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SM No. CSTP9999033731

PROPOSAL AND CONTRACT DOCUMENTS

FOR THE CONSTRUCTION OF

07

Bridge Repairs on SR 28 (Bridge Nos. 40.9 & 43.0) & US 49 W (Bridge Nos. 255.2 & 255.2B), known as Federal Aid Project Nos. STP-9999-03(373) / 108401301 & STP-9999-03(374) / 108401302 in Copiah & Sunflower Counties.

Project Completion: 105 Working Days

(STATE DELEGATED)

NOTICE

**BIDDERS MUST COMPLETE AN ONLINE REQUEST
FOR PERMISSION TO BID THIS PROJECT.**

Electronic addendum updates will be posted on www.gomdot.com

SECTION 900

OF THE CURRENT 2017 STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION

JACKSON, MISSISSIPPI

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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PROJECT: STP-9999-03(373)/108401301 - Copiah
STP-9999-03(374)/108401302 - Sunflower

SAM.GOV Registration and DUNS Number

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(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

08/26/2020 10:33 AM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 901 - ADVERTISEMENT

Electronic bids will be received by the Mississippi Transportation Commission at 10:00 o'clock A.M., Tuesday, September 22, 2020, from the Bid Express Service and shortly thereafter publicly read on the Sixth Floor For:

Bridge Repairs on SR 28 (Bridge Nos. 40.9 & 43.0) & US 49 W (Bridge Nos. 255.2 & 255.2B), known as Federal Aid Project Nos. STP-9999-03(373) / 108401301 & STP-9999-03(374) / 108401302 in Copiah & Sunflower Counties.

The attention of bidders is directed to the Contract Provisions governing selection and employment of labor. Minimum wage rates have been predetermined by the Secretary of Labor and are subject to Public Law 87-581, Work Hours Act of 1962, as set forth in the Contract Provisions.

The Mississippi Department of Transportation hereby notifies all bidders that it will affirmatively insure that in any contract entered into pursuant to this advertisement, disadvantaged business enterprises will be afforded full opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of race, color, sex, age, disability, religion or national origin in consideration for an award.

The award of this contract will be contingent upon the Contractor satisfying the DBE requirements.

The specifications are on file in the offices of the Mississippi Department of Transportation.

Contractors may request permission to bid online at <http://shopmdot.ms.gov> at no cost. Upon approval, Contractors shall be eligible to submit a bid using Bid Express at <http://bidx.com>. Specimen proposals may be viewed and downloaded online at no cost at <http://mdot.ms.gov> or purchased online at <http://shopmdot.ms.gov> at a cost of Ten Dollars (\$10.00) per proposal plus a small convenience fee. Cash or checks will not be accepted as payment.

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

MELINDA L. MCGRATH
EXECUTIVE DIRECTOR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Governing Specifications

The current (2017) Edition of the Standard Specifications for Road and Bridge Construction adopted by the Mississippi Transportation Commission is made a part hereof fully and completely as if it were attached hereto, except where superseded by special provisions, or amended by revisions of the Specifications contained within this proposal. Copies of the specification book may be purchased from the MDOT Construction Division, or online at shopmdot/default.aspx?StoreIndex=1.

A reference in any contract document to controlling requirements in another portion of the contract documents shall be understood to apply equally to any revision or amendment thereof included in the contract.

In the event the plans or proposal contain references to the 2004 Edition of the Standard Specifications for Road and Bridge Construction, it is to be understood that such references shall mean the comparable provisions of the 2017 Edition of the Standard Specifications.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Status of Right-of-Way

Although it is desirable to have acquired all rights-of-way and completed all railroad agreements, utility adjustments and work to be performed by others prior to receiving bids, sometimes it is not considered to be in the public interest to wait until each and every such clearance has been obtained. The bidder is hereby advised of possible unacquired rights-of-way, relocations, railroad agreements and utilities adjustments which have not been completed.

The status of right-of-way acquisition, utility adjustments, encroachments, potentially contaminated sites, railroad facilities, improvements, and asbestos contamination are set forth in the following attachments.

In the event right of entry is not available to ALL parcels of right-of-way and/or all work that is to be accomplished by others on the date set forth in the contract for the Notice to Proceed is not complete, the Department will issue a restricted Notice to Proceed.

STATUS OF RIGHT-OF-WAY

STP-9999-03(373)

108401/301000

Copiah County

All rights of way and legal rights of entry have been acquired **except:**

None.

ASBESTOS CONTAMINATION STATUS OF BUILDINGS
TO BE REMOVED BY THE CONTRACTOR
STP-9999-03(373)
108401/301000
Copiah County
July 14, 2020

Reference is made to notices to bidders entitled "Asbestos National Emission Standards for Hazardous Air Pollutants (NESHAP)" and "Removal of Obstructions".

The following pertinent information is furnished concerning asbestos containing materials (ACMs), if any, found in buildings to be removed by the Contractor.

There is no Right of Way required for this project. There are no buildings to be removed by the contractor.

STATUS OF POTENTIALLY CONTAMINATED SITES

STP-9999-03(373)

108401/301000

Copiah County

July 14, 2020

THERE IS NO RIGHT OF WAY REQUIRED FOR THIS PROJECT. NO INITIAL SITE ASSESSMENT WILL BE PERFORMED. IF CONTAMINATION ON EXISTING RIGHT OF WAY IS DISCOVERED, IT WILL BE HANDLED BY THE DEPARTMENT.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

Inter-Departmental Memorandum

To: Right of Way Division
Trudi Loflin

Date: July 13th, 2020

From: Pre-Construction Engineer District 3
Jarrett Taylor

Project No: 108401/301000
STP-9999-03(373)

County: Copiah

DISTRICT STATUS REPORT

1. **STATUS OF RIGHT OF WAY:** All work to be done within existing ROW.
2. **RIGHT OF WAY CLEARANCE:** There are no encroachments.
3. **STATUS OF AFFECTED RAILROAD OPERATING FACILITIES:** None affected.
4. **STATUS OF REQUIRED UTILITY RELOCATIONS:** No utility conflicts.
5. **STATUS OF CONSTRUCTION AGREEMENT:** None required.

JT:jt

pc: Eric Morgan (District Three)
Katherine Lamey (Construction Division)
Vicki Shows (Right of Way)
File

Improvements to be included in Notice to Bidders to be removed by the Construction Contractor
FMS Construction Project No: 108401-301000
External ROW No: STP-9999-03(373)

Parcel No:
Station No:
Property Owner:
Description/Pictures:

NA

STATUS OF RIGHT-OF-WAY

STP-9999-03(374)

108401/302000

Sunflower County

All rights of way and legal rights of entry have been acquired **except:**

None.

ASBESTOS CONTAMINATION STATUS OF BUILDINGS
TO BE REMOVED BY THE CONTRACTOR
STP-9999-03(374)
108401/302000
Sunflower County
July 14, 2020

Reference is made to notices to bidders entitled "Asbestos National Emission Standards for Hazardous Air Pollutants (NESHAP)" and "Removal of Obstructions".

The following pertinent information is furnished concerning asbestos containing materials (ACMs), if any, found in buildings to be removed by the Contractor.

There is no Right of Way required for this project. There are no buildings to be removed by the contractor.

STATUS OF POTENTIALLY CONTAMINATED SITES
STP-9999-03(374)
108401/302000
Sunflower County
July 14, 2020

THERE IS NO RIGHT OF WAY REQUIRED FOR THIS PROJECT. NO INITIAL SITE ASSESSMENT WILL BE PERFORMED. IF CONTAMINATION ON EXISTING RIGHT OF WAY IS DISCOVERED, IT WILL BE HANDLED BY THE DEPARTMENT.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

Inter-Departmental Memorandum

To:	Right of Way Division Trudi Loflin	Date:	July 13 th , 2020
From:	Pre-Construction Engineer District 3 Jarrett Taylor	Project No:	108401/302000 STP-9999-03(374)
		County:	Sunflower

DISTRICT STATUS REPORT

1. **STATUS OF RIGHT OF WAY:** All work to be done within existing ROW.
2. **RIGHT OF WAY CLEARANCE:** There are no encroachments.
3. **STATUS OF AFFECTED RAILROAD OPERATING FACILITIES:** None affected.
4. **STATUS OF REQUIRED UTILITY RELOCATIONS:** No utility conflicts.
5. **STATUS OF CONSTRUCTION AGREEMENT:** None required.

JT:jt

pc: Eric Morgan (District Three)
 Katherine Lamey (Construction Division)
 Vicki Shows (Right of Way)
 File

Improvements to be included in Notice to Bidders to be removed by the Construction Contractor
FMS Construction Project No: 108401-302000
External ROW No: STP-9999-03(374)

Parcel No:
Station No:
Property Owner:
Description/Pictures:

NA

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 3

CODE: (SP)

DATE: 01/17/2017

SUBJECT: Final Clean-Up

Immediately prior to final inspection for release of maintenance, the Contractor shall pick up, load, transport and properly dispose of all litter from the entire highway right-of-way that is within the termini of the project.

Litter shall include, but not be limited to, solid wastes such as glass, paper products, tires, wood products, metal, synthetic materials and other miscellaneous debris.

Litter removal is considered incidental to other items of work and will not be measured for separate payment.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 9

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Federal Bridge Formula

Bidders are hereby advised that the latest revision of Federal Highway Administration Publication No. FHWA-HOP-06-105, **BRIDGE FORMULA WEIGHTS**, dated August 2006, is made a part of this contract when applicable.

Prior to the preconstruction conference, the Contractor shall advise the Engineer, in writing, what materials, if any, will be delivered to the jobsite via Interstate route(s).

Copies of the **BRIDGE FORMULA WEIGHTS** publication may be obtained by contacting:

Federal Highway Administration
400 7th Street, SW
Washington, DC 20590
(202) 366-2212

or

http://www.ops.fhwa.dot.gov/Freight/publications/brdg_frm_wgths/bridge_formula_all_rev.pdf

An on line **BRIDGE FORMULA WEIGHTS CALCULATOR** is available at

http://ops.fhwa.dot.gov/freight/sw/brdgcalc/calc_page.htm

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 296

CODE: (SP)

DATE: 07/25/2017

SUBJECT: Reduced Speed Limit Signs

Bidders are advised that when the plans or contract documents require the speed limit on a project to be reduced, the Contractor shall begin work within 48 hours of installing the reduced speed limit signs. Should the Contractor not start work or have no plans to start work within 48 hours of installing the signs, the reduced speed limit signs shall be covered and existing speed limit signs uncovered.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 401

CODE: (SP)

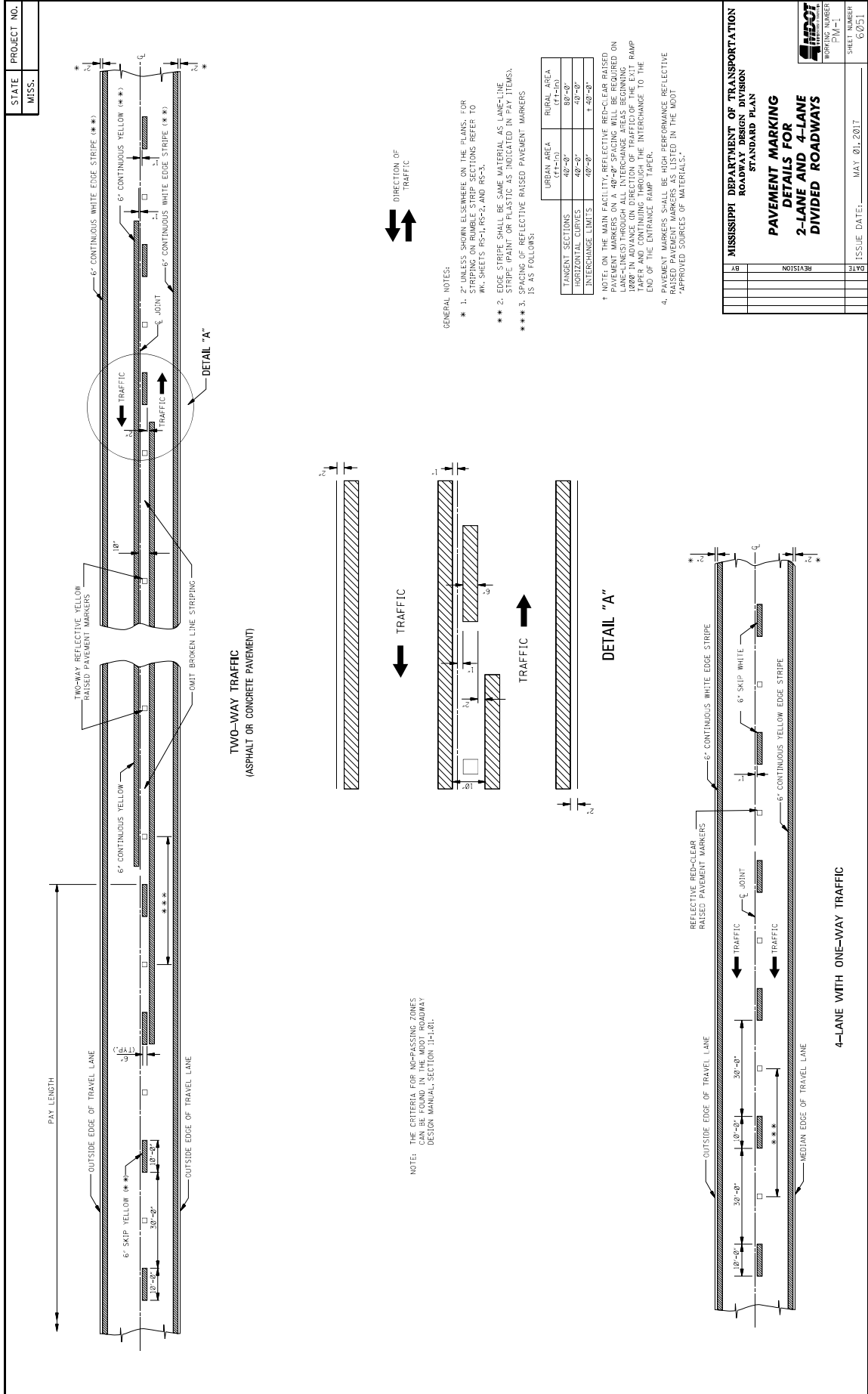
DATE: 09/12/2017

SUBJECT: Standard Drawings

Standard Drawings attached hereto shall govern appropriate items of required work.

Larger copies of Standard Drawings may be purchased from:

MDOT Plans Print Shop
MDOT Shop Complex, Building C, Room 114
2567 North West Street
P.O. Box 1850
Jackson, MS 39215-1850
Telephone: (601) 359-7460
or FAX: (601) 359-7461
or e-mail: plans@mdot.state.ms.us



MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
BY	
REVISION	
DATE	
ISSUE DATE:	MAY 01, 2017
SHEET NUMBER	G031



**PAVEMENT MARKING
DETAILS FOR
2-LANE AND 4-LANE
DIVIDED ROADWAYS**

STATE MISS.	PROJECT NO.	
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TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 3-LANE SECTION

TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION

DETAIL A
LATERAL PLACEMENT OF PAVEMENT MARKERS

GENERAL NOTE:
1. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS LISTED IN THE MOST APPROVED SOURCE OF MATERIALS.

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN**

**PAVEMENT MARKING
DETAILS FOR 3-LANE
4-LANE AND 3-LANE
UNDIVIDED ROADWAYS**

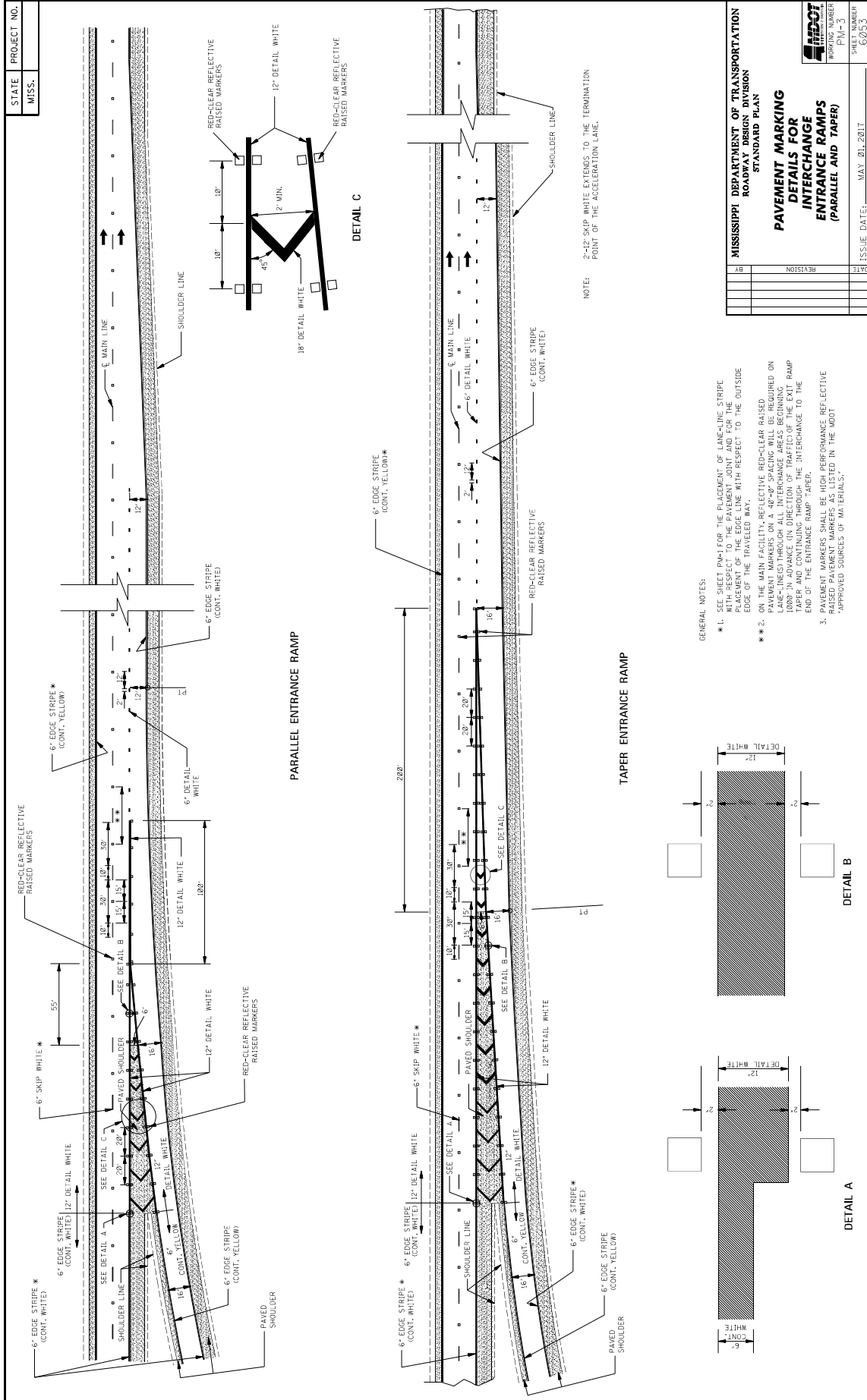
DETAIL B
LATERAL PLACEMENT OF PAVEMENT MARKERS

DATE

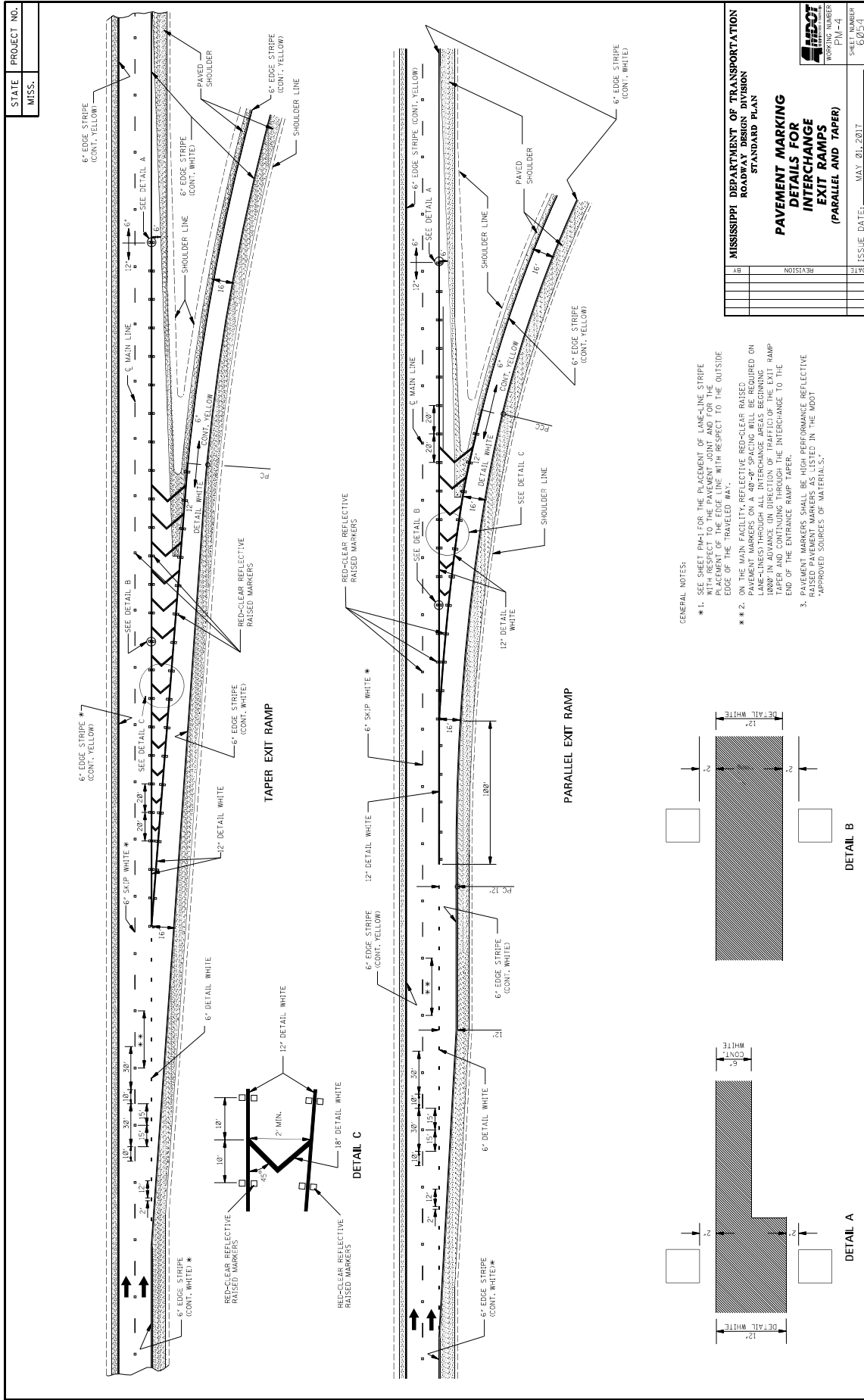
ISSUE DATE: MAY 21, 2017

SHEET NUMBER
PM-2

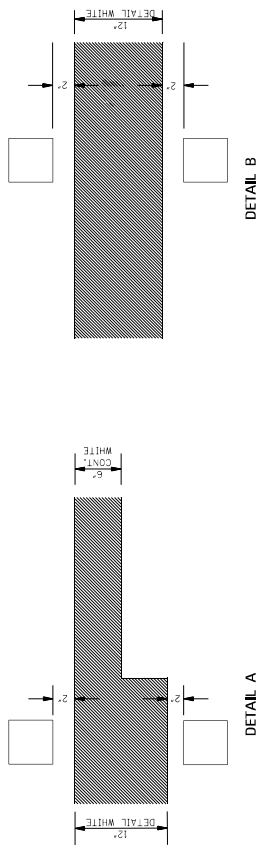
PROJECT NUMBER
60352



STATE	PROJECT NO.
MISS.	

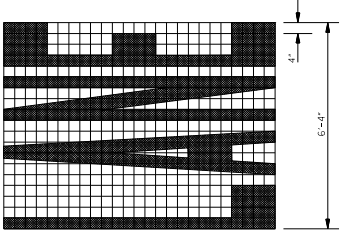
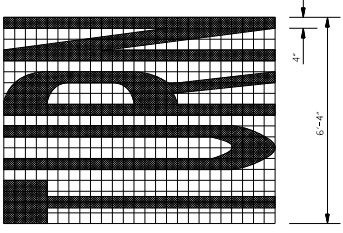
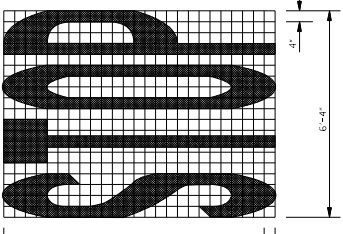
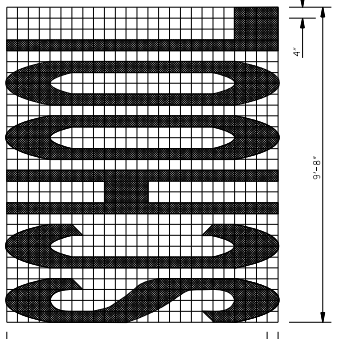
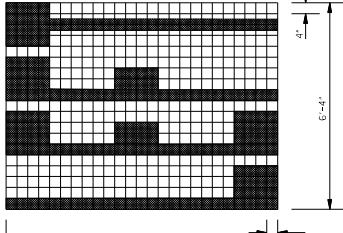
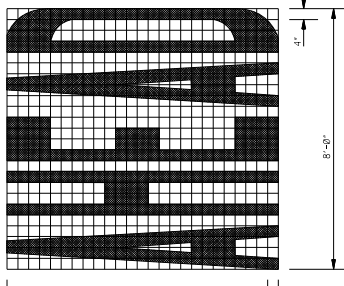
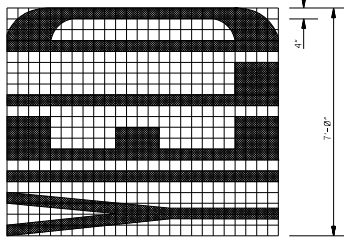
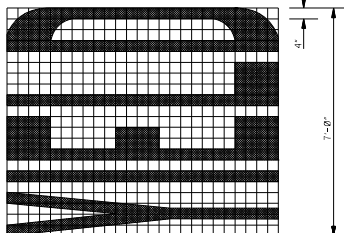
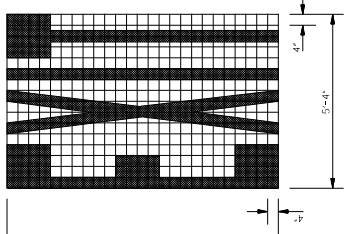
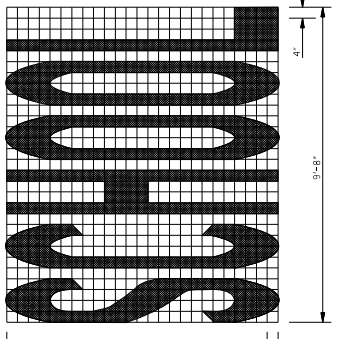


- GENERAL NOTES:
- ** 1. SEE SHEET PMA-1 FOR THE PLACEMENT OF LANE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACEMENT OF PAVED SHOULDER WITH RESPECT TO THE EDGE OF MAIN CASTER REFLECTIVE BEAD-BUS RAISES.
 - ** 2. PAVEMENT MARKERS ON A 40'-50' SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - 3. RAISED PAVEMENT MARKERS BE HIGH PERFORMANCE REFLECTIVE 'APPROVED SOURCES OF MATERIALS.'



MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
PAVEMENT MARKING DETAILS FOR INTERCHANGE EXIT RAMPS (PARALLEL AND TAPER)	
WORKING NUMBER	SHEET NUMBER
PM-4	602/5-1
DATE	ISSUE DATE:
	MAY 20, 2017

STATE MISS.	PROJECT NO.								
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GENERAL NOTES:

- TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTIONS) OF 1/2" LESS THAN THE STRETCHING LETTERS SHALL BE SHOWN IN THE LETTERS.
- FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

LEGEND	AREA (FT ²)
STOP	24.6
RIGHT	28.6
LEFT	19.5
TRAFFIC	22.2
AHEAD	32.3
YIELD	26.8
EXIT	18.5
SIGNAL	32.5
SCHOOL	35.5

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
LEGEND DETAILS**

DATE	REVISION	BY							
ISSUE DATE:	MAY 01, 2017								
SHEET NUMBER	PM-5								
WORKING NUMBER	6035								

STATE MISS.	PROJECT NO.	
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THRU ARROW

TURN ARROW

LANE-REDUCTION ARROW

COMBINATION ARROW

ONLY

YIELD LINE

1-WAY ARROW

GENERAL NOTES:

- TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTORS OF 1/16" OR LESS AND EXTENDING THE FULL WIDTH) ARE PERMITTED IN EACH LETTER.
- FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- DIMENSIONS OF THE YIELD LINE MAY VARY WITH APPROVAL OF THE ENGINEER. SEE MUTCD, LATEST EDITION, FOR ALLOWABLE DIMENSIONS.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

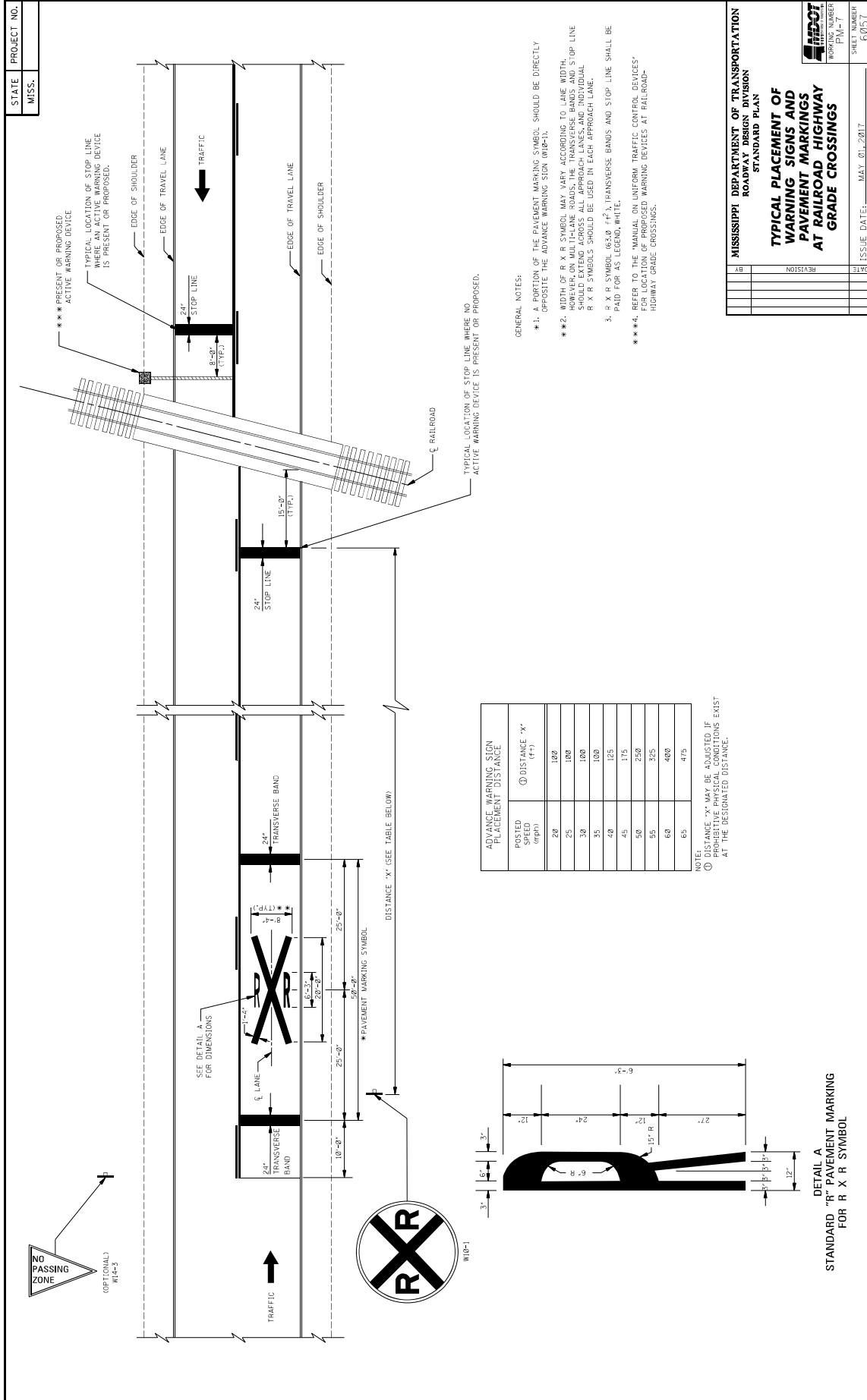
PAY QUANTITIES	
LEGEND/SYMBOL	AREA (FT ²)
ONLY	22.0
TURN ARROW	16.4
THRU ARROW	12.3
COMB. ARROW	27.5
1-WAY ARROW	24.3
LANE REDUCTION ARROW	40.0

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
LEGEND DETAILS**

SHEET NUMBER
PM-6
60/56

ISSUE DATE: MAY 01, 2017



- GENERAL NOTES:
- ** 1. A PORTION OF THE PAVEMENT MARKING SYMBOL SHOULD BE DIRECTLY OPPOSITE THE ADVANCE WARNING SIGN (W10-1).
 - ** 2. WIDTH OF R X R SYMBOL MAY VARY ACCORDING TO LANE WIDTH. SYMBOL SHOULD EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.
 - 3. R X R SYMBOL (63.8 #1) TRANSVERSE BANDS AND STOP LINE SHALL BE PAID FOR AS LEGEND WHITE.
 - ** 4. REFER TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR LOCATION OF PROPOSED WARNING DEVICES AT RAILROAD-HIGHWAY GRADE CROSSINGS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TYPICAL PLACEMENT OF WARNING SIGNS AND PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSINGS

WORKING NUMBER: PM-1

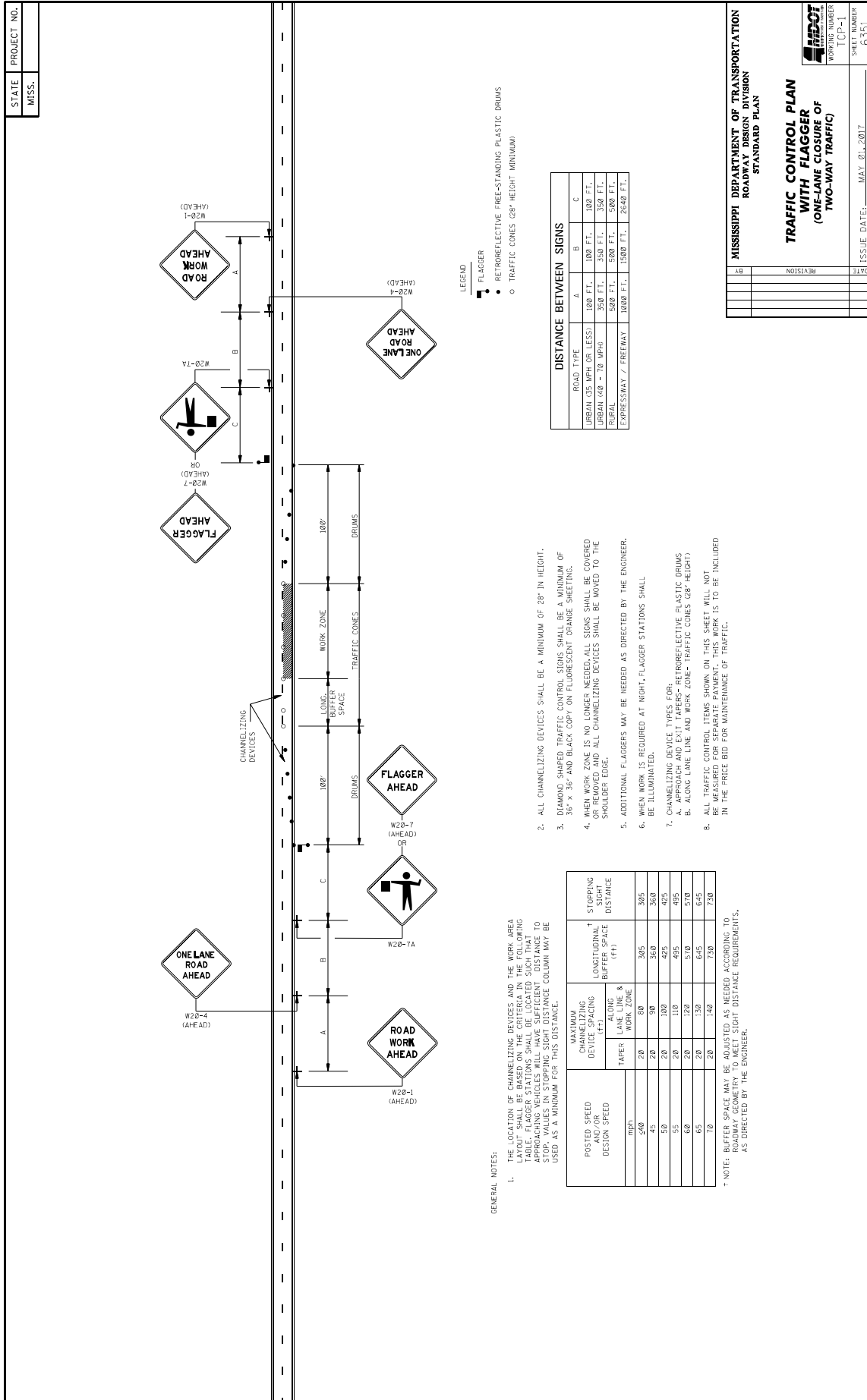
SHEET NUMBER: 6031

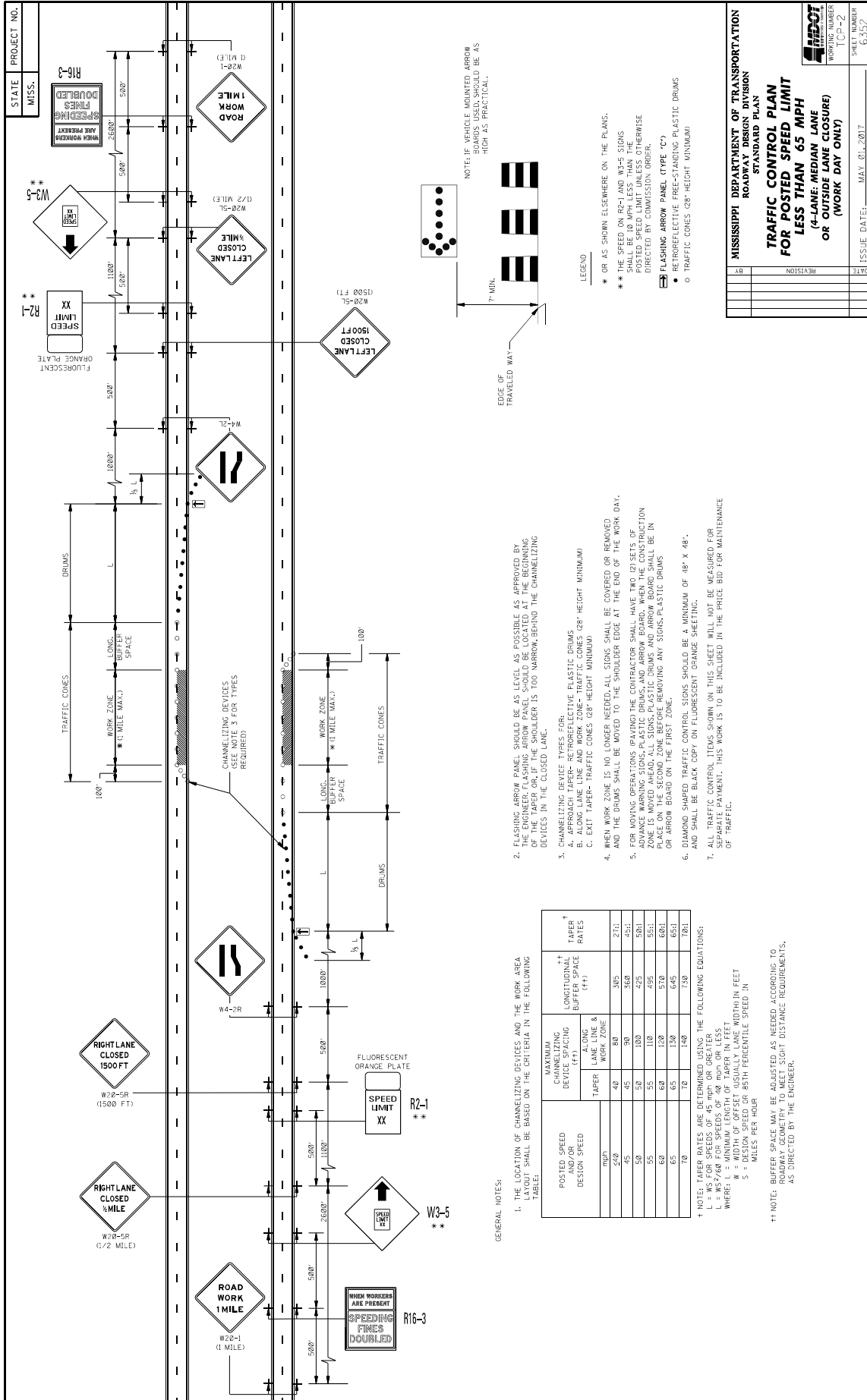
ISSUE DATE: MAY 01, 2017

DATE: _____

BY: _____

REVISION: _____





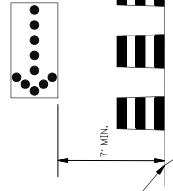
GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:
- | POSTED SPEED AND/OR DESIGN SPEED | MAXIMUM CHANNELIZING DEVICE SPACING (FT) | | LONGITUDINAL BUFFER SPACE (FT) | TAPER RATES |
|----------------------------------|--|-------|--------------------------------|-------------|
| | LANE LINE & WORK ZONE | TAPER | | |
| 45 | 40 | 80 | 305 | 27:1 |
| 50 | 45 | 90 | 350 | 45:1 |
| 55 | 50 | 100 | 425 | 50:1 |
| 60 | 55 | 110 | 495 | 55:1 |
| 65 | 60 | 120 | 570 | 60:1 |
| 70 | 65 | 130 | 645 | 65:1 |
| 75 | 70 | 140 | 730 | 70:1 |
- † NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 L = WS FOR SPEEDS OF 45 MPH OR GREATER
 L = 0.5WS FOR SPEEDS OF 50 MPH OR GREATER
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET (USUALLY LANE WIDTH IN FEET)
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
- †† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. THE PANEL SHOULD BE LOCATED AT THE END OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICES TYPES FOR:
 - A. APPROACH TAPER- RETROREFLECTIVE PLASTIC DRUMS
 - B. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT MINIMUM)
 - C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT MINIMUM)
4. WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED.
5. FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
6. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48" AND SHALL BE BLACK COPY ON FLOURESCENT ORANGE SHEETING.
7. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

LEGEND

- * OR AS SHOWN ELSEWHERE ON THE PLANS.
- ** THE SPEED ON R2-1 AND W3-5 SIGNS SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT UNLESS OTHERWISE DIRECTED BY COMMISSION ORDER.
- ▭ FLASHING ARROW PANEL (TYPE 'C')
- RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
- TRAFFIC CONES (28" HEIGHT MINIMUM)



STATE PROJECT NO. MISS. R16-3

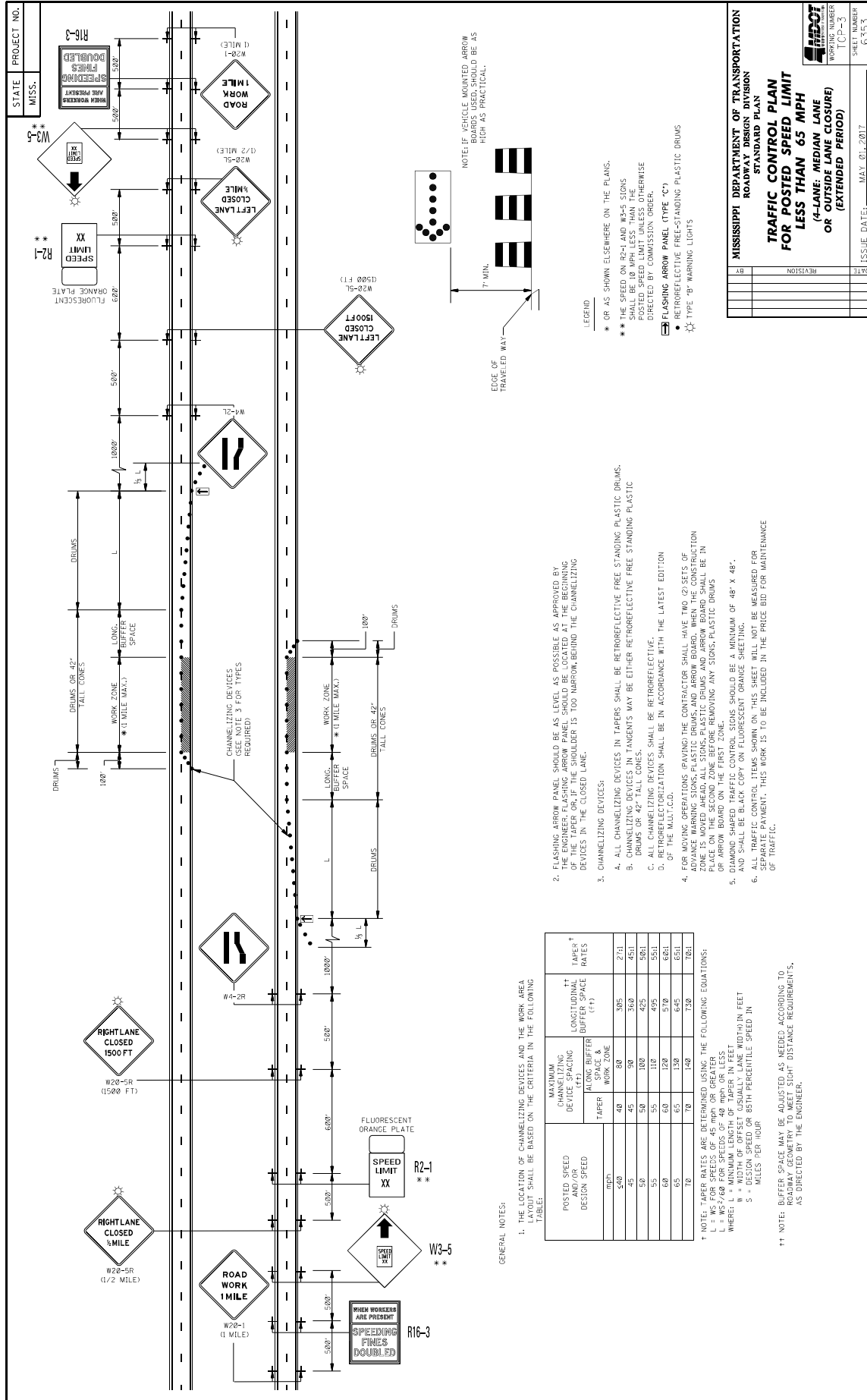
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT LESS THAN 65 MPH (4-LANE, MEDIAN LANE OR OUTSIDE LANE CLOSURE) OR (WORK DAY ONLY)

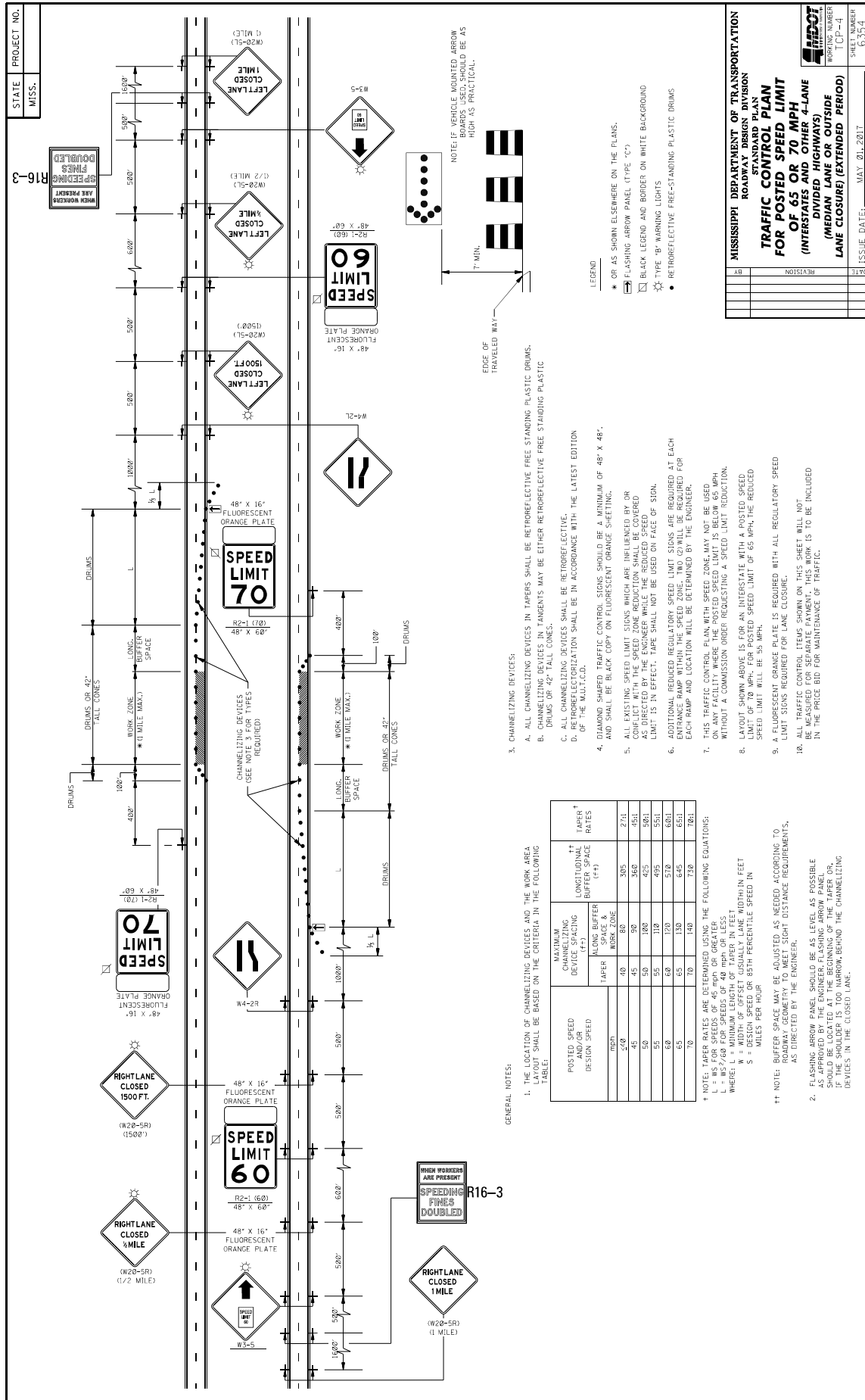
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

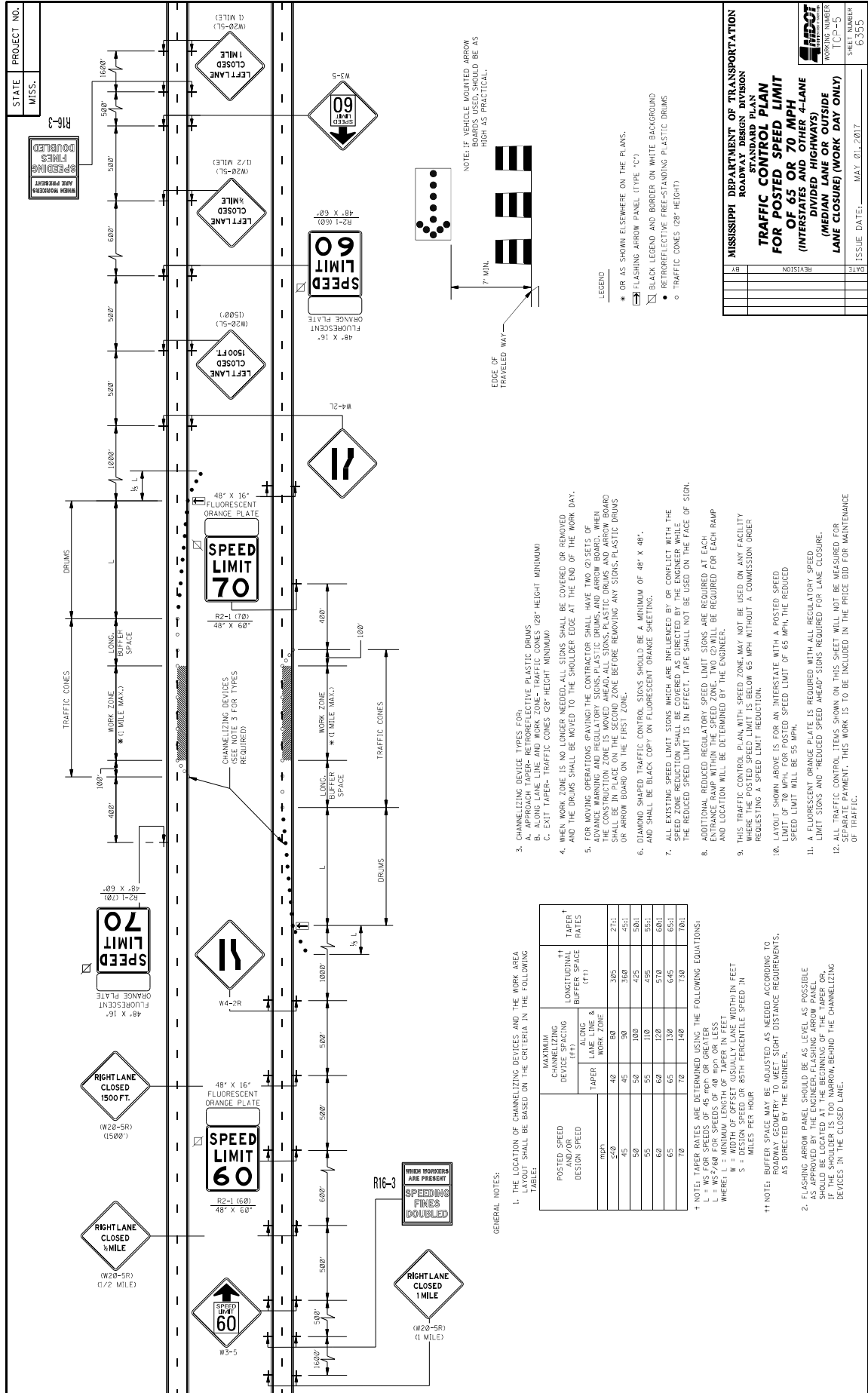
ISSUE DATE: MAY 01, 2017

WORKING NUMBER: T1P-2

SHEET NUMBER: 6032







STATE PROJECT NO.
MISS.

WORKING NUMBER
TCP-6
G350

WING BARRICADES

- WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER OF ROADWAYS OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
- WING BARRICADES SHOULD BE USED:
 - IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.

BARRICADE CLOSING A ROAD

BARRICADE CHARACTERISTICS

	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE **	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF RETROREFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS

* 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
 ** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 IN² OF REFLECTIVE AREA FACING TRAFFIC.

STANDARD BARRICADES

- THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).
- RAIL STRIPE SHALL BE 6 INCHES, EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.
- DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
- FOR ADDITIONAL INFORMATION OR DETAILS, SEE MUTCD, LATEST EDITION.
- BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES WHICH REQUIRE SUCCESSFUL CRASH TESTING. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE: http://safety.fhwa.dot.gov/roadway_dept/policy_guidance/road_hardware/cat2.cfm

CHEVRON SIGN DETAIL

- A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
- THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.
- CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.

PLASTIC DRUM STRIPING DETAIL

- PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDITED METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH THE MARKING OF THE ROADWAY. THE DRUMS SHALL BE PREPARED WITH RETROREFLECTIVE STRIPES 12 ORANGE & 2 WHITE 6" WIDE.
- DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
- WHERE PRACTICAL PLASTIC DRUMS SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

TYPE 3 OBJECT MARKER (OM-3R)

- TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
- THE OM-3R IS SHOWN. THE OM-3L IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
- THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.

HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

REVISION

DATE

ISSUE DATE: MAY 01, 2017

STATE MISS.	PROJECT NO.	
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MOBILE OPERATIONS ON MULTILANE ROAD

MOBILE OPERATIONS ON MULTILANE ROAD

MOBILE OPERATIONS ON TWO-LANE ROAD

MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES FOR MULTILANE LANE OPERATION:

- VEHICLES USED FOR THESE OPERATIONS SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT, SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLASERS, SIGNS, OR ARROW PANELS.
- SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK MOUNTED ATTENUATOR (TMA), AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE LEGIBLY DISPLAYED ON SHADOW VEHICLE 2 SO AS NOT TO OBSCURE THE ARROW PANEL.
- SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK-MOUNTED ATTENUATOR (TMA).
- SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
- WHEN ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, SHADOW VEHICLE 2 SHOULD BE ELIMINATED.
- ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE SHOULD BE USED (I.e., VEHICLE 3 ON THE SHOULDER OF PRACTICALLY, VEHICLE 2 IN THE CLOSED LANE, AND VEHICLE 1 IN THE CLOSED LANE).
- ARROW PANELS SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
- WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

NOTES FOR TWO-LANE OPERATION:

- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. SHADOW VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE SIGHT DISTANCE IS NOT AVAILABLE, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW VEHICLE SHALL BE EQUIPPED WITH BEACONS. HIGH-BEAM LIGHTS AND WORK LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN, SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ARROW BOARD TO BE USED IN CAUTION MODE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

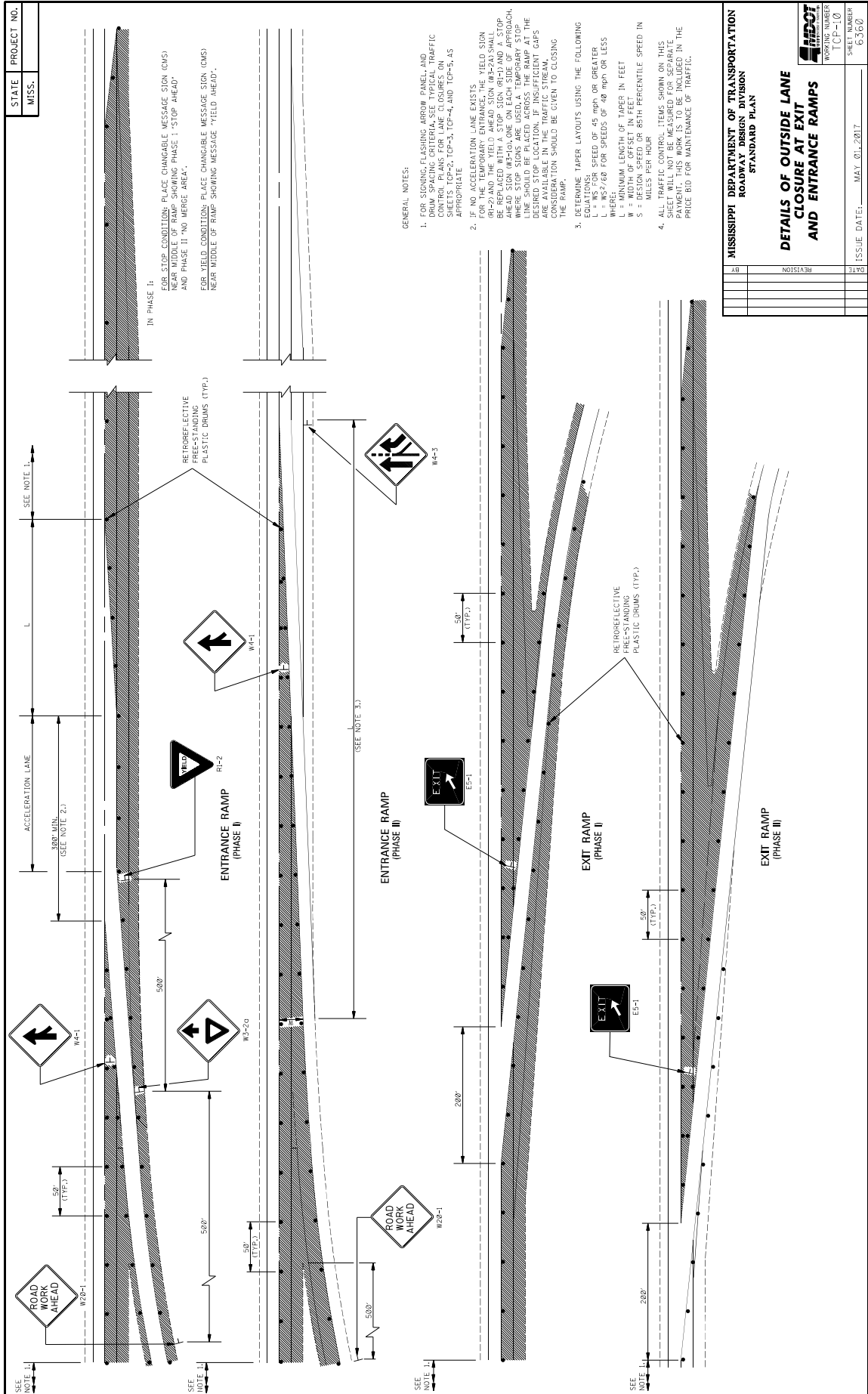
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TRAFFIC CONTROL PLAN
MOBILE OPERATIONS
MULTILANE ROADS
TWO-LANE ROADS

NO.	REVISION	DATE

ISSUE DATE: MAY 01, 2017

SHEET NUMBER TCP-9	PROJECT NUMBER G339
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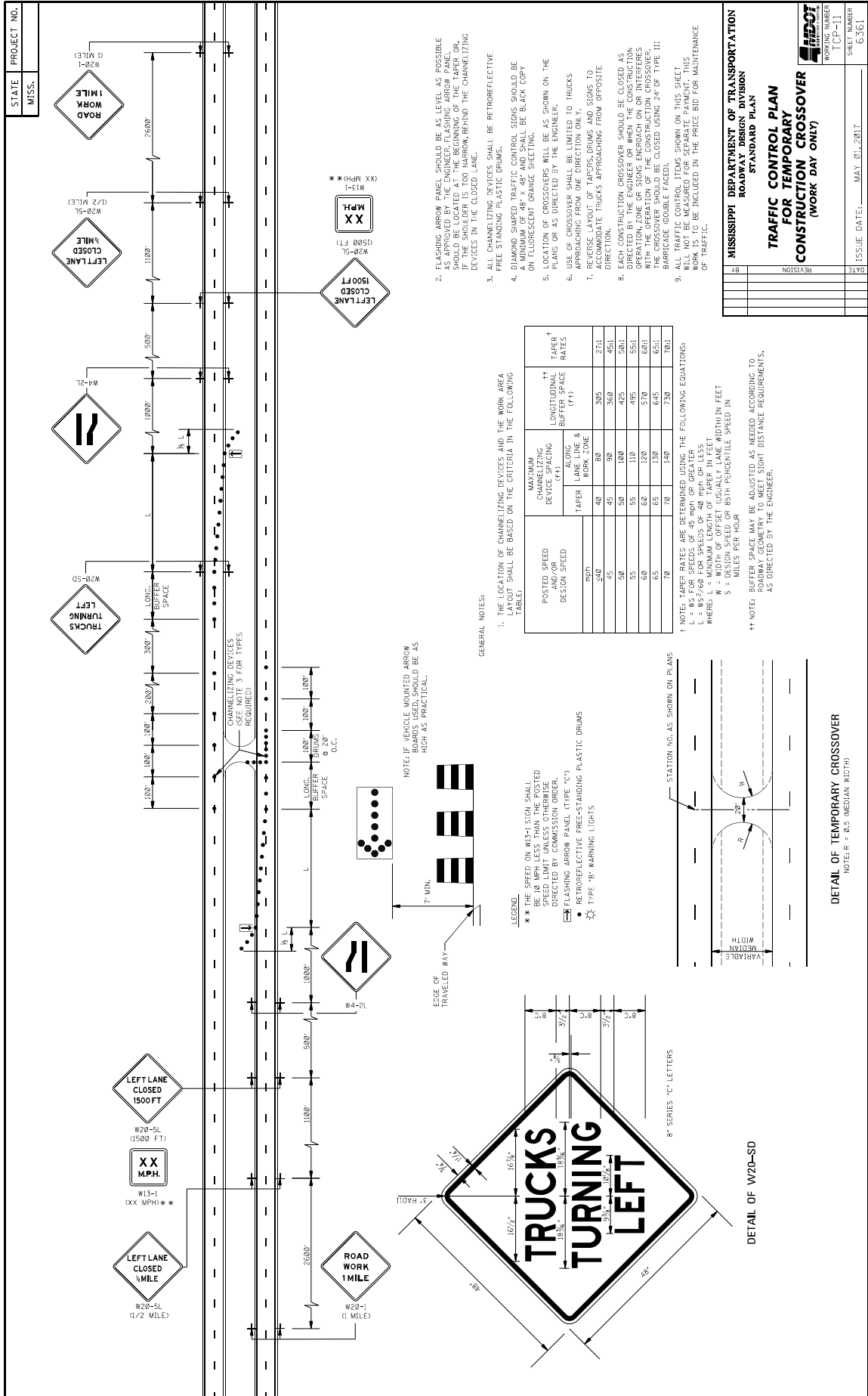


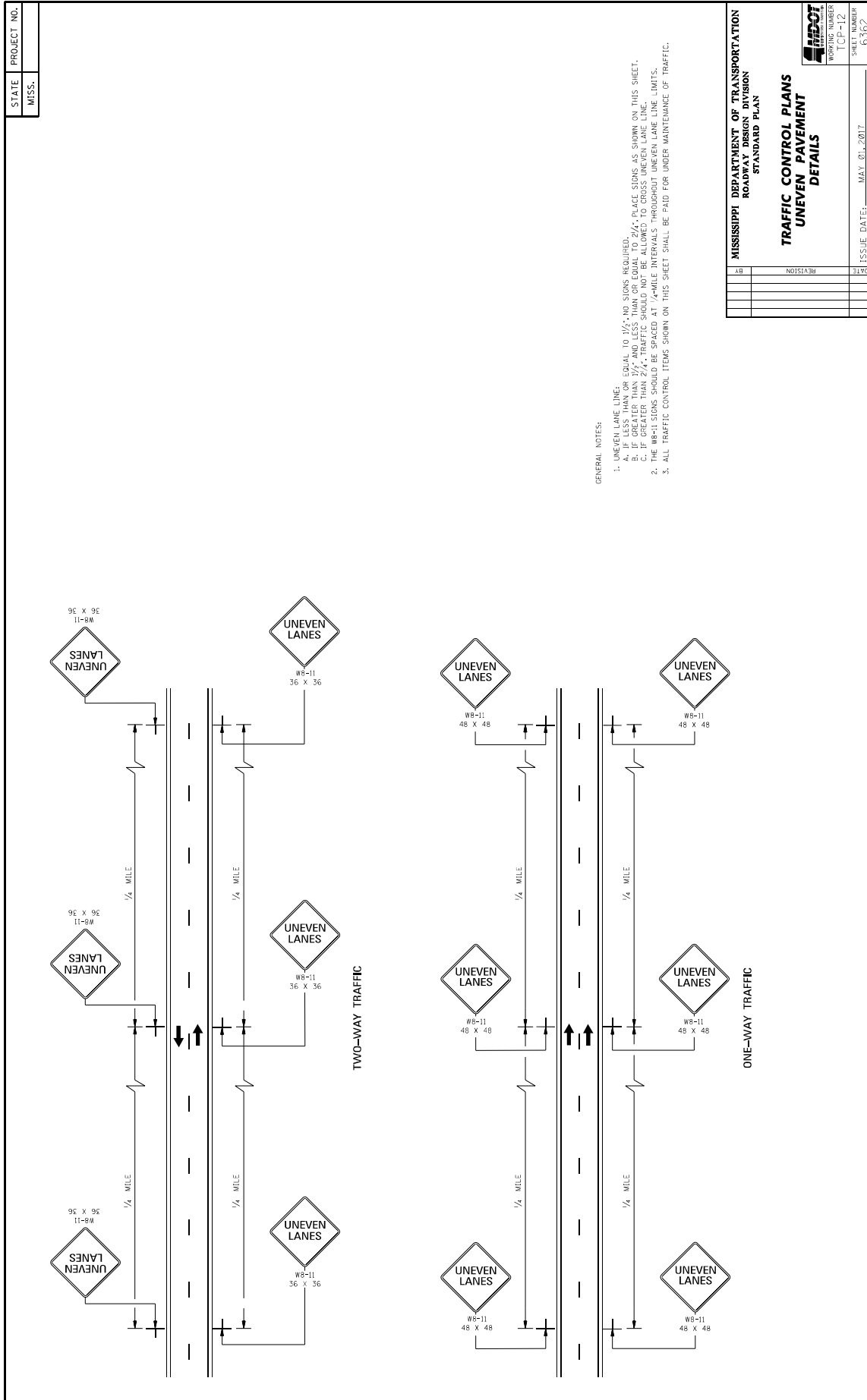
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

DETAILS OF OUTSIDE LANE CLOSURE AT EXIT AND ENTRANCE RAMPS

DATE	REVISION

ISSUE DATE:	MAY 01, 2017
WORKING NUMBER	ICP-110
SHEET NUMBER	6360

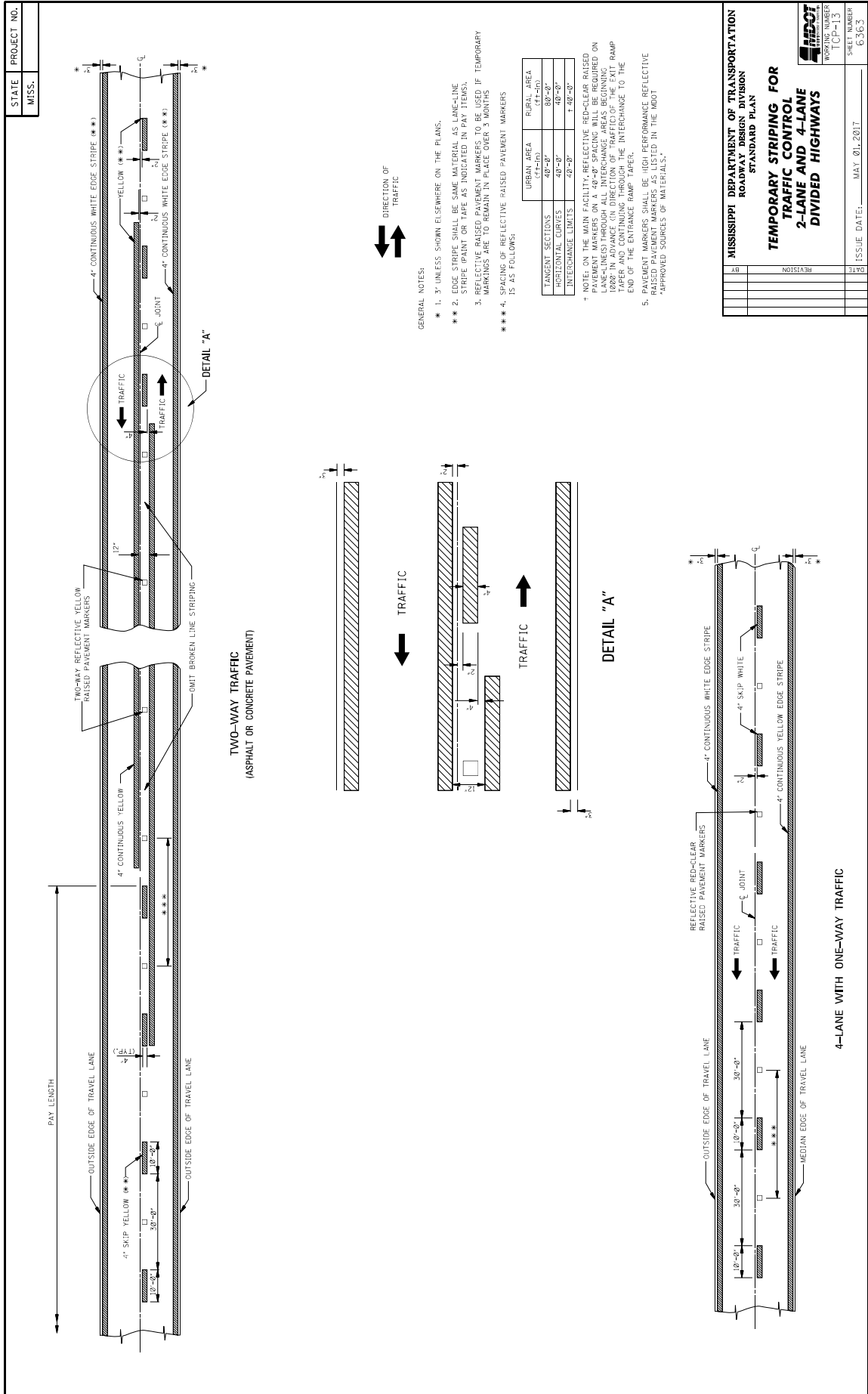




GENERAL NOTES:

1. UNEVEN LANE LINE.
 - A. IF LESS THAN OR EQUAL TO 1/4 MILE, NO SIGNS REQUIRED.
 - B. IF MORE THAN 1/4 MILE AND LESS THAN 1/2 MILE, PLACE SIGNS AS SHOWN ON THIS SHEET.
 - C. IF MORE THAN 1/2 MILE, PLACE SIGNS AS SHOWN ON THIS SHEET.
 - D. IF GREATER THAN 1/4 MILE, TRAFFIC SHOULD NOT BE ALLOWED TO CROSS UNEVEN LANE LINE.
2. THE W8-11 SIGNS SHOULD BE SPACED AT 1/4-MILE INTERVALS THROUGHOUT UNEVEN LANE LINE LIMITS.
3. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TRAFFIC CONTROL PLANS UNEVEN PAVEMENT DETAILS	
WORKING NUMBER TCP-12	SHEET NUMBER 6262
DATE	ISSUE DATE: MAY 01, 2017
REVISION	



STATE PROJECT NO. MISS.	MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN
TEMPORARY STRIPING FOR TRAFFIC CONTROL 4-LANE AND 5-LANE UNDIVIDED ROADWAYS	
WORKING NUMBER TCP-14	SHEET NUMBER 6264
ISSUE DATE: MAY 01, 2017	

**TYPICAL STRIPING AND RAISED
PAVEMENT MARKERS FOR 4-LANE SECTION**

**TYPICAL STRIPING AND RAISED
PAVEMENT MARKERS FOR 5-LANE SECTION**

**TYPICAL STRIPING AND RAISED
PAVEMENT MARKERS AT LEFT TURN LANES**

**TYPICAL TWO-WAY ARROW
INSTALLATION**

**DETAIL A
LATERAL PLACEMENT OF
PAVEMENT MARKERS**

**DETAIL B
LATERAL PLACEMENT OF
PAVEMENT MARKERS**

GENERAL NOTE:

- PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MDDI *APPROVED SOURCES OF MATERIALS*.
- REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS.
- TEMPORARY TURN ARROW TO BE PAID FOR AS TEMPORARY TRAFFIC STRIPE (LEGEND) ESTIMATED AT 10.9 SQ. FT. PER ARROW.

GENERAL NOTE:

- PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MDDI *APPROVED SOURCES OF MATERIALS*.
- REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS.
- TEMPORARY TURN ARROW TO BE PAID FOR AS TEMPORARY TRAFFIC STRIPE (LEGEND) ESTIMATED AT 10.9 SQ. FT. PER ARROW.

NOTES:

- CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY.
- IF SEGMENT IS LESS THAN 350', PLACE ONE SET OF ARROWS IN CENTER OF SEGMENT.
- IF SEGMENT IS GREATER THAN 350', PLACE FIRST SET OF ARROWS 50' TO 100' FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS (250' O.C.).

***NOTE: USE DETAIL STRIPING IF LENGTH IS 60' AT THIS LOCATION, OTHERWISE USE CONTINUOUS STRIPING.**

STATE MISS.	PROJECT NO.		
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**DIVIDED HIGHWAY SHOWN
(2 LANE - 2 WAY ROADWAY SIMILAR)
(PROJECT MORE THAN 1 MILE LENGTH)**

INTERCHANGE DETAIL

NOTES:

- R16-3 SIGN TO BE PLACED AS SHOWN OR AS DIRECTED BY THE ENGINEER.
- R16-3 SIGN SHOULD BE SPACED AT A MAXIMUM OF 2 MILES THROUGHOUT LENGTH OF PROJECT.
- THIS SHEET WILL ONLY APPLY TO SPEED REDUCTION SECTIONS.

48" x 60"
(INTERSTATE USE)

36" x 48"
(ALL OTHER HIGHWAYS)

R16-3

G20-2 MOUNTED ON TYPE III BARRICADE (SINGLE FACED) 6' WIDE

G20-1 MOUNTED ON TYPE III BARRICADE (SINGLE FACED) 6' WIDE

G20-2 MOUNTED ON TYPE III BARRICADE (SINGLE FACED) 6' WIDE

G20-1 MOUNTED ON TYPE III BARRICADE (SINGLE FACED) 6' WIDE

SIGN	DIMENSIONS (INCHES)									
	A	B	C	D	E	F	G	H	I	J
48" x 60"	60	48	3 1/2	1 1/4	5 1/4	4	4	4	4	4
36" x 48"	48	36	2 1/2	1 1/4	3 3/4	3 1/2	3 1/2	3 1/2	3 1/2	3 1/2

SIGN	DIMENSIONS (INCHES)									
	A	B	C	D	E	F	G	H	I	J
48" x 60"	60	48	3 1/2	1 1/4	5 1/4	4	4	4	4	4
36" x 48"	48	36	2 1/2	1 1/4	3 3/4	3 1/2	3 1/2	3 1/2	3 1/2	3 1/2

STATE MISS.	PROJECT NO.	
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TYPICAL SHOULDER CLOSURE

(1) TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
(2) TO BE USED WHEN CONSTRUCTION VEHICLES (EQUIPMENT) ENCRUSHES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.

TYPICAL SHOULDER WORK #1
(SEE NOTE A-I THIS SHEET)

TYPICAL SHOULDER WORK #2

NOTE:
WORK OUTSIDE TWO (2) FOOT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA, OR SEE NOTE A-3 THIS SHEET.

DETAIL OF DRUM PLACEMENT AT PAVEMENT EDGE DROP-OFF

GRANULAR MATERIAL REQUIRED (SAME CLASSIFICATION AS SHOULDER MATERIAL. SEE TYPICAL SECTIONS)

* A. PAVEMENT EDGE DROP-OFF

1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES NO PROTECTION REQUIRED. PLACE A SHOULDER SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE B (158'-0" C.C.).

2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 120 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. CONES MAY BE USED IN PLACE OF DRUMS, PANELS, AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE MULTIPLIER $L/3L$, WHERE L IS THE TAPER LENGTH IN FEET.

3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 4:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED.

4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.

5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

* B. DRUM SPACING

1. TANGENTS = $2 \times S$
2. CURVES = $S \times W$
WHERE:
L = TAPER LENGTH IN FEET
S = SPEED IN MPH (POSTED OR 85 PERCENTILE)
W = WIDTH OF OFFSET IN FEET

X X SPEED (MPH)	LENGTH (FEET)
25	35
30	40
35	45
40	50
45	55
50	60
55	65
60	70
65	75

* C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

TABLE V-1 GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

X X SPEED (MPH)	LENGTH (FEET)
25	35
30	40
35	45
40	50
45	55
50	60
55	65
60	70
65	75

* * POSTED SPEED, OFF-PEAK 85 PERCENTILE SPEED PRIOR TO WORK STARTING, OR THE ANTICIPATED OPERATING SPEED IN MPH.

DETAIL OF DRUM PLACEMENT AT PAVEMENT EDGE DROP-OFF

GRANULAR MATERIAL REQUIRED (SAME CLASSIFICATION AS SHOULDER MATERIAL. SEE TYPICAL SECTIONS)

* A. PAVEMENT EDGE DROP-OFF

1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES NO PROTECTION REQUIRED. PLACE A SHOULDER SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE B (158'-0" C.C.).

2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 120 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. CONES MAY BE USED IN PLACE OF DRUMS, PANELS, AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE MULTIPLIER $L/3L$, WHERE L IS THE TAPER LENGTH IN FEET.

3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 4:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED.

4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.

5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

* B. DRUM SPACING

1. TANGENTS = $2 \times S$
2. CURVES = $S \times W$
WHERE:
L = TAPER LENGTH IN FEET
S = SPEED IN MPH (POSTED OR 85 PERCENTILE)
W = WIDTH OF OFFSET IN FEET

X X SPEED (MPH)	LENGTH (FEET)
25	35
30	40
35	45
40	50
45	55
50	60
55	65
60	70
65	75

* C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

TABLE V-1 GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

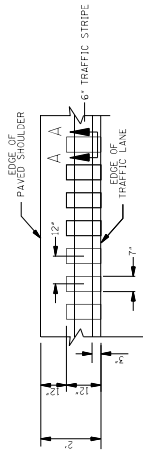
X X SPEED (MPH)	LENGTH (FEET)
25	35
30	40
35	45
40	50
45	55
50	60
55	65
60	70
65	75

* * POSTED SPEED, OFF-PEAK 85 PERCENTILE SPEED PRIOR TO WORK STARTING, OR THE ANTICIPATED OPERATING SPEED IN MPH.

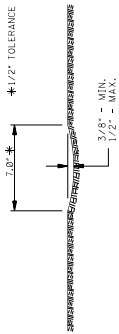
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY DESIGN DIVISION	
STANDARD PLAN	
TRAFFIC CONTROL DETAILS	
DRUM PLACEMENT	
SHOULDER CLOSURE	
WORKING NUMBER TCP-16	SHEET NUMBER G506
ISSUE DATE: MAY 01, 2017	DATE

STATE	PROJECT NO.
MISS.	

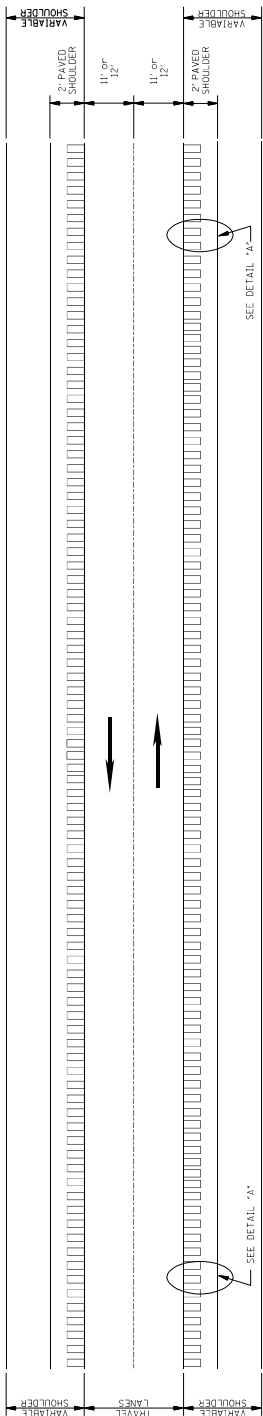
- GENERAL NOTES
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT.
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL PAVED SHOULDERS ON ROADWAYS OR OTHER INTERUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER.
 - COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS.
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
 - MAINLINE
 - INTERSECTING ROADWAY IF OVERLAD OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.W.
 - ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.
 - DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.



DETAIL "A"



SECTION "A-A"

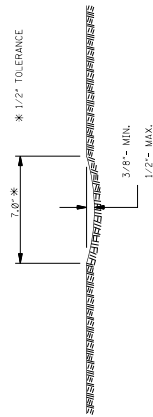


PLAN
NOT TO SCALE

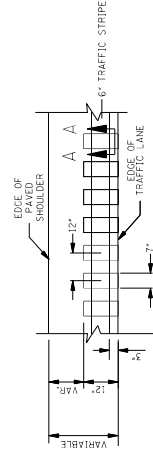
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
RUMBLE STRIPES 2-LANE HIGHWAYS (ASPHALT LANES, 2-FT ASPHALT SHOULDERS)	
BY	REVISION
DATE	ISSUE DATE: MAY 21, 2017
SHEET NUMBER RS-1	
TOTAL SHEETS 60/64	

STATE	PROJECT NO.
MISS.	

