

**SECTION 905 -- PROPOSAL (CONTINUED)**

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.	<u>  1  </u>	DATED	<u>  4/13/2020  </u>	ADDENDUM NO.	<u>          </u>	DATED	<u>          </u>
ADDENDUM NO.	<u>  2  </u>	DATED	<u>  4/21/2020  </u>	ADDENDUM NO.	<u>          </u>	DATED	<u>          </u>
ADDENDUM NO.	<u>  3  </u>	DATED	<u>  4/22/2020  </u>	ADDENDUM NO.	<u>          </u>	DATED	<u>          </u>

Number	Description
1	Revised Supplement to NTB 2214; Amendment EBSx Download Required.
2	Revised Wage Rates; Amendment EBSx Download Required.
3	Revised NTB No. 2329; Amendment EBSx Download Required.

TOTAL ADDENDA:   3    
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE \_\_\_\_\_

\_\_\_\_\_  
Contractor

BY \_\_\_\_\_  
Signature

TITLE \_\_\_\_\_

ADDRESS \_\_\_\_\_

CITY, STATE, ZIP \_\_\_\_\_

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of \_\_\_\_\_ and the names, titles and business addresses of the executives are as follows:

\_\_\_\_\_  
President Address

\_\_\_\_\_  
Secretary Address

\_\_\_\_\_  
Treasurer Address

The following is my (our) itemized proposal.

NH-1681-00(019) / 107634301

Madison County(ies)

Revised 01/26/2016

# MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2329

CODE: (SP)

DATE: 03/10/2020

SUBJECT: Scope of Work

PROJECT: NH-1681-00(019) / 107634/301 -- Madison County

The contract documents do not include an official set of construction plans, but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings". All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Work on the project shall consist of the following:

**MILL AND OVERLAY APPROXIMATELY 2.0 MILES OF US 51  
FROM 135 FEET' SOUTH OF DINKINS STREET (PAVEMENT CHANGE)  
TO 144 FEET NORTH OF FINLEY ROAD (PAVEMENT CHANGE)  
BEGINNING AT STATION 100+00(BOP) TO 211+56 (EOP)**

### **Station 100+00 (BOP) to Station 211+56 (EOP)**

Work in this area shall consist of milling the travel lanes, shoulders, local roads, and driveway pads to a depth of 2" and variable to provide for grade profile, cross-slope with intent to correct to 2% in the tangent sections. The milled area will then be inlaid with 2" of 12.5-mm, MT, asphalt as per the attached typical sections.

**General Notes:** These general notes are applicable to all sites.

Prior to beginning the milling operations, repair any failed areas by removal of existing 3½" of asphalt and 6" of concrete pavement. The removal area shall then be repaired with 12.5-mm, MT, asphalt. A table showing approximate locations of the failed areas is included.

Immediately following the overlay operation saw and seal all joints in asphalt pavement. Extend the saw/seal to the edge of the new pavement.

Islands are to be placed at any driveway opening of 50 feet or greater. Island locations are described in table provided and the attached details.

Mill parking area 2" between Peace Street and Center Street and overlay 2" of 12.5-mm, MT, asphalt.

Potholes that may exist are to be patched in a timely manner. Patching of potholes shall be considered an absorbed item.

Traffic will be allowed to run on the milled surface for a maximum of five (5) days. Milling shall be performed in accordance with the attached drawings. Traffic will be allowed to run on all milled tie-ins. Temporary pavement joints (paper joints) shall be at least three (3) paper-widths long shall be used at all milled tie-ins and shall be adequately maintained. Approved mix designs must be on hand prior to milling. Milling operations will not commence until such time that, in the opinion of the Engineer, weather conditions have been consistently suitable enough to allow the placement of the asphalt pavement after the milling operations. Outlets are to be provided in milled areas to prevent ponding on the roadway. No separate payment will be made for milling outlets. Milled material shall be removed from curb and gutter in a timely manner to prevent material from entering storm drains.

The Reclaimed Asphalt Pavement (RAP) material removed by the milling operation shall become the property of the Contractor.

In order to meet ADA requirements, the sidewalk ramps will be required to be modified as per attached Special Design Sheets. See attached table for locations. Truncated domes will be required at all street crossings. Areas where brick is to be removed to make ramps ADA compliant removal quantities will be paid for under pay item: 202-B: Removal of Concrete Sidewalk.

The existing signals at Peace Street, Center Street, and Yandell Avenue will be updated with radar detection as detailed in the attached tables.

Publicly maintained roads and streets should be milled and paved to the existing right-of-way. Privately owned entrances shall be paved to the shoulder line as per the included typical drawing. Pads shall be shaped horizontally and vertically to prevent excessive drop-offs. Granular material (Class 5, Group 'C') shall be provided around the pads to prevent shoulder drop-offs as directed and shall be placed in a timely manner. Drop-offs exceeding 2½" shall be corrected before end of day of placement of pad. Stabilizer aggregate shall be used as directed by the Engineer.

It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged during the life of the contract. No payment will be made for replacement or repair of damaged items.

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost is to be included in the price bid for pay item 618-A: Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background. Standard roadside construction signs and barricades will be paid for using the appropriate pay items.

Roadside construction signs, barrels, etc. shall be placed in accordance with the attached drawings or as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed.

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal; the cost is to be included in the prices of items bid. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor.

Where applicable the existing shoulders are to be raised to match the new pavement elevation by placing variable depth granular material (Class 5, Group C) on the existing shoulders. Placement of the granular material on the finished asphalt course shall not be permitted. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%). Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. **Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%). The cost of blading will be an absorbed item and is not to be included in the price of pay items bid.**

Removal of the existing shoulder material shall be coincident with the milling/overlaying operation to prevent the possible ponding of water. No payment will be made for blading or removal of the existing shoulder material. Any material excavated from the existing shoulder shall be used to raise the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer and will be an absorbed item. Material which cannot be placed in adjacent areas and deemed to be excess excavation by the Engineer shall be removed under pay item 203-G: Excess Excavation.

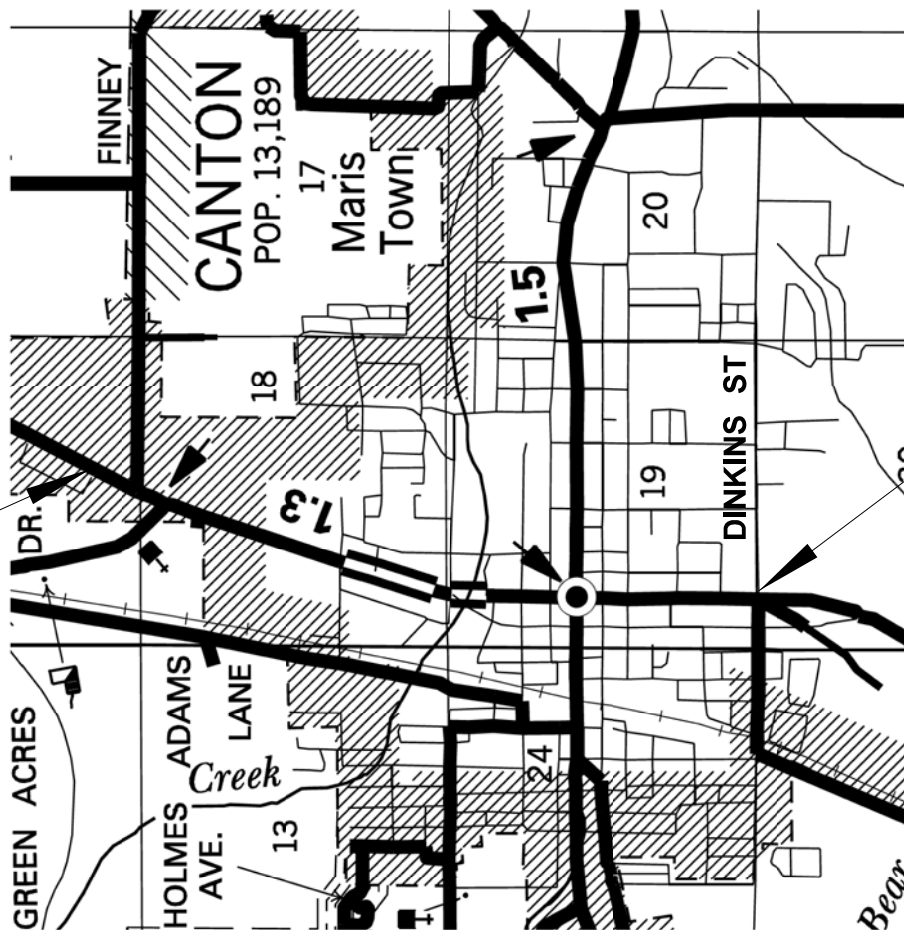
Permanent pavement markers are to be placed in accordance with the attached drawings and Standard Drawings. Two-way yellow markers are to be placed on two-way roads. Two-way clear markers are to be placed on county roads as shown on attached drawings. Red/Clear markers to be placed at all designated left turn intersections.

**Temporary traffic stripe will be required immediately after the required overlay/milling and prior to opening area to traffic. Temporary stripe is to be placed in the same location and configuration as the permanent stripe.**

All permanent striping will be thermoplastic double-drop. Edge lines shall be placed to accommodate the lane widths shown on the applicable typical sections unless prevented by field conditions.

