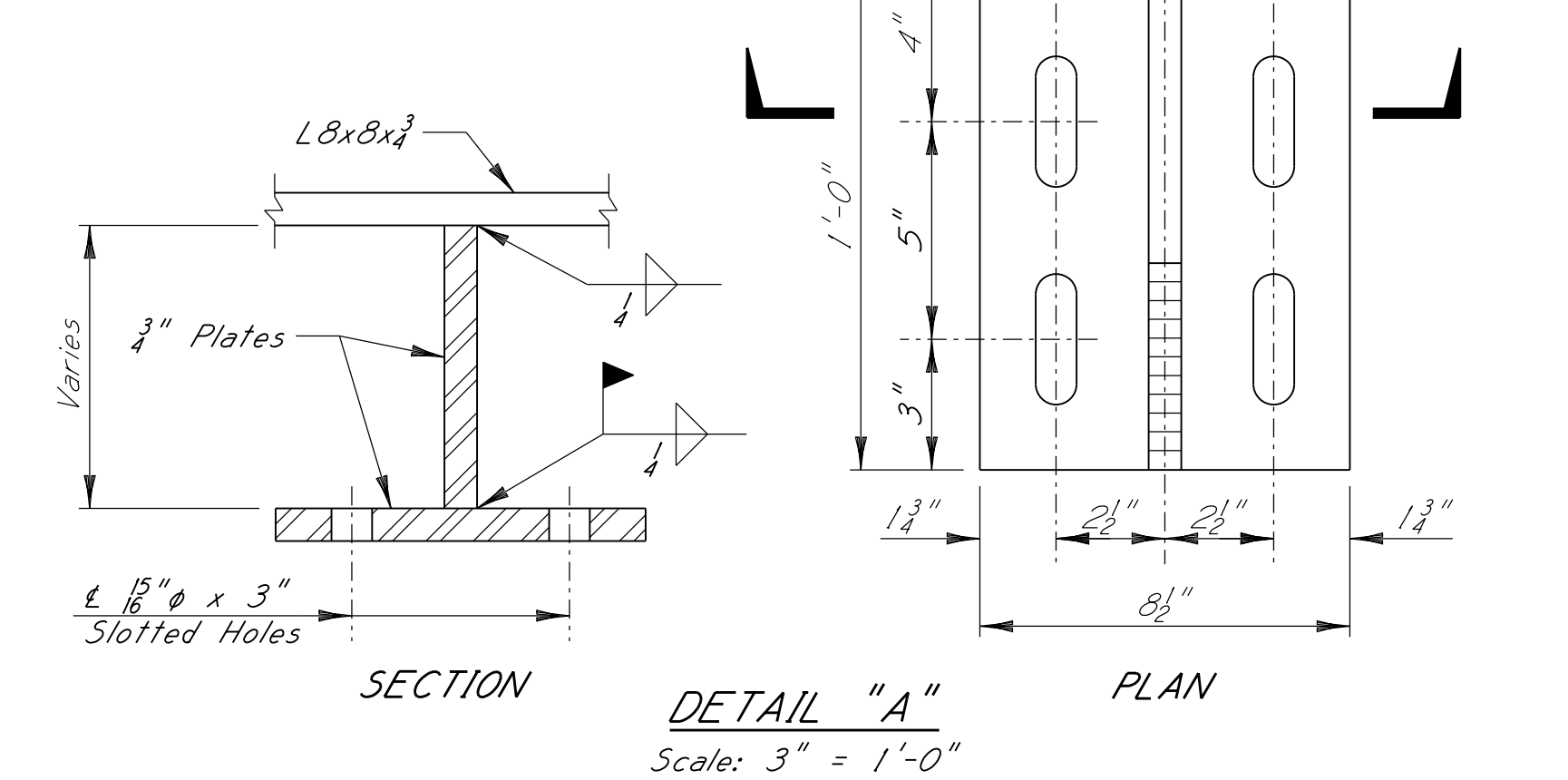


NOTE:
Flame Cutting Of The Finger Plate Shall Allow A 1/4" Width Between Edges Of Teeth. A Mechanical Guide Shall Be Used To Guide The Cutting Torch. All Burrs Shall Be Ground Smooth.