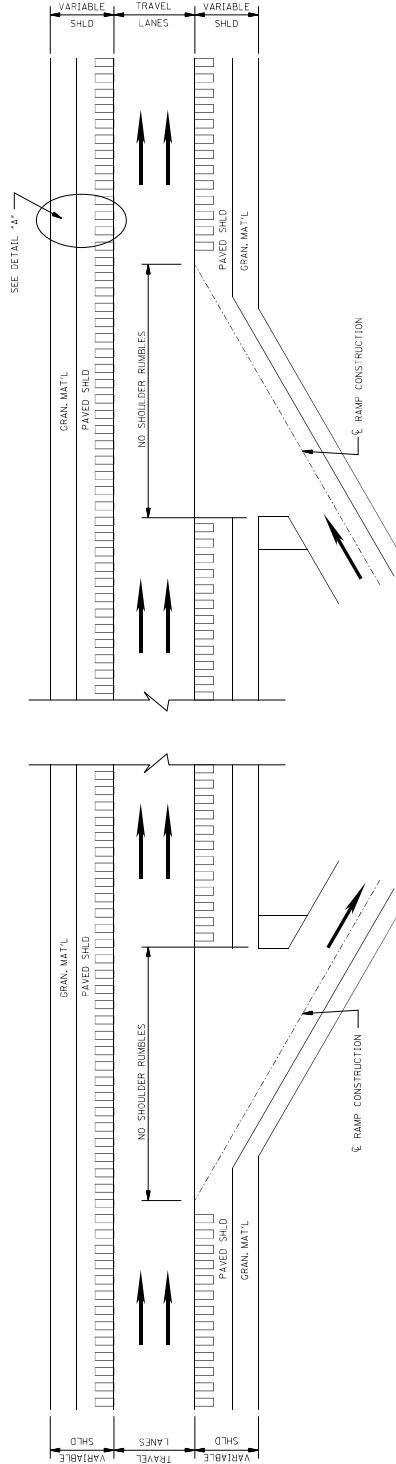
- GENERAL NOTES
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT.
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL PAVED SHOULDERS ON ROADWAYS OR OTHER INTERSECTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER.
 - COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS.
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
 - MAINLINE
 - INTERSECTING ROADWAY IF OVERLAD OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.W.
 - ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.



SECTION "A-A"

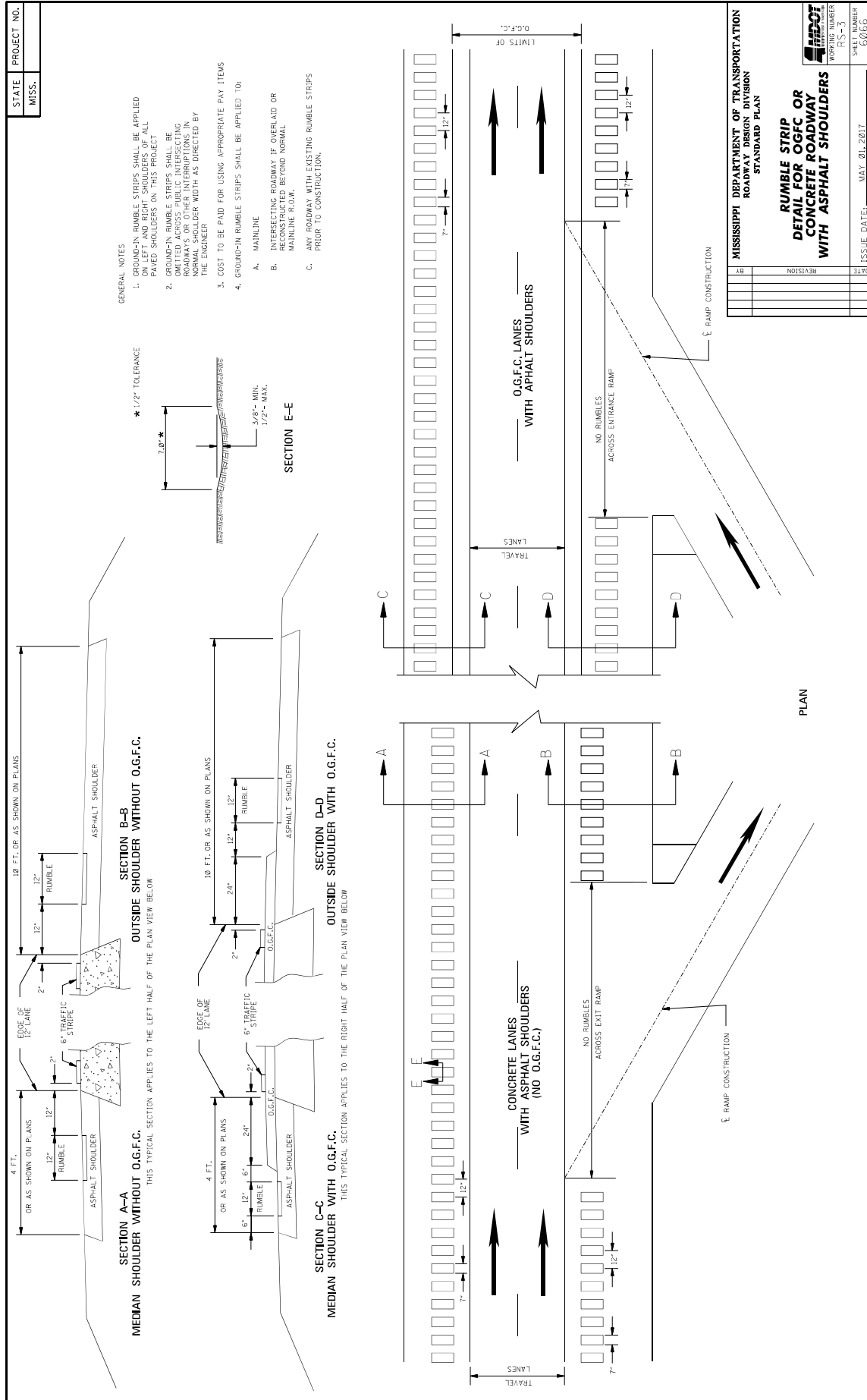


DETAIL "A"



PLAN
NOT TO SCALE
DETAILS OF
RUMBLE STRIPS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
RUMBLE STRIPES 4-LANE HIGHWAYS (ASPHALT LANES, 2-FT OR WIDER, ASPHALT SHOULDERS)	
BY	REVISION
DATE	ISSUE DATE: MAY 21, 2017
SHEET NUMBER R-3-2	PROJECT NUMBER 60603



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 445

CODE: (SP)

DATE: 10/10/2017

SUBJECT: Mississippi Agent or Qualified Nonresident Agent

Bidders are hereby advised of the requirements of Subsections 102.08, 103.05.2, and 107.14.2.1 of the *2017 Standard Specifications for Road and Bridge Construction* as it refers to bonding agents. Proposal guaranties, bonds, and liability insurance policies must be signed by a **Mississippi Agent or Qualified Nonresident Agent.**

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 516

CODE: (IS)

DATE: 11/28/2017

SUBJECT: Errata and Modifications to the 2017 Standard Specifications

<u>Page</u>	<u>Subsection</u>	<u>Change</u>
16	102.06	In the seventh full paragraph, change “Engineer” to “Director.”
33	105.05.1	In the sixth sentence, change “Contract Administration Engineer” to “Contract Administration Director.”
34	105.05.2.1	In subparagraph 2, change “SWPPP, ECP” to “SWPPP and the ECP”
35	105.05.2.2	In subparagraphs 2, add “ and” to the end of the sentence. In subparagraph 3, remove “, and” and add “.”.
90	109.04.2	In the last paragraph of subparagraph (a), place a period “.” at the end of the sentence.
93	109.04.2	In the last paragraph of subparagraph (g), place a period “.” at the end of the sentence. Also, in the first paragraph of subparagraph (h), place a period “.” at the end of the sentence.
97	109.07	Under ADJUSTMENT CODE, subparagraph (A1), change “HMA mixture” to “Asphalt mixtures.”
98	109.11	In the third sentence, change “Engineer” to “Director.”
219	308.04	In the last sentence of the last paragraph, change “Contractor’s decision” to “Engineer’s decision.”
300	405.02.5.9	In the first sentence of the second paragraph, change “Hot Mix Asphalt” to “Asphalt Mixtures.”
502	630.01.1	In the first paragraph, change “AASHTO” to “AASHTO’s LRFD”.
636	646.05	Change “each” to “per each” for the pay item units of payment.
640	656.02.6.2	In item 7), change “down stream” to “downstream”.
688	630.03.2	Change the subsection number from “630.03.2” to “680.03.2.”

725 702.08.3 In the second sentence of the first paragraph, change “hot-mix” to “asphalt.”

954 804.02.13.1.6 In the definition for “M” in the % Reduction formulas, change “paragraph 7.3” to “paragraph 5.3.”

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 977

CODE: (IS)

DATE: 07/25/2018

SUBJECT: DUNS Requirement for Federal Funded Projects

Bidders are advised that the Prime Contractor must maintain a current registration in the System for Award Management (<http://www.sam.gov>) at all times during this project. A Dun and Bradstreet Data Universal Numbering System (DUNS) Number (<http://www.dnb.com>) is one of the requirements for registration in the System for Award Management.

Bidders are also advised that prior to the award of this contract, they MUST be registered, active, and have no active exclusions in the System for Award Management.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1225

CODE: (SP)

DATE: 11/13/2018

SUBJECT: Early Notice to Proceed

Bidders are advised that if an early notice to proceed is allowed by the Department and the Contractor experiences problems or delays between the early notice to proceed date and the original notice to proceed date, this shall not be justification for any monetary compensation or an extension of contract time.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1226

CODE: (IS)

DATE: 11/16/2018

SUBJECT: Material Storage Under Bridges

Bidders are advised that Subsection 106.08 of the Standard Specifications allows the Contractor to store materials and equipment on portions of the right-of-way. However, the Contractor will not be allowed to store or stockpile materials under bridges without written permission from the Project Engineer. The Contractor shall submit a detailed request of all proposed materials to be stored under bridges to the Engineer a minimum of 14 calendar days prior to anticipated storage. This detail shall include, but not limited to, bridge location, material type, material quantity, and duration of storage. The Project Engineer and any other needed Division will review this information and determine whether to grant approval. The Contractor shall not store any material under any bridge without written approval from the Project Engineer.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1241

CODE: (IS)

DATE: 11/27/2018

SUBJECT: Fuel and Material Adjustments

Bidder's attention is brought to the last paragraph of Subsection 109.07 of the Standard Specifications which states that no fuel or material adjustment will be made after the completion of contract time. Any fuels consumed or materials incorporated into the work during the monthly estimate period falling wholly after the expiration of contract time will not be subject a fuel or material adjustment.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2206

CODE: (IS)

DATE: 01/14/2020

SUBJECT: MASH Compliant Devices

Bidders are hereby advised that compliance associated with the requirements of meeting either the National Cooperative Highway Research Program (NCHRP) Report 350 or the Manual for Assessing Safety Hardware (MASH) for installations of certain traffic control devices and permanent safety hardware devices (guardrails, guardrail terminals, permanent portable barriers, cast-in-place barriers, all other permanent longitudinal barriers, crash cushions, cable barriers, cable barrier terminals, bridge rails, bridge rail transitions, all other terminals, sign supports, and all other breakaway hardware) as listed throughout the Standard Specifications and/or the Standard Drawings, or both, is now replaced with the requirements of meeting the 2016 version of MASH after December 31, 2019. This change applies to new permanent installations and to full replacements of existing installations.

At the preconstruction conference or prior to starting any work on the project, the Contractor shall submit a letter stating that the traffic control devices and permanent safety hardware devices as outlined within the paragraph above that are to be used on the project are certified to meet MASH 2016.

When a MASH 2016-compliant device does not exist for the new permanent installations and/or full replacement installations of permanent safety hardware devices, as listed above, a MASH 2009-compliant or a NCHRP 350-compliant device may be proposed by the Contractor for the project. A written request for such instances must be submitted by the Contractor either at the preconstruction conference or prior to starting any work on the project. The Contractor shall submit the following items to the Project Engineer: (1) a detailed list of the proposed devices and locations thereof; and (2) certification letters indicating that the proposed devices are compliant with either MASH 2009 or NCHRP 350.

When a MASH 2016-compliant device does not exist for the temporary work zone traffic control devices (Category 1, Category 2, and Category 3 devices), a MASH 2009-compliant or a NCHRP 350-compliant device may be proposed by the Contractor for the project. Temporary work zone traffic control devices (Category 1, Category 2, and Category 3 devices) that are MASH 2009-compliant or NCHRP 350-compliant that have been in use prior to December 31, 2019, and that have a remaining service life may be proposed for use throughout their normal service life on the project by the Contractor. For either of these scenarios for temporary work zone traffic control devices, a written request must be submitted by the Contractor either at the preconstruction conference or prior to starting any work on the project. The Contractor shall submit the following items to the Project Engineer: (1) a detailed list of the proposed devices and locations thereof; and (2) certification letters indicating that the proposed devices are compliant with either MASH 2009 or NCHRP 350.

Work will only be allowed to proceed after the Department has granted written concurrence(s) with the proposed request(s) as listed above.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2207

CODE: (IS)

DATE: 01/08/2020

SUBJECT: Reflective Sheeting for Signs

Bidders are hereby advised that the retroreflective sign sheeting used for signs on this project shall be as listed below and shall meet the requirements of Subsection 721.06.

Temporary Construction Signs

Temporary traffic control (orange) sign sheeting shall be a minimum Type IX Fluorescent Orange sheeting as shown in Special Provision 907-721.

Permanent Signs

Permanent signs, except signs on traffic signal poles/mast arms, shall be as follows:

- Brown background sheeting on guide signs shall be a minimum Type VIII sheeting,
- Green and blue background sheeting on guide signs shall be a minimum Type IX sheeting, and
- All white, yellow, fluorescent yellow, and fluorescent yellow/green sheeting shall be Type XI sheeting.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2273

CODE: (SP)

DATE: 02/12/2020

SUBJECT: Mississippi Special Fuel Tax Law

Bidder's attention is brought to the second paragraph of Subsection 107.02 of the Standard Specifications which states that all Contractors and Subcontractors must comply with all requirements contained in the Mississippi Special Fuel Tax Law, Section 27-55-501, *et seq.* Attached are two Fact Sheets provided by the Mississippi Department of Revenue (MDOR) with additional information.

Gasoline and Dyed Diesel Used for Non-Highway Purposes

Mississippi provides a reduced rate for gasoline and dyed diesel used for non-highway purposes. The reduced rates are 6.44 cents per gallon and 5.75 cents per gallon of gasoline or dyed diesel. These fuels are generally taxed at 18 cents per gallon if for on road use.

Gasoline Used for Non-Highway Purposes

You may be entitled to a refund of 11.56 cents per gallon (making this an equivalent to a tax rate of 6.44 cents per gallon) if you desire to purchase gasoline to be used off road. The gasoline must be used for agricultural, maritime, industrial, manufacturing, domestic or non-highway purposes only.

Examples of non-highway include gasoline used in boats, golf carts, machinery used for manufacturing or farm equipment used exclusively in plowing, planting or harvesting farm products.

Refund Gasoline User

The refund is based on the amount of gallons used. Before a refund is issued, you are required to...

1. Obtain a refund gasoline user's permit and a certificate for refund booklet from the Department of Revenue;
2. Have a storage tank marked "REFUND GASOLINE"; and,
3. Purchase the gasoline from someone who holds a refund gasoline dealer's permit.

No refund will be allowed for gasoline used in motor vehicles owned or operated by a government entity or used in Mississippi government contracts.

Refund Gasoline Dealer

You must obtain a refund gasoline dealer's permit from the Department of Revenue before selling refund gasoline. At no time should the gasoline be delivered to a tank that is not properly marked. The gasoline must be dyed a distinctive mahogany color at the time of delivery.

The Department of Revenue may waive the dye requirement if the dye may cause damage to the equipment. The refund gasoline user is required to obtain the waiver from the Department of Revenue.

Dyed Diesel Used for Non-Highway Purposes

Unlike gasoline, you are not required to apply for a refund if you desire to purchase dyed diesel to be used off road. Mississippi provides a reduced rate of 5.75 cents per gallon on dyed diesel used off road. Diesel used on road is subjected to 18 cents per gallon. Dyed diesel used in motor vehicles owned or operated by a government entity or used in Mississippi government contracts will be subjected to 18 cents per gallon.

Dyed Diesel Used on the Highway

Any person who purchases, receives, acquires or uses dyed diesel for highway use will be liable to pay 18 cents per gallon and subject to a penalty in the amount of \$1000.

Identifying Dyed Diesel

Storage facilities for dyed diesel must be plainly marked "NONHIGHWAY DIESEL FUEL" or "NONHIGHWAY KEROSENE". Retailers are also required to mark all pumps or dispensing equipment.



Special Fuel Used on Government Contracts

State and Local Government Contracts

Special fuel purchased, acquired or used in performing contracts with the State of Mississippi, counties, municipalities or any political subdivision is taxed at a rate of 18 cents per gallon. Special fuel includes but is not limited to the following:

- Dyed diesel fuel;
- Kerosene;
- Undyed diesel fuel; and,
- Fuel oil.

State and local government contracts include construction, reconstruction and maintenance or repairs of projects such as roads, bridges, water systems, sewer systems, buildings, drainage canals and recreational facilities. The Department of Revenue may require contractors to remit the excise tax directly to the state in lieu of paying the tax to a distributor.

Special Fuel Direct Pay Permit

Contractors that remit the excise tax to the state will be issued a Special Fuel Direct Pay Permit. This permit relieves the distributor from collecting the tax and requires the contractor to file a monthly special fuel return. The distributor should include the contractor’s permit number on all invoices that are related to tax-free sales.

The contractor is required to furnish a surety or cash bond guaranteeing the payment of the excise tax prior to receiving the Special Fuel Direct Pay Permit. The Department of Revenue may accept a contractors tax bond if the bond covers the excise tax levied on special fuel.

Special Fuel Distributors

If the contractor does not have a Special Fuel Direct Pay Permit, distributors are required to collect the 18 cents excise tax and remit the tax to the Department of Revenue. The additional 12.25 cents levied on special fuel (excluding undyed diesel) should be reported on schedules 5F and 5G of the special fuel return.

Environmental Protection Fee

Special fuel distributors are required to collect the environmental protection fee even if the contractor has a Special Fuel Direct Pay Permit. The fee is levied at 4/10^{ths} of a cent per gallon. The fee is suspended or reinstated when the trust fund has exceeded or fallen below the obligatory balance.

Penalties

Any person who knowingly and willfully purchases untaxed fuel for use in equipment utilized on a road or highway construction site in this state is guilty of a misdemeanor and, upon conviction, shall be fined not less than \$1,000 or more than \$100,000, or imprisoned in the county jail for not more than one year, or both.



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SUPPLEMENT TO NOTICE TO BIDDERS NO. 2611

DATE: **05/02/2020**

The goal is 2 percent for the Disadvantaged Business Enterprise. All Bidders are required to submit Form OCR-481 for all DBEs. Bidders are advised to check the bid tabulation link for this project on the MDOT website at:

https://mdot.ms.gov/portal/current_letting

Bid tabulations are usually posted by 3:00 pm on Letting Day.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2611

CODE: (IS)

DATE: 05/21/2020

SUBJECT: Disadvantaged Business Enterprises In Federal-Aid Highway Construction

This contract is subject to the "Moving Ahead for Progress in the 21st Century Act (MAP-21)" and applicable requirements of "Title 49, Code of Federal Regulations, Part 26." Portions of the Act are set forth in this Notice as applicable to compliance by the Contractor and all of the Act, and the MDOT DBE Program, is incorporated by reference herein.

The Department has developed a Disadvantaged Business Enterprise Program that is applicable to this contract and is made a part thereof by reference.

Copies of the program may be obtained from:

Office of Civil Rights
Mississippi Department of Transportation
P. O. Box 1850
Jackson, Mississippi 39215-1850

POLICY

It is the policy of the Mississippi Department of Transportation to provide a level playing field, to foster equal opportunity in all federally assisted contracts, to improve the flexibility of the DBE Program, to reduce the burdens on small businesses, and to achieve that amount of participation that would be obtained in a non-discriminatory market place. In doing so, it is the policy of MDOT that there will be no discrimination in the award and performance of federally assisted contracts on the basis of race, color, sex, or national origin.

ASSURANCES THAT CONTRACTORS MUST TAKE

MDOT will require that each contract which MDOT signs with a sub-recipient or a Contractor, and each subcontract the Prime Contractor signs with a Subcontractor, includes the following assurances:

“The Contractor, sub-recipient or Subcontractor shall not discriminate on the basis of race, color, sex, or national origin in the performance of this contract. The Contractor shall carry out applicable requirements of 49 CFR 26 in the award and administration of federally assisted contracts. Failure by the Contractor to carry out these requirements is a material breach of this contract, which may result in the termination of this contract or such other remedy as MDOT deems appropriate.”

DEFINITIONS

For purposes of this provision the following definitions will apply:

"Disadvantaged Business" means a small business concern: (a) which is at least 51 percent owned by one or more socially and economically disadvantaged individual(s) or in the case of any publicly owned business, at least 51 percent of the stock of which is owned by one or more socially and economically disadvantaged individual(s); and (b) whose management and daily business operations are controlled by one or more of the socially and economically disadvantaged individual(s) who own it. It is important to note that the business owners themselves must control the operations of the business. Absentee ownership or title ownership by an individual who does not take an active role in controlling the business is not consistent with eligibility as a DBE under 49 CFR Part 26.71.

CONTRACTOR'S OBLIGATION

The Contractor and all Subcontractors shall take all necessary and reasonable steps to ensure that DBE firms can compete for and participate in the performance of a portion of the work in this contract and shall not discriminate on the basis of race, color, sex, or national origin. Failure on the part of the Contractor to carry out the DBE requirements of this contract constitutes a breach of contract and after proper notification the Department may terminate the contract or take other appropriate action as determined by the Department.

When a contract has a zero percent (0%) DBE goal, the Contractor still has the responsibility to take all necessary and reasonable steps to ensure that DBE firms can compete for and participate in the performance of the work in the contract. In this case, all work performed by a certified DBE firm is considered to be a "race neutral" measure and the Department will receive DBE credit towards the overall State goals when the DBE firm is paid for their work. If the Prime Contractor is a certified DBE firm, the Department can receive DBE credit only for the work performed by the Prime Contractor's work force or any work subcontracted to another DBE firm. Work performance by a non-DBE Subcontractor is not eligible for DBE credit.

CONTRACT GOAL

The goal for participation by DBEs is established for this contract in the attached Supplement. The Contractor shall exercise all necessary and reasonable steps to ensure that participation is equal to or exceeds the contract goal.

If the percentage of the contract that is proposed for DBEs is 1% or greater, the Contractor shall agree to meet or exceed the contract goal on the last bid sheet of the proposal.

All Bidders shall submit to the Office of Civil Rights Form OCR-481, signed by the Prime Contractor and the DBE Subcontractors, no later than the 3rd business day after opening of the bids.

Form OCR-481 is available on the MDOT website at www.mdot.ms.gov under the Civil Rights tab, or by calling 601-359-7466.

The OCR-481 Form must contain the following information:

The name and address of each certified DBE Contractor / Supplier;

The Reference Number, percent of work to be completed by the DBE subcontractor and the dollar amount of each item. If a portion of an item is subcontracted, a breakdown of that item including quantities and unit price must be attached, detailing what part of the item the DBE firm is to perform and who will perform the remainder of the item.

If the DBE Commitment shown on the last bid sheet of the proposal, does not equal or exceed the contract goal, the bidder must submit, to MDOT Contract Administration Division prior to bid opening, information to satisfy the Department that adequate good faith efforts have been made to meet the contract goal.

Failure of the lowest bidder to furnish acceptable proof of good faith efforts, submitted to MDOT Contract Administration Division prior to bid opening, shall be just cause for rejection of the proposal. Award may then be made to the next lowest responsive bidder or the project may be re-advertised.

GOOD FAITH EFFORTS

The following factors are illustrative of matters the Department will consider in judging whether or not the bidder has made adequate good faith effort to satisfy the contract goal.

- (1) Whether the bidder attended the pre-bid meeting that was scheduled by the Department to inform DBEs of subcontracting opportunities;
- (2) Whether the bidder advertised in general circulation, trade association, and minority-focus media concerning the subcontracting opportunities;
- (3) Whether the bidder provided written notice to a reasonable number of specific DBEs that their interest in the contract is being solicited;
- (4) Whether the bidder followed up initial solicitations of interest by contacting DBEs to determine with certainty whether they were interested;
- (5) Whether the bidder selected portions of the work to be performed by DBEs in order to increase the likelihood of meeting the contract goal;
- (6) Whether the bidder provided interested DBEs with adequate information about the plans, specifications and requirements of the contract;

- (7) Whether the bidder negotiated in good faith with interested DBEs and did not reject them as unqualified without sound reasons based on a thorough investigation of their capabilities; and
- (8) Whether the bidder made efforts to assist interested DBEs in obtaining any required bonding or insurance.
- (9) Whether the bidder has written notification to certified DBE Contractors soliciting subcontracting for items of work in the contract.
- (10) Whether the bidder has a statement of why an agreement was not reached.
- (11) Proof of written notification to certified DBE Contractors by certified mail that their interest is solicited in subcontracting the work defaulted by the previous DBE or in subcontracting other items of work in the contract.

The bidder’s execution of the signature portion of the proposal shall constitute execution of the following assurance:

The bidder hereby gives assurance pursuant to the applicable requirements of "Moving Ahead for Progress in the 21st Century Act (MAP-21)" and applicable requirements of "Title 49, Code of Federal Regulations, Part 26" that the bidder has made a good faith effort to meet the contract goal for DBE participation for which this proposal is submitted.

DIRECTORY

A list of “Certified DBE Contractors” which have been certified as such by the Mississippi Department of Transportation and other Unified Certification Partners (UPC) can be found on the Mississippi Department of Transportation website at www.mdot.ms.gov. The list is in the top left corner of the current Letting Calendar under Contracts & Letting. The DBE firm must be certified at the time the project is let and approved by MDOT to count towards meeting the DBE goal.

REPLACEMENT

If a DBE Subcontractor cannot perform satisfactorily, and this causes the OCR-481 commitment to fall below the contract goal, the Contractor shall take all necessary reasonable steps to replace the DBE with another certified DBE Subcontractor or submit information to satisfy the Mississippi Department of Transportation that adequate good faith efforts have been made to replace the DBE. The good faith efforts outlined previously in this document still apply. The replacement DBE must be a DBE who was on the Department's list of "Certified DBE Contractors" when the job was let, and who is still active. All DBE replacements must be approved by the Department.

Under no circumstances shall the Prime or any Subcontractor perform the DBE's work (as shown on the OCR-481) without prior written approval from the Department. See "Sanctions" at the end of this document for penalties for performing DBE's work.

When a Contractor proposes to substitute/replace/terminate a DBE that was originally named on the OCR-481, the Contractor must obtain a release, in writing, from the named DBE explaining why the DBE Subcontractor cannot perform the work. A copy of the original DBE's release must be attached to the Contractor's written request to substitute/replace/terminate along with appropriate Subcontract Forms for the substitute/replacement/terminated Subcontractor, all of which must be submitted to the DBE Coordinator and approved, in advance, by MDOT.

PRE-BID MEETING

A pre-bid meeting will be held in the [Commission Room on the 1st Floor](#) of the [MDOT Administration Building](#) in Jackson, at 2:00 P.M. on the day preceding the date of the bid opening.

This meeting is to inform DBE firms of subcontracting and material supply opportunities. Attendance at this meeting is considered of prime importance in demonstrating good faith effort to meet the contract goal.

PARTICIPATION / DBE CREDIT

Participation shall be counted toward meeting the goal in this contract as follows:

- (1) If the Prime Contractor is a certified DBE firm, only the value of the work actually performed by the DBE Prime can be counted towards the project goal, along with any work subcontracted to a certified DBE firm.
- (2) If the Contractor is not a DBE, the work subcontracted to a certified DBE Contractor will be counted toward the goal.
- (3) The Contractor may count toward the goal a portion of the total dollar value of a contract with a joint venture eligible under the standards of this provision equal to the percentage of the DBE partner in the joint venture.
- (4) Expenditures to DBEs that perform a commercially useful function may be counted toward the goal. A business is considered to perform a commercially useful function when it is responsible for the execution of a distinct element of the work and carries out its responsibilities by actually performing, managing, and supervising the work involved.
- (5) The Contractor may count 100% of the expenditures for materials and supplies obtained from certified DBE suppliers and manufacturers that produce goods from raw materials or substantially alters them for resale provided the suppliers and manufacturers assume the actual and contractual responsibility for the provision of the materials and supplies. The Contractor may count sixty percent (60%) of the expenditures to suppliers that are not

manufacturers, provided the supplier performs a commercially useful function in the supply process. Within 30 days after receipt of the materials, the Contractor shall furnish to the DBE Coordinator invoices from the certified supplier to verify the DBE goal.

- (6) Any work that a certified DBE firm subcontracts or sub-subcontracts to a non-DBE firm will not count towards the DBE goal.
- (7) Only the dollars actually paid to the DBE firm may be counted towards the DBE goal. The participation of a DBE Firm cannot be counted towards the Prime Contractor's DBE goal until the amount being counted towards the goal has been paid to the DBE.

AWARD

Award of this contract to the low bidder will be contingent upon the following conditions:

- (1) Concurrence from Federal Highway Administration, when applicable.
- (2) **All Bidders** must submit to the Office of Civil Rights for approval, Form OCR-481 (DBE Commitment) no later than the 3rd business day after opening of the bids to satisfy the Department and that adequate good faith efforts have been made to meet the contract goal. For answers to questions regarding Form OCR-481, contact the MDOT Office of Civil Rights at (601) 359-7466.
- (3) Bidder must include OCR-485 information with their bid proposal listing all firms that submitted quotes for material supplies or items to be subcontracted. OCR-485 information must be **signed and** included with the bid proposal. If the OCR-485 information is not included as part of bid proposal, your bid will be deemed irregular.

Prior to the start of any work, the bidder must notify the Project Engineer, in writing, of the name of the designated "DBE Liaison Officer" for this project. This notification must be posted on the bulletin board at the project site.

DEFAULT

If the contract goal established by MDOT in this proposal is 1% or greater, it must be met to fulfill the terms of the contract. The Contractor may list DBE Subcontractors and items that exceed MDOT's contract goal, but should unforeseen problems arise that would prevent a DBE from completing its total commitment percentage, the Contractor will meet the terms of the contract as long as it meets or exceeds MDOT's Contract Goal. For additional information, refer to "Replacement" section of this Notice.

DBE REPORTS

- (1) OCR-481: Refer to "CONTRACT GOAL" section of this Notice to Bidders for information regarding this form.

- (2) OCR-482: At the conclusion of the project, before the final estimate is paid and the project is closed out, the Prime Contractor will submit to the Project Engineer for verification of quantities and further handling Form OCR-482 whereby the Contractor certifies to the amounts of payments made to all Contractors / Suppliers over the life of the contract. The Project Engineer shall submit the completed Form OCR-482 to the DBE Coordinator (Office of Civil Rights). Final acceptance of the project is dependent upon Contract Administration Division's receipt of completed Form OCR-482 which they will receive from the Office of Civil Rights.
- (3) OCR-483: The Project Engineer/Inspector will complete Form OCR-483, the Commercially Useful Function (CUF) Performance Report, in accordance with MDOT S.O.P. No. OCR-03-05-02-483. Evaluations reported on this form are used to determine whether or not the DBE firm is performing a CUF. The Prime Contractor should take corrective action when the report contains any negative evaluations. DBE credit may be disallowed and/or other sanctions imposed if it is determined the DBE firm is not performing a CUF. This form should also be completed and returned to the DBE Coordinator (Office of Civil Rights).
- (4) OCR-484: Each month, the Prime Contractor will submit to the Project Engineer OCR-484 that certifies payments to all Subcontractors and shows all firms even if the Prime Contractor has paid no monies to the firm during that estimate period (negative report). The Project Engineer will attach the form to the monthly estimate before forwarding to the Contract Administration Division for further processing. Failure of the Contractor to submit the OCR-484 will result in the estimate not being processed and paid.
- (5) OCR-485: ALL BIDDERS must submit signed form with bid proposal of all firms that submitted quotes for material supplies or items to be subcontracted. If the OCR-485 information is not included as part of bid proposal, the bid will be deemed irregular.
- (6) OCR-487: Only used by Prime Contractors that are certified DBE firms. This form is used in determining the exact percentage of DBE credit for the specified project. The low Bidder should return this form to MDOT with the OCR-481 form, or can also be returned with the Permission to Subcontract Forms (CAD-720, CAD-725 and CAD-521).

DBE Forms, can be obtained from the Office of Civil Rights Division, MDOT Administration Building, 401 North West Street, Jackson, MS, or at www.mdot.ms.gov under the Civil Rights tab.

SANCTIONS

The Department has the option to enforce any of the following penalties for failure of the Prime Contractor to fulfill the DBE goal as stated on the OCR-481 form or any violations of the DBE program guidelines:

- (1) Disallow credit towards the DBE goal

(2) Withhold progress estimate payments

(3) Deduct from the final estimate or recover an amount equal to the unmet portion of the DBE goal which may include additional monetary penalties as outlined below based on the number of offenses and the severity of the violation as determined by MDOT.

1 st Offense	10% of unmet portion of goal	or	\$5,000 lump sum payment	or	Both
2 nd Offense	20% of unmet portion of goal	or	\$10,000 lump sum payment	or	Both
3 rd Offense	40% of unmet portion of goal	or	\$20,000 lump sum payment	or	\$20,000 lump sum payment and debarment

(4) Debar the Contractor involved from bidding on MDOT federally funded projects for a period of up to 12 months after notification by certified email.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2720

CODE: (SP)

DATE: 7/27/2020

SUBJECT: Contract Time

**PROJECT: STP-9999-03(373) / 108401301 – Copiah County
STP-9999-03(374) / 108401302 –Sunflower County**

The completion of work to be performed by the Contractor for this project will not be a specified date but shall be when all allowable working days are assessed, or any extension thereto as provided in Subsection 108.06. It is anticipated that the Notice of Award will be issued no later than October 13, 2020 and the date for Notice to Proceed / Beginning of Contract Time will be March 11, 2021

Should the Contractor request a Notice to Proceed earlier than March 11, 2021 and it is agreeable with the Department for an early Notice to Proceed, the requested date will become the new Notice to Proceed date.

105 Working Days have been allowed for the completion of work on this project.

Contract time was established with the assumption that Shop Drawing Submittals and Fabrication will begin upon Notice of Award.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2721

DATE: 07/28/2020

SUBJECT: Specialty Items

PROJECT: STP-9999-03(373)/108401301 & STP-9999-03(374)/108401302 - COPIAH & SUNFLOWER

Pursuant to the provisions of Section 108, the following work items are hereby designated as "Specialty Items" for this contract. Bidders are reminded that these items must be subcontracted in order to be considered as specialty items.

CATEGORY: PAVEMENT STRIPING AND MARKING

Line No	Pay Item	Description
0090	626-C001	6" Thermoplastic Double Drop Edge Stripe, Continuous White
0100	626-D002	6" Thermoplastic Double Drop Traffic Stripe, Skip Yellow
0110	626-F002	6" Thermoplastic Double Drop Edge Stripe, Continuous Yellow
0120	627-L001	Two-Way Yellow Reflective High Performance Raised Markers

CATEGORY: TRAFFIC CONTROL - TEMPORARY

Line No	Pay Item	Description
0030	619-A1002	Temporary Traffic Stripe, Continuous White
0040	619-A2002	Temporary Traffic Stripe, Continuous Yellow
0050	619-A6002	Temporary Traffic Stripe, Legend
0060	619-F1001	Concrete Median Barrier, Precast
0070	619-F2001	Remove and Reset Concrete Median Barrier, Precast

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 -NOTICE TO BIDDERS NO. 2722

CODE: (SP)

DATE: 07/08/2020

SUBJECT: Scope of Work

PROJECT: STP-9999-03(373) / 108401301 – Covich County
STP-9999-03(374) / 108401302 –Sunflower County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings". All other references to plans in the contract documents and Standard Specifications for Road and Bridge Construction are to be disregarded.

Minor changes in detail of design or construction procedure may be authorized by the Director of Structures, State Bridge Engineer provided such changes will not be cause for contract price adjustment. Work for which no pay item is provided will not be paid for directly and shall therefore be considered an absorbed item of work.

It shall be the responsibility of the Contractor to protect existing structures from damage which might occur during construction. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged by the Contractor during the life of the contract. No payment will be made for replacement or repair of damaged items.

All details are based on the dimensions shown on the original plans for the existing structure. The Contractor shall be responsible for adjusting the elements of the new construction to ensure a proper fit with the existing structure. The Contractor shall verify all dimensions of the existing structure prior to beginning work.

During construction, care shall be exercised to ensure that no debris falls into the roadway crossing below the structures. All debris, including any material that has accumulated on the bridge caps, shall become the property of the Contractor and shall be removed from the construction site.

Work on the project shall consist of bridge repairs on Bridges 40.9 (10835) & 43.0 (10844) on SR 28 in Covich County and on Bridges 255.2 & 255.2B (14545 & 14546) on US 49W in Sunflower County.

Work on the project shall consist of the following bridge repairs:

Joint Repair & Sealing:

The joint repair shall include installation of the preformed joint seal and other necessary work per the included standard drawings or as directed by the Engineer. All existing joint armor shall remain in place. All concrete approach slab joints shall be sealed. If the bridge has an asphalt

approach, the joint between the asphalt and concrete shall not be disturbed.

The joints shall be sealed by one of the three approved Manufacturers listed in Special Provision 907-823 and installed according to the Manufacturer's specifications.

Cap Cleaning:

The surface of all caps shall be cleaned to the satisfaction of the Engineer. All large debris shall be removed by hand. All other debris (dirt and rust) shall be removed by pressure washing to the satisfaction of the Project Engineer. All pressure washers shall be able to maintain 3,500 psi. This item of work shall be paid for under pay item 907-824-PP: Bridge Repair, Cap Cleaning.

Bearing Replacements:

All bearings should be removed and replaced according to Bearing Assembly Details. All structural steel shall conform to ASTM A709 Grade 50. All steel shall be new. Extreme care shall be exercised in removing the existing bearing plates that are welded to the ¾" anchor plates embedded in the prestressed beams. Existing anchor bolts shall be ground to ¼" below the concrete surface and grouted with epoxy mortar.

The bottom of the existing anchor plates shall be finished smooth to accommodate the new steel plates and painted with approved encapsulating paint. All pack rust and scale within the designated areas shall be removed by using small hand tools, mechanical process, or needle gun. All areas required to be painted containing grease films after the initial cleaning shall be cleaned with a biodegradable solvent. All debris removed from the existing structure shall become property of the Contractor and shall be disposed of properly. The Contractor shall provide technical data for the proposed encapsulating paint to be used on this project to the Project Engineer for approval by the Director of Structures, State Bridge Engineer. New paint shall be applied by hand, with either a brush or roller.

After the pads are vulcanized to the new steel plates, the new steel plates shall be cleaned and then painted with one shop coat of inorganic zinc, one field intermediate coat of acrylic latex, and one field top coat of acrylic latex per Section 814 of the Specifications.

Prior to any construction or fabrication, the Contractor shall comply with the submittal requirements listed in the bearing replacement details. The Contractor shall be responsible for adjusting the elements of the new construction to ensure a proper fit with the existing structure.

The Contractor shall provide adequate bracing and jacking arrangements as required to replace the existing bearings. The beam end shall only be raised to ¼" from its original position. Traffic shall be maintained on the bridge during the duration of the repair.

The Contractor shall employ the service of a Mississippi Registered Professional Engineer who is knowledgeable in the field of Bridge Design. A complete set of bracing and jacking arrangement plans along with design calculations shall be submitted to the Director of Structures, State Bridge Engineer through the Project Engineer for review prior to construction and shall bear the Design Engineer's seal.

Jacks shall be coupled to a common manifold. Jacking point shall be under the bottom flange of the beam at the bent and no jacking points will be allowed under any diaphragm or bay. After the beam is raised into position, temporary blocking shall be provided to secure the beam in this position while work is being performed. Temporary blocking points shall be under the bottom flange of the beam at the bent and no temporary blocking will be allowed under any diaphragm or bay.

Any damage to the bridge resulting from uneven or improper jacking shall be repaired by the Contractor at no additional cost to the State.

Payment for this work shall be made under pay item 907-824-PP: Bridge Repair, Bearing Replacement.

Anchor System:

Swedge anchor bolts shall meet or exceed ASTM F3125 and shall be galvanized in accordance with ASTM A153. Swedge bolt anchoring system shall be one of the following products:

- A. "HIT RE 500-V3 Epoxy Adhesive Anchor" shall be as manufactured by Hilti, Inc. www.us.hilti.com
- B. "EPCON C6+" shall be manufactured by ITW Ramset/Red Head. www.itwredhead.com
- C. "Ultrabond 1300" shall be as manufactured by Adhesives Technology Corp. www.atcepoxy.com

Installation of the anchoring system shall be in accordance with the Manufacturer's recommendations. A representative of the Manufacturer shall be present for sufficient time to assure that the Contractor is properly schooled in the installation of the anchoring system.

All costs associated with the anchor system shall be included in the bid price for pay item 907-824-PP: Bridge Repair, Bearing Replacement.

Contractor Submittals:

Prior to any construction or fabrication, the Contractor shall comply with the following submittal requirements.

Field Verification Submittal:

All dimensions of the existing bearing assemblies and caps shall be field verified.

Shop Drawing Submittal:

The Contractor shall submit shop drawings of the new bearing assemblies and anchor bolts to the Project Engineer for approval by the Director of Structures, State Bridge Engineer.

Welding Submittal:

- a. Certification for all welders
- b. Welding procedures
- c. Procedure for storage and handling of welding electrodes, wires, and flux
- d. A flux recovery procedure if applicable

Jacking Plan Submittal:

The Contractor shall submit a set of bracing and jacking arrangement plans along with design calculations. The Contractor shall employ the services of a Mississippi registered Professional Engineer knowledgeable in the field of bridge design. The submitted plans shall bear the seal of the Professional Engineer.

Traffic Control Plan:

The Contractor shall erect and maintain construction signing and provide all signs and traffic handling devices necessary to safely maintain traffic around or through the work areas in accordance with the Traffic Control Plan. Payment for traffic control devices for which no pay item is provided shall be included in the price bid for pay item 618-A: Maintenance of Traffic.

STATE	MISS	PROJECT NO.	STP-9999-03(373)
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SUMMARY OF QUANTITIES (SHEET 1)

PAY ITEM NO.	PAY ITEM	UNIT	COPIAH : 108401-301000	
			Prelim	Final
618-A001	Maintenance of Traffic	LS	1	
619-A1002	Temporary Traffic Stripe, Continuous White	LF	2,160	
619-A2002	Temporary Traffic Stripe, Continuous Yellow	LF	5,600	
619-A6002	Temporary Traffic Stripe, Legend	LF	192	
619-F1001	Concrete Median Barrier, Precast	LF	1,685	
619-F2001	Remove and Reset Concrete Median Barrier, Precast	LF	1,685	
620-A001	Mobilization	LS	1	
626-C001	6" Thermoplastic Double Drop Edge Stripe, Continuous White	LF	6,890	
626-D002	6" Thermoplastic Double Drop Traffic Stripe, Skip Yellow	LF	6,245	
626-E002	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow	LF	675	
627-L001	Two-Way Yellow Reflective High Performance Raised Markers	EA	157	

- 1 QUANTITY INCLUDES 1,080 LF FOR BRIDGE #40.9 AND 1,080 LF FOR BRIDGE #43.0.
- 2 QUANTITY INCLUDES 2,800 LF FOR BRIDGE #40.9 AND 2,800 LF FOR BRIDGE #43.0.
- 3 QUANTITY INCLUDES 96 LF FOR BRIDGE #40.9 AND 96 LF FOR BRIDGE #43.0.
- 4 QUANTITY INCLUDES 803 LF FOR BRIDGE #40.9 AND 882 LF FOR BRIDGE #43.0.
- 5 QUANTITY INCLUDES 3,366 LF FOR BRIDGE #40.9 AND 3,524 LF FOR BRIDGE #43.0.
- 6 QUANTITY INCLUDES 3,083 LF FOR BRIDGE #40.9 AND 3,162 LF FOR BRIDGE #43.0.
- 7 QUANTITY FOR BRIDGE #43.0.
- 8 QUANTITY INCLUDES 77 FOR BRIDGE #40.9 AND 80 FOR BRIDGE #43.0.

- 1
- 2
- 3
- 4
- 4
- 5
- 6
- 7
- 8

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES



Working Number	SQ-1
Sheet Number	1

PROJ NO: STP-9999-03(373)
COUNTY: COPIAH

Design Team: SMOI
Checked: _____ Date: 2/22/2021

FMS CON: 1084017301000
 PROJECT NO. STP-9999-03(373)
 STATE MISS.



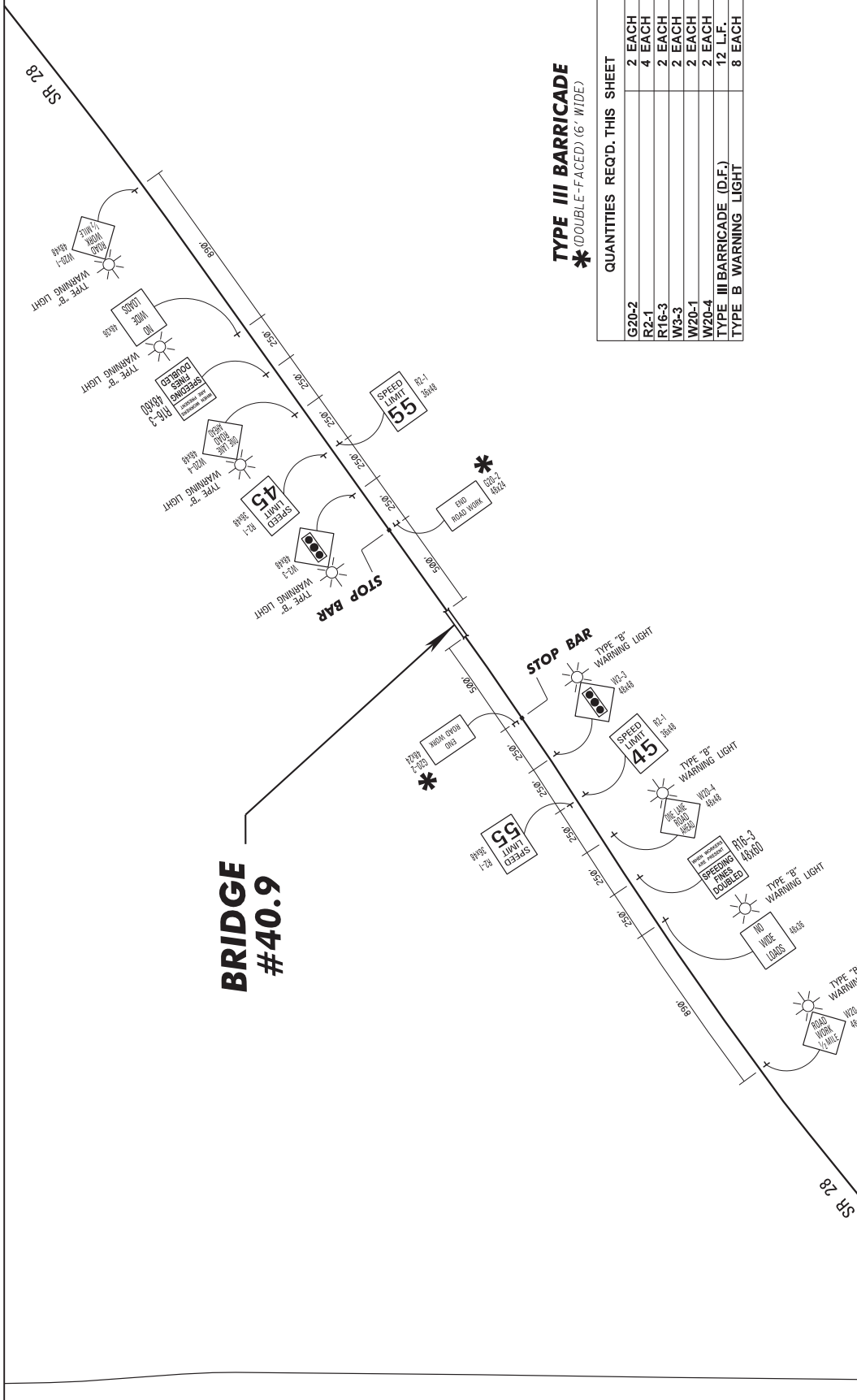
PROJECT NUMBER
 DCS-1
 SHEET NUMBER
 2

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
DETAIL OF CONSTRUCTION SIGNING

SR 28 (BR #40.9)
 PROJ. NO.: STP-9999-03(373)
 COUNTY: COPIAH

FILE NAME: DCS-1.dgn
 DESIGN TEAM: SMITH
 CHECKED: _____
 DATE: _____

BY	REVISION	DATE



TYPE III BARRICADE
 * (DOUBLE-FACED) (6' WIDE)

QUANTITIES REQ'D. THIS SHEET

G20-2	2 EACH
R2-1	4 EACH
R16-3	2 EACH
W3-3	2 EACH
W20-1	2 EACH
W20-4	2 EACH
TYPE III BARRICADE (D.F.)	12 L.F.
TYPE B WARNING LIGHT	8 EACH

BRIDGE #40.9

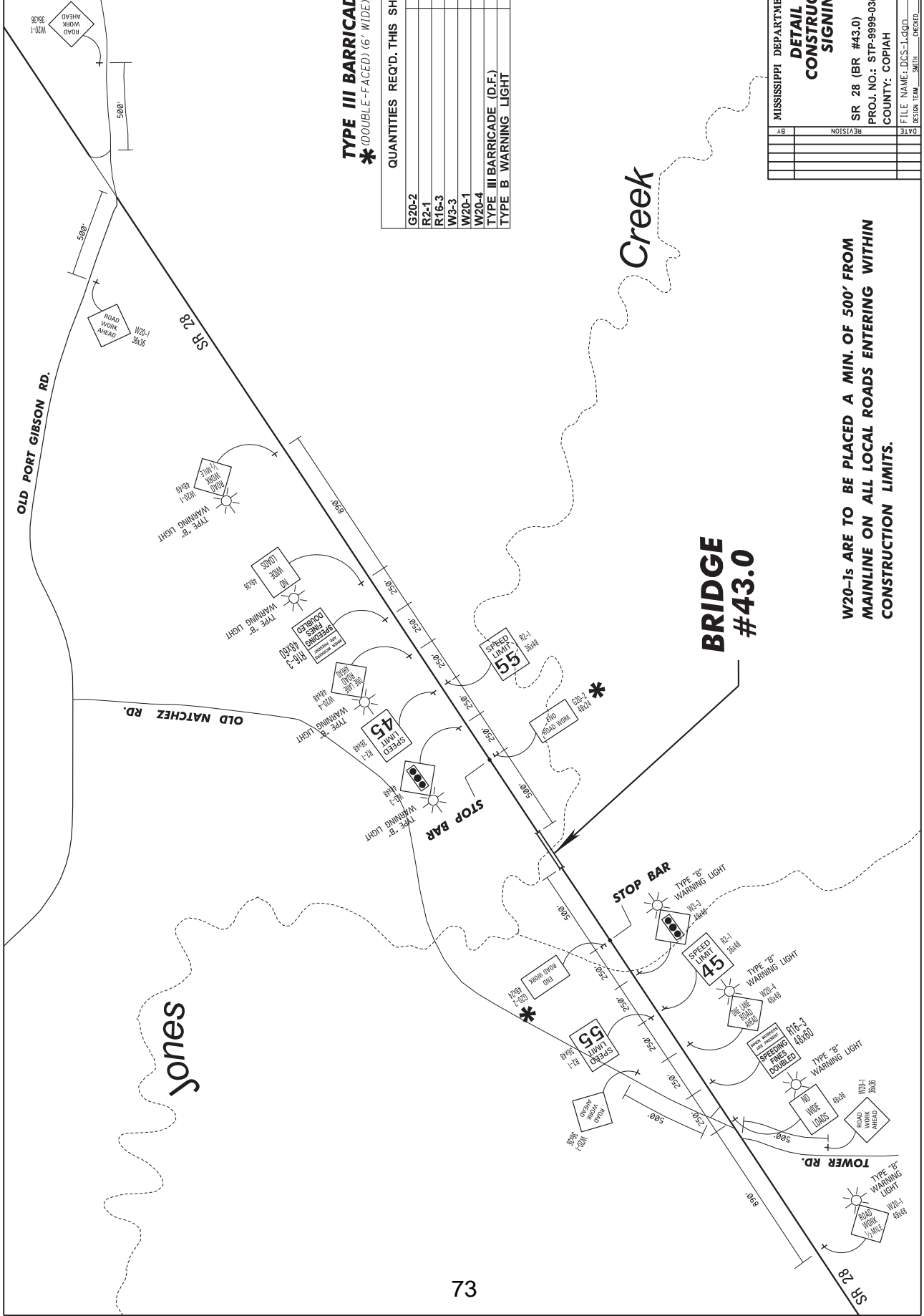
FMS CON: 1084017301000
 PROJECT NO. STP-9999-03(373)
 STATE MISS.

W GALLATIN RD.
 SR 28

OLD PORT GIBSON RD.

OLD NATCHEZ RD.

TOWER RD.



TYPE III BARRICADE
 * (DOUBLE-FACED) (6' WIDE)

QUANTITIES	REQ'D. THIS SHEET
G20-2	2 EACH
R2-1	4 EACH
R16-3	2 EACH
W3-3	2 EACH
W20-1	6 EACH
W20-4	2 EACH
TYPE III BARRICADE (D.F.)	12 L.F.
TYPE B WARNING LIGHT	8 EACH

Creek

BRIDGE #43.0

W20-1s ARE TO BE PLACED A MIN. OF 500' FROM MAINLINE ON ALL LOCAL ROADS ENTERING WITHIN CONSTRUCTION LIMITS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
DETAIL OF CONSTRUCTION SIGNING

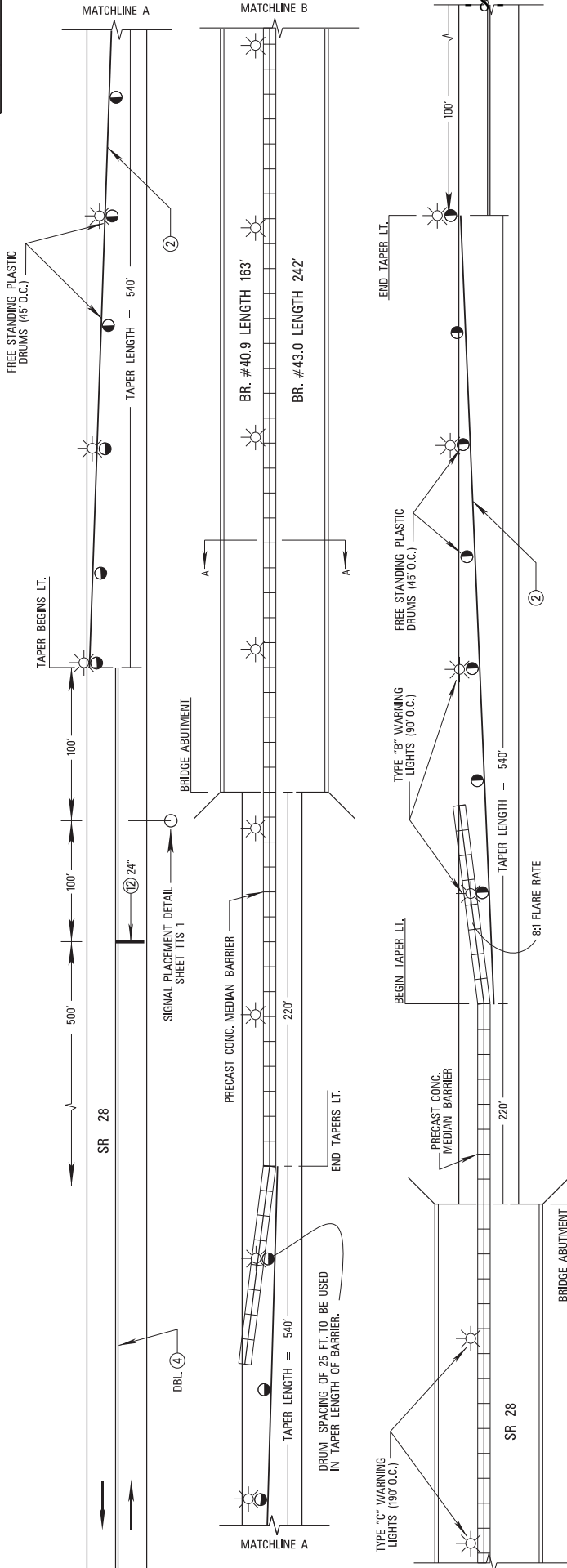
SR 28 (BR #43.0)
 PROJ. NO.: STP-9999-03(373)
 COUNTY: COPIAH

FILE NAME: DCS-1.4.DWG
 DESIGN TEAM: SMITH
 CHECKED: _____
 DATE: _____

REVISION: _____
 BY: _____

REGISTRATION LICENSE NO. 10000
 DRAWING NUMBER DCS-2
 SHEET NUMBER 3

FMS CON: 1084017301000
 PROJECT NO. STP-9999-03(373)
 STATE MISS.



TRAFFIC CONTROL ITEMS TO BE REUSED IN PHASE II

TRAFFIC CONTROL ITEMS	BR #40.9	BR #43.0
PRECAST CONC. MEDIAN BARRIERS	24	24
FREE STANDING PLASTIC DRUMS	24	24
TYPE "B" WARNING LIGHTS	12	12
TYPE "C" WARNING LIGHTS	12	12
TYPE "A" PORTABLE TRAFFIC SIGNALS	2	2

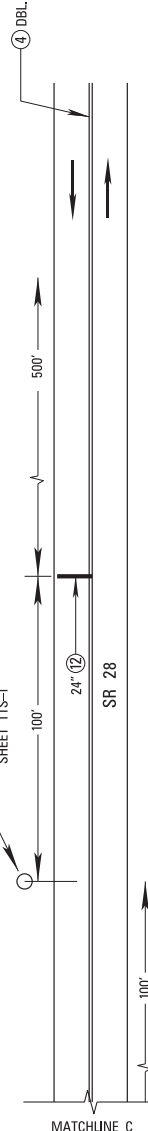
PAVEMENT MARKING (TEMPORARY)

ITEM	DESCRIPTION	BR #40.9	BR #43.0
1	TEMP. TRAFFIC STRIPE (CONT. WHITE)	1088.0'	1088.0'
2	TEMP. TRAFFIC STRIPE (CONT. WHITE)	2808.0'	2808.0'
3	TEMP. TRAFFIC STRIPE (CONT. YELLOW)	2808.0'	2808.0'
4	TEMP. TRAFFIC STRIPE (SKIP WHITE)	2808.0'	2808.0'
5	TEMP. TRAFFIC STRIPE (SKIP WHITE)	2808.0'	2808.0'
6	TEMP. TRAFFIC STRIPE (SKIP YELLOW)	2808.0'	2808.0'
7	TEMP. TRAFFIC STRIPE (SKIP YELLOW)	2808.0'	2808.0'
8	TEMP. TRAFFIC STRIPE (LEGEND)	2808.0'	2808.0'
9	TEMP. TRAFFIC STRIPE (LEGEND)	2808.0'	2808.0'

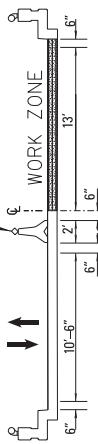
NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 TRAFFIC CONTROL PLANS
 BR. # 40.9 & 43.0
 PHASE I
 PROJ. NO.: STP-9999-03(373)
 COUNTY: COPIAH
 FILE NAME: IC-1.DWG
 DESIGN TEAM: SMITH
 CHECKED: []
 DATE: []

- NOTES:
- A) SEE TCP-6 AND TCP-9 FOR SIGN LAYOUT DURING LANE CLOSURE OPERATIONS.
 - B) TYPE "B" WARNING LIGHTS TO BE PLACED ON PLASTIC DRUMS IN THE TRAFFIC TAPER LIGHTS SPACED @ 30' O.C.
 - C) TYPE "C" WARNING LIGHTS TO BE PLACED ON PRECAST CONCRETE MEDIAN BARRIER LIGHTS SPACED @ 190' O.C.
 - D) ALL TEMPORARY TRAFFIC STRIPING NOT APPLICABLE TO THE CURRENT PHASE OF CONSTRUCTION SHALL BE REMOVED PRIOR TO BEGINNING WORK.
 - E) PERMANENT STRIPE TO BE REPLACED BY CONTRACTOR UPON COMPLETION OF PROJECT IN ACCORDANCE WITH SECTION 625.

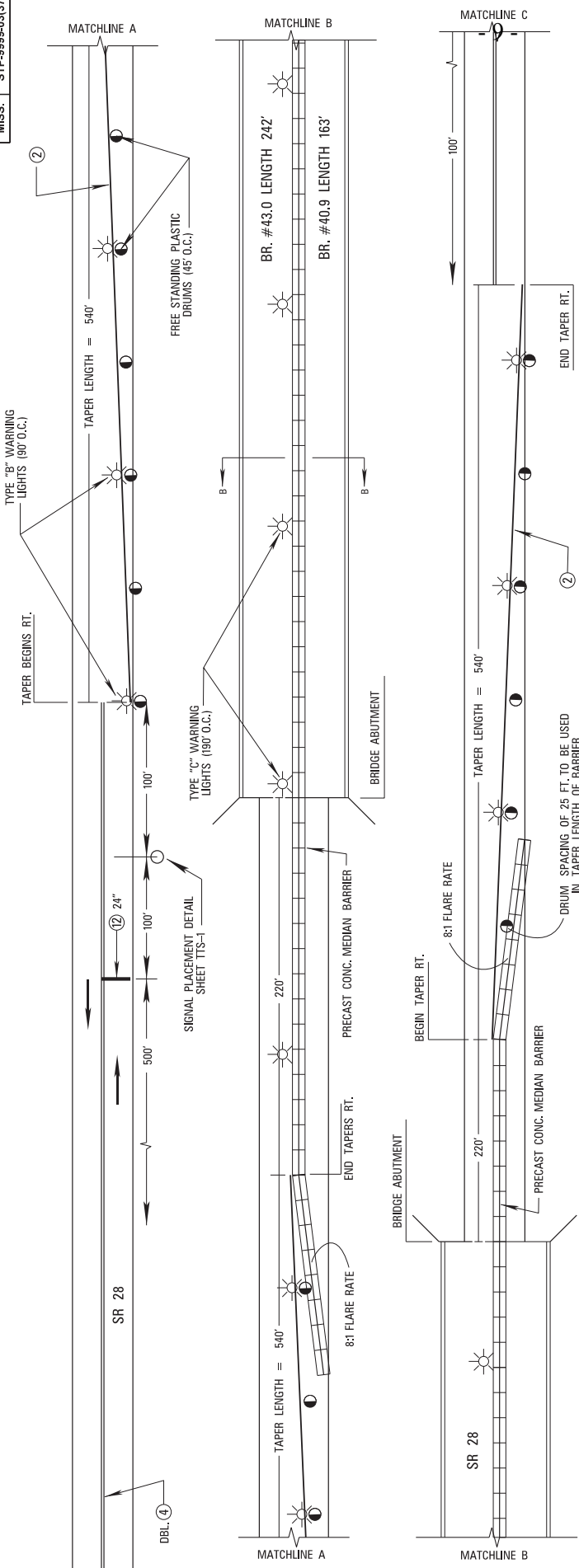


YELLOW DELINEATORS TO BE PLACED ON TOP OF CONCRETE MEDIAN BARRIER, AS PER STANDARD V.M.C. CMP-3, COST ABSORBED.



SECTION "A"-A"

FMS CON: 1684017301000
 PROJECT NO.
 STATE MISS. STP-9999-03(373)



TRAFFIC CONTROL ITEMS TO BE REUSED IN PHASE II

ITEM	DESCRIPTION	QUANTITY	REUSE
1	PRECAST CONCRETE MEDIAN BARRIERS	BR.#40.9	BR.#43.0
2	TYPE 'R' WARNING LIGHTS	52	12
3	TYPE 'C' WARNING LIGHTS	13	13
4	TYPE '2A' PORTABLE TRAFFIC SIGNALS	7	7

PAVEMENT MARKING — (TEMPORARY)

SYMBOL	DESCRIPTION	TYPE	BR.#40.9	BR.#43.0
1	TEMP. TRAFFIC STRIPES (CONT. WHITE)	PAINT	18880.0'	18880.0'
2	TEMP. TRAFFIC STRIPES (CONT. YELLOW)	PAINT	18880.0'	18880.0'
3	TEMP. TRAFFIC STRIPES (SKIP WHITE)	PAINT	18880.0'	18880.0'
4	TEMP. TRAFFIC STRIPES (SKIP YELLOW)	PAINT	18880.0'	18880.0'
5	TEMP. TRAFFIC STRIPES (DETAIL)	PAINT	18880.0'	18880.0'
6	TEMP. TRAFFIC STRIPES (LEGEND)	PAINT	18880.0'	18880.0'

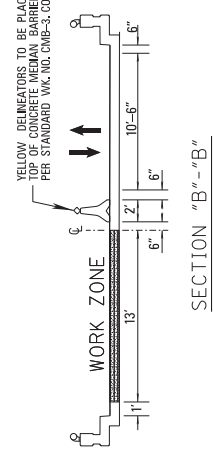
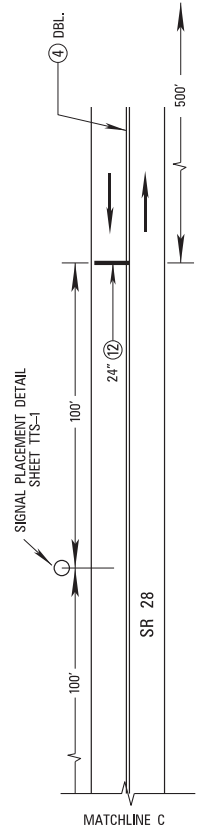
NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 TRAFFIC CONTROL PLANS
 BR.# 40.9 & 43.0
 PHASE II

PROJ. NO.: STP-9999-03(373)
 COUNTY: COPIAH

DESIGN TEAM: SMITH CHECKED: DATE: 5

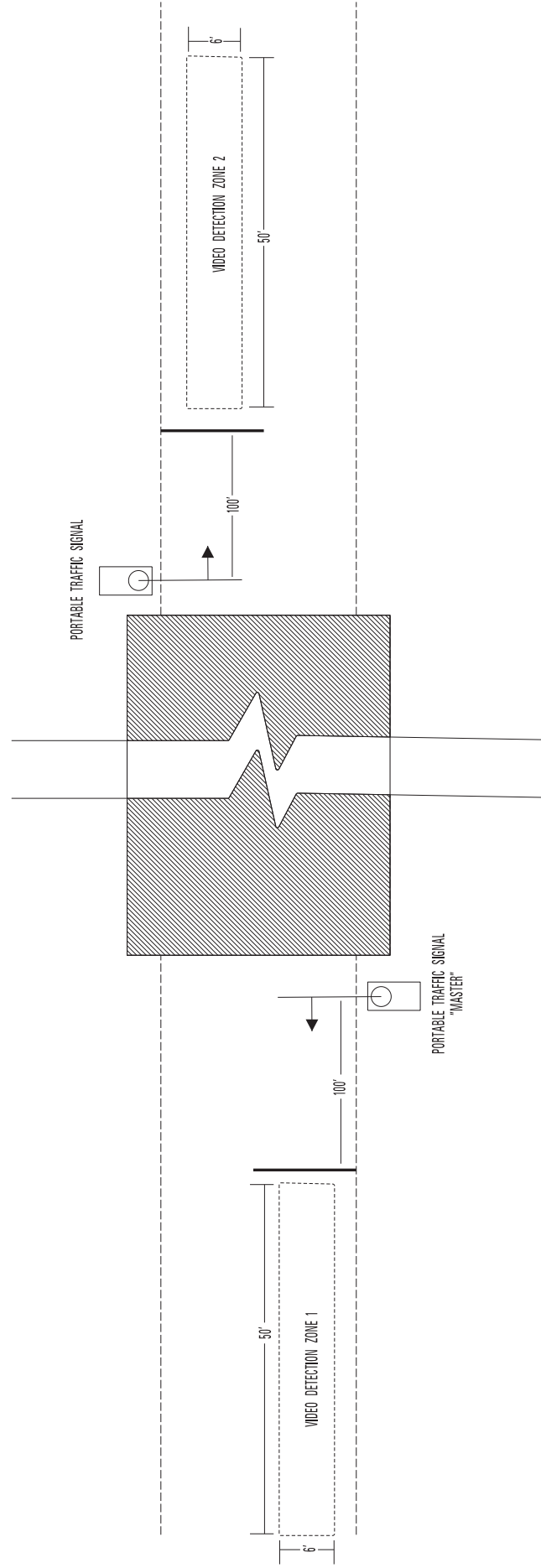
- NOTES:
- A) SEE TCR-6 AND TCR-9 FOR SIGN LAYOUT DURING LANE CLOSURE OPERATIONS.
 - B) TYPE 'R' WARNING LIGHTS TO BE PLACED ON PLASTIC DRUMS IN THE TRAFFIC TAPER (LIGHTS SPACED @ 30' O.C.).
 - C) TYPE 'C' WARNING LIGHTS TO BE PLACED ON PRECAST CONCRETE MEDIAN BARRIER (LIGHTS SPACED @ 190' O.C.).
 - D) ALL TEMPORARY TRAFFIC STRIPING NOT APPLICABLE TO THE CURRENT PHASE OF CONSTRUCTION SHALL BE REMOVED PRIOR TO BEGINNING WORK.
 - E) PERMANENT STRIPE TO BE REPLACED BY CONTRACTOR UPON COMPLETION OF PROJECT IN ACCORDANCE WITH SECTION 625.



FMS CON: 106401701000
 STATE PROJECT NO.
 MISS. STP-9999-03(373)

NOTE:

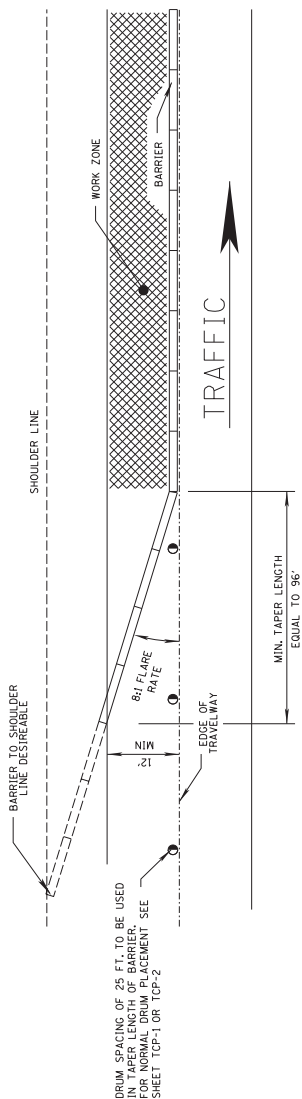
1. ALL TRAFFIC CONTROL DEVICES SHALL COMPLY WITH MUTCD (LATEST EDITION).
2. ALL SIGNALS AND TIMINGS SHALL BE COORDINATED THROUGH PRETIMED SIGNAL ACTUATION.
3. CONTRACTOR TO NOTIFY SIGNAL ENGINEER IN TRAFFIC ENGINEERING DIVISION TO BE PRESENT DURING PROGRAMMING. AMANDA CLARK (601) 359-1454
4. VIDEO DETECTION SHALL BE USED IN CONJUNCTION WITH PORTABLE TRAFFIC SIGNALS. ALL VIDEO EQUIPMENT SHALL BE INCLUDED UNDER PAY ITEM 618-4001, MAINTENANCE OF TRAFFIC.
5. SIGNALS SHALL COMMUNICATE WITH ONE ANOTHER TO ENSURE NO CONFLICT BETWEEN THEM.
6. DRAWING DEPICTS ONLY ONE PHASE OF CONSTRUCTION, BUT IT IS APPLICABLE FOR ALL PHASES.



NOT TO SCALE

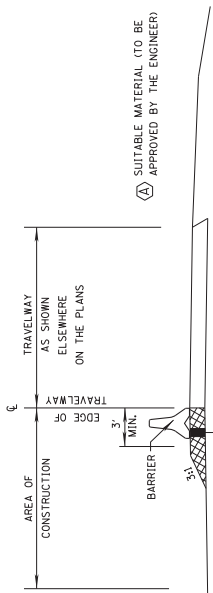
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
TEMPORARY TRAFFIC SIGNAL INSTALLATION	DESIGN NUMBER: TTS-1
PROJ. NO.: STP-9999-03(373)	SHEET NUMBER: 6
COUNTY: COPIAH	DATE: _____
FILE NAME: TTS-1.dwg	DESIGN TEAM: SMITH
DESIGNED BY: _____	CHECKED: _____
DATE: _____	DATE: _____
REVISION: _____	DATE: _____
BY: _____	DATE: _____





DETAIL OF POSITIVE BARRIER WITH TAPER

DETAIL OF POSITIVE BARRIER WITH IMPACT ATTENUATOR



ELEVATION VIEW FOR POSITIVE BARRIER

- NOTES:
1. POSITIVE BARRIER IS REQUIRED IN THE AREA OF OPEN PUNCH OUTS THAT ARE WITHIN SIX (6) FEET OF THE TRAVELWAY WHENEVER ACTUAL REPAIR WORK IS NOT BEING PERFORMED WITHIN THE LANE CLOSURE.
 2. MATERIAL USED TO SUPPORT POSITIVE BARRIER MUST BE AT SAME ELEVATION AS PAVEMENT IN ADJACENT TRAVELWAY.
 3. DELINEATORS REQUIRED ON ALL NON-REFLECTIVE BARRIER, AS SHOWN ON WORKING NO. CMB-3.



ELEVATION VIEW FOR DRUM

- NOTES:
1. WHILE WORK IS BEING PERFORMED WITHIN THE LANE CLOSURE DROP-OFFS MUST BE PROTECTED WITH DRUMS, ETC. IN EMERGENCIES EXCAVATED SECTION MAY BE BACKFILLED WITH GRANULAR MATERIAL, STONE OR OTHER APPROVED MATERIAL TO AVOID OVERTIGHT DROP-OFFS.
 2. LANE CLOSURES WITH OPEN PUNCH OUT AREAS MAY NOT BE LEFT UNATTENDED WHEN DRUMS ARE BEING USED FOR LANE CLOSURE

Notice to Bidders No. 272

GENERAL NOTES:

1. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER OTHER BID ITEMS.
2. FOR DETAILS OF DRUM PLACEMENT SEE OTHER TRAFFIC CONTROL PLANS.

DESIGN SPEED (mph)	OVER 10,000 veh/day	5,000-10,000 veh/day	1,000-5,000 veh/day	UNDER 1,000 veh/day
70	360	330	290	250
60	300	250	210	200
50	230	190	160	150
40	160	130	110	100
30	110	90	80	70

2. RUNOUT LENGTH (L_R) IS TO BE DETERMINED USING THE FOLLOWING TABLE:


WHERE: L_A = LATERAL EXTENT OF THE AREA OF CONCERN
 L_R = RUNOUT LENGTH
 L_2 = LATERAL OFFSET FROM EDGE OF TRAVELED WAY TO BARRIER.

1. LENGTH OF NEED, L.O.N. = $\frac{L_R L_A - L_2^2}{L_A}$

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
LANE CLOSURE DETAILS FOR GREATER THAN 3 INCH DROPOFF
 PROJECT NUMBER: STP-9999-03(373)
 COUNTY: COPIAH
 FILE NAME: SDICP-C.GGD.DCN
 DESIGN TEAM: SMITH
 DATE: UPDATE
 SHEET NUMBER: 7

FMS CON: 108401730200	
STATE	PROJECT NO.
MISS.	STP-9999-03(374)

Notice to Bidders No 27

 MISSISSIPPI DEPARTMENT OF TRANSPORTATION DETAIL OF CONSTRUCTION SIGNING		WORKING NUMBER	DATE
		DESIGN NUMBER	DATE
PROJ. NO.: STP-9999-03(374) COUNTY: SUNFLOWER		FILE NAME: DCS.DCD	CHECKED
DESIGN TEAM: SMITH		CHECKED	DATE

BRIDGE #225.2A

US 49W NORTHBOUND

BRIDGE #225.2B

Sunflower

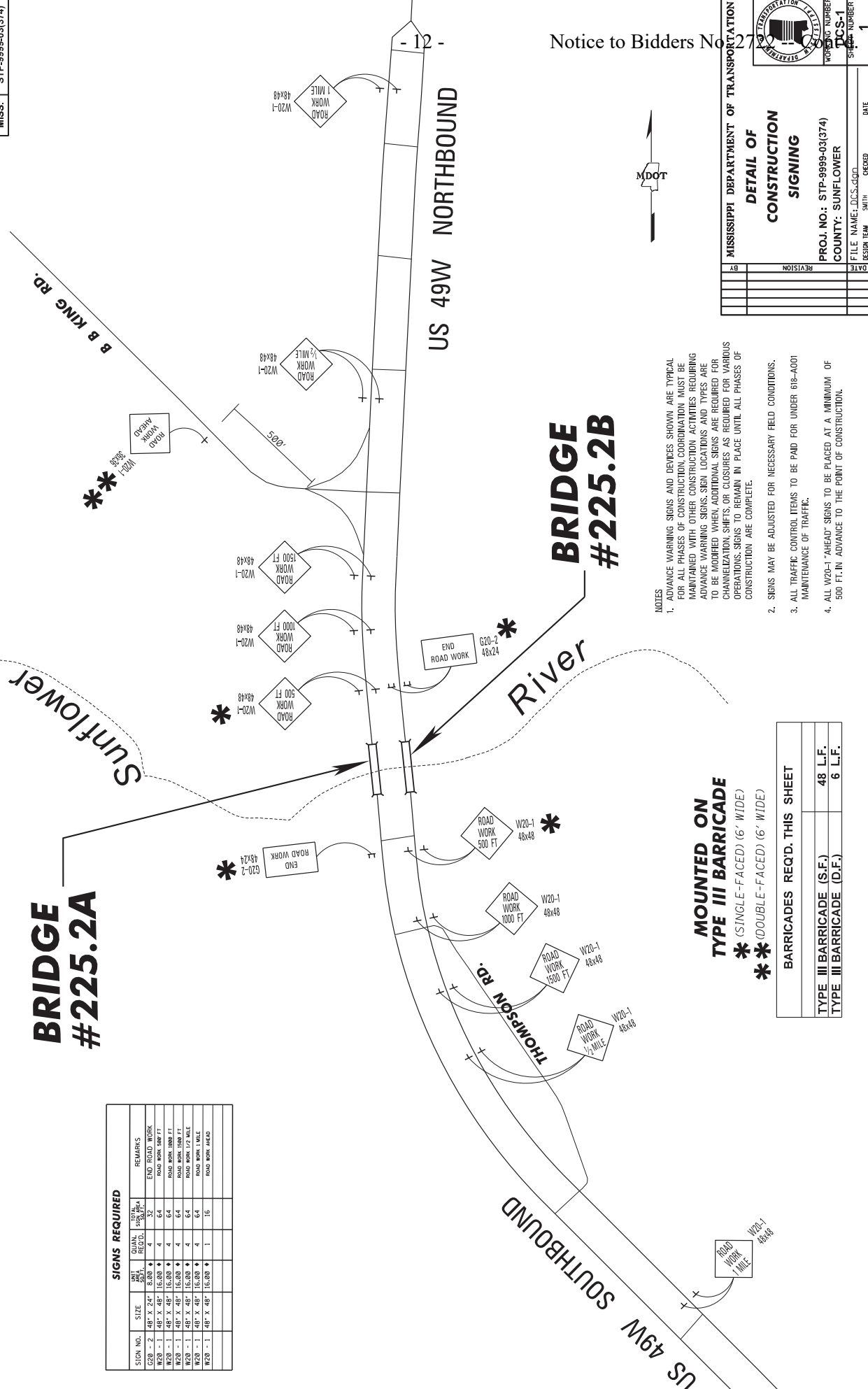
River

B B KING RD.

THOMPSON RD.

US 49W SOUTHBOUND

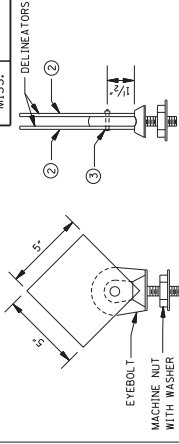
SIGNS REQUIRED				REMARKS
SIGN NO.	SIZE	QTY	REMARKS	
G20 - 2	48" X 24"	4	END ROAD WORK	
W20 - 1	48" X 48"	4	ROAD WORK 500 FT	
W20 - 1	48" X 48"	4	ROAD WORK 1000 FT	
W20 - 1	48" X 48"	4	ROAD WORK 1500 FT	
W20 - 1	48" X 48"	4	ROAD WORK 1 MILE	
W20 - 1	48" X 48"	1	ROAD WORK AHEAD	



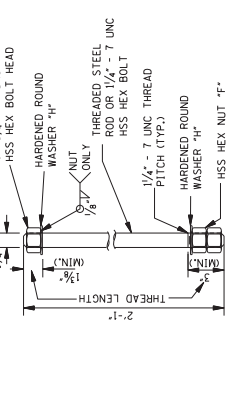
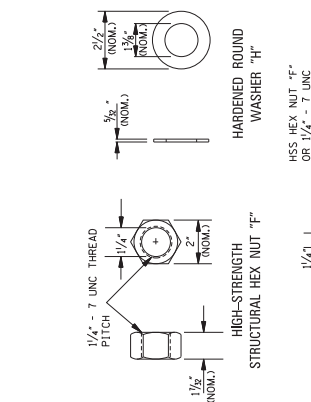
- NOTES**
- ADVANCE WARNING SIGNS AND DEVICES SHOWN ARE TYPICAL FOR ALL PHASES OF CONSTRUCTION. COORDINATION MUST BE MAINTAINED WITH OTHER CONSTRUCTION ACTIVITIES REQUIRING ADVANCE WARNING SIGNS. SIGN LOCATIONS AND TYPES ARE TO BE MODIFIED WHEN ADDITIONAL SIGNS ARE REQUIRED FOR CHANNELIZING, SHIFTS, OR CLOSURES AS REQUIRED FOR VARIOUS OPERATIONS. SIGNS TO REMAIN IN PLACE UNTIL ALL PHASES OF CONSTRUCTION ARE COMPLETE.
 - SIGNS MAY BE ADJUSTED FOR NECESSARY FIELD CONDITIONS.
 - ALL TRAFFIC CONTROL ITEMS TO BE PAID FOR UNDER 618-A001 MAINTENANCE OF TRAFFIC.
 - ALL W20-1 "AHEAD" SIGNS TO BE PLACED AT A MINIMUM OF 500 FT. IN ADVANCE TO THE POINT OF CONSTRUCTION.

MOUNTED ON TYPE III BARRICADE
 ** (SINGLE-FACED) (6' WIDE)
 ** (DOUBLE-FACED) (6' WIDE)

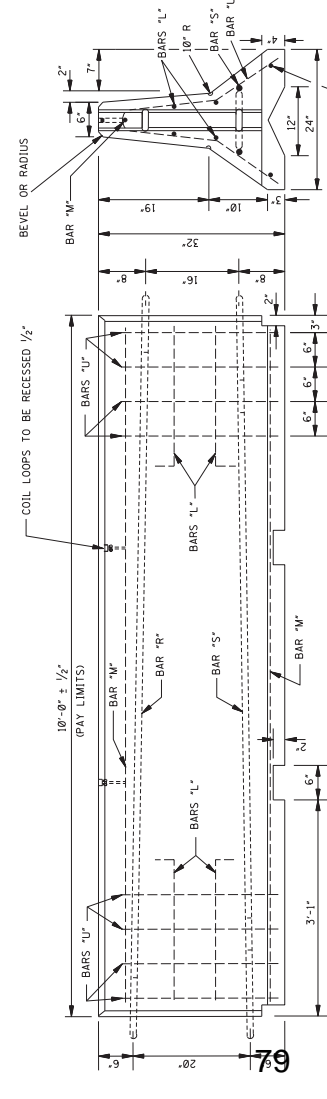
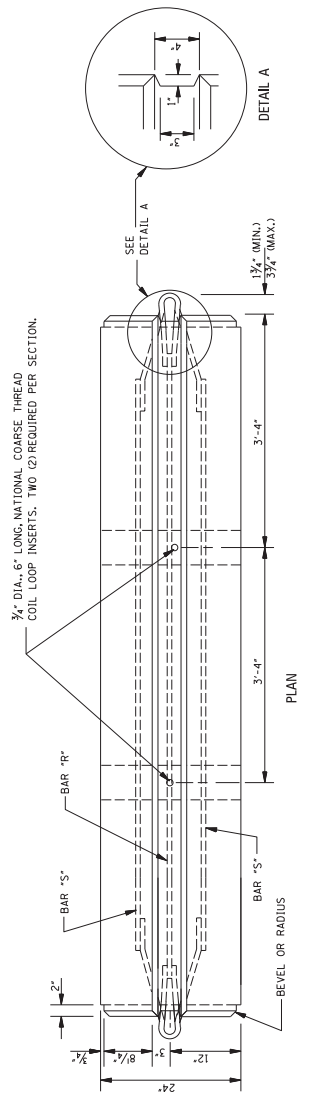
BARRICADES REQ'D. THIS SHEET	
TYPE III BARRICADE (S.F.)	48 L.F.
TYPE III BARRICADE (D.F.)	6 L.F.