107634/301000  
 US 51 OVERLAY  
 FROM DINKINS ST TO HWY 16 WEST  
 MADISON COUNTY

**EOP 211+56**

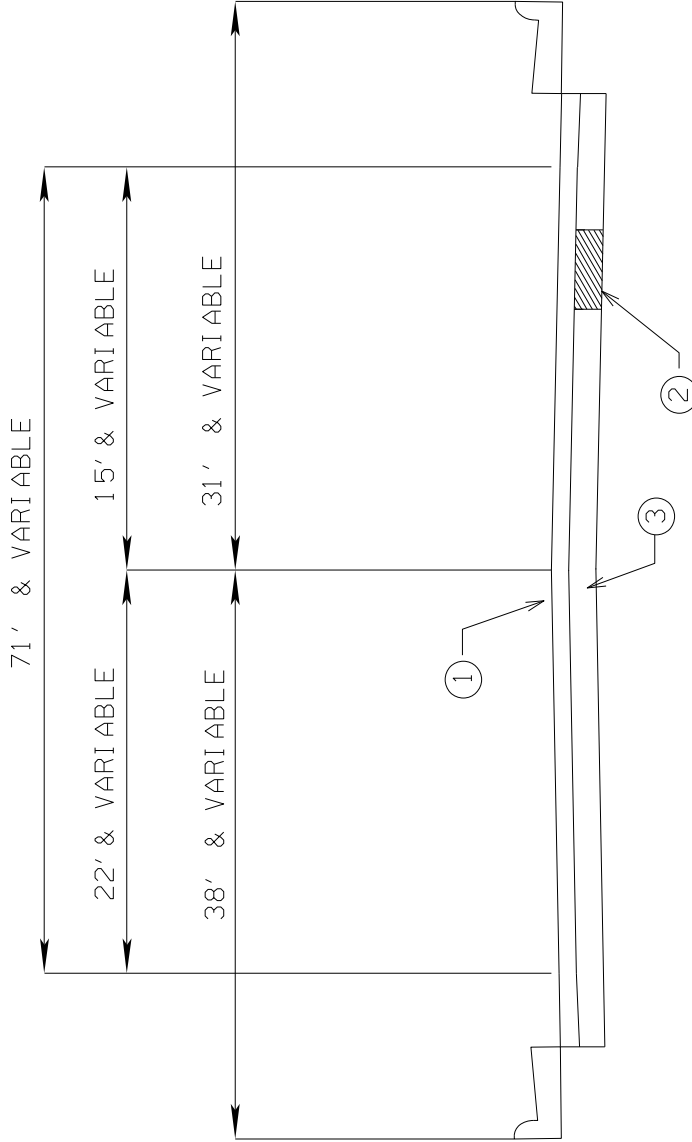


**BOP 100+00**

**MADISON COUNTY**  
**TYPICAL SECTION**  
**US 51 OVERLAY**  
**107634/301000**

DINKINS ST. - SR 16 WEST  
 THRU THE SQUARE SECTIONS

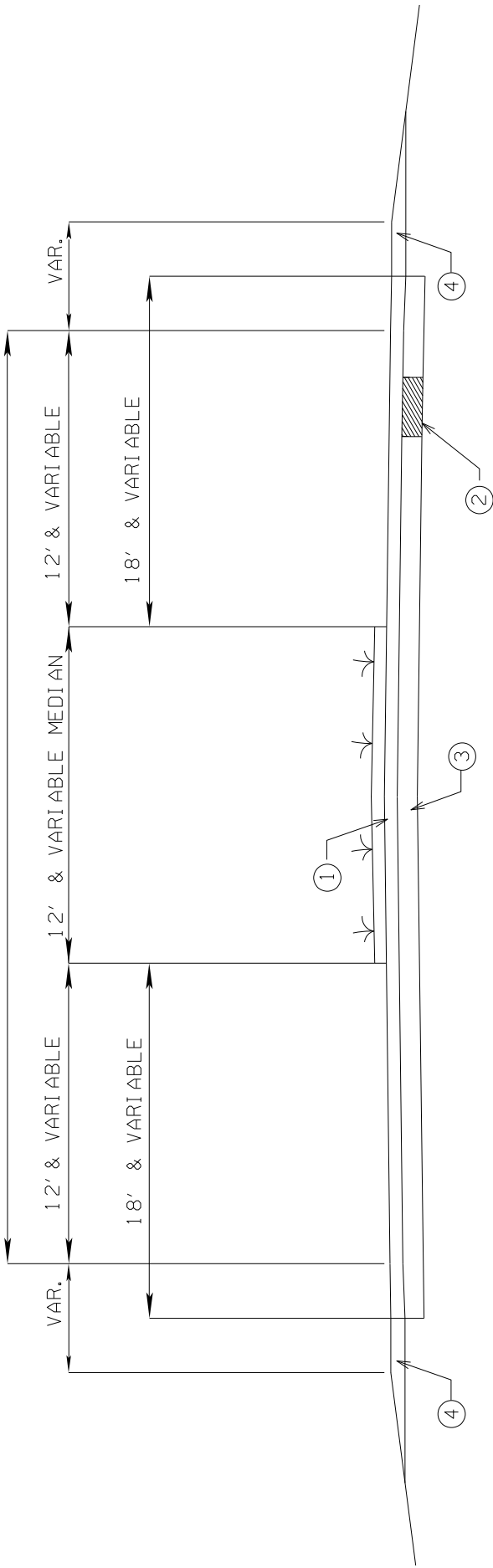
STA. 133+60 TO 135+37



- ① MILL & OVERLAY 2" WITH 12.5mm HMA or WMA,MT
- ② Failed areas to be removed and back filled with asphalt pavement 12.5mm Mix,MT as directed.
- ③ Existing Pavement Structure

**MADISON COUNTY**  
**TYPICAL SECTION**  
**US 51 OVERLAY**  
**107634/301000**

DINKINS ST. - SR 16 WEST  
 THRU DIVIDED SECTIONS  
 STA. 143+00 TO 171+85  
 32' TO 40' & VARIABLE



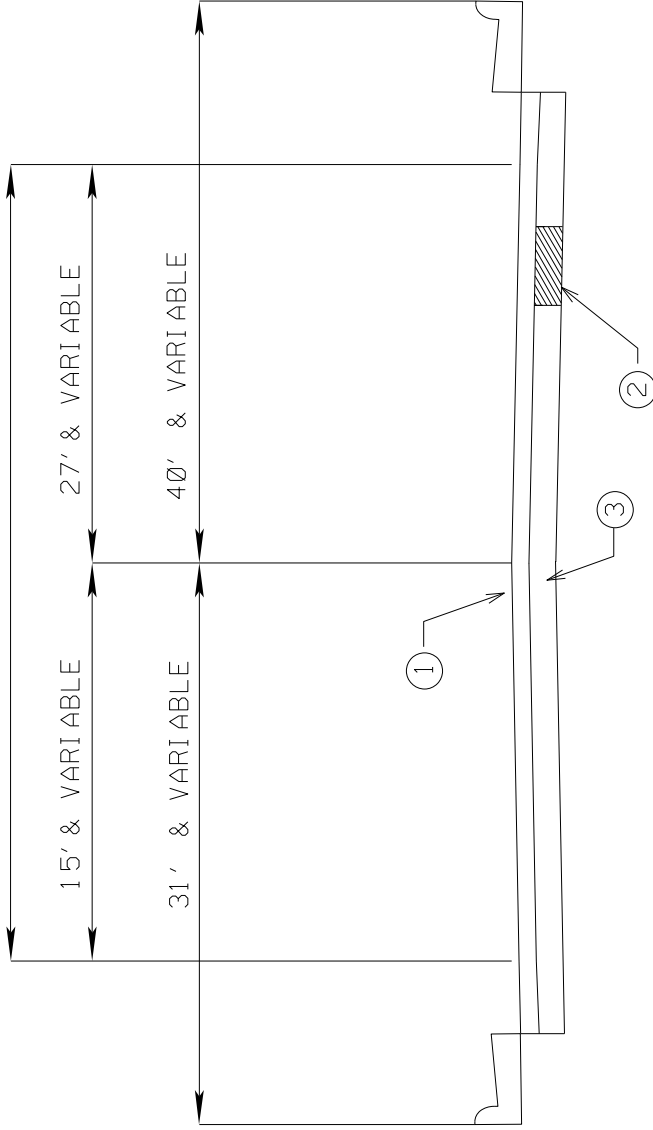
- ① MILL & OVERLAY 2" WITH 12.5mm HMA or WMA, MT
- ② Failed areas to be removed and back filled with asphalt pavement 12.5mm Mix, MT as directed.
- ③ Existing Pavement Structure
- ④ DEPTH 2" GRANULAR MATERIAL CLASS 5, GROUP C

**MADISON COUNTY  
TYPICAL SECTION  
US 51 OVERLAY  
107634/301000**

DINKINS ST. - SR 16 WEST  
THRU THE SQUARE SECTIONS

STA. 135+37 TO 137+12

71' & VARIABLE

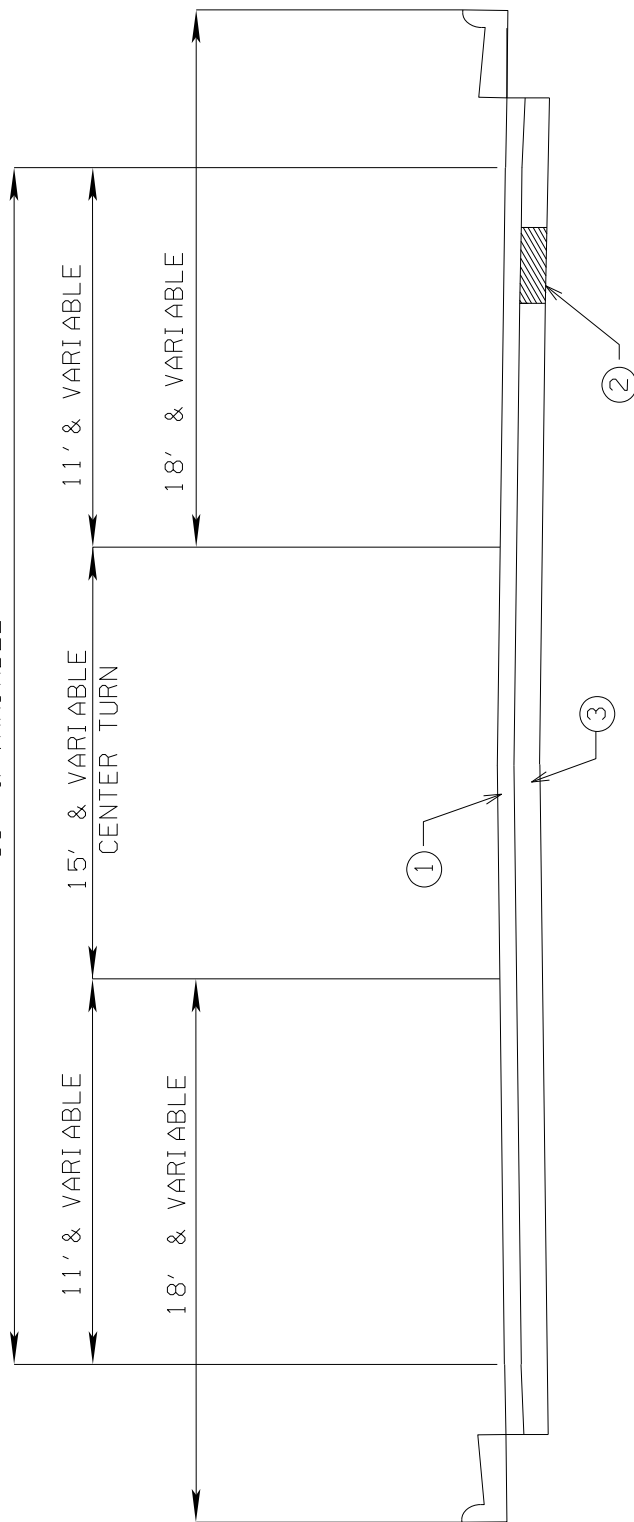


- ① MILL & OVERLAY 2" WITH 12.5mm HMA or WMA,MT
- ② Failed areas to be removed and back filled with asphalt pavement 12.5mm Mix,MT as directed.
- ③ Existing Pavement Structure



**MADISON COUNTY**  
**TYPICAL SECTION**  
**US 51 OVERLAY**  
**107634/301000**

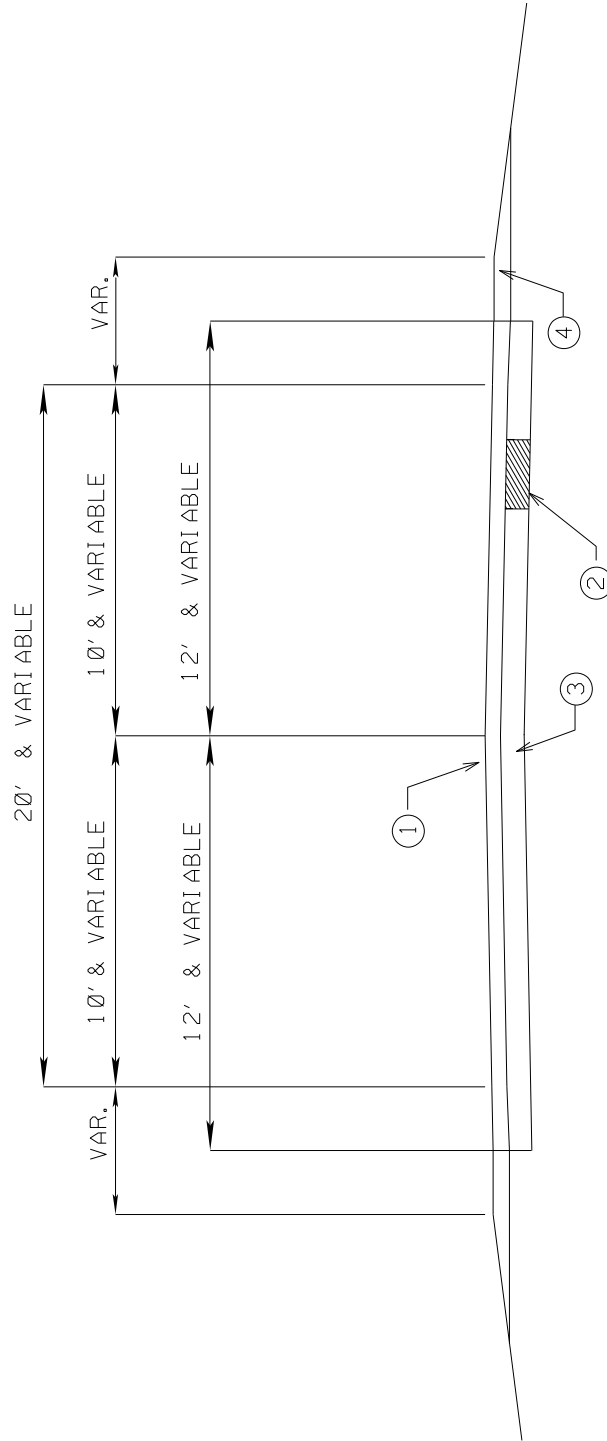
DINKINS ST. - SR 16 WEST  
 3 LANES SECTIONS  
 STA. 174+76 TO 180+27  
 38' & VARIABLE



- ① MILL & OVERLAY 2" WITH 12.5mm HMA or WMA,MT
- ② Failed areas to be removed and back filled with asphalt pavement 12.5mm M<sub>1x</sub>,MT as directed.
- ③ Existing Pavement Structure