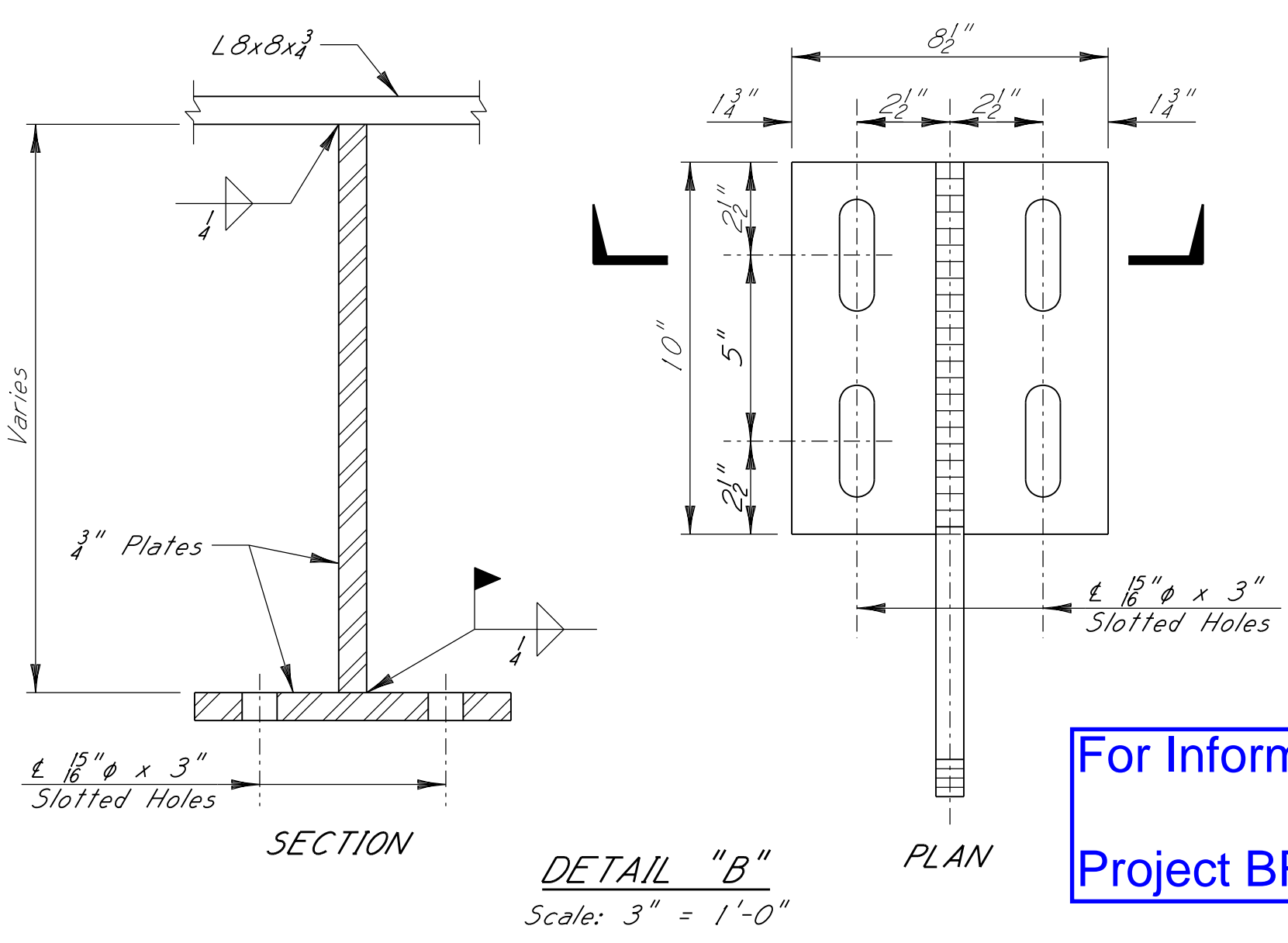


NOTE:
The 1/2" Bent @ Rail Shall Be ASTM A709, Grade 50 And Shall Be Galvanized Per ASTM A123 After Fabrication.

SECTION I-I
Scale: 1" = 1'-0"



PARTIAL PLAN OF FINGER PLATE
Scale: 1" = 1'-0"



TEMP. °F	JOINT OPENING
20°F	4 3/8"
30°F	4"
40°F	3 11/16"
50°F	3 5/8"
60°F	3"
70°F	2 11/16"
80°F	2 5/8"
90°F	2"
100°F	1 5/8"

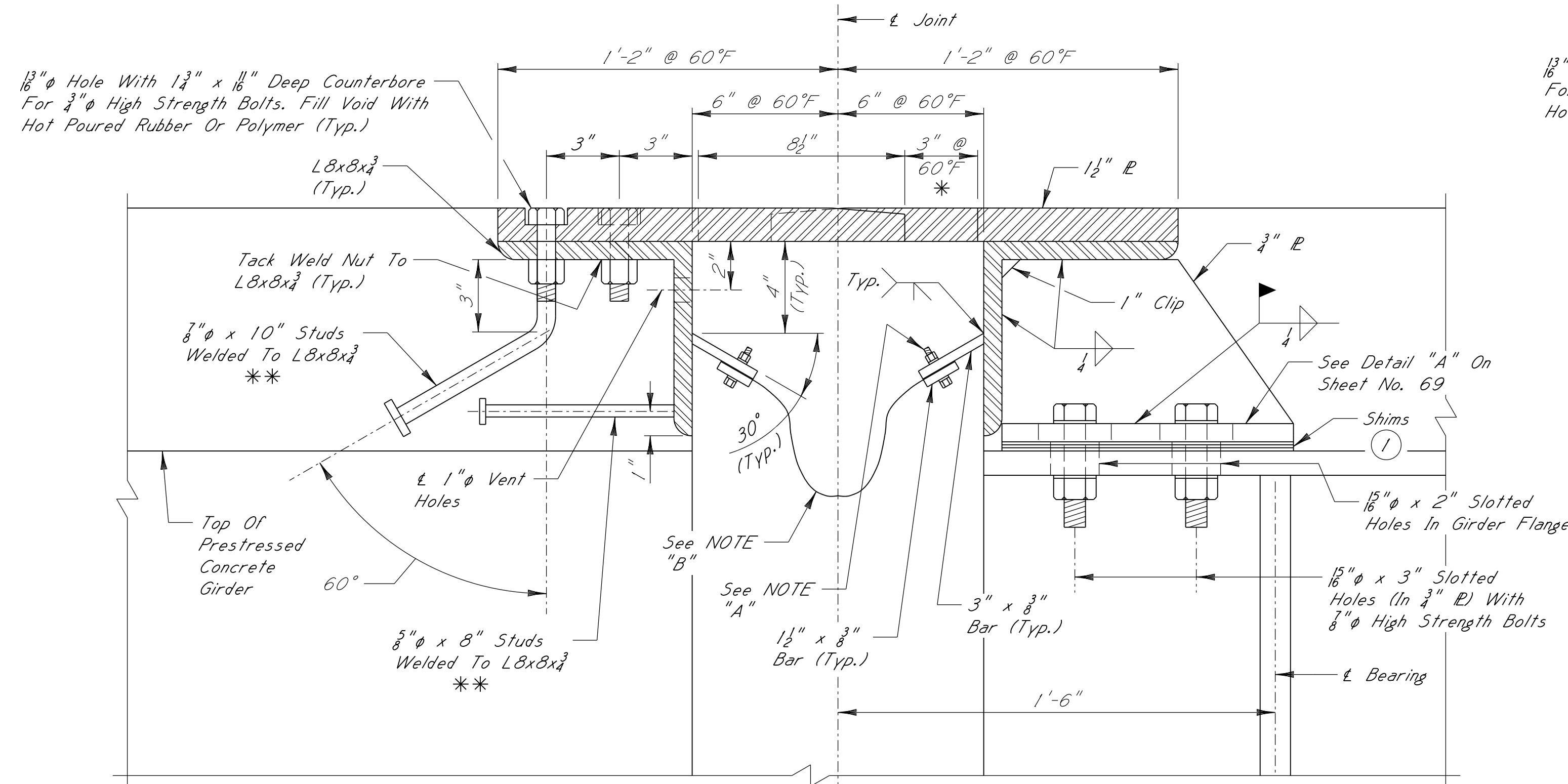
* **NOTE:**
If Temperature Is Different From 60°F At Time Of Erection Of Finger Joint, This Dimension Shall Be As Shown In The Erection Dimensions Table.