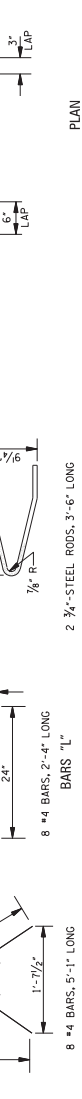
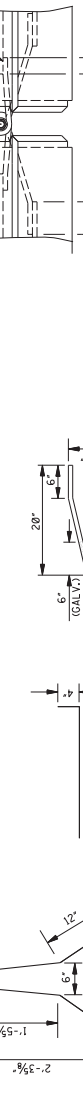
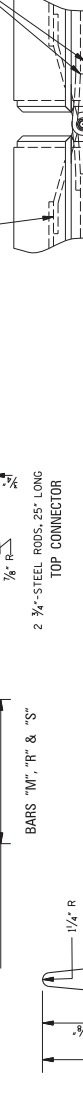
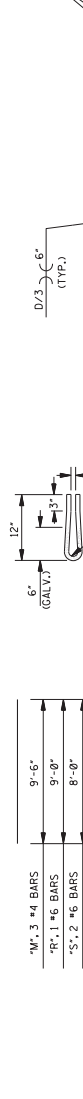
- DELINEATOR, CONCRETE
PRECAST MEDIAN BARRIER, TYPE 1**
- 1 DELINEATORS SHALL BE REQUIRED UNLESS INDICATED OTHERWISE ON THE PLANS. THIS ITEM WILL NOT BE MEASURED SEPARATELY, BUT WILL BE INCLUDED UNDER PAYMENT FOR PRECAST CONCRETE BARRIER.
 - 2 DELINEATORS SHALL BE ENCAPSULATED LEAK PROTECTIVE SHEETING ON ALUMINUM SHEET, 0.080" THICK, OR SHEET STEEL, 14 GAGE, WHICH IS GALVANIZED.
 - 3 ALUMINUM OR STAINLESS STEEL SLOTTED ROUND HEAD MACHINE SCREW, NO. 10, 1/2" LONG, 2-WASHERS AND 1-HEX HEAD NUT (COMMERCIAL QUALITY).
 - 4 THE DELINEATORS SHALL BE INSTALLED FACING TRAFFIC WITH YELLOW ON THE LEFT AND WHITE ON THE RIGHT, UNLESS OTHERWISE SPECIFIED.
 - 5 SPACING OF DELINEATORS: TANGENT SECTION - 10'-0" CURVED SECTION - 10'-0"
 - 6 OPTIONAL DELINEATORS WHICH ARE ON THE MISSISSIPPI DEPARTMENT OF TRANSPORTATION LIST OF APPROVED MATERIALS, WILL BE ACCEPTED.



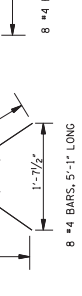
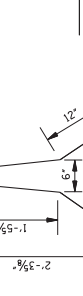
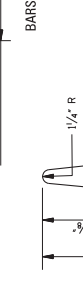
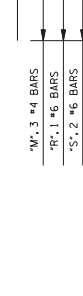
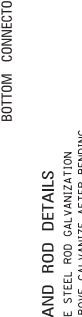
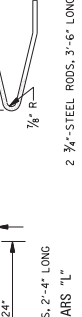
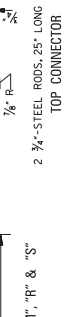
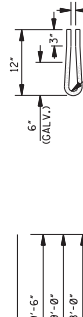
ANCHOR STUD CONNECTOR DETAILS
NOTE: ALTERNATE METHODS OF CONNECTING PRECAST BARRIERS SUCH AS CHOOKS, MAY BE SUBSTITUTED IF APPROVED BY THE ENGINEER.



PRECAST CONCRETE MEDIAN BARRIER
ELEVATION



BAR AND ROD DETAILS
NOTE: WHERE STEEL ROD GALVANIZATION IS SHOWN ABOVE, GALVANIZE AFTER BENDING.



GENERAL NOTES:

1. LIFTING DEVICES AND ATTACHMENTS TO BARRIER SECTIONS SHALL BE AS APPROVED BY THE ENGINEER.
2. PLACE ALL STEEL REINFORCEMENT 2" MINIMUM FROM OUTSIDE FACE OF WALL, EXCEPT AS OTHERWISE SHOWN.
3. THE ANCHOR STUD CONNECTOR SHALL CONFORM TO AASHTO M 314, GRADE 55. THE HSS HEX NUTS AND THE HARDENED ROUND WASHERS SHALL MEET THE REQUIREMENTS OF ASTM A 325.
4. CONCRETE SHALL BE CLASS "B" (CLASS 1 FINISH). REINFORCING STEEL SHALL MEET THE REQUIREMENTS OF AASHTO M 31.
5. STEEL RODS SHALL MEET THE REQUIREMENTS OF ASTM A 36.
6. CONNECTOR RODS, CONNECTOR PINS, NUTS, AND WASHERS SHALL BE GALVANIZED MEETING THE REQUIREMENTS OF AASHTO M 111.

APPROXIMATE QUANTITIES FOR 10' BARRIER			
WEIGHT (lbs.)	REINF. STEEL (cu yd)	STEEL RODS (lbs.)	CONCRETE (cu yd)
3875	184	18	0.931

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN**

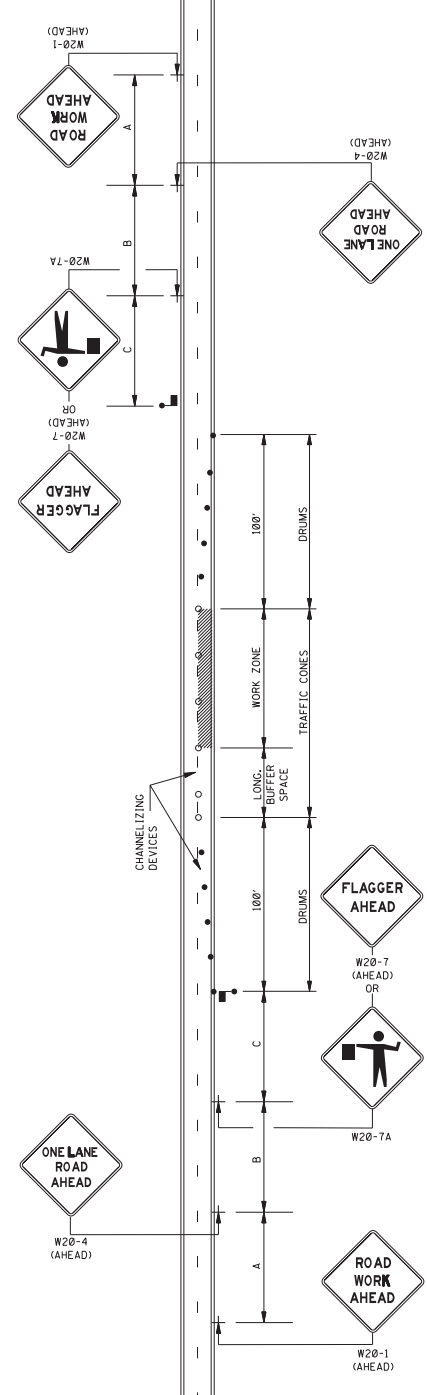
CONCRETE MEDIAN BARRIER (PRECAST) (32")

ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 2722

PLAN NUMBER: 6226

DATE	REVISION



LEGEND
 FLAGGER
 RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
 TRAFFIC CONES (28" HEIGHT MINIMUM)

DISTANCE BETWEEN SIGNS

ROAD TYPE	A	B	C
URBAN (35 MPH OR LESS)	100 FT.	100 FT.	100 FT.
URBAN (40 - 70 MPH)	350 FT.	350 FT.	350 FT.
RURAL	500 FT.	500 FT.	500 FT.
EXPRESSWAY / FREEWAY	1000 FT.	1500 FT.	2640 FT.

- ALL CHANNELIZING DEVICES SHALL BE A MINIMUM OF 28" IN HEIGHT.
- DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 36" X 36" AND BLACK COPY ON FLUORESCENT ORANGE SHEETING.
- WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND ALL CHANNELIZING DEVICES SHALL BE MOVED TO THE SHOULDER EDGE.
- ADDITIONAL FLAGGERS MAY BE NEEDED AS DIRECTED BY THE ENGINEER.
- WHEN WORK IS REQUIRED AT NIGHT, FLAGGER STATIONS SHALL BE ILLUMINATED.
- CHANNELIZING DEVICE TYPES FOR APPROACH AND EXIT TAPPERS:
 - ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28"-HEIGHT)
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

GENERAL NOTES:
 1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE. FLAGGER STATIONS SHALL BE LOCATED SUCH THAT APPROACH AND EXIT TAPPERS ARE LOCATED AT THE STOPPING DISTANCE. STOPPING DISTANCE COLUMN MAY BE USED AS A MINIMUM FOR THIS DISTANCE.

POSTED SPEED AND/OR DESIGN SPEED mph	MAXIMUM CHANNELIZING DEVICES (ft)		LONGITUDINAL BUFFER SPACE (ft)	STOPPING SIGHT DISTANCE
	TAPER	ALONG LANE LINE & WORK ZONE		
25	20	50	55	155
30	20	60	85	200
35	20	70	120	250
40	20	80	170	305
45	20	90	220	360
50	20	100	280	425
55	20	110	335	495
60	20	120	415	570
65	20	130	485	645

* NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

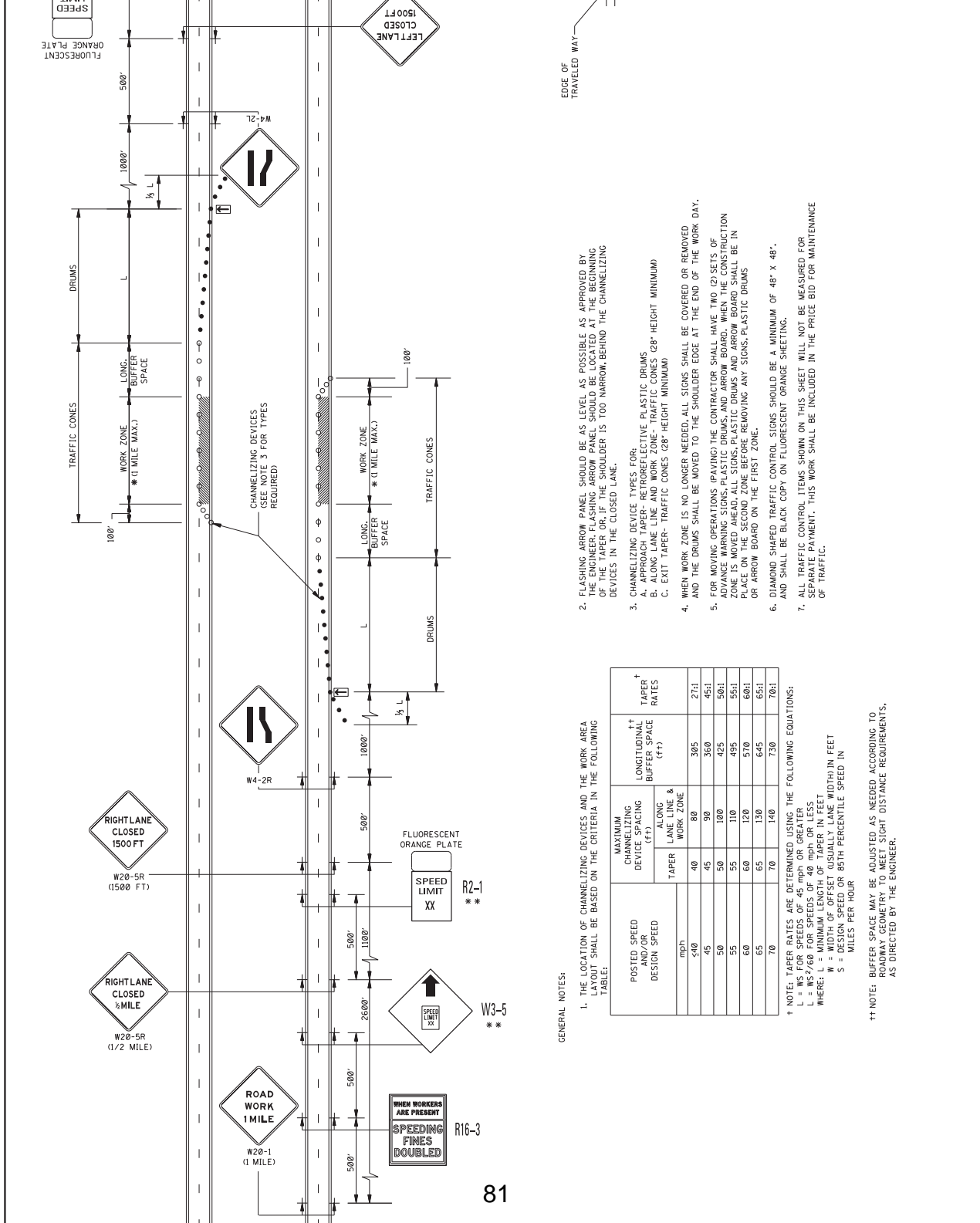
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN

**TRAFFIC CONTROL PLAN
 WITH FLAGGER
 (ONE-LANE CLOSURE OF
 TWO-WAY TRAFFIC)**

ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 6351

BY	REVISION	DATE



NOTE: IF VEHICLE MOUNTED ARROW BOARDS USED, SHOULD BE AS HIGH AS PRACTICAL.

7' MIN.

EDGE OF TRAVELED WAY

LEGEND

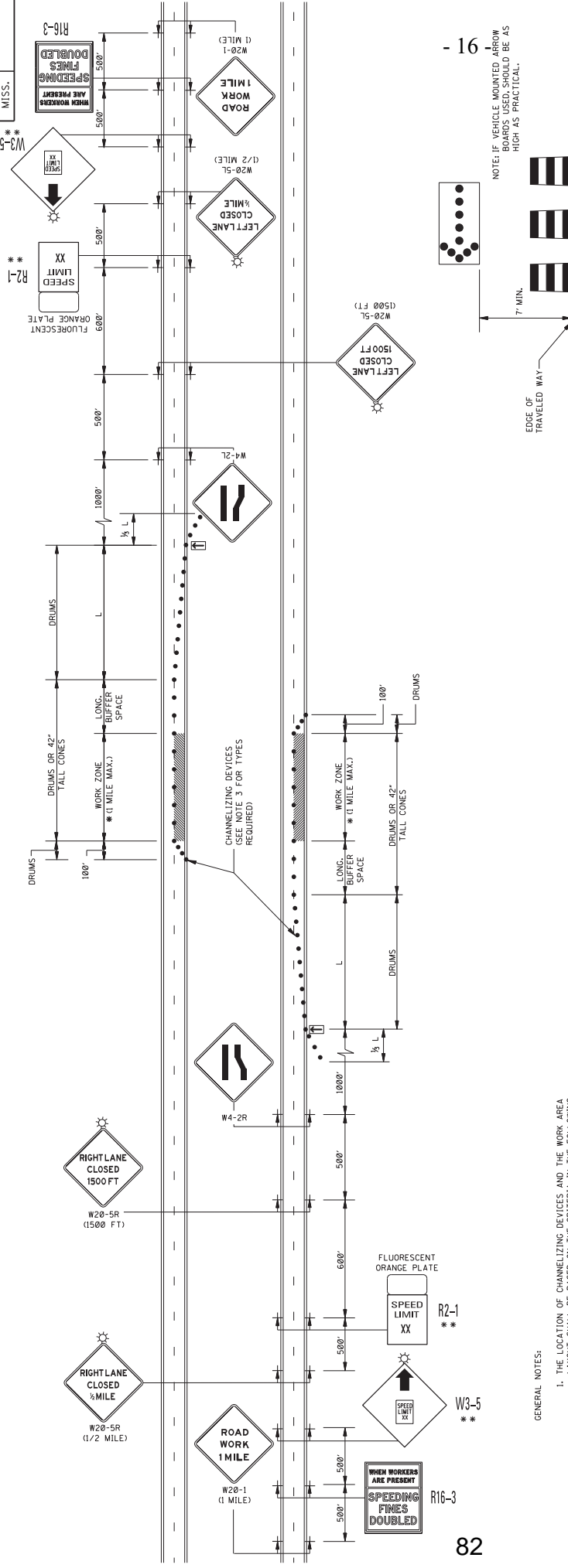
* OR AS SHOWN ELSEWHERE ON THE PLANS.
** THE SPEED ON R2-1 AND W3-5 SIGNS SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT UNLESS OTHERWISE DIRECTED BY COMMISSION ORDER.
○ FLASHING ARROW PANEL (TYPE "C")
● RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
○ TRAFFIC CONES (28" HEIGHT MINIMUM)

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LIMIT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:
2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICE TYPES FOR:
 - A. ALONG LANE LINE AND WORK ZONE - TRAFFIC CONES (28" HEIGHT MINIMUM)
 - B. EXIT TAPER - TRAFFIC CONES (28" HEIGHT MINIMUM)
 - C. CHANNELIZING DEVICE TYPES FOR:
 1. WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
 2. FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
 3. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48", AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
 4. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

POSTED SPEED AND/OR DESIGN SPEED (mph)	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft)	TAPER RATES
	ALONG LANE LINE & WORK ZONE	TAPER		
50	40	80	395	27:1
45	45	90	360	45:1
50	50	100	425	50:1
55	55	110	495	55:1
60	60	120	570	60:1
65	65	130	645	65:1
70	70	140	730	70:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
L = WS FOR SPEEDS OF 45 MPH OR GREATER
L = WS²/60 FOR SPEEDS OF 40 MPH OR LESS
WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
W = WIDTH OF OFFSET USUALLY LANE WIDTH IN FEET
S = SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

†† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.



GENERAL NOTES:

- THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (FT)	LONGITUDINAL BUFFER SPACE RATES		TAPER †
		ALONG BUFFER SPACE & WORK ZONE	WORK ZONE	
mph				
<40	40	80	305	27:1
45	45	90	360	45:1
50	50	100	425	50:1
55	55	110	495	55:1
60	60	120	570	60:1
65	65	130	645	65:1
70	70	140	730	70:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 $L = 100 \times S$ FOR SPEEDS OF 40 MPH OR LESS
 $L = W \times S^2 / 60$ FOR SPEEDS OF 40 MPH OR LESS
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET USUALLY LANE WIDTH IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

†† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO THE SHORT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

LEGEND

- * OR AS SHOWN ELSEWHERE ON THE PLANS.
- ** THE SPEED ON R2-1 AND W3-5 SIGNS SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT UNLESS OTHERWISE DIRECTED BY COMMISSION ORDER.
- ☐ FLASHING ARROW PANEL (TYPE "C")
- RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
- ☼ TYPE "B" WARNING LIGHTS

NOTE: IF VEHICLE MOUNTED ARROW BOARD IS USED, IT SHOULD BE AS HIGH AS PRACTICAL.



- FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
- CHANNELIZING DEVICES:
 - ALL CHANNELIZING DEVICES IN TAPERS SHALL BE RETROREFLECTIVE FREE STANDING PLASTIC DRUMS.
 - CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER RETROREFLECTIVE FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.
 - ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE.
 - RETROREFLECTORIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE M.U.T.C.D.
- FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION OPERATION IS COMPLETED, THE ADVANCE WARNING SIGNS SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS, OR ARROW BOARD ON THE FIRST ZONE.
- DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" x 48". ALL SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

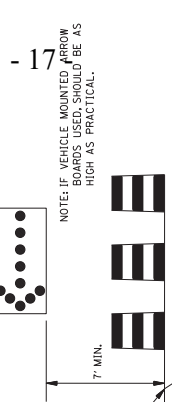
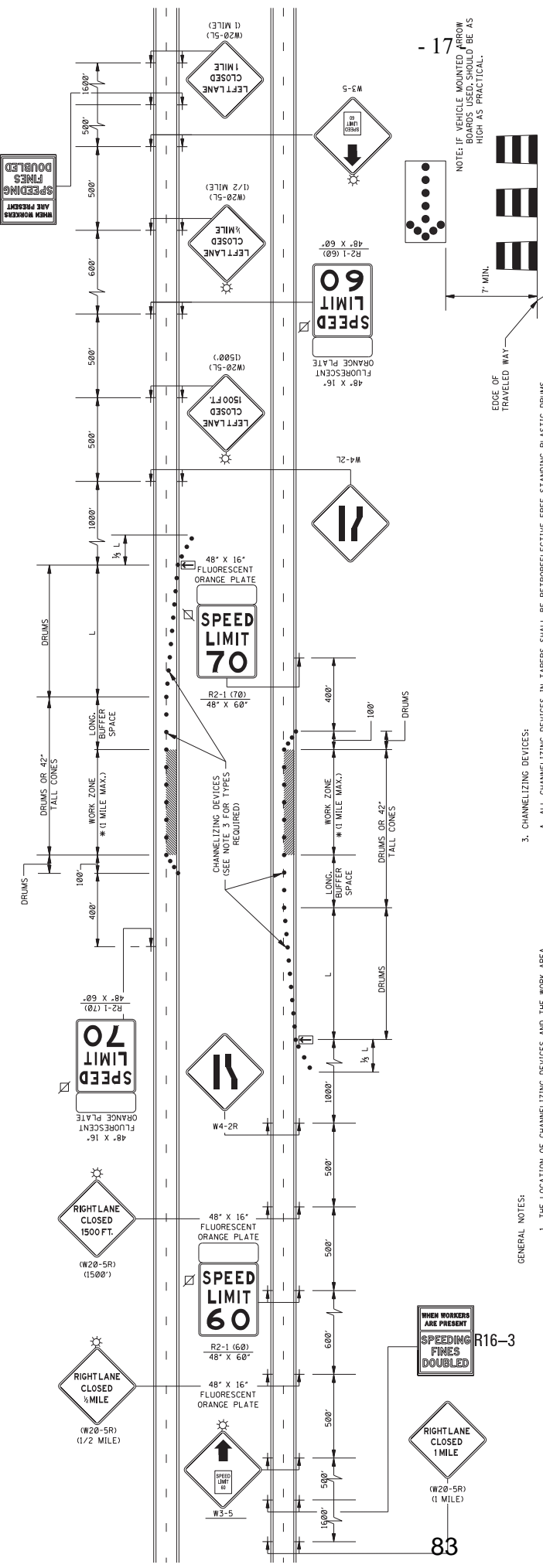
Notice to Bidders No. 2722

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN

**TRAFFIC CONTROL PLAN
 FOR POSTED SPEED LIMIT
 LESS THAN 65 MPH
 (4-LANE: MEDIUM LANE
 OR OUTSIDE LANE CLOSURE)
 (EXTENDED PERIOD)**

DATE	REVISION	BY

ISSUE DATE: AUGUST 01, 2017
 PLAN NUMBER: 6353



3. CHANNELIZING DEVICES:

- A. ALL CHANNELIZING DEVICES IN TAPERS SHALL BE RETROREFLECTIVE FREE STANDING PLASTIC DRUMS.
- B. CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER RETROREFLECTIVE FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.

- C. ALL CHANNELIZING DEVICES SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE M.U.T.I.C.D.
- D. RETROREFLECTORIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE M.U.T.I.C.D.

- 4. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48" AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.

- 5. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR ADJACENT TO A WORK ZONE SHALL BE AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON FACE OF SIGN.

- 6. ADDITIONAL REDUCED REGULATORY SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE AND EXIT TO THE SPEED ZONE. TWO (2) WILL BE REQUIRED FOR EACH RAMP AND LOCATION WILL BE DETERMINED BY THE ENGINEER.

- 7. THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE, MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.

- 8. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH. FOR POSTED SPEED LIMIT OF 65 MPH, THE REDUCED SPEED LIMIT WILL BE 55 MPH.

- 9. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS REQUIRED FOR LANE CLOSURE.

- 10. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

GENERAL NOTES:

- 1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED (mph)	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft)	TAPER [†] RATES
	TAPER	ALONG BUFFER SPACING & WORK ZONE		
<40	40	80	305	27:1
45	45	90	360	45:1
50	50	100	425	59:1
55	55	110	495	55:1
60	60	120	570	68:1
65	65	130	645	65:1
70	70	140	730	70:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 L = WS FOR SPEEDS OF 45 mph OR GREATER
 L = WS/100 FOR SPEEDS OF 40 mph OR LESS
 WHERE: W = MINIMUM LENGTH OF TAPER IN FEET
 S = WIDTH OF OFFSET (USUALLY LANE WIDTH) IN FEET
 MILES PER HOUR

†† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

- 2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE. THE ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.



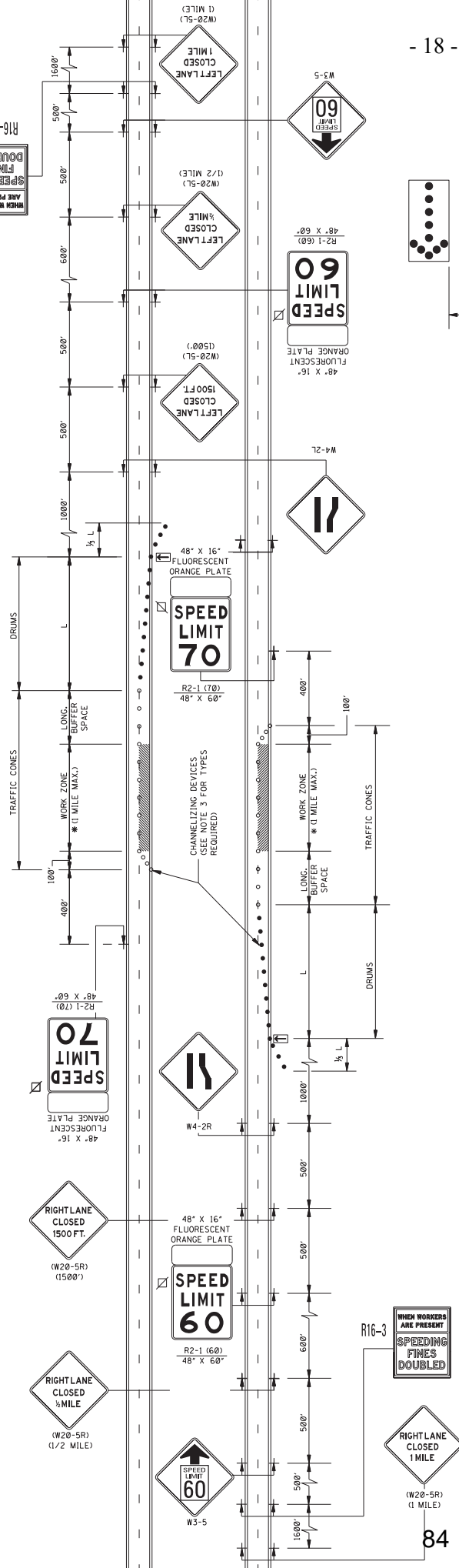
83

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 TRAFFIC CONTROL PLAN
 FOR POSTED SPEED LIMIT
 OF 65 OR 70 MPH
 (INTERSTATES AND OTHER 4-LANE
 DIVIDED HIGHWAYS)
 (MEDIAN LANE OR OUTSIDE
 LANE CLOSURE) (EXTENDED PERIOD)

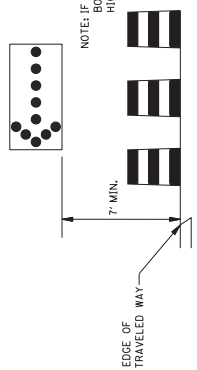
DATE	REVISION

ISSUE DATE: AUGUST 01, 2017
 SHEET NUMBER: 6354

WORKERS PRESENT
SPEEDING FINES DOUBLED
R16-3



Notice to Bidders No. 2722



NOTE: IF VEHICLE MOUNTED ARROW BOARDS USED, SHOULD BE AS HIGH AS PRACTICAL.

LEGEND

- * OR AS SHOWN ELSEWHERE ON THE PLANS.
- FLASHER ARROW PANEL (TYPE "C")
- BLACK LEGEND AND BORDER ON WHITE BACKGROUND
- RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
- TRAFFIC CONES (28" HEIGHT)

3. CHANNELIZING DEVICE TYPES FOR:
 - A. APPROACH TAPER- RETROREFLECTIVE PLASTIC DRUMS
 - B. ALONG LANE LINE AND WORK ZONE- TRAFFIC CONES (28" HEIGHT MINIMUM)
 - C. EXIT TAPER- TRAFFIC CONES (28" HEIGHT MINIMUM)
4. WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND THE DRUMS SHALL BE MOVED TO THE SHOULDER EDGE AT THE END OF THE WORK DAY.
5. FOR MOVING OPERATIONS (PAVING) THE CONTRACTOR SHALL HAVE TWO (2) SETS OF ADVANCE WARNING AND REGULATORY SIGNS, PLASTIC DRUMS, AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS, AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
6. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48". AND SHALL BE BLACK COPY ON FLOURESCENT ORANGE SHEETING.
7. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED AS DIRECTED BY THE ENGINEER WHILE THE REDUCED SPEED LIMIT IS IN EFFECT. THIS SHALL NOT BE USED ON THE FACE OF SIGN.
8. ADDITIONAL REDUCED REGULATORY SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP WITHIN THE SPEED ZONE. TWO (2) WILL BE REQUIRED FOR EACH RAMP AND LOCATION WILL BE DETERMINED BY THE ENGINEER.
9. WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A FACILITY REQUESTING A SPEED LIMIT REDUCTION.
10. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 65 MPH. THE REDUCED SPEED LIMIT WILL BE 55 MPH.
11. A FLOURESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS AND "REDUCED SPEED AHEAD" SIGNS REQUIRED FOR LANE CLOSURE.
12. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

GENERAL NOTES:
1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LIMIT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (ft)		LONGITUDINAL BUFFER SPACE (ft)	TAPER RATES
	ALONG LANE LINE & WORK ZONE	TAPER		
50	40	80	395	27:1
45	45	90	360	45:1
50	50	100	425	50:1
55	55	110	495	55:1
60	60	120	570	60:1
65	65	130	645	65:1
70	70	140	730	70:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
L = WS FOR SPEEDS OF 45 mph OR GREATER
L = WS²/60 FOR SPEEDS OF 40 mph OR LESS
WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
W = WIDTH OF OFFSET USUALLY LANE WIDTH IN FEET
S = WIDTH OF OFFSET FOR 85TH PERCENTILE SPEED IN MILES PER HOUR

†† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

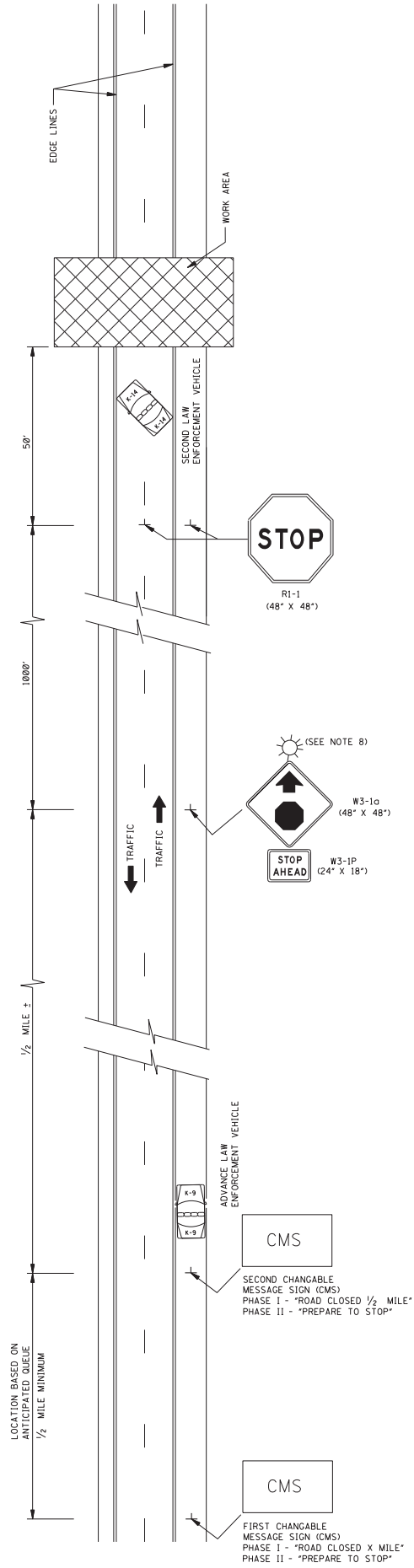
2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
TRAFFIC CONTROL PLAN
FOR POSTED SPEED LIMIT
OF 65 OR 70 MPH
(INTERSTATES AND OTHER 4-LANE
DIVIDED HIGHWAYS)
(MEDIAN LANE OR OUTSIDE
LANE CLOSURE) (WORK DAY ONLY)

ISSUE DATE: AUGUST 01, 2017

DATE	REVISION

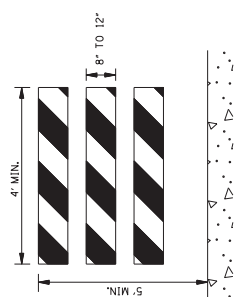
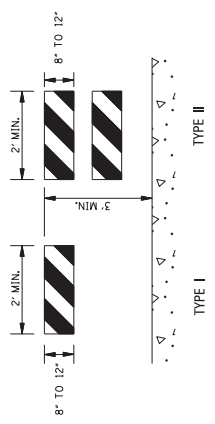
PROJECT NUMBER: 6355
SHEET NUMBER: 84



- GENERAL NOTES:**
- THIS TYPE OF HIGHWAY CLOSURE SHOULD ONLY BE USED FOR CONSTRUCTION OPERATIONS THAT WILL BE COMPLETED WITHIN 30 MINUTES. AFTER THE HIGHWAY HAS BEEN CLOSED AND REOPENED VIA THE CONTRACT DOCUMENTS, ALL OTHER SHORT DURATION CLOSURES, EXCEPT WITH THE APPROVAL OF THE ENGINEER.
 - AT LEAST TWO LAW ENFORCEMENT OFFICERS AND TWO LAW ENFORCEMENT VEHICLES SHOULD BE PROVIDED ON EACH APPROACH TO THE CLOSURE. EACH LAW ENFORCEMENT VEHICLE SHOULD HAVE A ROOF MOUNTED FLASHING BLUE LIGHT OR LIGHT BAR.
 - RESTRICTIONS ON ROAD CLOSURES ARE SPECIFIED IN THE CONTRACT DOCUMENT.
 - THE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD BE MOVED BACK AS REQUIRED BY THE QUEUING OF STOPPED VEHICLES.
 - IF QUEUE EXCEEDS THE FIRST CHANGABLE MESSAGE SIGN (CMS) AT ANY TIME DURING A CLOSURE, THE TRAFFIC CONTROL PLAN SHOULD BE ADJUSTED AS NECESSARY WITH APPROVAL OF THE ENGINEER.
- TRAFFIC CONTROL FOR THE CLOSURE SHOULD BE ACCOMPLISHED IN THE FOLLOWING ORDER:
 - FIRST CHANGABLE MESSAGE SIGN (CMS)
 - SECOND CHANGABLE MESSAGE SIGN (CMS)
 - ADVANCE LAW ENFORCEMENT VEHICLE, LIGHTS AND FLASHERS ON.
 - W3-1a (48" X 48") AND W3-1P (24" X 18") SIGNS ERECTED.
 - R1-1 (48" X 48") SIGNS ERECTED TO STOP TRAFFIC. THE ORDER OF ERECTION SHOULD BE IN THE FOLLOWING ORDER: RIGHT SHOULDER THEN CENTER.
 - SECOND LAW ENFORCEMENT VEHICLE, LIGHTS AND FLASHERS ON.
 - TRAFFIC CONTROL SHOULD BE REMOVED IN THE FOLLOWING ORDER:
 - WITH TRAFFIC STOPPED REMOVE THE "R1-1 SHOULDER THEN SIGN ON THE RIGHT CENTER THEN SIGN ON THE RIGHT SHOULDER. SECOND LAW ENFORCEMENT VEHICLE LEADS TRAFFIC THROUGH WORK AREA.
 - AFTER ALL STOPPED VEHICLES HAVE STARTED MOVING THE "W3-1a (48" X 48") AND "W3-1P (24" X 18") SIGNS SHOULD BE REMOVED. THESE SIGNS MAY BE COVERED IF RE-USE IS IMMINENT.
 - AFTER ALL VEHICLES HAVE RESUMED APPROXIMATELY NORMAL SPEED, THE CHANGABLE MESSAGE SIGNS TURNED OFF.
 - UNILLUMINATED SECTIONS OF HIGHWAYS SHOULD NOT BE CLOSED DURING HOURS OF DARKNESS EXCEPT FOR EMERGENCIES OR WITH THE APPROVAL OF THE ENGINEER. IF DARKNESS OCCURS DURING HOURS OF DARKNESS, A TYPE B HIGH INTENSITY FLASHING BARRICADE WARNING LIGHT SHALL BE USED ON EACH W3-1a SIGN.
 - IF AN ENTRANCE RAMP IS LOCATED BETWEEN THE SECOND CMS AND R1-1, THE CMS, SIGNS SHOULD ALSO BE ERECTED ON THE RAMP SHOULDER.
 - THE ABOVE DURATION WILL APPLY TO EACH APPROACH TO THE CLOSURE.
 - ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC, INCLUDING SECURING LAW ENFORCEMENT SERVICES.

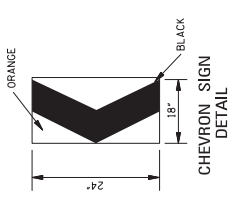
DATE	BY	REVISION

SHORT DURATION CLOSING OF TWO-LANE TWO-WAY HIGHWAYS

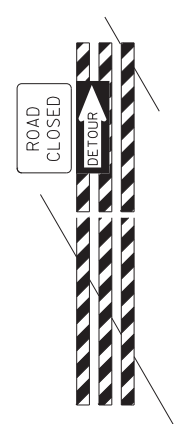


STANDARD BARRICADES

1. THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).
2. RAIL STRIPE SHOULD BE 6 INCHES, EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.
3. DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
4. FOR ADDITIONAL INFORMATION OR DETAILS: SEE MUTCD, LATEST EDITION.
5. BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES WHICH REQUIRE CRASHWORTHINESS ACCEPTANCE LETTERS. TO DATE, 2-IN. THICK TIMBER RAILS HAVE NOT BEEN SUCCESSFULLY CRASH TESTED. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE: http://safety.fhwa.dot.gov/roadway_dept/policy/guide/road_hardware/cat2.cfm



1. A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
2. THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.
3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.

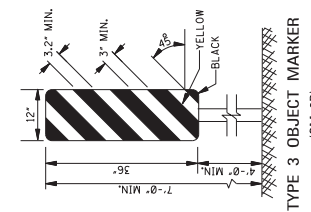


BARRICADE CLOSING A ROAD

BARRICADE CHARACTERISTICS

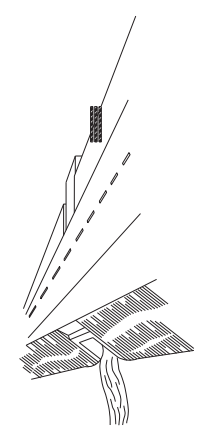
	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF FACTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS

- * 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- ** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 IN² OF REFLECTIVE AREA FACING TRAFFIC.



TYPE 3 OBJECT MARKER (OM-3R)

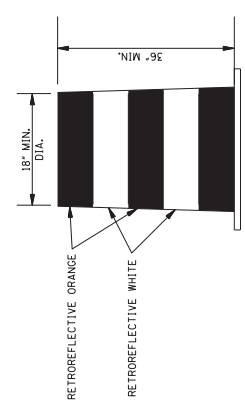
1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ENDS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.
2. THE OM-3R IS SHOWN. THE OM-3L IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
3. THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



WING BARRICADES

1. WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
2. WING BARRICADES SHOULD BE USED:
 - A. IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - B. IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.

PLASTIC DRUM STRIPING DETAIL



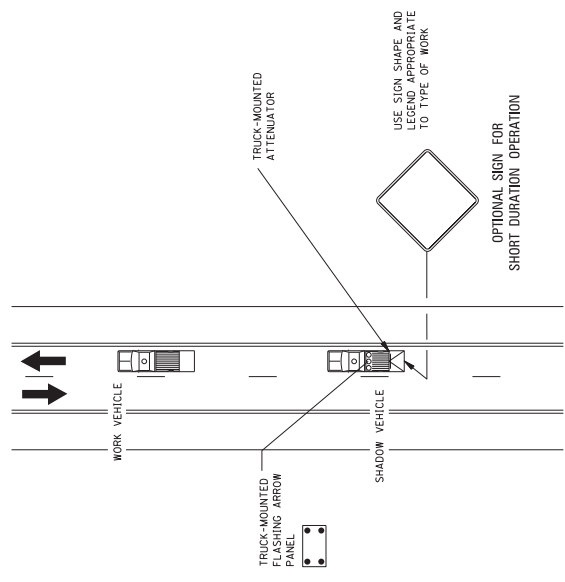
1. PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIENT METHOD OF STRIPING TO BE USED IN CONSTRUCTION ZONES WHERE PERMANENT STRIPING IS NOT PRACTICAL. THE DRUMS SHALL BE MARKED WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT COLOR ON DRUMS SHALL BE ORANGE WITH FOUR (4) RETROREFLECTIVE, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
2. DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
3. WHERE PRACTICAL PLASTIC DRUMS SHOULD BE PLACED NO CLOSER THAN 10' FROM THE EDGE OF TRAVELED LANE.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS

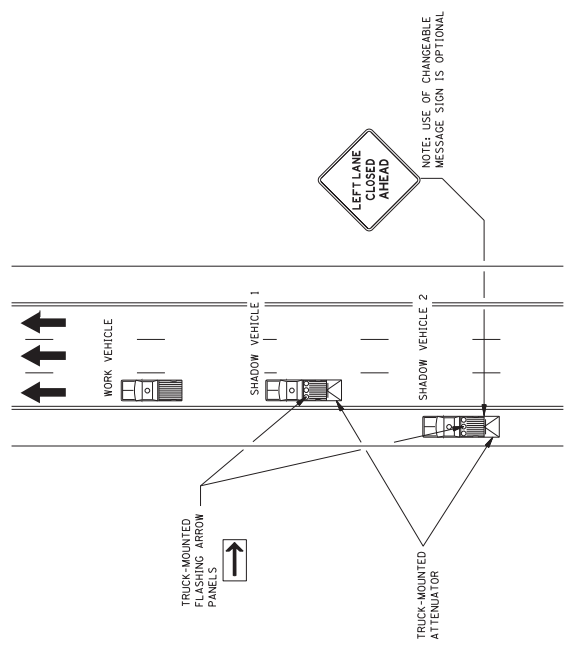
PROJECT NUMBER: CP-8
DRAWING NUMBER: 6358
ISSUE DATE: AUGUST 01, 2017

MOBILE OPERATIONS ON TWO-LANE ROAD



MOBILE OPERATIONS ON TWO-LANE ROAD

MOBILE OPERATIONS ON MULTILANE ROAD



MOBILE OPERATIONS ON MULTILANE ROAD

NOTES FOR TWO-LANE OPERATION:

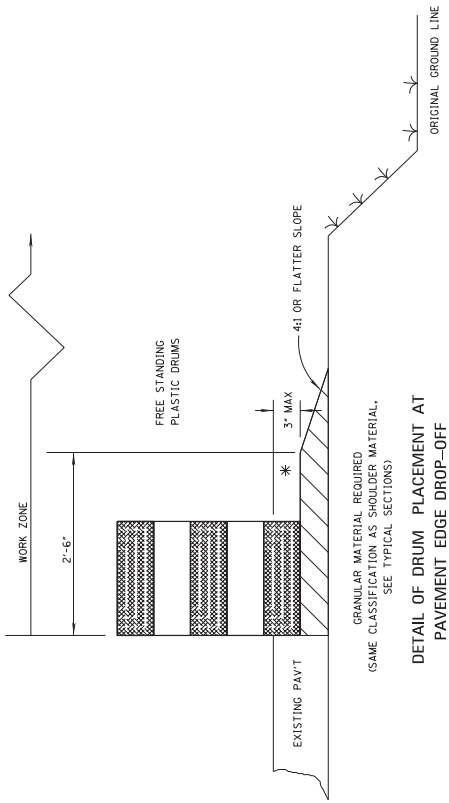
1. WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY, AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
2. THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO THE TYPE OF OPERATION. SHADOW VEHICLES SHOULD ALWAYS BE USED TO WARN TRAFFIC OF THE OPERATION AHEAD, WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR. THE SHADOW VEHICLE SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
3. ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
4. A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
5. THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW VEHICLES SHALL BE EQUIPPED WITH TWO HIGH-INTENSITY FLASHING LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN. SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
6. VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBTURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
7. ARROW BOARD TO BE USED IN CAUTION MODE.
8. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

NOTES FOR MULTILANE LANE OPERATION:

1. VEHICLES USED FOR THESE OPERATIONS SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT, SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLAGS, SIGNS, OR ARROW PANELS.
2. SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK MOUNTED ATTENUATOR (TMA), AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON SHADOW VEHICLE 2 SO AS NOT TO OBTURD THE ARROW PANEL.
3. SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK-MOUNTED ATTENUATOR (TMA).
4. SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
5. WHEN ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, SHADOW VEHICLE 2 SHOULD BE ELIMINATED.
6. ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE SHOULD BE USED (i.e., VEHICLE 3 ON THE SHOULDER (IF PRACTICAL), VEHICLE 2 IN THE CLOSED LANE, AND VEHICLE 1 IN THE CLOSED LANE).
7. ARROW PANELS SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
8. WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
9. VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBTURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
10. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

BY	REVISION

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO-LANE ROADS	
	ISSUE DATE: AUGUST 01, 2017
	DRAWING NUMBER CP-9
	SHEET NUMBER 6359



DETAIL OF DRUM PLACEMENT AT PAVEMENT EDGE DROP-OFF

NOTES:

- * A. PAVEMENT EDGE DROP-OFF
 1. IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (750'±0.C.).
 2. TWO AND ONE QUARTER TO THREE INCHES-PLACE DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MILES PER HOUR OR GREATER. CONES MAY BE USED IN PLACE OF DRUMS, PANELS, AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE M.U.T.C.D. (1 / 3 L, WHERE L IS THE TAPER LENGTH IN FEET).
 3. GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 4:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED.
 4. FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
 5. LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.
- * B. DRUM SPACING
 1. TANGENTS = 2 X S
 2. TAPERS = L / 3
 - WHERE L = L / 3
 - L = TAPER LENGTH IN FEET
 - S = SPEED IN MPH (POSTED OR 85 PERCENTILE)
 - W = WIDTH OF OFFSET IN FEET
- * C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

MIN. SPEED (MPH)	LENGTH (FEET)
20	35
25	55
30	75
35	95
40	110
45	120
50	130
55	140
60	150
65	160
70	170
75	180
80	190
85	200

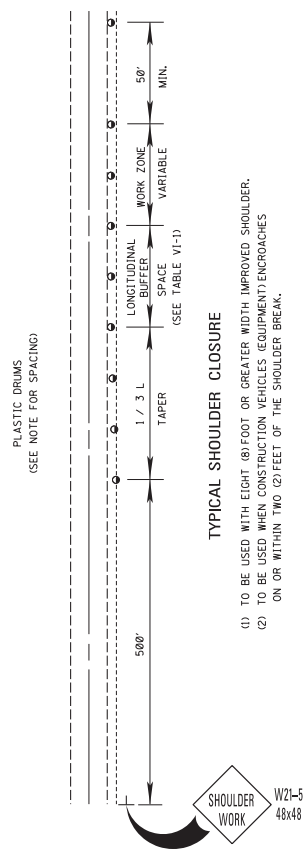
* POSTED SPEED, OFF-PEAK 85 PERCENTILE SPEED (MILES PER HOUR), OR THE ANTICIPATED OPERATING SPEED IN MPH.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TRAFFIC CONTROL DETAILS
DRUM PLACEMENT
AND
SHOULDER CLOSURE**

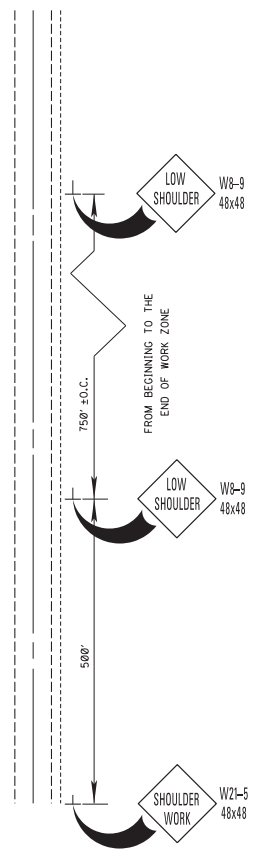
BY	REVISION	DATE

ISSUE DATE: AUGUST 01, 2017
DRAWING NUMBER: CP-16
SHEET NUMBER: 63/66

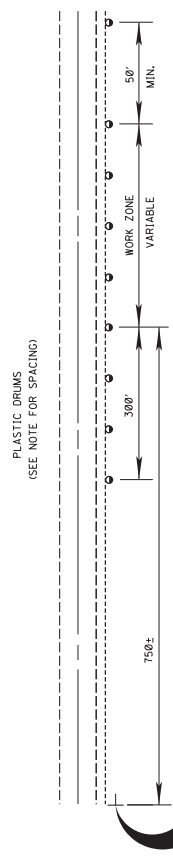


TYPICAL SHOULDER CLOSURE

- (1) TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
- (2) TO BE USED WHEN CONSTRUCTION VEHICLES (EQUIPMENT) ENCROACHES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.



TYPICAL SHOULDER WORK #1
(SEE NOTE A-1 THIS SHEET)

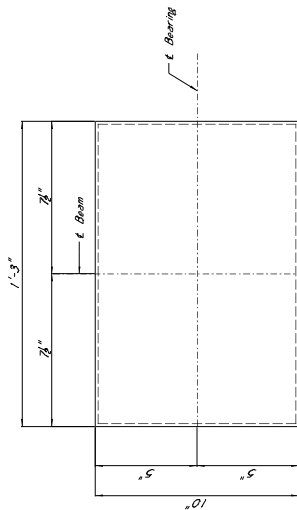


TYPICAL SHOULDER WORK #2

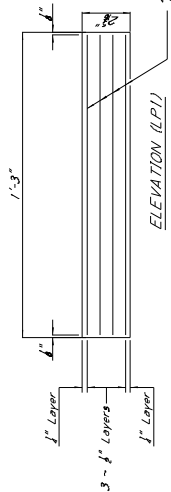
NOTE:
WORK OUTSIDE TWO (2) FOOT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLACING DRUMS ALONG THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA, OR SEE NOTE A-3 THIS SHEET.

108401301		Bridge 10835	Bridge 10844	
907-808-A003	Joint Repair Without Epoxy	166	326	LF
907-823-B001	Saw Cut, Type I	166	326	LF
907-823-A001	Preformed Joint Seal, Type I	83	163	LF
907-824-PP006	Bridge Repair, Bearing Replacement	12	36	EA
907-824-PP006	Bridge Repair, Cap Cleaning	5	6	EA

108401302		Bridge 14545	Bridge 14546	
907-808-A003	Joint Repair Without Epoxy	402	244	LF
907-823-B001	Saw Cut, Type I	402	244	LF
907-823-A001	Preformed Joint Seal, Type I	201	122	LF
907-824-PP006	Bridge Repair, Bearing Replacement	72	24	EA
907-824-PP006	Bridge Repair, Cap Cleaning	7	7	EA



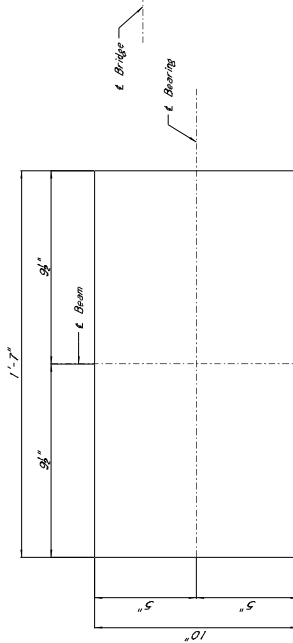
PLAN (LPI)



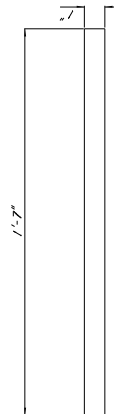
ELEVATION (LPI)

LAMINATED PAD DETAILS (LPI)

The appearance procedure shall be in accordance with Section 714.10.6 of the Specifications. Casters shall have a minimum shear modulus of 120 M.S.T. at 73°F. Bearing area on top of cap shall be cast smooth & true to grade.

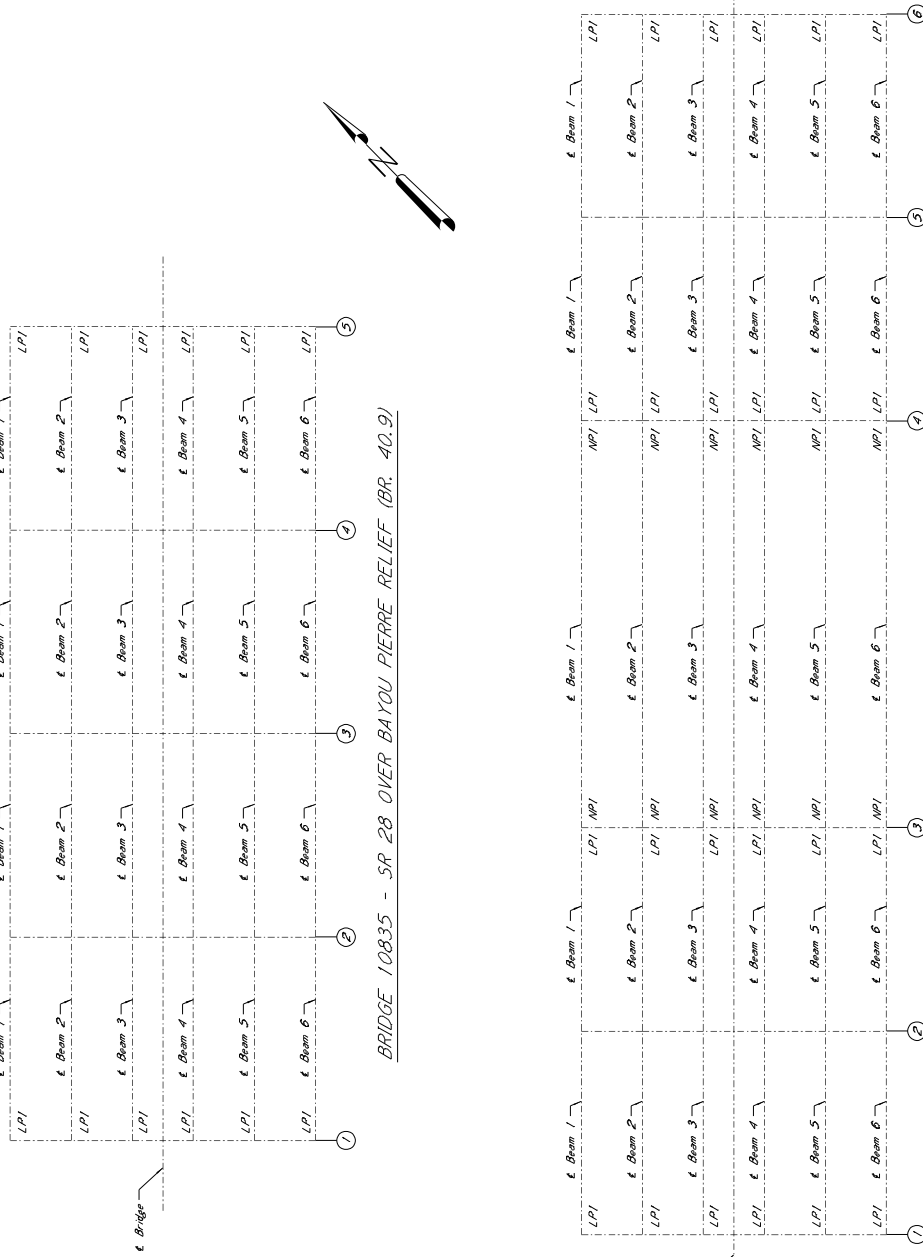


PLAN (NPI)



ELEVATION (NPI)

Casters shall have a minimum shear modulus of 120 M.S.T. at 73°F. Pads shall be vulcanized to steel bearing plates. In no case shall neoprene pads be split out. The top of cap shall be finished smooth and true to grade.



BRIDGE 10835 - SR 28 OVER BAYOU PIERRE RELIEF (BR. 40.9)

BRIDGE 10844 - SR 28 OVER JONES CREEK (BR. 43.0)

GENERAL NOTES:

1. Specifications: Mississippi Standard Specifications For Road and Bridge Construction.
2. No construction details will be permitted except by written approval of the Director of Structures, State Bridge Engineer.
3. Minor changes in the construction details may be made by the Contractor.
4. Work for which no pay item is provided will not be paid for directly.
5. All work shall be done in accordance with the specifications for the existing structure. The Contractor shall be responsible for providing the elements of the new construction to ensure a proper fit with the existing structure.
6. Any damage that occurs to the existing structure during the duration of the project shall be repaired to the satisfaction of the Engineer.
7. The work that is removed from the bridge shall become the property of the Contractor and shall be removed from the construction site.

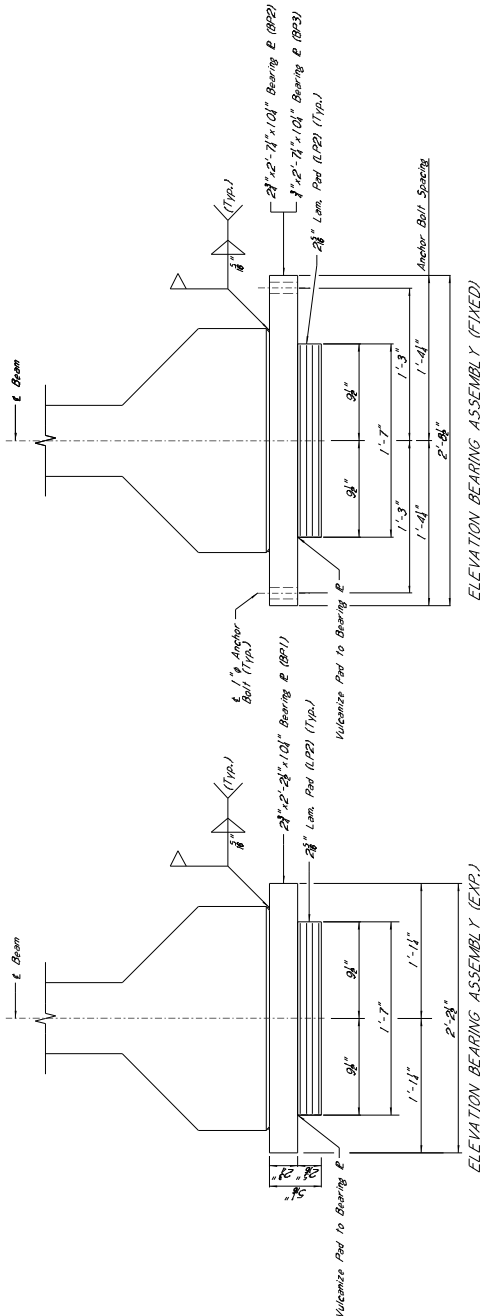
STRUCTURAL STEEL NOTES:

All steel plates shall conform to A.S.T.M. designation A709, Grade 50. All steel plates shall be new. The electric arc process and shall conform to the AWS D1.1 Code Specification for Structural Steel Welding. The performance shall, when applicable, and as directed herein, on each work shift where welding or other significant work is performed.

CONTRACTOR SUBMITTAL NOTES:

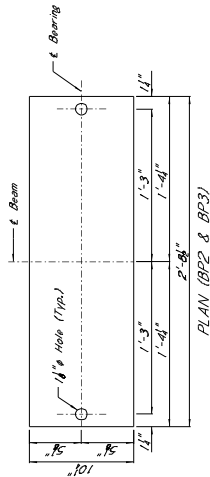
Plans to be constructed or fabricated, the following shall be submitted to the Director of Structures, State Bridge Engineer through the Project Engineer for approval. No fabrication shall begin until all submittals have been authorized by the Director of Structures, State Bridge Engineer.

1. FIELD VERIFICATION SUBMITTAL:
All dimensions of the existing, bearing assemblies, caps, & drawings shall be submitted to the State Bridge Engineer.
2. SHOP DRAWING SUBMITTAL:
The Contractor shall submit shop drawings of new bearing assemblies, caps, & drawings for approval by the State Bridge Engineer.
3. WELDING SUBMITTAL:
A procedure for storage and handling of welding electrodes, and a procedure for all welders.
4. JACKING:
A Plan SUBMITTAL, submit a set of brackets and jacking arrangement plans along with design calculations. The Contractor shall employ the services of a Mississippi registered Professional Engineer to design the jacking arrangement. The design engineer's plans and design calculations shall bear the seal of the Professional Engineer.

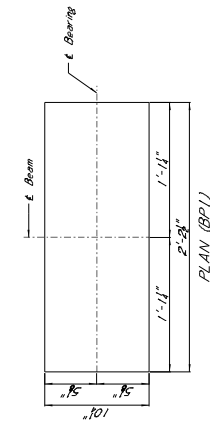


ELEVATION BEARING ASSEMBLY (EXIST)

ELEVATION BEARING ASSEMBLY (PROP.)



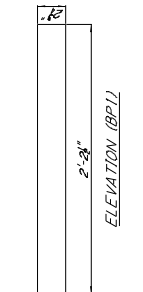
PLAN (BP2 & BP3)



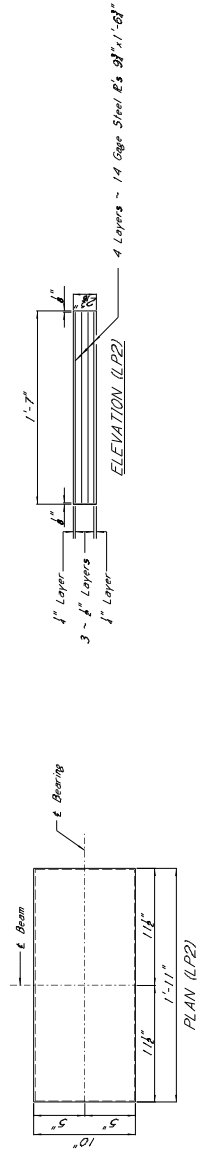
PLAN (LP1)



ELEVATION (BP2 & BP3)



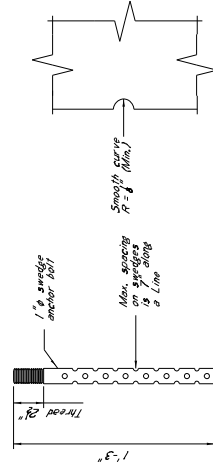
ELEVATION (LP1)



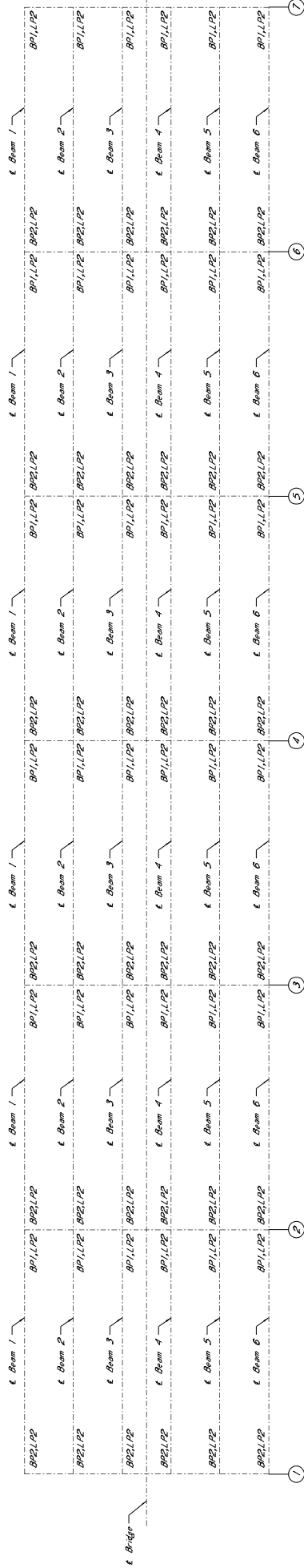
PLAN (LP2)

LAMINATED PAD DETAILS (LP2)

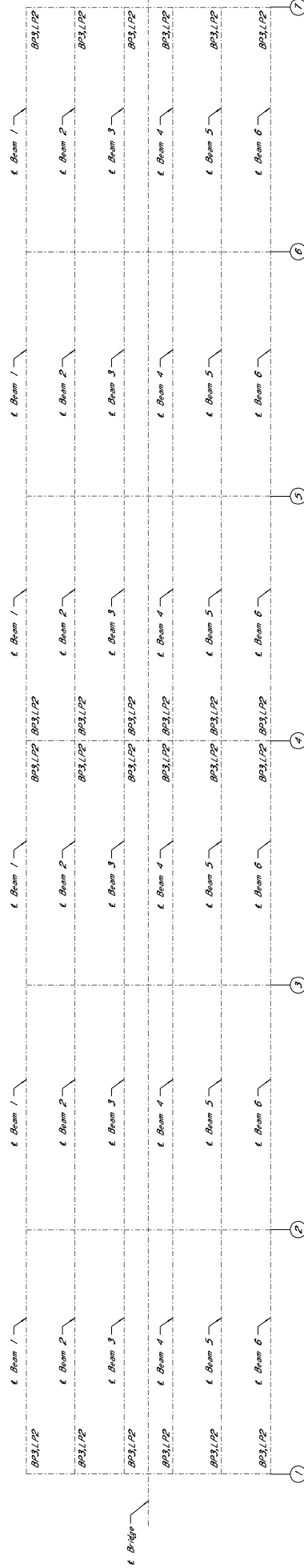
Fastening acceptance procedure shall be in accordance with Section 714.1.0.6 of the Specifications. Minimum shear modulus of 100 p.s.i. at 23°F. Bearing area on top of cap shall be cast smooth & true to grade.



WEDGE ANCHOR BOLT DETAILS



BRIDGE 14545 - US 49W OVER THE SUNFLOWER RIVER (BR. 255.2)



BRIDGE 14546 - US 49W OVER THE SUNFLOWER RIVER (BR. 255.2B)

NOTES ON ASSOCIATED ITEMS OF WORK:

907-808-A002 JOINT REPAIR

Description: Shall include the work necessary to repair joints in preparation for the placement of new expansion material, as designated in the detail drawings provided. Epoxy mortar shall also be included under this item of work. Removal of existing concrete shall be done in accordance with the specifications. Epoxy mortar shall be applied to the joint and shall be considered as absorbent under this item of work. All other requirements shall be in accordance with the applicable provisions of Section 808 of the specifications and any other sections specified therein.

Basis Of Payment: The accepted quantities will be paid for in linear feet at the contract unit price along the length of the bridge deck on each side of the centerline joint.

907-808-A003 JOINT REPAIR WITHOUT EPOXY

Description: Shall include the work necessary to repair joints in preparation for the placement of new expansion material, as designated in the detail drawings provided. Removal of existing concrete, sawing, grinding, and cleaning of the joint shall be done in accordance with the applicable provisions of Section 808 of the specifications and any other sections specified therein.

Basis Of Payment: The accepted quantities will be paid for in linear feet at the contract unit price along the length of the bridge deck on each side of the centerline joint.

907-823-8001 SAW CUT, TYPE I & 907-823-8002 SAW CUT, TYPE II

Description: The saw cut depth shall be equivalent to the installation depth required by the manufacturer's specifications. The saw cut type shall be the same as the preformed joint seal selected.

Basis of Payment: The accepted quantities will be paid for in linear feet at the contract unit price along the length of the bridge deck on each side of the centerline joint. It is the contractor's responsibility to ensure that the proper depth is selected based on the manufacturer's recommendations.

907-823-A001 PREFORMED JOINT SEAL, TYPE I

Description: Shall include the manufacturer's required joint preparation including sandblasting both sides of the joint and blowing the joint free of debris with compressed air and placement of the new preformed joint seal.

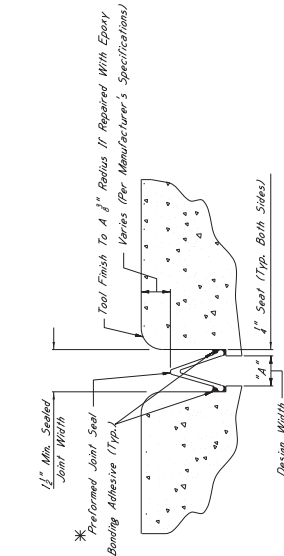
Basis Of Payment: The accepted quantities will be paid for in linear feet at the contract unit price along the length of the centerline joint.

EPOXY MORTAR AND POLYMER CONCRETE NOTES:

Either epoxy mortar or polymer concrete may be used. Guidelines for selection of materials can be found in Section 808 of the specifications.

GENERAL NOTES:

1. Specifications: Minimum Standard Specifications For Road And Bridge Construction, 2017.
2. No Change Of Plans Will Be Permitted Except By Written Approval Of The Director Of Structures, State Bridge Engineer. Any Change In Conditions, Conditions, Or Specifications May Be Authorized By The Bridge Engineer Provided Such Changes Will Not Be Cause For Contract Price Adjustment. Work For Which No Pay Item Is Provided In The Proposal Will Be Considered As Work For Which It Is Specifically And Shall Therefore Be Considered An Absorbed Item Of Work.



TYPICAL SECTION AT SAWCUT & SEALED JOINT

Showing Sealed Joint After Sawcut

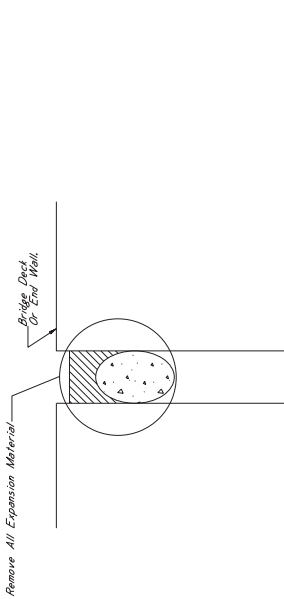
***NOTES:**

1. The Preformed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:

- A. Silcoflex Joint Sealing System Manufactured By R.J. Watson, Inc. In Alden, NY www.jwatson.com
- B. Migo SPS Joint Sealing System Manufactured By Migo Systems Corporation In Amherst, NY www.micorp.com
- C. Silplex SSS Silicone Strip Seal Manufactured By SSI Commercial & Highway Construction Materials www.ssi.com

2. For Estimating Purposes, The R.J. Watson Silcoflex Joint Sealing System Seal Structure, However, Should Another Supplier Be Chosen, It Is The Contractor's Responsibility To Obtain The Manufacturer's Installation Details And Adhesive Sealing Times, And Any Other Variances Between The Specifications Provided By The Manufacturer, To Ensure That The Contractor Is Properly Trained In Installation Of The Joint Material.

3. Joints Shall Be Sealed At Their Design Widths, Dimension "A", Which Is Defined As The Actual Width Of The Joint Opening. This Width Does Not Account For The Sealant Thickness. The Sealant Thickness Shall Be Determined From The Manufacturer's Installation Details. For Design Widths Greater Than Or Equal To 2" With The Minimum Design Width Of Expansion Material Shall Be Required As Directed By The Director Of Structures, State Bridge Engineer. It Is The Contractor's Responsibility To Ensure That The Size Selected Is Appropriate For The Width Of The Joint.

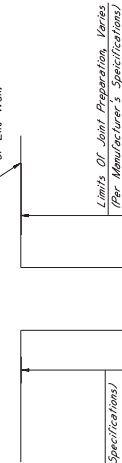


TYPICAL SECTION AT EXISTING JOINT

Showing Existing Expansion Materials To Be Removed And Replaced With Preformed Joint Seal

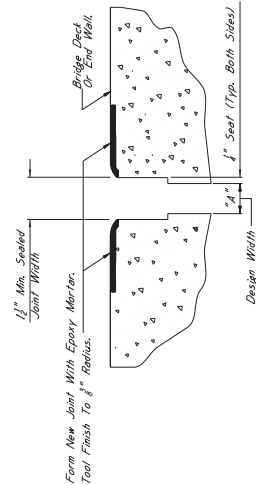
***NOTE:**

Limits Of Joint Preparation, Varies (Per Manufacturer's Specifications)



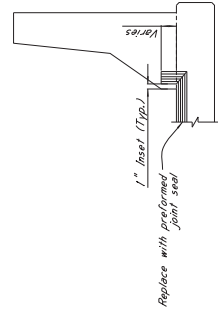
TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL AND SAWCUT

Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials And Sawcut



TYPICAL SECTION AT SAWCUT & JOINT REPAIR

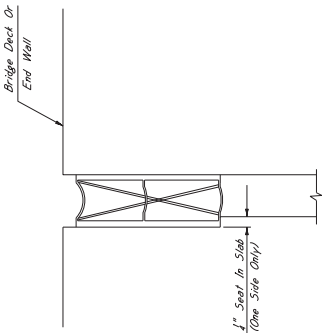
Showing Area Where Repairs Are Made After Sawcut With Epoxy Mortar Or Approved Equivalent



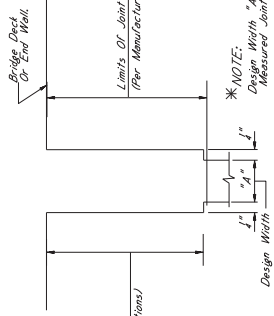
ELEVATION AT END OF SPAN

***NOTES:**

For Jersey Slope Barrier, The Minimum Required Vertical Joint Seal Dimension Within The Barrier Is 3". The Minimum Required Vertical Joint Seal Dimension Within The Barrier Is 6".



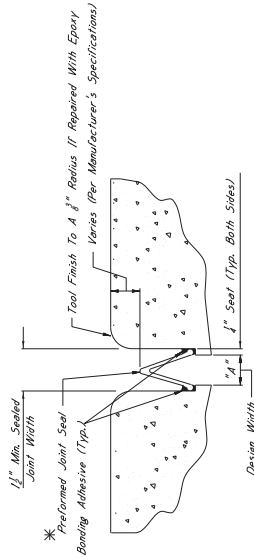
TYPICAL SECTION AT EXISTING JOINT
Showing Existing Expansion Device To Be Removed And Replaced With Preformed Joint Seal



TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL AND SAWCUT
Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials And Sawsut

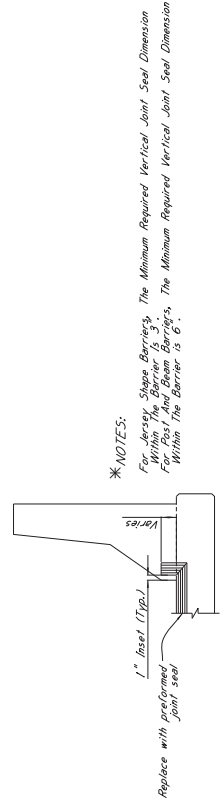


TYPICAL SECTION AT SAWCUT & JOINT REPAIR
Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials And Sawsut



TYPICAL SECTION AT SAWCUT & SEALED JOINT
Showing Sealed Joint After Sawsut And Repair With Epoxy Mortar

- *NOTES:
- The Preformed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:
 - Silicoflex Joint Sealing System
www.silicoflex.com
 - Wicks SBS Joint Sealing System
www.wicks.com
 - Silicone Strip Seal
www.silicone.com
 - For Estimating Purposes, The R.J. Watson Silicoflex Joint Sealing System Was Used For Design Widths Less Than 6". Preformed Joint Seal, Type II, Shall Be Used For Design Widths Greater Than Or Equal To 6". With The Maximum Design Width Of Expansion Material Shall Be As Directed By The Director Of Structures, State Bridge Engineer. It Is The Contractor's Responsibility To Ensure That The Size Selected Is Appropriate For The Width Of The Joint.



ELEVATION AT END OF SPAN

NOTES ON ASSOCIATED ITEMS OF WORK:

907-809-4002 JOINT REPAIR

Description:

Shall Include The Work Necessary To Repair Joints In Preparation For The Placement Of New Expansion Material, As Designated In The Detail Drawings Provided. Epoxy Mortar Of Existing Joints Shall Be Removed. This Item Of Work Shall Be Completed Before The Installation Of New Expansion Material. Epoxy Mortar Shall Be Applied Under The Direction Of The Bridge Engineer. Epoxy Mortar Shall Be Applied In Accordance With The Applicable Provisions Of Section 808 Of The Specifications And Any Other Sections Specified Therein.

Basis Of Payment:

The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint.

907-809-4003 JOINT REPAIR WITHOUT EPOXY

Description:

Shall Include The Work Necessary To Repair Joints In Preparation For The Placement Of New Expansion Material, As Designated In The Detail Drawings Provided. Removal Of Existing Expansion Material Shall Be Completed Before The Installation Of New Expansion Material. Epoxy Mortar Shall Be Applied Under The Direction Of The Bridge Engineer. Epoxy Mortar Shall Be Applied In Accordance With The Applicable Provisions Of Section 808 Of The Specifications And Any Other Sections Specified Therein.

Basis Of Payment:

The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint.

907-823-8001 SAW CUT, TYPE I & 907-823-8002 SAW CUT, TYPE II

Description:

The Saw Cut Depth Shall Be Equivalent To The Installation Depth Required By The Manufacturer's Specifications. The Saw Cut Type Shall Be The Same As The Preformed Joint Seal Selected.

Basis Of Payment:

The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint. It Is The Contractor's Responsibility To Ensure That The Proper Depth Is Selected Based On The Manufacturer's Recommendations.

907-823-4001 PREFORMED JOINT SEAL, TYPE I

Description:

Shall Include The Manufacturer's Required Joint Preparation Including Sandblasting Both Sides Of The Joint And Blowing The Joint Free Of Debris With Compressed Air And Placement Of The New Preformed Joint Seal

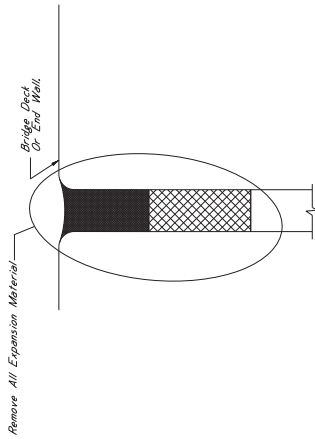
Basis Of Payment:

The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Centerline Joint.

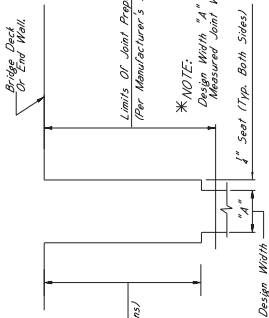
EPOXY MORTAR AND POLYMER CONCRETE NOTES:

GENERAL NOTES:

- Specifications, Mississippi Standard Specifications For Road And Bridge Construction, 2017, Shall Apply Unless Otherwise Specified.
- Approval Of The Director Of Structures, State Bridge Engineer, May Be Authorized By The Bridge Engineer Provided Such Changes Do Not Affect The Safety Or Structural Integrity Of The Work For Which No Pay Item Is Provided In The Proposal Will Not Be Paid For Directly And Shall Therefore Be Considered An Absorbed Item of Work.



TYPICAL SECTION AT EXISTING JOINT
Showing Existing Expansion Material To Be Removed And Replaced With Preformed Joint Seal



TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL AND SAWCUT
Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials And Sawcut



TYPICAL SECTION AT SAWCUT & JOINT REPAIR
Showing Area Where Repairs Are Made After Sawcut With Epoxy Mortar Or Approved Equivalent

NOTES ON ASSOCIATED ITEMS OF WORK:

907-808-4002 JOINT REPAIR

Description:

Shall include the work necessary to repair joints in preparation for the placement of new expansion material, as designated in the detail drawings. Epoxy Mortar or Existing Silicone Sealed Compression, and AC Sealed Joint Materials Will Not Be Paid For Directly, and Shall Be Considered As Absorbed Under This Item of Work. All Other Requirements Shall Be in Accordance With the Applicable Provisions of Section 808 of the Specifications and Any Other Sections Specified Therein.

Basis of Payment:

The Accepted Quantities Will Be Paid For in Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint.

907-808-4003 JOINT REPAIR WITHOUT EPOXY

Description:

Shall include the work necessary to repair joints in preparation for the placement of new expansion material, as designated in the detail drawings provided. Removal of Existing Silicone Sealed Compression, and AC Sealed Joint Materials Shall Be Included Under This Item of Work. All Other Requirements Shall Be in Accordance With the Applicable Provisions of Section 808 of the Specifications and Any Other Sections Specified Therein.

Basis of Payment:

The Accepted Quantities Will Be Paid For in Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint.

907-823-8001 SAW CUT, TYPE I & 907-823-8002 SAW CUT, TYPE II

Description:

The Saw Cut Depth Shall Be Equivalent To The Installation Depth Required By The Manufacturer's Specifications. The Saw Cut Type Shall Be The Same As The Preformed Joint Seal Selected.

Basis of Payment:

The Accepted Quantities Will Be Paid For in Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint. It Is The Contractor's Responsibility To Ensure That The Proper Depth Is Selected Based On The Manufacturer's Recommendations.

907-823-4001 REFORMED JOINT SEAL, TYPE I

Description:

Shall include the manufacturer's required joint preparation including sandblasting both sides of the joint and blowing the joint free of debris with compressed air and placement of the new preformed joint seal.

Basis of Payment:

The Accepted Quantities Will Be Paid For in Linear Feet At The Contract Unit Price Along The Length Of The Centerline Joint.

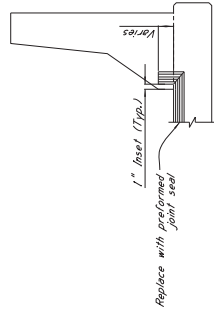
EPOXY MORTAR AND POLYMER CONCRETE NOTES:

Either Epoxy Mortar Or Polymer Concrete May Be Used. Guidelines for Selection of Materials Can Be Found in Section 808 of the Specifications.

GENERAL NOTES:

- Specifications: Mississippi Standard Specifications For Road And Bridge Construction, 2017.
- Approval of The Director of Structures, State Bridge Engineer, Minor Changes To Detail Of Design Or Construction Procedure May Be Authorized By The Bridge Engineer Provided Such Changes Will Not Affect The Safety, Strength, or Durability of the Work For Which No Pay Item Is Provided In The Proposal. The Proposal Will Not Be Paid For Directly And Shall Therefore Be Considered An Absorbed Item of Work.

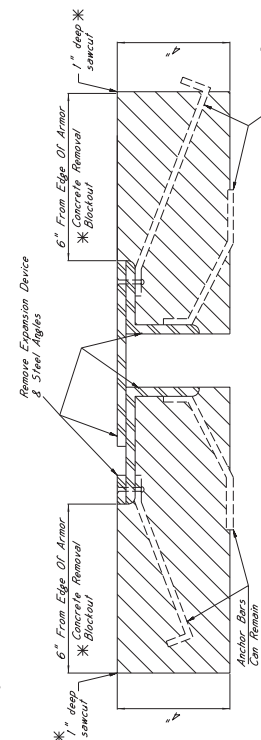
* NOTES:
For Jersey Slope Barriers, The Minimum Required Vertical Joint Seal Dimension Within The Barrier Is 3".
For Abs And Beam Barriers, The Minimum Required Vertical Joint Seal Dimension Within The Barrier Is 6".