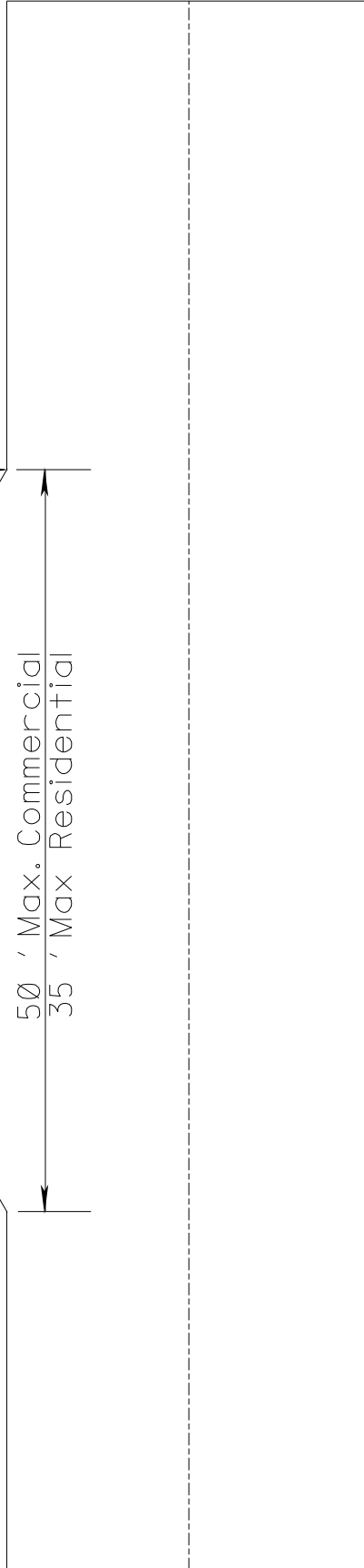
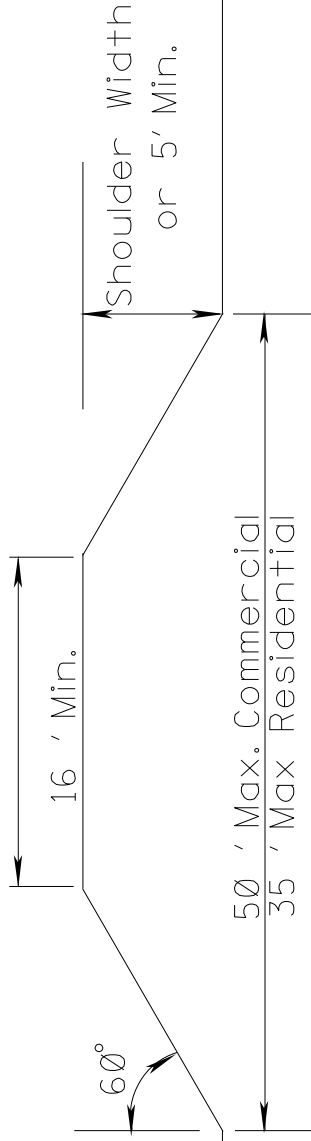
**MADISON COUNTY  
TYPICAL SECTION  
US 51 OVERLAY  
107634/301000**

DINKINS ST. - SR 16 WEST

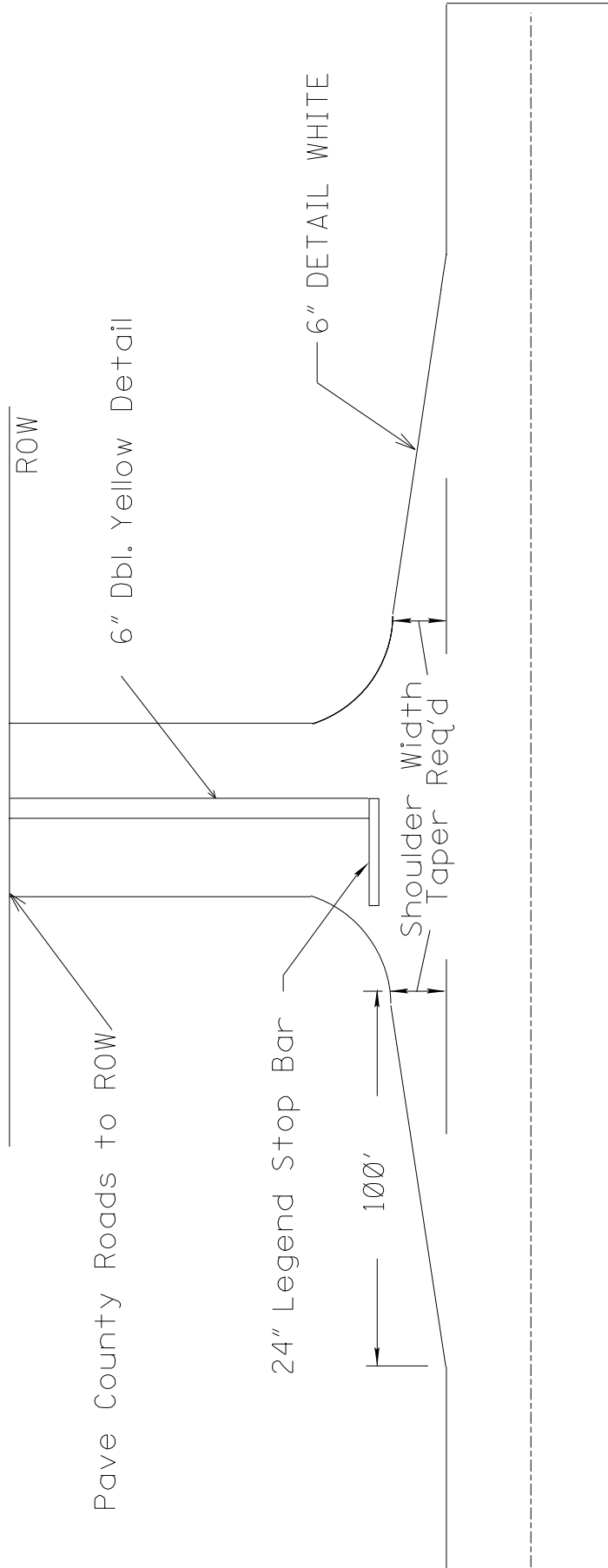


- ① MILL & OVERLAY 2" WITH 12.5mm HMA or WMA,MT
- ② Failed areas to be removed and back filled with asphalt pavement 12.5mm M<sub>1x</sub>,MT as directed.
- ③ Existing Pavement Structure
- ④ DEPTH 2" GRANULAR MATERIAL CLASS 5, GROUP C

TYPICAL RAMP/PAD DETAIL



Typical Section - County Roads



Pave County Roads to ROW

24" Legend Stop Bar

100'

Shoulder Width  
Taper Req'd

6" Dbl. Yellow Detail

6" DETAIL WHITE

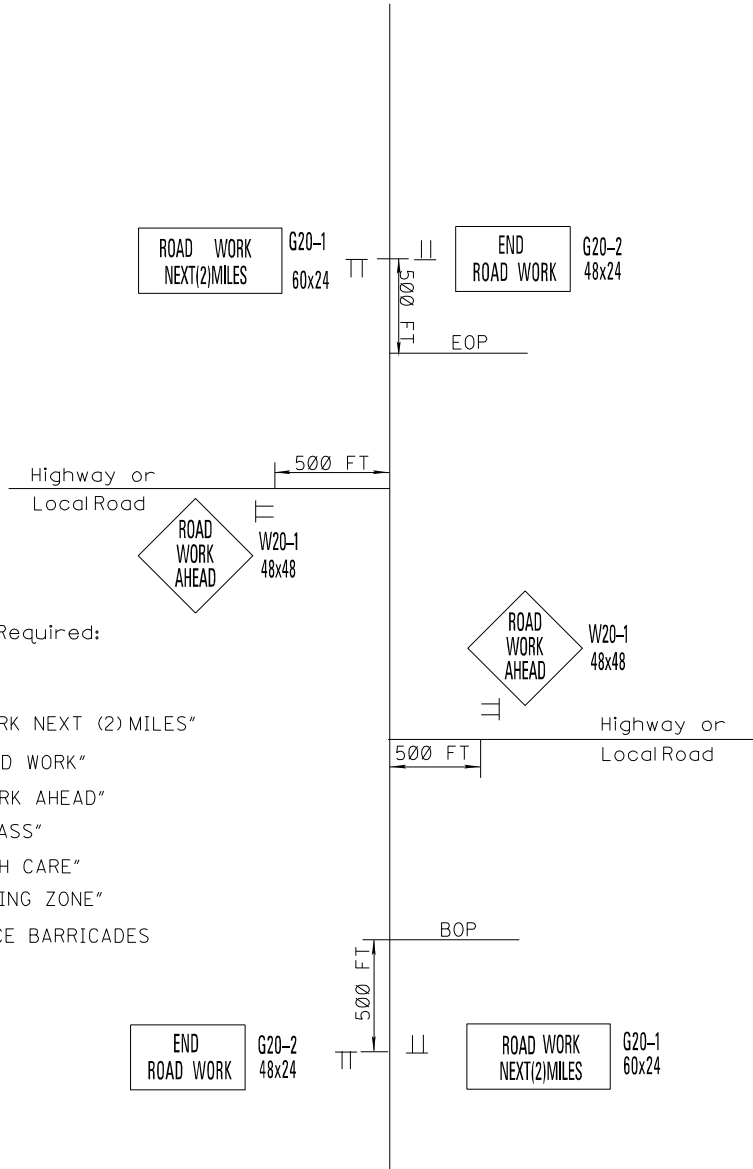
ROW

CONSTRUCTION SIGNING DETAIL  
 US 51 OVERLAY  
 MADISON COUNTY

ESTIMATED

Traffic Control Signs Required:

- 2 - G20-1 "ROAD WORK NEXT (2) MILES"
- 2 - G20-2 "END ROAD WORK"
- 28 - W20-1 "ROAD WORK AHEAD"
- 62 - R4-1 "DO NOT PASS"
- 21 - R4-2 "PASS WITH CARE"
- 19 - W14-3 "NO PASSING ZONE"
- 24 - TYPE III DBL. FACE BARRICADES



NOTES: One (1) W20-1 "ROAD WORK AHEAD" Sign is Required at each Local Road, Street or Highway Entering the Project.

G20-1 and G20-2 signs mounted on Type III Double Faced Barricade.

R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE" AND W14-3 "NO PASSING ZONE" signs are required in accordance with Subsection 618.03.3 and as specified in the MUTCD. If No Passing zones are 1000 ft or more, install additional "DO NOT PASS" signs on maximum spacing of 750 ft.

Payment for these signs will be under the appropriate pay item numbers in the summary of quantities

CONSTRUCTION SIGNS LESS THAN 10 SF						
NAME	TYPE	HEIGHT (IN)	WIDTH (IN)	AREA (SF)	TOTAL	TOTAL AREA (SF)
END ROAD WORK BOP	G20-2	24	48	8,000	1	8.00
END ROAD WORK EOP	G20-2	24	48	8,000	1	8.00

107634/301000

US 51

Madison County

619-D2001

CONSTRUCTION SIGNS 10 SF OR MORE

STATION	NAME	LT/RT	TYPE	HEIGHT (IN)	WIDTH (IN)	AREA (SF)
Dinkens Street LT	Road Work Ahead	LT	W-20	48	48	16.00
Dinkens Street RT	Road Work Ahead	RT	W-20	48	48	16.00
Hill St RT	Road Work Ahead	RT	W-20	48	48	16.00
Semmes St LT	Road Work Ahead	LT	W-20	48	48	16.00
Semmes St RT	Road Work Ahead	RT	W-20	48	48	16.00
Academy St RT	Road Work Ahead	RT	W-20	48	48	16.00
Academy St LT	Road Work Ahead	LT	W-20	48	48	16.00
Fulton St RT	Road Work Ahead	RT	W-20	48	48	16.00
Fulton St LT	Road Work Ahead	LT	W-20	48	48	16.00
East Peace St RT	Road Work Ahead	RT	W-20	48	48	16.00
West Peace St LT	Road Work Ahead	LT	W-20	48	48	16.00
Center St RT	Road Work Ahead	RT	W-20	48	48	16.00
Center St LT	Road Work Ahead	LT	W-20	48	48	16.00
North St RT	Road Work Ahead	RT	W-20	48	48	16.00
North St LT	Road Work Ahead	LT	W-20	48	48	16.00
Park St LT	Road Work Ahead	LT	W-20	48	48	16.00
Park Dr RT	Road Work Ahead	RT	W-20	48	48	16.00
Yandell Ave RT	Road Work Ahead	RT	W-20	48	48	16.00
Yandell Ave LT	Road Work Ahead	LT	W-20	48	48	16.00
N. Union LT	Road Work Ahead	LT	W-20	48	48	16.00
Doherty St RT	Road Work Ahead	RT	W-20	48	48	16.00
Sherwood	Road Work Ahead	RT	W-20	48	48	16.00
MLK DR LT	Road Work Ahead	LT	W-20	48	48	16.00
Liberty Village LT	Road Work Ahead	LT	W-20	48	48	16.00
Northgate Drive	Road Work Ahead	LT	W-20	48	48	16.00
HWY 16 INTERSECTION	Road Work Ahead	LT	W-20	48	48	16.00
Hwy 16	Road Work Ahead	LT	W-20	48	48	16.00
Finney Rd	Road Work Ahead	RT	W-20	48	48	16.00
BOP	Road Work Next 3 Miles	RT	G-20-1	24	60	10.00
EOP	Road Work Next 3 Miles	RT	G-20-2	24	60	10.00
				<b>Total</b>		<b>468.00</b>