NOTE:
The Location Of Studs Shall Be Adjusted As Necessary To Avoid Conflicts With Other Finger Joint Components.

NOTE:
For Sections A-A, B-B & C-C And General Notes, See Sheet No. 70.

For Information Only
Project BR-0020-01(248)

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
DATE		BRIDGE AT STA. 977+52.88	
REVISIONS		FINGER EXPANSION JOINT DETAILS	
DESIGNED		PROJECT 105022/301000	
CHECKED		BR-0020-01(192)	
ISSUED		HINDS COUNTY	
DATE		WORKING NUMBER 69 of 76	
DATE		SHEET NUMBER 8072	

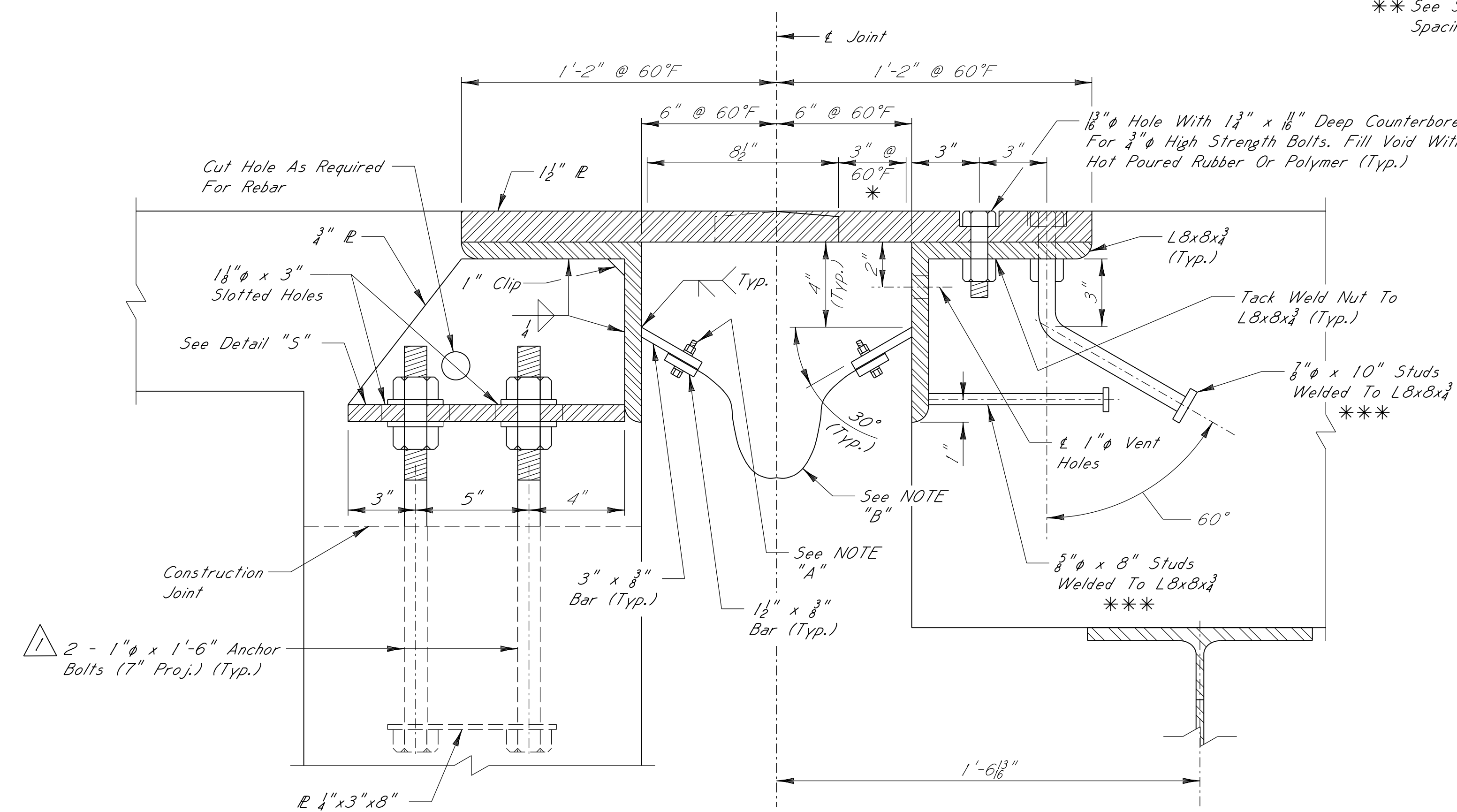


SECTION A-A
Scale: 3" = 1'-0"

* NOTE:
If Temperature Is Different From 60°F At Time Of Erection Of Finger Joint, This Dimension Shall Be As Shown In The Erection Dimensions Table On Sheet No. 69.