ELEVATION AT END OF SPAN

*** 1" SAWCUT NOTES:**
 All 1" Sawcuts Shall Be Considered An Absorbed Item of Work. The Contractor Shall Verify Depth of Reinforcing Steel Before Making Any Sawcuts. The Depth of The Reinforcing Steel Shall Be Repaired To The Satisfaction Of The Engineer At No Cost To The State.

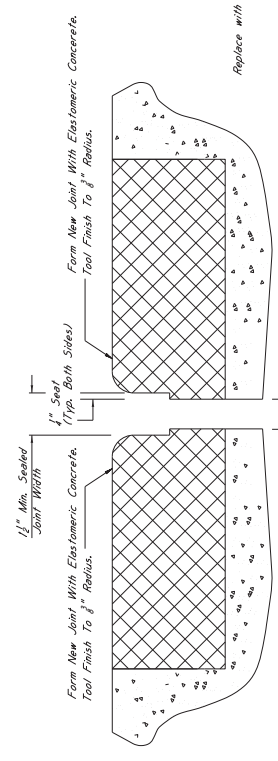
*** CONCRETE REMOVAL BLOCKOUT NOTES**
 Removal Of The Concrete Blockout Area Shall Be Considered An Absorbed Item Of Work Under Pay Item 202-B169. The Contractor Shall Use Hand Tools To Cut A Layer Thin 30 Lbs. To Complete This Work.



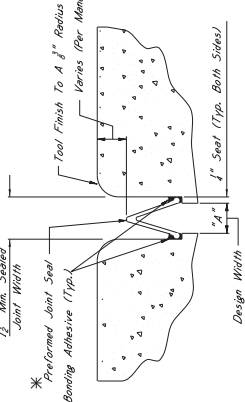
TYPICAL SECTION AT EXISTING JOINT
 Showing Existing Expansion Device To Be Removed And Replaced With Performed Joint Seal



TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL
 Showing Limits Of Joint Preparation For Application Of Seal Materials



TYPICAL SECTION AT SAWCUT & JOINT REPAIR
 Showing Area Where Repairs Are Made After Sawcut With Elastomeric Concrete



TYPICAL SECTION AT SAWCUT & SEALED JOINT
 Showing Sealed Joint After Sawcut And Repair With Elastomeric Concrete

*** NOTES:**
 1. The Performed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:
 A. Silicone Joint Seals System Manufactured By R.J. Watson, Inc. In Adena, NY
 B. Welo SP5 Joint System Manufactured By Watson Bowman Acme Corporation In Adena, NY
 C. Siligec 555 Silicone Strip Seal Manufactured By SSI Commercial & Highway Construction Materials www.ssi.com

2. For Estimating Purposes, The R.J. Watson Silicone Joint Seals System May Be Used As A Guide. The Contractor Shall Be Responsible To Ensure That The Manufacturer's Recommendations Are Followed For All Applications. In Addition, The Contractor Shall Be Responsible For Obtaining All Necessary Permits, Approvals, Sealing Times, And Other Information To Allow The Contractor To Perform The Work. A Manufacturer Representative Shall Be Present At The Time Joint Sealing Begins To Ensure That The Contractor Is Properly Trained In Installation Of The Joint Sealant.
 3. Joints Shall Be Sealed At Their Design Width. Dimension "A", Which Is Defined As Seal Applied On Both Sides Of The Joint, Performed Joint Seal, Type I, Shall Be Used For Design Widths Less Than 2". Performed Joint Seal, Type II, Shall Be Used For Design Widths Greater Than 2". In Cases Where Design Widths Are Greater Than 2", Another Type Of Expansion Material Shall Be Required As Directed By The Director Of Structures, Selected As Appropriate For The Width Of The Joint.

*** NOTES:**
 For Seals, Signs, Barriers, The Minimum Required Vertical Joint Seal Dimension Within The Barrier Is 6".
 For Posts And Beam Barriers, The Minimum Required Vertical Joint Seal Dimension Within The Barrier Is 6".

ELEVATION AT END OF SPAN

NOTES ON ASSOCIATED ITEMS OF WORK:
 202-B169 REMOVAL OF EXISTING JOINT MATERIAL

Description:
 Shall Include The Removal Of Material Associated With Armor, Sliding Plates and Measuring Devices, Points As Designated In The Detail Drawings Provided. Removal Of The Concrete Blockout Area Shall Be Absorbed Under This Item of Work. Other Joint Types Shall Not Be Included Under This Item of Work Unless Otherwise Directed By The Engineer.

Basis Of Payment:
 Removal of Armor And Sliding Plate Material Will Be Paid For As Shown On The Detail Drawings. The Saw Cut Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint, While Removal Of Neoprene Joint Material Will Only Be Paid For As The Length Along The Centerline Of The Joint.

907-823-8001 SAW CUT, TYPE I & 907-823-8002 SAW CUT, TYPE II
Description:
 The Saw Cut Depth Shall Be Established To The Installation Depth Recommended By The Manufacturer's Specifications. The Saw Cut Type Shall Be The Same As The Performed Joint Seal Selected.

Basis Of Payment:
 The Accepted Quantities Will Be Paid For As Linear Feet At On Each Side Of The Centerline Joint.

907-823-8001 REFORMED JOINT SEAL, TYPE I
907-823-8002 REFORMED JOINT SEAL, TYPE II
Description:
 Shall Include The Manufacturer's Required Joint Preparation, Forming, Placement, Compaction, Finishing, The Joint Free Of Debris With Compressed Air And Placement Of The New Performed Joint Seal

Basis Of Payment:
 The Accepted Quantities Will Be Paid For As Linear Feet At On Each Side Of The Centerline Joint.

ELASTOMERIC CONCRETE NOTES
907-824-9007 BRIDGE REPAIR, ELASTOMERIC CONCRETE
Description:
 Elastomeric Concrete Shall Be One Of The Following Products, Installed According To The Manufacturer's Specifications:
 A. Poly-Ton Elastomeric Concrete Manufactured By R.J. Watson, Inc. In Adena, NY
 B. WeloCreate II Manufactured By Watson Bowman Acme Corporation In Adena, NY
 C. Debreto Elastomeric Concrete Manufactured By The D.S. Brown Company In North Baltimore, MD
 www.dsbrown.com

Basis Of Payment:
 The Accepted Quantities Will Be Paid For In Cubic Yards The Contract Unit Price.

GENERAL NOTES:

- Specifications: Mississippi Standard Specifications For Road And Bridge Construction, 2017.
- No Change Of Plans Will Be Permitted Except By Written Approval Of The Engineer. Any Change Of Construction Procedure May Be Authorized By The Bridge Engineer Provided Such Changes Will Not Be Cause For Contract Price Adjustment. Proposal Will Be Considered As A Change Order. Payment For Work Not Be Paid For Directly And Shall Therefore Be Considered An Absorbed Item of Work.

NOTES ON ASSOCIATED ITEMS OF WORK:

202-8169 REMOVAL OF EXISTING JOINT MATERIAL

Description: Shall include the removal of material associated with armor, stiling plate, and neoprene expansion joints, as designated in the detail drawings provided. Removal of material shall include the removal of neoprene joint seal, armor, and stiling plate. Other joint types shall not be included by this item of work unless otherwise directed by the Engineer.

Basis of Payment: Removal of armor and stiling plate joint material will be paid for in linear feet at the contract unit price. The contract unit price shall include the removal of neoprene joint seal, armor, and stiling plate along the entire length of the joint.

907-823-8001 SAW CUT, TYPE I & 907-823-8002 SAW CUT, TYPE II

Description: The saw cut depth shall be equivalent to the installation depth required by the manufacturer's specifications. The saw cut type shall be the same as the preformed joint seal selected.

Basis of Payment: The accepted quantities will be paid for in linear feet at the contract unit price along the length of the bridge deck on each side of the centerline joint.

907-823-A001 PREFORMED JOINT SEAL, TYPE I

Description: Shall include the manufacturer's required joint preparation including sandblasting both sides of the joint and blowing the joint free of debris with compressed air and placement of the new preformed joint seal.

Basis of Payment: The accepted quantities will be paid for in linear feet at the contract unit price along the length of the centerline joint.

ELASTOMERIC CONCRETE REPAIR

Description: Elastomeric concrete shall be one of the following products, installed according to the manufacturer's specifications:

- A. Poly-Ton Elastomeric Concrete
Manufactured by R.J. Watson, Inc. in Alden, NY
www.rjwatson.com
- B. Waka-Crete II
Manufactured by Watson Bowman Acme Corporation in Amherst, NY
www.wbcorp.com
- C. Dylacrete Elastomeric Concrete
Manufactured by The U.S. Brown Company in North Baltimore, MD
www.usbrown.com

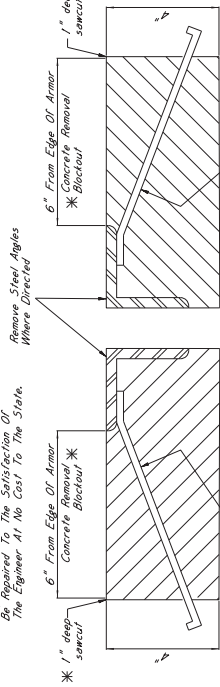
Basis of Payment: The accepted quantities will be paid for in cubic yards at the contract unit price.

GENERAL NOTES:

1. Specifications: Mississippi Standard Specifications For Road And Bridge Construction, 2017.
2. No Change Of Plans Will Be Permitted Except By Written Approval Of The Engineer. Any Change In Design Or Construction Method Changes To Detail Or Design Or Construction Procedure May Be Authorized By The Bridge Engineer Provided Such Changes Will Not Be Cause For Contract Price Adjustment. Requests For Contract Price Adjustment Will Be Considered Only If Approved In Advance. Requests Will Not Be Paid For Directly And Shall Therefore Be Considered An Absorbed Item of Work.

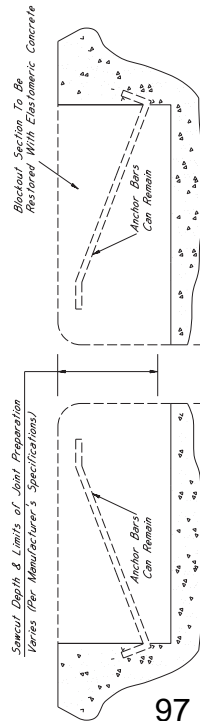
*** CONCRETE REMOVAL BLOCKOUT NOTES**

Removal of the concrete blockout area shall be considered an absorbed item of work. The contractor shall use a hammer and pry bar to remove the concrete. The contractor shall use a hammer no larger than 30 lbs to complete this work.



TYPICAL SECTION AT EXISTING JOINT

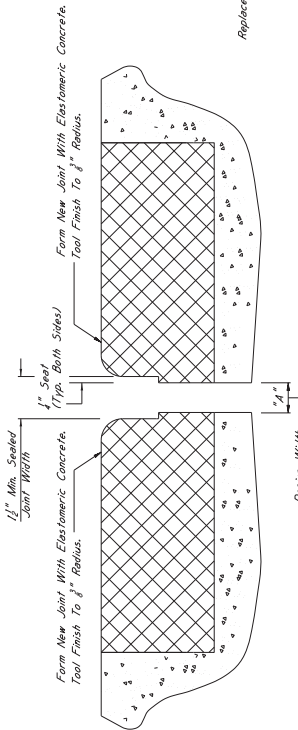
Showing Existing Elastomeric Joint Seal Wherever And Replaced With Preformed Joint Seal



*** NOTE:** Design Width "A" is Defined As The Actual Measured Joint Width.

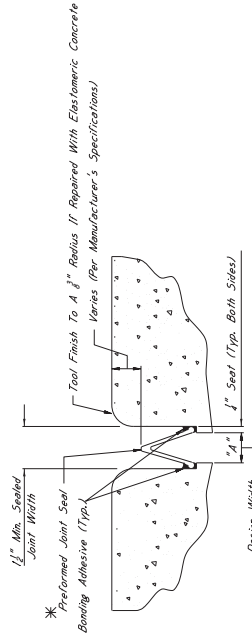
TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL

Showing Limits Of Joint Preparation For Application Of New Joint Seal Adhesive



TYPICAL SECTION AT SAWCUT & JOINT REPAIR

Showing Area Where Repairs Are Made After Sawcut With Elastomeric Concrete

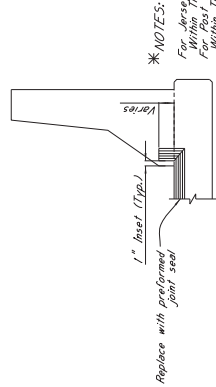


TYPICAL SECTION AT SAWCUT & SEALED JOINT

Showing Sawcut Used After Sawcut And Repair With Elastomeric Concrete

*** NOTES:**

1. The preformed joint seal shall be one of the following, installed according to the manufacturer's specifications:
 - A. Silicolite Joint Sealing System
Manufactured by R.J. Watson, Inc. in Alden, NY
www.rjwatson.com
 - B. Waka-SFS Joint Sealing System
Manufactured by Watson Bowman Acme Corporation in Amherst, NY
www.wbcorp.com
 - C. Silseal 555 Silicone Strip Seal
Manufactured by SSI Commercial & Highway Construction Materials
www.ssi.com
2. For estimating purposes, the R.J. Watson Silicolite Joint Sealing System was selected. However, should another supplier be chosen, it is the contractor's responsibility to ensure that the joint preparation, installation depths and widths, adhesive setting times, and any other variances between the specifications provided by the manufacturer, are properly addressed. The contractor is properly schooled in installation of the joint material.
3. Joints shall be sealed at their design widths, dimension "A", which is defined as the actual width of the joint opening. This width does not account for the sealant. The contractor shall use the preformed joint seal, type "1", shall be used for design widths greater than or equal to 2" with the maximum design width of expansion material shall be required as determined by the contractor of structures. State Bridge Engineer. It is the contractor's responsibility to ensure that the size selected is appropriate for the width of the joint.

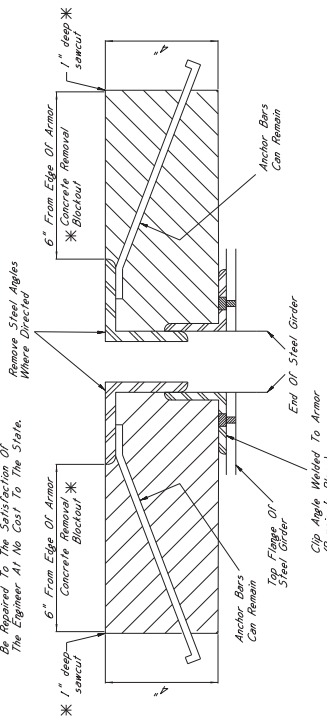


*** NOTES:**
For Jersey Slab Barriers, The Minimum Required Vertical Joint Seal Dimension Will Be 6".
For Post-Tensioned Beam Barriers, The Minimum Required Vertical Joint Seal Dimension Within The Barrier is 6".

ELEVATION AT END OF SPAN

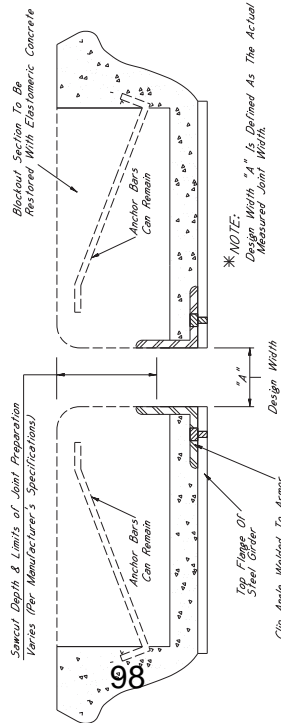
*** 1" SAWCUT NOTES:**

All 1" Sawcuts Shall Be Considered An Absorbed Item of Work. The Contractor Shall Verify Depth of Reinforcing Steel Prior to Making Sawcut. Sawcut Depth of the Depth of the Reinforcing Steel. No More Than Any Damage To The Reinforcing Steel Shall Be Repaired To The Satisfaction Of The Engineer At No Cost To The State.



TYPICAL SECTION AT EXISTING JOINT

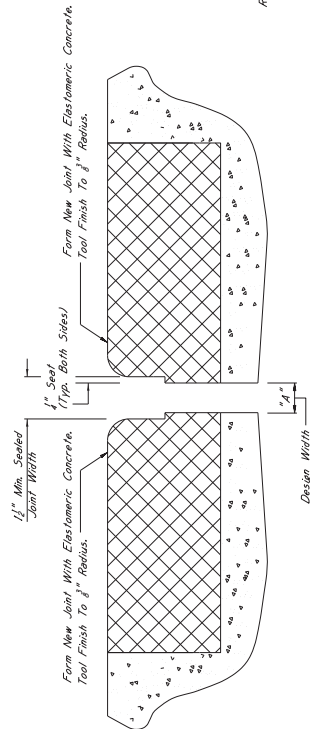
Showing Existing Elongation Devices To Be Replaced And Replaced With Performed Joint Seal



*** NOTE:**
Design Width "A" Is Defined As The Actual Measured Joint Width.

TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL

Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials

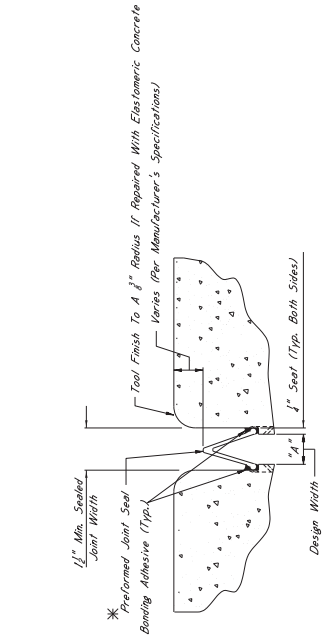


TYPICAL SECTION AT SAWCUT & JOINT REPAIR

Showing Area Where Repairs Are Made After Sawcut With Elastomeric Concrete

*** CONCRETE REMOVAL BLOCKOUT NOTES**

Removal Of The Concrete Blockout Area Shall Be Considered An Absorbed Item Of Work Under Item 202-9169. The Contractor Shall Remove Material To A Depth Of 30" To Complete This Work.



TYPICAL SECTION AT SAWCUT & SEALED JOINT

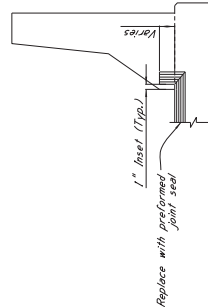
Showing Sealed Joint After Sawcut And Repair With Elastomeric Concrete

*** NOTES:**

- The Performed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:
 - Silcoflex Joint Sealing System Manufactured By R.J. Watson, Inc. In Allen, NY www.rjwatson.com
 - Waka SFS Joint Sealing System Manufactured By Watson Bowman Acme Corporation In Amherst, NY www.wbscorp.com
 - Silcrete SSS Silicone Strip Seal Manufactured By SSI Commercial & Highway Construction Materials www.ssi.com
- For Estimating Purposes, The R.J. Watson Silicone Joint Sealing System Was Selected. However, Should Another Supplier Be Chosen, It Is The Contractor's Responsibility To Ensure That The Manufacturer's Recommendations Are Followed. Any Other Variance Between The Specifications Provided By The Manufacturer's Representative And The Specifications Shall Be Presented At The Time Joint Sealing Begins. A Manufacturer Representative Shall Be Present At The Time Joint Sealing Begins. The Contractor Is Responsible For Ensuring That The Joint Sealing System Selected Is Appropriate For The Width Of The Joint.
- Joints Shall Be Sealed At Their Design Widths, Dimension "A", Which Is Defined As The Actual Width Of The Joint Opening. This Width Does Not Account For The Seal Required On Both Sides Of The Joint. Performed Joint Seal, Type Shall Be Selected To Match The Design Width. The Seal Shall Be Applied To The Seal Bed For Design Widths Greater Than Or Equal To 2" With The Maximum Design Width Being 2". In Cases Where Design Widths Are Greater Than 2", Another Type Seal Shall Be Selected. The Contractor Shall Be Responsible For Ensuring That The Seal Selected Is Appropriate For The Width Of The Joint.

*** NOTES:**

For Any Steps, Barriers, The Minimum Required Vertical Joint Seal Dimension Within The Joint Shall Be 6". For Post And Beam Barriers, The Minimum Required Vertical Joint Seal Dimension Within The Barrier Is 6".



Replace with performed joint seal

ELEVATION AT END OF SPAN

NOTES ON ASSOCIATED ITEMS OF WORK:

202-9169 REMOVAL OF EXISTING JOINT MATERIAL

Description:

Shall Include The Removal Of Material Associated With Armor, Slicing Plates, And Neoprene Expansion Joints, As Well As The Concrete Blockout Area. The Contractor Shall Absorb This Item Of Work. Other Joint Types Shall Not Be Included Under This Item Of Work Unless Otherwise Directed By The Engineer.

Basis Of Payment:

Removal Of Armor And Slicing Plate Joint Material Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Of The Joint.

907-823-8001 SAW CUT, TYPE I & 907-823-8002 SAW CUT, TYPE II

Description:

The Saw Cut Depth Shall Be Equivalent To The Installation Depth Required By The Manufacturer's Specifications. The Saw Cut Type Shall Be The Same As The Performed Joint Seal Selected. The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Of The Joint.

907-823-4001 PERFORMED JOINT SEAL, TYPE I

907-823-4002 PERFORMED JOINT SEAL, TYPE II

Description:

Shall Include The Manufacturer's Required Joint Preparation Including Sandblasting Both Sides Of The Joint And Blowing The Joint Of Debris With Compressed Air And Placement Of The New Performed Joint Seal.

Basis Of Payment:

The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Centerline Joint.

ELASTOMERIC CONCRETE NOTES

907-824-99007 BRIDGE REPAIR ELASTOMERIC CONCRETE

Description: Elastomeric Concrete Shall Be One Of The Following Products, Installed According To The Manufacturer's Specifications:

- Poly-Ton Elastomeric Concrete Manufactured By R.L. Watson, Inc. In Allen, NY www.rjwatson.com
- WakaCrete II Manufactured By Watson Bowman Acme Corporation In Amherst, NY www.wbscorp.com
- Delcrete Elastomeric Concrete Manufactured By The U.S. Brown Company In North Beloit, OH www.usbrown.com

Basis Of Payment:

The Accepted Quantities Will Be Paid For In Cubic Yards At The Contract Unit Price.

GENERAL NOTES:

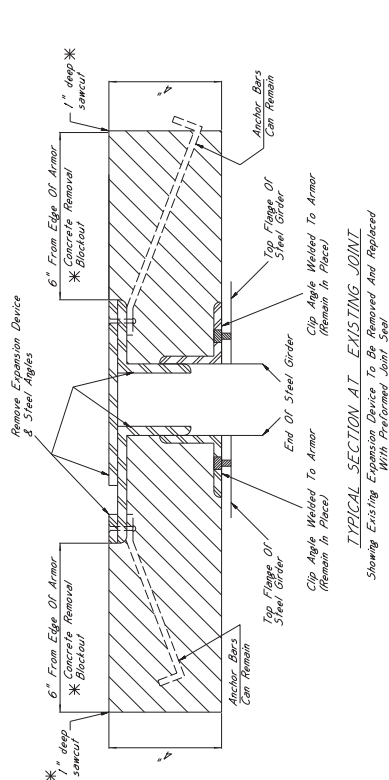
- Specifications, Manufacturer Standard Specifications For Road And Bridge Construction, 2017.
- No Change Of Plans Will Be Permitted Except By Written Approval Of The Director Of Structures, State Bridge Engineer. Any Change To The Specifications Shall Be Approved In Writing. The Contractor Shall Be Responsible For Obtaining All Necessary Permits. The Contractor Shall Be Responsible For Obtaining All Necessary Permits. The Contractor Shall Be Responsible For Obtaining All Necessary Permits. The Contractor Shall Be Responsible For Obtaining All Necessary Permits.
- Work For Which No Pay Item Is Provided In The Proposal Will Be Considered An Absorbed Item of Work.

*** 1" SAWCUT NOTES:**

All 1" sawcuts shall be considered in Absorbed Item 202-9169. The Contractor Shall Verify Depth Of Reinforcing Steel Before Making Any Sawcuts. The Depth Of The Sawcut Shall Be No More Than 1/4" From Edge Of Concrete. Any Damage To Reinforcing Steel Shall Be Repaired To The Satisfaction Of The Engineer At No Cost To The State.

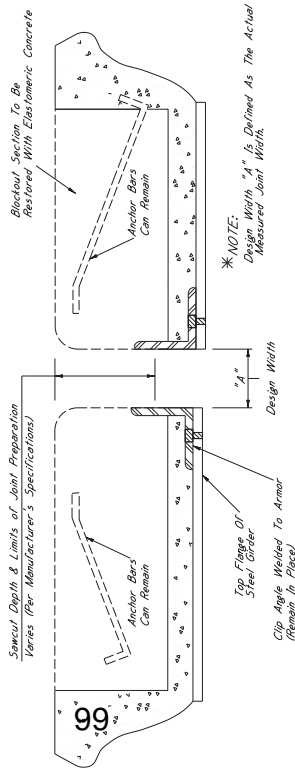
*** CONCRETE REMOVAL BLOCKOUT NOTES**

All 1" sawcuts shall be considered in Absorbed Item 202-9169. The Contractor Shall Verify Depth Of Reinforcing Steel Before Making Any Sawcuts. The Depth Of The Sawcut Shall Be No More Than 30 LBS To Complete This Work.



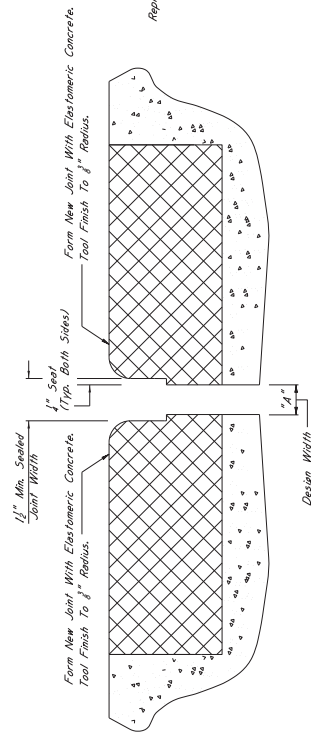
TYPICAL SECTION AT SAWCUT & SEALED JOINT

Showing Existing Expansion Device To Be Removed And Replaced With Preformed Joint Seal



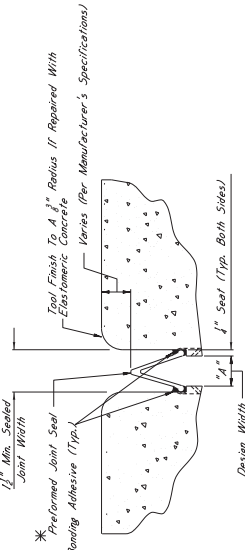
TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL

Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials



TYPICAL SECTION AT SAWCUT & JOINT REPAIR

Showing Area Where Repairs Are Made After Sawcut



TYPICAL SECTION AT SAWCUT & SEALED JOINT

Showing Sealed Joint After Sawcut And Repair With Elastomeric Concrete

*** NOTES:**

1. The Preformed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:

- A. Silcoliter Joint Sealing System
www.jackson.com
- B. Wabco SPS Joint Sealing System
Manufactured By Watson Bowman Acme Corporation In Amherst, NY
www.wbcorp.com
- C. Silagoc S55 Silicone Strip Seal
Manufactured By SSI Commercial & Highway Construction Materials
www.ssiinc.com

2. For Estimating Purposes, The R.J. Watson Silcoliter Joint Sealing System Was Used For Design Purposes. The Contractor Shall Be Responsible For Ensuring That The Manufacturer's Recommendations Are Followed For Joint Preparation, Installation Depth, And Width, Adhesive Sealing Times, And Material Application. The Contractor Shall Provide A Manufacturer Representative Seal To Ensure That The Contractor Is Properly Schooled In Installation Of The Joint Material.

3. Joints Shall Be Sealed At Their Design Widths, Dimension "A", Which Is Defined As Seal Required On Both Sides Of Joint. Preformed Joint Seal Width Shall Be Used For Design Widths Less Than 2". Preformed Joint Seal Type In Being Significant In Cases Where Design Widths Are Greater Than Design Width. Expansion Material Shall Be Required As Directed By The Director Of Structures, State Bridge Engineer. The Contractor Shall Be Responsible To Ensure That The Size Selected Is Appropriate For The Width Of The Joint.

*** NOTES:**

For Jersey Slope Barrings The Minimum Required Vertical Joint Seal Dimension For Post And Beam Barrings, The Minimum Required Vertical Joint Seal Dimension Within The Barrier Is 6".

NOTES ON ASSOCIATED ITEMS OF WORK:

202-9169 REMOVAL OF EXISTING JOINT MATERIAL

Shall Include The Removal Of Material Associated With Armor, Sliding Plate, And Neoprene Expansion Joints, As Designated In The Detail Drawings Provided. Removal Of Material From The Joint Shall Be Done In A Manner That Allows Other Joint Types Shall Not Be Directed By The Engineer.

Removal Of Armor And Sliding Plate Joint Material Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Material Will Not Be Paid For As The Length Along The Centerline Of The Joint.

907-823-8001 SAW CUT, TYPE I & 907-823-8002 SAW CUT, TYPE II

The Saw Cut Depth Shall Be Equivalent To The Installation Depth Required By The Manufacturer's Specifications. The Saw Cut Type Shall Be The Same As The Preformed Joint Seal Selected.

The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint.

907-823-8001 PREFORMED JOINT SEAL, TYPE I

Shall Include The Manufacturer's Required Joint Preparation Including Sandblasting Both Sides Of The Joint And Blowing The Joint Free Of Debris With Compressed Air And Placement Of The New Preformed Joint Seal

907-823-8002 PREFORMED JOINT SEAL, TYPE II

The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Centerline Joint.

ELASTOMERIC CONCRETE REPAIR, ELASTOMERIC CONCRETE

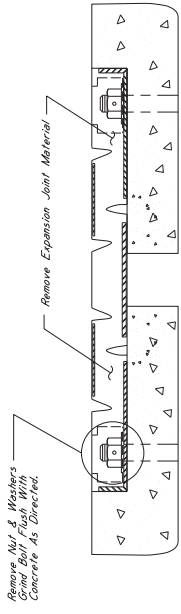
Elastomeric Concrete Shall Be One Of The Following Products, Installed According To The Manufacturer's Specifications:

- A. Poly-Ton Elastomeric Concrete
Manufactured By R.J. Watson, Inc. In Alden, NY
www.rjwatson.com
- B. WabcoCrete II
Manufactured By Watson Bowman Acme Corporation In Amherst, NY
www.wbcorp.com
- C. Delecta Elastomeric Concrete
Manufactured By The D.S. Brown Company In North Baltimore, OH
www.dsbrown.com

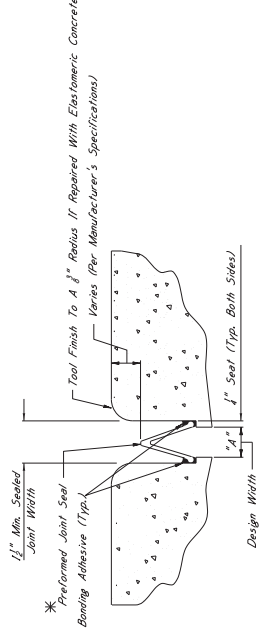
The Accepted Quantities Will Be Paid For In Cubic Feet At The Contract Unit Price.

GENERAL NOTES:

1. Specifications: Mississippi Standard Specifications For Road And Bridge Construction, 2017.
2. Approval Of The Director Of Structures, State Bridge Engineer, May Be Authorized By The Bridge Engineer Provided Such Changes Work For Which No Pay Item Is Provided In The Proposal Will Not Be Paid For Directly And Shall Therefore Be Considered An Absorbed Item Of Work.



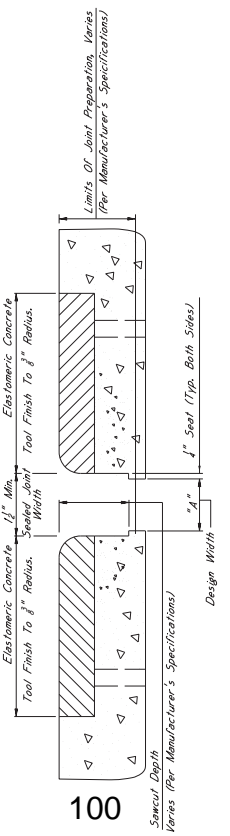
TYPICAL SECTION AT EXISTING JOINT
Showing Existing Expansion Device To Be Removed and Replaced With Preformed Joint Seal



TYPICAL SECTION AT SAWCUT & SEALED JOINT
Showing Sealed Joint After Sawcut and Repair With Elastomeric Concrete

*NOTES:

- The Preformed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:
 - SilicoFlex Joint Sealing System Manufactured By R.J. Watson, Inc. In Aiken, NY www.rjwatson.com
 - Weldo 355 Joint System Manufactured By Watson Bowman Acme Corporation In Amherst, NY www.wbcorp.com
 - Silgore SSS Silicone Strip Seal Manufactured By SSI Commercial & Highway Construction Materials www.ssi.com
- For Estimating Purposes, The R.J. Watson SilicoFlex Joint Sealing System Was Selected. However, Should Another Supplier Be Chosen, It Is The Contractor's Responsibility To Verify That The Sealant Meets The Manufacturer's Specifications For Joint Preparation, Installation Depths And Widths, Adhesive Setting Times, And Any Other Parameters Between The Specifications Provided By The Manufacturer, To Ensure That The Contractor Is Properly Schooled In Installation Of The Joint Material.
- Joints Shall Be Sealed At Their Design Widths, Dimension "A", Which Is Defined As The Actual Width Of The Joint Opening. This Width Does Not Account For The Sealant Compression. The Sealant Shall Be Applied To The Joint Surface And Used For Design Widths Less Than 2". Joint Preformed Joint Seal System Shall Be Used For Design Widths Greater Than or Equal To 2". With The Maximum Design Width Of Expansion Material Shall Be As Directed By The Director Of Structures, State Bridge Engineer. It Is The Contractor's Responsibility To Ensure That The Size Selected Is Appropriate For The Width Of The Joint.



TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL
Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials

*NOTE:
Design Width "A" Is Defined As The Actual Measured Joint Width.

NOTES ON ASSOCIATED ITEMS OF WORK:

202-0169 REMOVAL OF EXISTING JOINT MATERIAL
Description: Shall Include The Removal Of Material Associated With Armor, Sliding Plates, And Negreene Expansion Joints, As Designated In The Technical Drawings Prepared Pursuant To The Terms Of Work Unless Otherwise Directed By The Engineers.

Basis Of Payment: Removal Of Armor And Sliding Plates Joint Material Will Be Paid For As Shown On The Drawings. Removal Of Negreene Expansion Joints Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint, While Removal Of Negreene Joint Material Will Only Be Paid For As The Length Along The Centerline Of The Joint.

907-023-0001 SAW CUT, TYPE I & 907-023-0002 SAW CUT, TYPE II
Description: The Saw Cut Depth Shall Be Equivalent To The Installation Depth Required By The Manufacturer Specifications. The Saw Cut Type Shall Be The Same As The Preformed Joint Seal Selected.

Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint.

907-023-0001 PREFORMED JOINT SEAL, TYPE I
907-023-0002 PREFORMED JOINT SEAL, TYPE II
Description: Shall Include The Manufacturer's Required Joint Preparation Free Of Debris With Compressed Air And Placement Of The New Preformed Joint Seal

Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Centerline Joint.

ELASTOMERIC CONCRETE NOTES

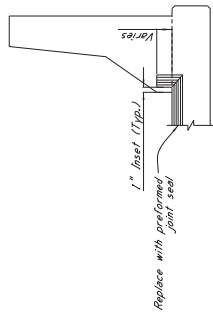
907-024-0007 BRIDGE REPAIR, ELASTOMERIC CONCRETE
Description: Elastomeric Concrete Shall Be One Of The Following Products Installed According To The Manufacturer's Specifications:

- Poly-Ton Elastomeric Concrete Manufactured By R.J. Watson, Inc. In Aiken, NY www.rjwatson.com
- WeldoCrete II Manufactured By Watson Bowman Acme Corporation In Amherst, NY www.wbcorp.com
- Dycrete Elastomeric Concrete Manufactured By The D.S. Brown Company In North Baltimore, MD www.dsbrown.com

Basis Of Payment: The Accepted Quantities Will Be Paid For In Cubic Yards At The Contract Unit Price.

GENERAL NOTES:

- Specifications: Mississippi Standard Specifications For Road And Bridge Construction, 2017.
- Approval: All Work Shall Be Approved In Advance By The Director Of Structures, State Bridge Engineer. Any Changes To Detail Of Design Or Construction Procedure May Be Authorized By The Bridge Engineer Provided Such Changes Do Not Affect The Safety, Functionality, Or Appearance Of The Work For Which No Part Of The Proposal Will Be Considered An Assured Item Of Work.



ELEVATION AT END OF SPAN

*NOTES:

For Jersey Slope Barriers, The Minimum Required Vertical Joint Seal Dimension Within The Barrier Barriers, The Minimum Required Vertical Joint Seal Dimension Within The Barrier is 6".

NOTES ON ASSOCIATED ITEMS OF WORK:

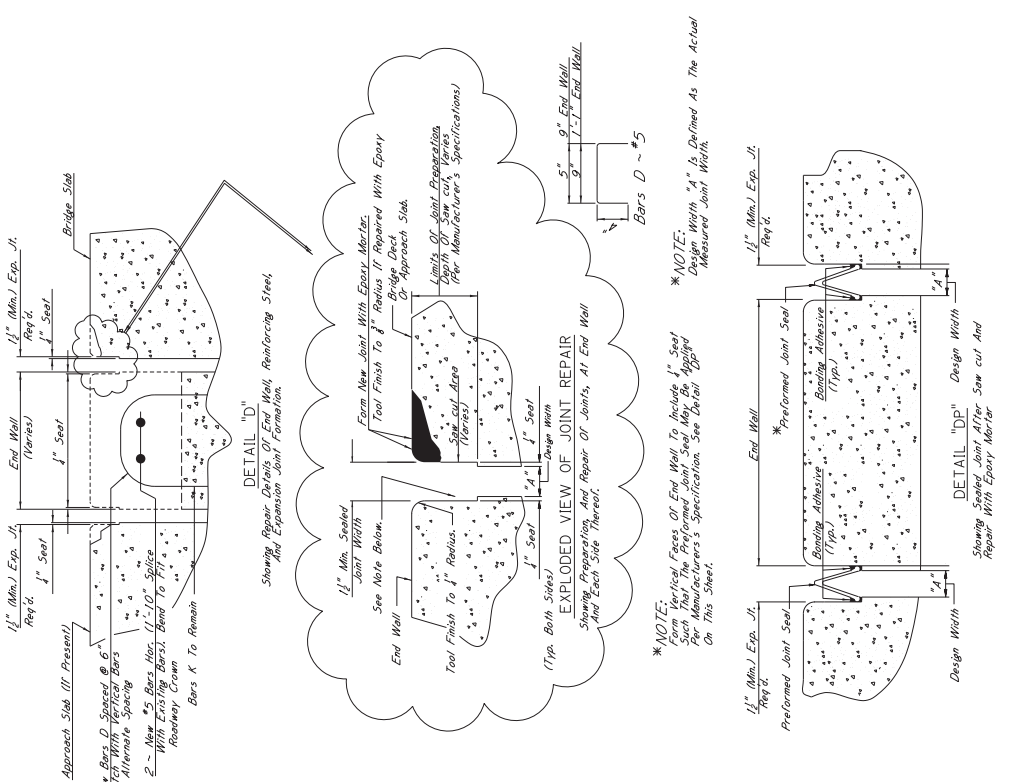
907-824-PP008 BRIDGE REPAIR, ENDWALL REPAIR
 Description: Shall Include The Work Necessary To Remove And Replace The Damaged Concrete On The End Wall Of The Damaged Section, The Specified Depth Of Limiting The Repair To The Damaged Section, The Specified Depth Of Endwall Shall Be Removed Along The Entire Width Of The Bridge Deck.
 Basis of Payment: The Assessed Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Width Of The Bridge Deck.
 Damage Caused To Other Elements Of The Structure Or Roadway While Completing Work Of Work Shall Be Repaired By The Contractor At No Cost To The Department.
 Prior To Placing New Concrete, All Concrete Surfaces That Will Be In Contact With The New Concrete Shall Be Painted With An Approved Epoxy Primer Designed To Bond New Concrete To Old.
 New Concrete Shall Be High Early Strength Bridge Concrete, As Follows:
 The concrete mixture design shall be furnished by the Contractor for approval by the Materials Division. Mixture design parameters are as follows:
 Required Strength: 5000 psi
 Maximum Slump: 6 inches
 Non-chloride based accelerator may be used if the ambient temperature is 50°F or less, but shall not be used if the ambient temperature is greater than 50°F. Synthetic structural fibers shall be used. The Contractor shall select a manufacturer from ADOT's Approved Products List, and the manufacturer's recommendations shall be followed for the dosage rate.
 Curing to be continuous until 2500 psi is attained. Traffic is to be diverted from the repair area until this value is reached. The Contractor may use the Ministry of Transportation 307-2010 to estimate the curing time. However, final acceptance of the in-place concrete shall be determined using eight concrete test cylinders, which shall be cured in a container moist to the concrete placement. Two cylinders are to be tested at 3, 16, and 28-hour compressive strength. The remaining shall be used to determine the 28-day compressive strength of the concrete.
 The Removal Of Existing Expansion Material May Require Any Number Of The Pay Associated Items Of Work.
 Corresponding Joint Repair Detail Sheet For Additional Details On The Associated Items Of Work.

907-808-A002 JOINT REPAIR WITHOUT EPOXY
 Description: Shall include the work necessary to remove and replace the damaged concrete on the end wall of the damaged section, the specified depth of limiting the repair to the damaged section, the specified depth of endwall shall be removed along the entire width of the bridge deck.
 Basis of Payment: The assessed quantities will be paid for in linear feet at the contract unit price along the width of the bridge deck.
 Damage caused to other elements of the structure or roadway while completing work of work shall be repaired by the contractor at no cost to the department.
 Prior to placing new concrete, all concrete surfaces that will be in contact with the new concrete shall be painted with an approved epoxy primer designed to bond new concrete to old.
 New concrete shall be high early strength bridge concrete, as follows:
 The concrete mixture design shall be furnished by the contractor for approval by the materials division. Mixture design parameters are as follows:
 Required strength: 5000 psi
 Maximum slump: 6 inches
 Non-chloride based accelerator may be used if the ambient temperature is 50°F or less, but shall not be used if the ambient temperature is greater than 50°F. Synthetic structural fibers shall be used. The contractor shall select a manufacturer from ADOT's approved products list, and the manufacturer's recommendations shall be followed for the dosage rate.
 Curing to be continuous until 2500 psi is attained. Traffic is to be diverted from the repair area until this value is reached. The contractor may use the Ministry of Transportation 307-2010 to estimate the curing time. However, final acceptance of the in-place concrete shall be determined using eight concrete test cylinders, which shall be cured in a container moist to the concrete placement. Two cylinders are to be tested at 3, 16, and 28-hour compressive strength. The remaining shall be used to determine the 28-day compressive strength of the concrete.
 The removal of existing expansion material may require any number of the pay associated items of work.
 Corresponding joint repair detail sheet for additional details on the associated items of work.

907-823-A001 PREFORMED JOINT SEAL, TYPE I
 Description: Shall include the work necessary to remove and replace the damaged concrete on the end wall of the damaged section, the specified depth of limiting the repair to the damaged section, the specified depth of endwall shall be removed along the entire width of the bridge deck.
 Basis of Payment: The assessed quantities will be paid for in linear feet at the contract unit price along the width of the bridge deck.
 Damage caused to other elements of the structure or roadway while completing work of work shall be repaired by the contractor at no cost to the department.
 Prior to placing new concrete, all concrete surfaces that will be in contact with the new concrete shall be painted with an approved epoxy primer designed to bond new concrete to old.
 New concrete shall be high early strength bridge concrete, as follows:
 The concrete mixture design shall be furnished by the contractor for approval by the materials division. Mixture design parameters are as follows:
 Required strength: 5000 psi
 Maximum slump: 6 inches
 Non-chloride based accelerator may be used if the ambient temperature is 50°F or less, but shall not be used if the ambient temperature is greater than 50°F. Synthetic structural fibers shall be used. The contractor shall select a manufacturer from ADOT's approved products list, and the manufacturer's recommendations shall be followed for the dosage rate.
 Curing to be continuous until 2500 psi is attained. Traffic is to be diverted from the repair area until this value is reached. The contractor may use the Ministry of Transportation 307-2010 to estimate the curing time. However, final acceptance of the in-place concrete shall be determined using eight concrete test cylinders, which shall be cured in a container moist to the concrete placement. Two cylinders are to be tested at 3, 16, and 28-hour compressive strength. The remaining shall be used to determine the 28-day compressive strength of the concrete.
 The removal of existing expansion material may require any number of the pay associated items of work.
 Corresponding joint repair detail sheet for additional details on the associated items of work.

907-823-A002 PREFORMED JOINT SEAL, TYPE II
 Description: Shall include the work necessary to remove and replace the damaged concrete on the end wall of the damaged section, the specified depth of limiting the repair to the damaged section, the specified depth of endwall shall be removed along the entire width of the bridge deck.
 Basis of Payment: The assessed quantities will be paid for in linear feet at the contract unit price along the width of the bridge deck.
 Damage caused to other elements of the structure or roadway while completing work of work shall be repaired by the contractor at no cost to the department.
 Prior to placing new concrete, all concrete surfaces that will be in contact with the new concrete shall be painted with an approved epoxy primer designed to bond new concrete to old.
 New concrete shall be high early strength bridge concrete, as follows:
 The concrete mixture design shall be furnished by the contractor for approval by the materials division. Mixture design parameters are as follows:
 Required strength: 5000 psi
 Maximum slump: 6 inches
 Non-chloride based accelerator may be used if the ambient temperature is 50°F or less, but shall not be used if the ambient temperature is greater than 50°F. Synthetic structural fibers shall be used. The contractor shall select a manufacturer from ADOT's approved products list, and the manufacturer's recommendations shall be followed for the dosage rate.
 Curing to be continuous until 2500 psi is attained. Traffic is to be diverted from the repair area until this value is reached. The contractor may use the Ministry of Transportation 307-2010 to estimate the curing time. However, final acceptance of the in-place concrete shall be determined using eight concrete test cylinders, which shall be cured in a container moist to the concrete placement. Two cylinders are to be tested at 3, 16, and 28-hour compressive strength. The remaining shall be used to determine the 28-day compressive strength of the concrete.
 The removal of existing expansion material may require any number of the pay associated items of work.
 Corresponding joint repair detail sheet for additional details on the associated items of work.

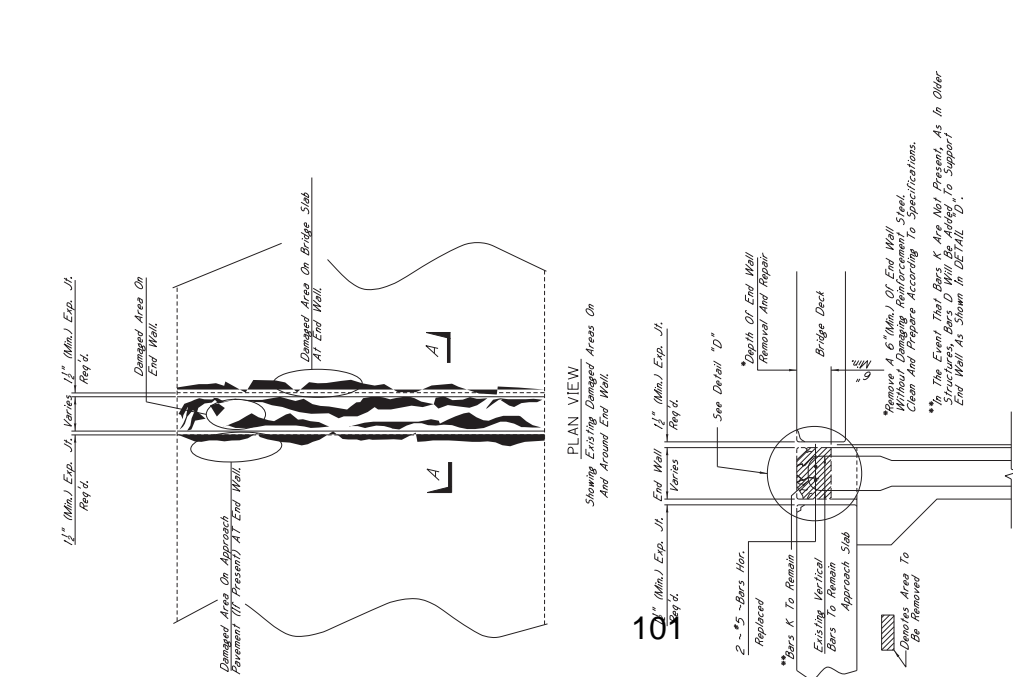
GENERAL NOTES:
 1. And Bridge Construction 2017.
 2. No Change Of Plans Will Be Permitted Except By Written Approval Of The Department Of Transportation Construction Engineer. Any Changes To Detail Of Design Constructed In Accordance With The Contract Price Adjustment Provisions Will Not Be Cause For Contract Price Adjustment. Provisions Will Not Be Paid For Directly And Shall Therefore Be Considered An Absorbed Item Of Work.



*** NOTE:**
 Vertical Faces Of End Wall To Include 1" Seal Form Preparation, And Repair Applied Per Manufacturer's Specification. See Detail Above On This Sheet.

*** NOTE:**
 Design Width "A" Is Defined As The Actual Measured Joint Width.

*** NOTE:**
 Joints Shall Be Sealed At Their Design Widths. Dimension "A", Which Is Defined As Seal Provided On Both Sides Of The Preformed Joint Seal, Shall Be Used For Design Widths Less Than 2". Preformed Joint Seal, Type II, Shall Be Used In Cases Where The Design Width Is Greater Than 2" And The Minimum Joint Seal Design Width Is 2". The Contractor Shall Be Responsible For Ensuring That The Sealant Material Shall Be Applied As Directed By The Director Of Structures. Sealer Is Appropriate For The Width Of The Joint.



*** NOTES:**
 1. The Preformed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:
 A. Silcaflex Joint Sealing System www.silcaflex.com
 B. Wicks SSS Silcaflex System www.wicks.com
 C. Silcaflex SSS Silicone Strip Seal Manufactured By SSS Commercial & Highway Construction Materials www.ssicm.com

DESCRIPTION OF SHEETS
SPECIAL DESIGN SHEETS - BRIDGE DRAWINGS

DESCRIPTION OF SHEETS
SPECIAL DESIGN SHEETS - DETOUR BRIDGE DRAWINGS

DESCRIPTION OF SHEETS	WORKING NUMBER	SHEET NUMBER
BRIDGE AT STA. 108+00.00 - STATE HWY. NO. 28	A1 OF 13	486
BAYOU PIERRE RELIEF		
FOUNDATION PLAN		
BORING DATA		
INT. BENT NO. 1 & 5 DETAILS	A2 OF 13	487
END BENT DETAILS	A3 OF 13	488
INT. BENT NO. 2, 3, 4 DETAILS	A4 OF 13	489
40 FT. SPAN DETAILS	A5 OF 13	490
MISC. SPAN DETAILS	A6 OF 13	491
RAILING DETAILS	A7 OF 13	492
BEAM 40-1 DETAILS - TYPE 1-2	A8 OF 13	493
LAMINATED ELASTOMERIC BEARING	A9 OF 13	494
PAV. DETAILS	A10 OF 13	495
BEAM 40-2 DETAILS - TYPE 1-2	A11 OF 13	496
	A12 OF 13	497
	A13 OF 13	498
BRIDGE AT STA. 108+00.21 - STATE HWY. NO. 28	B1 OF 15	479
BAYOU PIERRE		
FOUNDATION PLAN		
BORING DATA		
INT. BENT NO. 5 DETAILS	B2 OF 15	480
INT. BENT NO. 6 DETAILS	B3 OF 15	481
INT. BENT NO. 7 DETAILS	B4 OF 15	482
INT. BENT NO. 8 DETAILS	B5 OF 15	483
INT. BENT NO. 9 DETAILS	B6 OF 15	484
INT. BENT NO. 10 DETAILS	B7 OF 15	485
INT. BENT NO. 11 DETAILS	B8 OF 15	486
60 FT. SPAN DETAILS	B9 OF 15	487
100 FT. SPAN DETAILS	B10 OF 15	488
100 FT. SPAN DETAILS	B11 OF 15	489
100 FT. SPAN DETAILS	B12 OF 15	490
BEAM 60-1 DETAILS - TYPE 1-2	B13 OF 15	491
BEAM 100-1 DETAILS - TYPE IV	B14 OF 15	492
	B15 OF 15	493
	C1 OF 3	494
	C2 OF 3	495
	C3 OF 3	496
BRIDGE AT STA. 108+00.21 - STATE HWY. NO. 28	D1 OF 9	497
JONES CREEK		
FOUNDATION PLAN		
BORING DATA		
INT. BENT NO. 3	D2 OF 9	498
INT. BENT NO. 4	D3 OF 9	499
80 FT. SPAN DETAILS	D4 OF 9	500
80 FT. SPAN DETAILS	D5 OF 9	501
80 FT. SPAN DETAILS	D6 OF 9	502
80 FT. SPAN DETAILS	D7 OF 9	503
80 FT. SPAN DETAILS	D8 OF 9	504
BEAM 80-1 DETAILS - TYPE III	D9 OF 9	505
DETOUR BRIDGE AT STA. 89+56.01		
TIMBER ABUTMENT AND INT. BENT FOR TIMBER CAPS AND		
19'-0" PRECAST CONCRETE SPANS 24'-0" CLEAR ROADWAY	D001 OF 8	506
TIMBER ABUTMENT AND INT. BENT FOR TIMBER CAPS AND	D002 OF 8	507
31'-0" PRECAST CONCRETE SPANS 24'-0" CLEAR ROADWAY	D003 OF 8	508
3'-6" PRECAST CONCRETE BRIDGE SLAB DETAILS -	D004 OF 8	509
19'-0" SPANS 24'-0" CLEAR ROADWAY	D005 OF 8	510
4'-6" PRECAST CONCRETE BRIDGE SLAB DETAILS -	D006 OF 8	511
19'-0" SPANS 24'-0" CLEAR ROADWAY	D007 OF 8	512
4'-6" PRECAST CONCRETE BRIDGE SLAB DETAILS -	D008 OF 8	513
19'-0" SPANS 24'-0" CLEAR ROADWAY		
ALTERNATE DETORMED WIRE MESH -		
19' AND 31' PRECAST CONCRETE SPANS		
DETOUR BRIDGE AT STA. 89+56.01	D001 OF 1	514

BRIDGE DIVISION	SHEET NO.	BY
10835	479	ALH
10836	479	ALH
10843	479	ALH
10844	479	ALH

BRIDGE DIVISION	SHEET NO.	BY
10835	479	ALH
10836	479	ALH
10843	479	ALH
10844	479	ALH

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

DETAILED INDEX

BR-013-1(10)

PROJECT 85-0013-01-010-10

COPY 2

COUNTY COPIAH

WORKING NUMBER 012

SHEET NUMBER 3

DATE 2-7-92

ISSUED 2-7-92

DESIGNED

CHECKED

For Information Only

PAY ITEM NO.	PAY ITEM	UNIT	QUANTITIES	
			PRELIMINARY	FINAL
803-A	Test Piles	Each	7	
803-B	Loadng Test	Each	7	
803-G	HP12X53 Steel Piling	L.F.	11,895.0	
803-G	HP14X73 Steel Piling	L.F.	1365.0	
804-A	Bridge Concrete Class "A"	C.Y.	2014.14	△
804-C	40 Ft. Prest. Conc. Beam	L.F.	5194.50	
804-C	60 Ft. Prest. Conc. Beam	L.F.	717.00	
804-C	80 Ft. Prest. Conc. Beam	L.F.	478.50	
804-C	100 Ft. Prest. Conc. Beam	L.F.	498.75	
805-A	Reinforcement	Lb.	306,068	
813-A	Concrete Rolling	L.F.	2360.00	
815-A	Loose Riprap (300*)	Ton	1670.0	

10835
10836
10843
10844

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
(BRIDGE ITEMS)

PROJECT BR-0013-1(10)
COUNTY COPIAH

ISSUED 10/13/13
CHECKED 7/21/17
DATE 2/16/24

WISCONSIN NUMBER COUNTY
SHEET NUMBER

For Information Only

BRIDGE	BEARING STATION	SPAN SIZE	OVERALL LENGTH	ITEM	Test Piles	Leaving Trestle	HP12X53 Steel Piling	HP14X73 Steel Piling	Class AA Bridge Concrete	40 Ft. Prest. Conc. Beam	60 Ft. Prest. Conc. Beam	80 Ft. Prest. Conc. Beam	100 Ft. Prest. Conc. Beam	Reinforce- ment	Concrete Casting	Losses (3.0%)		
																	Each	L.F.
10835	91+46.21	4 x 8'	181'-7"	Spone														
				End Bents														
				Intr. Bents	1	1000.0	427.5	186.27	943.50									
		Continuous For Live Load Only																
				Total	1	1850.0	547.5	283.75	943.50									
10836	102+02.21	16x10'-9"-50'-24x10'-9"-50'-24x10'-9"-50'	611'-7"	Spone														
				End Bents														
				Intr. Bents	3	1100.0	427.5	178.78	2295.50	717.00								
		Continuous For Live Load Only																
				Total	3	5660.0	1365.0	1084.75	2295.50	717.00								
10843	115+02.21	3 x 8'	121'-7"	Spone														
				End Bents														
				Intr. Bents	1	1100.0	427.5	139.94	708.50									
		Continuous For Live Load Only																
				Total	1	1760.0	219.45	708.50										
10844	102+02.21	26x10'-9"-50'-24x10'-9"-50'-24x10'-9"-50'	241'-7"	Spone														
				End Bents														
				Intr. Bents	2	800.0	427.5	282.17	947.50	478.50								
		Continuous For Live Load Only																
				Total	2	1815.0	81.26	418.19	947.50	478.50								
Project Totals																		
					7	11,885.0	1385.0	2014.14	5194.50	717.00	478.50	498.75	305,086	23860.00	1670.0			

10835
10836
10843
10844

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ESTIMATED QUANTITIES
(BRIDGE ITEMS)

PROJECT BR-0013-(10)
85-0013-01-010-1B

COUNTY COPIAH

FORMING NUMBER
SHEET NUMBER

DATE
DATE

For Information Only

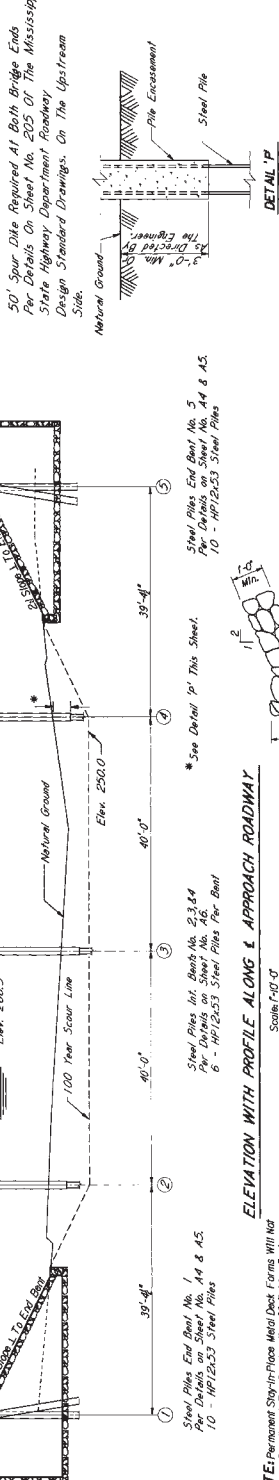
STATE	PROJECT NO.
MISS.	BR-03-110D

GENERAL NOTES:
 Specifications Mississippi Standard Specifications For Road and Bridge Construction, 1990.
 Approval of Plans Will Be Permitted Except In Detail By The Bridge Engineer. Minor Changes In Detail By The Bridge Engineer May Be Made Without Cause For Contract Price Adjustment.
 The Final Surface Texture Of The Bridge Deck Shall Be The Transverse The Finish Per Section 503.03.04.4 Of The Specifications. See Misc-Span Details For Limits Of The Finish On Bridge Deck.
 Expansion Joint Material Shall Be Bituminous Fiber Type Unless Otherwise Noted.
 No Payment Will Be Allowed For Excavation Incidental To The Construction Of End Beams.
 Bar Bending Details Shall Be In Accordance With Manual Of Steel Construction, 10th Edition, Detailing Reinforced Concrete Structures (ACI 318-90).
 Shop Drawings Of Reinforced Beams, Including An Erection Plan, Shall Be Submitted In Duplicate To The Bridge Engineer For Approval Prior To The Manufacture Of Beams.
 Concrete Surfaces Shall Receive A Class 2 Rubber Or Spray Reinforcement. The Spacing Shall Be ASTM A653/Grade 60 unless Otherwise Noted.
 Work For Which No Pay Item Is Provided In The Proposal Will Not Be Paid For Directly And Compensation Therefor Will Be Included In The Prices And Payments For Bid Items.

SPAN NOTE:
 See Deck Joint & Bridge Ends Per Details on Sheet No. A4.

500 YEAR SCOUR ELEVATION	
Beam No.	Elevation
2384	249.0

NOTE:
 50' Spour Dike Required At Both Bridge Ends Per Details On Sheet No. 205 Of The Mississippi State Highway Department Roadway Design Standard Drawings. On The Upstream Side.



ELEVATION WITH PROFILE ALONG & APPROACH ROADWAY

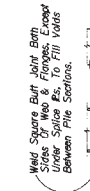
Scale: 1" = 10'

MINIMUM PILE BEARING CAPACITY AND TIP ELEVATION SCHEDULE	
Beam No.	Required Bearing (TIP Elevation)
1	229.0
2	229.0
3	229.0
4	229.0
5	34

TEST PILE SCHEDULE	
Beam No.	Min. Lgt. - Ft. / Tip Elevation
3	60' / 222.44'

ESTIMATED QUANTITIES	
Item	Quantity
Test Piles	1
HP 12x53 Steel Piling	1,950.0
Class 144 Conc. Beam	42.84
Reinforcement	320.0
Losses	370.0
Loose Riprap (130P)	370.0

NOTE: Permanent Stop-In-Place Metal Deck Forms Will Not Be Allowed For Construction Of Bridge Deck.



HP12x53 Steel Piles



HP12x53 Steel Piles

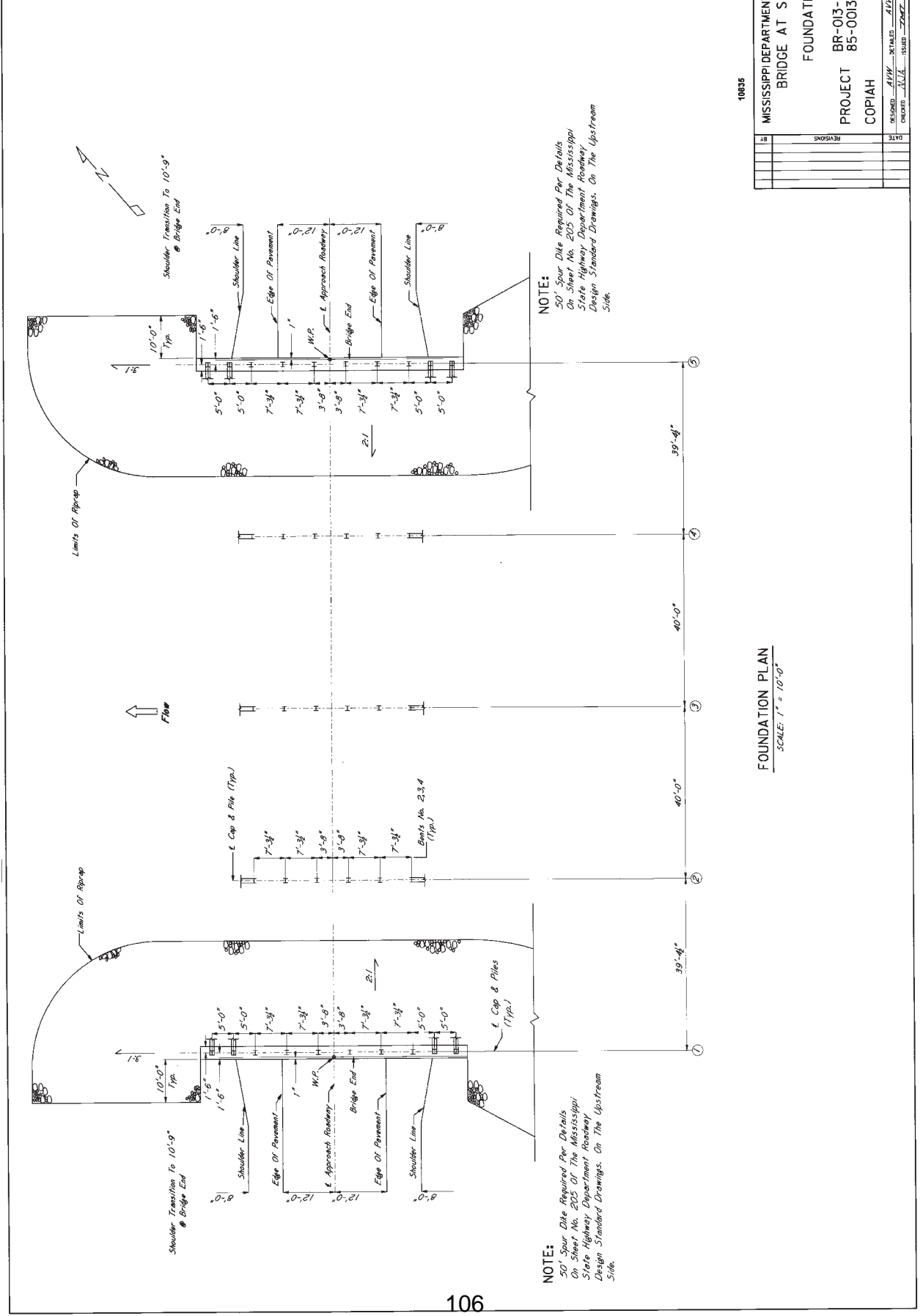
NOTE: Pile Encasement Shall Be Reinforced With 6x6 W 16x16 Welded Wire Fabric. The Encasement Shall Be 24" Thick. For As Reinforcing Steel, Chamfer Corners Of Encasement 1/4".

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 BRIDGE AT STA. 91+48.21
 STATE HWY. NO. 28
 BAYOU PIERRE RELIEF
 PROJECT BR-03-110D
 COUNTY COPIAH

WORKING DRAWING
 SHEET NO. 468
 DATE 2-27-92

For Information Only

STATE	PROJECT NO.
MISS.	BR-03-110D



NOTE:
 50' Spur Dike Required Per Details
 On Sheet No. 205 Of The Mississippi
 State Highway Department Roadway
 Design Standard Drawings. On The Upstream
 Side.

NOTE:
 50' Spur Dike Required Per Details
 On Sheet No. 205 Of The Mississippi
 State Highway Department Roadway
 Design Standard Drawings. On The Upstream
 Side.

10835

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
BRIDGE AT STA. 91+48.21	
FOUNDATION PLAN	
PROJECT	BR-03-110D
COUNTY	COPIAH
DESIGNED BY	AVW
CHECKED BY	MLA
DATE	2-28-94
ISSUED	2-28-94
TRACED	CAJZ
SHEET NUMBER	40
WORKING CENTER	AZ 10673

FOUNDATION PLAN
 SCALE: 1" = 10'-0"

For Information Only

10835

MISSISSIPPI STATE HIGHWAY DEPARTMENT
 BRIDGE AT STA 91 + 48.21
 BORING DATA
 PROJECT BR-013-1(10)
 85-0013-01-010-10
 COPIAH COUNTY

DATE SHEET NUMBER 368
 SHEET NUMBER A3413
 DRAWN: H.A. TRACED: P.T.
 CHECKED: J.A.A. DATE: 2-2-72

LOG OF BORING NO. 92-15-2218-2
 79-0013-0013-10
 THE INDIAN WASH/SHRUB & SHERBY TUBE LOCATION STA. 80+461.457 R.L. E. HWY. 28

DEPTH, FT.	DESCRIPTION OF MATERIAL	CONSTRUCTION LOG	ELEVATION, FT.
0	0' 0" FINE BROWN TO GRAY CLAYE SILT (SHELLS)	100	216.37
10	0' 25" MEDIUM DENSE GRAY TO BROWN FINE TO MEDIUM SAND TO FINE SILT	90	218.37
20	0' 25" MEDIUM DENSE GRAY TO BROWN FINE TO MEDIUM SAND TO FINE SILT	80	220.37
30	0' 25" MEDIUM DENSE GRAY TO BROWN FINE TO MEDIUM SAND TO FINE SILT	70	218.37
40	0' 25" MEDIUM DENSE GRAY TO BROWN FINE TO MEDIUM SAND TO FINE SILT	60	208.37
50	0' 25" MEDIUM DENSE GRAY TO BROWN FINE TO MEDIUM SAND TO FINE SILT	50	198.37
60	0' 25" MEDIUM DENSE GRAY TO BROWN FINE TO MEDIUM SAND TO FINE SILT	40	188.37
70	0' 25" MEDIUM DENSE GRAY TO BROWN FINE TO MEDIUM SAND TO FINE SILT	30	
80	0' 25" MEDIUM DENSE GRAY TO BROWN FINE TO MEDIUM SAND TO FINE SILT	20	
90	0' 25" MEDIUM DENSE GRAY TO BROWN FINE TO MEDIUM SAND TO FINE SILT	10	
100	0' 25" MEDIUM DENSE GRAY TO BROWN FINE TO MEDIUM SAND TO FINE SILT	0	

WILLIAMS
 COMPLETION DEPTH: 71.45'
 DATE: 2-20-72
 S: Split Spoon T: Shelby Tube
 DEPTH TO WATER IN BORING: NOT DETERMINED

PLATE 18

LOG OF BORING NO. 92-15-2218-1
 79-0013-0013-10
 THE INDIAN WASH/SHRUB & SHERBY TUBE LOCATION STA. 80+411.427 R.L. E. HWY. 28

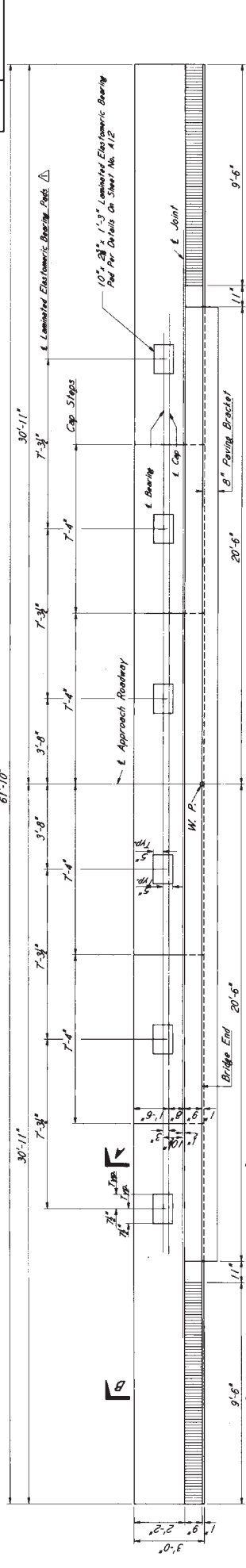
DEPTH, FT.	DESCRIPTION OF MATERIAL	CONSTRUCTION LOG	ELEVATION, FT.
0	0' 5" VERY DENSE GRAY TO YELLOW FINE TO MEDIUM SAND TO FINE SILT	100	216.37
10	0' 5" VERY DENSE GRAY TO YELLOW FINE TO MEDIUM SAND TO FINE SILT	90	218.37
20	0' 5" VERY DENSE GRAY TO YELLOW FINE TO MEDIUM SAND TO FINE SILT	80	220.37
30	0' 5" VERY DENSE GRAY TO YELLOW FINE TO MEDIUM SAND TO FINE SILT	70	218.37
40	0' 5" VERY DENSE GRAY TO YELLOW FINE TO MEDIUM SAND TO FINE SILT	60	208.37
50	0' 5" VERY DENSE GRAY TO YELLOW FINE TO MEDIUM SAND TO FINE SILT	50	198.37
60	0' 5" VERY DENSE GRAY TO YELLOW FINE TO MEDIUM SAND TO FINE SILT	40	188.37
70	0' 5" VERY DENSE GRAY TO YELLOW FINE TO MEDIUM SAND TO FINE SILT	30	
80	0' 5" VERY DENSE GRAY TO YELLOW FINE TO MEDIUM SAND TO FINE SILT	20	
90	0' 5" VERY DENSE GRAY TO YELLOW FINE TO MEDIUM SAND TO FINE SILT	10	
100	0' 5" VERY DENSE GRAY TO YELLOW FINE TO MEDIUM SAND TO FINE SILT	0	

WILLIAMS
 COMPLETION DEPTH: 71.45'
 DATE: 2-20-72
 S: Split Spoon T: Shelby Tube
 DEPTH TO WATER IN BORING: NOT DETERMINED

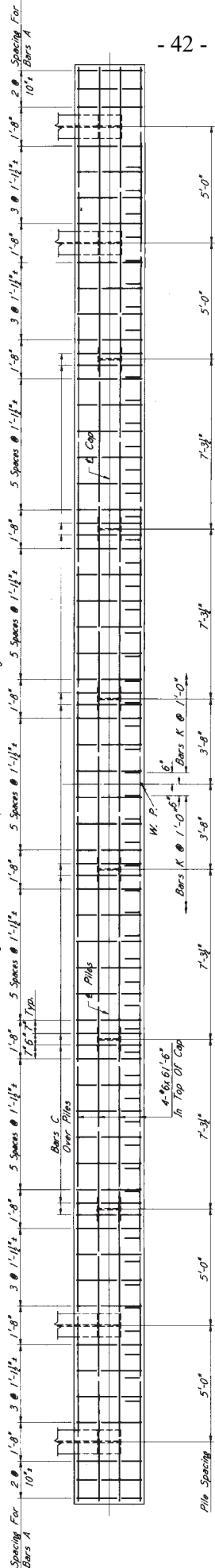
PLATE 17

LOG OF BORING
 Boring Data Shown is For Information Only And Its Accuracy
 For Construction Purposes is Not Guaranteed.

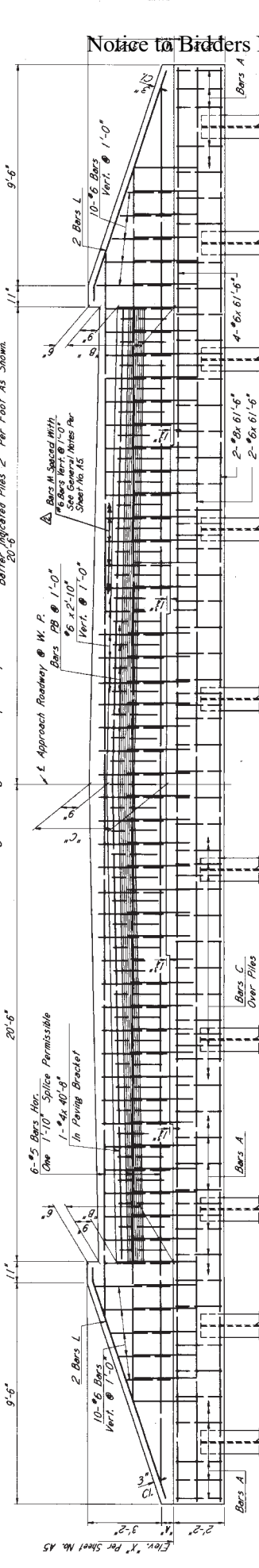
For Information Only



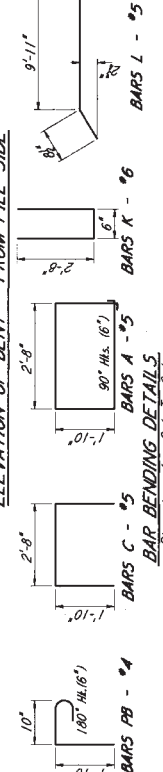
PLAN OF BENT
Showing Concrete Dimensions & Laminated Elastomeric Bearing Pads



PLAN OF CAP
Showing Reinforcing Steel In Top Of Cap



ELEVATION OF BENT - FROM FILL SIDE



BAR BENDING DETAILS
Dimensions Are Out To Out

NOTE: Vertical Dimensions Shown Are Measured Along Fill Face Of End Wall (Bridge End).
FOR GENERAL NOTES, TABLE OF ELEVATIONS AND DIMENSIONS, AND OTHER DETAILS SEE SHEET NO. AS.

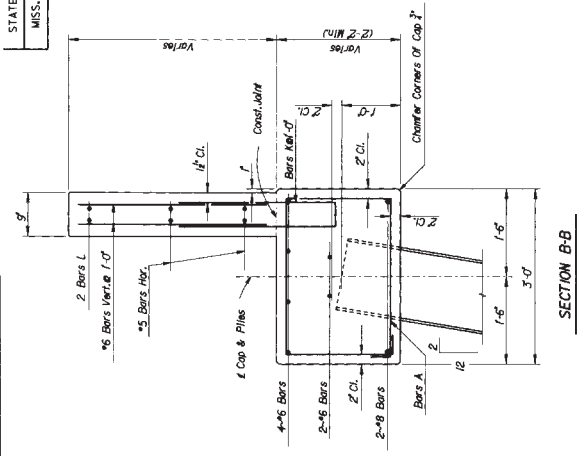
SPLICE NOTE: Cap May Be Lapped Spliced Long Bars In As Follows: #6 - 2'3\"/>

Exp. Bars Per Sheet No. AS

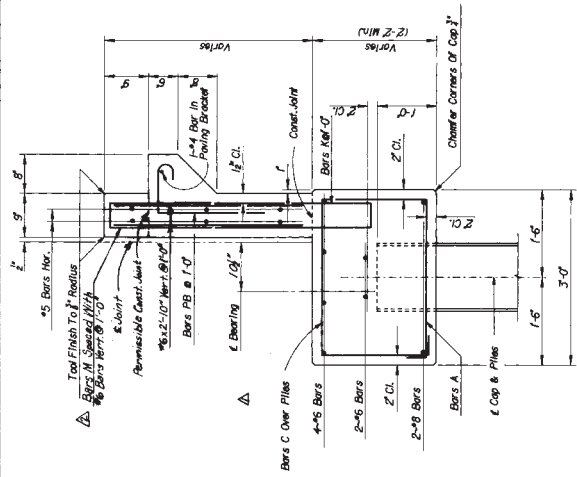
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 BRIDGE AT STA 91+48.21
 END BENT NO. 1 & 5 DETAILS
 PROJECT BR-013-1(10)
 COPIAH COUNTY
 DESIGNED: JAW
 CHECKED: JAW
 DATE: 1/18/21
 REVISIONS:
 REVISIONS:
 DRAWN: JAW
 CHECKED: JAW
 DATE: 1/18/21

For Information Only

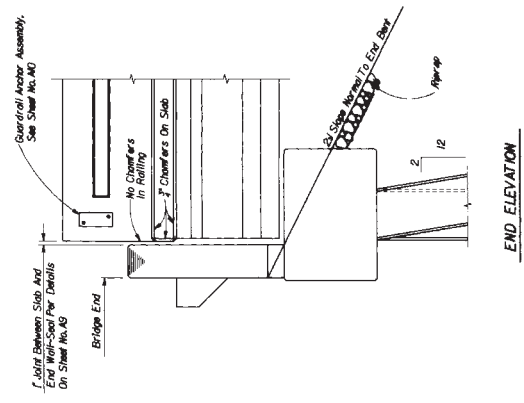
STATE PROJECT NO.
MISS. BR-013-1(10)



SECTION B-B



SECTION A-A



END ELEVATION

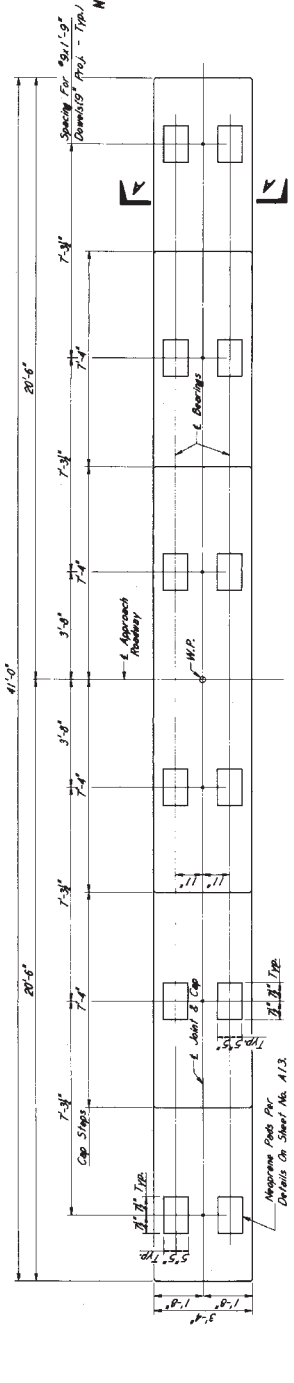
TABLE OF ELEVATIONS AND DIMENSIONS

END BENT NO.	ELEV. *X*	DIM. *A*	DIM. *B*	DIM. *C*
Bridge A	268.5572	6'0"	3'-2 1/2"	3'-3 3/8"
Bridge B	269.0705	6'0"	3'-2 1/2"	3'-3 3/8"
Bridge C	270.2416	6'0"	3'-2 1/2"	3'-3 3/8"
Bridge D	268.6295	6'0"	3'-2 1/2"	3'-3 3/8"
	261.1731	6'0"	3'-2 1/2"	3'-3 3/8"
	260.8085	6'0"	3'-2 1/2"	3'-3 3/8"

GENERAL NOTES:
 All Concrete in End Bents Shall Be Class "A".
 Reinforcement in End Bents Shall Be Class "A".
 Reinforcement in End Bents Shall Be Class "A".
 Reinforcement in End Bents Shall Be Class "A".

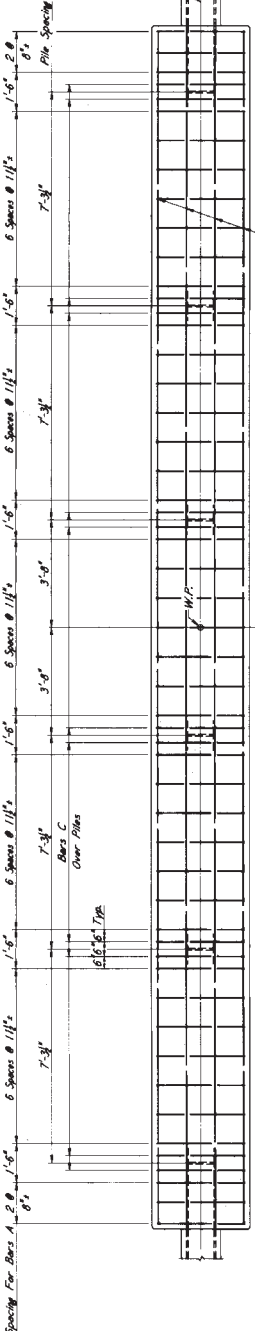
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 BRIDGE AT STA. 91+48.2
 END BENT DETAILS
 PROJECT BR-013-1(10)
 COUNTY COPIAH
 SHEET NUMBER 470
 DATE 2-2-58
 DESIGNED BY J.A. ROSS
 CHECKED BY A.H.W. RETALD
 DRAWN BY A.H.W. RETALD
 DATE 2-2-58

For Information Only



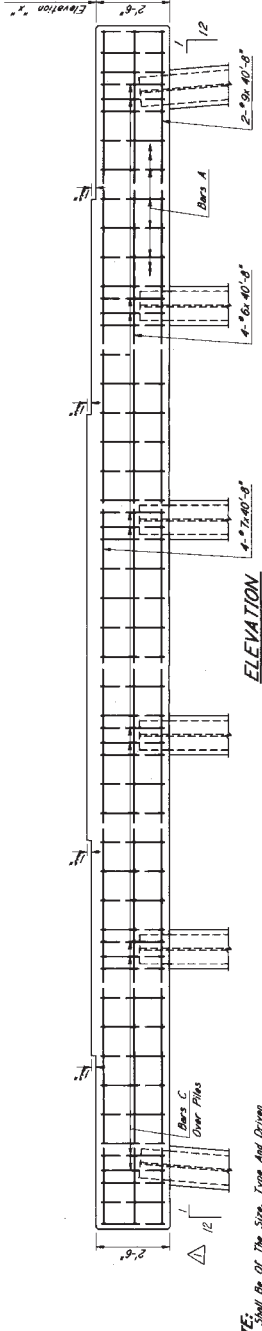
PLAN OF BENT

Showing Concrete Dimensions & Driven Spacings



PLAN OF CAP

Showing Reinforcement in Top of Cap and Pile Spacing



ELEVATION

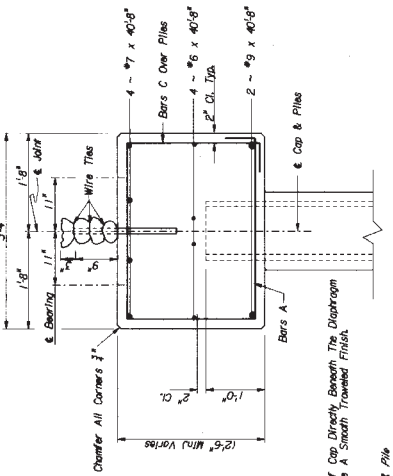
GENERAL NOTES:
All Concrete in Cap Shall Be Class "AA"
Reinforcing Bars Shall Be Class "AA"
Reinforcing Bars Shall Be Class "AA"
Concrete Surfaces Are Clear Distances.