107207/301000  
US 51 Overlay

Sawing and Sealing Transverse Joints in Asphalt Pavements

\*All Locations to be field verified by the Contractor

STATION	LENGTH (LINEAR FEET)	STATION	LENGTH (LINEAR FEET)	STATION	LENGTH (LINEAR FEET)	STATION	LENGTH (LINEAR FEET)
100+00	35	105+20	27	110+40	35	115+60	35
100+20	35	105+40	27	110+60	35	115+80	35
100+40	35	105+60	27	110+80	35	116+00	35
100+60	35	105+80	27	111+00	35	116+20	35
100+80	35	106+00	27	111+20	35	116+40	35
101+00	35	106+20	27	111+40	35	116+60	35
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101+80	27	107+00	35	112+20	35	117+40	35
102+00	27	107+20	35	112+40	35	117+60	35
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102+40	27	107+60	35	112+80	35	118+00	35
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104+80	27	110+00	35	115+20	35	120+40	35
105+00	27	110+20	35	115+40	35	120+60	35
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Station	Length	Station	Length	Station	Length	Station	Length	Station	Length	Station	Length	Total
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121+00	35	126+80	35	132+60	40	138+40	40					
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126+40	35	132+20	40	138+00	40	143+80	45					
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Station	Length	Station	Length	Station	Length	Station	Length	Station	Length	Total
144+00	45	149+80	40	155+60	40	161+40	40			
144+20	45	150+00	40	155+80	40	161+60	40			
144+40	45	150+20	40	156+00	40	161+80	40			
144+60	45	150+40	40	156+20	40	162+00	40			
144+80	45	150+60	40	156+40	40	162+20	40			
145+00	55	150+80	40	156+60	40	162+40	40			
145+20	40	151+00	40	156+80	40	162+60	40			
145+40	40	151+20	40	157+00	40	162+80	40			
145+60	40	151+40	40	157+20	40	163+00	40			
145+80	40	151+60	40	157+40	40	163+20	40			
146+00	40	151+80	40	157+60	40	163+40	40			
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149+60	40	155+40	40	161+20	40	167+00	40			
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Station	Length	Station	Length	Station	Length	Station	Length	Station	Length	Station	Length	Total
167+20	40	173+00	55	178+80	55	184+60	52					
167+40	40	173+20	55	179+00	55	184+80	52					
167+60	40	173+40	55	179+20	55	185+00	45					
167+80	40	173+60	55	179+40	55	185+20	45					
168+00	40	173+80	55	179+60	55	185+40	45					
168+20	40	174+00	55	179+80	55	185+60	45					
168+40	40	174+20	55	180+00	55	185+80	45					
168+60	40	174+40	55	180+20	55	186+00	45					
168+80	40	174+60	55	180+40	55	186+20	30					
169+00	40	174+80	55	180+60	55	186+40	30					
169+20	40	175+00	55	180+80	55	186+60	30					
169+40	40	175+20	55	181+00	55	186+80	30					
169+60	40	175+40	55	181+20	55	187+00	30					
169+80	40	175+60	55	181+40	55	187+20	30					
170+00	40	175+80	55	181+60	55	187+40	30					
170+20	40	176+00	55	181+80	55	187+60	30					
170+40	40	176+20	55	182+00	55	187+80	30					
170+60	40	176+40	55	182+20	52	188+00	30					
170+80	40	176+60	55	182+40	52	188+20	30					
171+00	40	176+80	55	182+60	52	188+40	30					
171+20	40	177+00	55	182+80	52	188+60	30					
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171+60	40	177+40	55	183+20	52	189+00	30					
171+80	40	177+60	55	183+40	52	189+20	30					
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172+20	55	178+00	55	183+80	52	189+60	30					
172+40	55	178+20	55	184+00	52	189+80	30					
172+60	55	178+40	55	184+20	52	190+00	30					
172+80	55	178+60	55	184+40	52	190+20	30					
<b>Total</b>	<b>1235</b>	<b>Total</b>	<b>1595</b>	<b>Total</b>	<b>1559</b>	<b>Total</b>	<b>1004</b>					

Station	Length	Station	Length	Station	Length	Station	Length	Station	Length
190+40	30	196+20	30	202+00	40	207+80	40		
190+60	30	196+40	30	202+20	40	208+00	40		
190+80	30	196+60	30	202+40	40	208+20	40		
191+00	30	196+80	30	202+60	40	208+40	30		
191+20	30	197+00	30	202+80	40	208+60	30		
191+40	30	197+20	30	203+00	40	208+80	30		
191+60	30	197+40	30	203+20	40	209+00	30		
191+80	30	197+60	30	203+40	40	209+20	30		
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192+20	30	198+00	30	203+80	40	209+60	30		
192+40	30	198+20	30	204+00	40	209+80	30		
192+60	30	198+40	30	204+20	40	210+00	30		
192+80	30	198+60	30	204+40	40	210+20	30		
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193+80	30	199+60	30	205+40	40	211+20	30		
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194+20	30	200+00	30	205+80	40	<b>Total</b>	<b>600</b>		
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195+60	30	201+40	30	207+20	40				
195+80	30	201+60	30	207+40	40				
196+00	30	201+80	40	207+60	40				
							<b>Grand Total</b>	<b>22272</b>	

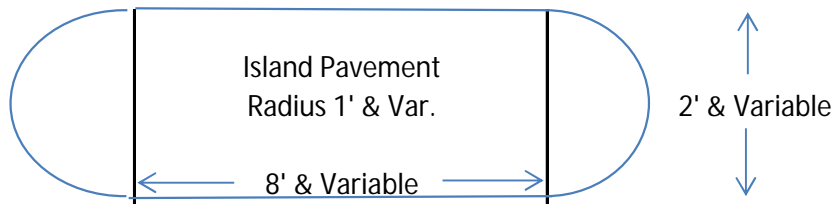
**107634/301000**  
**US 51 Dinkins St to SR 16 West**  
**Madison County**  
**ADA Ramp Replacement**

Location	Length	Width	202-B080	202-B092	608-B001	907-608-C001	609-D001
			Removal of Concrete Sidewalk (S.Y.)	Removal of Curb (LF)	Concrete Sidewalk, with Reinforcement (S.Y.)	Detectable Warning Panels (SF)	Combination Curb and Gutter, Type 1 (L.F.)
NE Corner Seemes St	17	5	9.4	0	9.4	10	0
NE Corner Fulton St	31	8	27.6	10	27.6	10	10
SE Corner North St	26	5	14.4	28	14.4	10	28
NE Corner North St	22	5	12.2	6	12.2	10	6
SW Corner North St	40	5	22.2	40	22.2	10	40
NW Corner North St	29	6	19.3	0	19.3	10	0
SE Corner Park St	10	6	6.7	11	6.7	10	11
NE Corner Park St	20	5	11.1	0	11.1	10	0
SE Corner Yandell Ave	47	5	0.0	5	26.1	10	5
NE Corner Yandell Ave	15	5	8.3	0	8.3	10	0
SE Doherty St	10	5	5.6		5.6	10	0
NE Doherty St	10	5	5.6	0	5.6	10	0
SE Sherwood Dr	13	5	7.2	5	7.2	10	5
NE Sherwood Dr	16	5	8.9	5	8.9	10	5
SW Sherwood Dr	12	5	6.7	5	6.7	10	5
Sta 135+37 LT	8	5	4.4	8	4.4	10	8
Sta 135+47 LT	8	5	4.4	8	4.4	10	8
Sta 135+37 RT	8	5	4.4	8	4.4	10	8
<b>Total</b>			<b>178.6</b>	<b>139</b>	<b>204.7</b>	<b>180</b>	<b>139</b>

**107634/301000  
US 51 Overlay  
Madison County**

**Asphalt Curb and Island Pavement**

<b>Station</b>	<b>Location</b>	<b>Curb</b>	<b>Island Pavement</b>
109+75	RT	24 LF	3 S.Y.
110+25	RT	24 LF	3 S.Y.
110+75	RT	24 LF	3 S.Y.
179+25	RT	28 L.F.	3 S.Y.
		<b>Total</b>	<b>99 L.F.</b>
			<b>12 S.Y.</b>



**107634/301000**  
**US 51 Overlay**  
**From Dinkins St. to Hwy 16**  
**Madison County**  
**Failed Areas**

Station	Location	Length	Width	Depth	Asphalt Removal S.Y.	Concrete Removal S.Y.	Saw Cut	Excess C.Y.	Asphalt Tons	Tack Gallons
124+39	RT Lane	20	11	2	24.4	24.4	42	16.3	31.9	1
125+03	RT Lane	25	11	2	30.6	30.6	47	20.4	39.9	2
125+09	RT Lane	11	11	2	13.4	13.4	33	9.0	17.5	1
128+80	RT Lane	8	11	2	9.8	9.8	30	6.5	12.8	0
154+65	In Intersection	22	22	2	53.8	53.8	66	35.9	70.2	3
154+85	RT Lane	27	22	2	66.0	66.0	71	44.0	86.1	3
166+50	RT Lane	46	11	2	56.2	56.2	68	37.5	73.4	3
167+18	RT Lane	27	11	2	33.0	33.0	49	22.0	43.1	2
174+06	RT Lane	15	11	2	18.3	18.3	37	12.2	23.9	1
176+35	Lt Lane	12	11	2	14.7	14.7	34	9.8	19.1	1
179+04	RT Lane	39	11	2	47.7	47.7	61	31.8	62.2	2
180+23	RT Lane	46	11	2	56.2	56.2	68	37.5	73.4	3
181+30	Lt Lane	91	11	2	111.2	111.2	113	74.1	145.1	6
181+84	RT Lane	106	11	2	129.6	129.6	128	86.4	169.1	6
210+56	Lt Lane	51	11	2	62.3	62.3	73	41.6	81.3	3
203+41	Lt Lane	19	11	2	23.2	23.2	41	15.5	30.3	1
Ramp to Yandell Ave.	RT Ramp	15	30	2	50.0	50.0	75	33.3	65.3	3
	<b>Total</b>				<b>800.4</b>	<b>800.4</b>	<b>1616</b>	<b>533.6</b>	<b>1044.6</b>	<b>40</b>

107634/301000  
 US 51 Overlay Packet  
 Dinkins Street to SR 16 West  
 Madison County  
 Signal Quantities

PAY ITEM NO.	PAY ITEM	Location	UNIT	QUANTITY	Notes
907-632-D001	Solid State Traffic Actuated Controller, Type 1	US 51 @ Center Street and US 51 @ Yandell Avenue	EA	2	#1
907-641-A002	Signal Stop Bar Radar Vehicle Detection Sensor, Type 2	US 51 @ Peace St, US 51 @ Center St, and US 51 @ Yandell Ave	EA	13	#2
907-641-D001	Radar Detection Communication Cable	US 51 @ Peace St, US 51 @ Center St, and US 51 @ Yandell Ave	LF	970	#3 #4

#1 Existing M34 Controllers to be salvaged to MIDOT Signal Shop 601-359-1493. Contractor shall be responsible for transferring existing controller data to the new controller.

#2 Radar units to be mounted per manufacturer recommendations.

#3 Contractor may remove existing detection loop cable, if necessary.