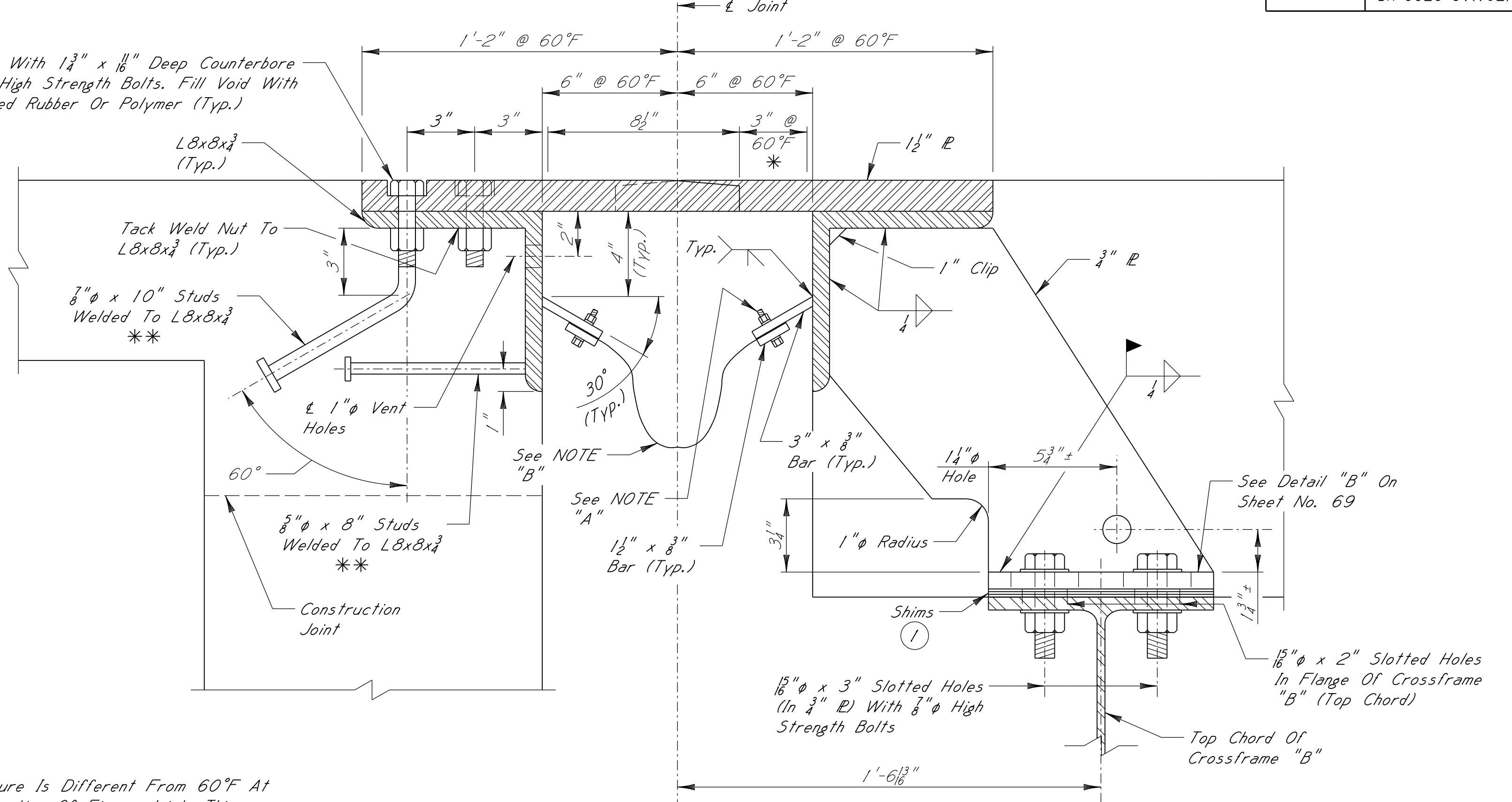
** See Section E-E On Sheet No. 69 For Spacing.

① A Shim Pack, Consisting Of 1 - 1/4" And 2 - 3/8" Plates, Shall Be Provided At Each Location Where Shims Are Required.



SECTION C-C
Scale: 3" = 1'-0"

*** See Section D-D On Sheet No. 69 For Spacing.



SECTION B-B
Scale: 3" = 1'-0"

For Information Only
Project BR-0020-01(248)

GENERAL NOTES:

All Steel In The Finger Expansion Joint And Finger Joint Drains Shall Be ASTM A709, Grade 50 Steel.
For Location Of Sections A-A, B-B & C-C, And Detail "A" & Detail "B", See Sheet No. 69.
All Structural Steel And Miscellaneous Steel Shall Be Galvanized Per ASTM A123 After Fabrication Unless Noted Otherwise.
All Bolts, Hardened Washers, Lock Washers And Nuts Shall Be Galvanized In Accordance With ASTM A153.
All Material For Finger Expansion Joints And Finger Joint Drains, Including Structural Steel, Miscellaneous Steel, Bolts, Washers, Studs, Downspouts, Neoprene Troughs And Supports, Shall Be Included In The Pay Item "Structural Steel, A709, Grade 50W".

FABRICATION NOTES:

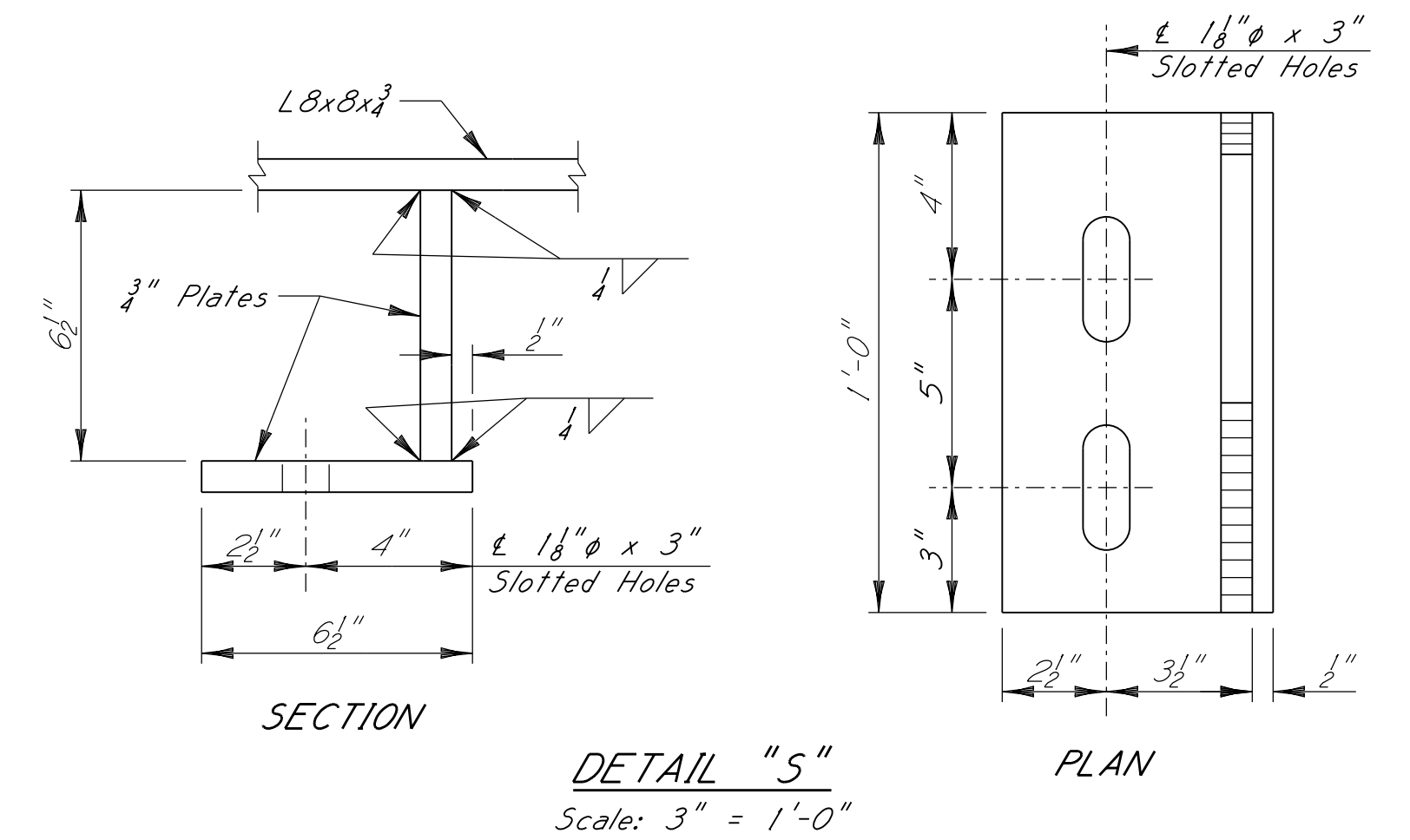
Any Bolt, Stud Or Vent Hole That Falls Directly On Or Within 2" Of An Intermediate Or Girder Support Brace May Be Eliminated On The Shop Drawings.
Finger Joints Will Be Shipped Fully Assembled.