BAP BEYOND DETAILS

Dimensions Are Out To Out

NOTE: Wrap #9 Details With Three Layers of 30# Roofing Felt And The With Wire Ties As Shown.



SECTION A-A

NOTE:
Portion of Cap Directly Beneath The Diaphragm Shall Have A Smooth Troweled Finish.

TABLE OF ELEVATIONS

BENT NO.	ELEV. "x"
2	266.0072
3	266.9431
4	268.0673
5	270.7833
6	270.8000
7	270.8705
8	270.8918
9	270.8486
10	270.5473
11	270.5473
12	270.8689
13	270.8689
14	270.8203
15	268.6206
16	268.5165
17	261.3176
18	261.0899

10035

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
BRIDGE AT STA. 91+48.2: 2732

INT. BENT NO. 2.3.84 DETAILS

PROJECT BR-013-1(10)
85-0013-01-010-10

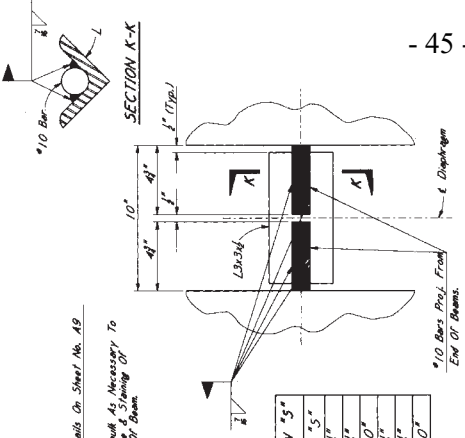
COPIAH COUNTY

WORKSHEET NUMBER AS 07 13
SHEET NUMBER 071

DESIGNED: JHW - DETAIL: JHW - CHECK: GAO
DATE: 11/11/01 - DATE: 11/11/01

For Information Only

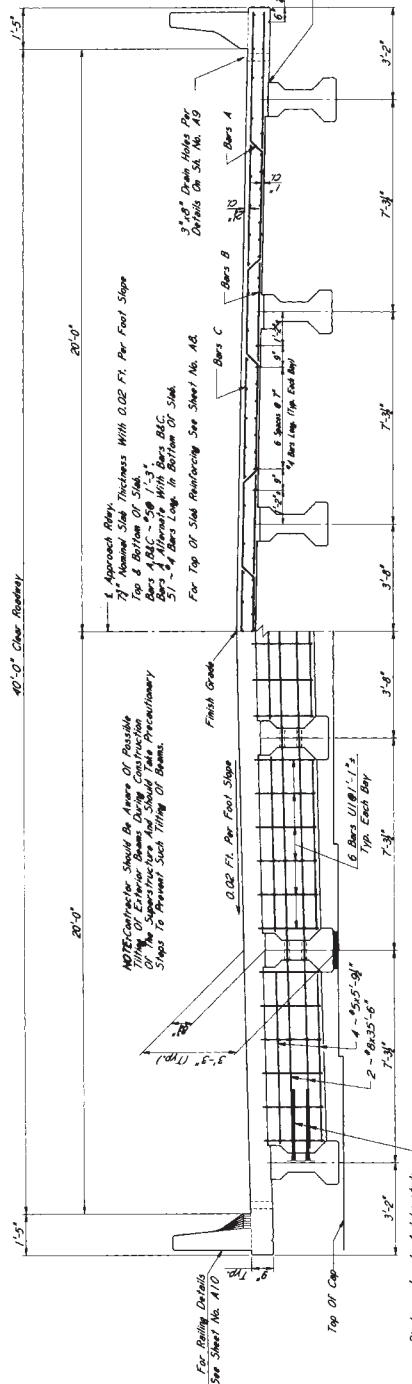
Span No.	X"	Y"
Bridge A	1-4	127
Bridge B	1-4	127
Bridge C	11-14	127
Bridge D	1-2	63



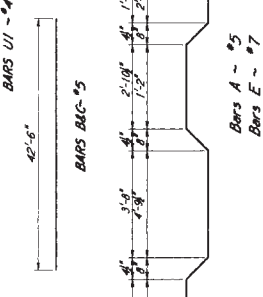
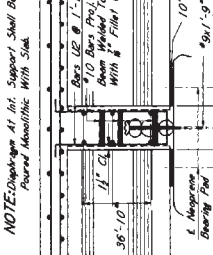
NOTE: The Volume of Concrete in the Slabs, Beams and Girders has been Estimated by Using One Half (1/2) of the Filler Height at the Bearing Length of the Beams. This Volume Shall Be Checked For Final Quantity by Other Methods. For Typical Span Details See Sheets No. 48, 49 & 50.

DESIGN DATA:
Specifications A.A.S.H.T.O., 1992
Slab Stresses 5.24, 6.02, 6.11, 6.16, 6.21, 6.26
Prestressed Beam Stresses . . . See Sheets No. 47 & 48

MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 91+48.21
PROJECT BR-013-1(10)
CORNER COPIAH COUNTY
WORKING NUMBER AR 8-13
DRAWN BY
CHECKED DATE
DATE BY
SCALE 1/4" = 1'-0"



Beam No.	Dim. "S"
Bridge A	1.8.5
Bridge B	1.11.8.15
Bridge C	1.8.8
Bridge D	1.8.6

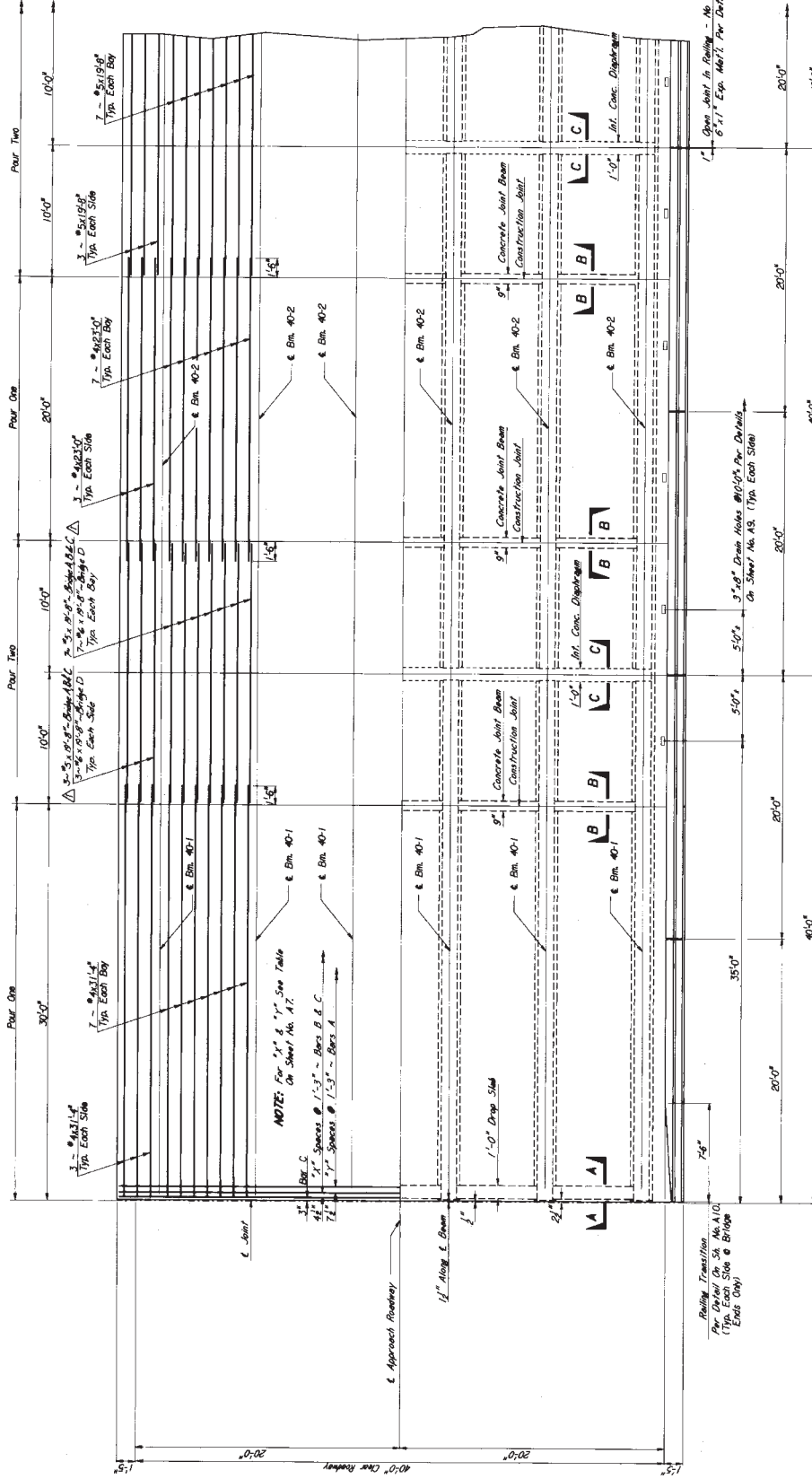


BAR BENDING DETAILS
Dimensions Are Out To Out

For Information Only

STATE	PROJECT NO.
MISS.	BR-013-1(10)

NOTE: The Dead Paving Schedule Shall Be As Shown On These Plans And Any Alternate Sequence Will Not Be Permitted.



NOTE: For 3" & 1" Spacing
On Sheet No. A7
1" Spacing @ 1'-3" - Bars B & C
3" Spacing @ 1'-3" - Bars A

PLAN OF 40 FT. INT. SPAN
Top Half Showing Longitudinal Reinforcing in Top of Slab

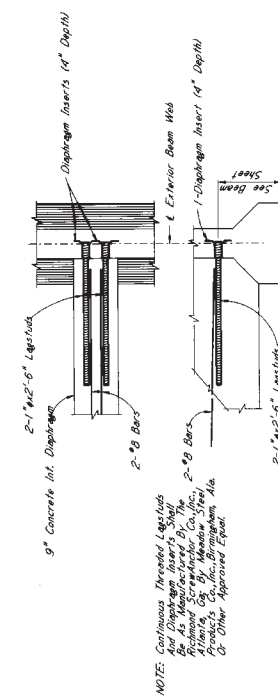
PLAN OF 40 FT. END SPAN
Top Half Showing Longitudinal Reinforcing in Top of Slab

NOTE: For GENERAL NOTES AND OTHER SPAN DETAILS See Sheets No. A7B&A-1,3.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
BRIDGE AT STA. 91+48.21	
40 FT. SPAN DETAILS	
PROJECT	BR-013-1(10)
COPIAH COUNTY	85-0013-01-010-10
WORKING NUMBER	AB of 33
SHEET NUMBER	473
DESIGNED BY	DATE
CHECKED BY	DATE

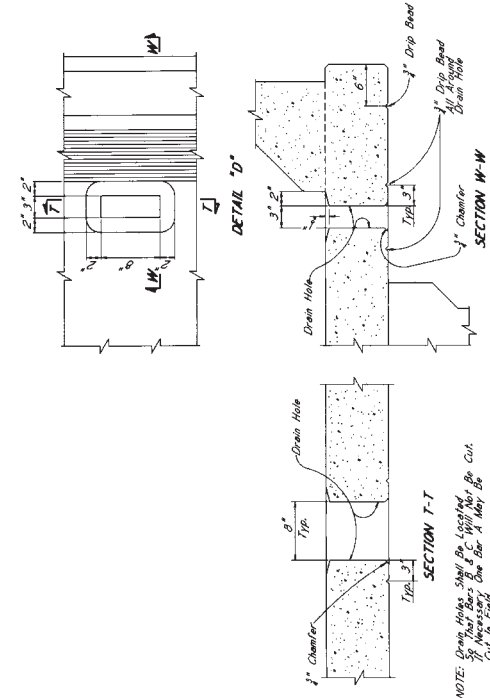
RAILING BARS			
END SPAN	IN T. SPAN		
MARK	NO.	MARK	NO.
D	82	D	100
R	82	R	100
DT	24		
RT	24		

For Information Only



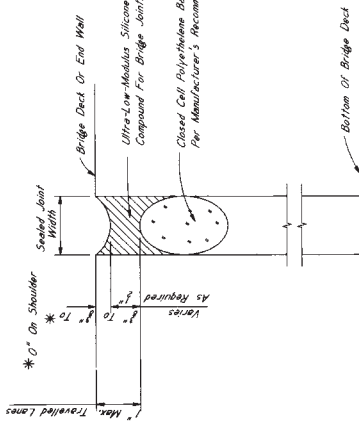
NOTE: Continuous Threaded Lag Stud Be As Manufactured By The Richmond Screw Anchor Co., Inc., Products Co., Inc., Birmingham, Ala. Or Other Approved Equal.

DIAPHRAGM INSERT AND LAGSTUD DETAILS



NOTE: Drop Holes Shall Be Located So That Bars 'B' & 'C' Will Not Be Cut. If Necessary One Bar 'A' May Be Cut On Field.

DRAIN HOLE DETAILS
Use Where Shown On The Span Detail Sheet.



TYPICAL SECTION
- SILICONE SEALED JOINT

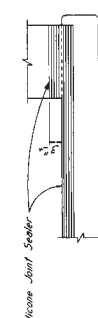
NOTE: The Contractor Should Be Aware That The Joint Openings At 60°F Temperature At And Should Plan Installation Of The Joint Sealer Material Accordingly.

NOTE: Seal Joints At The Location Specified In The SPAN NOTE On The SCHEDULE DRAWINGS. Seal Joints At Or Near The Diaphragm Width.



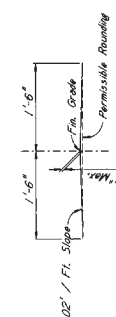
LIMITS OF FINE FINISH

NEOPRENE PAD THICKNESS TABLE	
PAD COMPRESSED	THICKNESS PAD THICKNESS
1"	3"
2 1/2"	2 1/2"
	2 1/2"

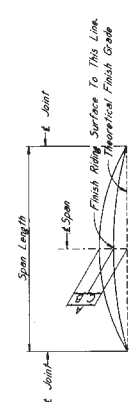


ELEVATION AT END OF SPAN

NOTE: Where Newly Constructed Bridges Are Exposed To Construction Traffic Prior To Paving Of The Approach Roadways, The Joints Shall Be Protected From Traffic. This Protection Shall Be Required According To The Engineer's Instructions. This Protection Shall Be Required At All Joints. The Contractor Shall Be Responsible For The Protection At No Additional Cost To The State.



CROWN DETAILS



DEFLECTION DIAGRAM

A = Total Deflection Allowance For Deflection
B = Estimated Deflection Due To Dead Load Of Slab & Bent
C = A-B = Net Initial Camber In Riding Surface, Which Includes An Allowance For Creep.
NOTE: For Values Of A, B & C, See Span Detail Sheets.

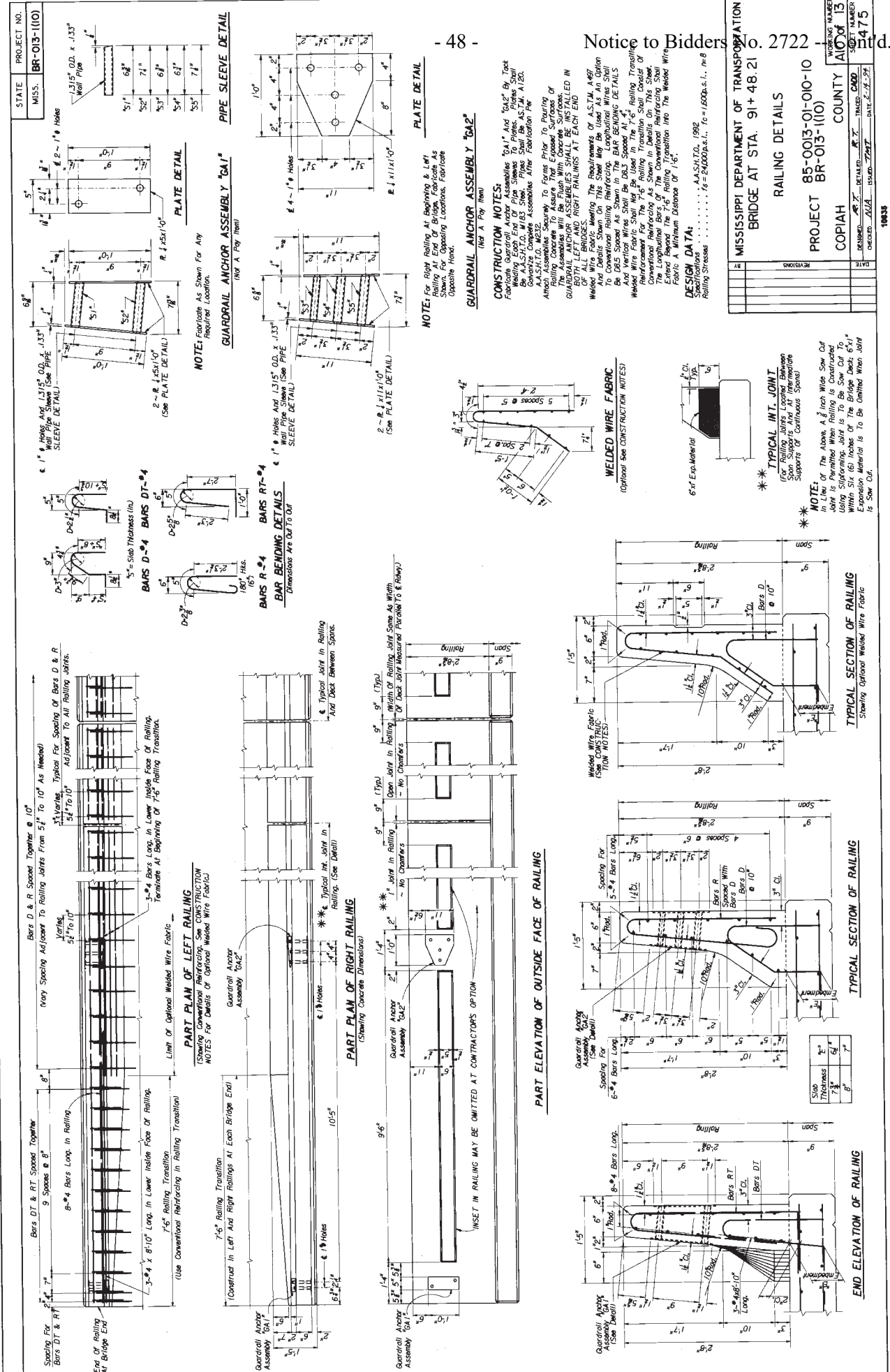
GENERAL NOTES:
All Concrete In Span And Railing Shall Be Class "AA" Concrete. All Layers Shall Be Properly Consolidated. All Reinforcing Steel To Concrete Surfaces Shall Be Properly Lapped. All Reinforcing Steel To Concrete Surfaces Shall Be Properly Lapped. All Reinforcing Steel To Concrete Surfaces Shall Be Properly Lapped. All Reinforcing Steel To Concrete Surfaces Shall Be Properly Lapped.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
BRIDGE AT STA. 91+48.21
MISC. SPAN DETAILS
PROJECT BR-013-01-010-10
COPIAH COUNTY

DATE	ISSUED	REVISED	DATE
8-22-88	7-27-87	4-27-87	2-24-84

10088

For Information Only



MISSISSIPPI DEPARTMENT OF TRANSPORTATION
BRIDGE AT STA. 91+48.21

RAILING DETAILS

PROJECT 85-0013-01-010-10
BR-013-110

COPIAH COUNTY

WORKING NUMBER
A102 of 13

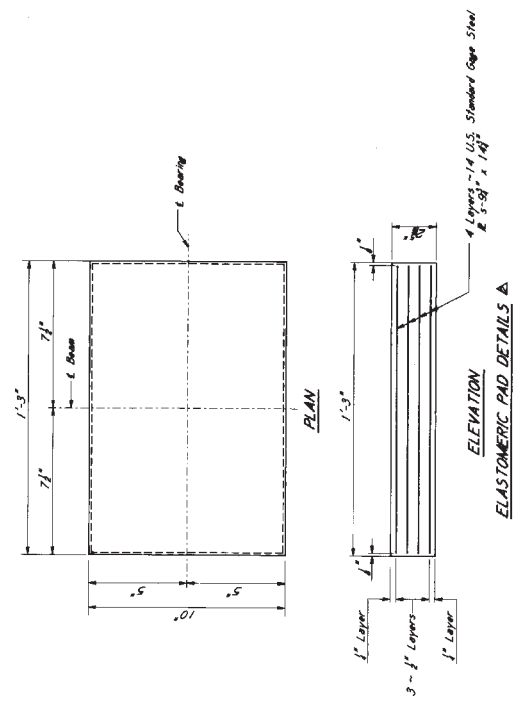
SHEET NUMBER
4475

DATE	REVISION

DESIGNED BY: J.A. TRIMMER
CHECKED BY: J.A. TRIMMER
DATE: 1-27-72

For Information Only

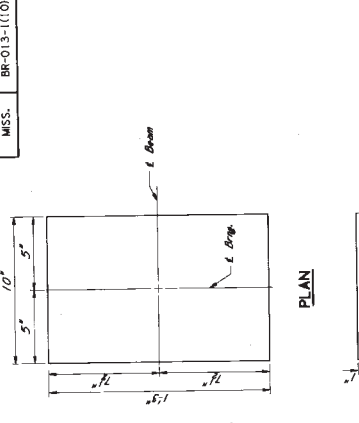
NOTE: Elastomer Compound Shall Have A Min. Shear Modulus At 75°F Of 120 psi.
 Testing Acceptance Procedure Shall Be In Accordance With Section 714.10.6 Of The Specifications.



MISSISSIPPI DEPARTMENT OF TRANSPORTATION			
BRIDGE AT STA. 91+48.21-102			
LAMINATED ELASTOMERIC BEARING PAD DETAILS			
TYPE (H+2)			
PROJECT BR-013-1(10)			
COUNTY COPIAH			
SHEET NUMBER A12.1 of 13			
DATE	DESIGNED	CHECKED	SCALE
BY	BY	BY	BY
REV	REV	REV	REV
REV	REV	REV	REV

For Information Only

STATE MISS. PROJECT NO. BR-013-1(10)

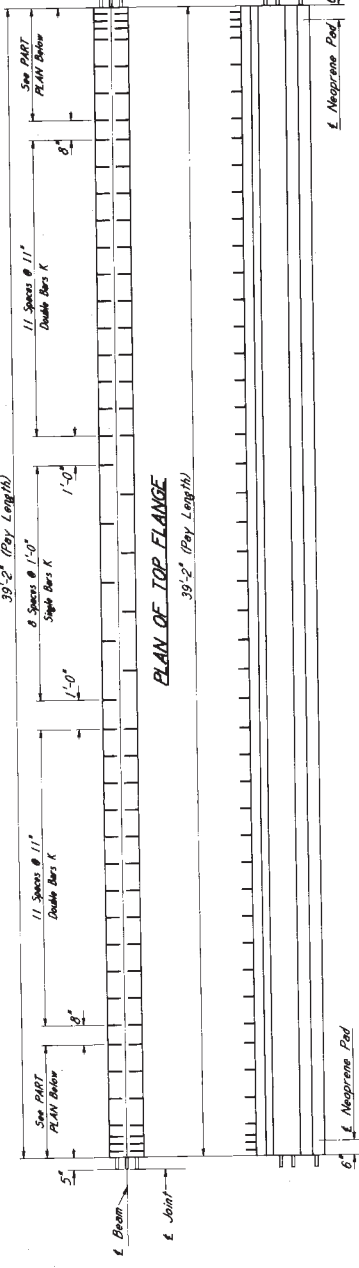


PLAN

ELEVATION

NEOPRENE PAD DETAIL

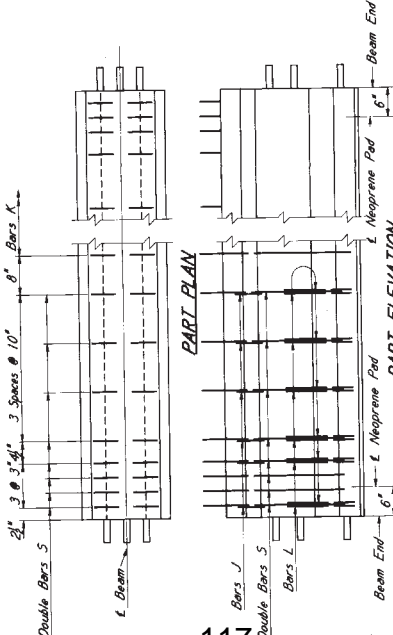
NOTES: In No Case Shall Neoprene Pads Be Cut Smooth And True To Grade.



PLAN OF TOP FLANGE

ELEVATION

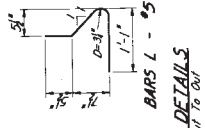
NOTES: Cut Strands Flush-As Cooling Required Both Ends.



PART PLAN

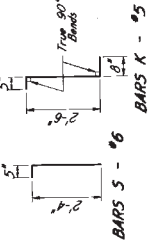
PART ELEVATION

STRAITS NOT SHOWN FOR CLARITY



BAR BENDING DETAILS

DIMENSIONS ARE GUT TO GUT



BARS S - #6

BARS K - #5

GENERAL NOTES:
 REINFORCEMENT SHALL BE MANUFACTURED IN ACCORDANCE WITH MISSISSIPPI STANDARD SPECIFICATIONS FOR ROAD & BRIDGE CONSTRUCTION, 1992.
 THE TOP OF BEAMS SHALL BE FINISHED WITH A COURSE OF BRUSH TO REMOVE ALL LUBRICANT AND PROVIDE A RAUGHENED SURFACE FOR BONDING. OTHER SURFACES SHALL BE FINISHED PER SPECIFICATIONS FOR BONDING. THE LIMITS SHOWN IN TABLE.
 STEEL SURFACES OF THE BEARING ASSEMBLY SHALL BE FINISHED PER SPECIAL PROVISIONS OF THE SPECIFICATIONS.
 STRANDS SHALL BE PLACED AS SHOWN IN TABLE. ALTERNATE STRAND PATTERNS AS SHOWN IN TABLE MAY BE USED AT THE CONTRACTOR'S OPTION. SHOP DRAWINGS OF REINFORCEMENT SHALL SHOW THE CYLINDER STRENGTH OF THE CONCRETE SHALL BE AS SHOWN IN TABLE.

Notice to Bidders Co. 2722

DESIGN DATA
 Unit Stresses Are In Accordance With A.A.S.H.T.O., 1992:

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
BEAM 40-2 DETAILS	
TYPE I+2	
PROJECT BR-013-1(10)	
COUNTY COPIAH	
WORKING NUMBER	85-0013-01-010-10
DATE	4/8/94
DRAWN BY	AWW - EMMET
CHECKED BY	AWW - TRACY
DATE	2-2-94
SHEET NUMBER	418

ALTERNATE PRESTRESS REQUIREMENTS

SP. Indicates Location of Strands	Strand Type	Minimum Strength	Initial Tension	Required Number of Strands	Maximum and Minimum Spacing	Distance From Center Line to Strand	Direction of Prestress			Concrete Strength At Time of Casting
							A	B	C	
I-#270 K-LR	41,300	28,910	30,980	12	7.00"	6.00"	0" to 1"	0" to 1"	0" to 1"	4200 p.s.i.
							8.17"	8.17"	8.17"	4200 p.s.i.

END ELEVATION

For Information Only

PROJECT NO.	BR-013-11(10)
STATE	MISS.
SECTION	DETOUR BRIDGE AT STA. 98+56.01
LOCATION	ACROSS JONES CREEK
PROJECT	BR-013-11(10)
COUNTY	COPIAH
WORKS NUMBER	85-0013-01-010-10
SECTION NUMBER	DB16 OF 8
DATE	2/2/24
ISSUED	2/2/24
REVISIONS	

GENERAL NOTES:

- 1. Standard Mississippi Standard Specifications For Road And Bridge Construction Shall Apply.
- 2. No Change Of Plans Will Be Permitted Except By Written Approval Of The Bridge Engineer. Minor Changes Of Detail Of Design Or Construction Procedures May Be Authorized By The Bridge Engineer Provided Such Changes Will Not Be A Cause For Contract Price Adjustment.
- 3. All Work Shall Be Done In Accordance With The Plans Unless Otherwise Noted.
- 4. Timber Piles May Be Unseasoned And Rough And Shall Be New Material. Timber Piles May Be Unseasoned For Piling Size See Section 719 Of The Specifications.
- 5. All Work Shall Be Done In Accordance With The Plans Unless Otherwise Noted.
- 6. The Elevation Of The Permanent Structure.
- 7. Payment For The Detour Bridge Will Be Made Under Pay Item No. 507-40-C, Construction And Removal Of Detour Bridges.
- 8. All Work Shall Be Done In Accordance With The Plans Unless Otherwise Noted.
- 9. Work For Which No Pay Item Is Provided In The Proposal Will Not Be Paid For Directly And Compensation Thereof Will Be Included In The Prices And Payments For Bid Items.
- 10. Items In These Plans Including But Not Limited To: In Road Concrete Structures, Reinforcing Steel, And In Retaining Walls, Reinforcing Steel Shall Be Furnished By The Contractor And Subject To Approval By The Project Engineer Prior To Erection.
- 11. After The Permanent Structure Has Been Opened To Traffic, The Detour Bridge Shall Be Removed By The Contractor, All Material In The Bridge Shall Be Removed The Property Of The Contractor And Shall Be Removed From The Site.

SPECIAL PROVISIONS REQUIRED:

Construction & Removal Of Detour Bridges No. 507-68

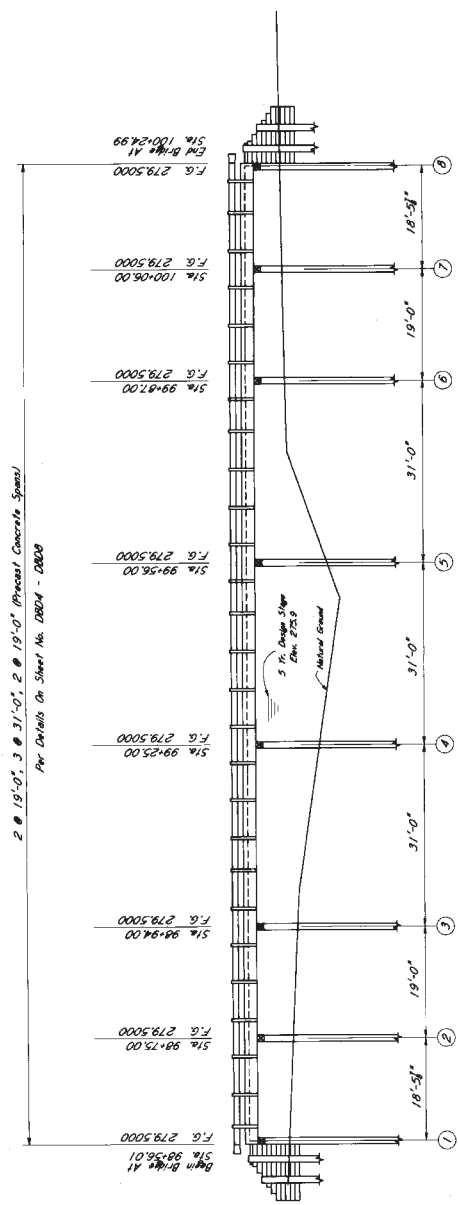
DRAINAGE DATA:

Drainage Area 137 Sq. Mi.
OS (U.S.G.S) 4600 c.f.s.
Effective Area 900 Sq.Ft.

DESIGN DATA:

Specifications AASHTO M 292
Loading HS20-44
Roadway Width 24'-0" Gutter To Gutter

Total Length Of Bridge = 160'-11 1/2"
0.0008 Grade



Untreated Timber Pile
Pile Dia. 8" x 8"
On Sheet No. DB022
10 - Piles Per Bent.

Untreated Timber
Pile Dia. 8" x 8"
On Sheet No. DB022
8 - Piles Per Bent.

Untreated Timber
Pile Dia. 8" x 8"
On Sheet No. DB022
6 - Piles Per Bent.

Untreated Timber Pile
Pile Dia. 8" x 8"
On Sheet No. DB022
10 - Piles Per Bent.

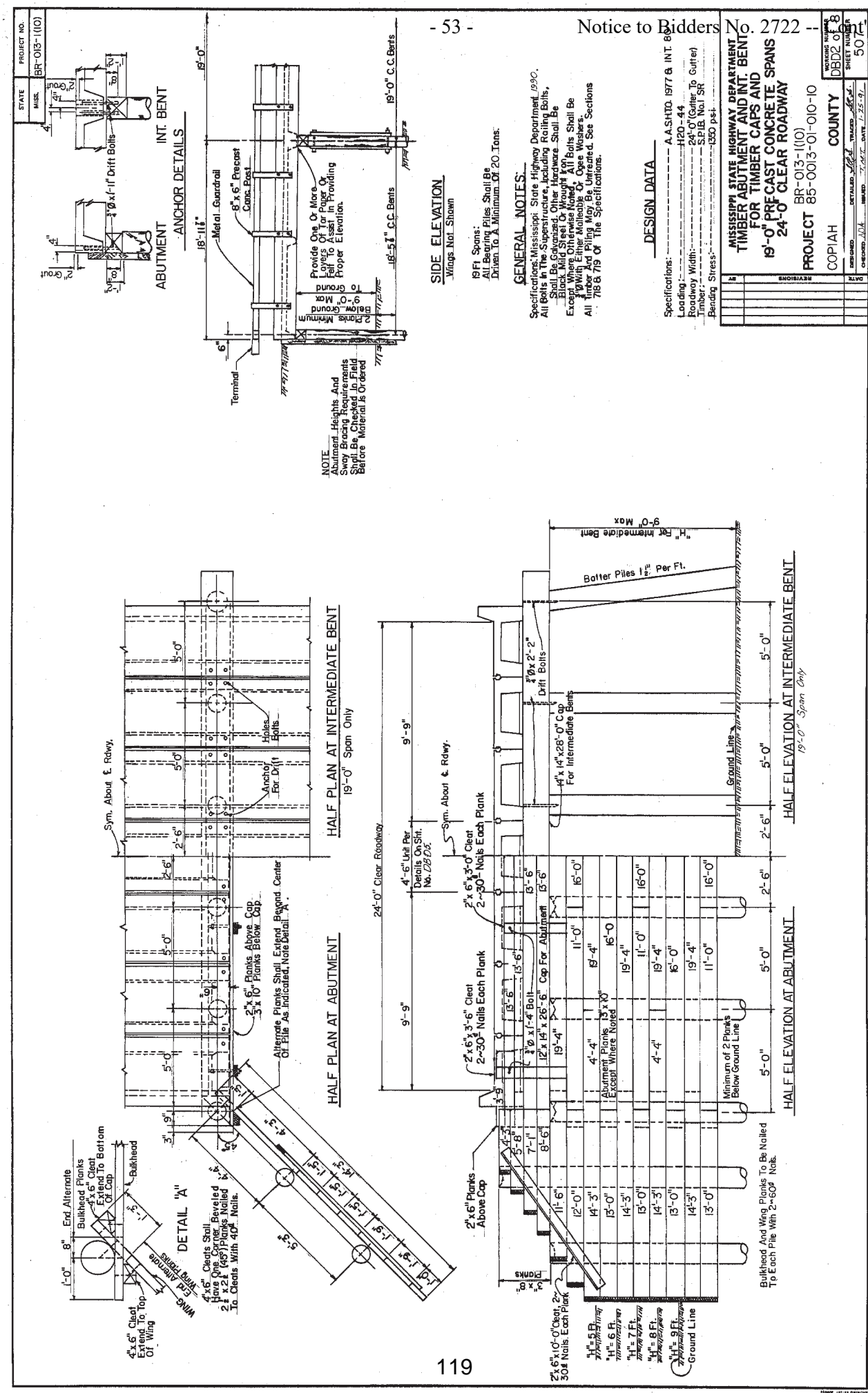
SPECIAL NOTES:
The Contractor Shall Drive Piles Of Sufficient Length To Insure Stability Of Substructure. The Alignment Of The Substructure To The Banks Of The Creek May Be Adjusted To Provide The Most Effective Location. These Geometric And Structural Requirements, Minimum Piles And Design Computations Meeting Minimum Requirements Shall Be Submitted To The Engineer And If Approved By The Bridge Engineer, May Be Used At No Additional Cost To The State.

DETOUR BRIDGE ACROSS JONES CREEK

Scale: 1" = 10'-0"

MINIMUM PILE BEARING CAPACITY	RECD. BEARING (TONS)
BENT NO.	
1, 2, 7, 8	20
3, 4, 5, 6	22

For Information Only



STATE PROJECT NO. MUSK. BR-013-1(10)

NOTE:
Abutment Heights And
Sway Bracing Requirements
Shall Be Checked In Field
Before Material Is Ordered

GENERAL NOTES:

Specifications: Mississippi State Highway Department 1930.
All Bolts In The Superstructure, Including Railing Bolts,
Shall Be Galvanized, Other Hardware Shall Be
Except Where Otherwise Noted All Bolts Shall Be
With Either Malleable Or Steel Washers.
All Timber And Piling May Be Unaired. See Sections
718 & 719 Of The Specifications.

DESIGN DATA

Specifications: A.A.S.H.T.O. 1977 & INT. 80
Loading: H-20-44
Roadway Width: 24'-0" (Center To Center)
Timber: S.P.I.B. No. 1 SR
Bending Stress: 1350 psi

MISSISSIPPI STATE HIGHWAY DEPARTMENT
TIMBER ABUTMENT AND INT. BENT
FOR TIMBER CAPS AND
19'-0" PRECAST CONCRETE SPANS
24'-0" CLEAR ROADWAY

PROJECT BR-013-1(10)
COPIAH COUNTY

REVISIONS	DATE	BY	NO.

ISSUED: 3-14-37

10835

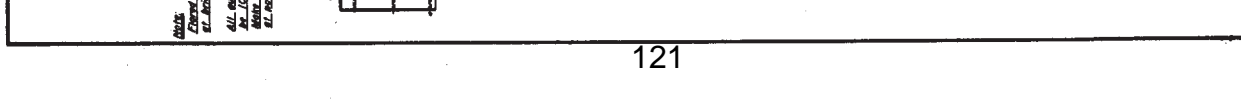
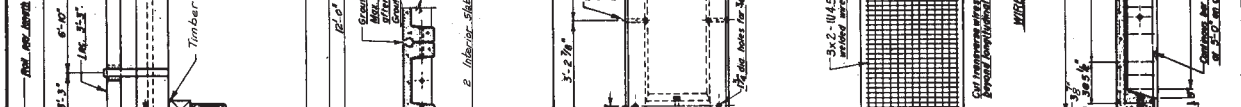
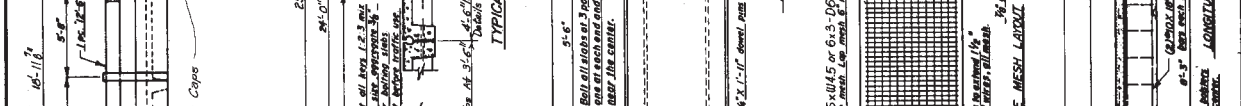
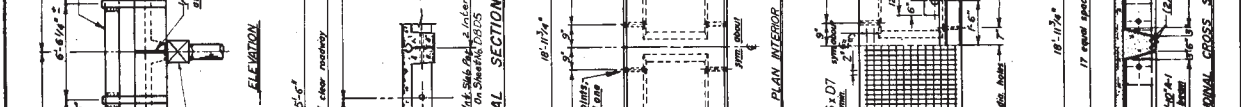
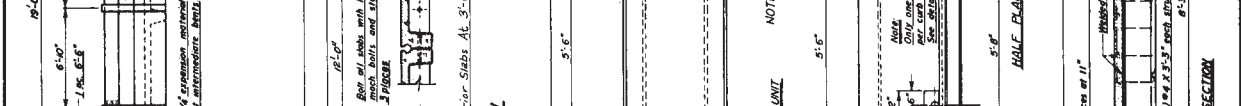
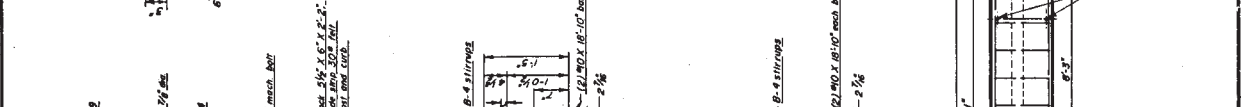
For Information Only

MISSISSIPPI STATE HIGHWAY DEPARTMENT
 3'-6" PRECAST CONCRETE BRIDGE SLAB DETAILS
 19'0" SPANS 24'-0" CLEAR ROADWAY
 PROJECT BR-013-1(10) 85-0015-01-01C-10

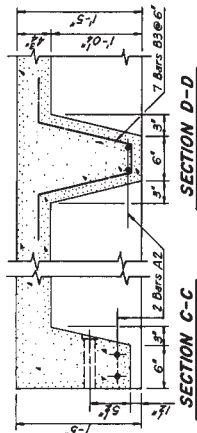
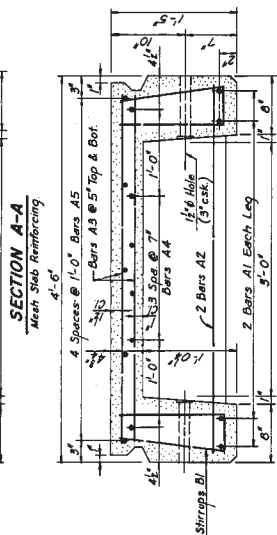
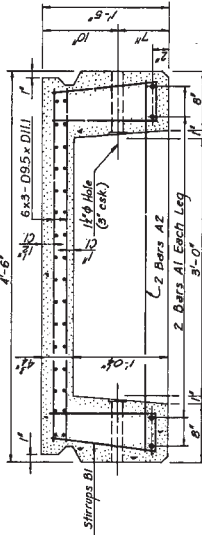
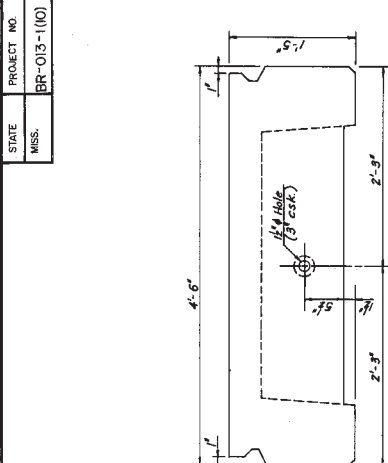
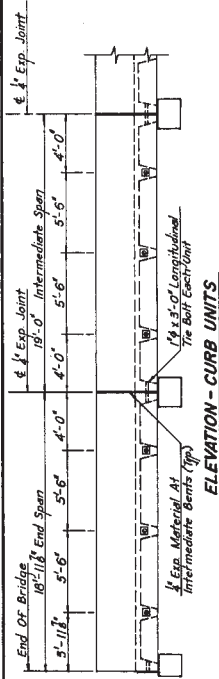
DESIGNED BY	DATE	SHEET NO.
Checked by	7 July 1971	59
Drawn by		
Scale		
Project No.	BR-013-1(10)	
Sheet No.	59	

COPIAH
 WORKING NO. BDD4 of 8
 JULY 1971

GENERAL NOTES:
 Mississippi State Highway Department, 1980.
 Open Loading A.A.S.H.T.O. HS20-44
 Concrete All concrete shall be Class 30 bridge concrete with a minimum compressive strength of 3000 P.S.I. at 28 days.
 All exposed concrete to be cleaned 50% 40% unless otherwise noted.
 Concrete will not be paid for severity, but will be considered subsidiary to the item of precast slab unit.
Reinforcing Steel: Reinforcing steel shall be as specified in the forms and ready mixed concrete shall be placed in the forms.
 Reinforcing steel shall be placed in accordance with A.S.T.M. A615. All bars shall be lap spliced in accordance with A.S.T.M. A615. The above steel will not be paid for severity, but will be considered subsidiary to the item of precast slab unit.
Work: Work shall conform to the specifications for related steel work A.S.T.M. A 165 or A.S.T.M. A 497.
 Work shall be in accordance with the specifications for precast concrete in accordance with the item of precast slab unit.
Hardware: All machine bolts connecting slab units will not be paid for severity, but will be considered subsidiary to the item of precast slab unit.
 All bolts, washers, and nuts shall be galvanized or stainless steel.
Grout: Grout will be placed in all voids, maximum size aggregate 3/8" and shall be placed in accordance with A.S.T.M. A 615. Grout will not be paid for severity, but will be considered subsidiary to the item of precast slab unit.
Formwork: A dimension of more than 1/2" in dimensions will be cause for rejection of the unit.



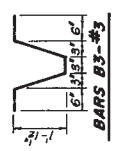
NOTE: The Nominal "2" Shall Be Located Inside And Outside Of One End Of Each Slab Unit Or At Some Other Location As Approved By The Bridge Engineer. The Nominal Shall Be 2 High And 4" Deep.



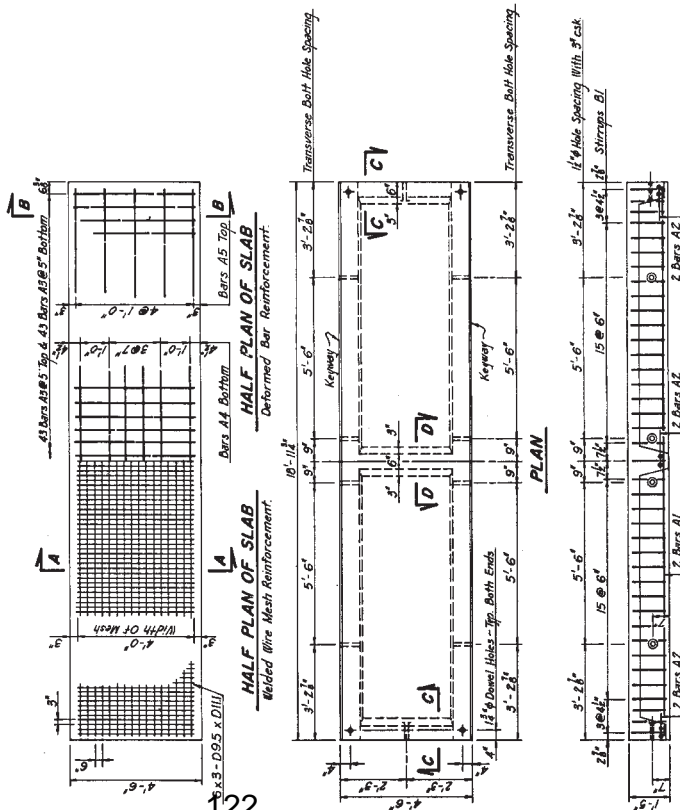
NOTE: The Numerical '2' Shall Be Located Inside And Outside Of One End Of Each Straight Bar. The Bar Size Engineering Approved By The Bridge Engineer. The Numerical '2' Shall Be 2" High And 1/4" Deep.

STRAIGHT BARS

Mark	Size	Length	Mesh Slab	Bar Slab	No. Required
A1	#9	10'-9"	4	4	4
A2	#4	4'-3"	6	6	6
A3	#4	8'-1"	0	66	0
A4	#3	10'-9"	0	6	0
A5	#3	10'-9"	0	6	0



BAR BENDING DETAILS
 Dimensions Are Given To Cent.



GENERAL NOTES:

Specifications: Mississippi State Highway Department, 1990.
 Design Loading: A.A.S.H.T.O. HS20-44
 Concrete: All Concrete Shall Obtain A Minimum Compressive Strength Of 3,000 p.s.i. At 28 Days & Unless Noted Otherwise Reinforcing Bars Shall Conform To A.S.T.M. A-615. Primary Tension Reinforcing Bars In Bottom Of Slabs (Bars A1), And Transverse Bars In Top Of Slabs (Bars A3), For Alternate Slab Reinforcing Shall Be Grade 60. All Other Reinforcing Bars Shall Be Grade 40. The Reinforcing Bars Shall Be Welded To The Mesh Directly. Bar Will Be Considered As Subsidiary To The Item Of The Precast Slab Unit.
 The Mesh Shall Conform To The Specification For Welded Wire Mesh For Concrete Reinforcement, A.S.T.M. A 181.
 The Mesh Will Not Be Used For Concrete Reinforcement As Subsidiary To The Item Of The Precast Slab Unit.
 All Meshing Done For Precast Slabs Shall Be Substandard or Standard. All Meshing Done For Precast Slabs Shall Be Standard or Considered Subsidiary To The Item Of The Precast Slab Unit.
 Grouting Of Keyways: All Material And Labor For Mining And Placing Grout For Keyways Will Not Be Paid For Directly, But Will Be Considered Subsidiary To The Item Of The Precast Slab Unit.
 To Be Considered Subsidiary To The Item Of The Precast Slab Unit. Cause For Rejection Of The Unit.

DESIGN DATA:

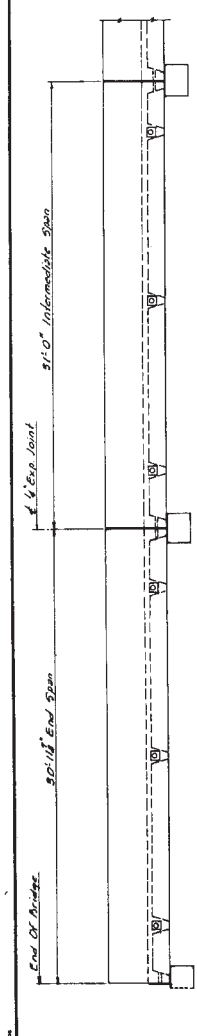
Specifications: A.A.S.H.T.O. 1993
 Design Load: HS20-44
 Concrete: 3,000 p.s.i. Grade 40
 Steel: 60,000 p.s.i. Grade 60
 Mesh: 3,000 p.s.i. Grade 40
 n = 9

MISSISSIPPI STATE HIGHWAY DEPARTMENT
 4' 6" PRECAST CONCRETE
 BRIDGE SLAB DETAILS
 19' 0" SPANS 24' 0" CLEAR ROADWAY
 PROJECT BR-013-1(10)
 85-0013-01-010-10

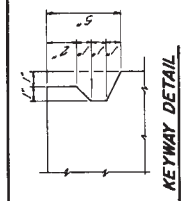
COPYRIGHT COUNTY SHEET NO. 5118
 DATE: 1/27/93
 DRAWN: B.S.B. CHECKED: J.L.S. APPROVED: J.H.C. DATE: 1/17/93

For Information Only

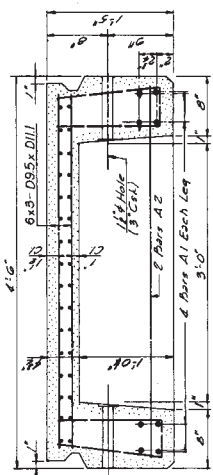
STATE PROJECT NO. MISS. BR-013-110



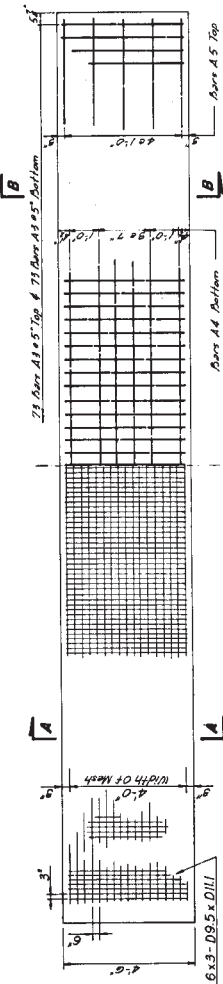
ELEVATION - CURB UNITS



KEYWAY DETAIL

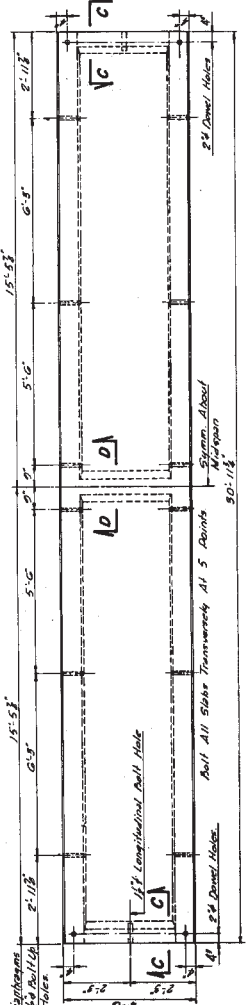


SECTION A-A
Mesh Sub-Reinforcing

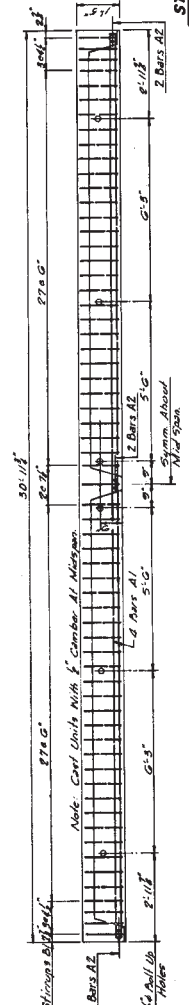


HALF PLAN OF SLAB
Welded Wire Mesh Reinforcement

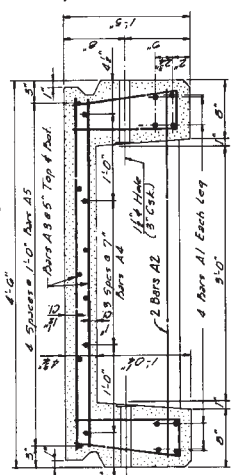
HALF PLAN OF SLAB
Determined Bar Reinforcement



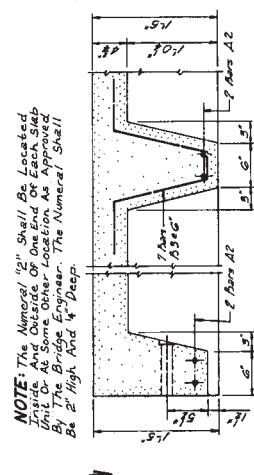
PLAN



LONGITUDINAL SECTION
Slab Reinforcing Not Shown



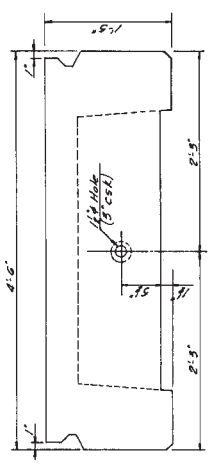
SECTION B-B
Deformed Bar Slab Reinforcing



SECTION C-C

SECTION D-D

NOTE: The Numerical 1/2\"/>



END ELEVATION - INTERIOR UNIT

GENERAL NOTES:

Specifications: Mississippi State Highway Department, 1980.
 Design Loading: AASHTO, HS20-44
 Concrete: All Concrete Shall Obtain A Minimum Compressive Strength of 5000 psi. At 28 Days.
 Chamfer Exposed Edges Unless Noted. Shall Be Deformed Bars Reinforcing To ASTM A-615. Primary Transverse Reinforcing Bars In Bottom Of Leaps, (Bars A1), And Transverse Bars In The Slab, (Bars A2), For Alternate Slab Reinforcing (SRA) Be Grade 60 Steel Will Not Be Used For Directly Reinforcing Slab Unit.
 Wire Mesh: The Mesh Shall Conform To The Specifications For ASTM A 977, Wire Mesh Reinforcing Steel.
 Mesh Will Not Be Aid For Directly, But Will Be Considered As Subsidiary To The Item Of The Mesh Reinforcing Steel. It Shall Be Governed By The Minimum Reinforcing And Will Not Be Aid For Directly, But Will Be Considered As Subsidiary To The Item Of The Precast Slab Unit.
 Grounding For Keyways Will Not Be Aid For Directly, But Will Be Considered As Subsidiary To The Item Of The Precast Slab Unit. More Than 1/2\"/>

Design Data: Specifications, AASHTO 1995.
 11-40000 psi, Grade 40
 11-40000 psi, Grade 60

STRAIGHT BARS		Mesh Sub-Reinforcing	
Max. Size	Length	Max. Size	Length
A1	30'-9"	B	6"
A2	4'-5"	C	6"
A3	4'-7"	XC	6"
A4	30'-9"	D	6"
A5	30'-9"	E	5"



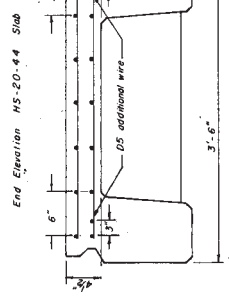
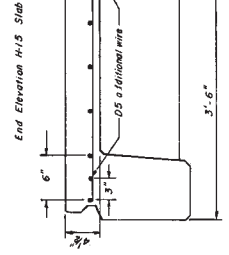
STIRRUPS B1-#3, BARS B3-#3, BARS B5-#3
BAR BENDING DETAILS
Dimensions Are Out To Out

MISSISSIPPI STATE HIGHWAY DEPARTMENT
 4-6" PRECAST CONCRETE BRIDGE SLAB DETAILS
 31'-0" SPANS - 24'-0" CLEAR ROADWAY
 PROJECT BR-013-110(10)
 WORKING NO. DBD7 of 8
 COUNTY COPIAH
 SHEET NO. 82
 DATE 11/18/88
 DRAWN BY: J.K.C. DATE: 11/18/88

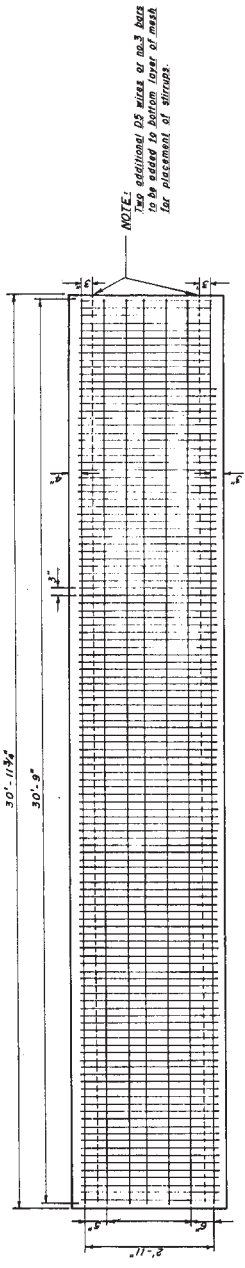
10835

For Information Only

STATE MISS. PROJECT NO. BR-013-1(10)

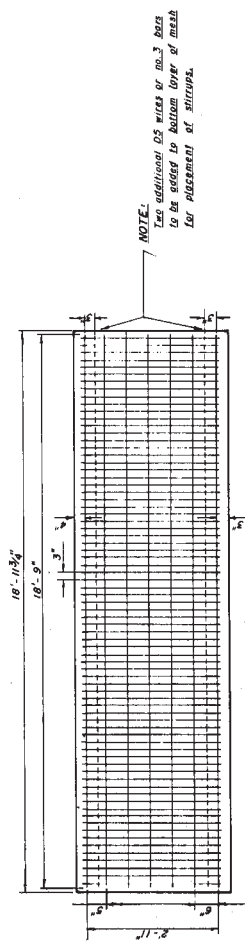


SPECIFICATIONS:
 Mississippi State Highway Department, 1990
 ASTM A 497
 Yield strength 70,000 psi
 Tensile strength 80,000 psi
 Reinforcing steel shall be deformed bars conforming to ASTM A615 grade 60



NOTE:
 Use additional 05 wires of no. 3 bars to be added to bottom layer of mesh for placement of through.

3'-6" x 31'-0" Precast Concrete Span
 DE.X.D7-6X3 Deformed Wire Mesh Reinforcement



NOTE:
 Use additional 05 wires of no. 3 bars to be added to bottom layer of mesh for placement of through.

3'-6" x 19'-0" Precast Concrete Span
 DE.X.D7-6X3 Deformed Wire Mesh Reinforcement

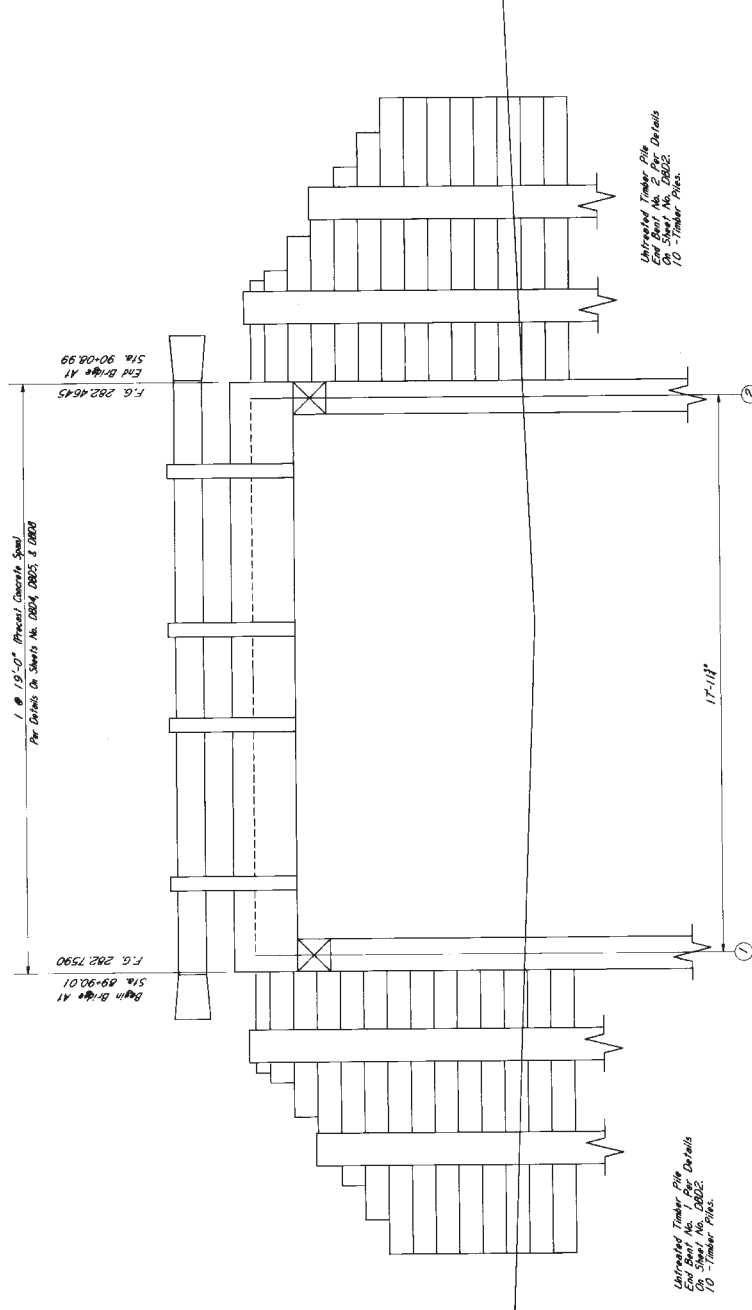
MISSISSIPPI STATE HIGHWAY DEPARTMENT	
ALTERNATE DEFORMED WIRE MESH	
19' and 31' PRECAST CONCRETE SPANS	
PROJECT BR-013-1(10)	
85-0013-01-010-10	
COPIAHS	COUNTY CO.
DESIGNED BY: JVC	WORKING NO. DBD8 of 8
CHECKED BY: JVC	DATE: 27 JULY 1987
DRAWN BY: JVC	SHEET NO. 013

10835

For Information Only

STATE MISS. PROJECT NO. BR-013-11(1)

Total Length of Bridge = 18'-11"
-1.55% Grade



GENERAL NOTES:

- Specifications, Mississippi Standard Specifications For Road And Bridge Construction 1990.
- Change Of Plans Will Be Permitted Except By Written Approval Of The Bridge Engineer. All Changes Must Be Authorized By The Bridge Engineer. Proposed Such Changes Will Not Be Cause For Contract Price Adjustment.
- Expansion Joint Material Shall Be Bituminous Filler Type Unless Otherwise Noted.
- Timber May Be Untreated And Rough And Shall Be New Material. This Includes All Components For Filing Size See Section 719 Of The Specifications.
- At All Times While The Detour Bridge Is In Place, The Waterway Shall Be Kept Clear Of Debris, Drift And Other Unnecessary Obstructions To The Flow Of The Water.
- Detour Bridge Piles Shall Be Piled Or Cut Off Below The Ground Line Elevation Of The Water Surface.
- For Construction And Removal Of Detour Bridges, No. S37-585C, Construction And Removal Of Detour Bridges, Bolts, Nuts And Washers In The Superstructure, Including Railing, Shall Be Galvanized. Other Hardware May Be Black Mild Steel Or wrought iron.
- Work For Which No Pay Item Is Provided In The Proposal Will Be Included In The Bridge And Payments For Bidding Items In These Plans Including But Not Limited To Precast Concrete Slab Units, Caps, Blocks, Posts, Guardrails And All Necessary Hardware Shall Be Furnished By The Contractor And Subject To Approval By The Project Engineer Prior To Erection.
- After Construction Of The Detour Bridge Is Completed, The New Bridge Shall Be Reopened By The Contractor. All Material In The Bridge Shall Become The Property Of The Contractor And Shall Be Removed From The Site.

SPECIAL PROVISIONS REQUIRED:
Construction & Removal Of Detour Bridges No. 907-58

DRAINAGE DATA:
Drainage Area ROADWAY DRAINAGE
Effective Area AC. SQ. FT.

DESIGN DATA:
Specifications AASHTO, 1992
Roadway Width 24' 0" Center To Center

SPECIAL NOTES:
The Contractor Shall Provide Piles Of Sufficient Length To Insure Stability Of Substructure.
The Abutments Of The Bridge In Addition To The Piles Shall Be Adjusted To Provide The Most Effective Location.
These Detour Bridge Plans Represent The Minimum Requirements For Construction. Minimum Requirements May Be Submitted By The Contractor And If Approved By The Engineer, The Contractor Shall Be Allowed To Use The Plans.

DETOUR BRIDGE AT STA. 89+90.01
Scale: 1" = 2'-0"

MINIMUM PILE BEARING CAPACITY	
BENT NO.	REQD. BEARING (TONS)
1&2	20

Notice to Bidders

MISSISSIPPI DEPARTMENT OF TRANSPORTATION DETOUR BRIDGE AT STA. 89+90.01	
PROJECT	BR-013-1(110)
COUNTY	COPIAH
WORKING NUMBER	85-0013-01-010-10-1
SHEET NUMBER	514
DESIGNED BY	ALW
DRAWN BY	TRW
CHECKED BY	GLD
DATE	11/2/88

10835

For Information Only

**DESCRIPTION OF SHEETS
SPECIAL DESIGN SHEETS - BRIDGE DRAWINGS**

**DESCRIPTION OF SHEETS
SPECIAL DESIGN SHEETS - DETOUR BRIDGE DRAWINGS**

DESCRIPTION OF SHEETS	WORKING NUMBER	SHEET NUMBER
BRIDGE AT STA. 108+56.01 - STATE HWY. NO. 28	A1 OF 13	486
BAYOU PIERRE RELIEF		
FOUNDATION PLAN		
BORING DATA	A2 OF 13	487
INT. BENT NO. 1 & 5 DETAILS	A3 OF 13	488
END BENT DETAILS	A4 OF 13	489
INT. BENT NO. 2, 3 & 4 DETAILS	A5 OF 13	470
40 FT. SPAN DETAILS	A6 OF 13	471
40 FT. SPAN DETAILS	A7 OF 13	472
MISC. SPAN DETAILS	A8 OF 13	473
RAILING DETAILS	A9 OF 13	474
BEAM 40-1 DETAILS - TYPE 1+2	A10 OF 13	475
LAMINATED ELASTOMERIC BEARING	A11 OF 13	476
PAV. DETAILS	A12 OF 13	477
BEAM 40-2 DETAILS - TYPE 1+2	A13 OF 13	478
BRIDGE AT STA. 108+02.21 - STATE HWY. NO. 28	B1 OF 15	479
BAYOU PIERRE		
FOUNDATION PLAN	B2 OF 15	480
BORING DATA	B3 OF 15	481
INT. BENT NO. 5 DETAILS	B4 OF 15	482
INT. BENT NO. 6 DETAILS	B5 OF 15	483
INT. BENT NO. 7 DETAILS	B6 OF 15	484
INT. BENT NO. 8 DETAILS	B7 OF 15	485
INT. BENT NO. 11 DETAILS	B8 OF 15	486
60 FT. SPAN DETAILS	B9 OF 15	487
60 FT. SPAN DETAILS	B10 OF 15	488
100 FT. SPAN DETAILS	B11 OF 15	489
100 FT. SPAN DETAILS	B12 OF 15	490
100 FT. SPAN DETAILS	B13 OF 15	491
BEAM 60-1 DETAILS - TYPE 1+2	B14 OF 15	492
BEAM 100-1 DETAILS - TYPE IV	B15 OF 15	493
BRIDGE AT STA. 115+02.21 - STATE HWY. NO. 28	C1 OF 3	494
BAYOU PIERRE RELIEF		
FOUNDATION PLAN	C2 OF 3	495
BORING DATA	C3 OF 3	496
BRIDGE AT STA. 198+02.21 - STATE HWY. NO. 28	D1 OF 9	497
JONES CREEK		
FOUNDATION PLAN	D2 OF 9	498
BORING DATA	D3 OF 9	499
INT. BENT NO. 3	D4 OF 9	500
INT. BENT NO. 4	D5 OF 9	501
80 FT. SPAN DETAILS	D6 OF 9	502
80 FT. SPAN DETAILS	D7 OF 9	503
80 FT. SPAN DETAILS	D8 OF 9	504
BEAM 80-1 DETAILS - TYPE III	D9 OF 9	505

DESCRIPTION OF SHEETS	WORKING NUMBER	SHEET NUMBER
DETOUR BRIDGE AT STA. 89+56.01	D001 OF 8	506
TIMBER ABUTMENT AND INT. BENT FOR TIMBER CAPS AND	D002 OF 8	507
19'-0" PRECAST CONCRETE SPANS 24'-0" CLEAR ROADWAY		
TIMBER ABUTMENT AND INT. BENT FOR TIMBER CAPS AND	D003 OF 8	508
31'-0" PRECAST CONCRETE SPANS 24'-0" CLEAR ROADWAY		
3'-6" PRECAST CONCRETE BRIDGE SLAB DETAILS -	D004 OF 8	509
19'-0" SPANS 24'-0" CLEAR ROADWAY		
4'-6" PRECAST CONCRETE BRIDGE SLAB DETAILS -	D005 OF 8	510
19'-0" SPANS 24'-0" CLEAR ROADWAY		
3'-6" PRECAST CONCRETE BRIDGE SLAB DETAILS -	D006 OF 8	511
31'-0" SPANS 24'-0" CLEAR ROADWAY		
4'-6" PRECAST CONCRETE BRIDGE SLAB DETAILS -	D007 OF 8	512
31'-0" SPANS 24'-0" CLEAR ROADWAY		
ALTERNATE DETORMED WIRE MESH -	D008 OF 8	513
19' AND 31' PRECAST CONCRETE SPANS		
DETOUR BRIDGE AT STA. 89+80.01	D0E1 OF 1	514

BRIDGE DIVISION	SHEET NO.	BY
10835	477, 478, 479	ALM
10836	472, 473, 475	LCB
10843	474, 486, 489, 492, 493	LCB
10844	476, 477, 482, 488	LCB
	494, 495, 496	LCB

BRIDGE DIVISION	SHEET NO.	BY
10835	477, 478, 479	ALM
10836	472, 473, 475	LCB
10843	474, 486, 489, 492, 493	LCB
10844	476, 477, 482, 488	LCB
	494, 495, 496	LCB

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

DETAILED INDEX

BR-013-1(10)

PROJECT 85-0013-01-010-10

COUNTY COPIAH

WORKING NUMBER 012

DATE 2-7-92

ISSUED 2-7-92

DESIGNED

CHECKED

For Information Only

PAY ITEM NO.	PAY ITEM	UNIT	QUANTITIES	
			PRELIMINARY	FINAL
803-A	Test Piles	Each	7	
803-B	Leading Test	Each	7	
803-G	HP12X53 Steel Piling	L.F.	11,895.0	
803-G	HP14X73 Steel Piling	L.F.	1365.0	
804-A	Bridge Concrete Class "A"	C.Y.	2014.14	△
804-C	40 Ft. Prest. Conc. Beam	L.F.	5194.50	
804-C	60 Ft. Prest. Conc. Beam	L.F.	717.00	
804-C	80 Ft. Prest. Conc. Beam	L.F.	478.50	
804-C	100 Ft. Prest. Conc. Beam	L.F.	498.75	
805-A	Reinforcement	Lb.	306,068	
813-A	Concrete Rolling	L.F.	2360.00	
815-A	Loose Riprap (300*)	Ton	1670.0	

10835
10836
10843
10844

DESIGNED BY	DATE	REVISION	DATE
CHECKED BY	DATE	REVISION	DATE
MISSISSIPPI DEPARTMENT OF TRANSPORTATION SUMMARY OF QUANTITIES (BRIDGE ITEMS) PROJECT BR-0013-1(10) COPIAH COUNTY			
WISCONSIN NUMBER	SHEET NUMBER		

For Information Only

BRIDGE	BEARING STATION	SPAN SIZE	OVERALL LENGTH	ITEM	Test Piles	Leaving Trestle	HP12X53 Steel Piling	HP14X73 Steel Piling	Class AA Bridge Concrete	40 Ft. Prest. Conc. Span	60 Ft. Prest. Conc. Span	80 Ft. Prest. Conc. Span	100 Ft. Prest. Conc. Span	Reinforce- ment	Concrete Casting	Losses (3.0%)		
																	Each	L.F.
10835	91+46.21	4 x 40'	181'-7"	Spone														
				End Bents														
				Intr. Bents	1	1000.0	427.5	186.27	943.50									
		Continuous For Live Load Only																
				Total	1	1850.0	547.5	283.75	943.50									
10836	102+02.21	16x40'-9"-50'-240'-0"	611'-7"	Spone														
				End Bents														
				Intr. Bents	3	1100.0	427.5	136.27	2295.50	717.00								
		Continuous For Live Load Only																
				Total	3	5660.0	1365.0	1084.75	2295.50	717.00								
10843	115+02.21	3 x 40'	121'-7"	Spone														
				End Bents														
				Intr. Bents	1	1100.0	427.5	136.27	708.50									
		Continuous For Live Load Only																
				Total	1	1760.0	219.45	708.50										
10844	102+02.21	6x40'-9"-50'-241'-7"	241'-7"	Spone														
				End Bents														
				Intr. Bents	2	800.0	427.5	282.17	947.50	478.50								
		Continuous For Live Load Only																
				Total	2	2615.0	418.19	947.50										
Project Totals																		
					7	11,885.0	1385.0	2014.14	5194.50	717.00	478.50	488.75	305,086	23860.00	1670.0			

10835
10836
10843
10844

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ESTIMATED QUANTITIES
(BRIDGE ITEMS)

PROJECT BR-0013-1(10)
85-0013-01-010-1B

COUNTY COPIAH

FORMING NUMBER
SHEET NUMBER

DATE
ISSUED
CHECKED
DATE

For Information Only

GENERAL NOTES:

Specifications: Mississippi Standard Specifications For Road And Bridge Construction, 1992 Edition. No Change Or Plans Will Be Permitted Except By Written Approval Of The Bridge Engineer. Minor Changes In Detail Or Design Or Construction Procedure May Be Authorized By The Bridge Engineer. Provisional Such Changes Will Not Be Cause For Contract Price Adjustment. Deck Shall Be The Final Surface Features Of The Bridge. 80% OF THE SPECIFICATIONS SEE MISSISSIPPI STANDARD SPECIFICATIONS FOR BRIDGES. SEE MISSISSIPPI STANDARD SPECIFICATIONS FOR BRIDGES FOR DETAILS OF THE BRIDGE. FINISH ON BRIDGE DECK: Bridge Concrete Shall Be Class "A". Expansion Joint Material Shall Be Bituminous Fiber Type Unless Otherwise Noted. No Payment Will Be Made For Excavation Incidental To The Structures (AQ 315-50). Stop Drawings Or Prefabricated Beams, including An Erection Pins, Shall Be Submitted In Duplicate To The Bridge Engineer At Least One Week Before Installation. Concrete Surfaces Shall Receive A Class 2, Rubbed Or Spray Finish In Accordance With The Specifications. Reinforcing Steel Shall Be ASTM A615, Grade 60, Unless Otherwise Noted. Work For Which No Pay Item Is Provided In The Proposal Will Not Be Paid For Unless A Pay Item Has Been Established In The Proposal And Payments For Bid Items.

PILE NOTES:

Test Piles Shall Be Driven As Permanent Piles At The Location And To The Tip Elevations Shown In The TEST PILE SCHEDULE ON THE SHEET UNLESS OTHERWISE DIRECTED BY THE BRIDGE ENGINEER. ALL PILES SHALL BE DRIVEN TO THE DESIGN LOAD IN THE TEST PILE SCHEDULE. TEST PILES SHALL BE TWO AND ONE-HALF (2 1/2) TIMES THE MINIMUM PILE BEARING CAPACITY. ELEVATION NO HIGHER THAN THE ELEVATION SHOWN IN THE MINIMUM PILE BEARING CAPACITY AND TIP ELEVATION SCHEDULE. WHEN TEST PILES ARE DRIVEN TO THE DESIGN LOAD, FULL LENGTH AND SHALL NOT BE SPILTED EXCEPT BY AUTHORITY OF THE BRIDGE ENGINEER. All Welding Shall Be Done By The Electric Arc Process. Welding Shall Be Done With Approved Electrodes And By Certified Welders.

SPECIAL PROVISIONS REQUIRED:

Site-cast Metal Forms 907-804
Painting Metal Structures 907-804

DRAINAGE DATA:

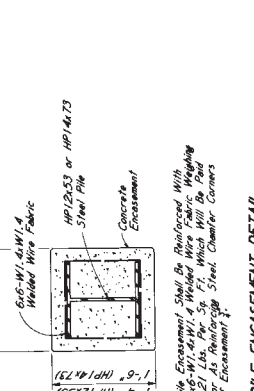
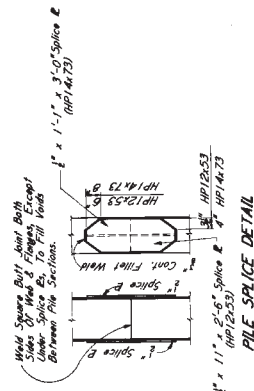
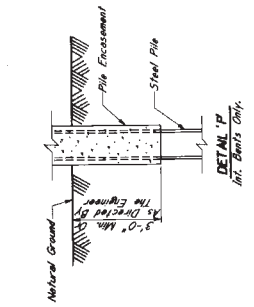
Drainage Area 137 Sq. Mi.
DSB (U.S.G.S.) 7830 c.f.s.
Effective Area 1537 Sq. Ft.

DESIGN DATA:

Specifications AASHTO 1992
Loading 40' OF Softer To Gutter
Concrete Class "A" (4000 P.S.I.)

DATE	BY	BY	REVISIONS	
			DESCRIPTION	DATE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
BRIDGE AT STA. 198+00.21
STATE HWY. NO. 28 ACROSS
JONES CREEK
PROJECT BR-013-1100
COPIAH COUNTY MISSISSIPPI
WORKING NUMBER: 108-44
DRAWING NO.: 108-44-12
SHEET NUMBER: 4



DETAIL TP
HP Beams Only.

PILE SPICE DETAIL
HP12x53 & HP14x73 Steel Piles

PILE ENCASUREMENT DETAIL
HP12x53 & HP14x73 Steel Piles

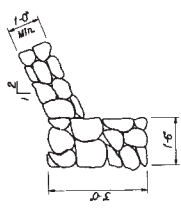
NOTE: Pile Encasement Shall Be Reinforced With 6"x6" W/1" S/W/1" & W/6" W/1" S/W/1" & W/6" W/1" S/W/1" For As Reinforcing Steel. Chamfer Corners Of Encasement.

Bent No.	Required Bearing Capacity (Tons)	Tip Elevation
1	34	234.0
2	34	233.0
3	34	233.0
4	34	234.0
5	34	233.0
6	34	233.0

NOTE: Permanent Stay-in-Place Weld Deck Forms Will Not Be Allowed For Construction Of Bridge Deck.

TEST PILE SCHEDULE		
Bent No.	Min. Lgth.-Ft.	Tip Elevation
4	55	224.29
	55	223.16

End Bent No. 1: HP12x53 Steel Piles x 40'-0"
HP Beams: No. 5: HP12x53 Steel Piles x 25'-0"
HP Beams: No. 4: HP12x53 Steel Piles x 25'-0"
HP Beams: No. 3: HP12x53 Steel Piles x 50'-0"
HP Beams: No. 2: HP12x53 Steel Piles x 50'-0"
HP Beams: No. 1: HP12x53 Steel Piles x 40'-0"

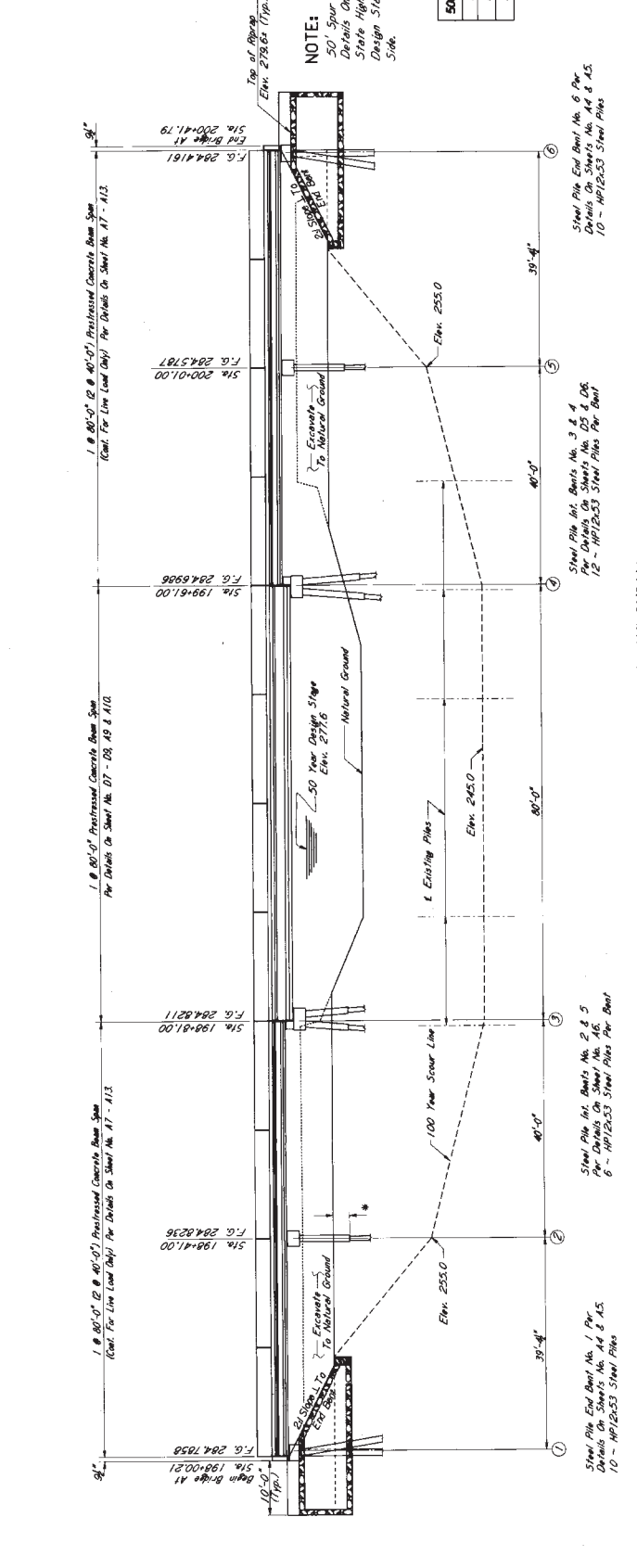
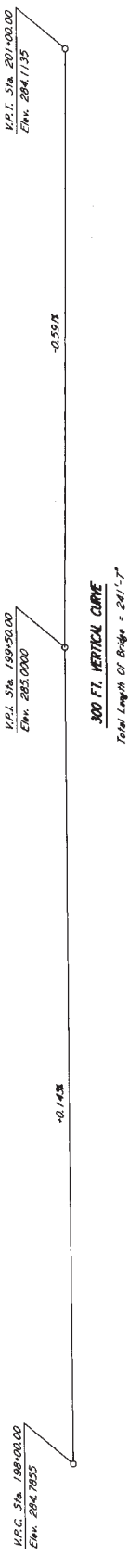


RIPRAP TOE DETAILS

ESTIMATED QUANTITIES							
Item	Test Piles	Loading Test	HP 12x53 Steel Piling	Class "A" Bridge Concrete	80 Ft. Prefab Concrete Type II		Ties (300P)
					Type I	Type II	
Location	Each	Each	LF.	LF.	LF.	LF.	LF.
HP Beams	2	2	890.0	41.7%	41.7%	41.7%	41.7%
W/ Beams	2	2	1615.0	91.4%	91.4%	91.4%	91.4%
TOTALS	2	2	2445.0	416.1%	997.00	979.50	980.00
							3000.0

For Information Only

STATE PROJECT NO.
MISS. BR-013-1(10)



NOTE:
50' Spur Dike Required At Bent No. 6 Per
Details On Sheet No. 205 Of The Mississippi
State Highway Department Roadway
Design Standard Drawings. On The Upstream
Side.