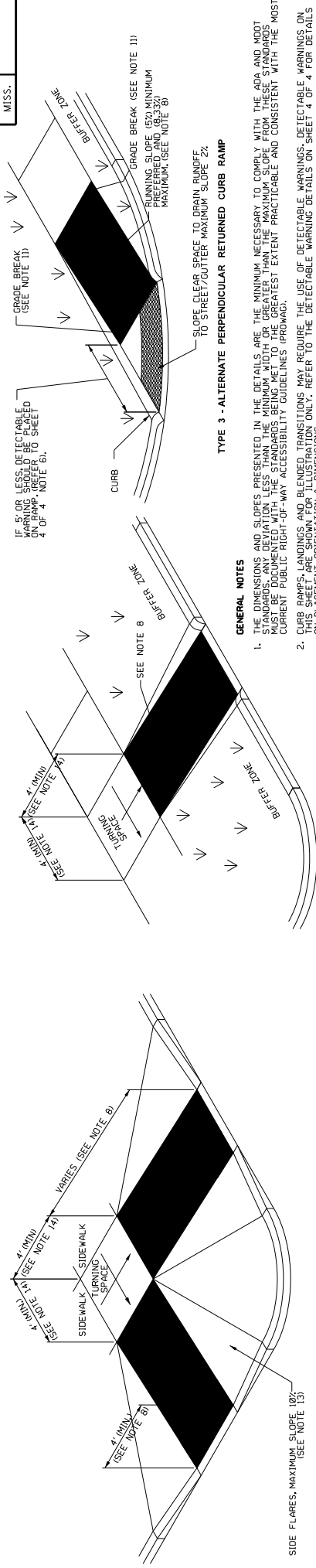
#4 Cable lengths may be adjusted based on radar locations per manufacturer recommendations

TRAFFIC SIGNAL RADAR DETECTION CHART

Intersection	Detection Zone Location	Phase #	Detection Zone Size	Card Rack Slots Available	STOPBAR Radar Units Required	Controller Type	Existing Pole Configuration
US 51 @ Peace St	WB Left Turn Lane	1	6'X50'	4	4	M34 Controller	MAST ARM SIGNAL
	WB Thru Lane	6	6'X50'				
	EB Left Turn Lane	5	6'X50'				
	EB Thru Lane	2	6'X50'				
	NB Left Turn Lane	3	6'X50'				
	NB Thru Lane	8	6'X50'				
	SB Left Turn Lane	7	6'X50'				
	SB Thru Lane	4	6'X50'				
	NB Left Turn Lane	5	6'X50'				
	NB Thru Lane	2	6'X50'				
US 51 @ Center St	SB Left Turn Lane	1	6'X50'	2	4	M34 Controller (New Controller Required)	MAST ARM SIGNAL
	SB Thru Lane	6	6'X50'				
	WB Left Turn Lane	8	6'X50'				
	WB Thru Lane	8	6'X50'				
	EB Left Turn Lane	4	6'X50'				
	EB Thru Lane	4	6'X50'				
	NB Thru Lane	2	6'X50'				
	NE Bound Thru Lane	3	6'X50'				
US 51 @ Yandell Ave	SB Thru Lane	6	6'X50'	2	5	M34 Controller (New Controller Required)	MAST ARM SIGNAL
	EB Thru Lane	4B	6'X50'				
	WB Thru Lane	4A	6'X50'				

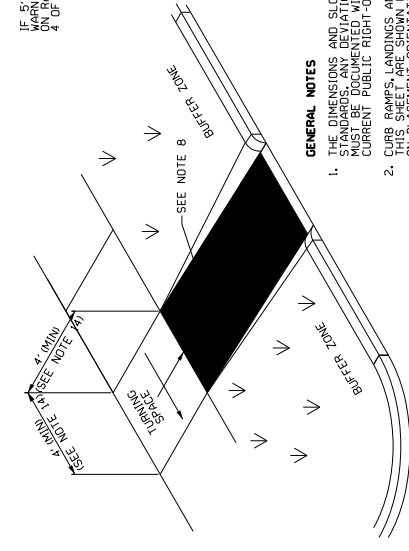


PROJECT NO.	
STATE	MISS.



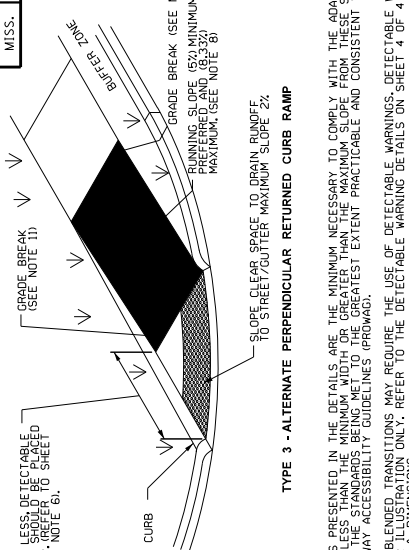
TYPE 1 - PERPENDICULAR CURB RAMP

SIDE FLARES, MAXIMUM SLOPE 10% (SEE NOTE 13)



TYPE 2 - PERPENDICULAR RETURNED CURB RAMP

SIDES OF CURB RAMPS MAY BE RETURNED, PROVIDING USEFUL DIRECTIONAL CUES, IF PROTECTED FROM CROSS TRAVEL BY LANDSCAPING, STREET FURNITURE, POLES, OR EQUIPMENT.



TYPE 3 - ALTERNATE PERPENDICULAR RETURNED CURB RAMP

GENERAL NOTES

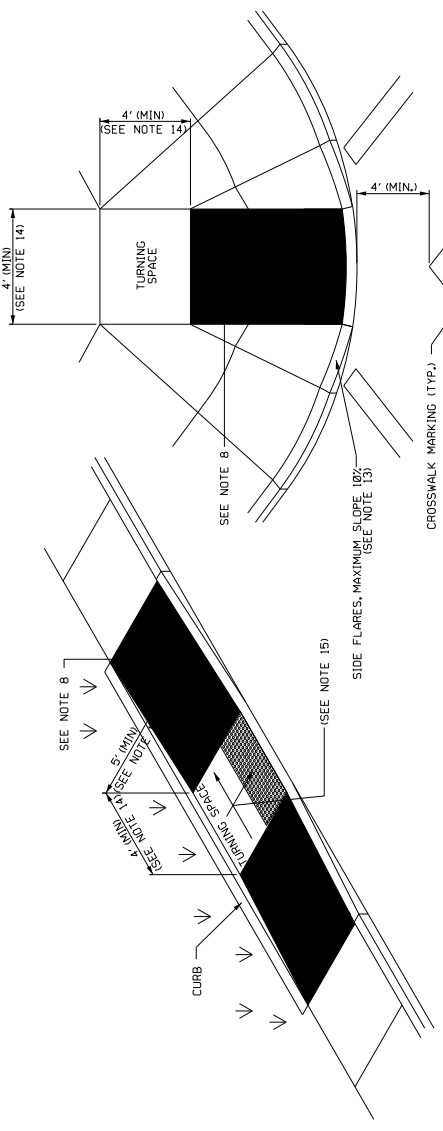
1. THE DIMENSIONS AND SLOPES PRESENTED IN THE DETAILS ARE THE MINIMUM NECESSARY TO COMPLY WITH THE ADA AND MDT STANDARDS. ANY DEVIATION LESS THAN THE MINIMUM WIDTH OR GREATER THAN THE MAXIMUM SLOPE FROM THESE STANDARDS SHALL BE NOTED ON THE DRAWING. THE MINIMUM WIDTH SHALL BE PRACTICABLE AND CONSISTENT WITH THE MOST CURRENT PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES PROVIDED.
2. CURB RAMPS, LANDINGS, AND BLENDED TRANSITIONS MAY REQUIRE THE USE OF DETECTABLE WARNINGS. DETECTABLE WARNINGS ON PLACEMENT, ORIENTATION & DIMENSIONS.
3. THE LOCATION, ORIENTATION, AND TYPE OF CURB RAMPS SHALL BE AS SHOWN IN THE PLANS.
4. ANY COMBINATION OF PERPENDICULAR, PERPENDICULAR RETURNED, AND PARALLEL CURB RAMPS MAY BE USED TO ACHIEVE AN ACCESSIBLE DESIGN AS LONG AS THE BASIC REQUIRMENTS FOR CURB RAMPS ARE MET.
5. CURB RAMPS SHALL BE PAID FOR AS SIDEWALK.
6. THE THICKNESS OF THE CURB RAMP SHALL BE A MINIMUM OF 4".
7. BEYOND THE BOTTOM GRADE BREAK, A CLEAR SPACE OF 4' MINIMUM BY 4' MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE.

CURB RAMP NOTES:

8. THE CLEAR WIDTH OF CURB RAMP RUNS (EXCLUDING ANY FLARED SIDES), BLENDED TRANSITIONS, AND TURNING SPACES, SHALL BE 4' MINIMUM. BLENDED TRANSITIONS SHALL BE 5' MINIMUM, AND 6.33% MAXIMUM.
9. WHERE THE SLOPE OF THE ROADWAY EXCEEDS 8.33%, THE CURB RAMP LENGTH IS THE LENGTH NECESSARY TO MEET THE EXISTING SIDEWALK. IT IS NOT NECESSARY THAT THE RAMP EXCEED 19'.
10. THE CROSS SLOPE OF CURB RAMPS, BLENDED TRANSITIONS, AND TURNING SPACES SHALL BE 2% MAXIMUM (1.5% PREFERRED). AT PEDESTRIAN STREET CROSSINGS WITHOUT YIELD OR STOP CONTROL, AND AT MIDDLEBLOCK PEDESTRIAN STREET CROSSINGS, THE SLOPE IS PERMITTED TO EQUAL THE STREET OR HIGHWAY GRADE.
11. GRADE BREAKS AT THE TOP AND BOTTOM OF CURB RAMP RUNS SHALL BE PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN. GRADE BREAKS SHALL NOT BE PERMITTED ON THE SURFACE OF RAMP RUNS AND TURNING SPACES THAT MEET AT RIGHT ANGLES.
12. RAMP TRANSITIONS BETWEEN WALKS, TURNING SPACES, LANDINGS, GUTTERS, OR STREETS SHALL BE FLUSH AND FREE OF OBSTACLE.
13. WHERE A PEDESTRIAN CIRCULATION PATH CROSSES THE CURB RAMP, FLARED SIDES SHALL BE SLOPED 10% MAXIMUM, MEASURED PARALLEL TO THE CURB LINE.

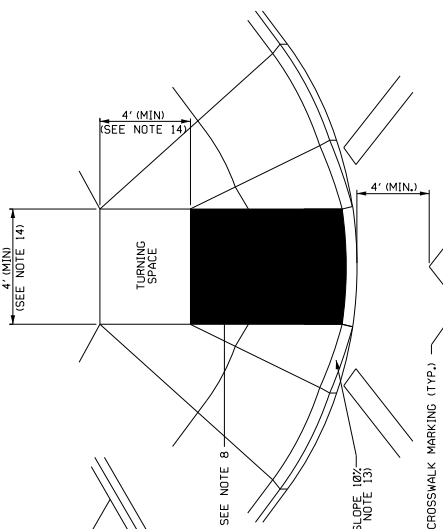
TURNING SPACE NOTES:

14. A TURNING SPACE 4' MINIMUM BY 4' MINIMUM SHALL BE PROVIDED AT THE TOP OF PERPENDICULAR RAMPS AND AT THE BOTTOM OF PERPENDICULAR RETURNED RAMPS. TURNING SPACES SHALL BE 5' MINIMUM BY 5' MINIMUM, AND 5% MAXIMUM. TURNING SPACES SHALL BE 6' MINIMUM BY 6' MINIMUM, AND 5% MAXIMUM. TURNING SPACES SHALL BE 8' MINIMUM BY 8' MINIMUM, AND 5% MAXIMUM. TURNING SPACES SHALL BE 10' MINIMUM BY 10' MINIMUM, AND 5% MAXIMUM. TURNING SPACES SHALL BE 12' MINIMUM BY 12' MINIMUM, AND 5% MAXIMUM. TURNING SPACES SHALL BE 14' MINIMUM BY 14' MINIMUM, AND 5% MAXIMUM. TURNING SPACES SHALL BE 16' MINIMUM BY 16' MINIMUM, AND 5% MAXIMUM. TURNING SPACES SHALL BE 18' MINIMUM BY 18' MINIMUM, AND 5% MAXIMUM. TURNING SPACES SHALL BE 20' MINIMUM BY 20' MINIMUM, AND 5% MAXIMUM. TURNING SPACES SHALL BE 22' MINIMUM BY 22' MINIMUM, AND 5% MAXIMUM. TURNING SPACES SHALL BE 24' MINIMUM BY 24' MINIMUM, AND 5% MAXIMUM. TURNING SPACES SHALL BE 26' MINIMUM BY 26' MINIMUM, AND 5% MAXIMUM. TURNING SPACES SHALL BE 28' MINIMUM BY 28' MINIMUM, AND 5% MAXIMUM. TURNING SPACES SHALL BE 30' MINIMUM BY 30' MINIMUM, AND 5% MAXIMUM.
15. THE RUNNING SLOPE OF TURNING SPACES SHALL BE 2% MAXIMUM (1.5% PREFERRED). THE CROSS SLOPE OF TURNING SPACES SHALL BE 2% MAXIMUM (1.5% PREFERRED). AT PEDESTRIAN STREET CROSSINGS WITHOUT YIELD OR STOP CONTROL AND AT MIDDLEBLOCK PEDESTRIAN STREET CROSSINGS, THE CROSS SLOPE SHALL BE PERMITTED TO EQUAL THE STREET OR HIGHWAY GRADE.
16. BEYOND THE BOTTOM GRADE BREAK, A CLEAR SPACE 4' MINIMUM BY 4' MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE.
17. THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE FOOT OF CURB RAMP RUNS, BLENDED TRANSITIONS, AND TURNING SPACES SHALL BE 5% MAXIMUM. IT IS BEST PRACTICE TO PROVIDE A 2' LEVEL STRIP AT THE GUTTER IF THE GRADE BREAK EXCEEDS 11%.