NOTE "A":

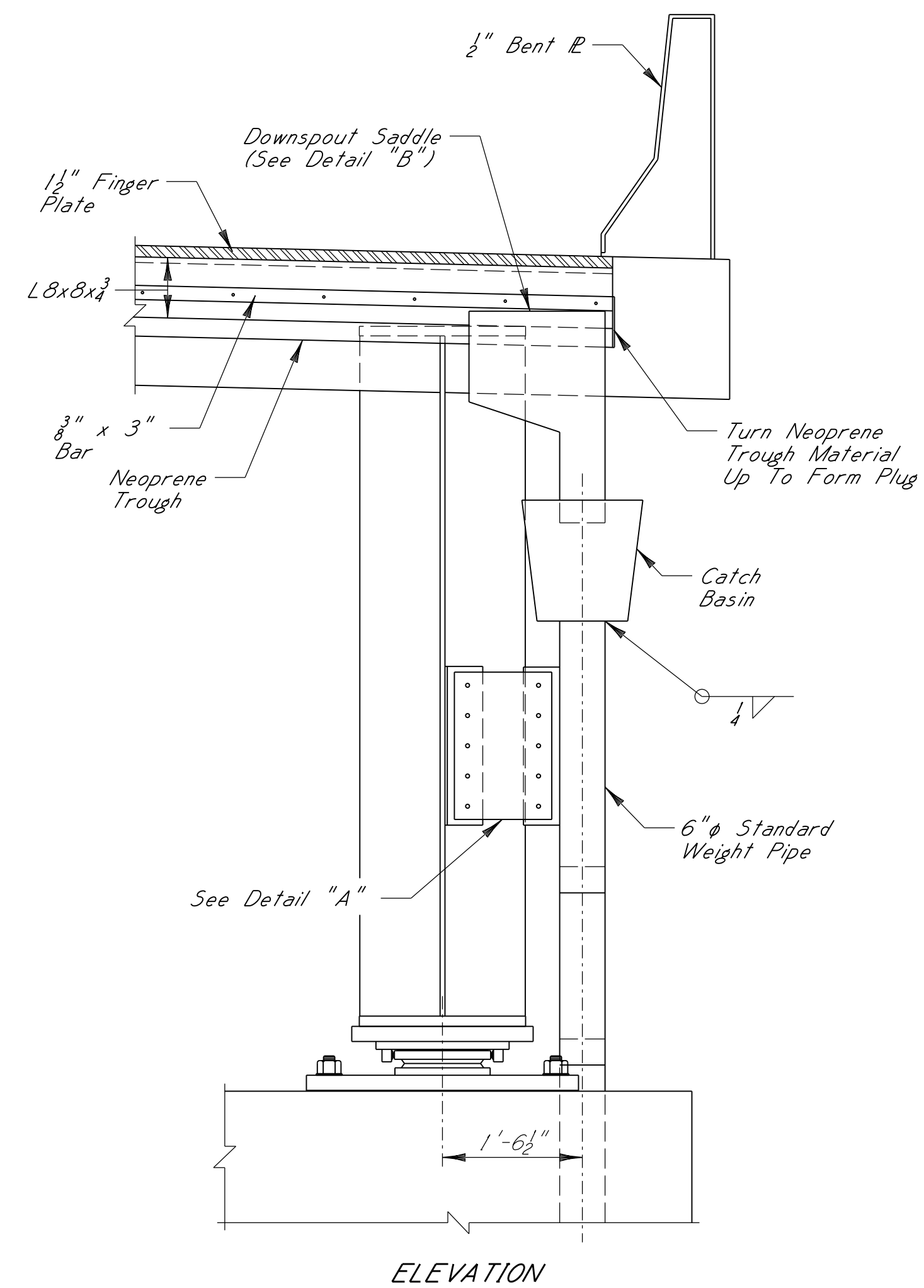
3/8" Galvanized Bolts At 12" O.C.; Tack Weld Nuts To 3/8" x 3" Bar.