500 YEAR SCOUR ELEVATION	
Bent No.	Elevation
2, 5	254.0
3, 4	243.0

SPAN NOTE:
Span Deck Height Bridge Ends Per
Details on Sheet No. 80.

ELEVATION WITH PROFILE ALONG & APPROACH ROADWAY
Scale 1"=10'-0"

* See Detail "p" On Sheet No. D1.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
BRIDGE AT STA. 198+00.21
STATE HWY. NO. 28
JONES CREEK

PROJECT BR-013-1(10)
COPIAH COUNTY

ISSUING ENGINEER: D2 0219
DESIGNED BY: APW
CHECKED BY: JJA

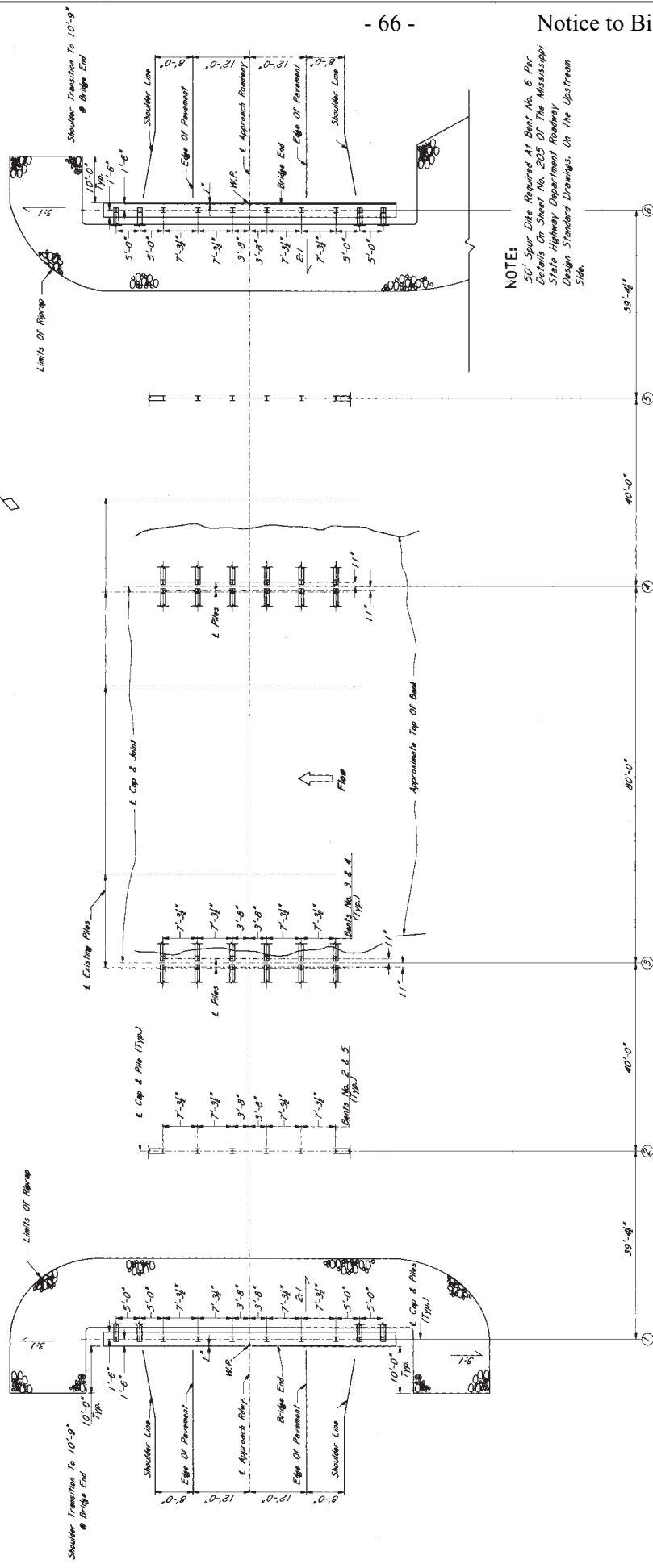
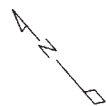
DATE: 7/27/83

For Information Only

STATE	PROJECT NO.
MISS.	BR-013-1100

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Notice to Bidders No. 2722 --



NOTE:
50' Spur Dike Required At Bent No. 6 Per
Details On Sheet No. 205 Of The Mississippi
State Highway Department Roadway
Design Standard Drawings. On The Upstream
Side.

FOUNDATION PLAN
SCALE: 1" = 10'-0"

NOTE: When Steel Piles Are In Conflict With
Existing Substructure The Contractor Shall Be
Responsible For Removal Of Existing Substructure.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION BRIDGE AT STA. 198+00.21	
FOUNDATION PLAN	
PROJECT BR-013-1100	
COUNTY COPIAH	
DESIGNED BY: AVM	DATE: 2-17-54
CHECKED BY: JUA	ISSUED: 2-17-54
WORKING NUMBER: D3 0163	SHEET NUMBER: 499

10844

For Information Only

LOG OF BORING NO. 82-15-2221-1
 79-0013-01-013-10
 THE ROTARY WASHINGTON Spoon & Shelby Tube LOCATION STA. 198+56.30 RT. E. HWY 28
 DATE 12-24-88

DEPTH SAMPLE	DESCRIPTION OF MATERIAL	CONSECUTIVE FEET				DEPTH TO WATER IN BORING NOT DETERMINED DATE 12-24-88
		1	2	3	4	
10	2' STIFF, GRAY SILTY CLAY (ALLUMINUM)					244.1'
15	4' LOOSE, BROWN TO GRAY, MEDIUM SAND					244.1'
20	5' MEDIUM TO COARSE SAND WITH FINE SILT					244.1'
25	3' STIFF, BROWN TO GRAY, SILTY CLAY WITH SAND					244.1'
30	5' MEDIUM TO COARSE SAND WITH SILT					244.1'
35	3' STIFF, GRAY SILTY CLAY WITH SAND					244.1'
40	5' MEDIUM TO COARSE SAND WITH SILT					244.1'
45	3' STIFF, GRAY SILTY CLAY WITH SAND					244.1'
50	5' MEDIUM TO COARSE SAND					244.1'
55	3' STIFF, GRAY SILTY CLAY WITH SAND					244.1'
60	5' MEDIUM TO COARSE SAND					244.1'
65	3' STIFF, GRAY SILTY CLAY WITH SAND					244.1'
70	5' MEDIUM TO COARSE SAND					244.1'
75	3' STIFF, GRAY SILTY CLAY WITH SAND					244.1'
80	5' MEDIUM TO COARSE SAND					244.1'
85	3' STIFF, GRAY SILTY CLAY WITH SAND					244.1'
90	5' MEDIUM TO COARSE SAND					244.1'
95	3' STIFF, GRAY SILTY CLAY WITH SAND					244.1'
100	5' MEDIUM TO COARSE SAND					244.1'

COMPLETION DEPTH 72.0' DATE 12-24-88
 S: Split Spoon T: Shelby Tube
 PLATE 96

LOG OF BORING NO. 82-15-2221-2
 79-0013-01-013-10
 THE ROTARY WASHINGTON Spoon & Shelby Tube LOCATION STA. 200+51.40 RT. E. HWY 28
 DATE 12-24-88

DEPTH SAMPLE	DESCRIPTION OF MATERIAL	CONSECUTIVE FEET				DEPTH TO WATER IN BORING NOT DETERMINED DATE 12-24-88
		1	2	3	4	
10	2' STIFF, GRAY SILTY CLAY (ALLUMINUM)					242.8'
15	4' LOOSE, BROWN TO GRAY, MEDIUM SAND					242.8'
20	5' MEDIUM TO COARSE SAND WITH FINE SILT					242.8'
25	3' STIFF, BROWN TO GRAY, SILTY CLAY WITH SAND					242.8'
30	5' MEDIUM TO COARSE SAND WITH SILT					242.8'
35	3' STIFF, GRAY SILTY CLAY WITH SAND					242.8'
40	5' MEDIUM TO COARSE SAND					242.8'
45	3' STIFF, GRAY SILTY CLAY WITH SAND					242.8'
50	5' MEDIUM TO COARSE SAND					242.8'
55	3' STIFF, GRAY SILTY CLAY WITH SAND					242.8'
60	5' MEDIUM TO COARSE SAND					242.8'
65	3' STIFF, GRAY SILTY CLAY WITH SAND					242.8'
70	5' MEDIUM TO COARSE SAND					242.8'
75	3' STIFF, GRAY SILTY CLAY WITH SAND					242.8'
80	5' MEDIUM TO COARSE SAND					242.8'
85	3' STIFF, GRAY SILTY CLAY WITH SAND					242.8'
90	5' MEDIUM TO COARSE SAND					242.8'
95	3' STIFF, GRAY SILTY CLAY WITH SAND					242.8'
100	5' MEDIUM TO COARSE SAND					242.8'

COMPLETION DEPTH 72.0' DATE 12-24-88
 S: Split Spoon T: Shelby Tube
 PLATE 97

LOG OF BORING
 Boring Data Shown is For Information Only And Its Accuracy
 For Construction Purposes is Not Guaranteed.

MISSISSIPPI STATE HIGHWAY DEPARTMENT
 BRIDGE AT STA. 198+00.21

BORING DATA
 PROJECT BR-013-1(10)
 85-0013-01-010-100

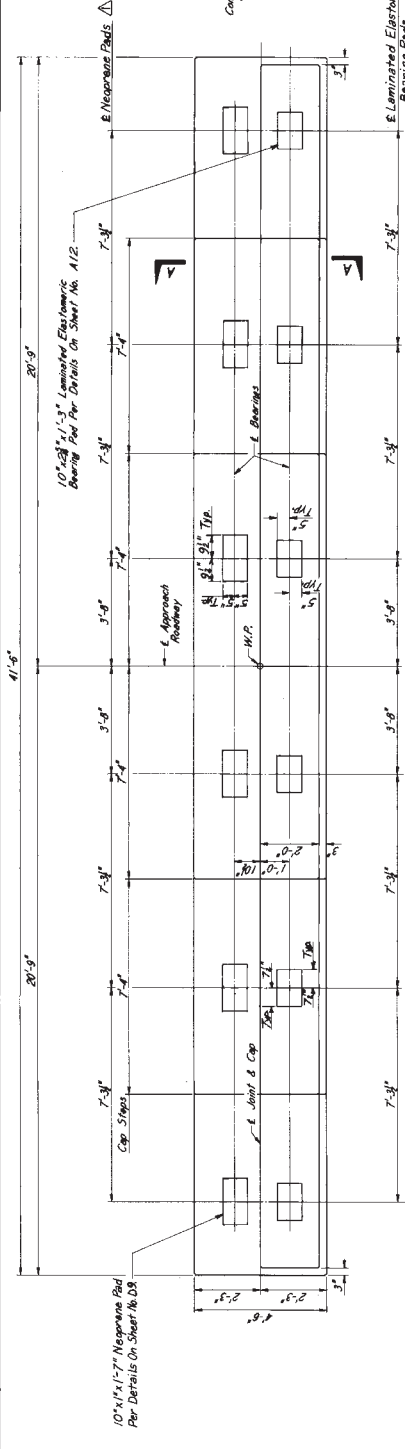
COPIAH COUNTY

WORKING SHEET NO. 04879
 SHEET NUMBER 000

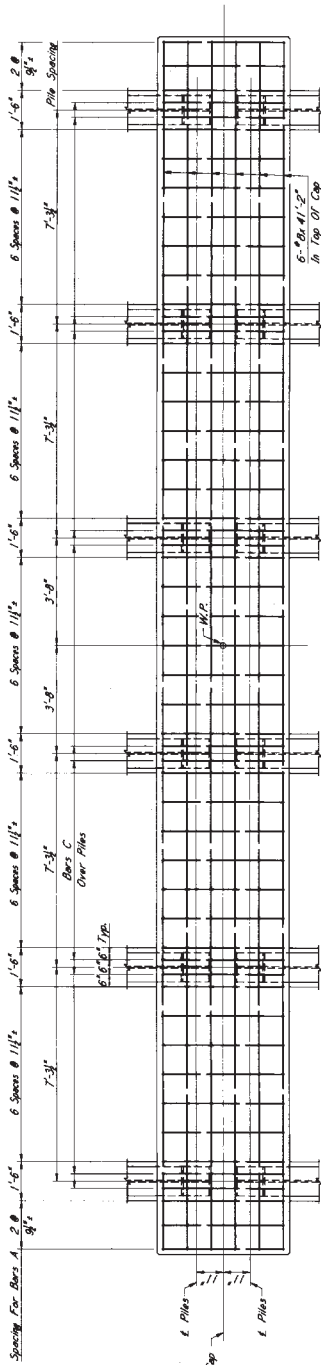
DESIGNED N.A. TRACED R.T.
 CHECKED M.A. DRAWN T.M.T. DATE 2-14-87

For Information Only

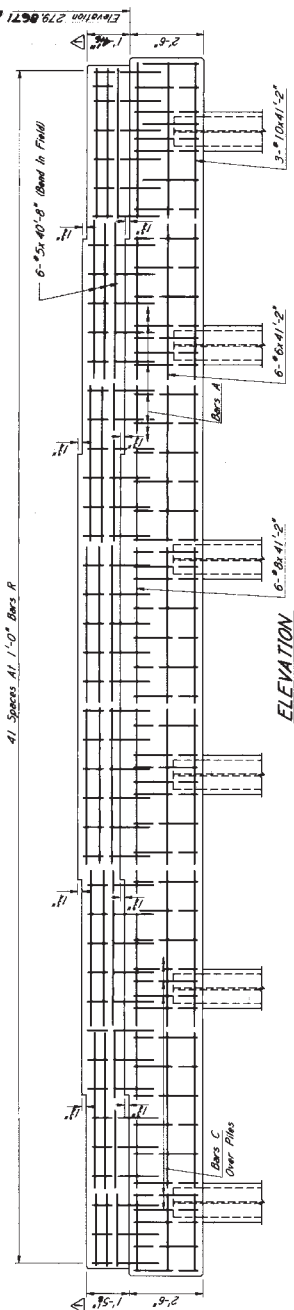
STATE MISS. PROJECT NO. BR-013-11(10)



PLAN OF BENT
Showing Laminated Elastomeric Bearing Pad & Concrete Dimensions

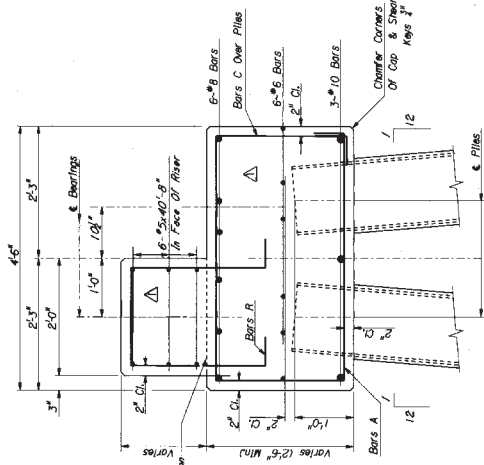


PLAN OF CAP
Showing Reinforcing in Top of Cap and Pile Spacing



ELEVATION

NOTE: All Bars to Be of The Size, Type and Drive To The Minimum Bearing Capacity As Shown On The Layout Sheet. Better Indicated Piles 1/2" Per Foot As Shown.



SECTION A-A

BAR BENDING DETAILS
Dimensions Are Gull To Gull.

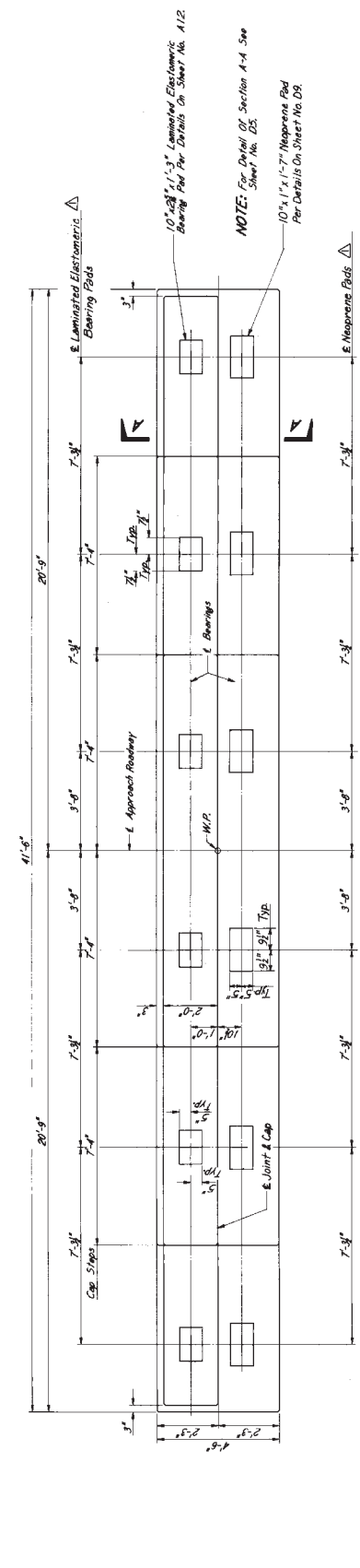
GENERAL NOTES:
All Concrete Shall Be Class "44"
Clearer All Edges 1" Unless Otherwise Noted
Piling Dimensions From Reinforcing Steel To Concrete Surfaces Are Clear Distances.

DESIGNED BY	AWW	TRAINED	CAJED
CHECKED BY	WJL	ISSUED	2208 7
DATE	10/24/10	PROJECT	BR-013-11(10)
NO. OF SHEETS	9	COUNTY	COPIAH
WORKSHEET NO.	9	BRIDGE AT STA.	198-00.21
SHEET NUMBER	9	INT. BENT NO.	3
PROJECT	BR-013-11(10)	MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
COUNTY	COPIAH	BRIDGE AT STA.	198-00.21

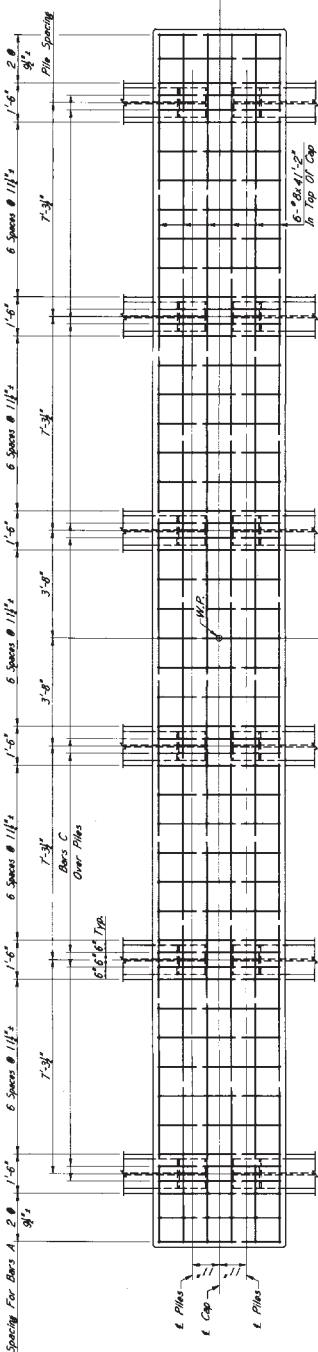
10844

For Information Only

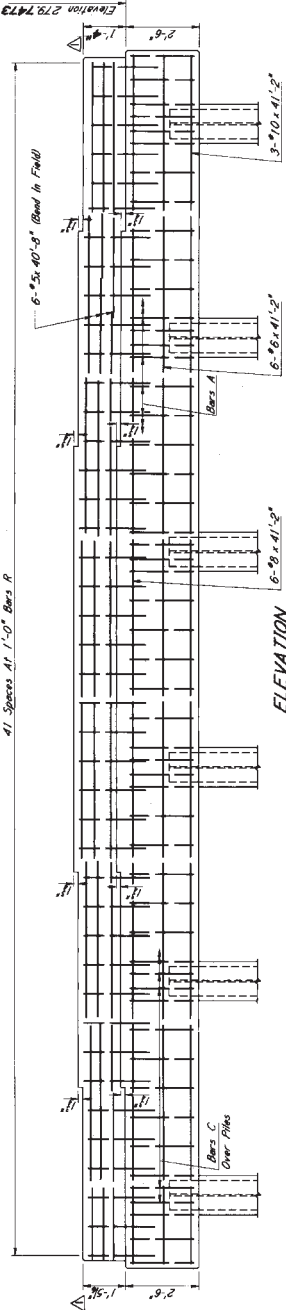
STATE MISS. PROJECT NO. BR-013-11(10)



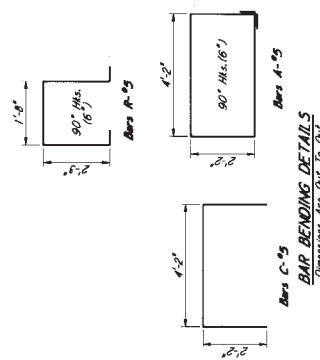
PLAN OF BENT
Showing Laminated Elastomeric Bearing Pads & Concrete Dimensions



PLAN OF CAP
Showing Reinforcing in Top Of Cap And Pile Spacing



ELEVATION



BAR BENDING DETAILS
Dimensions Are Out To Out.

NOTE: To The Minimum Bearing Capacity As Shown On The Foundation Better Indicated Piles On The Foot As Shown.

GENERAL NOTES:
All Casters Shall Be Class "A"
Chair All Edges 3" Unless Otherwise Noted
Pacing Dimensions From Reinforcing Steel To Concrete Surfaces Are Other Distances.

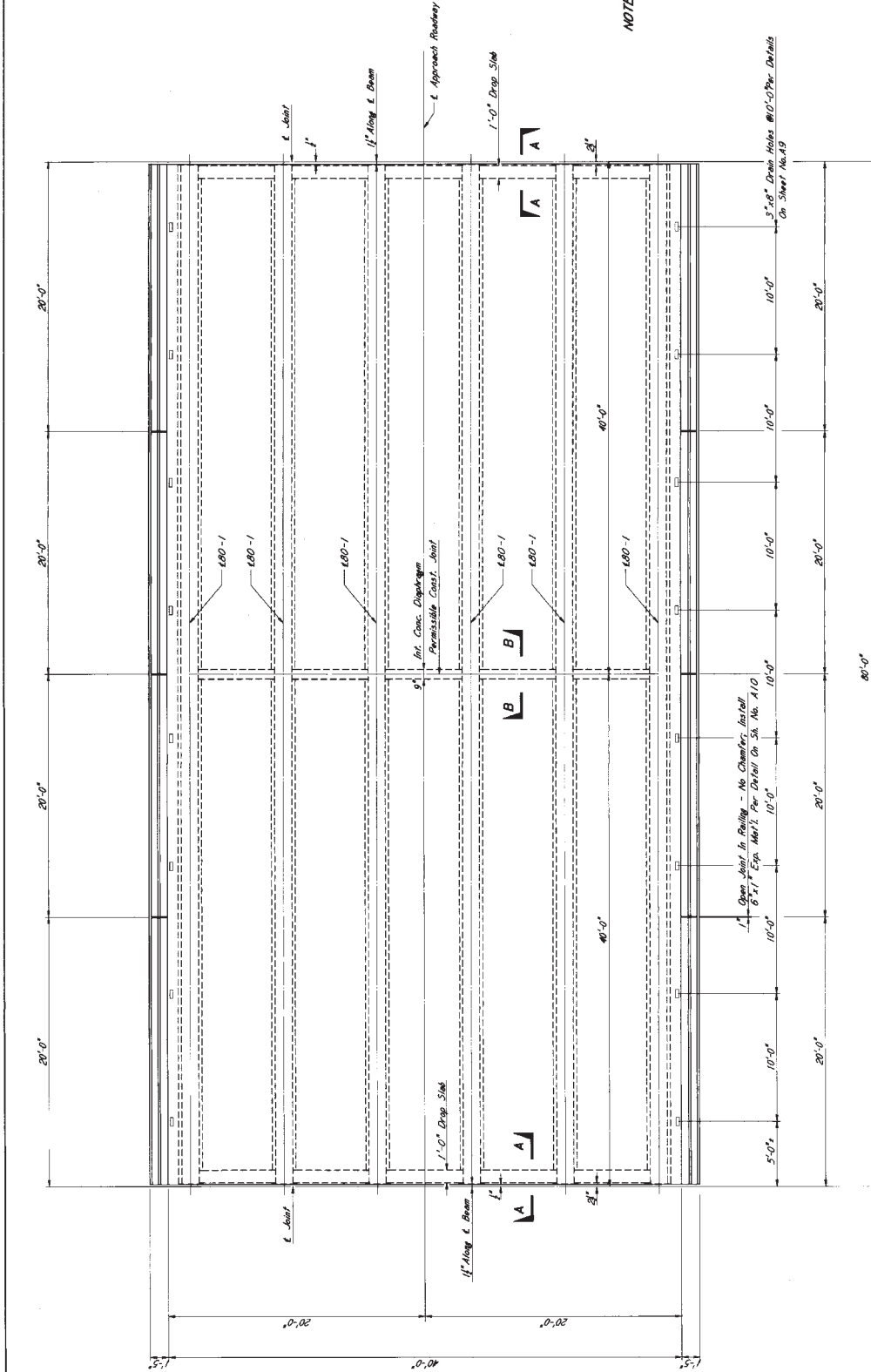
MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
BRIDGE AT STA. 198+00.2 IN	
INT. BENT NO. 4	
PROJECT BR-013-11(10)	
COPIAH COUNTY	
WORK NUMBER	85-0013-01-010-10
DRAWING NUMBER	602
DESIGNED BY	ATW
CHECKED BY	ATW
INVESTIGATED BY	ATW
DATE	2-7-52

10844

For Information Only

STATE	PROJECT NO.
MISS.	BR-013-1(10)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
BRIDGE AT STA. 198+00.21	
80 FT. SPAN DETAILS	
PROJECT	BR-013-1(10)
COUNTY	COPIAH
DESIGNED BY	AMW
CHECKED BY	AMW
DATE	11/13/85
ISSUED BY	ZWATZ
DATE	2/27/86
WORKING NUMBER	D8 009
SHEET NUMBER	504



NOTE: For GENERAL NOTES, See Sheets No. D1, A8, A10
Details See Sheets No. D1, A8, A10

NOTE: The Deck Pouring Schedule Shall Be As Shown
On These Plans And Any Alternate Sequence Will
Not Be Permitted.

PLAN OF 80 FT. INT. SPAN

Bar	Size	Spacing
D	#4	2000
R	#4	2000
DT	#4	2000
RT	#4	2000

SPRICE NOTE: #4 Bars Longitudinal in Slab May Be Lap Spliced 1'-4"

For Information Only

STATE	PROJECT NO.
MISS.	

- 73 -

Notice to Bidders No. 2722 - Cont'd

14545

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

NO
SUMMARY OF QUANTITY
NOR
RECAP SHEETS

AP	REVISIONS	DATE

DESIGNED	DRAWN	CHECKED	DATE
WORKING NUMBER	SHEET NUMBER		

For Information Only

STATE MISS. PROJECT NO. SP-0072-4(14)

14539-LT.LN. 14540-RT.LN.
14546

MISSISSIPPI STATE HIGHWAY DEPARTMENT	
SUMMARY OF QUANTITIES (BRIDGE ITEMS)	
PROJECT SP-0072-4(14) 96-0072-04-014-10	
WORKING NUMBER	14546
SHEET NUMBER	14546
DESIGNED	DATE
CHECKED	DATE
DRAWN	DATE
REVISIONS	DATE

PAY ITEM NO. PAY ITEM UNIT QUANTITIES
BRIDGE SUMMARY PRELIMINARY FINAL

803-A	16" x 16" Prest Conc. Test Pile	Each	4	
803-B	Loading Tests	Each	3	
803-F	14" x 14" Prest Conc. Piling	L.F.	6060.0	
803-F	16" x 16" Prest Conc. Piling	L.F.	5250.0	
803-M	20" ϕ Pre-formed Pile Holes	L.F.	900.0	
804-AA	Class AA Bridge Conc	C.Y.	1204.33	
804-T	56 FT. Prest Conc. Beam	L.F.	1865.00	
804-T	70 FT. Prest Conc. Beam	L.F.	2080.83	
805	Reinforcement	Lb.	24922	
815-A	Conc. Railing	L.F.	1500.00	
815-A	Loose Riprap	Ton	794.0	

For Information Only

BRIDGE	BEGIN STATION	SPANS - SIZE	OVERALL LENGTH	ITEM	Class "AA" Bridge Concrete C.Y.	Reinforce-ment Lb.	Concrete Railing L.F.	55 Ft. Prest. Concrete Beams L.F.	70 Ft. Prest. Concrete Beams L.F.	14'x 14" Prest Conc. Piling L.F.	16'x 16" Prest. Conc. Piling L.F.	16'x 16" Prest. Conc. Test Pile Each	Loading Test Each	20" Pre-formed Pile Holes L.F.	Loose Riprap (300 ^{cu}) Ton
A	Left Lane 14539-LT.LN.	165'-0" (5*55)	167'-5 3/4"	Spans End Bents Int. Bents	188.80 61.93 28.64	36529 8772 3766	330.00	977.50	2080.0	825.0	825.0	1	1		397.0
			BRIDGE "A" LT. LN. TOTALS		279.37	49,069	330.00	977.50	2080.0	825.0	825.0	1	1		397.0
A	Right Lane 14540-RT.LN.	165'-0" (5*55)*	167'-5 3/4"	Spans End Bents Int. Bents	188.80 61.34 28.64	36529 8772 3766	330.00	977.50	2080.0	825.0	825.0	1	1		397.0
			BRIDGE "A" RT. LN. TOTALS		278.78	49,069	330.00	977.50	2080.0	825.0	825.0	1	1		397.0
B	14546	295'-00.88 (2*210'-0" (5*70)-(5*70)*)		Spans End Bents Int. Bents	478.42 8796 111.80	98692 8796 9056	840.00		2080.83	1900.0	3600.0	2	1	900.0	
			BRIDGE "B" TOTALS		646.18	116,744	840.00		2080.83	1900.0	3600.0	2	1	900.0	
			Project Totals		1204.33	214,922	1500.00	1955.00	2080.83	6060.0	5250.0	4	3	900.0	794.0

*Continuous For Live Load Only
PROJECT TOTALS

*Continuous For Live Load Only

14539-LT.LN. 14540-RT.LN.
14546

MISSISSIPPI STATE HIGHWAY DEPARTMENT
ESTIMATED BRIDGE QUANTITIES

PROJECT SP-0072-4(14)
96-0072-04-014-10

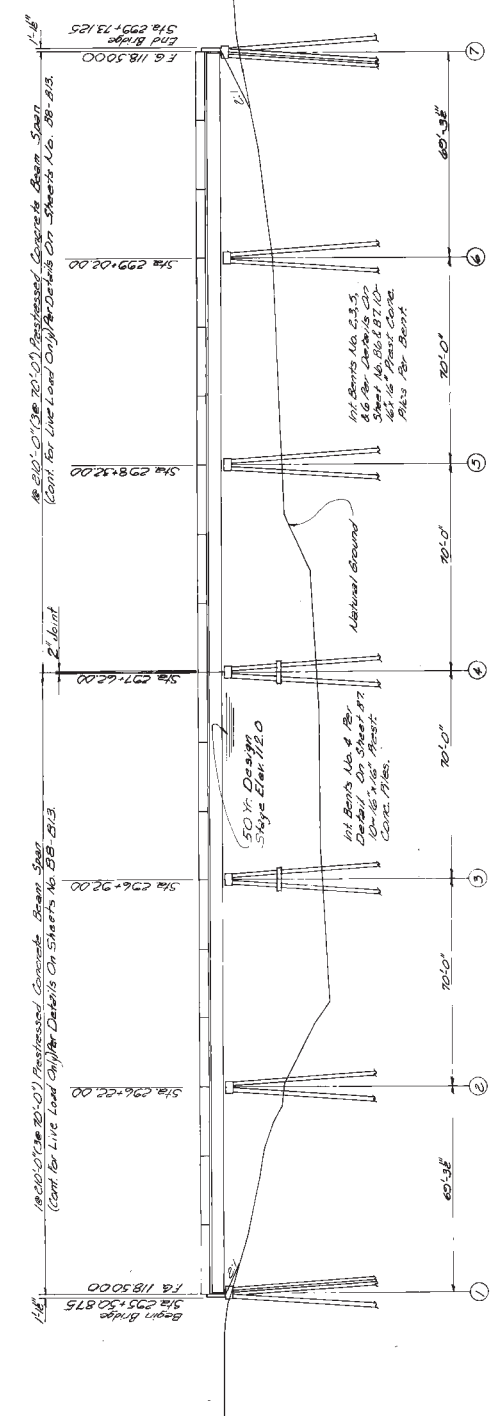
SUNFLOWER COUNTY

WORKING NUMBER
DESIGNED BY
CHECKED BY
DATE

SHEET NUMBER

For Information Only

Total Length of Bridge = 482'-3"
0.1% Grade



ELEVATION WITH PROFILE ON ROADWAY
Scale 1" = 20'-0"

Concrete Pile End Bents Per
Specifications Mississippi State Highway Department, 1976
Per End Bent

GENERAL NOTES:
Specifications Mississippi State Highway Department, 1976
No Change of Plans Will Be Accepted Except By Written Authority Of
The Bridge Engineer. Minor Changes In Details Of Design Or
Construction Shall Be Allowed In Writing By The Design Engineer.
Price Adjustments Are Not Authorized. Proceed For Contract
The Final Surface Profile Of The Bridge Deck Shall Be Attained By
Use Of The Drag Finish, Or Some Other Or Best Finish Method, Per
Test Pile 12.0. All Concrete Piles Shall Be Reinforced Type Per Sheet No. S14.
Test Piles Shall Be Driven As Permanent Bents At The Locations
And To The Tip Elevations Shown In The TEST PILE SCHEDULE
Unless Otherwise Directed By The Bridge Engineer. And Will Be
In The Error Load Test As Required, The Maximum Test Load Shall
Be 125 Tons.
Test Pile 12.0, And Recommended Pile Lengths Shall Be Submitted
Upon Joint Approval Of The Design Engineer And The Bridge Engineer.
Construction Of End Bents, Foundations Incidental To The
Construction Of End Bents, And Foundations Incidental To The
Standard Practice For Drilling Reinforced Concrete
Structures (A.C.I. 318-57)
All Work For Piles, 18' Spacing, As Provided In The Proposal Will
Not Be Held For Directly And Components Therefor Will Be
Shown On Drawings Of Reinforced Concrete Structures. The
Show Drawings Of Reinforced Concrete Structures, Including An Erection Plan,
Shall Be Submitted In Duplicate To The Bridge Engineer For
Approval Prior To The Manufacturing Of Bents.
Concrete Surfaces Shall Have The Smooth Finish Indicated On Sprayed
All Reinforcing Steel Shall Be A.S.T.M. A615 Grade 60, Unless
Otherwise Noted.

SPAN NOTE:
Steel Deck Joists At Bents
No. 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18
On Sheet No. S14

PREFORMED PILE HOLES:
All Piles In Bents No. 1 & 18 Shall Be
Driven In 20' to 25' Diameter Holes That Have
Been Pre-Drilled To A Depth Of 10'
20' At Bent No. 1, And 15' At Bent No. 18.
Prior To Drilling Piles In Reinforced Holes, At Least
One Hole At Each Bent Shall Be Filled
With A Cementitious Grout.
The Slurry Shall Be Prepared By Mixing At Least One
Hundred (100) Pounds Of Reinforced Commercial
Grade Type 1 Portland Cement With
Gallons Of Water And Small Gravel. No Lumps
Of Dry Cementite.
The Bed Item For Preformed Pile Holes Shall Include
The Cost Of Drilling, Grouting And Filling The
Holes. Study And All Work Incidental
Thereof.

MINIMUM PILE BEARING CAPACITY
34 Tons
End Bents 238.6
Int Bents 384

TEST PILE SCHEDULE

Bent No.	Min. Load - Pile Elevation
1	12.0
2	12.0
3	12.0
4	12.0
5	12.0
6	12.0
7	12.0
8	12.0
9	12.0
10	12.0
11	12.0
12	12.0
13	12.0
14	12.0
15	12.0
16	12.0
17	12.0
18	12.0

ESTIMATED QUANTITIES

Item	Class For Bridge Estimate	Reinforcement	Concrete	20' FT. Prest. Conc. Piling	18' x 18" Prest. Conc. Piling	18' x 18" Prest. Conc. Piling	20' x 20' Pre-Formed Pile Holes
Location	5.1	28892	840.00	2080.83	L.F.	L.F.	Each
End Bents	55.98	8796		1900.0	3600.0	2	1
Int. Bents	111.80	3056	840.00	2080.83	1900.0	2	1
Totals	645.18	116744	840.00	2080.83	1900.0	2	1

MISSISSIPPI STATE HIGHWAY DEPARTMENT
BRIDGE AT STA. 295 + 50.875 R172 N.
U.S. HWY. NO. 49W ACROSS
SUNFLOWER RIVER
PROJECT SP-0072-4(4)
SUNFLOWER COUNTY
SHEET NUMBER 483

SPECIAL PROVISIONS REQUIRED:
Concrete Bridges And Structures No. SOT-904
Aluminum Bent No. SOT-710
Reinforce Beams/Piles No. SOT-716
Drainage Area 23,500 S.F.
Erection Area 10,000 S.F.
Design Area 14,72 S.F.
DESIGN DATA:
A.A.S.H.T.O. 1988, Part 10, 1988
Loadings: HS 20-44
40'-0" Center To Center
Roadway Width

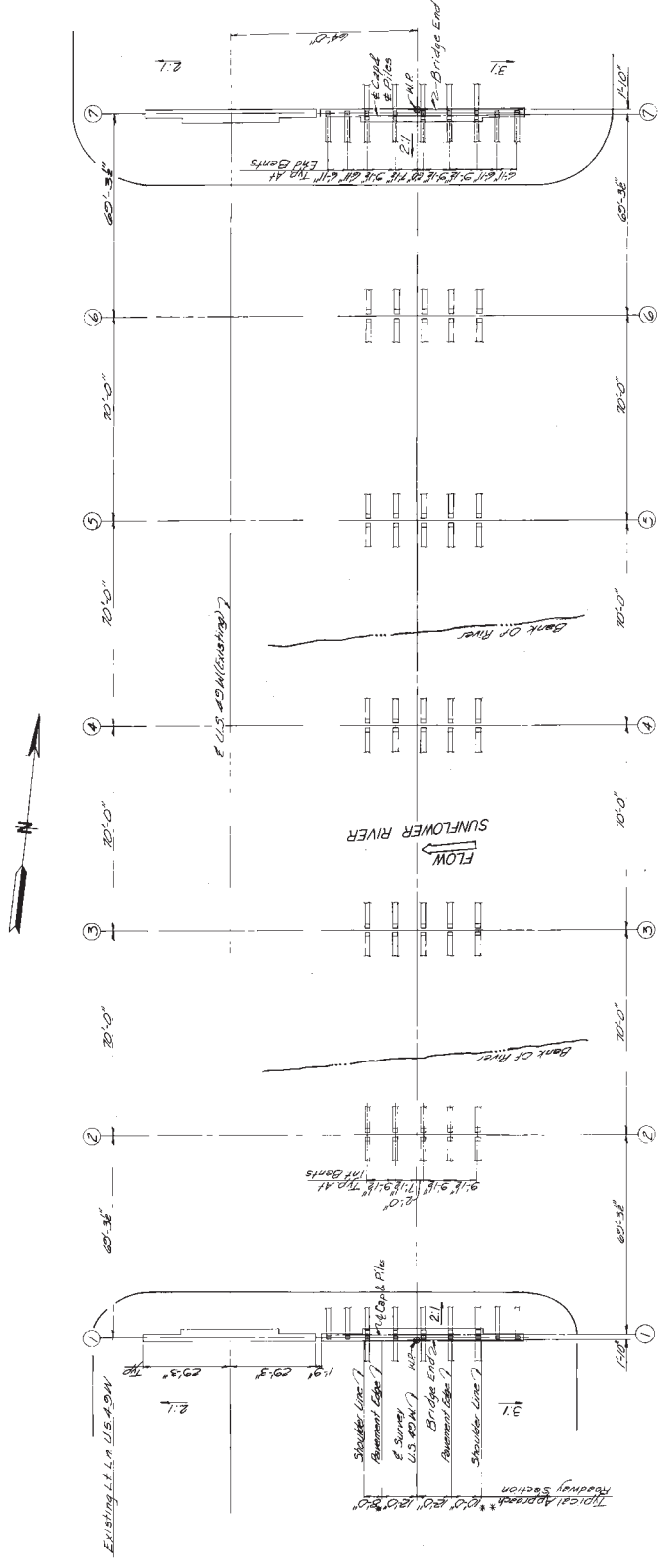
For Information Only

US49W
255.2B

STATE MISS. PROJECT NO. SP-0072-4(14)

14546

MISSISSIPPI STATE HIGHWAY DEPARTMENT BRIDGE AT STA. 295+50.875 RTZLN. FOUNDATION PLAN		WORKING NUMBER B2 of 14
PROJECT SP-0072-4(14) 96-0072-04-014-10		SHEET NUMBER 284
SUNFLOWER COUNTY		DATE 11/17
DESIGNED BY P.D.	DRAWN BY M.V.	CHECKED BY H.S.



FOUNDATION PLAN
Scale 1"=20'-0"

- * 8'-0" Shoulder Transition to 8'-0" at Bridge End
- * 10'-0" Shoulder Transition to 10'-0" at Bridge End

For Information Only

STATE MISS. PROJECT NO. SP-0072-4(14)

14546

MISSISSIPPI STATE HIGHWAY DEPARTMENT
 BRIDGE AT STA. 295+50.875 RT. L.N. 2722
 PROJECT 96-0072-4(14)
 96-0072-04-014-10
 SUNFLOWER COUNTY
 BORING LOG
 DESIGNED P.D.D. DATE: 4/12/96
 CHECKED: [Signature] DATE: 4/12/96
 SHEET NUMBER 185

BORING NOTE: Boring Data Shown is for Information Only and is Accurate for Construction Purposes is Not Guaranteed.

LOG OF BORINGS

LOG OF BORING NO. 75-67-013-2
 Location Station: 300+20, 10' L.S. Survey

DEPTH, FT.	TYPE	DESCRIPTION OF MATERIAL	WATER PERCENT	UNIT WEIGHT	ELEVATION, FT.
0		Surface Elev. 114.7'			
1		Brown, Silty Clay			104.7
10		Thin, Silty Sand			94.7
20		Thin, Silty Sand			84.7
30		Gray, Silty Sand with traces of Clay @ 30'			74.7
40			25		64.7
50			36		54.7
60			22		44.7
70			52		34.7
80			47		24.7
90			100		
100			100		

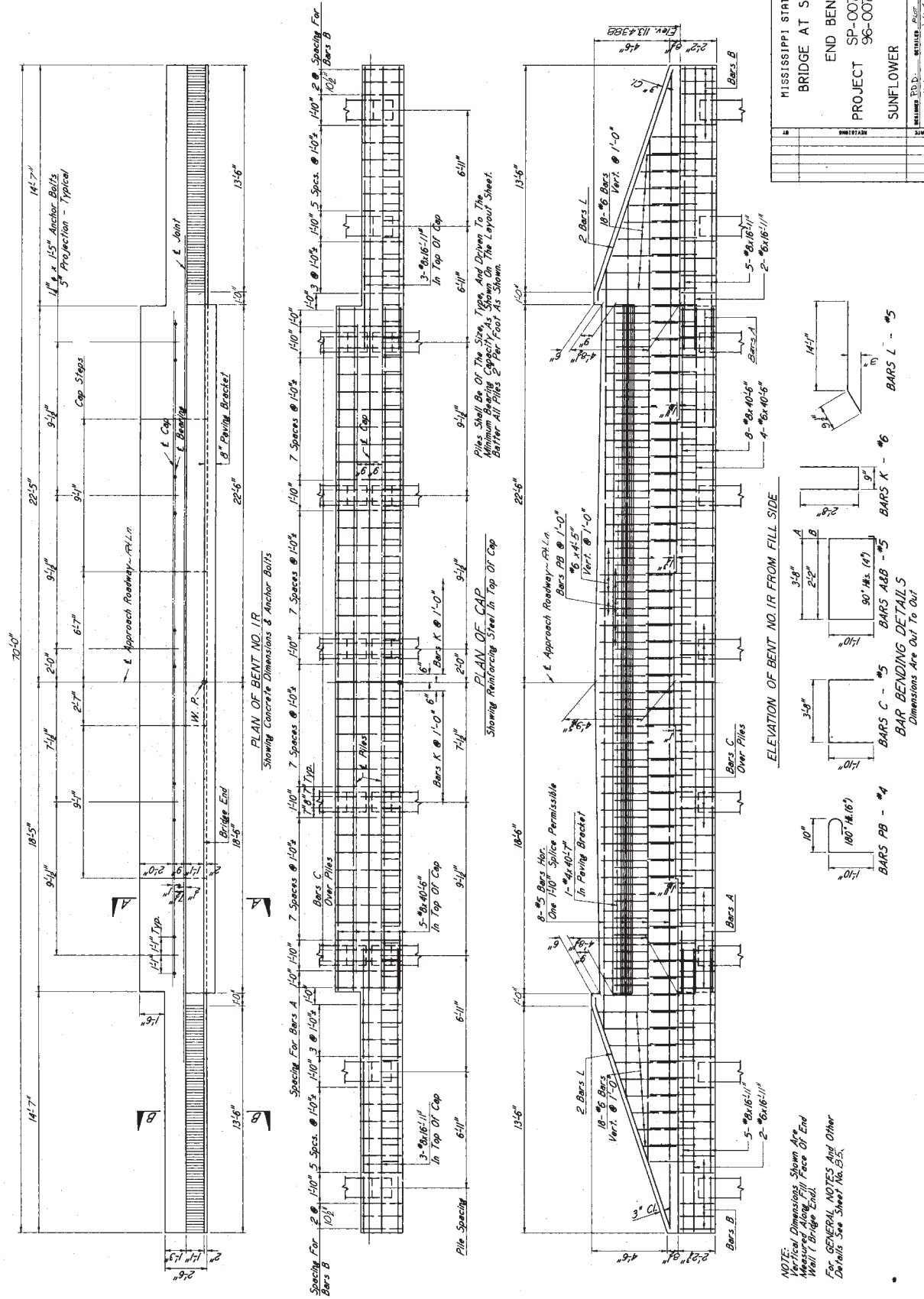
COMPLETION DEPTH: 91.3' DATE: 8-2-95
 DEPTH TO WATER IN BORING: NONE
 S. 504 Spm T. Shelby 704 PLATE 130

LOG OF BORING NO. 75-67-013-1
 Location Station: 295+45, 20' L.S. Survey

DEPTH, FT.	TYPE	DESCRIPTION OF MATERIAL	WATER PERCENT	UNIT WEIGHT	ELEVATION, FT.
0		Surface Elev. 105.24'			
10		Brown, Silty Clay			95.24
20					85.24
30		Gray, Silty Sand	7		75.24
40		Thin Layer of Lignite @ 40'	18		65.24
50			24		55.24
60			17		45.24
70			22		35.24
80			100		25.24
90			100		15.24
100			36		5.24

COMPLETION DEPTH: 91.3' DATE: 8-2-95
 DEPTH TO WATER IN BORING: NONE
 S. 504 Spm T. Shelby 704 PLATE 129

For Information Only



MISSISSIPPI STATE HIGHWAY DEPARTMENT	
BRIDGE AT STA. 295+50.875 AT LN	
END BENT DETAILS	
PROJECT SP-0072-4(4)	
SUNFLOWER COUNTY	
WORK NO NUMBER	B4 14
SHEET NUMBER	406
DESIGNED BY	W.S.
CHECKED BY	W.S.
DATE	4/17

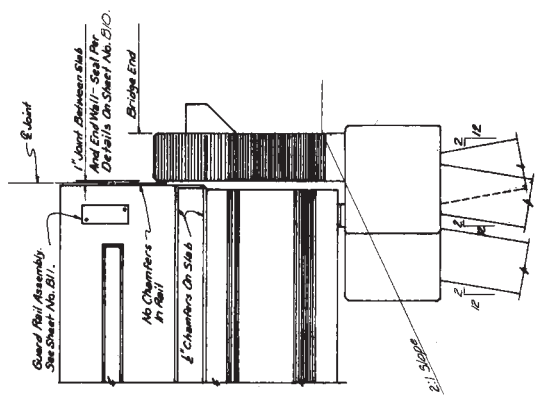
For Information Only

STATE	PROJECT NO.
MISS.	SP-0072-4(14)

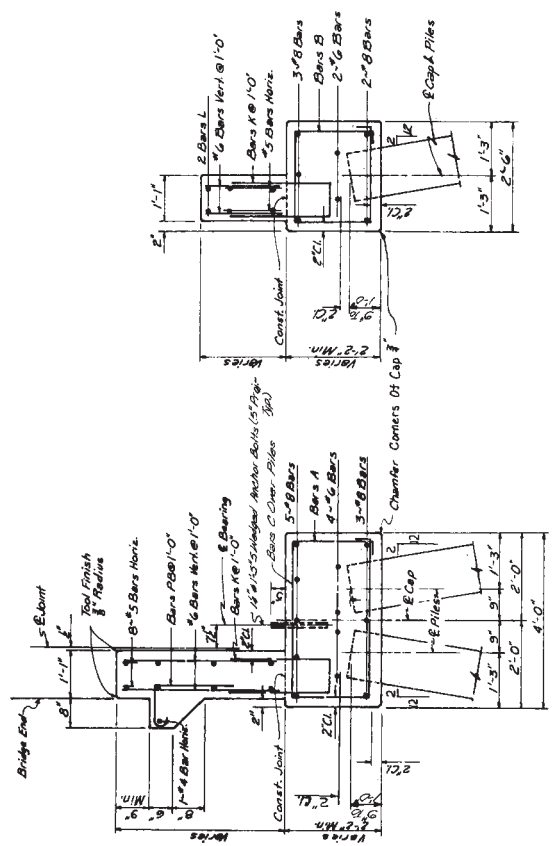
GENERAL NOTES:
 1. Concrete shall be Class 70.
 2. All reinforcement shall be Class 40.
 3. All reinforcement shall be constructed in accordance with the provisions of the Mississippi Highway Department Specifications for Highway Construction.
 4. All reinforcement shall be constructed in accordance with the provisions of the Mississippi Highway Department Specifications for Highway Construction.
 5. All reinforcement shall be constructed in accordance with the provisions of the Mississippi Highway Department Specifications for Highway Construction.
 6. All reinforcement shall be constructed in accordance with the provisions of the Mississippi Highway Department Specifications for Highway Construction.

14546

MISSISSIPPI STATE HIGHWAY DEPARTMENT	
BRIDGE AT STA. 295+50875 R.T. N.	
END BENT DETAILS	
SP-0072-4(14)	
PROJECT 96-0072-04-04-10	
SUNFLOWER COUNTY	COUNTY
MISSISSIPPI	STATE
BRIDGE NO. 114	SHEET NUMBER
487	



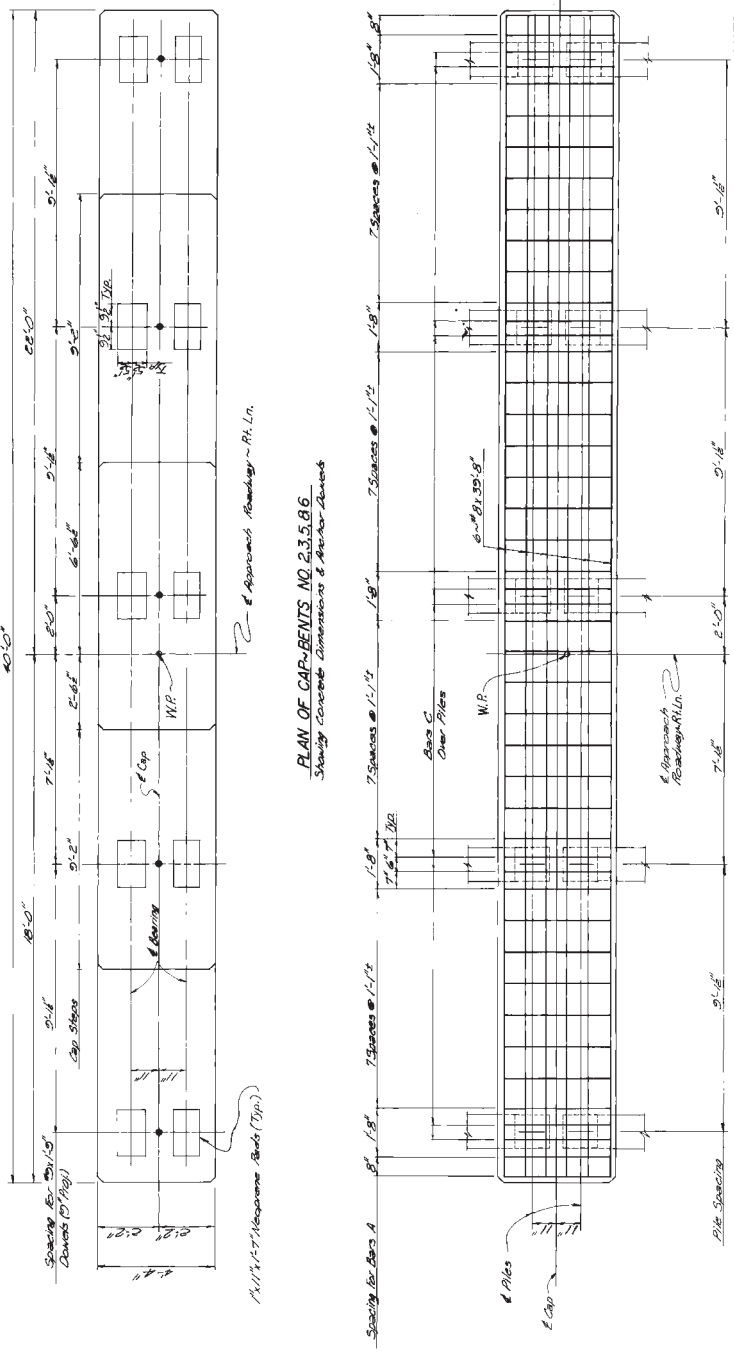
END ELEVATION



NOTE: For Reinforcing Details and Other Details See Sp-04.

For Information Only

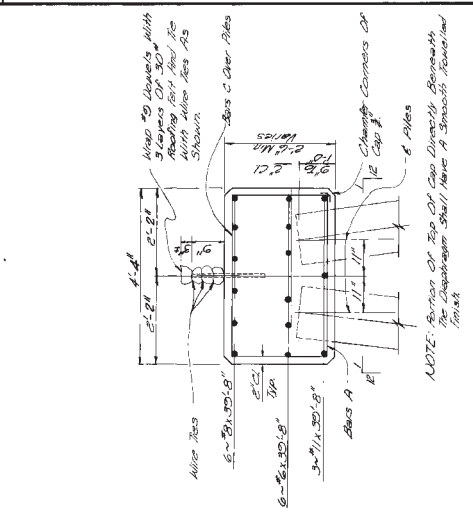
STATE PROJECT NO.
MIS. SF-0072-4(4)



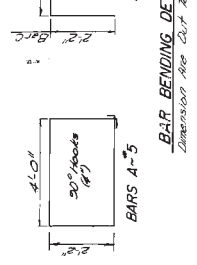
PLAN OF CAP-BENTS NO. 2, 3, 5, 8, 6
Showing Concrete Dimensions & Anchor Details

PLAN OF CAP-BENTS NO. 2, 3, 5, 8, 6
Showing Reinforcing in Top of Cap and Tie Spacing

NOTE: Callout Reinforced At Bent No. 3 For Details of Corner See Sheet No. 57



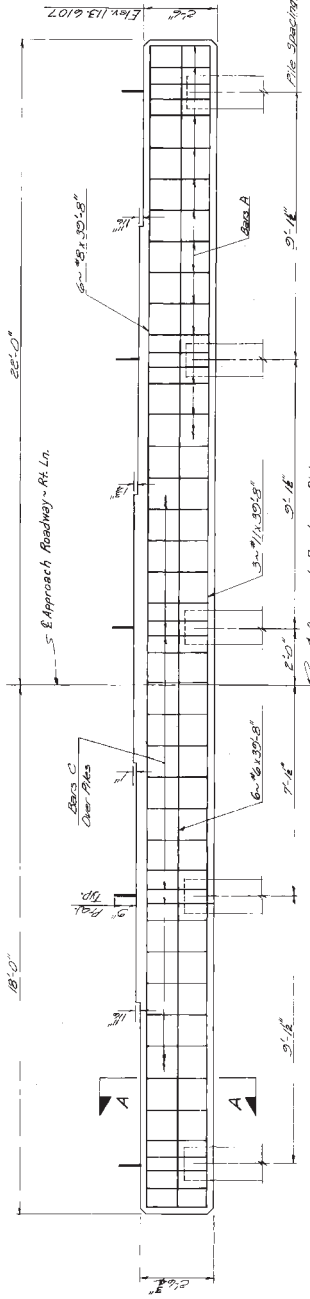
SECTION A-A



BAR BENDING DETAILS
Dimensions See Call To Detail

GENERAL NOTES:
All Concrete Shall Be Class 3500.
Chamber All Exposed Edges & Unless Otherwise Indicated
Reinforcing Dimensions From Reinforcing Steel.
Concrete Surfaces Are Clear Distances

NOTE: Callout Reinforced At Bent No. 3 For Details of Corner See Sheet No. 57



ELEVATION

NOTE: 5' Approach Roadway - Rt. Ln.

Notice to Bidder No. 7222

MISSISSIPPI STATE HIGHWAY DEPARTMENT
BRIDGE AT STA. 295 + 50.875 RT. LN.
INT. BENT NO. 2, 3, 5, 8, 6
PROJECT SP-0072-4(4)
96-0072-04-014-10

14566

NO.	REVISIONS

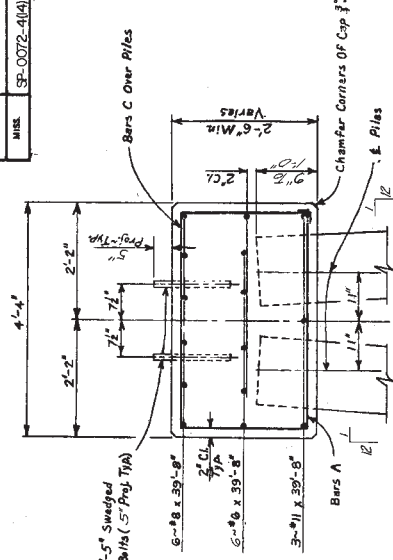
WORK NUMBER
66 of 14
SHEET NUMBER

SUNFLOWER COUNTY
DESIGNED BY: J.D. DUNN
CHECKED BY: J.S. DUNN
DATE: 11/7/12

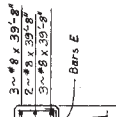
888

For Information Only

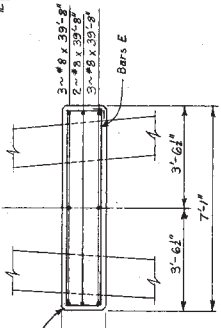
STATE	PROJECT NO.
MS	SP-0072-4(14)



SECTION A-A



SECTION B-B



BARS A - #5

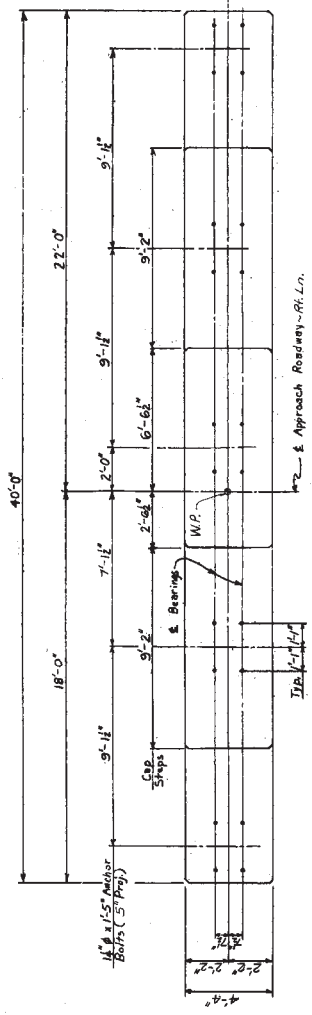
BARS C - #5

BARS E - #8

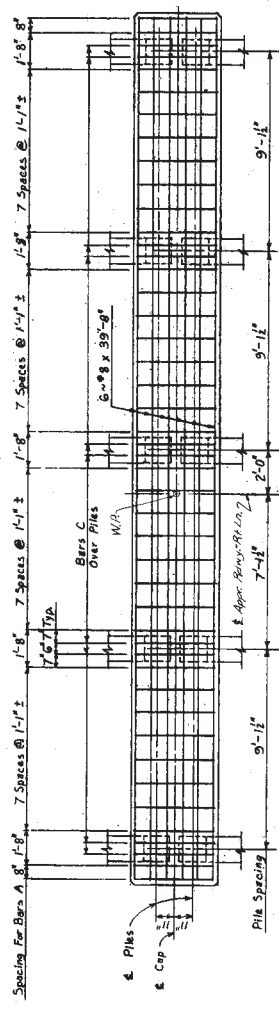
BAR BENDING DETAILS
Dimensions Are Out To Out

GENERAL NOTES:
All Concrete Shall Be Class "A"
Chamfer All Exposed Edges Unless Otherwise Noted.
Piling Dimensions From Reinforcing Steel to Concrete Surface Are Clear Distances.

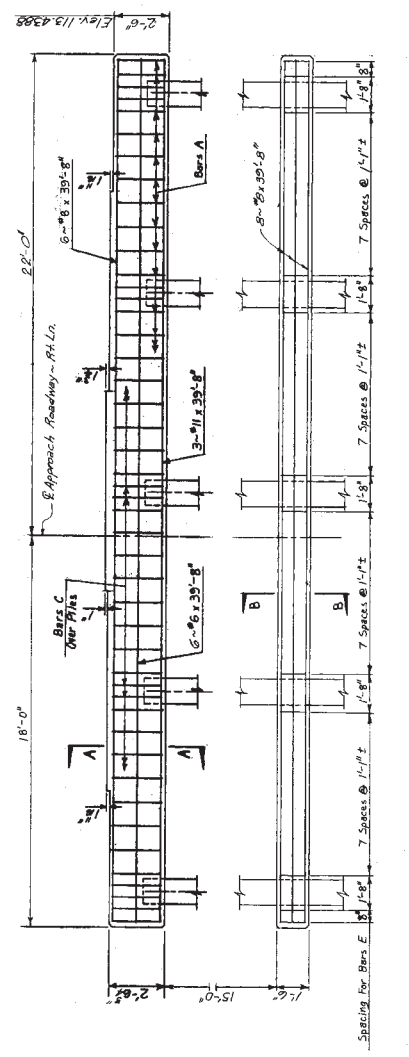
MISSISSIPPI STATE HIGHWAY DEPARTMENT	
BRIDGE AT STA. 295+50.875 RT. LN.	
INT. BENT NO. 4	
PROJECT SP-0072-4(14)	
SUNFLOWER COUNTY	
WORKING NUMBER	B7 of 14
SHEET NUMBER	989
DESIGNED BY	TRACED BY
CHECKED BY	DATE



PLAN OF CAP & BENT NO. 4
Showing Concrete Dimensions and Anchor Bolt Spacing



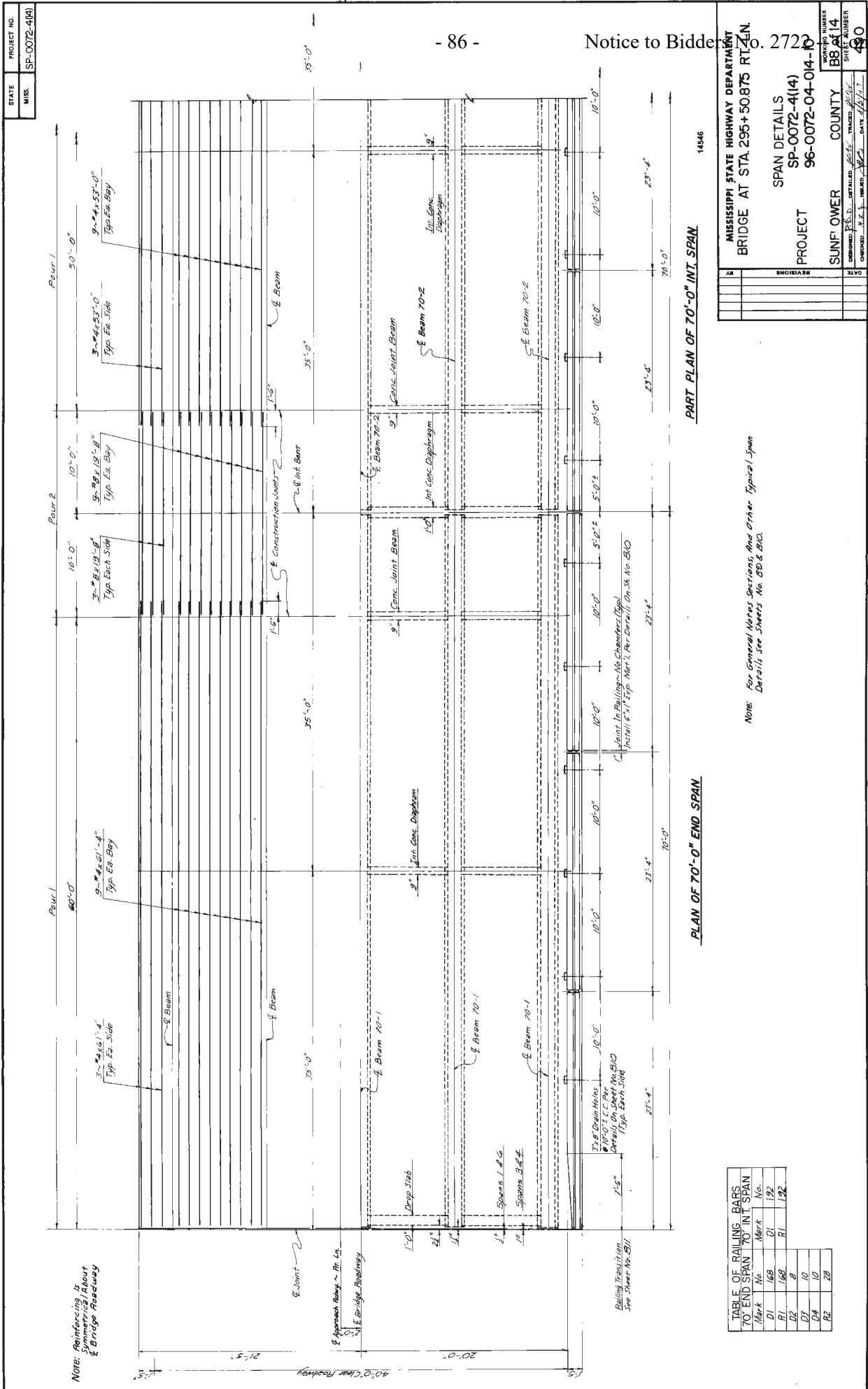
PLAN OF GAP & BENT NO. 4
Showing Reinforcing in Top of Cap and Pile Spacing



ELEVATION
Showing Color Detail

END ELEVATION

For Information Only



NOTE: Reinforcing is Symmetric About Bridge Roadway

TABLE OF RAILING BARS		
70' END SPAN 70' INT. SPAN		
Mark	No.	Mark
D1	168	D1
D2	168	D1
D3	8	
D4	10	
D5	20	

Note: For General Notes, Sections, and Other, Typical Span Details see Sheets No. 85 & 86.

PART PLAN OF 70'-0" INT. SPAN

PLAN OF 70'-0" END SPAN

14546

MISSISSIPPI STATE HIGHWAY DEPARTMENT
BRIDGE AT STA. 295+50.875 RT. 4 IN

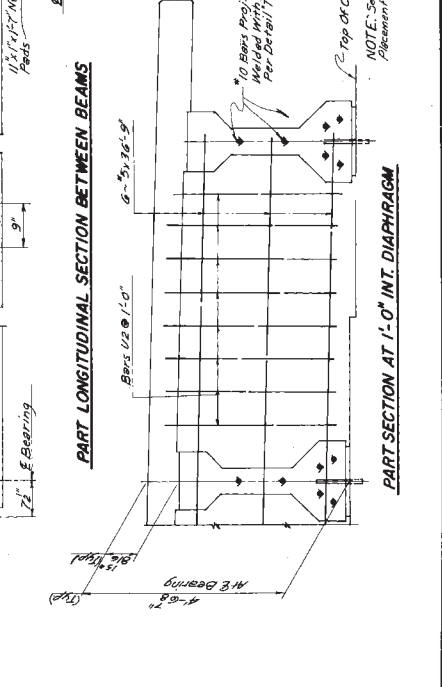
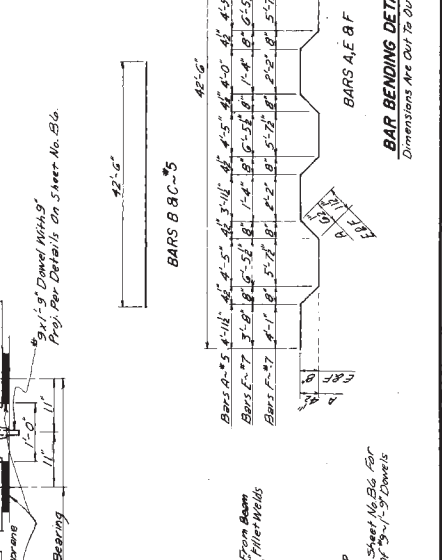
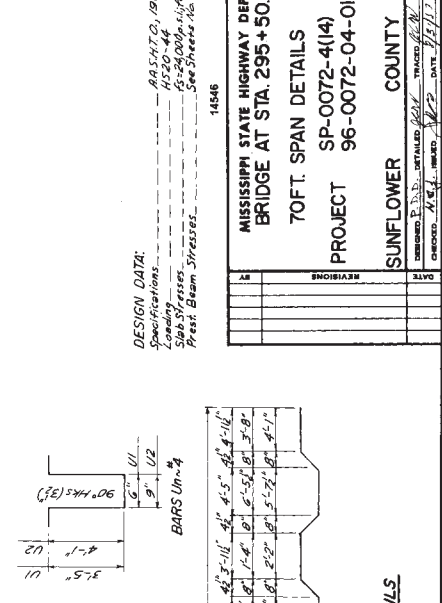
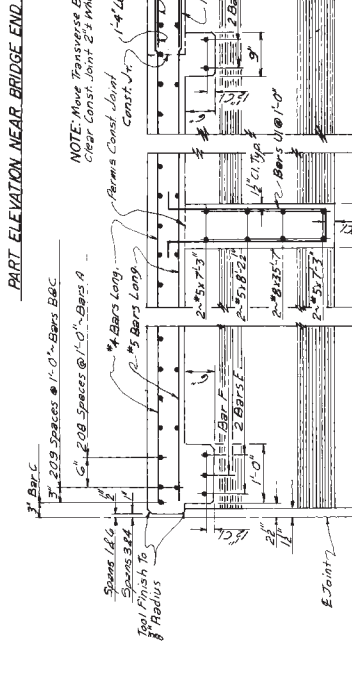
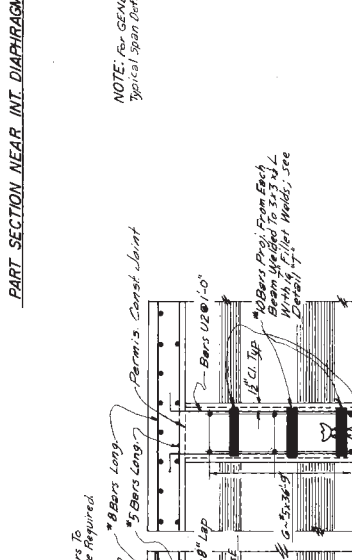
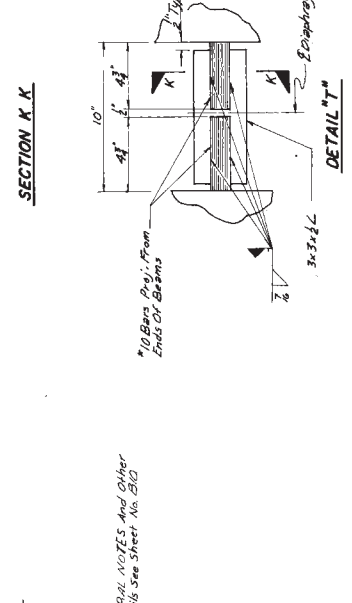
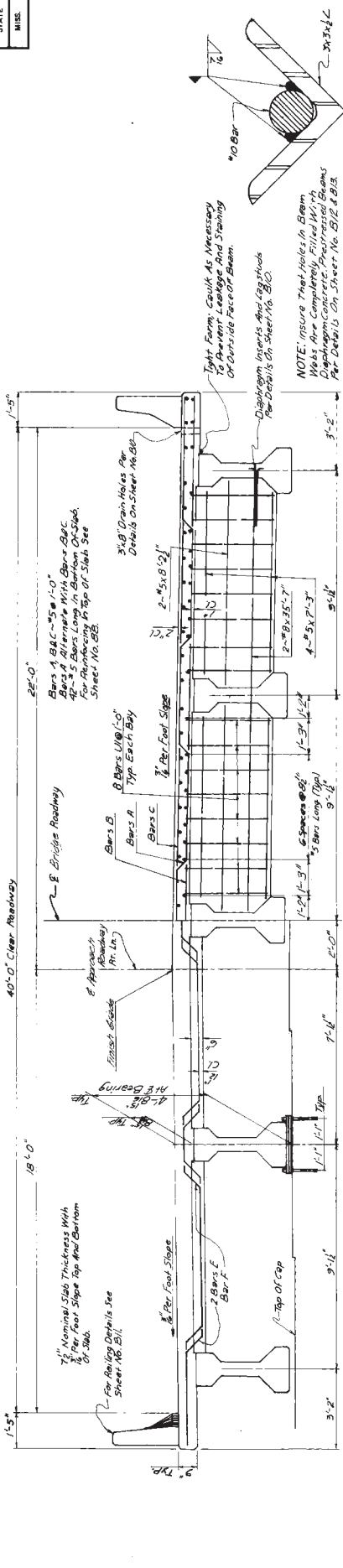
PROJECT SP-0072-4(4)
96-0072-04-04-10

SUNFLOWER COUNTY

WORKING NUMBER: 88 of 14
SHEET NUMBER: 80

DATE: 11/17

For Information Only



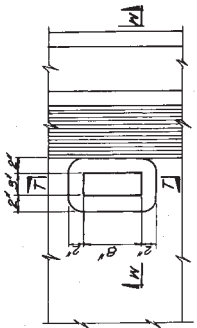
DESIGN DATA:

Specifications: AASHTO, 1988, Part 1004.2.1081
 MS-24.4
 MS-24.00B Life 120,000 psi
 MS-24.00C Life 120,000 psi
 See Sheets No. 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100, 101, 102, 103, 104, 105, 106, 107, 108, 109, 110, 111, 112, 113, 114, 115, 116, 117, 118, 119, 120, 121, 122, 123, 124, 125, 126, 127, 128, 129, 130, 131, 132, 133, 134, 135, 136, 137, 138, 139, 140, 141, 142, 143, 144, 145, 146, 147, 148, 149, 150, 151, 152, 153, 154, 155, 156, 157, 158, 159, 160, 161, 162, 163, 164, 165, 166, 167, 168, 169, 170, 171, 172, 173, 174, 175, 176, 177, 178, 179, 180, 181, 182, 183, 184, 185, 186, 187, 188, 189, 190, 191, 192, 193, 194, 195, 196, 197, 198, 199, 200, 201, 202, 203, 204, 205, 206, 207, 208, 209, 210, 211, 212, 213, 214, 215, 216, 217, 218, 219, 220, 221, 222, 223, 224, 225, 226, 227, 228, 229, 230, 231, 232, 233, 234, 235, 236, 237, 238, 239, 240, 241, 242, 243, 244, 245, 246, 247, 248, 249, 250, 251, 252, 253, 254, 255, 256, 257, 258, 259, 260, 261, 262, 263, 264, 265, 266, 267, 268, 269, 270, 271, 272, 273, 274, 275, 276, 277, 278, 279, 280, 281, 282, 283, 284, 285, 286, 287, 288, 289, 290, 291, 292, 293, 294, 295, 296, 297, 298, 299, 300, 301, 302, 303, 304, 305, 306, 307, 308, 309, 310, 311, 312, 313, 314, 315, 316, 317, 318, 319, 320, 321, 322, 323, 324, 325, 326, 327, 328, 329, 330, 331, 332, 333, 334, 335, 336, 337, 338, 339, 340, 341, 342, 343, 344, 345, 346, 347, 348, 349, 350, 351, 352, 353, 354, 355, 356, 357, 358, 359, 360, 361, 362, 363, 364, 365, 366, 367, 368, 369, 370, 371, 372, 373, 374, 375, 376, 377, 378, 379, 380, 381, 382, 383, 384, 385, 386, 387, 388, 389, 390, 391, 392, 393, 394, 395, 396, 397, 398, 399, 400, 401, 402, 403, 404, 405, 406, 407, 408, 409, 410, 411, 412, 413, 414, 415, 416, 417, 418, 419, 420, 421, 422, 423, 424, 425, 426, 427, 428, 429, 430, 431, 432, 433, 434, 435, 436, 437, 438, 439, 440, 441, 442, 443, 444, 445, 446, 447, 448, 449, 450, 451, 452, 453, 454, 455, 456, 457, 458, 459, 460, 461, 462, 463, 464, 465, 466, 467, 468, 469, 470, 471, 472, 473, 474, 475, 476, 477, 478, 479, 480, 481, 482, 483, 484, 485, 486, 487, 488, 489, 490, 491, 492, 493, 494, 495, 496, 497, 498, 499, 500, 501, 502, 503, 504, 505, 506, 507, 508, 509, 510, 511, 512, 513, 514, 515, 516, 517, 518, 519, 520, 521, 522, 523, 524, 525, 526, 527, 528, 529, 530, 531, 532, 533, 534, 535, 536, 537, 538, 539, 540, 541, 542, 543, 544, 545, 546, 547, 548, 549, 550, 551, 552, 553, 554, 555, 556, 557, 558, 559, 560, 561, 562, 563, 564, 565, 566, 567, 568, 569, 570, 571, 572, 573, 574, 575, 576, 577, 578, 579, 580, 581, 582, 583, 584, 585, 586, 587, 588, 589, 590, 591, 592, 593, 594, 595, 596, 597, 598, 599, 600, 601, 602, 603, 604, 605, 606, 607, 608, 609, 610, 611, 612, 613, 614, 615, 616, 617, 618, 619, 620, 621, 622, 623, 624, 625, 626, 627, 628, 629, 630, 631, 632, 633, 634, 635, 636, 637, 638, 639, 640, 641, 642, 643, 644, 645, 646, 647, 648, 649, 650, 651, 652, 653, 654, 655, 656, 657, 658, 659, 660, 661, 662, 663, 664, 665, 666, 667, 668, 669, 670, 671, 672, 673, 674, 675, 676, 677, 678, 679, 680, 681, 682, 683, 684, 685, 686, 687, 688, 689, 690, 691, 692, 693, 694, 695, 696, 697, 698, 699, 700, 701, 702, 703, 704, 705, 706, 707, 708, 709, 710, 711, 712, 713, 714, 715, 716, 717, 718, 719, 720, 721, 722, 723, 724, 725, 726, 727, 728, 729, 730, 731, 732, 733, 734, 735, 736, 737, 738, 739, 740, 741, 742, 743, 744, 745, 746, 747, 748, 749, 750, 751, 752, 753, 754, 755, 756, 757, 758, 759, 760, 761, 762, 763, 764, 765, 766, 767, 768, 769, 770, 771, 772, 773, 774, 775, 776, 777, 778, 779, 780, 781, 782, 783, 784, 785, 786, 787, 788, 789, 790, 791, 792, 793, 794, 795, 796, 797, 798, 799, 800, 801, 802, 803, 804, 805, 806, 807, 808, 809, 810, 811, 812, 813, 814, 815, 816, 817, 818, 819, 820, 821, 822, 823, 824, 825, 826, 827, 828, 829, 830, 831, 832, 833, 834, 835, 836, 837, 838, 839, 840, 841, 842, 843, 844, 845, 846, 847, 848, 849, 850, 851, 852, 853, 854, 855, 856, 857, 858, 859, 860, 861, 862, 863, 864, 865, 866, 867, 868, 869, 870, 871, 872, 873, 874, 875, 876, 877, 878, 879, 880, 881, 882, 883, 884, 885, 886, 887, 888, 889, 890, 891, 892, 893, 894, 895, 896, 897, 898, 899, 900, 901, 902, 903, 904, 905, 906, 907, 908, 909, 910, 911, 912, 913, 914, 915, 916, 917, 918, 919, 920, 921, 922, 923, 924, 925, 926, 927, 928, 929, 930, 931, 932, 933, 934, 935, 936, 937, 938, 939, 940, 941, 942, 943, 944, 945, 946, 947, 948, 949, 950, 951, 952, 953, 954, 955, 956, 957, 958, 959, 960, 961, 962, 963, 964, 965, 966, 967, 968, 969, 970, 971, 972, 973, 974, 975, 976, 977, 978, 979, 980, 981, 982, 983, 984, 985, 986, 987, 988, 989, 990, 991, 992, 993, 994, 995, 996, 997, 998, 999, 1000.

For Information Only

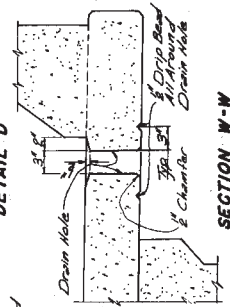
MISSISSIPPI STATE HIGHWAY DEPARTMENT
BRIDGE AT STA. 295+50875 RT. 11
MISC. SPAN DETAILS
PROJECT SP-0072-4(14)
SUNFLOWER COUNTY
DATE
BY
CHECKED
APPROVED
DATE
NO.
REV.
BY
DATE
NO. OF 14
692

GENERAL NOTES:
 All Concrete In Span And Railing Shall Be Class "A"
 Chamber All Edges $\frac{1}{4}$ " Unless Otherwise Noted.
 See Layout Sheet For Finishing Of Concrete Surfaces
 Placing Joint Positions For Reinforcing Steel To Concrete Surfaces
 To Determine The Dimension From Finish Grade To Top Of
 Accompanys At The Time Original Contract Was Made
 Detail Sheets. The Bridge Engineer Shall Be Notified If
 The Camber Is Not Within These Limits.
 NOTE: "r-1" For $\frac{1}{4}$ " Thick Pads And "r-2" For 1" Thick Pads.