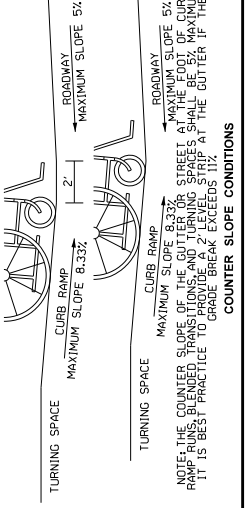
TYPE 4 - PARALLEL CURB RAMP

THE CURB RAMPS ARE UNACCEPTABLE IN NEW CONSTRUCTION, BUT IS SUGGESTED FOR RETAINING SOIL AND PROVIDING AN EDGE FOR PEDESTRIANS WITH VISUAL IMPAIRMENTS.



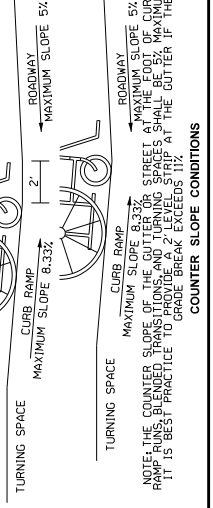
TYPE 5 - DIAGONAL CURB RAMP

DIAGONAL CURB RAMPS ARE UNACCEPTABLE IN NEW CONSTRUCTION, BUT IS SUGGESTED FOR RETAINING SOIL ONLY IF IT IS THE ONLY OPTION THAT WILL WORK.

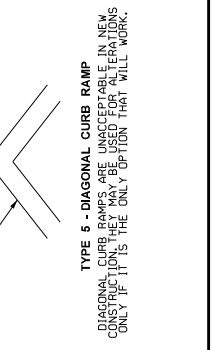


COUNTER SLOPE CONDITIONS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
CURB RAMPS	
RAMP DESIGN ELEMENTS	
SHEET 1 OF 4	
COUNTY: MADISON	
PROJ. NUM: SDCOR-1	
FILENAME: SDCOR-1.DGN	CHECKED: _____
DESIGN TEAM	DATE

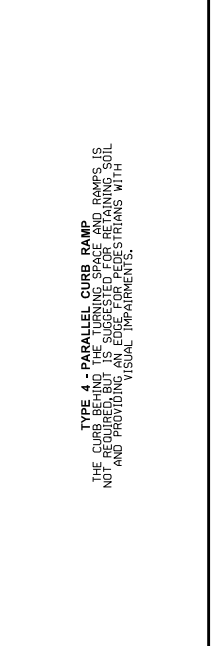


COUNTER SLOPE CONDITIONS



TYPE 5 - DIAGONAL CURB RAMP

DIAGONAL CURB RAMPS ARE UNACCEPTABLE IN NEW CONSTRUCTION, BUT IS SUGGESTED FOR RETAINING SOIL ONLY IF IT IS THE ONLY OPTION THAT WILL WORK.



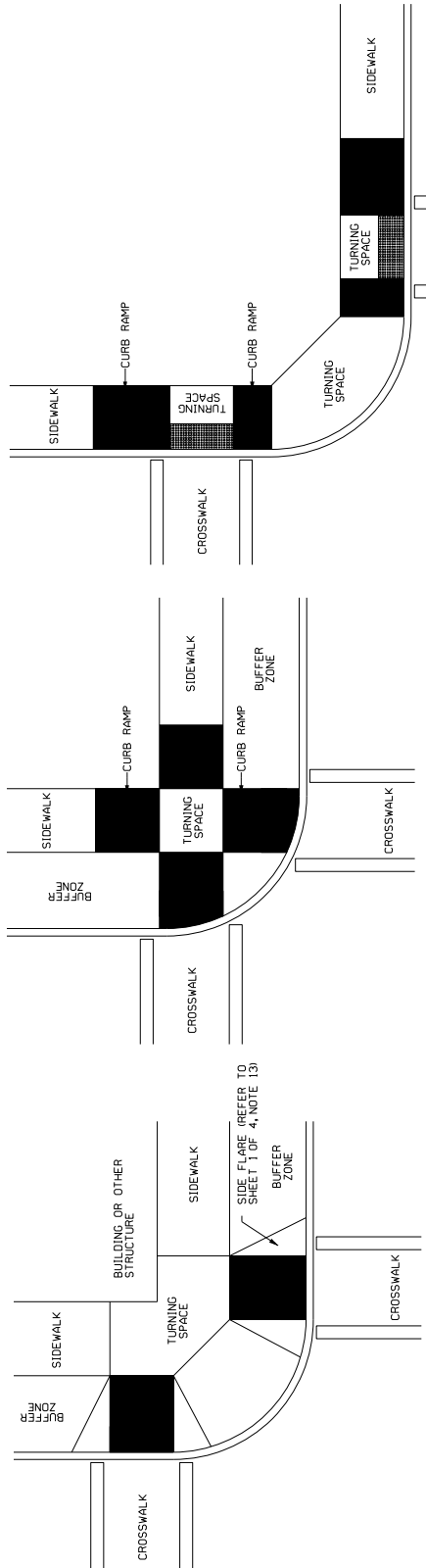
TYPE 4 - PARALLEL CURB RAMP

THE CURB RAMPS ARE UNACCEPTABLE IN NEW CONSTRUCTION, BUT IS SUGGESTED FOR RETAINING SOIL AND PROVIDING AN EDGE FOR PEDESTRIANS WITH VISUAL IMPAIRMENTS.

STATE	PROJECT NO.
MISS.	

**NOTES:**

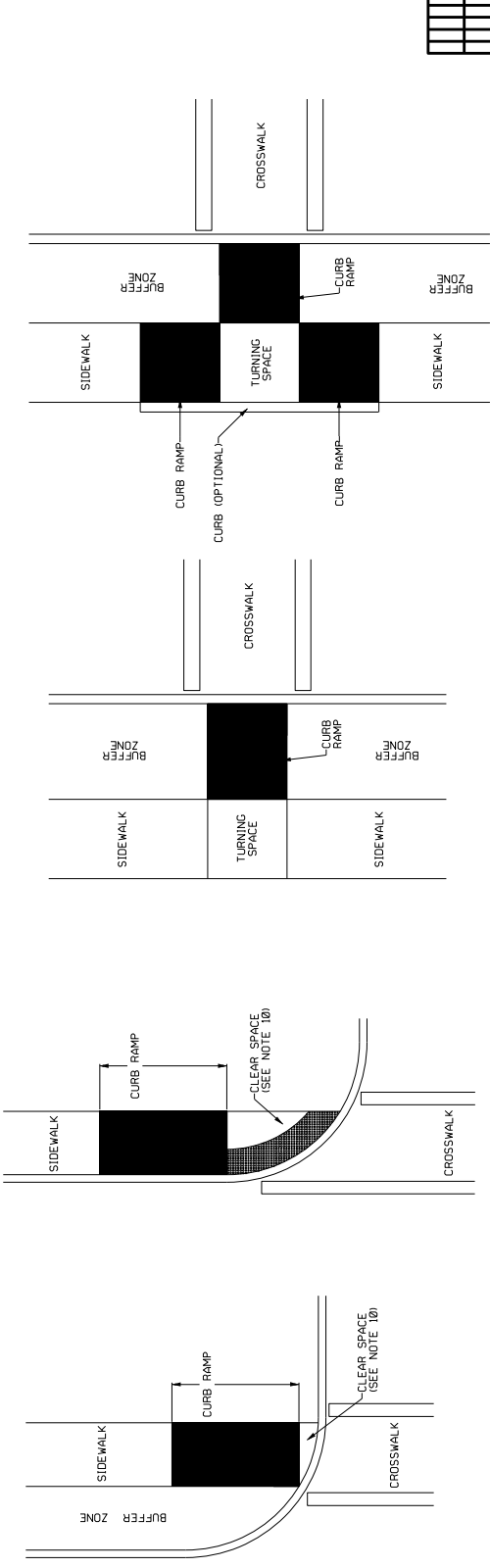
- FOR DIMENSIONS & GEOMETRIC VALUES REFER TO SHEET 1 OF 4.
- THE CONFIGURATIONS SHOWN GENERALLY REPRESENT THE INTENT TO PRESENT CURB RAMP DESIGN CONCEPTS. SITE CONDITIONS AT INDIVIDUAL LOCATIONS REQUIRE SPECIFIC DESIGNS. CURB RAMP DESIGNS MUST BE CONSISTENT WITH THE PROVISIONS OF SHEETS 1, 2, AND 3 OF 4.
- COORDINATE TRAFFIC CONTROL DEVICES AT UTILITY LOCATIONS. SIGNS, STREET FURNITURE AND DRAINAGE TO ENSURE A CONTINUOUS PEDESTRIAN ACCESS ROUTE AT ALL CURB RAMP LOCATIONS. GUIDANCE FOR CROSSWALK ALLIANCE AND TRAFFIC CONTROL DEVICES IS PROVIDED IN THE MUTCD.
- DETECTABLE WARNING SIGNAL ON THIS SHEET ARE FOR ILLUSTRATION ONLY FOR SPECIFIC PLACEMENT ORIENTATIONS AND DIMENSIONS REFER TO SHEET 4 OF 4.
- THE CROSS SLOPE OF CURB RAMP, BLENDED TRANSITIONS, AND TURNING SPACES SHALL BE 2% MAXIMUM (1.5% PREFERRED). AT PEDESTRIAN STREET CROSSINGS WITHOUT SIDEWALKS, THE CROSS SLOPE OF THE CURB RAMP SHALL BE PERMITTED TO EQUAL THE STREET OR HIGHWAY GRADE.
- DIAGONAL CURB RAMP ARE UNACCEPTABLE IN NEW CONSTRUCTION, THEY MAY BE USED FOR ALTERATIONS ONLY IF IT IS THE ONLY OPTION THAT WILL WORK.
- GRATES SHALL NOT BE LOCATED ON CURB RAMP, BLENDED TRANSITIONS, TURNING SPACES OR LANDINGS. ACCESS COVERS OF SIMILAR SURFACES SHALL COMPLY WITH APPLICABLE SURFACE REQUIREMENTS.
- UTILITIES, SIGNS, AND OTHER FIXED OBJECTS SHALL NOT BE PLACED IN A CURB RAMP OR PEDESTRIAN ACCESS ROUTE, BE IN A POSITION THAT INTERFERES WITH THE USE OF THE CURB RAMP.
- THE SURFACE OF ALL CURB RAMP SHALL BE STABLE, FIRM, AND SLIP RESISTANT. A COARSE BROODY FINISH, RUNNING PERPENDICULAR TO THE SLOPE, IS RECOMMENDED ON CONCRETE, ASPHALT SURFACES, EXCLUSIVE OF DETECTABLE WARNING FIELDS.
- THERE SHALL BE A CLEAR SPACE AT THE BOTTOM OF CURB RAMP. THE MINIMUM CLEARANCE SHALL BE 4' AND IT SHALL SLOPE TO DRAIN RUNOFF TO STREET LIGHTER AND HAVE A MAXIMUM SLOPE OF 2% (1.5% PREFERRED).
- TURNING SPACES MAY OVERLAP WITH ADJACENT TURNING MULTIPLE CURB RAMP.
- TURNING SPACES MAY OVERLAP WITH THE CLEAR GROUND SPACE REQUIRED AT PEDESTRIAN SIGNAL PUSH BUTTONS.
- THE CLEAR WIDTH OF PEDESTRIAN ACCESS ROUTE WITHIN MINIMUM MEDIAN AND PEDESTRIAN REFUGE ISLANDS SHALL BE 5' MINIMUM.
- BEYOND THE BOTTOM GRADE BREAK A CLEAR SPACE OF 4' MINIMUM BY 4' MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE.