NOTE "B":

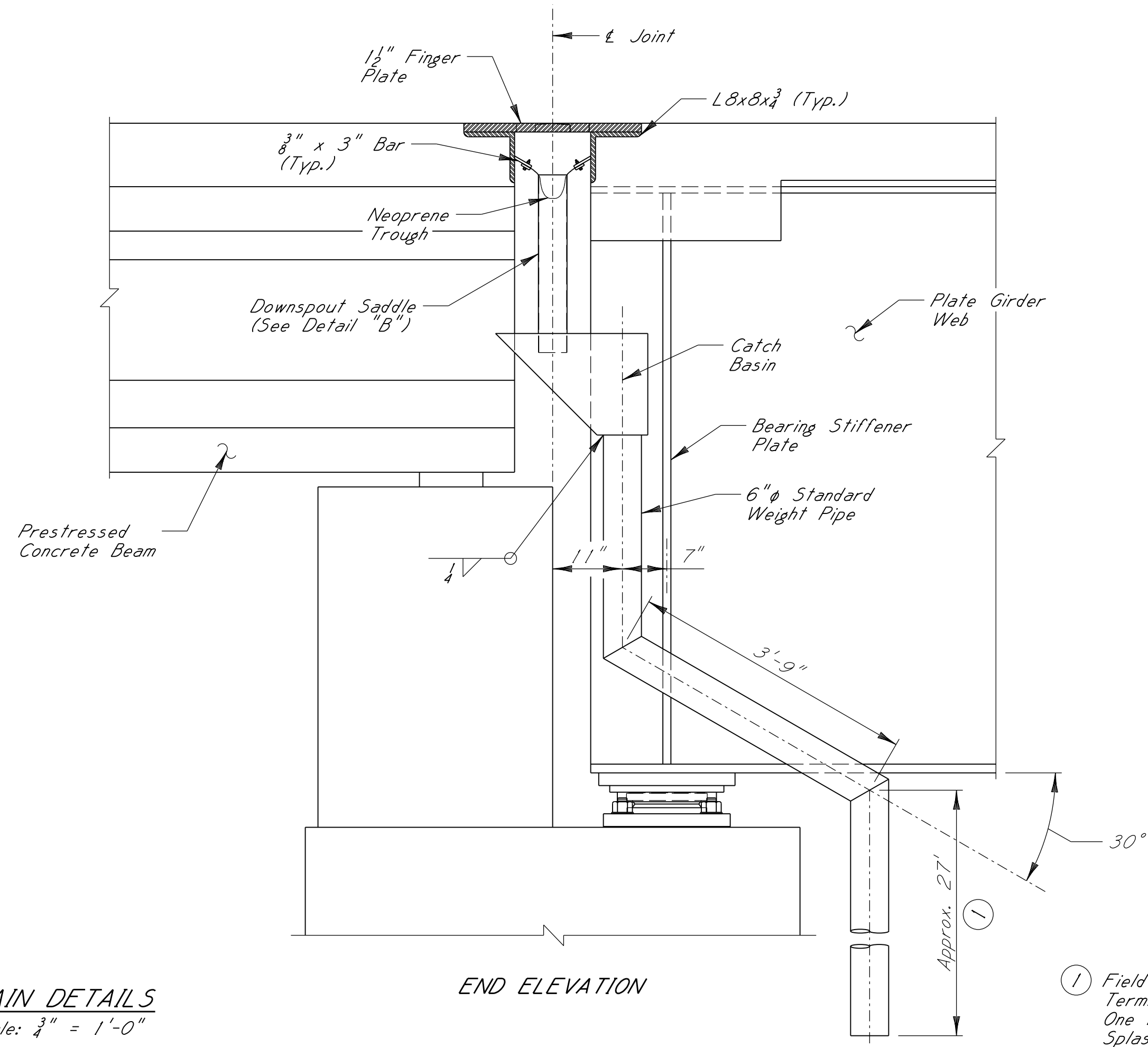
A 1 1/8" x 20" Neoprene Trough (Not A Pay Item) Is Required At All Finger Joint Locations. The Neoprene Material For Drain Trough Shall Be In Accordance With ASTM D-2000 "Line Call-Out" 2 BC615 A14 F17 And Is To Be Integrally Molded With Two Layers Of Fabric Reinforcement.
Fabric Shall Be Of Good Quality And Equal To Fabric Used By Manufacturers Of Sheet Rubber Products Designed For Use In Highway Bridge Expansion Joint Applications. Immediately After Placement Of Concrete In Blockout And It Has Been Vibrated Through Air Holes In Angles, The Neoprene Trough Shall Be Thoroughly Flushed Out To Remove All Concrete Debris.



6/15/15		DATE		DESIGNED: ABH		DETAILED: ABH		TRACED: ABH		WORKING NUMBER	
				CHECKED: JHR		ISSUED: NJA		DATE		70 of 76	
MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 977+52.88 FINGER EXPANSION JOINT DETAILS PROJECT 105022/301000 BR-0020-01(192) HINDS COUNTY SHEET NUMBER 8073											