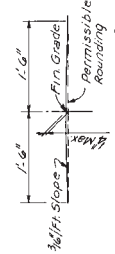
SECTION T-T

NOTE: Drain Hole Shall Be Located
 5/8" From Edge Of Slab To Be Cut
 If Necessary One Bar A May Be
 Cut In Field

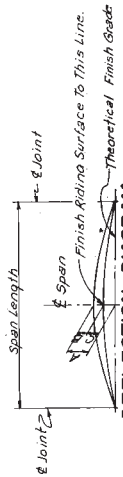


SECTION W-W

DRAIN HOLE DETAILS
 Use Where Shown On The Span Detail Sheet

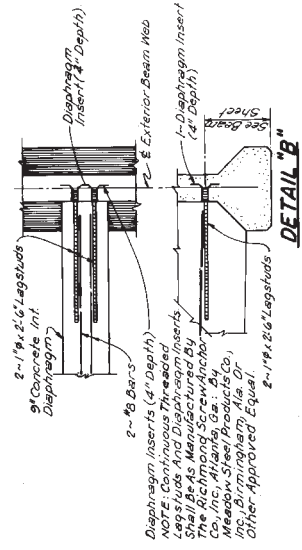


CROWN DETAILS



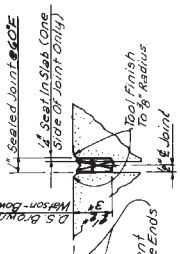
DEFLECTION DIAGRAM

A = Total Recommended Allowance For Deflection
 B = Estimated Deflection Due To Dead Load Of Slab & Rail
 C = Includes An Allowance For Creep
 NOTE: For Values Of A, B & C, See Beam Detail Sheet's.

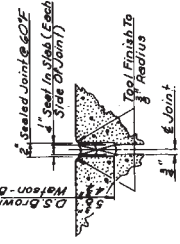


DETAIL "B"

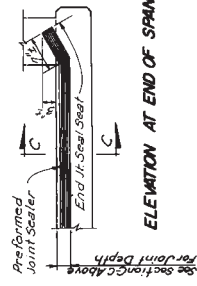
Diaphragm Inserts (4" Depth)
 NOT E. Concrete Diaphragm Inserts
 Shall Be As Manufactured By
 The Richmond Screw Anchor
 Co., Inc., Atlanta, Ga., Or By
 an Equivalent One, Or
 Other Approved Equal.



SECTION C-C
 For Beams No. 117



SECTION C-C
 For Beam No. 4



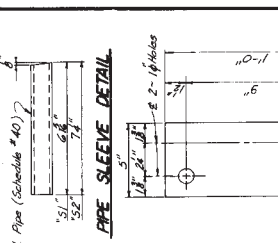
ELEVATION AT END OF SPAN

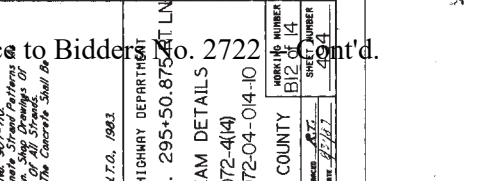
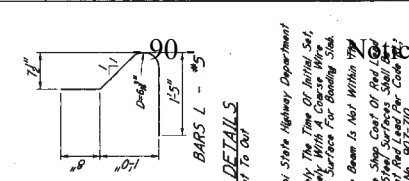
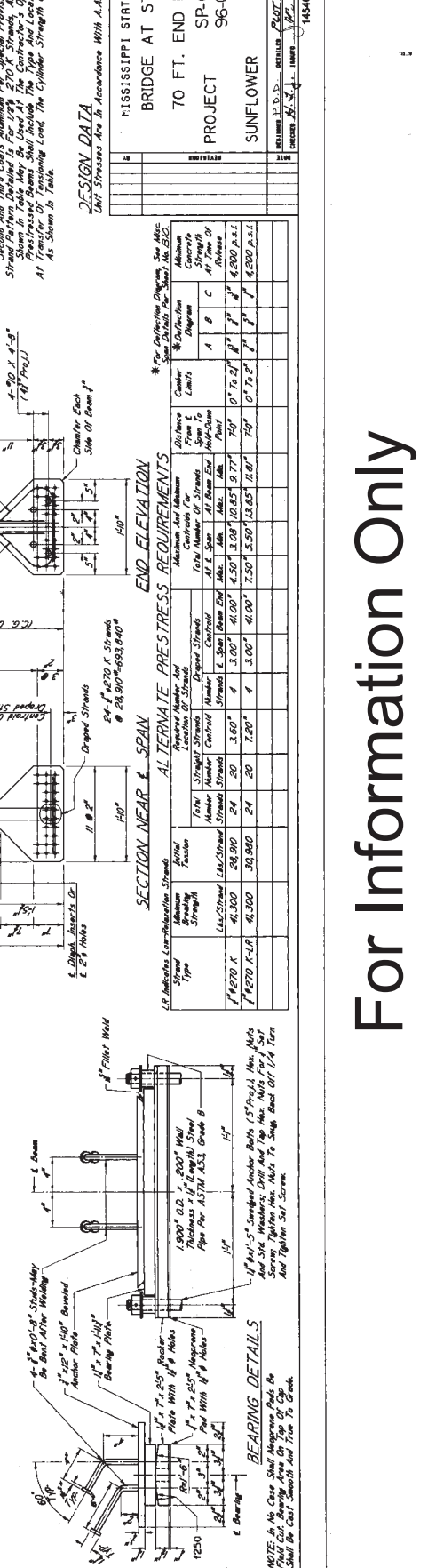
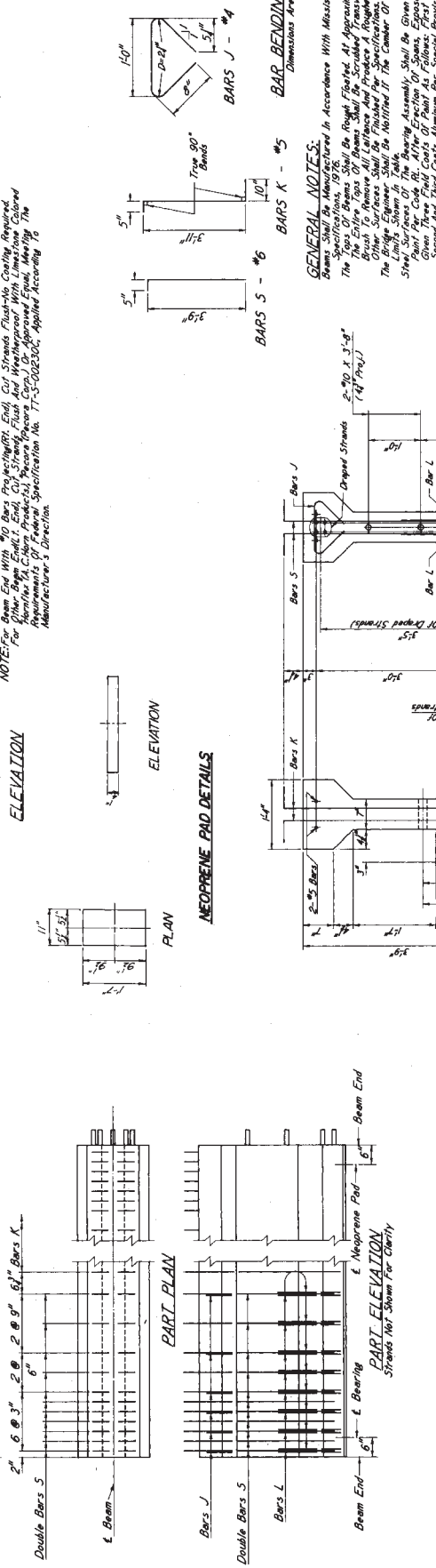
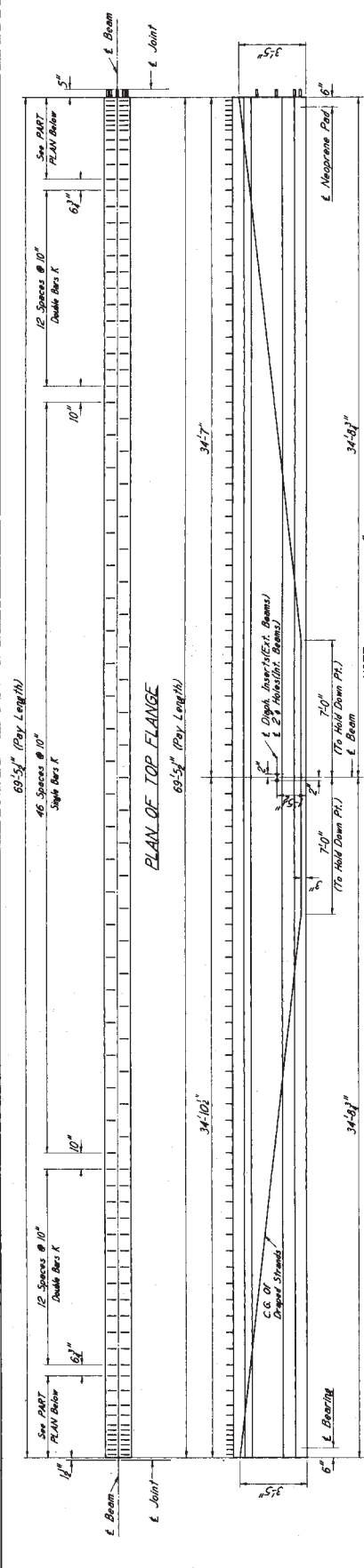
Performed Elastic Joint Sealer, Shall Be Type G-2000 Or H-3500
 As Manufactured By The D.S. Brown Co., North Baltimore, Ohio
 Type MU-200L Or W-350 As Manufactured By D. S. Brown Company
 Equal, Installed According To Manufacturer's Directions.

TYPICAL JOINT SEAL
 Use Where Indicated On The Layout Sheet

For Information Only

STATE _____ PROJECT NO. SP-0072-4(A)
 DATE _____





GENERAL NOTES:
 Beams shall be manufactured in accordance with Mississippi State Highway Department Specifications, 1976.
 The entire top of beams shall be rough finished, approximately the type of finish set forth in the Specifications. The entire top of beams shall be scrubbed transversely with a coarse wire brush to remove all laitance and produce a roughened surface for bonding steel to concrete.
 The bridge expansion shall be notified if the center of the beam is not within 1/4\"/>

DESIGN DATA
 UNIT STRENGTHS ARE IN ACCORDANCE WITH A.A.S.H.T.O., 1983.
 MISSISSIPPI STATE HIGHWAY DEPARTMENT
 BRIDGE AT STA. 295+50.875 AT LN 20 FT. END BEAM DETAILS
 PROJECT SP-0072-4(14)
 96-0072-04-014-10
 SUNFLOWER COUNTY
 WORKING NUMBER B12 OF 14
 SHEET NUMBER 2 OF 4
 CHECKED BY: J.S. J. DATE: 11/17/87
 DRAWN BY: J.S. J.

TABLE: ALTERNATE PRESTRESS REQUIREMENTS

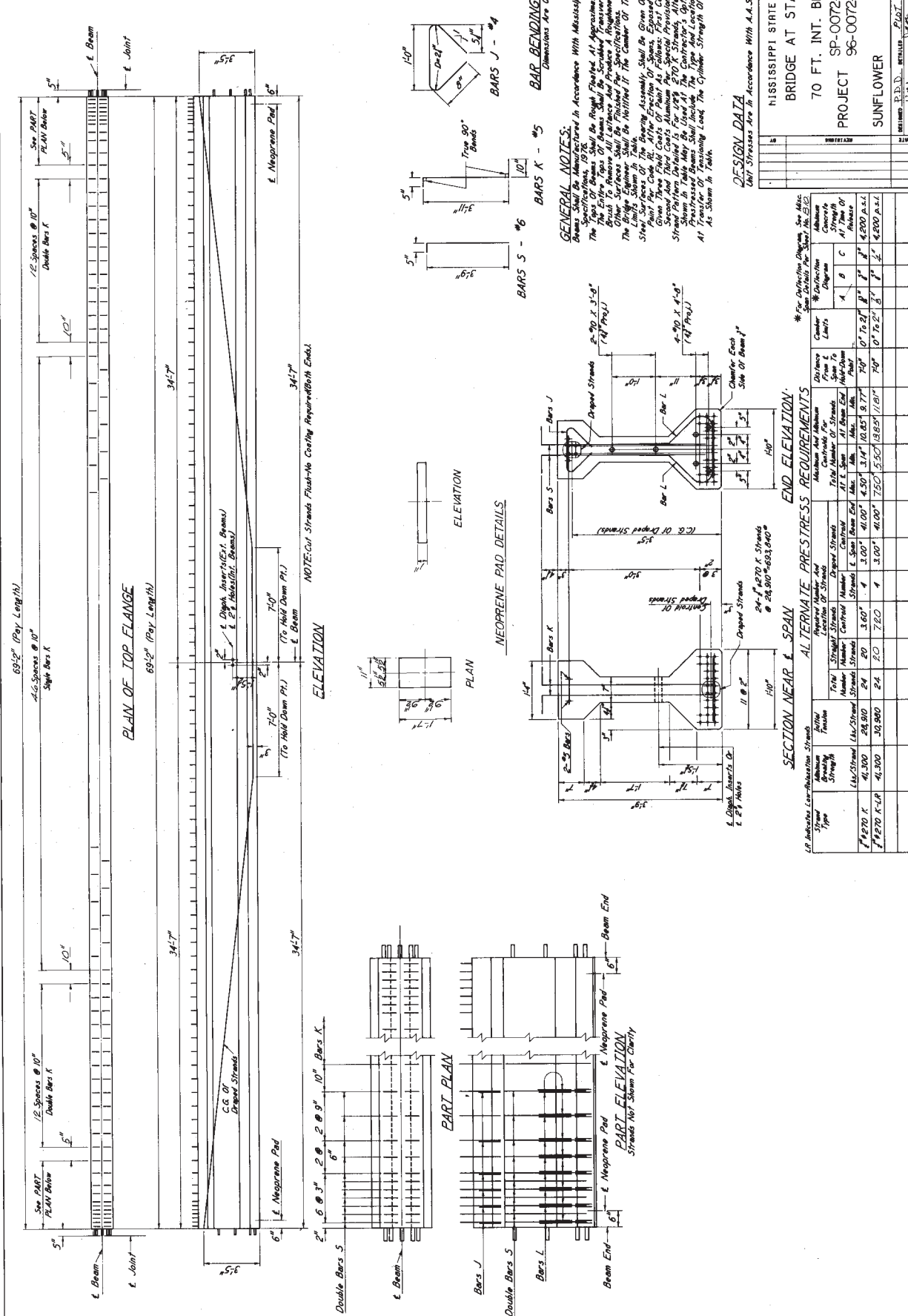
Strand Type	Minimum Breaking Strength	Initial Tension	Maximum and Minimum Force	Distance From End of Beam	Number of Strands	Location of Strands
F#270 K	41,800	28,940	41,800	3.00"	24	20
F#270 L-R	41,800	30,940	41,800	3.00"	24	20

BEARING DETAILS:
 NOTE: Do Not Cast Neoprene Pads By Hand. Machine Cast Neoprene Pads. They Shall Be Cast Smooth And True To Shape.

156

For Information Only

STATE: MISS. PROJECT NO.: SP-0072-4(4)



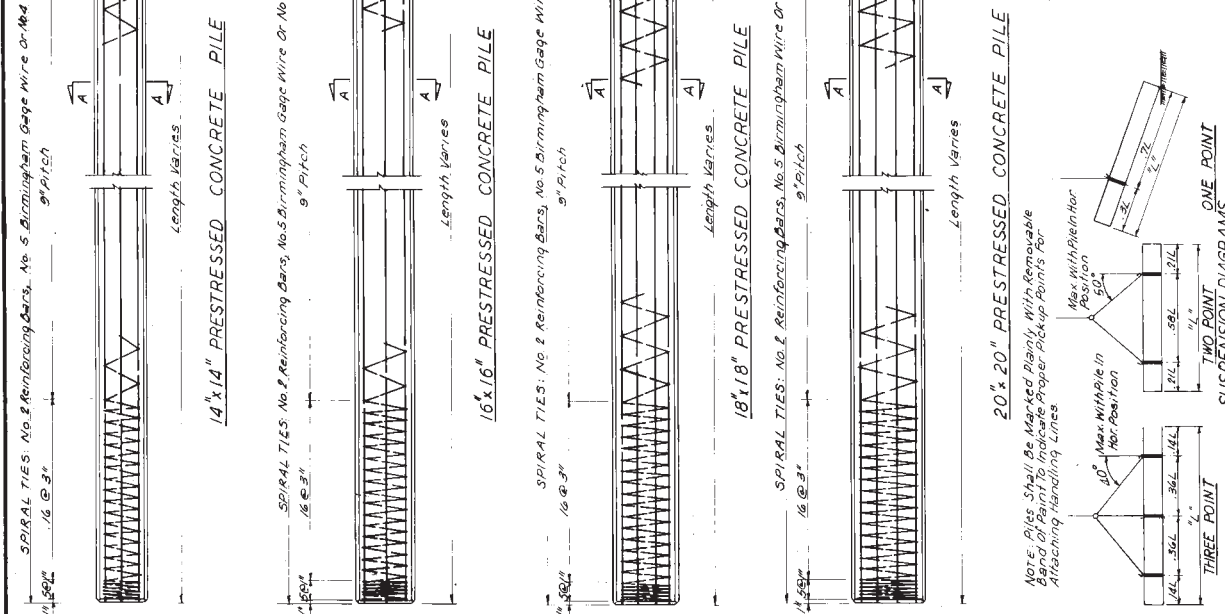
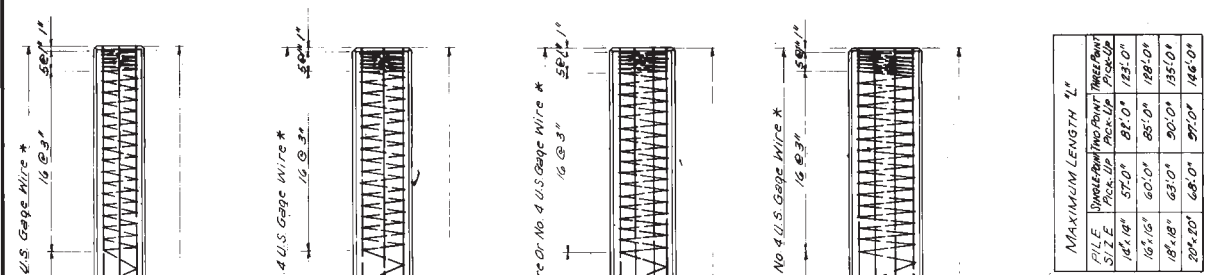
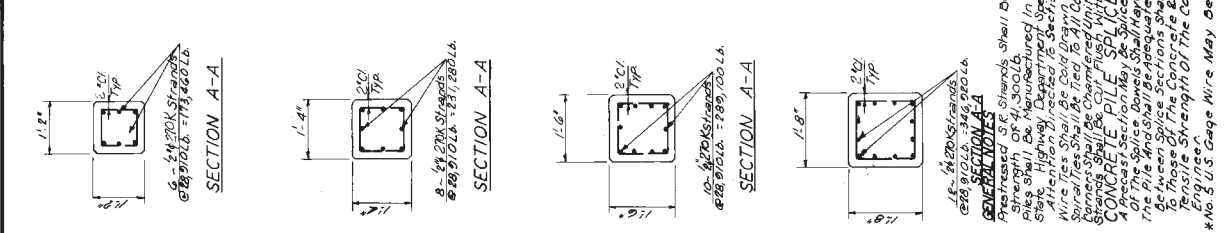
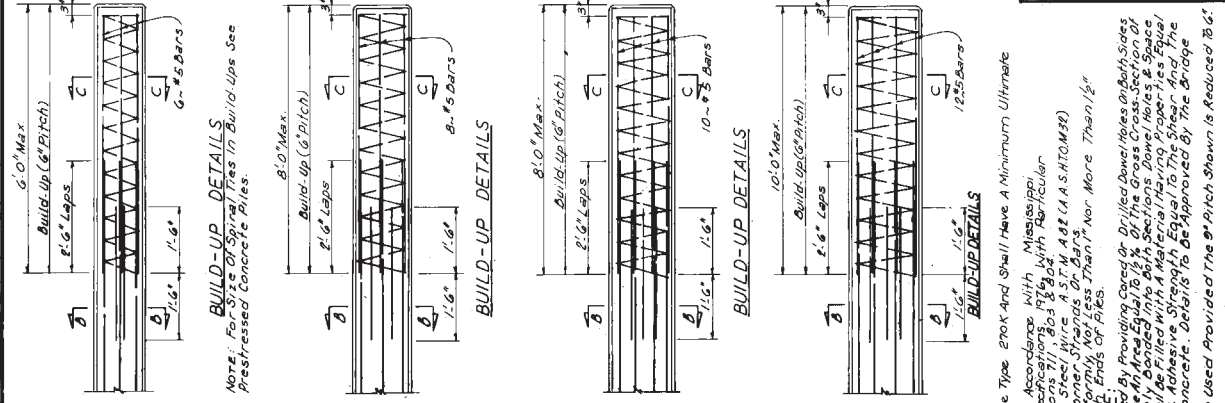
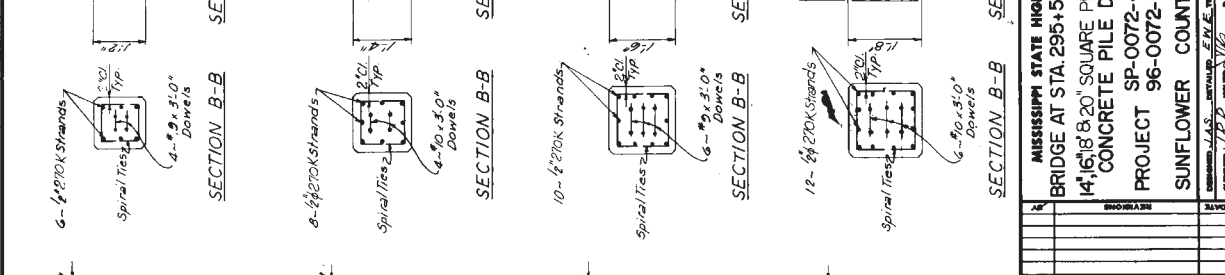
GENERAL NOTES:
 Beams Shall Be Manufactured In Accordance With Mississippi State Highway Department Specifications, 1976.
 The Ends Of Beams Shall Be Rough Finished At Approximately The Top Of Each End. The Ends Of Beams Shall Be Sanded Transversely With A Coarse Wire Brush To Remove All Entrapped Air And Produce A Roughened Surface For Bonding Steel.
 Other Surfaces Shall Be Finished For Specification. The Beam Is Not To Be Finished With A Smooth Surface.
 The Limits Shown In Table.
 Steel Surfaces Of The Bearing Assembly Shall Be Given One Shop Coat Of Epoxy Primer And Three Coats Of Paint As Follows: First Coat: Red Lead For Corrosion Protection. Second And Third Coats: Aluminum Oxide. Special Provision No. 907-710. The Contractor Shall Be Responsible For The Application Of The Paint.
 The Limits In Table May Be Used At The Contractor's Option. Shop Drawings Of Prestressed Beams Shall Include The Type And Location Of All Strands.
 All Transfer Or Positioning Load The Cylinder Strength Of The Concrete Shall Be As Shown In Table.

DESIGN DATA
 Unit Stresses Are In Accordance With A.A.S.H.T.O., 1963.
 MISSISSIPPI STATE HIGHWAY DEPARTMENT
 BRIDGE AT STA. 295+50.872 ET LN.
 70 FT. INT. BEAM DETAILS
 PROJECT SP-0072-4(4)
 SUNFLOWER COUNTY
 DRAWING NUMBER B31014
 SHEET NUMBER 14 OF 14
 DATE: 12/12/62

ALTERNATE PRESTRESS REQUIREMENTS

Strand Type	Ultimate Compressive Strength	Ultimate Tensile Strength	Yield Point	Modulus of Elasticity	Strand Length	Strand Location	Strand Number	Strand Area	Strand Spacing	Strand Spacing	Strand Spacing	Strand Spacing	Strand Spacing	Strand Spacing	Strand Spacing	Strand Spacing	Strand Spacing	Strand Spacing
F#270 K	41,000	28,800	24	2.0	720	4	3.00"	41,000	4,901	3.14"	10,851	9.77"	707	0" 0.50"	1"	4,200	4,200	4,200
F#270 K-LP	41,000	30,900	24	2.0	720	4	3.00"	41,000	750	5.50"	11.01"	707	0" 0.50"	1"	4,200	4,200	4,200	4,200

For Information Only

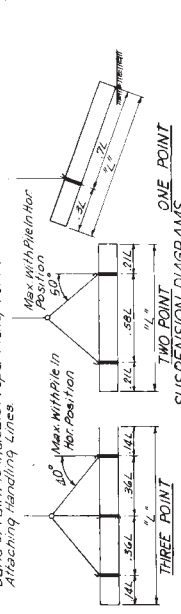


NOTE: PILES SHALL BE MARKED PLAINLY WITH REMOVABLE BAND OF PAINT TO INDICATE PROPER PICKUP POINTS FOR ATTACHING HANDLING LINES.

THREE POINT SUSPENSION DIAGRAMS

ONE POINT SUSPENSION DIAGRAMS

PILE SIZE	MAXIMUM LENGTH "L"	
	Pick-Up	Pick-Up
14" x 14"	57'-0"	183'-0"
16" x 16"	60'-0"	188'-0"
18" x 18"	63'-0"	193'-0"
20" x 20"	66'-0"	198'-0"



NOTE TO BIDDERS: PRESTRESSED STRANDS SHALL BE TYPE 270K AND SHALL HAVE A MINIMUM ULTIMATE STRENGTH OF 4,300 LB. SPIRAL TIES SHALL BE MANUFACTURED IN ACCORDANCE WITH MISSISSIPPI HIGHWAY DEPARTMENT TECHNICAL SPECIFICATION 80.5 WITH REINFORCING WIRE TIES SHALL BE COLD DRAWN STEEL WIRE A S.T.M. A 8 (A.S.H.T.M.F.R.) STRANDS SHALL BE COLD FINISH WITH BREAK STRENGTH MORE THAN 1/2" A PRECAST SECTION MAY BE SPliced BY PROVIDING CURED OR DRILLED DOWEL HOLES ON BOTH SIDES OF THE PILE WITH SUFFICIENT SPACING TO ALLOW SECTION TO BE PLACED IN PLACE BETWEEN SPICE SECTIONS SHALL BE FILLED WITH A MATERIAL HAVING PROPERTIES EQUAL TO THOSE OF THE CONCRETE AND ADHESIVE STRENGTH EQUAL TO THE STRIP AND THE TENSILE STRENGTH OF THE CONCRETE. DETAILS TO BE APPROVED BY THE BRIDGE DIVISION. *NO. 3 U.S. GAUGE WIRE MAY BE USED PROVIDED THE PITCH SHOWN IS REDUCED TO 6".

MISSISSIPPI STATE HIGHWAY DEPARTMENT

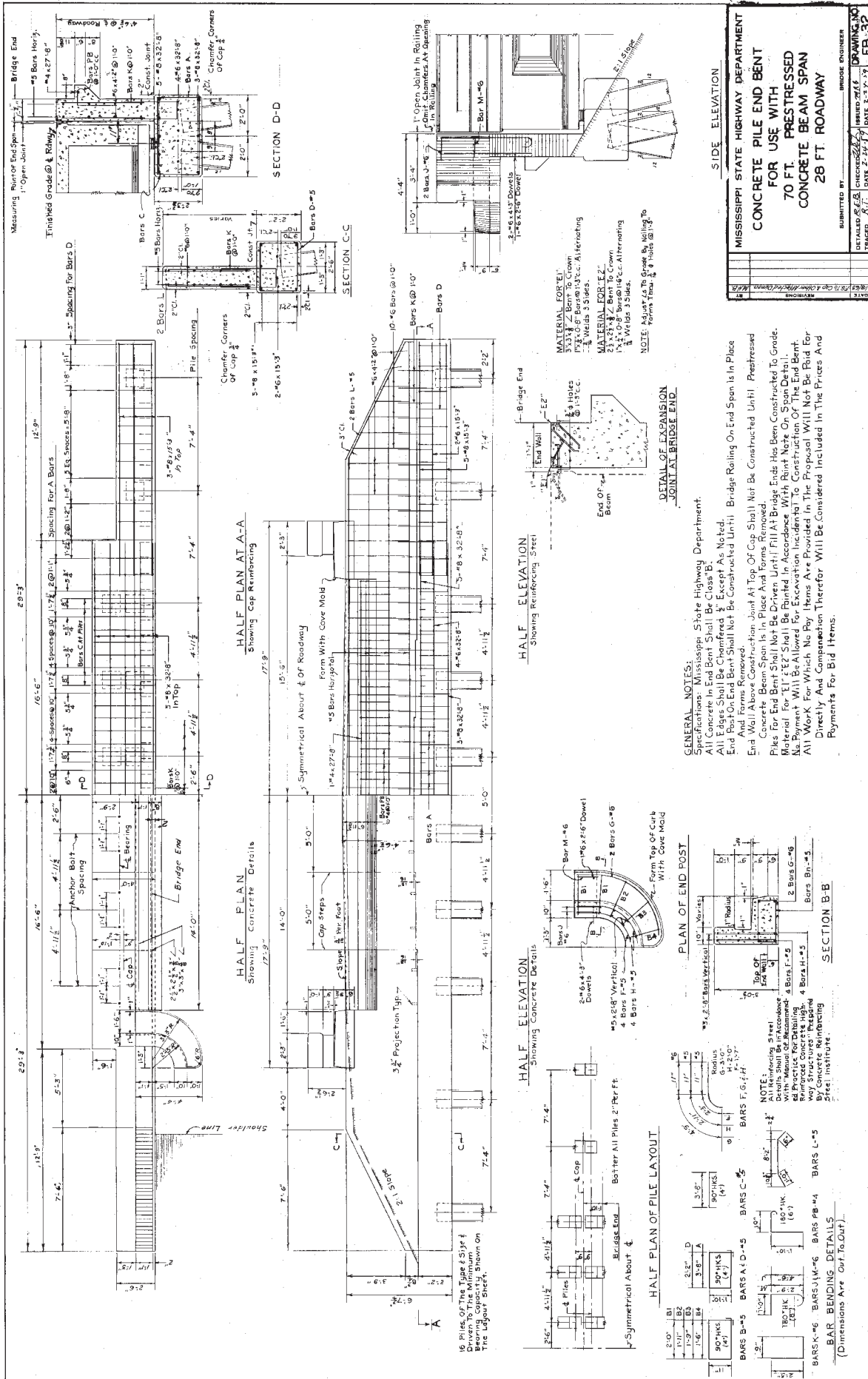
BRIDGE AT STA. 295+50.875 RT. LN.2

PROJECT SP-0072-4(14)

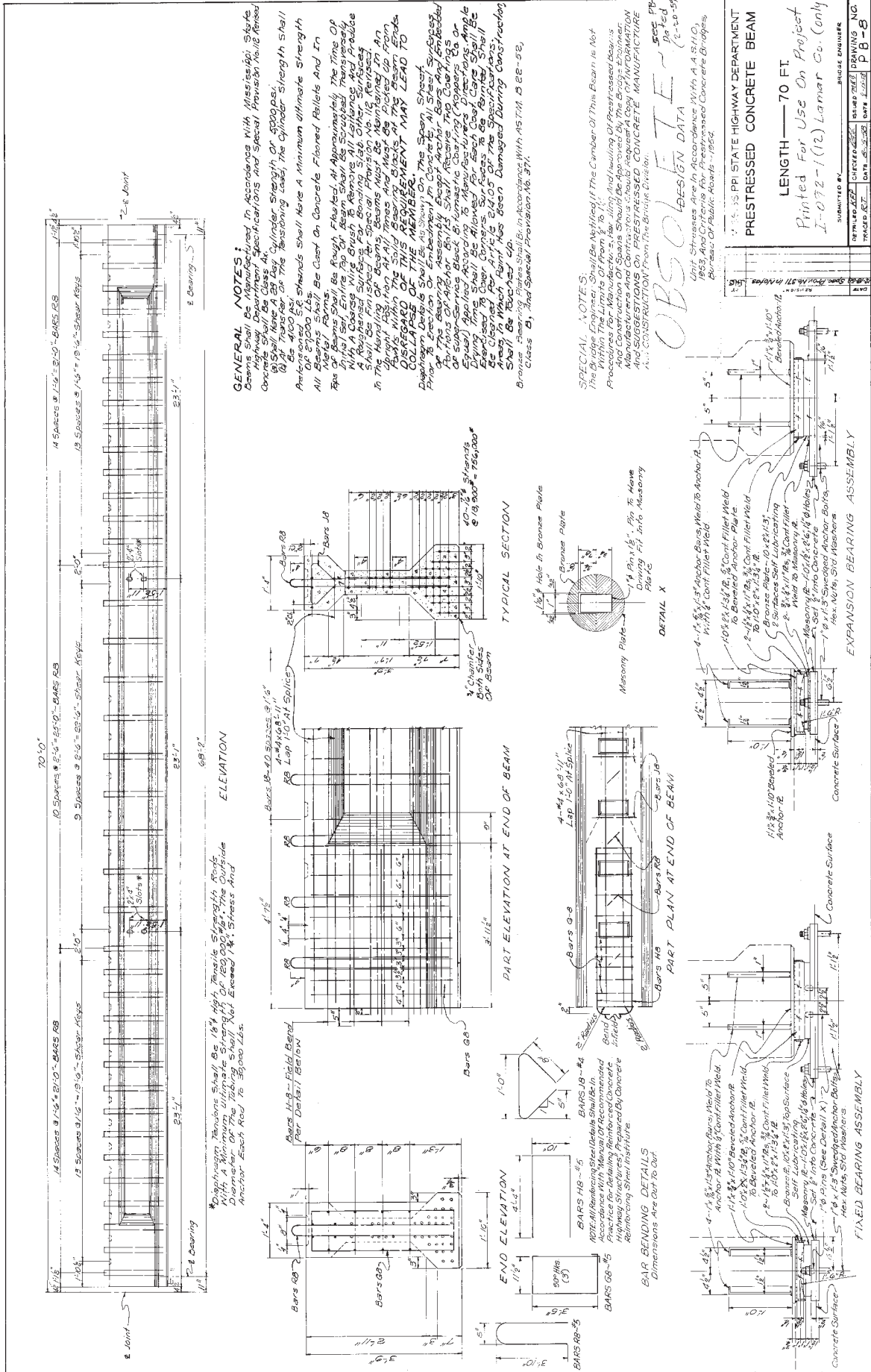
SUNFLOWER COUNTY

DATE: 11-15-58

For Information Only



For Information Only



For Information Only

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2782

CODE: (SP)

DATE: 8/13/2020

SUBJECT: DBE Pre-Bid Meeting

Due to the COVID-19 pandemic and the Department not allowing visitors in the Administration Building at this time, the DBE Pre-Bid Meeting referenced on Page 5 of Notice to Bidders No. 2611 will be held by **video conference only**. The meeting will be held at 2:00 P.M. on the day preceding the date of the bid opening using Zoom video conferencing software. Anyone interested in participating can download Zoom and connect to the meeting at the below link.

<https://zoom.us/j/5548736403?pwd=SDh5S2hQSE5pNG5FOEkzR3NsUnBYQT09>

Password (if prompted): 272147

For those unable to participate via Zoom, the below teleconference number may be used instead.

1-888-227-7517

Conference Code: 404496

"General Decision Number: MS20200129 03/20/2020

Superseded General Decision Number: MS20190129

State: Mississippi

Construction Type: Highway

County: Copiah County in Mississippi.

HIGHWAY CONSTRUCTION PROJECTS

Note: Under Executive Order (EO) 13658, an hourly minimum wage of \$10.80 for calendar year 2020 applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2015. If this contract is covered by the EO, the contractor must pay all workers in any classification listed on this wage determination at least \$10.80 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in calendar year 2020. If this contract is covered by the EO and a classification considered necessary for performance of work on the contract does not appear on this wage determination, the contractor must pay workers in that classification at least the wage rate determined through the conformance process set forth in 29 CFR 5.5(a)(1)(ii) (or the EO minimum wage rate, if it is higher than the conformed wage rate). The EO minimum wage rate will be adjusted annually. Please note that this EO applies to the above-mentioned types of contracts entered into by the federal government that are subject to the Davis-Bacon Act itself, but it does not apply to contracts subject only to the Davis-Bacon Related Acts, including those set forth at 29 CFR 5.1(a)(2)-(60). Additional information on contractor requirements and worker protections under the EO is available at www.dol.gov/whd/govcontracts.

Modification Number	Publication Date
0	01/03/2020
1	03/20/2020

* ELEC0480-010 01/01/2020

	Rates	Fringes
TRAFFIC SIGNALIZATION		
Electrician.....	\$ 25.90	9.56

SUMS2010-052 08/04/2014		

	Rates	Fringes
CARPENTER (Form Work Only).....	\$ 13.28	0.00
CEMENT MASON/CONCRETE FINISHER...	\$ 15.01	0.00
ELECTRICIAN.....	\$ 24.04	5.87
HIGHWAY/PARKING LOT STRIPING:		
Truck Driver (Line Striping		
Truck).....	\$ 12.05	0.00
INSTALLER - GUARDRAIL.....	\$ 12.07	0.00

INSTALLER - SIGN.....	\$ 12.38	0.00
IRONWORKER, REINFORCING.....	\$ 15.47	0.00
LABORER: Common or General, Including Asphalt Raking, Shoveling, Spreading.....	\$ 10.06	0.00
LABORER: Flagger.....	\$ 10.40	0.00
LABORER: Grade Checker.....	\$ 9.94	0.00
LABORER: Luteman.....	\$ 12.88	0.00
LABORER: Mason Tender - Cement/Concrete.....	\$ 11.27	0.00
LABORER: Pipelayer.....	\$ 13.44	0.00
LABORER: Laborer-Cones/ Barricades/Barrels - Setter/Mover/Sweeper.....	\$ 8.25	0.00
OPERATOR: Asphalt Spreader.....	\$ 14.71	0.00
OPERATOR: Backhoe/Excavator/Trackhoe.....	\$ 14.69	0.00
OPERATOR: Bobcat/Skid Steer/Skid Loader.....	\$ 11.64	0.00
OPERATOR: Broom/Sweeper.....	\$ 10.14	0.00
OPERATOR: Bulldozer.....	\$ 15.25	0.00
OPERATOR: Concrete Saw.....	\$ 16.50	0.00
OPERATOR: Crane.....	\$ 19.22	0.00
OPERATOR: Distributor.....	\$ 11.17	0.00
OPERATOR: Grader/Blade.....	\$ 11.67	0.00
OPERATOR: Grinding/Grooving Machine.....	\$ 16.86	0.00
OPERATOR: Loader.....	\$ 16.75	0.00
OPERATOR: Mechanic.....	\$ 19.16	0.00
OPERATOR: Milling Machine.....	\$ 14.68	0.00
OPERATOR: Mixer.....	\$ 14.25	0.00
OPERATOR: Oiler.....	\$ 12.35	0.00
OPERATOR: Paver (Asphalt, Aggregate, and Concrete).....	\$ 11.40	0.00
OPERATOR: Roller (All Types)....	\$ 11.29	0.00
OPERATOR: Scraper.....	\$ 12.25	0.00

OPERATOR: Tractor.....	\$ 11.22	0.00
TRUCK DRIVER: Flatbed Truck.....	\$ 14.06	0.00
TRUCK DRIVER: Lowboy Truck.....	\$ 11.96	0.00
TRUCK DRIVER: Mechanic.....	\$ 13.00	0.00
TRUCK DRIVER: Water Truck.....	\$ 19.00	0.00
TRUCK DRIVER: Dump Truck (All Types).....	\$ 12.66	0.00
TRUCK DRIVER: Semi/Trailer Truck.....	\$ 14.60	0.00

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

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Note: Executive Order (EO) 13706, Establishing Paid Sick Leave for Federal Contractors applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2017. If this contract is covered by the EO, the contractor must provide employees with 1 hour of paid sick leave for every 30 hours they work, up to 56 hours of paid sick leave each year. Employees must be permitted to use paid sick leave for their own illness, injury or other health-related needs, including preventive care; to assist a family member (or person who is like family to the employee) who is ill, injured, or has other health-related needs, including preventive care; or for reasons resulting from, or to assist a family member (or person who is like family to the employee) who is a victim of, domestic violence, sexual assault, or stalking. Additional information on contractor requirements and worker protections under the EO is available at www.dol.gov/whd/govcontracts.

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (ii)).

The body of each wage determination lists the classification and wage rates that have been found to be prevailing for the cited type(s) of construction in the area covered by the wage determination. The classifications are listed in alphabetical order of ""identifiers"" that indicate whether the particular rate is a union rate (current union negotiated rate for local), a survey rate (weighted average rate) or a union average rate (weighted union average rate).

Union Rate Identifiers

A four letter classification abbreviation identifier enclosed in dotted lines beginning with characters other than ""SU"" or ""UAVG"" denotes that the union classification and rate were prevailing for that classification in the survey. Example:

PLUM0198-005 07/01/2014. PLUM is an abbreviation identifier of the union which prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. 07/01/2014 is the effective date of the most current negotiated rate, which in this example is July 1, 2014.

Union prevailing wage rates are updated to reflect all rate changes in the collective bargaining agreement (CBA) governing this classification and rate.

Survey Rate Identifiers

Classifications listed under the "SU" identifier indicate that no one rate prevailed for this classification in the survey and the published rate is derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As this weighted average rate includes all rates reported in the survey, it may include both union and non-union rates. Example: SULA2012-007 5/13/2014. SU indicates the rates are survey rates based on a weighted average calculation of rates and are not majority rates. LA indicates the State of Louisiana. 2012 is the year of survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. 5/13/2014 indicates the survey completion date for the classifications and rates under that identifier.

Survey wage rates are not updated and remain in effect until a new survey is conducted.

Union Average Rate Identifiers

Classification(s) listed under the UAVG identifier indicate that no single majority rate prevailed for those classifications; however, 100% of the data reported for the classifications was union data. EXAMPLE: UAVG-OH-0010 08/29/2014. UAVG indicates that the rate is a weighted union average rate. OH indicates the state. The next number, 0010 in the example, is an internal number used in producing the wage determination. 08/29/2014 indicates the survey completion date for the classifications and rates under that identifier.

A UAVG rate will be updated once a year, usually in January of each year, to reflect a weighted average of the current negotiated/CBA rate of the union locals from which the rate is based.

WAGE DETERMINATION APPEALS PROCESS

1.) Has there been an initial decision in the matter? This can be:

- * an existing published wage determination
- * a survey underlying a wage determination
- * a Wage and Hour Division letter setting forth a position on a wage determination matter

* a conformance (additional classification and rate) ruling

On survey related matters, initial contact, including requests for summaries of surveys, should be with the Wage and Hour Regional Office for the area in which the survey was conducted because those Regional Offices have responsibility for the Davis-Bacon survey program. If the response from this initial contact is not satisfactory, then the process described in 2.) and 3.) should be followed.

With regard to any other matter not yet ripe for the formal process described here, initial contact should be with the Branch of Construction Wage Determinations. Write to:

Branch of Construction Wage Determinations
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

2.) If the answer to the question in 1.) is yes, then an interested party (those affected by the action) can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Write to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and by any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3.) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

4.) All decisions by the Administrative Review Board are final.

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END OF GENERAL DECISION"

"General Decision Number: MS20200115 01/03/2020

Superseded General Decision Number: MS20190115

State: Mississippi

Construction Type: Highway

County: Sunflower County in Mississippi.

HIGHWAY CONSTRUCTION PROJECTS

Note: Under Executive Order (EO) 13658, an hourly minimum wage of \$10.80 for calendar year 2020 applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2015. If this contract is covered by the EO, the contractor must pay all workers in any classification listed on this wage determination at least \$10.80 per hour (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract in calendar year 2020. If this contract is covered by the EO and a classification considered necessary for performance of work on the contract does not appear on this wage determination, the contractor must pay workers in that classification at least the wage rate determined through the conformance process set forth in 29 CFR 5.5(a)(1)(ii) (or the EO minimum wage rate, if it is higher than the conformed wage rate). The EO minimum wage rate will be adjusted annually. Please note that this EO applies to the above-mentioned types of contracts entered into by the federal government that are subject to the Davis-Bacon Act itself, but it does not apply to contracts subject only to the Davis-Bacon Related Acts, including those set forth at 29 CFR 5.1(a)(2)-(60). Additional

information on contractor requirements and worker protections under the EO is available at www.dol.gov/whd/govcontracts.

Modification Number Publication Date
 0 01/03/2020

SUMS2010-038 08/04/2014

	Rates	Fringes
CARPENTER, Excludes Form Work....	\$ 14.03	0.00
CEMENT MASON/CONCRETE FINISHER...	\$ 13.13	0.00
ELECTRICIAN.....	\$ 21.80	7.93
FORM WORKER.....	\$ 12.28	0.00
HIGHWAY/PARKING LOT STRIPING:		
Truck Driver (Line Striping Truck).....	\$ 14.11	0.00
INSTALLER - GUARDRAIL.....	\$ 11.42	0.00
INSTALLER - SIGN.....	\$ 11.73	0.00
IRONWORKER, REINFORCING.....	\$ 16.29	0.00
LABORER: Common or General, Including Asphalt Raking, Shoveling, Spreading.....	\$ 10.38	0.00
LABORER: Concrete Worker.....	\$ 10.91	0.00
LABORER: Flagger.....	\$ 11.48	0.00
LABORER: Grade Checker.....	\$ 11.32	0.00

LABORER: Landscape.....	\$ 9.77	0.00
LABORER: Mason Tender - Cement/Concrete.....	\$ 11.08	0.00
LABORER: Pipelayer.....	\$ 11.34	0.00
LABORER: Laborer-Cones/ Barricades/Barrels - Setter/Mover/Sweeper.....	\$ 15.15	0.00
OPERATOR: Asphalt Spreader.....	\$ 16.03	0.00
OPERATOR: Backhoe/Excavator/Trackhoe.....	\$ 13.38	0.00
OPERATOR: Broom/Sweeper.....	\$ 10.77	0.00
OPERATOR: Bulldozer.....	\$ 13.67	0.00
OPERATOR: Concrete Saw.....	\$ 12.95	0.00
OPERATOR: Crane.....	\$ 21.25	0.00
OPERATOR: Distributor.....	\$ 12.38	0.00
OPERATOR: Drill.....	\$ 19.22	0.00
OPERATOR: Grader/Blade.....	\$ 14.44	0.00
OPERATOR: Grinding/Grooving Machine.....	\$ 15.94	0.00
OPERATOR: Loader.....	\$ 12.21	0.00
OPERATOR: Mechanic.....	\$ 15.32	0.00
OPERATOR: Milling Machine.....	\$ 18.16	0.00
OPERATOR: Oiler.....	\$ 12.33	0.48

OPERATOR: Paver (Asphalt, Aggregate, and Concrete).....	\$ 12.69	0.00
OPERATOR: Piledriver.....	\$ 15.13	0.00
OPERATOR: Roller (All Types)....	\$ 11.51	0.00
OPERATOR: Scraper.....	\$ 12.96	0.00
OPERATOR: Tractor.....	\$ 11.46	0.00
OPERATOR: Trencher.....	\$ 15.00	0.00
TRUCK DRIVER: Flatbed Truck.....	\$ 12.64	0.00
TRUCK DRIVER: Lowboy Truck.....	\$ 13.80	0.00
TRUCK DRIVER: Mechanic.....	\$ 14.08	0.00
TRUCK DRIVER: Off the Road Truck.....	\$ 12.29	0.00
TRUCK DRIVER: Water Truck.....	\$ 10.89	0.00
TRUCK DRIVER: Dump Truck (All Types).....	\$ 12.00	0.00
TRUCK DRIVER: Semi/Trailer Truck.....	\$ 15.29	0.00

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Note: Executive Order (EO) 13706, Establishing Paid Sick Leave for Federal Contractors applies to all contracts subject to the Davis-Bacon Act for which the contract is awarded (and any solicitation was issued) on or after January 1, 2017. If this

contract is covered by the EO, the contractor must provide employees with 1 hour of paid sick leave for every 30 hours they work, up to 56 hours of paid sick leave each year. Employees must be permitted to use paid sick leave for their own illness, injury or other health-related needs, including preventive care; to assist a family member (or person who is like family to the employee) who is ill, injured, or has other health-related needs, including preventive care; or for reasons resulting from, or to assist a family member (or person who is like family to the employee) who is a victim of, domestic violence, sexual assault, or stalking. Additional information on contractor requirements and worker protections under the EO is available at www.dol.gov/whd/govcontracts.

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (ii)).

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indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. 07/01/2014 is the effective date of the most current negotiated rate, which in this example is July 1, 2014.

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for the classifications and rates under that identifier.

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WAGE DETERMINATION APPEALS PROCESS

1.) Has there been an initial decision in the matter? This can be:

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- * a survey underlying a wage determination
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- * a conformance (additional classification and rate) ruling

On survey related matters, initial contact, including requests for summaries of surveys, should be with the Wage and Hour Regional Office for the area in which the survey was conducted because those Regional Offices have responsibility for the Davis-Bacon survey program. If the response from this initial contact is not satisfactory, then the process described in 2.) and 3.) should be followed.

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U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

2.) If the answer to the question in 1.) is yes, then an

interested party (those affected by the action) can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Write to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and by any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3.) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

4.) All decisions by the Administrative Review Board are final.

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END OF GENERAL DECISION

"

SUPPLEMENT TO FORM FHWA-1273

DATE: 12/17/2018

SUBJECT: **Federal Contract Provisions for Subcontracts and Cargo Preference Act**

Federal Contract Provisions for Subcontracts

All subcontracts shall be in writing and contain all pertinent provisions and requirements of the prime contract.

Each “Request for Permission to Subcontract” (Mississippi Department of Transportation Form CAD-720) shall include a copy of the subcontract. The federal contract provisions (FHWA-1273, SUPPLEMENT TO FORM FHWA-1273, NOTICE OF REQUIREMENT FOR AFFIRMATIVE ACTION TO ENSURE EQUAL EMPLOYMENT OPPORTUNITY (EXECUTIVE ORDER 11246), DAVIS-BACON AND RELATED ACT PROVISIONS (WAGE RATES)) must be physically incorporated as part of the subcontract. A completed Mississippi Department of Transportation Form CAD-521 and Form CAD-725 must be attached to the CAD-720.

Cargo Preference Act

The Contractor is hereby advised of the requirements set forth in the following Attachment (Title 46 - Shipping) as it pertains to the implementation of Cargo Preference Act (CPA) requirements in the Federal-aid Highway Program.

By signing this contract, the Contractor agrees to conform to the requirements of the CPA.

Attachment

Title 46- Shipping

Volume: 8

Date: 2014-10-01

Original Date: 2014-10-01

Title: Section 381.7 - Federal Grant, Guaranty, Loan and Advance at Funds Agreements.

Context: Title 46- Shipping. CHAPTER II- MARITIME ADMINISTRATION, DEPARTMENT OF TRANSPORTATION. SUBCHAPTER J - MISCELLANEOUS. PART 381 - CARGO PREFERENCE-U.S.- FLAG VESSELS.

§ 381.7 Federal Grant, Guaranty, Loan and Advance of Funds Agreements.

In order to insure a fair and reasonable participation by privately owned United States-flag commercial vessels in transporting cargoes which are subject to the Cargo Preference Act of 1954 and which are generated by U.S. Government Grant, Guaranty, Loan and/or Advance of Funds Programs, the head of each affected department or agency shall require appropriate clauses to be inserted in those Grant, Guaranty, Loan and/or Advance of Funds Agreements and all third party contracts executed between the borrower/grantee and other parties, where the possibility exists for ocean transportation of items procured, contracted for or otherwise obtained by or on behalf of the grantee, borrower, or any of their contractors or subcontractors. The clauses required by this part shall provide that at least 50 percent of the freight revenue and tonnage of cargo generated by the U.S. Government Grant, Guaranty, Loan or Advance of Funds be transported on privately owned United States-flag commercial vessels. These clauses shall also require that all parties provide to the Maritime Administration the necessary shipment information as set forth in § 381.3. A copy of the appropriate clauses required by this part shall be submitted by each affected agency or department to the Secretary, Maritime Administration, for approval no later than 30 days after the effective date of this part. The following are suggested acceptable clauses with respect to the use of United States-flag vessels to be incorporated in the Grant, Guaranty, Loan and/or Advance of Funds Agreements as well as contracts and subcontracts resulting therefrom:

(a) *Agreement Clauses.* "Use of United States-flag vessels:

"(1) Pursuant to Pub. L 664 (43 U.S.C. 1241(b)) at least 50 percent of any equipment, materials or commodities procured, contracted for or otherwise obtained with funds granted, guaranteed, loaned, or advanced by the U.S. Government under this agreement, and which may be transported by ocean vessel, shall be transported on privately owned United States-flag commercial vessels, if available.

"(2) Within 20 days following the date of loading for shipments originating within the United States or within 30 working days following the date of loading for shipments originating outside the United States, a legible copy of a rated, 'on-board' commercial ocean bill-of-lading in English for each shipment of cargo described in paragraph (a)(1) of this section shall be furnished to both the Contracting Officer (through the prime contractor in the case of subcontractor bills-of-lading) and to the Division of National Cargo, Office of Market Development, Maritime Administration, Washington, DC 20590."

(b) *Contractor and Subcontractor Clauses.* "Use of United States-flag vessels: The contractor agrees --

"(1) To utilize privately owned United States-flag commercial vessels to ship at least 50 percent of the gross tonnage (computed separately for dry bulk carriers, dry cargo liners, and tankers) involved, whenever shipping any equipment, material, or commodities pursuant to this contract, to the extent such vessels are available at fair and reasonable rates for United States-flag commercial vessels.

"(2) To furnish within 20 days following the date of loading for shipments originating within the United

States or within 30 working days following the date of loading for shipments originating outside the United States, a legible copy of a rated, 'on-board' commercial ocean bill-of-lading in English for each shipment of cargo described in paragraph (b) (1) of this section to both the Contracting Officer (through the prime contractor in the case of subcontractor bills-of-lading) and to the Division of National Cargo, Office of Market Development, Maritime Administration, Washington, DC 20590.

"(3) To insert the substance of the provisions of this clause in all subcontracts issued pursuant to this contract."

(Reorganization Plans No.21 of 1950(64 Stat. 1273) and No. 7 of 1961 (75 Stat. 840) as amended by Pub. L 91.469 (84 Stat 1036) and Department of Commerce Organization Order 10-8 (38 FR 19707, July 23, 1973)) (42 FR 57126, Nov. 1, 1977]

**REQUIRED CONTRACT PROVISIONS
FEDERAL-AID CONSTRUCTION CONTRACTS**

- I. General
- II. Nondiscrimination
- III. Nonsegregated Facilities
- IV. Davis-Bacon and Related Act Provisions
- V. Contract Work Hours and Safety Standards Act Provisions
- VI. Subletting or Assigning the Contract
- VII. Safety: Accident Prevention
- VIII. False Statements Concerning Highway Projects
- IX. Implementation of Clean Air Act and Federal Water Pollution Control Act
- X. Compliance with Governmentwide Suspension and Debarment Requirements
- XI. Certification Regarding Use of Contract Funds for Lobbying

ATTACHMENTS

A. Employment and Materials Preference for Appalachian Development Highway System or Appalachian Local Access Road Contracts (included in Appalachian contracts only)

I. GENERAL

1. Form FHWA-1273 must be physically incorporated in each construction contract funded under Title 23 (excluding emergency contracts solely intended for debris removal). The contractor (or subcontractor) must insert this form in each subcontract and further require its inclusion in all lower tier subcontracts (excluding purchase orders, rental agreements and other agreements for supplies or services).

The applicable requirements of Form FHWA-1273 are incorporated by reference for work done under any purchase order, rental agreement or agreement for other services. The prime contractor shall be responsible for compliance by any subcontractor, lower-tier subcontractor or service provider.

Form FHWA-1273 must be included in all Federal-aid design-build contracts, in all subcontracts and in lower tier subcontracts (excluding subcontracts for design services, purchase orders, rental agreements and other agreements for supplies or services). The design-builder shall be responsible for compliance by any subcontractor, lower-tier subcontractor or service provider.

Contracting agencies may reference Form FHWA-1273 in bid proposal or request for proposal documents, however, the Form FHWA-1273 must be physically incorporated (not referenced) in all contracts, subcontracts and lower-tier subcontracts (excluding purchase orders, rental agreements and other agreements for supplies or services related to a construction contract).

2. Subject to the applicability criteria noted in the following sections, these contract provisions shall apply to all work performed on the contract by the contractor's own organization and with the assistance of workers under the contractor's immediate superintendence and to all work performed on the contract by piecework, station work, or by subcontract.

3. A breach of any of the stipulations contained in these Required Contract Provisions may be sufficient grounds for withholding of progress payments, withholding of final payment, termination of the contract, suspension / debarment or any other action determined to be appropriate by the contracting agency and FHWA.

4. Selection of Labor: During the performance of this contract, the contractor shall not use convict labor for any purpose within the limits of a construction project on a Federal-aid highway unless it is labor performed by convicts who are on parole, supervised release, or probation. The term Federal-aid highway does not include roadways functionally classified as local roads or rural minor collectors.

II. NONDISCRIMINATION

The provisions of this section related to 23 CFR Part 230 are applicable to all Federal-aid construction contracts and to all related construction subcontracts of \$10,000 or more. The provisions of 23 CFR Part 230 are not applicable to material supply, engineering, or architectural service contracts.

In addition, the contractor and all subcontractors must comply with the following policies: Executive Order 11246, 41 CFR 60, 29 CFR 1625-1627, Title 23 USC Section 140, the Rehabilitation Act of 1973, as amended (29 USC 794), Title VI of the Civil Rights Act of 1964, as amended, and related regulations including 49 CFR Parts 21, 26 and 27; and 23 CFR Parts 200, 230, and 633.

The contractor and all subcontractors must comply with: the requirements of the Equal Opportunity Clause in 41 CFR 60-1.4(b) and, for all construction contracts exceeding \$10,000, the Standard Federal Equal Employment Opportunity Construction Contract Specifications in 41 CFR 60-4.3.

Note: The U.S. Department of Labor has exclusive authority to determine compliance with Executive Order 11246 and the policies of the Secretary of Labor including 41 CFR 60, and 29 CFR 1625-1627. The contracting agency and the FHWA have the authority and the responsibility to ensure compliance with Title 23 USC Section 140, the Rehabilitation Act of 1973, as amended (29 USC 794), and Title VI of the Civil Rights Act of 1964, as amended, and related regulations including 49 CFR Parts 21, 26 and 27; and 23 CFR Parts 200, 230, and 633.

The following provision is adopted from 23 CFR 230, Appendix A, with appropriate revisions to conform to the U.S. Department of Labor (US DOL) and FHWA requirements.

1. Equal Employment Opportunity: Equal employment opportunity (EEO) requirements not to discriminate and to take affirmative action to assure equal opportunity as set forth under laws, executive orders, rules, regulations (28 CFR 35, 29 CFR 1630, 29 CFR 1625-1627, 41 CFR 60 and 49 CFR 27) and orders of the Secretary of Labor as modified by the provisions prescribed herein, and imposed pursuant to 23 U.S.C. 140 shall constitute the EEO and specific affirmative action standards for the contractor's project activities under

this contract. The provisions of the Americans with Disabilities Act of 1990 (42 U.S.C. 12101 et seq.) set forth under 28 CFR 35 and 29 CFR 1630 are incorporated by reference in this contract. In the execution of this contract, the contractor agrees to comply with the following minimum specific requirement activities of EEO:

a. The contractor will work with the contracting agency and the Federal Government to ensure that it has made every good faith effort to provide equal opportunity with respect to all of its terms and conditions of employment and in their review of activities under the contract.

b. The contractor will accept as its operating policy the following statement:

"It is the policy of this Company to assure that applicants are employed, and that employees are treated during employment, without regard to their race, religion, sex, color, national origin, age or disability. Such action shall include: employment, upgrading, demotion, or transfer; recruitment or recruitment advertising; layoff or termination; rates of pay or other forms of compensation; and selection for training, including apprenticeship, pre-apprenticeship, and/or on-the-job training."

2. EEO Officer: The contractor will designate and make known to the contracting officers an EEO Officer who will have the responsibility for and must be capable of effectively administering and promoting an active EEO program and who must be assigned adequate authority and responsibility to do so.

3. Dissemination of Policy: All members of the contractor's staff who are authorized to hire, supervise, promote, and discharge employees, or who recommend such action, or who are substantially involved in such action, will be made fully cognizant of, and will implement, the contractor's EEO policy and contractual responsibilities to provide EEO in each grade and classification of employment. To ensure that the above agreement will be met, the following actions will be taken as a minimum:

a. Periodic meetings of supervisory and personnel office employees will be conducted before the start of work and then not less often than once every six months, at which time the contractor's EEO policy and its implementation will be reviewed and explained. The meetings will be conducted by the EEO Officer.

b. All new supervisory or personnel office employees will be given a thorough indoctrination by the EEO Officer, covering all major aspects of the contractor's EEO obligations within thirty days following their reporting for duty with the contractor.

c. All personnel who are engaged in direct recruitment for the project will be instructed by the EEO Officer in the contractor's procedures for locating and hiring minorities and women.

d. Notices and posters setting forth the contractor's EEO policy will be placed in areas readily accessible to employees, applicants for employment and potential employees.

e. The contractor's EEO policy and the procedures to implement such policy will be brought to the attention of employees by means of meetings, employee handbooks, or other appropriate means.

4. Recruitment: When advertising for employees, the contractor will include in all advertisements for employees the notation: "An Equal Opportunity Employer." All such advertisements will be placed in publications having a large circulation among minorities and women in the area from which the project work force would normally be derived.

a. The contractor will, unless precluded by a valid bargaining agreement, conduct systematic and direct recruitment through public and private employee referral sources likely to yield qualified minorities and women. To meet this requirement, the contractor will identify sources of potential minority group employees, and establish with such identified sources procedures whereby minority and women applicants may be referred to the contractor for employment consideration.

b. In the event the contractor has a valid bargaining agreement providing for exclusive hiring hall referrals, the contractor is expected to observe the provisions of that agreement to the extent that the system meets the contractor's compliance with EEO contract provisions. Where implementation of such an agreement has the effect of discriminating against minorities or women, or obligates the contractor to do the same, such implementation violates Federal nondiscrimination provisions.

c. The contractor will encourage its present employees to refer minorities and women as applicants for employment. Information and procedures with regard to referring such applicants will be discussed with employees.

5. Personnel Actions: Wages, working conditions, and employee benefits shall be established and administered, and personnel actions of every type, including hiring, upgrading, promotion, transfer, demotion, layoff, and termination, shall be taken without regard to race, color, religion, sex, national origin, age or disability. The following procedures shall be followed:

a. The contractor will conduct periodic inspections of project sites to insure that working conditions and employee facilities do not indicate discriminatory treatment of project site personnel.

b. The contractor will periodically evaluate the spread of wages paid within each classification to determine any evidence of discriminatory wage practices.

c. The contractor will periodically review selected personnel actions in depth to determine whether there is evidence of discrimination. Where evidence is found, the contractor will promptly take corrective action. If the review indicates that the discrimination may extend beyond the actions reviewed, such corrective action shall include all affected persons.

d. The contractor will promptly investigate all complaints of alleged discrimination made to the contractor in connection with its obligations under this contract, will attempt to resolve such complaints, and will take appropriate corrective action within a reasonable time. If the investigation indicates that the discrimination may affect persons other than the complainant, such corrective action shall include such other persons. Upon completion of each investigation, the contractor will inform every complainant of all of their avenues of appeal.

6. Training and Promotion:

a. The contractor will assist in locating, qualifying, and increasing the skills of minorities and women who are

applicants for employment or current employees. Such efforts should be aimed at developing full journey level status employees in the type of trade or job classification involved.

b. Consistent with the contractor's work force requirements and as permissible under Federal and State regulations, the contractor shall make full use of training programs, i.e., apprenticeship, and on-the-job training programs for the geographical area of contract performance. In the event a special provision for training is provided under this contract, this subparagraph will be superseded as indicated in the special provision. The contracting agency may reserve training positions for persons who receive welfare assistance in accordance with 23 U.S.C. 140(a).

c. The contractor will advise employees and applicants for employment of available training programs and entrance requirements for each.

d. The contractor will periodically review the training and promotion potential of employees who are minorities and women and will encourage eligible employees to apply for such training and promotion.

7. Unions: If the contractor relies in whole or in part upon unions as a source of employees, the contractor will use good faith efforts to obtain the cooperation of such unions to increase opportunities for minorities and women. Actions by the contractor, either directly or through a contractor's association acting as agent, will include the procedures set forth below:

a. The contractor will use good faith efforts to develop, in cooperation with the unions, joint training programs aimed toward qualifying more minorities and women for membership in the unions and increasing the skills of minorities and women so that they may qualify for higher paying employment.

b. The contractor will use good faith efforts to incorporate an EEO clause into each union agreement to the end that such union will be contractually bound to refer applicants without regard to their race, color, religion, sex, national origin, age or disability.

c. The contractor is to obtain information as to the referral practices and policies of the labor union except that to the extent such information is within the exclusive possession of the labor union and such labor union refuses to furnish such information to the contractor, the contractor shall so certify to the contracting agency and shall set forth what efforts have been made to obtain such information.

d. In the event the union is unable to provide the contractor with a reasonable flow of referrals within the time limit set forth in the collective bargaining agreement, the contractor will, through independent recruitment efforts, fill the employment vacancies without regard to race, color, religion, sex, national origin, age or disability; making full efforts to obtain qualified and/or qualifiable minorities and women. The failure of a union to provide sufficient referrals (even though it is obligated to provide exclusive referrals under the terms of a collective bargaining agreement) does not relieve the contractor from the requirements of this paragraph. In the event the union referral practice prevents the contractor from meeting the obligations pursuant to Executive Order 11246, as amended, and these special provisions, such contractor shall immediately notify the contracting agency.

8. Reasonable Accommodation for Applicants / Employees with Disabilities: The contractor must be familiar

with the requirements for and comply with the Americans with Disabilities Act and all rules and regulations established there under. Employers must provide reasonable accommodation in all employment activities unless to do so would cause an undue hardship.

9. Selection of Subcontractors, Procurement of Materials and Leasing of Equipment: The contractor shall not discriminate on the grounds of race, color, religion, sex, national origin, age or disability in the selection and retention of subcontractors, including procurement of materials and leases of equipment. The contractor shall take all necessary and reasonable steps to ensure nondiscrimination in the administration of this contract.

a. The contractor shall notify all potential subcontractors and suppliers and lessors of their EEO obligations under this contract.

b. The contractor will use good faith efforts to ensure subcontractor compliance with their EEO obligations.

10. Assurance Required by 49 CFR 26.13(b):

a. The requirements of 49 CFR Part 26 and the State DOT's U.S. DOT-approved DBE program are incorporated by reference.

b. The contractor or subcontractor shall not discriminate on the basis of race, color, national origin, or sex in the performance of this contract. The contractor shall carry out applicable requirements of 49 CFR Part 26 in the award and administration of DOT-assisted contracts. Failure by the contractor to carry out these requirements is a material breach of this contract, which may result in the termination of this contract or such other remedy as the contracting agency deems appropriate.

11. Records and Reports: The contractor shall keep such records as necessary to document compliance with the EEO requirements. Such records shall be retained for a period of three years following the date of the final payment to the contractor for all contract work and shall be available at reasonable times and places for inspection by authorized representatives of the contracting agency and the FHWA.

a. The records kept by the contractor shall document the following:

(1) The number and work hours of minority and non-minority group members and women employed in each work classification on the project;

(2) The progress and efforts being made in cooperation with unions, when applicable, to increase employment opportunities for minorities and women; and

(3) The progress and efforts being made in locating, hiring, training, qualifying, and upgrading minorities and women;

b. The contractors and subcontractors will submit an annual report to the contracting agency each July for the duration of the project, indicating the number of minority, women, and non-minority group employees currently engaged in each work classification required by the contract work. This information is to be reported on [Form FHWA-1391](#). The staffing data should represent the project work force on board in all or any part of the last payroll period preceding the end of July. If on-the-job training is being required by special provision, the contractor

will be required to collect and report training data. The employment data should reflect the work force on board during all or any part of the last payroll period preceding the end of July.

III. NONSEGREGATED FACILITIES

This provision is applicable to all Federal-aid construction contracts and to all related construction subcontracts of \$10,000 or more.

The contractor must ensure that facilities provided for employees are provided in such a manner that segregation on the basis of race, color, religion, sex, or national origin cannot result. The contractor may neither require such segregated use by written or oral policies nor tolerate such use by employee custom. The contractor's obligation extends further to ensure that its employees are not assigned to perform their services at any location, under the contractor's control, where the facilities are segregated. The term "facilities" includes waiting rooms, work areas, restaurants and other eating areas, time clocks, restrooms, washrooms, locker rooms, and other storage or dressing areas, parking lots, drinking fountains, recreation or entertainment areas, transportation, and housing provided for employees. The contractor shall provide separate or single-user restrooms and necessary dressing or sleeping areas to assure privacy between sexes.

IV. DAVIS-BACON AND RELATED ACT PROVISIONS

This section is applicable to all Federal-aid construction projects exceeding \$2,000 and to all related subcontracts and lower-tier subcontracts (regardless of subcontract size). The requirements apply to all projects located within the right-of-way of a roadway that is functionally classified as Federal-aid highway. This excludes roadways functionally classified as local roads or rural minor collectors, which are exempt. Contracting agencies may elect to apply these requirements to other projects.

The following provisions are from the U.S. Department of Labor regulations in 29 CFR 5.5 "Contract provisions and related matters" with minor revisions to conform to the FHWA-1273 format and FHWA program requirements.

1. Minimum wages

a. All laborers and mechanics employed or working upon the site of the work, will be paid unconditionally and not less often than once a week, and without subsequent deduction or rebate on any account (except such payroll deductions as are permitted by regulations issued by the Secretary of Labor under the Copeland Act (29 CFR part 3)), the full amount of wages and bona fide fringe benefits (or cash equivalents thereof) due at time of payment computed at rates not less than those contained in the wage determination of the Secretary of Labor which is attached hereto and made a part hereof, regardless of any contractual relationship which may be alleged to exist between the contractor and such laborers and mechanics.

Contributions made or costs reasonably anticipated for bona fide fringe benefits under section 1(b)(2) of the Davis-Bacon Act on behalf of laborers or mechanics are considered wages

paid to such laborers or mechanics, subject to the provisions of paragraph 1.d. of this section; also, regular contributions made or costs incurred for more than a weekly period (but not less often than quarterly) under plans, funds, or programs which cover the particular weekly period, are deemed to be constructively made or incurred during such weekly period. Such laborers and mechanics shall be paid the appropriate wage rate and fringe benefits on the wage determination for the classification of work actually performed, without regard to skill, except as provided in 29 CFR 5.5(a)(4). Laborers or mechanics performing work in more than one classification may be compensated at the rate specified for each classification for the time actually worked therein: Provided, That the employer's payroll records accurately set forth the time spent in each classification in which work is performed. The wage determination (including any additional classification and wage rates conformed under paragraph 1.b. of this section) and the Davis-Bacon poster (WH-1321) shall be posted at all times by the contractor and its subcontractors at the site of the work in a prominent and accessible place where it can be easily seen by the workers.

b. (1) The contracting officer shall require that any class of laborers or mechanics, including helpers, which is not listed in the wage determination and which is to be employed under the contract shall be classified in conformance with the wage determination. The contracting officer shall approve an additional classification and wage rate and fringe benefits therefore only when the following criteria have been met:

(i) The work to be performed by the classification requested is not performed by a classification in the wage determination; and

(ii) The classification is utilized in the area by the construction industry; and

(iii) The proposed wage rate, including any bona fide fringe benefits, bears a reasonable relationship to the wage rates contained in the wage determination.

(2) If the contractor and the laborers and mechanics to be employed in the classification (if known), or their representatives, and the contracting officer agree on the classification and wage rate (including the amount designated for fringe benefits where appropriate), a report of the action taken shall be sent by the contracting officer to the Administrator of the Wage and Hour Division, Employment Standards Administration, U.S. Department of Labor, Washington, DC 20210. The Administrator, or an authorized representative, will approve, modify, or disapprove every additional classification action within 30 days of receipt and so advise the contracting officer or will notify the contracting officer within the 30-day period that additional time is necessary.

(3) In the event the contractor, the laborers or mechanics to be employed in the classification or their representatives, and the contracting officer do not agree on the proposed classification and wage rate (including the amount designated for fringe benefits, where appropriate), the contracting officer shall refer the questions, including the views of all interested parties and the recommendation of the contracting officer, to the Wage and Hour Administrator for determination. The Wage and Hour Administrator, or an authorized representative, will issue a determination within 30 days of receipt and so advise

the contracting officer or will notify the contracting officer within the 30-day period that additional time is necessary.

(4) The wage rate (including fringe benefits where appropriate) determined pursuant to paragraphs 1.b.(2) or 1.b.(3) of this section, shall be paid to all workers performing work in the classification under this contract from the first day on which work is performed in the classification.

c. Whenever the minimum wage rate prescribed in the contract for a class of laborers or mechanics includes a fringe benefit which is not expressed as an hourly rate, the contractor shall either pay the benefit as stated in the wage determination or shall pay another bona fide fringe benefit or an hourly cash equivalent thereof.

d. If the contractor does not make payments to a trustee or other third person, the contractor may consider as part of the wages of any laborer or mechanic the amount of any costs reasonably anticipated in providing bona fide fringe benefits under a plan or program, Provided, That the Secretary of Labor has found, upon the written request of the contractor, that the applicable standards of the Davis-Bacon Act have been met. The Secretary of Labor may require the contractor to set aside in a separate account assets for the meeting of obligations under the plan or program.

2. Withholding

The contracting agency shall upon its own action or upon written request of an authorized representative of the Department of Labor, withhold or cause to be withheld from the contractor under this contract, or any other Federal contract with the same prime contractor, or any other federally-assisted contract subject to Davis-Bacon prevailing wage requirements, which is held by the same prime contractor, so much of the accrued payments or advances as may be considered necessary to pay laborers and mechanics, including apprentices, trainees, and helpers, employed by the contractor or any subcontractor the full amount of wages required by the contract. In the event of failure to pay any laborer or mechanic, including any apprentice, trainee, or helper, employed or working on the site of the work, all or part of the wages required by the contract, the contracting agency may, after written notice to the contractor, take such action as may be necessary to cause the suspension of any further payment, advance, or guarantee of funds until such violations have ceased.

3. Payrolls and basic records

a. Payrolls and basic records relating thereto shall be maintained by the contractor during the course of the work and preserved for a period of three years thereafter for all laborers and mechanics working at the site of the work. Such records shall contain the name, address, and social security number of each such worker, his or her correct classification, hourly rates of wages paid (including rates of contributions or costs anticipated for bona fide fringe benefits or cash equivalents thereof of the types described in section 1(b)(2)(B) of the Davis-Bacon Act), daily and weekly number of hours worked, deductions made and actual wages paid. Whenever the Secretary of Labor has found under 29 CFR 5.5(a)(1)(iv) that the wages of any laborer or mechanic include the amount of any costs reasonably anticipated in providing benefits under a plan or program described in section 1(b)(2)(B) of the Davis-

Bacon Act, the contractor shall maintain records which show that the commitment to provide such benefits is enforceable, that the plan or program is financially responsible, and that the plan or program has been communicated in writing to the laborers or mechanics affected, and records which show the costs anticipated or the actual cost incurred in providing such benefits. Contractors employing apprentices or trainees under approved programs shall maintain written evidence of the registration of apprenticeship programs and certification of trainee programs, the registration of the apprentices and trainees, and the ratios and wage rates prescribed in the applicable programs.

b. (1) The contractor shall submit weekly for each week in which any contract work is performed a copy of all payrolls to the contracting agency. The payrolls submitted shall set out accurately and completely all of the information required to be maintained under 29 CFR 5.5(a)(3)(i), except that full social security numbers and home addresses shall not be included on weekly transmittals. Instead the payrolls shall only need to include an individually identifying number for each employee (e.g. , the last four digits of the employee's social security number). The required weekly payroll information may be submitted in any form desired. Optional Form WH-347 is available for this purpose from the Wage and Hour Division Web site at <http://www.dol.gov/esa/whd/forms/wh347instr.htm> or its successor site. The prime contractor is responsible for the submission of copies of payrolls by all subcontractors. Contractors and subcontractors shall maintain the full social security number and current address of each covered worker, and shall provide them upon request to the contracting agency for transmission to the State DOT, the FHWA or the Wage and Hour Division of the Department of Labor for purposes of an investigation or audit of compliance with prevailing wage requirements. It is not a violation of this section for a prime contractor to require a subcontractor to provide addresses and social security numbers to the prime contractor for its own records, without weekly submission to the contracting agency..

(2) Each payroll submitted shall be accompanied by a "Statement of Compliance," signed by the contractor or subcontractor or his or her agent who pays or supervises the payment of the persons employed under the contract and shall certify the following:

(i) That the payroll for the payroll period contains the information required to be provided under §5.5 (a)(3)(ii) of Regulations, 29 CFR part 5, the appropriate information is being maintained under §5.5 (a)(3)(i) of Regulations, 29 CFR part 5, and that such information is correct and complete;

(ii) That each laborer or mechanic (including each helper, apprentice, and trainee) employed on the contract during the payroll period has been paid the full weekly wages earned, without rebate, either directly or indirectly, and that no deductions have been made either directly or indirectly from the full wages earned, other than permissible deductions as set forth in Regulations, 29 CFR part 3;

(iii) That each laborer or mechanic has been paid not less than the applicable wage rates and fringe benefits or cash equivalents for the classification of work performed, as specified in the applicable wage determination incorporated into the contract.

(3) The weekly submission of a properly executed certification set forth on the reverse side of Optional Form WH-347 shall satisfy the requirement for submission of the "Statement of Compliance" required by paragraph 3.b.(2) of this section.

(4) The falsification of any of the above certifications may subject the contractor or subcontractor to civil or criminal prosecution under section 1001 of title 18 and section 231 of title 31 of the United States Code.

c. The contractor or subcontractor shall make the records required under paragraph 3.a. of this section available for inspection, copying, or transcription by authorized representatives of the contracting agency, the State DOT, the FHWA, or the Department of Labor, and shall permit such representatives to interview employees during working hours on the job. If the contractor or subcontractor fails to submit the required records or to make them available, the FHWA may, after written notice to the contractor, the contracting agency or the State DOT, take such action as may be necessary to cause the suspension of any further payment, advance, or guarantee of funds. Furthermore, failure to submit the required records upon request or to make such records available may be grounds for debarment action pursuant to 29 CFR 5.12.

4. Apprentices and trainees

a. Apprentices (programs of the USDOL).

Apprentices will be permitted to work at less than the predetermined rate for the work they performed when they are employed pursuant to and individually registered in a bona fide apprenticeship program registered with the U.S. Department of Labor, Employment and Training Administration, Office of Apprenticeship Training, Employer and Labor Services, or with a State Apprenticeship Agency recognized by the Office, or if a person is employed in his or her first 90 days of probationary employment as an apprentice in such an apprenticeship program, who is not individually registered in the program, but who has been certified by the Office of Apprenticeship Training, Employer and Labor Services or a State Apprenticeship Agency (where appropriate) to be eligible for probationary employment as an apprentice.

The allowable ratio of apprentices to journeymen on the job site in any craft classification shall not be greater than the ratio permitted to the contractor as to the entire work force under the registered program. Any worker listed on a payroll at an apprentice wage rate, who is not registered or otherwise employed as stated above, shall be paid not less than the applicable wage rate on the wage determination for the classification of work actually performed. In addition, any apprentice performing work on the job site in excess of the ratio permitted under the registered program shall be paid not less than the applicable wage rate on the wage determination for the work actually performed. Where a contractor is performing construction on a project in a locality other than that in which its program is registered, the ratios and wage rates (expressed in percentages of the journeyman's hourly rate) specified in the contractor's or subcontractor's registered program shall be observed.

Every apprentice must be paid at not less than the rate specified in the registered program for the apprentice's level of progress, expressed as a percentage of the journeymen hourly

rate specified in the applicable wage determination. Apprentices shall be paid fringe benefits in accordance with the provisions of the apprenticeship program. If the apprenticeship program does not specify fringe benefits, apprentices must be paid the full amount of fringe benefits listed on the wage determination for the applicable classification. If the Administrator determines that a different practice prevails for the applicable apprentice classification, fringes shall be paid in accordance with that determination.

In the event the Office of Apprenticeship Training, Employer and Labor Services, or a State Apprenticeship Agency recognized by the Office, withdraws approval of an apprenticeship program, the contractor will no longer be permitted to utilize apprentices at less than the applicable predetermined rate for the work performed until an acceptable program is approved.

b. Trainees (programs of the USDOL).

Except as provided in 29 CFR 5.16, trainees will not be permitted to work at less than the predetermined rate for the work performed unless they are employed pursuant to and individually registered in a program which has received prior approval, evidenced by formal certification by the U.S. Department of Labor, Employment and Training Administration.

The ratio of trainees to journeymen on the job site shall not be greater than permitted under the plan approved by the Employment and Training Administration.

Every trainee must be paid at not less than the rate specified in the approved program for the trainee's level of progress, expressed as a percentage of the journeyman hourly rate specified in the applicable wage determination. Trainees shall be paid fringe benefits in accordance with the provisions of the trainee program. If the trainee program does not mention fringe benefits, trainees shall be paid the full amount of fringe benefits listed on the wage determination unless the Administrator of the Wage and Hour Division determines that there is an apprenticeship program associated with the corresponding journeyman wage rate on the wage determination which provides for less than full fringe benefits for apprentices. Any employee listed on the payroll at a trainee rate who is not registered and participating in a training plan approved by the Employment and Training Administration shall be paid not less than the applicable wage rate on the wage determination for the classification of work actually performed. In addition, any trainee performing work on the job site in excess of the ratio permitted under the registered program shall be paid not less than the applicable wage rate on the wage determination for the work actually performed.

In the event the Employment and Training Administration withdraws approval of a training program, the contractor will no longer be permitted to utilize trainees at less than the applicable predetermined rate for the work performed until an acceptable program is approved.

c. Equal employment opportunity. The utilization of apprentices, trainees and journeymen under this part shall be in conformity with the equal employment opportunity requirements of Executive Order 11246, as amended, and 29 CFR part 30.

d. Apprentices and Trainees (programs of the U.S. DOT).

Apprentices and trainees working under apprenticeship and skill training programs which have been certified by the Secretary of Transportation as promoting EEO in connection with Federal-aid highway construction programs are not subject to the requirements of paragraph 4 of this Section IV. The straight time hourly wage rates for apprentices and trainees under such programs will be established by the particular programs. The ratio of apprentices and trainees to journeymen shall not be greater than permitted by the terms of the particular program.

5. Compliance with Copeland Act requirements. The contractor shall comply with the requirements of 29 CFR part 3, which are incorporated by reference in this contract.

6. Subcontracts. The contractor or subcontractor shall insert Form FHWA-1273 in any subcontracts and also require the subcontractors to include Form FHWA-1273 in any lower tier subcontracts. The prime contractor shall be responsible for the compliance by any subcontractor or lower tier subcontractor with all the contract clauses in 29 CFR 5.5.

7. Contract termination: debarment. A breach of the contract clauses in 29 CFR 5.5 may be grounds for termination of the contract, and for debarment as a contractor and a subcontractor as provided in 29 CFR 5.12.

8. Compliance with Davis-Bacon and Related Act requirements. All rulings and interpretations of the Davis-Bacon and Related Acts contained in 29 CFR parts 1, 3, and 5 are herein incorporated by reference in this contract.