CURB RAMP CONFIGURATION: TYPE A

CURB RAMP CONFIGURATION: TYPE B

CURB RAMP CONFIGURATION: TYPE C



CURB RAMP CONFIGURATION: TYPE D

CURB RAMP CONFIGURATION: TYPE E

CURB RAMP CONFIGURATION: TYPE F

CURB RAMP CONFIGURATION: TYPE G

MID BLOCK CROSSING  
 THE CURB BEHIND THE TURNING SPACE AND RAMP IS NOT REQUIRED, BUT IS SUGGESTED FOR RETAINING SOIL AND PROVIDING VISUAL IMPROVEMENTS.

## Notice to Bidders No. 2229

MISSISSIPPI DEPARTMENT OF TRANSPORTATION <b>CURB RAMP</b>		DESIGN NUMBER <b>SDCCR-2</b>	SHEET NUMBER <b>2 OF 4</b>	COUNTY: <b>MADISON</b>	PROJ. NUM.: <b>SDCCR-2.DGN</b>	DATE
						DESIGN TEAM
BY	REVISION					



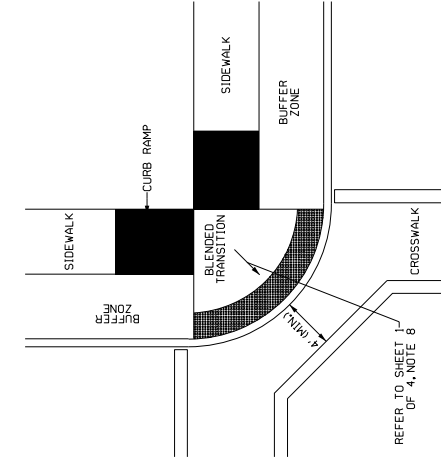
NOTE: REFER TO SHEET 4 OF 4 FOR DETAIL WORKING DIMENSIONS AND PLACEMENT ORIENTATION.

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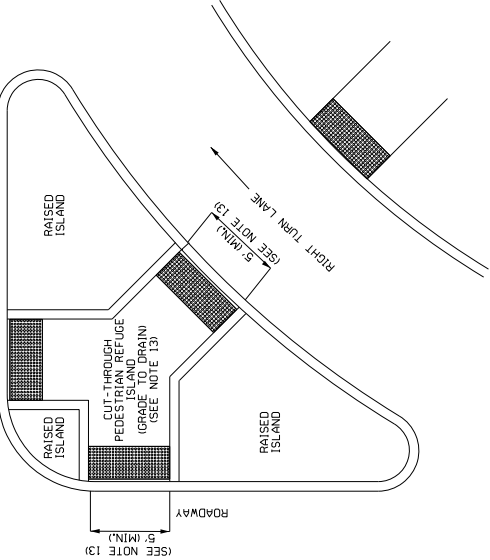
STATE	PROJECT NO.
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**NOTES:**

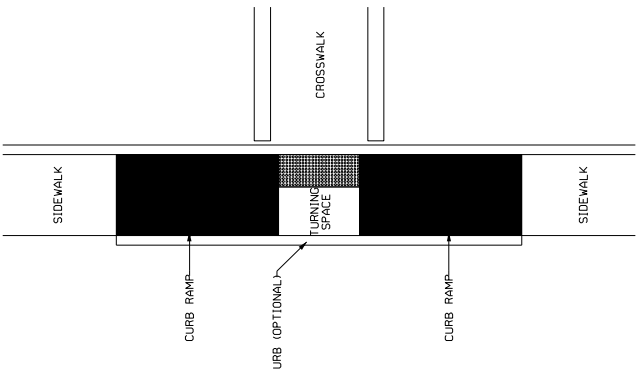
- FOR DIMENSIONS & GEOMETRIC VALUES REFER TO SHEET 1 OF 4.
- THE CONFIGURATIONS SHOWN GENERALLY REPRESENT THE INTENT TO PRESENT CURB RAMP DESIGN CONCEPTS. SITE CONDITIONS AT INDIVIDUAL LOCATIONS REQUIRE SPECIFIC DESIGNS. CURB RAMP LOCATIONS MUST BE CONSISTENT WITH THE PROVISIONS OF SHEETS 1, 2, AND 3 OF 4.
- COORDINATE TRAFFIC CONTROL DEVICES UTILITY LOCATIONS, SIGNS, STREET FURNITURE AND DRAINAGE TO ENSURE A CONTINUOUS PEDESTRIAN ACCESS ROUTE AT ALL CURB RAMP LOCATIONS. GUIDANCE FOR CROSSWALK AND TRAFFIC CONTROL DEVICES IS PROVIDED IN THE MUTCD.
- DETECTABLE MARKINGS SHOWN ON THIS SHEET ARE FOR ORIENTATION ONLY. FOR SPECIFIC PLACEMENT, ORIENTATIONS AND DIMENSIONS REFER TO SHEET 4 OF 4.
- THE CROSS SLOPE OF CURB RAMPS, BLENDED TRANSITIONS, AND TURNING SPACES SHALL BE 2% MAXIMUM (1.5% PREFERRED). AT PEDESTRIAN STREET CROSSINGS WITHOUT SIDEWALKS, THE CROSS SLOPE OF CURB RAMPS SHALL BE PERMITTED TO EQUAL THE STREET OR HIGHWAY GRADE.
- DIAGONAL CURB RAMPS ARE UNACCEPTABLE IN NEW CONSTRUCTION. THEY MAY BE USED FOR ALTERATIONS ONLY IF IT IS THE ONLY OPTION THAT WILL WORK.
- GRATES SHALL NOT BE LOCATED ON CURB RAMPS, BLENDED TRANSITIONS, TURNING SPACES, OR LANDINGS. ACCESS COVERS OF SIMILAR SURFACES SHALL COMPLY WITH APPLICABLE SURFACE REQUIREMENTS.
- UTILITIES, SIGNS, AND OTHER FIXED OBJECTS MAY NOT BE IN A MANNER THAT INTERFERES WITH THE USE OF THE CURB RAMP.
- THE SURFACE OF ALL CURB RAMPS SHALL BE STABLE, FIRM, AND SLIP RESISTANT TO A COARSE BROODY FINISH, RUNNING PERPENDICULAR TO THE SLOPE. IT IS RECOMMENDED TO USE CONCRETE SURFACES, EXCLUSIVE OF THE DETECTABLE MARKING FIELDS.
- THERE SHALL BE A CLEAR SPACE AT THE BOTTOM OF THE CURB RAMP. THE TURNING SPACE SHALL BE 4' MINIMUM. THE TURNING SPACE SHALL HAVE A MAXIMUM SLOPE OF 2% (1.5% PREFERRED).
- TURNING SPACES MAY OVERLAP WITH ADJACENT TURNING SPACES OR A SINGLE TURNING SPACE MAY SERVE MULTIPLE CURB RAMPS.
- TURNING SPACES MAY OVERLAP WITH THE CLEAR GROUND SPACE REQUIRED AT PEDESTRIAN SIGNAL PUSH BUTTONS.
- THE CLEAR WIDTH OF PEDESTRIAN ACCESS ROUTES WITHIN MEDIANS AND PEDESTRIAN REFUGE ISLANDS SHALL BE 5' MINIMUM.
- BEYOND THE BOTTOM GRADE BREAK, A CLEAR SPACE OF 4' MINIMUM BY 4' MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TURNING LANE.



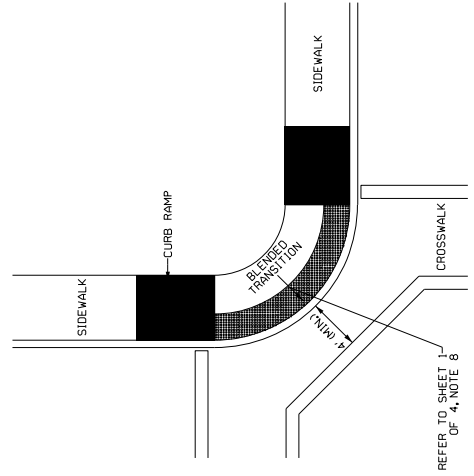
**CURB RAMP CONFIGURATION: TYPE J**  
NOT RECOMMENDED  
REFER TO NOTE 8



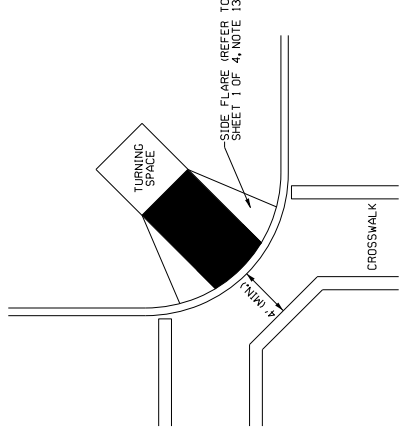
**CURB RAMP CONFIGURATION: TYPE I**  
NOT RECOMMENDED  
REFER TO NOTE 8



**CURB RAMP CONFIGURATION: TYPE H**  
NOT RECOMMENDED  
REFER TO NOTE 8



**CURB RAMP CONFIGURATION: TYPE K**  
NOT RECOMMENDED  
REFER TO NOTE 8



**CURB RAMP CONFIGURATION: TYPE L**  
NOT RECOMMENDED  
REFER TO NOTE 8

NOTE: REFER TO SHEET 4 OF 4 FOR DETAIL WORKING DIMENSIONS AND PLACEMENT ORIENTATION.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**CURB RAMPS**

**PLACEMENT DETAILS**

**SHEET 3 OF 4**

COUNTY: MADISON

PROJ. NUM.: SDCOR-3

FILENAME: SDCOR-3.DGN

DESIGN TEAM: \_\_\_\_\_

CHECKED: \_\_\_\_\_

DATE: \_\_\_\_\_

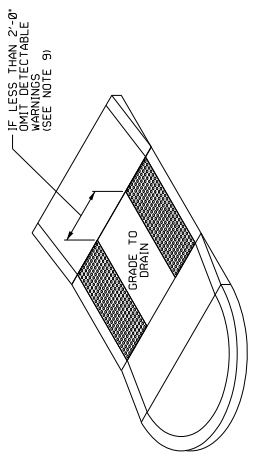
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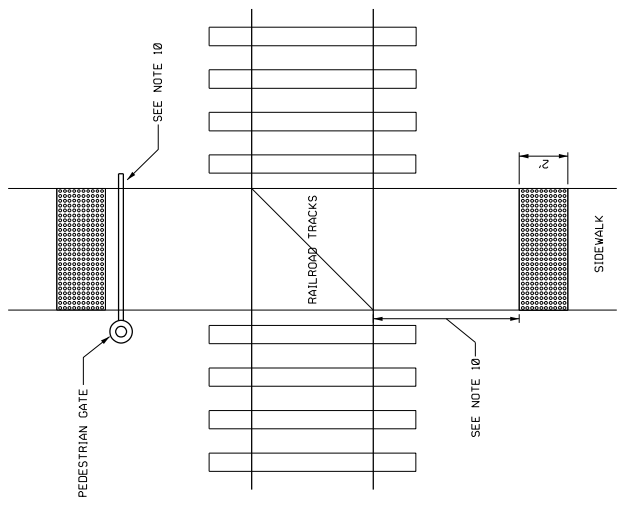
STATE	PROJECT NO.
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**NOTES:**  
 1. THE DETAILS PROVIDED ARE NOT DRAWN TO SCALE. THE QUANTITY OF DOMES DEPICTED ON THE DETECTABLE WARNING UNIT (THE DOMES AND THE ENTIRE 2' LEVEL SURFACE) IS FOR ILLUSTRATION ONLY.  
 2. ALL DETECTABLE WARNINGS SHOWN ON THIS SHEET SHALL BE ABSORBED IN OTHER PAY ITEMS BID.  
**DETECTABLE WARNING UNIT DIMENSIONS:**  
 3. DETECTABLE WARNING SURFACES SHALL EXTEND 2' MINIMUM IN THE DIRECTION OF PEDESTRIAN TRAVEL. AT CURB RAMPS AND BLENDED TRANSITIONS, DETECTABLE WARNING SURFACES SHALL EXTEND THE FULL LENGTH OF THE TRANSITION OR TURNING SPACE AT PEDESTRIAN AT-GRADE RAIL CROSSINGS NOT LOCATED WITHIN A STREET OR HIGHWAY. DETECTABLE WARNINGS SHALL EXTEND THE FULL WIDTH OF THE DETECTABLE WARNING SURFACES AT PEDESTRIAN AT-GRADE RAIL CROSSINGS. DETECTABLE WARNING SURFACES SHALL EXTEND THE FULL LENGTH OF THE PUBLIC USE AREAS OF THE PLATFORM, AT BOARDING AND TRANSIT STOPS FOR RAIL VEHICLES. DETECTABLE WARNING SURFACES SHALL EXTEND THE FULL LENGTH OF THE TRANSIT STOP.