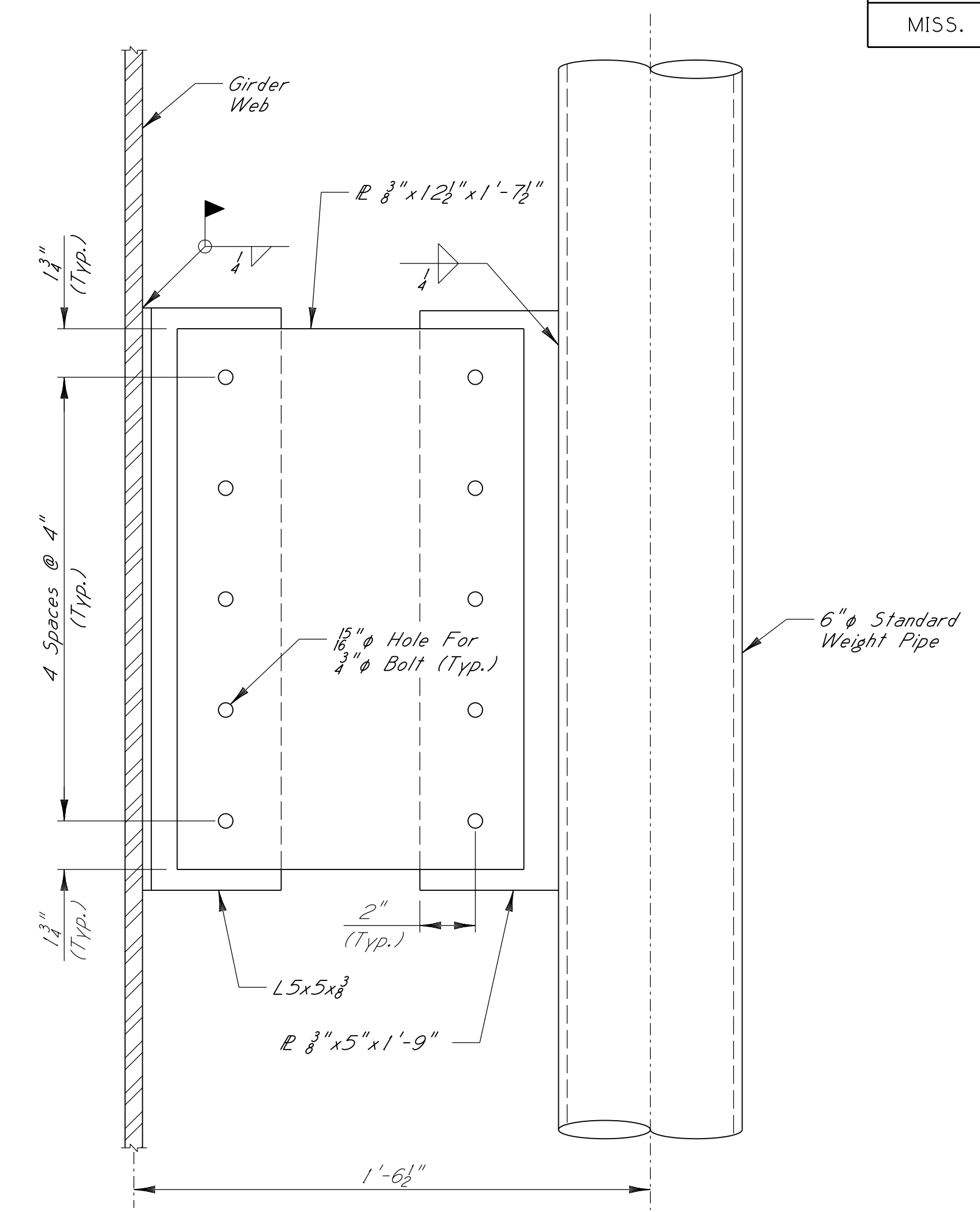


ELEVATION



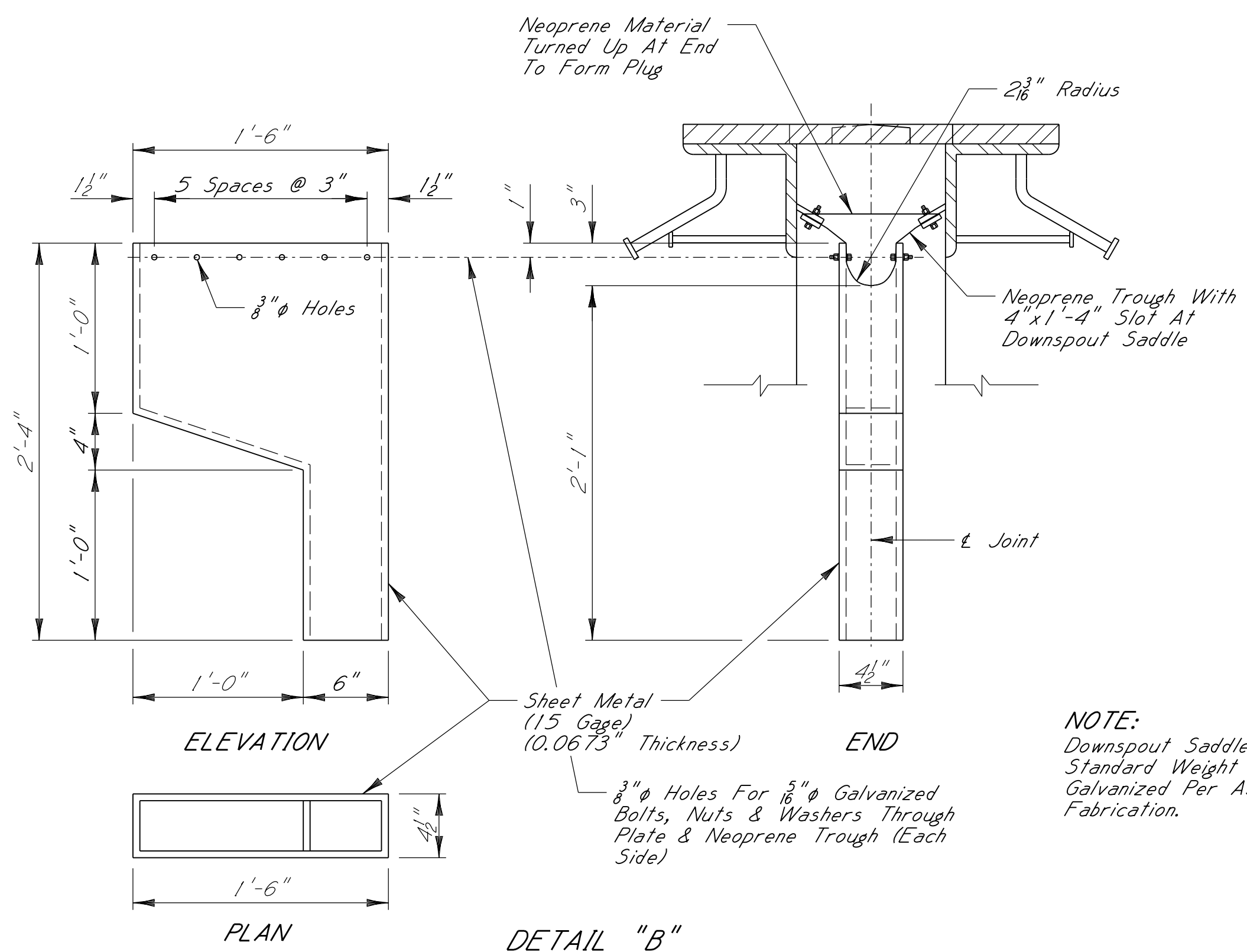
END ELEVATION

DRAIN DETAILS
Scale: 3/4" = 1'-0"



DETAIL "A"
Scale: 3" = 1'-0"

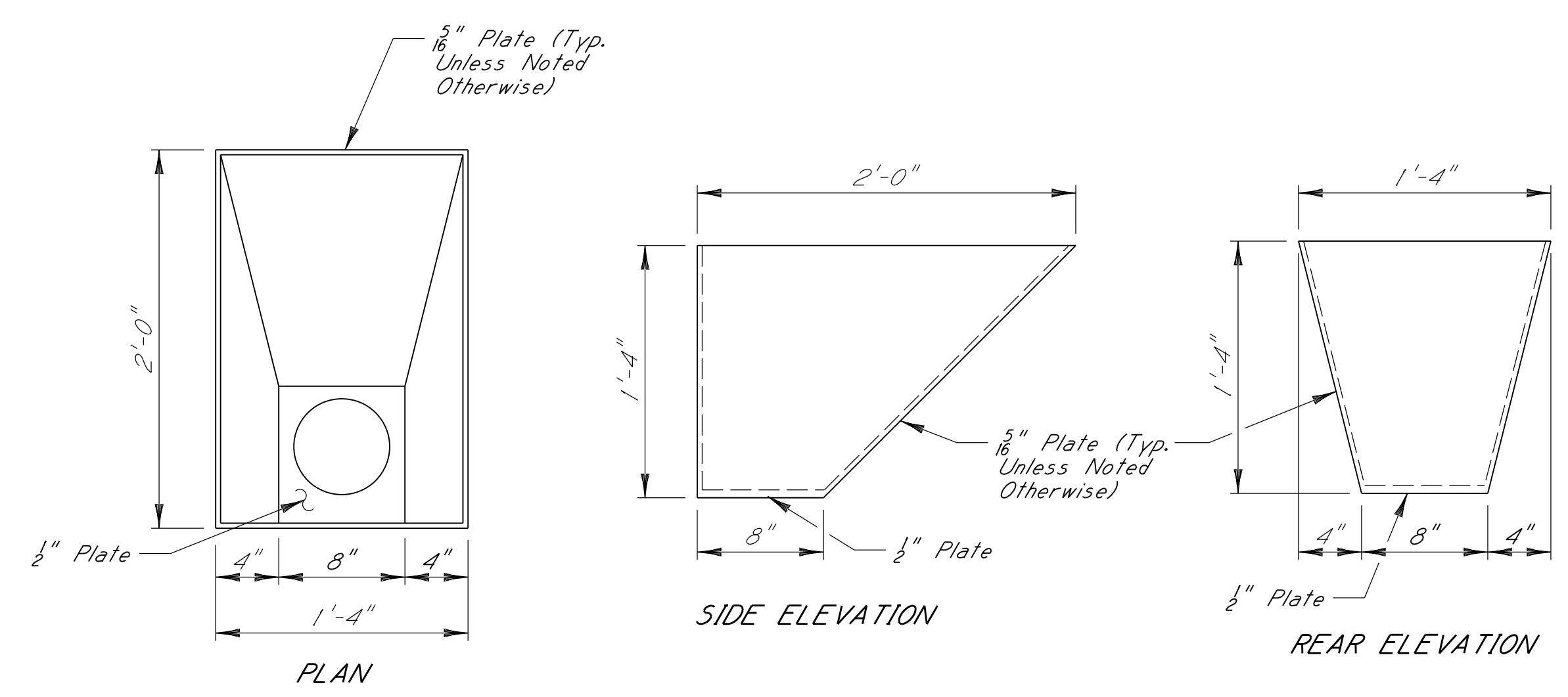
① Field Verify. Pipe Should Terminate Approximately One Foot Above Riprap Splash Area On Ground.



ELEVATION

PLAN

DETAIL "B"
Scale: 1/2" = 1'-0"

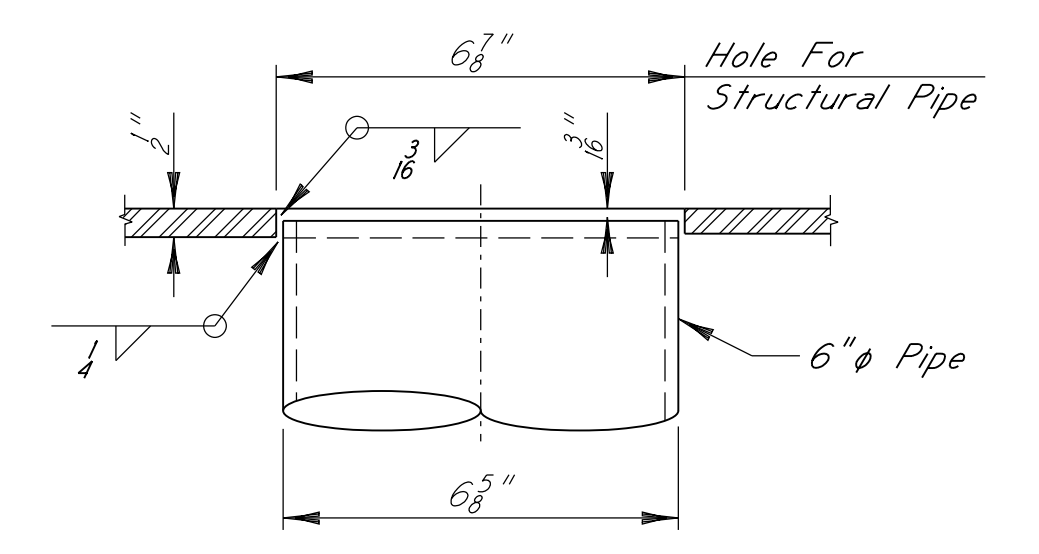


PLAN

SIDE ELEVATION

REAR ELEVATION

CATCH BASIN DETAILS
Scale: 1/2" = 1'-0"



STRUCTURAL PIPE TO CATCH BASIN WELD DETAIL

NOTE:
Downspout Saddle, Catch Basin & Standard Weight Pipe Shall Be Galvanized Per ASTM A123 After Fabrication.

For Information Only
Project BR-0020-01(248)

BY		MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
		BRIDGE AT STA. 977+52.88	
		FINGER EXPANSION JOINT	
		DRAIN DETAILS	
PROJECT		105022/301000	
		BR-0020-01(192)	
HINDS		COUNTY	
DESIGNED ABH		DETAILED ABH	
CHECKED JHR		ISSUED NJA	
DATE		DATE	
		WORKING NUMBER	
		71 of 76	
		SHEET NUMBER	
		8074	

10:26:41 AM 8/28/2014 L:\2011\11027042 - I-20 EB Lanes over Meridian Speedway RR Drawings\BRIDGE\74 - Finger Joint Drain Details.dgn