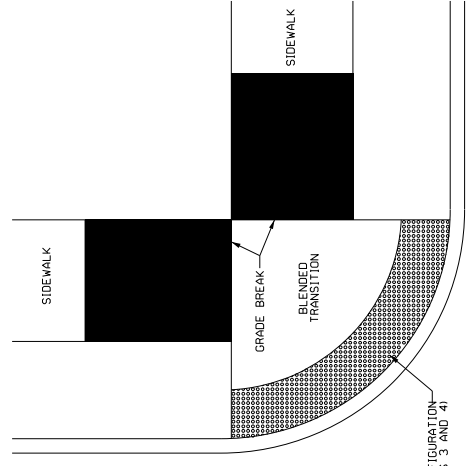
**DOME ALIGNMENT:**  
 4. THE ROWS OF DOMES SHALL BE ALIGNED TO BE PERPENDICULAR OR PARALLEL TO THE CURB BREAK AT THE RAMP LANDING OR BETWEEN THE CURB RAMP AND THE STREET.  
 5. WHERE DOMES ARE ARRANGED RADIALLY, THEY MAY DIFFER IN DOME SPACING AND ALIGNMENT TO MAINTAIN CENTER SPACING WITHIN THE RANGES SPECIFIED ON THIS SHEET.  
**COLOR REQUIREMENTS:**  
 6. DETECTABLE WARNING SURFACES SHALL CONTRAST VISUALLY WITH ADJACENT GUTTER, STREET OR HIGHWAY, OR PEDESTRIAN ACCESS ROUTE SURFACE, EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT.  
**DETECTABLE WARNING LOCATIONS:**  
 7. ON PERPENDICULAR CURB RAMPS, WHERE THE ENDS OF THE BOTTOM WARNING SURFACES ARE IN FRONT OF THE BACK OF CURB, DETECTABLE WARNING SURFACES SHALL BE PLACED AT THE BACK OF CURB, WHERE THE ENDS OF THE BOTTOM WARNING SURFACES ARE BEHIND THE BACK OF CURB AND THE DISTANCE FROM EITHER END OF THE BOTTOM GRADE BREAK TO THE BACK OF CURB IS 5' OR LESS. DETECTABLE WARNING SURFACES SHALL BE PLACED AT THE BACK OF CURB AND THE DISTANCE FROM EITHER END OF THE BOTTOM GRADE BREAK TO THE BACK OF CURB IS 5' OR LESS. DETECTABLE WARNING SURFACES SHALL BE PLACED ON EITHER END OF THE BOTTOM GRADE BREAK AND THE DISTANCE FROM EITHER END OF THE BOTTOM GRADE BREAK TO THE BACK OF CURB SHALL BE PLACED ON THE LOWER LANDING AT THE BACK OF CURB.  
 8. ON PARALLEL CURB RAMPS, DETECTABLE WARNING SURFACES SHALL BE PLACED ON THE LOWER LANDING AT THE FLUSH TRANSITION BETWEEN THE STREET AND SIDEWALK.  
 9. ON BLENDED TRANSITIONS, DETECTABLE WARNING SURFACES SHALL BE PLACED AT THE BACK OF CURB, WHERE RAISED PEDESTRIAN STREET CROSSINGS, DEPRESSIONS, OR OTHER LEVEL PEDESTRIAN STREET CROSSINGS ARE PROVIDED. DETECTABLE WARNING SURFACES SHALL BE PLACED AT THE FLUSH TRANSITION BETWEEN THE STREET AND THE SIDEWALK.  
 10. AT CUT-THROUGH PEDESTRIAN REFUGE ISLANDS, DETECTABLE WARNING SURFACES SHALL BE PLACED AT THE EDGES OF THE PEDESTRIAN SURFACE WITHOUT DETECTABLE WARNINGS.  
 11. AT PEDESTRIAN AT-GRADE RAIL CROSSINGS NOT LOCATED WITHIN A STREET OR HIGHWAY, DETECTABLE WARNING SURFACES SHALL BE PLACED AT THE BACK OF CURB, WHERE RAISED PEDESTRIAN STREET CROSSINGS, DEPRESSIONS, OR OTHER LEVEL PEDESTRIAN STREET CROSSINGS ARE PROVIDED. DETECTABLE WARNING SURFACES SHALL BE PLACED AT THE FLUSH TRANSITION BETWEEN THE STREET AND THE SIDEWALK.  
 12. AT BOARDING PLATFORMS FOR BUSES AND RAIL VEHICLES, DETECTABLE WARNING SURFACES SHALL BE PLACED AT THE BOARDING EDGE OF THE PLATFORM.  
 13. AT BOARDING AND ALIGHTING AREAS AT SIDEWALK OR STREET LEVEL, TRANSIT STOPS FOR RAIL VEHICLES, DETECTABLE WARNING SURFACES SHALL BE PLACED AT THE SIDE OF THE BOARDING AND ALIGHTING AREA FACING THE RAIL VEHICLES.



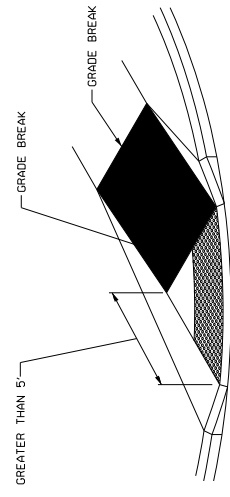
DETECTABLE WARNINGS AT MEDIAN ISLANDS  
NON-ELEVATED CROSSING



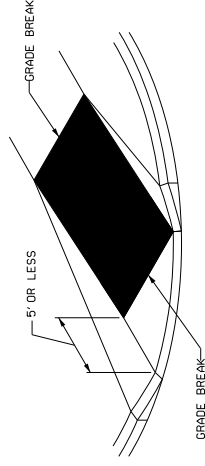
DETECTABLE WARNINGS AT RAILROAD CROSSING



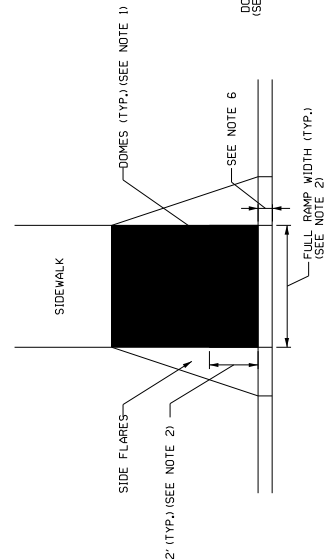
DETECTABLE WARNING AT BLENDED TRANSITION (CONFIGURATION: TYPES K AND J)



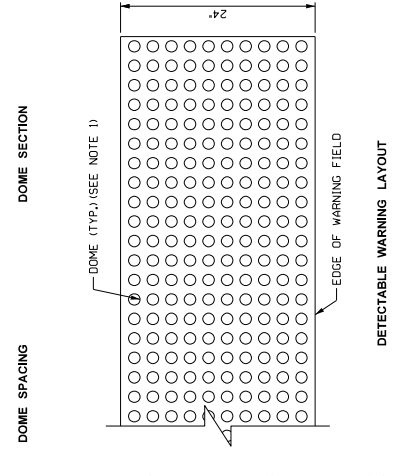
DETECTABLE WARNING PLACEMENT DETAIL 1  
 NOTE: IF THE DISTANCE FROM THE GRADE BREAK IS LESS THAN OR EQUAL TO 5', DETECTABLE WARNINGS SHALL BE PLACED ALONG THE RADIUS OF THE CURVE AS SHOWN IN THE ABOVE DETAIL.



DETECTABLE WARNING PLACEMENT DETAIL 2  
 NOTE: IF THE DISTANCE FROM THE GRADE BREAK IS LESS THAN OR EQUAL TO 5', DETECTABLE WARNINGS SHALL BE PLACED ALONG THE RADIUS OF THE CURVE AS SHOWN IN THE ABOVE DETAIL.



DETECTABLE WARNING AT CURB RAMP



DETECTABLE WARNING LAYOUT

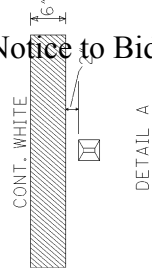
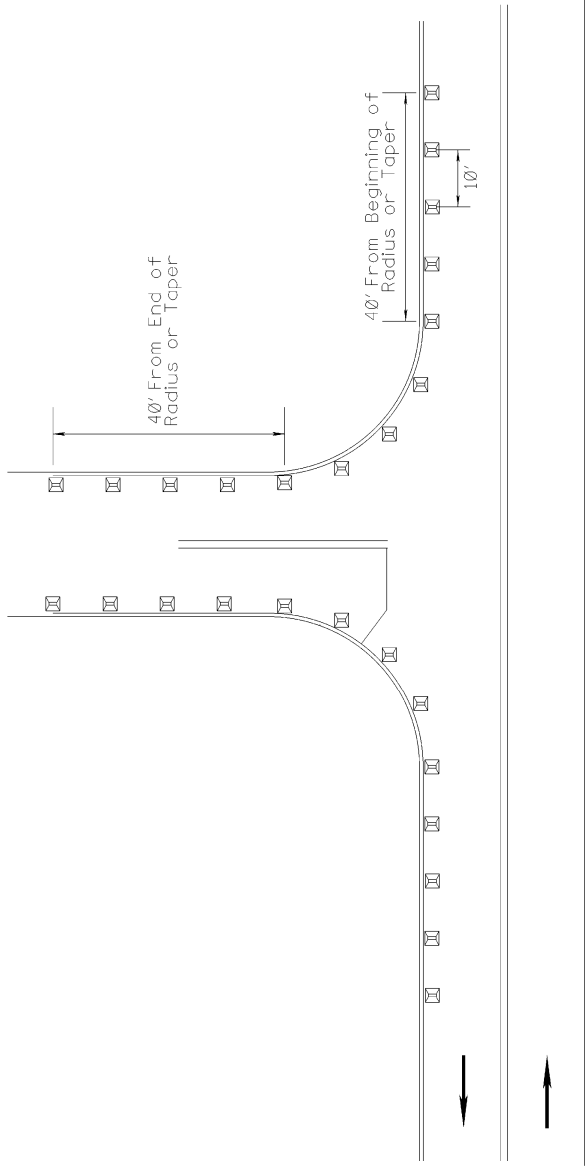


MISSISSIPPI DEPARTMENT OF TRANSPORTATION  
**CURB RAMPS**  
 DETECTABLE WARNING  
 DETAILS  
 SHEET 4 OF 4  
 COUNTY: MADISON  
 PROJ. NUM.:  
 FILENAME: SDCCR-4.DGN  
 DESIGN TEAM  
 CHECKED  
 DATE

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WORKS NUMBER: SDCCR-4  
 SHEET NUMBER: 4  
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# TYPICAL FOR RAISED PAVEMENT MARKERS PLACED ON SIDE ROAD RADIUS 2-LANE, 2-WAY TRAFFIC



→ DIRECTION OF TRAFFIC

- NOTE 1: MARKERS SHALL BE PLACED EVERY 10 FEET.
- NOTE 2: MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
- NOTE 3: MARKERS SHALL BE HIGH PERFORMANCE TWO WAY CLEAR.
- NOTE 4: FIVE (5) MARKERS SHALL BE PLACED ALONG MAINLINE EDGE STRIPE.
- NOTE 5: MARKERS FOR COUNTY ROADS SHALL CONTINUE DOWN THE EDGE STRIPE A DISTANCE OF 40 FEET.
- NOTE 6: MARKERS SHALL NOT BE ROTATED WHEN BEING PLACED ALONG RADIUS OF LOCAL ROAD.

STATE	PROJECT NO.
MISS.	STP-0213-00(026)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
BY	DATE
REVISION	DATE
2-LANE, 2-WAY	
PRELIMINARY	
NOT FOR CONSTRUCTION	
2-WAY CLEAR RAISED PAVEMENT MARKERS PLACED ON SIDE ROADS	
PROJECT NO.	FILENAME: SPASIDERDRP.MDGN
COUNTY :	DESIGN TEAM
CLIP NUMBER	CHECKED
SHEET NUMBER	DATE