9. Disputes concerning labor standards. Disputes arising out of the labor standards provisions of this contract shall not be subject to the general disputes clause of this contract. Such disputes shall be resolved in accordance with the procedures of the Department of Labor set forth in 29 CFR parts 5, 6, and 7. Disputes within the meaning of this clause include disputes between the contractor (or any of its subcontractors) and the contracting agency, the U.S. Department of Labor, or the employees or their representatives.

10. Certification of eligibility.

a. By entering into this contract, the contractor certifies that neither it (nor he or she) nor any person or firm who has an interest in the contractor's firm is a person or firm ineligible to be awarded Government contracts by virtue of section 3(a) of the Davis-Bacon Act or 29 CFR 5.12(a)(1).

b. No part of this contract shall be subcontracted to any person or firm ineligible for award of a Government contract by virtue of section 3(a) of the Davis-Bacon Act or 29 CFR 5.12(a)(1).

c. The penalty for making false statements is prescribed in the U.S. Criminal Code, 18 U.S.C. 1001.

V. CONTRACT WORK HOURS AND SAFETY STANDARDS ACT

The following clauses apply to any Federal-aid construction contract in an amount in excess of \$100,000 and subject to the overtime provisions of the Contract Work Hours and Safety Standards Act. These clauses shall be inserted in addition to the clauses required by 29 CFR 5.5(a) or 29 CFR 4.6. As used in this paragraph, the terms laborers and mechanics include watchmen and guards.

1. Overtime requirements. No contractor or subcontractor contracting for any part of the contract work which may require or involve the employment of laborers or mechanics shall require or permit any such laborer or mechanic in any workweek in which he or she is employed on such work to work in excess of forty hours in such workweek unless such laborer or mechanic receives compensation at a rate not less than one and one-half times the basic rate of pay for all hours worked in excess of forty hours in such workweek.

2. Violation; liability for unpaid wages; liquidated damages. In the event of any violation of the clause set forth in paragraph (1.) of this section, the contractor and any subcontractor responsible therefor shall be liable for the unpaid wages. In addition, such contractor and subcontractor shall be liable to the United States (in the case of work done under contract for the District of Columbia or a territory, to such District or to such territory), for liquidated damages. Such liquidated damages shall be computed with respect to each individual laborer or mechanic, including watchmen and guards, employed in violation of the clause set forth in paragraph (1.) of this section, in the sum of \$10 for each calendar day on which such individual was required or permitted to work in excess of the standard workweek of forty hours without payment of the overtime wages required by the clause set forth in paragraph (1.) of this section.

3. Withholding for unpaid wages and liquidated damages. The FHWA or the contacting agency shall upon its own action or upon written request of an authorized representative of the Department of Labor withhold or cause to be withheld, from any moneys payable on account of work performed by the contractor or subcontractor under any such contract or any other Federal contract with the same prime contractor, or any other federally-assisted contract subject to the Contract Work Hours and Safety Standards Act, which is held by the same prime contractor, such sums as may be determined to be necessary to satisfy any liabilities of such contractor or subcontractor for unpaid wages and liquidated damages as provided in the clause set forth in paragraph (2.) of this section.

4. Subcontracts. The contractor or subcontractor shall insert in any subcontracts the clauses set forth in paragraph (1.) through (4.) of this section and also a clause requiring the subcontractors to include these clauses in any lower tier subcontracts. The prime contractor shall be responsible for compliance by any subcontractor or lower tier subcontractor with the clauses set forth in paragraphs (1.) through (4.) of this section.

VI. SUBLETTING OR ASSIGNING THE CONTRACT

This provision is applicable to all Federal-aid construction contracts on the National Highway System.

1. The contractor shall perform with its own organization contract work amounting to not less than 30 percent (or a greater percentage if specified elsewhere in the contract) of the total original contract price, excluding any specialty items designated by the contracting agency. Specialty items may be performed by subcontract and the amount of any such specialty items performed may be deducted from the total original contract price before computing the amount of work required to be performed by the contractor's own organization (23 CFR 635.116).

a. The term "perform work with its own organization" refers to workers employed or leased by the prime contractor, and equipment owned or rented by the prime contractor, with or without operators. Such term does not include employees or equipment of a subcontractor or lower tier subcontractor, agents of the prime contractor, or any other assignees. The term may include payments for the costs of hiring leased employees from an employee leasing firm meeting all relevant Federal and State regulatory requirements. Leased employees may only be included in this term if the prime contractor meets all of the following conditions:

(1) the prime contractor maintains control over the supervision of the day-to-day activities of the leased employees;

(2) the prime contractor remains responsible for the quality of the work of the leased employees;

(3) the prime contractor retains all power to accept or exclude individual employees from work on the project; and

(4) the prime contractor remains ultimately responsible for the payment of predetermined minimum wages, the submission of payrolls, statements of compliance and all other Federal regulatory requirements.

b. "Specialty Items" shall be construed to be limited to work that requires highly specialized knowledge, abilities, or equipment not ordinarily available in the type of contracting organizations qualified and expected to bid or propose on the contract as a whole and in general are to be limited to minor components of the overall contract.

2. The contract amount upon which the requirements set forth in paragraph (1) of Section VI is computed includes the cost of material and manufactured products which are to be purchased or produced by the contractor under the contract provisions.

3. The contractor shall furnish (a) a competent superintendent or supervisor who is employed by the firm, has full authority to direct performance of the work in accordance with the contract requirements, and is in charge of all construction operations (regardless of who performs the work) and (b) such other of its own organizational resources (supervision, management, and engineering services) as the contracting officer determines is necessary to assure the performance of the contract.

4. No portion of the contract shall be sublet, assigned or otherwise disposed of except with the written consent of the contracting officer, or authorized representative, and such consent when given shall not be construed to relieve the contractor of any responsibility for the fulfillment of the contract. Written consent will be given only after the

contracting agency has assured that each subcontract is evidenced in writing and that it contains all pertinent provisions and requirements of the prime contract.

5. The 30% self-performance requirement of paragraph (1) is not applicable to design-build contracts; however, contracting agencies may establish their own self-performance requirements.

VII. SAFETY: ACCIDENT PREVENTION

This provision is applicable to all Federal-aid construction contracts and to all related subcontracts.

1. In the performance of this contract the contractor shall comply with all applicable Federal, State, and local laws governing safety, health, and sanitation (23 CFR 635). The contractor shall provide all safeguards, safety devices and protective equipment and take any other needed actions as it determines, or as the contracting officer may determine, to be reasonably necessary to protect the life and health of employees on the job and the safety of the public and to protect property in connection with the performance of the work covered by the contract.

2. It is a condition of this contract, and shall be made a condition of each subcontract, which the contractor enters into pursuant to this contract, that the contractor and any subcontractor shall not permit any employee, in performance of the contract, to work in surroundings or under conditions which are unsanitary, hazardous or dangerous to his/her health or safety, as determined under construction safety and health standards (29 CFR 1926) promulgated by the Secretary of Labor, in accordance with Section 107 of the Contract Work Hours and Safety Standards Act (40 U.S.C. 3704).

3. Pursuant to 29 CFR 1926.3, it is a condition of this contract that the Secretary of Labor or authorized representative thereof, shall have right of entry to any site of contract performance to inspect or investigate the matter of compliance with the construction safety and health standards and to carry out the duties of the Secretary under Section 107 of the Contract Work Hours and Safety Standards Act (40 U.S.C.3704).

VIII. FALSE STATEMENTS CONCERNING HIGHWAY PROJECTS

This provision is applicable to all Federal-aid construction contracts and to all related subcontracts.

In order to assure high quality and durable construction in conformity with approved plans and specifications and a high degree of reliability on statements and representations made by engineers, contractors, suppliers, and workers on Federal-aid highway projects, it is essential that all persons concerned with the project perform their functions as carefully, thoroughly, and honestly as possible. Willful falsification, distortion, or misrepresentation with respect to any facts related to the project is a violation of Federal law. To prevent any misunderstanding regarding the seriousness of these and similar acts, Form FHWA-1022 shall be posted on each Federal-aid highway project (23 CFR 635) in one or more places where it is readily available to all persons concerned with the project:

18 U.S.C. 1020 reads as follows:

"Whoever, being an officer, agent, or employee of the United States, or of any State or Territory, or whoever, whether a person, association, firm, or corporation, knowingly makes any false statement, false representation, or false report as to the character, quality, quantity, or cost of the material used or to be used, or the quantity or quality of the work performed or to be performed, or the cost thereof in connection with the submission of plans, maps, specifications, contracts, or costs of construction on any highway or related project submitted for approval to the Secretary of Transportation; or

Whoever knowingly makes any false statement, false representation, false report or false claim with respect to the character, quality, quantity, or cost of any work performed or to be performed, or materials furnished or to be furnished, in connection with the construction of any highway or related project approved by the Secretary of Transportation; or

Whoever knowingly makes any false statement or false representation as to material fact in any statement, certificate, or report submitted pursuant to provisions of the Federal-aid Roads Act approved July 1, 1916, (39 Stat. 355), as amended and supplemented;

Shall be fined under this title or imprisoned not more than 5 years or both."

IX. IMPLEMENTATION OF CLEAN AIR ACT AND FEDERAL WATER POLLUTION CONTROL ACT

This provision is applicable to all Federal-aid construction contracts and to all related subcontracts.

By submission of this bid/proposal or the execution of this contract, or subcontract, as appropriate, the bidder, proposer, Federal-aid construction contractor, or subcontractor, as appropriate, will be deemed to have stipulated as follows:

1. That any person who is or will be utilized in the performance of this contract is not prohibited from receiving an award due to a violation of Section 508 of the Clean Water Act or Section 306 of the Clean Air Act.

2. That the contractor agrees to include or cause to be included the requirements of paragraph (1) of this Section X in every subcontract, and further agrees to take such action as the contracting agency may direct as a means of enforcing such requirements.

X. CERTIFICATION REGARDING DEBARMENT, SUSPENSION, INELIGIBILITY AND VOLUNTARY EXCLUSION

This provision is applicable to all Federal-aid construction contracts, design-build contracts, subcontracts, lower-tier subcontracts, purchase orders, lease agreements, consultant contracts or any other covered transaction requiring FHWA approval or that is estimated to cost \$25,000 or more – as defined in 2 CFR Parts 180 and 1200.

1. Instructions for Certification – First Tier Participants:

a. By signing and submitting this proposal, the prospective first tier participant is providing the certification set out below.

b. The inability of a person to provide the certification set out below will not necessarily result in denial of participation in this

covered transaction. The prospective first tier participant shall submit an explanation of why it cannot provide the certification set out below. The certification or explanation will be considered in connection with the department or agency's determination whether to enter into this transaction. However, failure of the prospective first tier participant to furnish a certification or an explanation shall disqualify such a person from participation in this transaction.

c. The certification in this clause is a material representation of fact upon which reliance was placed when the contracting agency determined to enter into this transaction. If it is later determined that the prospective participant knowingly rendered an erroneous certification, in addition to other remedies available to the Federal Government, the contracting agency may terminate this transaction for cause of default.

d. The prospective first tier participant shall provide immediate written notice to the contracting agency to whom this proposal is submitted if any time the prospective first tier participant learns that its certification was erroneous when submitted or has become erroneous by reason of changed circumstances.

e. The terms "covered transaction," "debarred," "suspended," "ineligible," "participant," "person," "principal," and "voluntarily excluded," as used in this clause, are defined in 2 CFR Parts 180 and 1200. "First Tier Covered Transactions" refers to any covered transaction between a grantee or subgrantee of Federal funds and a participant (such as the prime or general contractor). "Lower Tier Covered Transactions" refers to any covered transaction under a First Tier Covered Transaction (such as subcontracts). "First Tier Participant" refers to the participant who has entered into a covered transaction with a grantee or subgrantee of Federal funds (such as the prime or general contractor). "Lower Tier Participant" refers any participant who has entered into a covered transaction with a First Tier Participant or other Lower Tier Participants (such as subcontractors and suppliers).

f. The prospective first tier participant agrees by submitting this proposal that, should the proposed covered transaction be entered into, it shall not knowingly enter into any lower tier covered transaction with a person who is debarred, suspended, declared ineligible, or voluntarily excluded from participation in this covered transaction, unless authorized by the department or agency entering into this transaction.

g. The prospective first tier participant further agrees by submitting this proposal that it will include the clause titled "Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion-Lower Tier Covered Transactions," provided by the department or contracting agency, entering into this covered transaction, without modification, in all lower tier covered transactions and in all solicitations for lower tier covered transactions exceeding the \$25,000 threshold.

h. A participant in a covered transaction may rely upon a certification of a prospective participant in a lower tier covered transaction that is not debarred, suspended, ineligible, or voluntarily excluded from the covered transaction, unless it knows that the certification is erroneous. A participant is responsible for ensuring that its principals are not suspended, debarred, or otherwise ineligible to participate in covered transactions. To verify the eligibility of its principals, as well as the eligibility of any lower tier prospective participants, each participant may, but is not required to, check the Excluded Parties List System website (<https://www.epls.gov/>), which is compiled by the General Services Administration.

i. Nothing contained in the foregoing shall be construed to require the establishment of a system of records in order to render in good faith the certification required by this clause. The knowledge and information of the prospective participant is not required to exceed that which is normally possessed by a prudent person in the ordinary course of business dealings.

j. Except for transactions authorized under paragraph (f) of these instructions, if a participant in a covered transaction knowingly enters into a lower tier covered transaction with a person who is suspended, debarred, ineligible, or voluntarily excluded from participation in this transaction, in addition to other remedies available to the Federal Government, the department or agency may terminate this transaction for cause or default.

* * * * *

2. Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion – First Tier Participants:

a. The prospective first tier participant certifies to the best of its knowledge and belief, that it and its principals:

(1) Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participating in covered transactions by any Federal department or agency;

(2) Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, or receiving stolen property;

(3) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated in paragraph (a)(2) of this certification; and

(4) Have not within a three-year period preceding this application/proposal had one or more public transactions (Federal, State or local) terminated for cause or default.

b. Where the prospective participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

2. Instructions for Certification - Lower Tier Participants:

(Applicable to all subcontracts, purchase orders and other lower tier transactions requiring prior FHWA approval or estimated to cost \$25,000 or more - 2 CFR Parts 180 and 1200)

a. By signing and submitting this proposal, the prospective lower tier is providing the certification set out below.

b. The certification in this clause is a material representation of fact upon which reliance was placed when this transaction was entered into. If it is later determined that the prospective lower tier participant knowingly rendered an erroneous certification, in addition to other remedies available to the Federal Government, the department, or agency with which

this transaction originated may pursue available remedies, including suspension and/or debarment.

c. The prospective lower tier participant shall provide immediate written notice to the person to which this proposal is submitted if at any time the prospective lower tier participant learns that its certification was erroneous by reason of changed circumstances.

d. The terms "covered transaction," "debarred," "suspended," "ineligible," "participant," "person," "principal," and "voluntarily excluded," as used in this clause, are defined in 2 CFR Parts 180 and 1200. You may contact the person to which this proposal is submitted for assistance in obtaining a copy of those regulations. "First Tier Covered Transactions" refers to any covered transaction between a grantee or subgrantee of Federal funds and a participant (such as the prime or general contract). "Lower Tier Covered Transactions" refers to any covered transaction under a First Tier Covered Transaction (such as subcontracts). "First Tier Participant" refers to the participant who has entered into a covered transaction with a grantee or subgrantee of Federal funds (such as the prime or general contractor). "Lower Tier Participant" refers any participant who has entered into a covered transaction with a First Tier Participant or other Lower Tier Participants (such as subcontractors and suppliers).

e. The prospective lower tier participant agrees by submitting this proposal that, should the proposed covered transaction be entered into, it shall not knowingly enter into any lower tier covered transaction with a person who is debarred, suspended, declared ineligible, or voluntarily excluded from participation in this covered transaction, unless authorized by the department or agency with which this transaction originated.

f. The prospective lower tier participant further agrees by submitting this proposal that it will include this clause titled "Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion-Lower Tier Covered Transaction," without modification, in all lower tier covered transactions and in all solicitations for lower tier covered transactions exceeding the \$25,000 threshold.

g. A participant in a covered transaction may rely upon a certification of a prospective participant in a lower tier covered transaction that is not debarred, suspended, ineligible, or voluntarily excluded from the covered transaction, unless it knows that the certification is erroneous. A participant is responsible for ensuring that its principals are not suspended, debarred, or otherwise ineligible to participate in covered transactions. To verify the eligibility of its principals, as well as the eligibility of any lower tier prospective participants, each participant may, but is not required to, check the Excluded Parties List System website (<https://www.epls.gov/>), which is compiled by the General Services Administration.

h. Nothing contained in the foregoing shall be construed to require establishment of a system of records in order to render in good faith the certification required by this clause. The knowledge and information of participant is not required to exceed that which is normally possessed by a prudent person in the ordinary course of business dealings.

i. Except for transactions authorized under paragraph e of these instructions, if a participant in a covered transaction knowingly enters into a lower tier covered transaction with a person who is suspended, debarred, ineligible, or voluntarily excluded from participation in this transaction, in addition to other remedies available to the Federal Government, the

department or agency with which this transaction originated may pursue available remedies, including suspension and/or debarment.

* * * * *

Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion--Lower Tier Participants:

1. The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participating in covered transactions by any Federal department or agency.

2. Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

* * * * *

XI. CERTIFICATION REGARDING USE OF CONTRACT FUNDS FOR LOBBYING

This provision is applicable to all Federal-aid construction contracts and to all related subcontracts which exceed \$100,000 (49 CFR 20).

1. The prospective participant certifies, by signing and submitting this bid or proposal, to the best of his or her knowledge and belief, that:

a. No Federal appropriated funds have been paid or will be paid, by or on behalf of the undersigned, to any person for influencing or attempting to influence an officer or employee of any Federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any Federal contract, the making of any Federal grant, the making of any Federal loan, the entering into of any cooperative agreement, and the extension, continuation, renewal, amendment, or modification of any Federal contract, grant, loan, or cooperative agreement.

b. If any funds other than Federal appropriated funds have been paid or will be paid to any person for influencing or attempting to influence an officer or employee of any Federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with this Federal contract, grant, loan, or cooperative agreement, the undersigned shall complete and submit Standard Form-LLL, "Disclosure Form to Report Lobbying," in accordance with its instructions.

2. This certification is a material representation of fact upon which reliance was placed when this transaction was made or entered into. Submission of this certification is a prerequisite for making or entering into this transaction imposed by 31 U.S.C. 1352. Any person who fails to file the required certification shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000 for each such failure.

3. The prospective participant also agrees by submitting its bid or proposal that the participant shall require that the language of this certification be included in all lower tier subcontracts, which exceed \$100,000 and that all such recipients shall certify and disclose accordingly.

**NOTICE OF REQUIREMENTS FOR AFFIRMATIVE
ACTION TO ENSURE EQUAL EMPLOYMENT
OPPORTUNITY (EXECUTIVE ORDER 11246)**

1. The Offeror’s or Bidder’s attention is called to the “Equal Opportunity Clause” and the “Standard Federal Equal Employment Opportunity Construction Contract Specifications” set forth herein.

2. The goal for female participation, expressed in percentage terms for the Contractor’s aggregate workforce in each trade on all construction work, is 6.9%.

Until further notice	Goals for minority participation for each trade (percent)
SHSA Cities:	
Pascagoula - Moss Point -----	16.9
Biloxi - Gulfport -----	19.2
Jackson -----	30.3
SMSA Counties:	
Desoto -----	32.3
Hancock, Harrison, Stone-----	19.2
Hinds, Rankin -----	30.3
Jackson -----	16.9
Non-SMSA Counties:	
George, Greene-----	26.4
Alcorn, Benton, Bolivar, Calhoun, Carroll, Chickasaw, Clay, Coahoma, Grenada, Itawamba, Lafayette, Lee, Leflore, Marshall, Monroe, Montgomery, Panola, Pontotoc, Prentiss, Quitman, Sunflower, Tallahatchie, Tate, Tippah, Tishomingo, Tunica, Union, Washington, Webster, Yalobusha -----	26.5
Attala, Choctaw, Claiborne, Clarke, Copiah, Covington, Franklin, Holmes, Humphreys, Issaquena, Jasper, Jefferson, Jefferson Davis, Jones Kemper, Lauderdale, Lawrence, Leake, Lincoln, Lowndes, Madison, Neshoba, Newton, Noxubee, Oktibbeha, Scott, Sharkey, Simpson, Smith, Warren, Wayne, Winston, Yazoo-----	32.0
Forrest, Lamar, Marion, Pearl River, Perry, Pike, Walthall-----	27.7
Adams, Amite, Wilkinson -----	30.4

These goals are applicable to all the Contractor’s construction work (whether or not it is Federal or federally assisted) performed in the covered area. If the contractor performs construction work in a geographical area located outside of the covered area, it shall apply the goals established for such geographical area where the work is actually performed. With regard to this second area, the contractor also is subject to the goals for both its federally involved and nonfederally involved construction.

The Contractor’s compliance with the Executive Order and the regulations in 41 CFR Part 60-4 shall be based on its implementation of the Equal Opportunity clause, specific affirmative action obligations required by the specifications set forth in 41 CFR 60-4.3(a), and its efforts to meet the goals. The hours of minority and female employment and training must be substantially uniform throughout the length of the contract, and in each trade, and the Contractor shall make a good faith effort to employ minorities and women evenly on each of its projects. The transfer of minority or female employees or trainees from Contractor to Contractor or from project to project for the sole purpose of meeting the Contractor’s goals shall be a violation of the contract, the Executive Order and the regulations in 41 CFR Part 60-4.2(d). Compliance with the goals will be measured against the total work hours performed.

3. The Contractor shall provide written notification to the Director of the Office of Federal Contract Compliance Programs within 10 working days of award of any construction subcontract in excess of \$10,000 at any tier for construction work under the contract resulting from this solicitation. The notification shall list the name, address and telephone number of the subcontractor, employer identification number of the subcontractor, estimated dollar amount of the subcontract; estimated starting and completion dates of the subcontract; and the geographical area in which the subcontract is to be performed.

4. As used in this Notice, and in the contract resulting from this solicitation, the “covered area” is to the county and city (if any), stated in the advertisement.

5. The notification required in Paragraph 3 shall be addressed to the following:

Contract Compliance Officer
Mississippi Department of Transportation
P.O. Box 1850
Jackson, Mississippi 39215-1850

(12/04/2018)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-102-2

CODE: (IS)

DATE: 11/22/2017

SUBJECT: **Bidding Requirements and Conditions**

Section 102, Bidding Requirements and Conditions, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-102.01--Prequalification of Bidders. Delete the last sentence of the third paragraph of Subsection 102.01 on page 13, and substitute the following.

The Bidder's Certificate of Responsibility number must be on file with the Department's Contract Administration Division prior to request for permission to bid.

907-102.02--Contents of Proposal Forms. Delete the fourth paragraph in Subsection 102.02 on page 13, and substitute the following.

Prospective bidders must complete an online request for permission to be eligible to bid a project. Upon approval, the bidder will be authorized to submit a bid electronically using Bid Express at <http://bidx.com>.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-109-1

CODE: (IS)

DATE: 05/08/2019

SUBJECT: Measurement and Payment

Section 109, Measurement and Payment, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-109.01--Measurement of Quantities. Delete the sixth full paragraph of Subsection 109.01 on page 88, and substitute the following.

If appropriate based on the specific circumstances of the project, the Contractor may request that material specified to be measured by the cubic yard or ton be converted to the other measure. The Contractor must submit this request to the Engineer. The Engineer will provide an approval or denial in writing. The decision is in the sole discretion of the Engineer. If approved, factors for this conversion will be determined by the District Materials Engineer and agreed to by the Contractor. The conversion of the materials along with the conversion factor will be incorporated into the Contract by supplemental agreement. The supplemental agreement must be executed before such method of measurement is used.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-701-2

CODE: (IS)

DATE: 01/08/2020

SUBJECT: Hydraulic Cement

Section 701, Hydraulic Cement, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-701.01--General. In the first sentence of the second paragraph of Subsection 701.01 on page 718, change “mills” to “plants.”

In the second sentence of the sixth paragraph of Subsection 701.01 on pages 718 and 719, change “shall” to “will.”

907-701.02--Portland Cement.

907-701.02.1-General.

907-701.02.1.2--Alkali Content. Delete the sentence in Subsection 701.02.1.2 on page 719, and substitute the following.

The Equivalent alkali content for all cement types in this Subsection shall not exceed 0.60%.

907-701.02.2--Replacement by Other Cementitious Materials. Delete the paragraph in Subsection 701.02.2 on page 719, and substitute the following.

The maximum replacement of cement by weight is 25% for fly ash or 50% for ground granulated blast furnace slag (GGBFS). Replacement contents below 20% for fly ash or 45% for GGBFS may be used, but will not be given any special considerations, such as the maximum acceptance temperature for portland cement concrete containing pozzolans in Subsection 804.02.13.1.5. Special considerations shall only apply for replacement of cement by fly ash or GGBFS.

Delete Subsection 701.02.2.1 on pages 719 and 720, and substitute the following.

907-701.02.2.1--Portland Cement Concrete Exposed to Soluble Sulfate Conditions or Seawater.

When portland cement concrete is exposed to moderate or severe soluble sulfate conditions, or to seawater, cement types and replacement of cement by Class F fly ash or GGBFS shall be as follows in Table 1. Class C fly ash shall not be used as a replacement for cement in any of the sulfate exposure conditions listed in Table 1.

Table 1- Cementitious Materials for Soluble Sulfate Conditions or Seawater

Sulfate Exposure	Water-soluble sulfate (SO ₄) in soil, % by mass	Sulfate (SO ₄) in water, ppm	Cementitious material required
Moderate and Seawater	0.10 - 0.20	150 - 1,500	Type I cement with one of the following replacements of cement by weight: 24.5 - 25.0% Class F fly ash, or 49.5 - 50.0% GGBFS or Type II ^{*,**} cement
Severe	0.20 - 2.00	1,500 - 10,000	Type I cement with a replacement by weight of 49.5 - 50.0% GGBFS, or Type II [*] cement with one of the following replacements of cement by weight: 24.5 - 25.0% Class F fly ash, or 49.5 - 50.0% GGBFS

* Type III cement conforming to AASHTO M85 with a maximum 8% tricalcium aluminate (C₃A) may be used in lieu of Type II cement as allowed in Subsection 701.02.1; this cement is given the designation “Type III(MS).”

** Class F fly ash or GGBFS may be added as a replacement for cement as allowed in Subsection 907-701.02.2.

Delete Subsection 701.02.2.2 on page 720, and substitute the following.

907-701.02.2.2--Portland Cement for Soil Stabilization Exposed to Soluble Sulfate Conditions or Seawater. When portland cement for use in soil stabilization is exposed to moderate or severe soluble sulfate conditions, or to seawater, cement types and replacement of cement by Class F fly ash or GGBFS shall meet the requirements of Subsection 701.02.2.1.

907-701.04--Blended Hydraulic Cement.

907-701.04.1--General. Delete Subsection 701.04.1.1 on page 720, and substitute the following.

907-701.04.1.1--Types of Blended Hydraulic Cement. Blended hydraulic cements (blended cements) shall be of the following types and conform to AASHTO M 240:

- Type IL – Portland-limestone cement
- Type IP – Portland-pozzolan cement
- Type IS – Portland blast-furnace slag cement

Blended cement Types IL, IP, and IS meeting the “MS” sulfate resistance requirement listed in AASHTO M 240, Table 3 shall have the “(MS)” suffix added to the type designation.

907-701.04.1.2--Alkali Content. Delete the sentence in Subsection 701.04.1.2 on page 720, and substitute the following.

All blended cement types shall be made with clinker that would result in cement meeting the requirements of Subsection 701.02.1.2 when used in the production of AASHTO M 85, Type I or Type II cement.

907-701.04.2--Replacement by Other Cementitious Materials. Delete the paragraph in Subsection 701.04.2 on page 720, and substitute the following.

The maximum replacement of blended cement Type IL by weight is 35% for fly ash or 50% for GGBFS. Replacement contents below 20% for fly ash or 45% for GGBFS may be used, but will not be given any special considerations, such as the maximum acceptance temperature for blended cement concrete containing pozzolans in Subsection 804.02.13.1.5. Special considerations shall only apply for replacement of blended cement by fly ash or GGBFS.

No additional cementitious materials, such as portland cement, blended cement, fly ash, GGBFS, or others, shall be added to or as a replacement for blended cement Types IP and IS.

Delete Subsection 701.04.2.1 on pages 720 and 721, and substitute the following.

907-701.04.2.1--Blended Cement Concrete Exposed to Soluble Sulfate Conditions or Seawater. When blended cement concrete is exposed to moderate or severe soluble sulfate conditions, or to seawater, cement types and replacement of cement by Class F fly ash or GGBFS shall be as follows in Table 2. Class C fly ash shall not be used as a replacement for cement in any of the sulfate exposure conditions listed in Table 2.

Table 2- Cementitious Materials for Soluble Sulfate Conditions or Seawater

Sulfate Exposure	Water-soluble sulfate (SO ₄) in soil, % by mass	Sulfate (SO ₄) in water, ppm	Cementitious material required
Moderate and Seawater	0.10 - 0.20	150 - 1,500	Type IL (MS)* cement, Type IL cement with one of the following replacements of cement by weight: 24.5 - 35.0% Class F fly ash, or 49.5 - 50.0% GGBFS, Type IP (MS) cement, or Type IS (MS) cement
Severe	0.20 - 2.00	1,500 - 10,000	Type IL cement with a replacement of cement by weight of 49.5 - 50.0% GGBFS, or Type IL (MS) cement with one of following replacements of cement by weight: 24.5 - 35.0% Class F fly ash, or 49.5 - 50.0% GGBFS

- * Class F fly ash or GGBFS may be added as a replacement for cement as allowed in Subsection 907-701.04.2.

Delete Subsection 701.04.2.2 on page 721, and substitute the following.

907-701.04.2.2--Blended Cement for Soil Stabilization Exposed to Soluble Sulfate Conditions or Seawater. When blended cement for use in soil stabilization is exposed to moderate or severe soluble sulfate conditions, or to seawater, cement types and replacement of cement by Class F fly ash or GGBFS shall meet the requirements of Subsection 701.04.2.1.

Delete Subsection 701.04.3 on page 721.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-702-4

CODE: (IS)

DATE: 09/11/2018

SUBJECT: Bituminous Materials

Section 702, Bituminous Materials, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-702.04--Sampling. Delete the sentence in Subsection 702.04 on page 722, and substitute the following.

Sampling of bituminous materials shall be as set out in AASHTO R 66.

907-702.07--Emulsified Asphalt. Delete the last sentence in Subsection 702.07 on page 724, and substitute the following.

Asphalt for fog seal shall conform to the requirements of Subsection 907-702.12, Table V.

907-702.12--Tables. Delete Table V in Subsection 702.12 on page 729, and substitute the following.

**TABLE V
SPECIFICATION FOR FOG SEAL**

Test Requirements	LD-7		CHPF-1		Test Method
	Min.	Max.	Min.	Max.	
Viscosity, Saybolt Furol, @ 25°C, Sec.	10	100	-	100	AASHTO T 72
Storage Stability Test, 24 hr, %	-	1	-	1	AASHTO T 59
Settlement, 5 day, %	-	5	-	-	AASHTO T 59
Oil Distillate, %	-	1	-	-	AASHTO T 59
Sieve Test, % *	-	0.3	-	0.1	AASHTO T 59
Residue by Distillation, %	40	-	40	-	AASHTO T 59
Test on Residue from Distillation					
Penetration @ 25°C, 100g, 5 sec	-	20	40	90	AASHTO T 49
Softening Point, °C	65	-	-	-	ASTM D 36
Solubility in trichloroethylene, %	97.5	-	-	-	AASHTO T 44
Elastic Recovery @ 25°C, %	-	-	40	-	AASHTO T 301
Original DSR @ 82° (G*/Sinδ, 10 rad/sec)	1	-	-	-	AASHTO T 111

* The Sieve Test result is tested for reporting purposes only and may be waived if no application problems are present in the field.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-703-1

CODE: (IS)

DATE: 06/13/2018

SUBJECT: Gradation

Section 703, Aggregates, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-703.03--Course Aggregates for Hydraulic Cement Concrete.

907-703.03.2--Detail Requirements.

907-703.03.2.4--Gradation. In the table in Subsection 703.03.2.4 on page 734, add 100 for the percent passing by weight on the 1½-inch sieve for Size No. 67 aggregates.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-705-1

CODE: (IS)

DATE: 06/13/2018

SUBJECT: Stone Riprap

Section 705, Stone Blanket Protection and Filter Blanket Materials, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-705.04--Stone Riprap. Delete the last sentence of the first paragraph of Subsection 705.04 on page 750, and substitute the following.

Quality requirements for rock to be furnished under these specifications will come from a pre-approved source and be visually approved prior to use.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-707-2

CODE: (IS)

DATE: 06/05/2019

SUBJECT: Joint Materials

Section 707, Joint Materials, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-707.02.3--Wood. Delete paragraph (b) of Subsection 707.02.3 on page 755, and substitute the following:

- (b) Dimensions shall be as shown on the plans. Dimensions shown on the plans are “dressed” sizes in accordance with Table 3 of the American Softwood Lumber Standard, SP-20. At the discretion of the Engineer, a 3/4-inch dressed board may be used in lieu of a 1-inch dressed board. A tolerance of plus or minus 1/16 inch thickness and plus or minus 1/8 inch width will be permitted. For slip-form paving a tolerance of minus 1/4 inch on each end in length will be permitted.

907-707.06--Flexible Plastic Gasket for Joining Conduit. Delete the third paragraph of Subsection 707.06 on page 756, and substitute the following.

The Department may require the performance test described in ASTM C 990.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-711-2

CODE: (IS)

DATE: 09/11/2018

SUBJECT: Plain Steel Wire

Section 711, Reinforcement and Wire Rope, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-711.02--Deformed and Plain Carbon-Steel Bars for Concrete Reinforcing.

907-711.02.3--Steel Welded and Non-Welded Wire Reinforcement, Plain and Deformed, for Concrete.

907-711.02.3.1--Plain Steel Wire. Delete the sentence in Subsection 711.02.3.1 on pages 780 and 781, and substitute the following.

Plain steel wire and plain steel welded wire shall conform to the requirements of AASHTO M 336.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-720-2

CODE: (IS)

DATE: 09/11/2018

SUBJECT: Acceptance Procedure for Glass Beads

Section 720, Pavement Marking Materials, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-720.01--Glass Beads.

907-720.01.4--Acceptance Procedures. Delete the last sentence of the paragraph in Subsection 720.01.4 on page 841, and substitute the following.

Acceptance sampling and testing of glass beads will be in accordance with the Department's Materials Division Inspection, Testing, and Certification Manual, Section 2.9.2 -- Glass Beads.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-721-2

CODE: (IS)

DATE: 01/08/2020

SUBJECT: Materials for Signing

Section 721, Materials for Signing, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-721.06--Reflective Sheeting.

907-721.06.2--Performance Requirements. Delete Table 4 and Table 5 in Subsection 721.06.2 on pages 860 & 861, and substitute the following.

**MINIMUM COEFFICIENTS OF RETROREFLECTION
Candela per foot candle per square foot (cd/ft²)
Per ASTM Designation D4956**

**TABLE 4
Type IX Sheeting**

Observation Angle	Entrance Angle	White	Yellow	Green	Red	Blue	Fluorescent Yellow/Green	Fluorescent Yellow	Fluorescent Orange
0.2°	-4.0°	380	285	38	76	17	300	230	115
0.2°	+30.0°	215	162	22	43	10	170	130	65
0.5°	-4.0°	240	180	24	48	11	190	145	72
0.5°	+30.0°	135	100	14	27	6.0	110	81	41
1.0°	-4.0°	80	60	8.0	16	3.6	64	48	24
1.0°	+30.0°	45	34	4.5	9.0	2.0	36	27	14

**TABLE 5
Type XI Sheeting**

Observation Angle	Entrance Angle	White	Yellow	Green	Red	Blue	Brown	Fluorescent Yellow/Green	Fluorescent Yellow	Fluorescent Orange
0.2°	-4.0°	580	435	58	87	26	17	460	350	175
0.2°	+30.0°	220	165	22	33	10	7.0	180	130	66
0.5°	-4.0°	420	315	42	63	19	13	340	250	125
0.5°	+30.0°	150	110	15	23	7.0	5.0	120	90	45
1.0°	-4.0°	120	90	12	18	5.0	4.0	96	72	36
1.0°	+30.0°	45	34	5.0	7.0	2.0	1.0	36	27	14

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-808-1

CODE: (IS)

DATE: 11/01/2018

SUBJECT: Joint Repair

Section 808, Joint Repair, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-808.04--Method of Measurement. Delete the paragraph in Subsection 808.04 on page 1009, and substitute the following.

When a pay item is included in the plans, joint repair will be measured by the linear foot and mortar mix will be measured by the gallon. The volume of measurement for the epoxy/sand mortar mix will be determined from the summation of the volumes of the epoxy components and the volume of sand will not be measured for payment.

907-808.05--Basis of Payment. Delete the paragraph in Subsection 808.05 on page 1009, and substitute the following.

When a pay item is included in the plans, joint repair, measured as prescribed above, will be paid for at the contract unit price per linear foot, which price shall be full compensation for furnishing and placing all materials, labor, tools, equipment, and all incidentals necessary to complete the work.

When a pay item is included in the plans, mortar mix, measured as prescribed above, will be paid for at the contract unit price per gallon, which price shall be full compensation for furnishing all materials including sand and forming materials, and all incidentals necessary to complete the work. No payment will be made for the sand used in the epoxy mortar mix.

The price bid for each item of work shall include the cost of continuous maintenance of traffic and protective services as required by the Department's Traffic Control Plan. This shall include all required individual traffic control devices.

Payment will be made under:

907-808-A: Joint Repair - per linear foot

907-808-B: Mortar Mix - per gallon

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISIONS NO. 907-823-6

CODE: (SP)

DATE: 07/18/2019

SUBJECT: **Preformed Joint Seal**

Section 907-823, Preformed Joint Seal, is hereby added to and becomes a part of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction as follows.

SECTION 907-823--PREFORMED JOINT SEAL

907-823.01--Description. This work consists of furnishing and installing preformed joint seals in accordance with these specifications and the details shown in the Plans or drawings provided.

907-823.02--Materials. The Contractor shall furnish a manufacturer's certification stating that the material used meets the requirements of this specification.

The preformed joint seal shall be one of the following, or an approved equal. The size of the seal, Type I or Type II, shall be determined based on the size of the joint opening, as detailed in the Plans or drawings provided. It is the Contractor's responsibility to ensure that the size selected is appropriate for the width of the joint. Type I shall be used for joint openings less than two inches (2"). Type II shall be used for joint openings greater than two inches (2"), with the maximum joint opening being two and one-half inches (2½"). In cases where the joint opening is greater than two and one-half inches (2½"), another type of expansion material shall be required as directed by the Director of Structures, State Bridge Engineer.

1. Silicoflex Joint Sealing System
Manufactured by R.J. Watson, Inc. in Alden, NY
www.rjwatson.com
2. Wabo@SPS Joint System
Manufactured by Watson Bowman Acme Corporation in Amherst, NY
www.wbacorp.com
3. Silspec SSS Silicone Strip Seal
Manufactured by SSI Commercial & Highway Construction Materials in Tulsa, OK
www.ssicm.com

907-823.03--Construction Methods. Preformed joint seals shall be installed in accordance with the manufacturer's recommendations. The material shall seal the deck surface, gutters, and curbs to prevent moisture or other contaminants from leaking through the joints. The joint seal shall be installed in such a manner that the top surface of the material is within the minimum and maximum depths below the roadway or bridge surface recommended by the manufacturer.

Saw cutting for the joint repair shall be accomplished by sawing at the locations and depth shown

on the joint repair detail sheets in the plans or in the contract documents. Saw cuts shall be as near vertical as possible at the saw line of the repair area. The saw cut depth shall be equivalent to the installation depth required by the manufacturer's specifications, and the type specified shall be the same as the type specified for preformed joint seal.

907-823.04--Method of Measurement. Preformed joint seal of the type specified will be measured in linear feet along the length of the centerline joint.

Saw cuts of the type specified will be measured by the linear foot along the length of the bridge deck on each side of the centerline joint.

907-823.05--Basis of Payment. Preformed joint seal, measured as prescribed above, will be paid for at the contract unit price per linear foot, which shall be full compensation for furnishing all labor, equipment, tools, materials, and incidentals necessary to complete the work.

Saw cuts, measured as prescribed above, will be paid for at the contract unit price per linear foot, which shall be full compensation for furnishing all labor, equipment, tools, materials, and incidentals necessary to complete the work.

Payment will be made under:

907-823-A: Preformed Joint Seal, Type ____ - per linear foot

907-823-B: Saw Cut, Type _____ - per linear foot

NOTES ON ASSOCIATED ITEMS OF WORK:

907-808-4002 JOINT REPAIR

Description:

Shall include the work necessary to repair joints in concrete bridge deck slabs. Epoxy mortar shall be included under this item of work. Removal of existing epoxy mortar, preparation and placement of new epoxy mortar shall be included under this item of work. All other requirements shall be in accordance with the applicable provisions of the specifications and any other sections specified therein.

Basis of Payment:

The accepted quantities will be paid for in linear feet at the contract unit price along the length of the bridge deck on each side of the centerline joint.

907-808-4003 JOINT REPAIR WITHOUT EPOXY

Description:

Shall include the work necessary to repair joints in concrete bridge deck slabs. Epoxy mortar shall be included under this item of work. Removal of existing epoxy mortar, preparation and placement of new epoxy mortar shall be included under this item of work. All other requirements shall be in accordance with the applicable provisions of the specifications and any other sections specified therein.

Basis of Payment:

The accepted quantities will be paid for in linear feet at the contract unit price along the length of the bridge deck on each side of the centerline joint.

907-823-0001 SAW CUT, TYPE I & 907-823-0002 SAW CUT, TYPE II

Description:

The saw cut depth shall be established to the full depth of the concrete. The saw cut shall be the same as the preformed joint seal selected.

Basis of Payment:

The accepted quantities will be paid for in linear feet at the contract unit price along the length of the bridge deck on each side of the centerline joint. It is the contractor's responsibility to ensure that the proper depth is selected based on the manufacturer's recommendations.

907-823-4001 PREFORMED JOINT SEAL, TYPE I

Description:

Shall include the manufacturer's required joint preparation including sandblasting both sides of the joint and blowing the preformed joint seal compressed air and placement of the new epoxy mortar.

Basis of Payment:

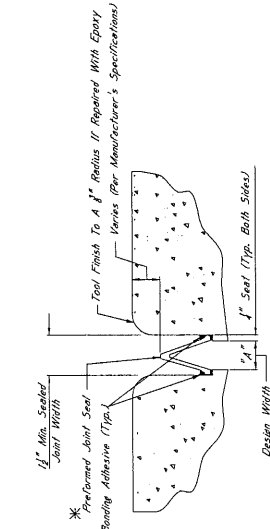
The accepted quantities will be paid for in linear feet at the contract unit price along the length of the centerline joint.

EPOXY MORTAR AND POLYMER CONCRETE NOTES:

Either epoxy mortar or polymer concrete may be used. Guidelines for selection of materials can be found in Section 808 of the specifications.

GENERAL NOTES:

- Specifications, Missouri Standard Specifications For Road And Bridge Construction 2017.
- No Change Of Plans Will Be Permitted Except By Written Approval From The Missouri Department Of Transportation. Any Changes To Detail Or Design Or Construction Procedure May Be Authorized By The Bridge Engineer. Proposed Such Changes Will Not Be Considered For Contract Award Unless They Have Not Be Paid For Directly And Shall Therefore Be Considered An Assembled Item of Work.

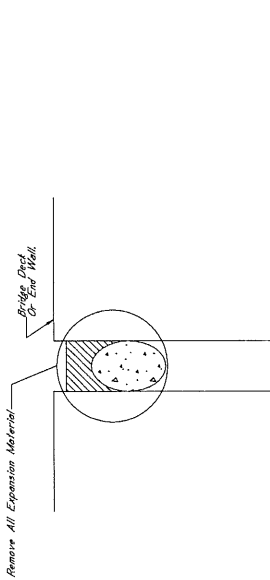


TYPICAL SECTION AT SAWCUT & SEALED JOINT

Showing Sealed Joint After Sawcut

***NOTES:**

- The preformed joint seal shall be one of the following, installed according to the manufacturer's specifications:
 1. Manufactured By Seal Systems, Inc. In Akron, NY
 www.seal.com
 2. Welo 593 Joint System
 Manufactured By Welo Bannum Acme Corporation In Amherst, NY
 www.welo.com
 3. Silgru 558-557 Silicone Strip Seal
 Manufactured By R.J. Watson, Inc. In Adam, NY
 www.rjwatson.com
 4. Welo 593 Joint System
 Manufactured By Welo Bannum Acme Corporation In Amherst, NY
 www.welo.com
- For Estimating Purposes, The R.J. Watson Silicone Joint Sealing System Was Specified. However, Should Another Supplier Be Chosen, It Is The Contractor's Responsibility To Verify That The Supplier's System Meets The Requirements For Joint Preparation, Installation Depth And Widths, Adhesive Setting Times, And Any Other Parameters Between The Specifications Applicable To The Manufacturer. To Ensure That The Contractor Is Properly Selected In Installation Of The Joint Seal.
- Joints Shall Be Sealed At Their Design Widths. Dimension "A" Which Is Defined As Seal Depth On Both Sides Of The Joint. The Preformed Joint Seal Type To Be Used For Design Purposes Shall Be The Same As The Preformed Joint Seal Type To Be Used In Cases Where Design Widths Are Greater Than The Manufacturer's Specified Seal Depth. The Contractor Shall Be Responsible For Ensuring That The Seal Depth Is Appropriate For The Width Of The Joint.

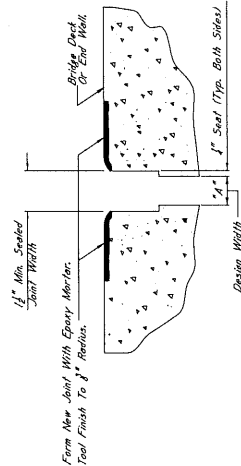


TYPICAL SECTION AT EXISTING JOINT

Showing Existing Expansion Materials To Be Removed And Replaced With Preformed Joint Seal

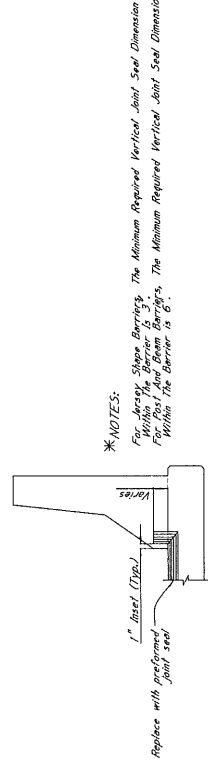
TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL AND SAWCUT

Showing Limits of Joint Preparation For Application of New Joint Seal Materials And Sawcut



TYPICAL SECTION AT SAWCUT & JOINT REPAIR

Showing Area Where Repairs Are Made After Sawcut With Epoxy Mortar Or Approved Equivalent



***NOTES:**

Prepare and slope barrier. The minimum required vertical joint seal dimension for post and beam barriers. The minimum required vertical joint seal dimension within the barrier is 6\"/>

ELEVATION AT END OF SPAN

NOTES ON ASSOCIATED ITEMS OF WORK:

907-808-402 JOINT REPAIR

Description:

Shall include the work necessary to repair joints in concrete bridge deck on each side of the centerline joint. As Detailed in the Detail Drawings Provided. Epoxy Mortar Shall Also Be Included Under This Item of Work. Removal of Existing Silicone Sealant, Compression, and AC Sealed Joint Materials Shall Be Included Under This Item of Work. All Other Requirements Shall Be in Accordance With the Applicable Provisions of Section 808 of the Specifications and Any Other Sections Specified Herein.

Basis of Payment:

The Accepted Quantities Will Be Paid For in Linear Feet At Contract Unit Price Along the Length of the Bridge Deck On Each Side of the Centerline Joint.

907-808-403 JOINT REPAIR WITHOUT EPOXY

Description:

Shall include the work necessary to repair joints in preparation for the placement of new expansion material. This includes the removal of all old sealant, silicone sealant, AC sealed joint materials, and any other materials under this item of work. All other requirements shall be in accordance with the applicable provisions of section 808 of the specifications and any other sections specified therein.

Basis of Payment:

The Accepted Quantities Will Be Paid For in Linear Feet At Contract Unit Price Along the Length of the Bridge Deck On Each Side of the Centerline Joint.

907-823-8001 SAW CUT, TYPE I & 907-823-8002 SAW CUT, TYPE II

Description:

The Saw Cut Depth Shall Be Equal to the Installation Depth of the Sealant as Specified in the Specifications. The Sealant Type Shall Be the Same as the Performed Joint Seal Selected.

Basis of Payment:

The Accepted Quantities Will Be Paid For in Linear Feet At Contract Unit Price Along the Length of the Bridge Deck On Each Side of the Centerline Joint. It is the Contractor's Responsibility to Ensure that the Proper Depth is Selected Based on the Manufacturer's Recommendation.

907-823-4001 PREFORMED JOINT SEAL, TYPE I

Description:

Shall include the Manufacturer's Required Joint Preparation Including Sandblasting Both Sides of the Joint and Blowing the Joint Free of All Debris. The Compressor Air After Placement of the New Performed Joint Seal.

Basis of Payment:

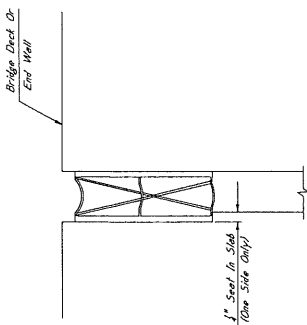
The Accepted Quantities Will Be Paid For in Linear Feet At Contract Unit Price Along the Length of the Centerline Joint.

EPOXY MORTAR AND POLYMER CONCRETE NOTES:

Either Epoxy Mortar or Polymer Concrete May Be Used. Questions Regarding Specifications Can Be Found in Section 808 of the Specifications.

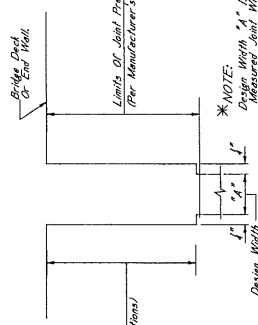
GENERAL NOTES:

1. Specifications, Mississippi Standard Specifications For Road Construction, shall apply to all work unless otherwise specified.
2. No Change of Plans Will Be Permitted Except By Written Approval of the Director of Structures, State Bridge Engineer. Minor Changes To Detail or Design or Construction Procedures Will Not Be Cause For Contract Price Adjustments. Work For Which No Pay Item is Provided in the Proposal Will Not Be Paid For. Correctly And Shall Therefore Be Considered An Abstract Item of Work.
- 3.



TYPICAL SECTION AT EXISTING JOINT

Showing Existing Expansion Device To Be Removed And Replaced With Performed Joint Seal

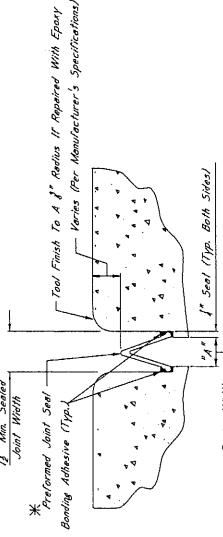


*NOTE:

Design Width "A" is Defined As The Actual Measured Joint Width.

TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL AND SAWCUT

Showing Limits of Joint Preparation For Application of New Joint Seal Materials And Sawcut



TYPICAL SECTION AT SAWCUT & SEALED JOINT

Showing Sealed Joint After Sawcut And Repair With Epoxy Mortar

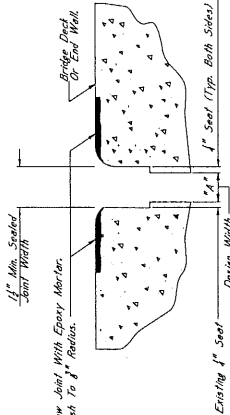
*NOTES:

1. The Performed Joint Seal Shall Be One of The Following, Installed According To The Manufacturer's Specifications:

- A. Silicone Joint Sealing System Manufactured By R.J. Watson, Inc. in Allen, NY www.rjwatson.com
- B. Wedo SFS Joint Sealing System Manufactured By R.J. Watson, Inc. in Allen, NY www.watson.com
- C. Silosec SSS Silicone High Seal Manufactured By SSI Commercial & Highway Construction Materials www.ssi.com

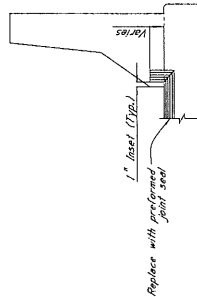
2. For Estimating Purposes, The R.J. Watson Silicone Joint Sealing System Was Selected However, Other Approved Sealants May Be Used. The Contractor Shall Be Responsible For Joint Preparation, Installation Details, and Methods, Adhesive, Sealing Times, and Materials. The Contractor Shall Be Responsible For Ensuring That the Manufacturer's Requirements Shall Be Present At The Time Joint Sealing Begins To Ensure That the Contractor is Properly Schooled in Installation of the Joint Material.

3. Joints Shall Be Sealed At Their Design Widths. Dimension "A" is Which is Defined As Seal Required on Both Sides of the Joint. Performed Joint Seal, Type I, Shall Be Used. Seal Depth Shall Be at Least 2" and Shall be Placed in Seal. Seal Type II Shall be Used in Cases Where Design Width is Greater Than 6". Another Type of Expansion Material Shall Be the Contractor's Responsibility to Ensure That the Size Selected is Appropriate For the Width of the Joint.



TYPICAL SECTION AT SAWCUT & JOINT REPAIR

Showing Area Where Repairs Are Made After Sawcut, With Epoxy Mortar Or Approved Equivalent



ELEVATION AT END OF SPAN

*NOTES:

Performs, Shows Details. The Minimum Required Vertical Joint Seal Dimension For Post And Beam Barrages. The Minimum Required Vertical Joint Seal Dimension Within The Barrage is 6\"/>

NOTES ON ASSOCIATED ITEMS OF WORK:

907-808-002 JOINT REPAIR

Description:

Shall include the Work Necessary To Repair Joints In Preparation For The Placement Of New Expansion Material As Designated In The Detail Drawing Provided. Epoxy Mortar Shall Also Be Included Under This Item Of Work. Epoxy Mortar Materials Will Not Be Paid For Directly And Shall Be Considered As Absorbed Under This Item Of Work. All Other Requirements Shall Be As Stated In The Application For Approval. Specifications Shall Be As Stated In The Specifications And Any Other Sections Specified Therein.

Basis Of Payment:

The Accepted Quantities Will Be Paid For In Linear Feet Al On Each Side Of The Centerline Joint.

907-808-003 JOINT REPAIR WITHOUT EPOXY

Description:

Shall include the Work Necessary To Repair Joints In Preparation For The Placement Of New Expansion Material Or Existing Silicone Seals, Compression And AC Seals. Joint Materials Shall Be Included Under This Item Of Work. All Other Requirements Shall Be As Stated In The Application For Approval. Specifications Shall Be As Stated In The Specifications And Any Other Sections Specified Therein.

Basis Of Payment:

The Accepted Quantities Will Be Paid For In Linear Feet Al On Each Side Of The Centerline Joint.

907-823-001 SAW CUT, TYPE I & 907-823-002 SAW CUT, TYPE II

Description:

The Saw Cut Depth Shall Be Equivalent To The Installation Depth Required By The Manufacturer's Specifications. The Saw Cut Type Shall Be The Same As The Performed Joint Seal Selected.

Basis Of Payment:

The Accepted Quantities Will Be Paid For In Linear Feet Al On Each Side Of The Centerline Joint. It Is The Contractor's Responsibility To Obtain The Manufacturer's Recommendations.

907-823-001 SAW CUT, TYPE I

Description:

Shall include the Manufacturer's Required Joint Preparation Including Sawcutting Both Sides Of The Placement Of The New Performed Joint Seal.

Basis Of Payment:

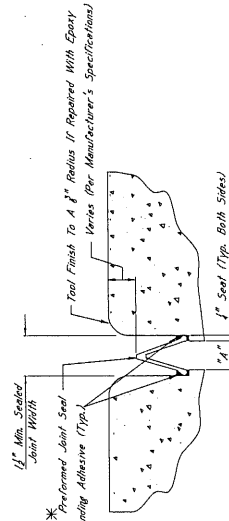
The Accepted Quantities Will Be Paid For In Linear Feet Al On Each Side Of The Centerline Joint.

EPOXY MORTAR AND POLYMER CONCRETE PATCHES:

Either Epoxy Mortar Or Polymer Concrete May Be Used. Guidelines For Selection Of Materials Can Be Found In Section 808 of the Specifications.

GENERAL NOTES:

1. Specifications: Mississippi Standard Specifications For Road And Bridge Construction (MS-SS).
2. Approval Of The Director Of Structures, State Bridge Engineer. Minor Changes To Detail Of Design Or Construction Procedure Shall Not Be Cause For Contract Price Adjustment. Work For Which No Pay Item Is Provided In The Proposal Will Not Be Paid For Directly And Shall Therefore Be Considered An Absorbed Item Of Work.



TYPICAL SECTION AT SAWCUT & SEALED JOINT

Showing Sealed Joint After Sawcut And Repair With Epoxy Mortar

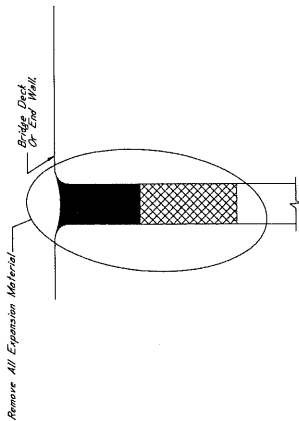
***NOTES:**

1. The Performed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:

- A. Silastic Joint Sealing System Manufactured By R.J. Watson, Inc. In Albany, NY www.rjwatson.com
- B. Wega 555 Joint Sealing System Manufactured By Wega Systems, Inc. In Amherst, NY www.wegasystems.com
- C. Silastic 555 Silicone Strip Seal Manufactured By 3SI Commercial & Highway Construction Materials www.3si.com

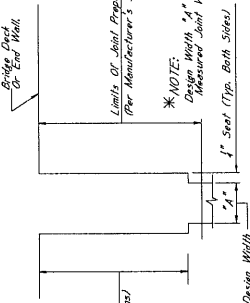
2. For Estimating Purposes, The R.J. Watson Silastic Joint Sealing System Was Selected. However, The Manufacturer's Recommendations Are Followed For Joint Preparation, Installation Depth, And Width, Adhesive, Setting Times, And Cure For All Other Details. The Manufacturer's Representative Shall Be Present At The Time Joint Sealing Begins To Ensure That The Contractor Is Properly Scheduled In Installation Of The Joint Material.

3. Joints Shall Be Sealed At Their Design Widths, Quantities $\pm 1\%$ Which Is Defined As Seal Required On Both Sides Of The Joint. Preformed Joint Seal Type B Shall Be Used For Joints Greater Than 1/2" In Width. The Manufacturer's Representative Shall Be Present At The Time Joint Sealing Begins To Ensure That The Contractor Is Properly Scheduled In Installation Of The Joint Material. In Cases Where Design Widths Are Greater Than 24" Another Sealing System Shall Be Selected. It Is The Contractor's Responsibility To Ensure That The Size Selected Is Appropriate For The Width Of The Joint.



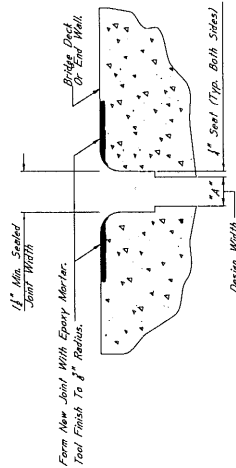
TYPICAL SECTION AT EXISTING JOINT

Showing Existing Expansion Material To Be Removed And Replaced With Preformed Joint Seal



TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL AND SAWCUT

Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials And Sawcut



TYPICAL SECTION AT SAWCUT & JOINT REPAIR

Showing Area Where Repairs Are Made After Sawcut With Epoxy Mortar Or Approved Equivalent

***NOTES:**

1. For Jersey Slab Bearings, The Minimum Required Vertical Joint Seal Dimension Within The Bearing Is 3/4". The Minimum Required Vertical Joint Seal Dimension Within The Barrier Is 6".

Replace with preformed joint seal

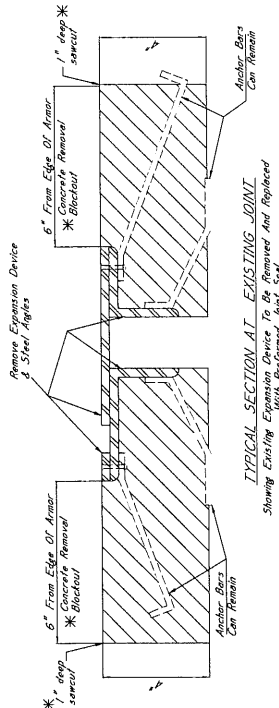
ELEVATION AT END OF SPAN

*** 1" SAWCUT NOTES:**

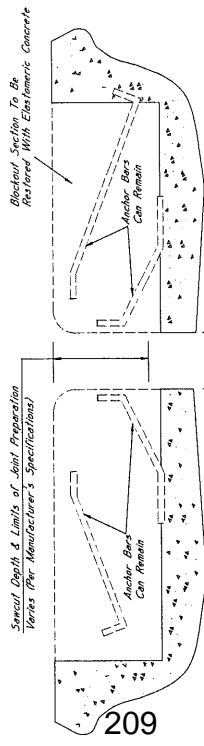
All sawcuts shall be considered to be made in the concrete. The Contractor shall verify the depth of reinforcing steel before making any sawcuts. The depth of the reinforcing steel shall be determined by the Contractor and approved by the Engineer at the cost to the State.

*** CONCRETE REMOVAL BLOCKOUT NOTES**

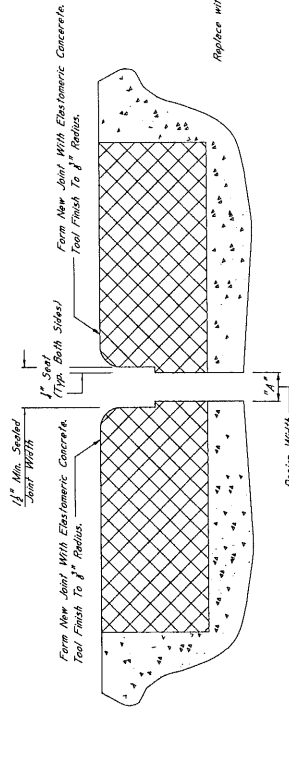
Concrete removal shall be considered an absorbed item of work under Item 202-21.69. The Contractor shall use a hammer and chisel to complete this work.



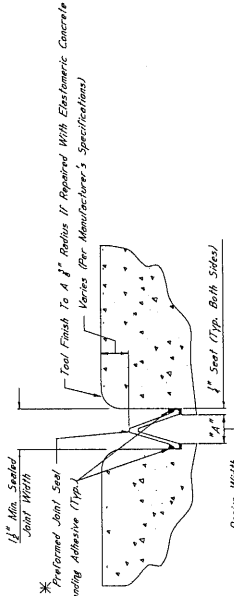
TYPICAL SECTION AT EXISTING JOINT
Showing Existing Expansion Device To Be Removed And Replaced With Performed Joint Seal



TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL
Showing Limits of Joint Preparation For Application of New Joint Seal Materials



TYPICAL SECTION AT SAWCUT & JOINT REPAIR
Showing Area Where Repairs Are Made After Sawcut With Elastomeric Concrete



TYPICAL SECTION AT SAWCUT & SEALED JOINT
Showing Sealed Joint After Sawcut And Repair With Elastomeric Concrete

*** NOTES:**

- The Preformed Joint Seal Shall Be One Of The Following Installed According To The Manufacturer's Specifications:
 - Silcoflex Joint Sealing System Manufactured By R.L. Watson, Inc. In Akron, NY
 - Wichita 505 Seals System Manufactured By Welton Bowman Acme Corporation In Amherst, NY
 - Silgoec 555 Silicone Strip Seal Manufactured By SST Commercial & Highway Construction Materials
- For Estimating Purposes, The 6" Width Silcoflex Seal System Shall Be Used. The Contractor Shall Be Responsible To Ensure That The Manufacturer's Recommendations Are Followed. Any Other Information Between The Specifications Provided By The Manufacturer, Manufacturer Representative Shall Be Present At The Time Joint Sealing Begins Material.
- Joints Shall Be Sealed At Their Design Widths, Dimension "A", Which Is Defined As The Actual Width Of The Joint Opening. This Width Does Not Account For The Seal Required On Widths Smaller Than The Preformed Joint Seal. The Seal Shall Be Applied To The Joint Widths Greater Than Or Equal To "A" With The Maximum Design Width Of Expansion Material Shall Be As Required As Directed By The Director Of Structures, State Bridge Engineer. It Is The Contractor's Responsibility To Ensure That The Seal Selected Is Appropriate For The Width Of The Joint.

*** NOTES:**

- For Jersey Slabs Barring, The Minimum Required Vertical Joint Seal Dimension Within The Barrier Is 3".
- For Jersey Slabs Barring, The Minimum Required Vertical Joint Seal Dimension Within The Barrier Is 6".

ELEVATION AT END OF SPAN

NOTES ON ASSOCIATED ITEMS OF WORK:

202-21.69 REMOVAL OF EXISTING JOINT MATERIAL

Description: Shall include the removal of material associated with armor, including the joint preparation expansion device, removal of the concrete blockout area shall be absorbed under this item of work. Other joint types shall not be included. The Contractor shall use a hammer and chisel to complete this work.

Basis of Payment: Removal of armor and slabs shall be paid for in linear feet along the length of the bridge deck on each side of the centerline joint, while removal of expansion joint shall be paid for as the length along the centerline of the joint.

907-823-8001 SAW CUT, TYPE I & 907-823-8002 SAW CUT, TYPE II
Description: The Saw Cut Depth Shall Be Equivalent To The Installation Depth Required By The Manufacturer's Specifications. The Saw Cut Type Shall Be The Same As The Preformer Joint Seal Selected.

Basis of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint.

907-823-4001 PREFORMED JOINT SEAL, TYPE I
907-823-4002 PREFORMED JOINT SEAL, TYPE II
Description: Shall include the manufacturer's required joint preparation including the joint preparation expansion device, removal of the concrete blockout area shall be absorbed under this item of work.

Basis of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Centerline Joint.

ELASTOMERIC CONCRETE NOTES

907-824-9007 BRIDGE REPAIR ELASTOMERIC CONCRETE
Description: Elastomeric concrete shall be one of the following products, installed according to the manufacturer's specifications:

- Poly-Ton Elastomeric Concrete, Inc. In Akron, NY
- Wichita 505 Seals System, Inc. In Amherst, NY
- Dakota Elastomeric Concrete, Inc. In North Baltimore, OH

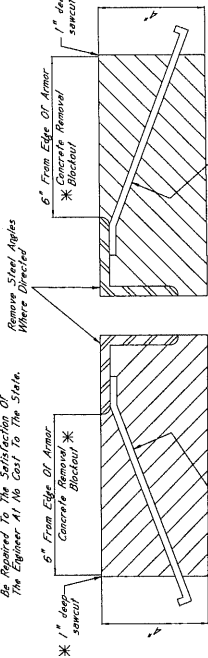
Basis of Payment: The Accepted Quantities Will Be Paid For In Cubic Yards At The Contract Unit Price.

GENERAL NOTES:

- Specifications, Manufacturer Standard Specifications For Road And Bridge Construction Shall Be Permitted Except By Written Approval Of The Director Of Structures, State Bridge Engineer.
- Minor Changes To Detail Of Design Or Construction Procedure Shall Be Permitted Provided That The Contractor Shall Not Be Paid For Which No Pay Item Is Provided In The Proposal Will Not Be Paid For Directly And Shall Therefore Be Considered An Absorbed Item of Work.

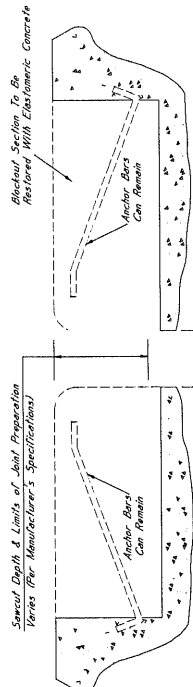
*** 1" SAWCUT NOTES:**

All 1" Sawcuts Shall Be Considered As Absorbed Item of Work. The Contractor Shall Verify Depth of Reinforcing Steel At The Depth Of The Sawcut Shall Be No More Than 6" From Edge Of Slab. Any Change To Reinforcing Steel Shall Be Made At The Cost To The State.



TYPICAL SECTION AT EXISTING JOINT

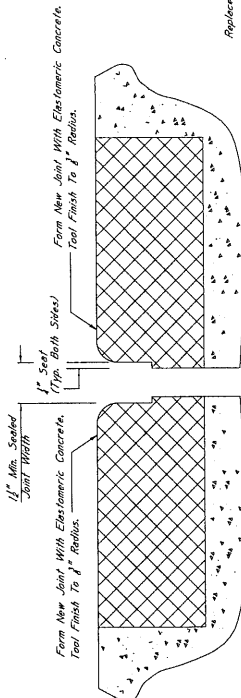
Showing Existing Expansion Devices To Be Removed And Replaced With Preformed Joint Seal



*** NOTE:**
Channel Width 5/8" Is Defined As The Actual Measured Joint Width.

TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL

Showing Limits of Joint Preparation For Application of New Joint Seal Materials

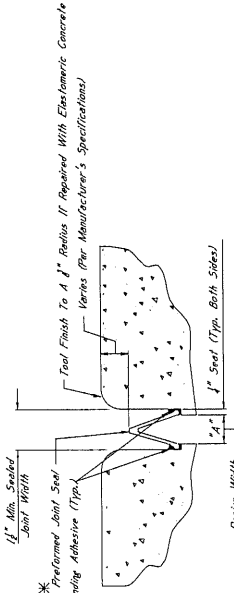


TYPICAL SECTION AT SAWCUT & JOINT REPAIR

Showing Area Where Repairs Are Made After Sawcut With Elastomeric Concrete

*** CONCRETE REMOVAL BLOCKOUT NOTES**

Removal Of The Concrete Blockout Area Shall Be Considered An Absorbed Item Of Work. The Contractor Shall Verify Depth of Reinforcing Steel At The Depth Of The Sawcut Shall Be No More Than 30 LBS To Complete This Work.



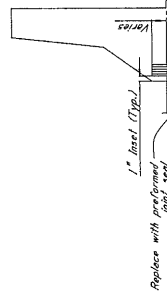
TYPICAL SECTION AT SAWCUT & SEALED JOINT

Showing Sealed Joint After Sawcut And Repair With Elastomeric Concrete

*** NOTES:**
1. The Preformed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:
A. Silibond Joint Sealing System, Manufactured By R.J. Watson, Inc. In Akron, NY
www.rjwatson.com
B. Wobol SPS Joint System Manufactured By Watson-Boman Acme Corporation In Amherst, NY
www.watson.com
C. Silibond SPS 650000 Strip Seal, Manufactured By SSI Commercial & Highway Construction Materials
www.ssi.com

2. For Estimating Purposes, The R.J. Watson Silicone Joint Sealing System Was Selected. However, Should Another Supplier Be Used, The Contractor Shall Verify That The Joint Seal Material Meets The Following Requirements:
A. The Joint Seal Material Shall Be Applied To The Joint After The Joint Sealant Has Been Applied To The Joint.
B. The Joint Seal Material Shall Be Applied To The Joint After The Joint Sealant Has Been Applied To The Joint.
C. The Joint Seal Material Shall Be Applied To The Joint After The Joint Sealant Has Been Applied To The Joint.
D. The Joint Seal Material Shall Be Applied To The Joint After The Joint Sealant Has Been Applied To The Joint.

3. Joints Shall Be Sealed At Their Design Widths. Dimension "A" Which Is Defined As Seal Required On Both Sides Of The Joint, For Preformed Joint Seal Type Is, Shall Be Used For Design Widths Less Than Seal Required Sealant. Dimension "B" Which Is Defined As Seal Required On One Side Of The Joint, For Preformed Joint Seal Type Is, Shall Be Used For Design Widths Greater Than Seal Required Sealant. Another Sealant Type May Be Used To Ensure That The Contractor Is Properly Sealed In Installation Of The Joint Material.



*** NOTES:**
For Design Widths, The Minimum Required Vertical Joint Seal Dimension For Post-And Beam Barriers, The Minimum Required Vertical Joint Seal Dimension Within The Barrier Is 6".

ELEVATION AT END OF SPAN

NOTES ON ASSOCIATED ITEMS OF WORK:

202-B169 REMOVAL OF EXISTING JOINT MATERIAL

Description: Shall include The Removal Of Material Associated With Joint Sealing, Plate and Wagon Expansion Joints, As Designated In The Detail Drawings Expansion Removal of The Concrete Blockout Area Shall Be Absorbed Under This Item Of Work Unless Otherwise Directed By The Engineer.

Basis Of Payment: Removal of Armor And Sliding Plate Joint Material Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Material Will Only Be Paid For As The Length Along The Centerline Of The Joint.

907-823-8001 SAW CUT, TYPE I & 907-823-8002 SAW CUT, TYPE II

Description: The Saw Cut Depth Shall Be Equivalent To The Installation Depth Required By The Manufacturer's Specifications. The Type Seal Be The Same As The Preformed Joint Seal Selected.

Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint.

907-823-4001 PREFORMED JOINT SEAL, TYPE I

907-823-4002 PREFORMED JOINT SEAL, TYPE II

Description: Shall include The Manufacturer's Required Joint Preparation Including Sealing Both Sides Of The Joint And Blowing The Joint Free Of Debris With Compressed Air And Placement Of The New Preformed Joint Seal.

Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Centerline Joint.

ELASTOMERIC CONCRETE REPAIR, ELASTOMERIC CONCRETE

Description: Elastomeric Concrete Shall Be One Of The Following Products, Installed According To The Manufacturer's Specifications:

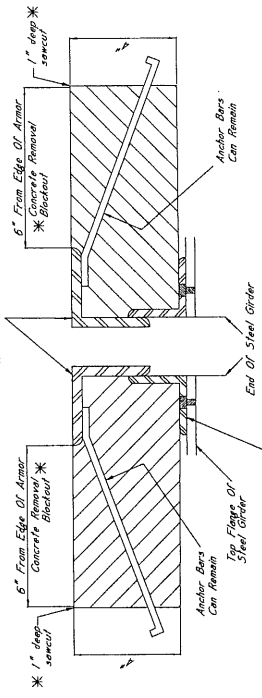
- A. Poly-Ton Elastomeric Concrete Manufactured By R.J. Watson, Inc. In Akron, NY www.rjwatson.com
- B. Wobol-Crete II Manufactured By Watson-Boman Acme Corporation In Amherst, NY www.watson.com
- C. Ductile Elastomeric Concrete Manufactured By The R.J. Broom Company In North Baltimore, OH www.ducton.com

Basis Of Payment: The Accepted Quantities Will Be Paid For In Cubic Yards At The Contract Unit Price.

GENERAL NOTES:

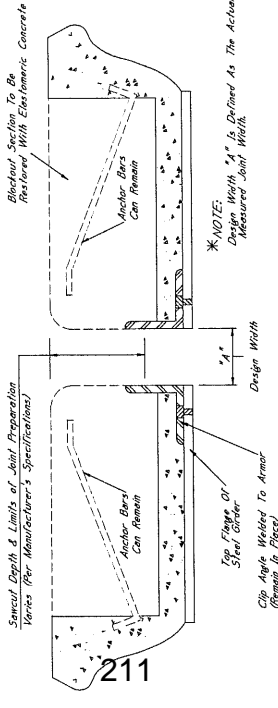
1. Specifications: Massachusetts Standard Specifications For Road And Bridge Construction, 2012.
2. No Change Of The Direction Of Structures, State Bridge Engineer, Minor Changes To Detail Of Design Or Construction Procedure May Be Authorized By The Bridge Engineer. Provided Such Changes Will Not Affect The Overall Dimensions Or Functionality Of The Work For Which The Pay Item Is Provided In The Proposal Will Not Be Paid For Directly And Shall Therefore Be Considered An Absorbed Item of Work.

*** 1" SAWCUT NOTES:**
 All 1" Sawcuts Shall Be Considered An
 Integral Part Of The Concrete. All Repairs
 Shall Be Made In Accordance With
 Work Under Pay Item 202-0165. The
 Contractor Shall Use A Hammer No Larger
 Than 30 Lbs. To Complete This Work.

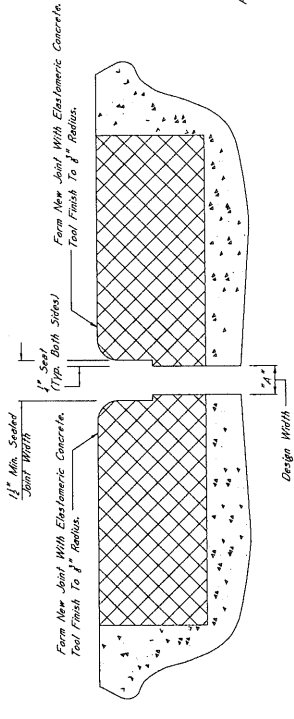


TYPICAL SECTION AT EXISTING JOINT
 Showing Existing Existing Seal To Be Repaired and Repaired
 With Performed Joint Seal

*** CONCRETE REMOVAL BLOTTOUT NOTES**
 Blotting Of The Concrete Blotout Line Of
 Work Shall Be Made In Accordance With
 Work Under Pay Item 202-0165. The
 Contractor Shall Use A Hammer No Larger
 Than 30 Lbs. To Complete This Work.



TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL
 Showing Limits Of Joint Preparation For Application Of New Joint
 Seal Materials



TYPICAL SECTION AT SAWCUT & JOINT REPAIR
 Showing Area Where Repairs Are Made After Sawcut
 With Elastomeric Concrete

NOTES ON ASSOCIATED ITEMS OF WORK:

202-0165 REMOVAL OF EXISTING JOINT MATERIAL

Description:
 Shall include The Removal Of Material Associated With
 Armor, Sliding Plate, And Mesquite Expansion Joints, As
 Designated In The Detail Drawings. The Contractor Shall
 Be Responsible For The Removal Of Material From This
 Item Of Work. Other Joint Types Shall Not Be Included
 Under This Item Of Work Unless Otherwise Directed By
 The Engineer.

Basis Of Payment:
 Removal Of Armor And Sliding Plate Joint Material Will
 Be Paid For In Linear Feet At The Contract Unit Price
 Indicated On The Schedule Of Materials. Removal Of The
 Mesquite Expansion Joint Material Will Only Be Paid For
 Along The Length Of The
 Centerline Of The Joint.

907-823-0001 SAW CUT, TYPE I & 907-823-0002 SAW CUT, TYPE II

Description:
 The Saw Cut Depth Shall Be Equivalent To The Installation Depth
 Required By The Manufacturer's Specifications. The Saw Cut
 Type Shall Be The Same As The Performed Joint Seal Selected.

Basis Of Payment:
 The Accepted Quantities Will Be Paid For In Linear Feet At
 The Contract Unit Price Along The Length Of The Bridge Deck
 On Each Side Of The Centerline Joint.

907-823-0001 PREFORMED JOINT SEAL, TYPE I
907-823-0002 PREFORMED JOINT SEAL, TYPE II

Description:
 Shall include The Manufacturer's Required Joint Preparation
 Including Sandblasting Both Sides Of The Joint And Blowing
 Free Of Debris With Compressed Air And Placement Of The New
 Performed Joint Seal.

Basis Of Payment:
 The Accepted Quantities Will Be Paid For In Linear Feet At
 The Contract Unit Price Along The Length Of The Centerline
 Joint.

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ELASTOMERIC CONCRETE REPAIR, ELASTOMERIC CONCRETE

Description:
 Elastomeric Concrete Shall Be One Of The Following Products,
 Installed According To The Manufacturer's Specifications:

- Poly-Tren Elastomeric Concrete
 Manufactured By
 www.jmboston.com
- Weld-Crete II
 Manufactured By
 www.weldcrete.com
- Decorative Elastomeric Concrete
 Manufactured By
 www.decorative.com

Basis Of Payment:
 The Accepted Quantities Will Be Paid For In Cubic Yards At
 The Contract Unit Price.

GENERAL NOTES:

- Specifications: Massachusetts Standard Specifications For Road
 And Bridge Construction, EDI 7.
- No Change Of The Detail Shall Be Made Except By Written
 Approval From The Engineer. No Change Of Construction Procedure
 May Be Authorized By The Bridge Engineer Provided Such Changes
 Will Not Affect The Safety Or Functionality Of The Bridge.
 Work For Which No Pay Item Is Provided In The Proposal Will
 Not Be Paid For Directly And Shall Therefore Be Considered An
 Absorbed Item Of Work.

GENERAL NOTES:

- Specifications: Massachusetts Standard Specifications For Road
 And Bridge Construction, EDI 7.
- No Change Of The Detail Shall Be Made Except By Written
 Approval From The Engineer. No Change Of Construction Procedure
 May Be Authorized By The Bridge Engineer Provided Such Changes
 Will Not Affect The Safety Or Functionality Of The Bridge.
 Work For Which No Pay Item Is Provided In The Proposal Will
 Not Be Paid For Directly And Shall Therefore Be Considered An
 Absorbed Item Of Work.

TYPICAL SECTION AT SAWCUT & SEALED JOINT
 Showing Sealed Joint After Sawcut And
 Repair With Elastomeric Concrete

*** NOTES:**

- The Performed Joint Seal Shall Be One Of The Following, Installed According To The
 Manufacturer's Specifications:

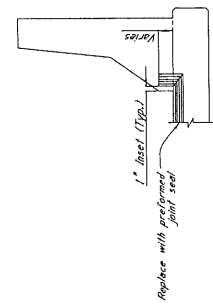
- Silicone Sealant
 Manufactured By
 www.jmboston.com
- Weld-Sp Sealant System
 Manufactured By
 www.weldcrete.com
- Silicone Sealant
 Manufactured By
 www.silicone.com

For Estimating Purposes, The RJ-3000 Silicone Sealant System Was
 Selected. However, Should Another Supplier Be Chosen, It Is The Contractor's
 Responsibility To Verify That The Supplier's Sealant System Meets The
 Requirements Of The Specifications. The Contractor Shall Verify That The Sealant System
 Meets The Requirements Of The Specifications. The Contractor Shall Verify That The Sealant System
 Meets The Requirements Of The Specifications. The Contractor Shall Verify That The Sealant System
 Meets The Requirements Of The Specifications.

Joints Shall Be Sealed At Their Design Widths. Dimension "A" Is Defined As
 The Width Of The Joint. The Contractor Shall Seal The Joint To A Depth Of
 One (1) Inch. The Contractor Shall Seal The Joint To A Depth Of One (1) Inch.
 The Contractor Shall Seal The Joint To A Depth Of One (1) Inch. The Contractor
 Shall Seal The Joint To A Depth Of One (1) Inch. The Contractor Shall Seal The
 Joint To A Depth Of One (1) Inch. The Contractor Shall Seal The Joint To A
 Depth Of One (1) Inch. The Contractor Shall Seal The Joint To A Depth Of
 One (1) Inch. The Contractor Shall Seal The Joint To A Depth Of One (1) Inch.

*** NOTES:**

For Jersey Slope Barriers, The Minimum Required Vertical Joint Seal Dimension
 Within The Barrier Is 18". The Minimum Required Vertical Joint Seal Dimension
 Within The Barrier Is 6".



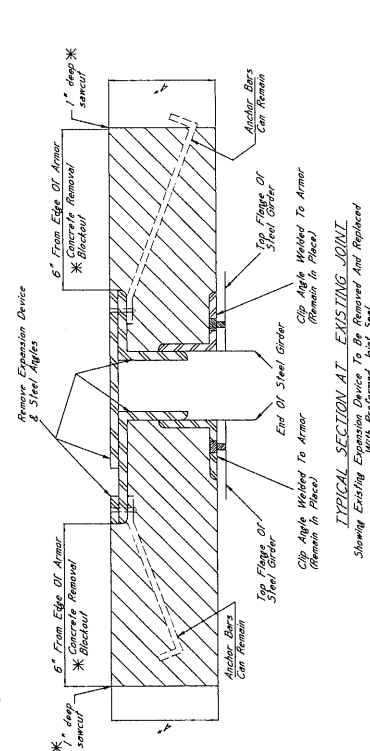
ELEVATION AT END OF SPAN

*** 1" SAWCUT NOTES:**

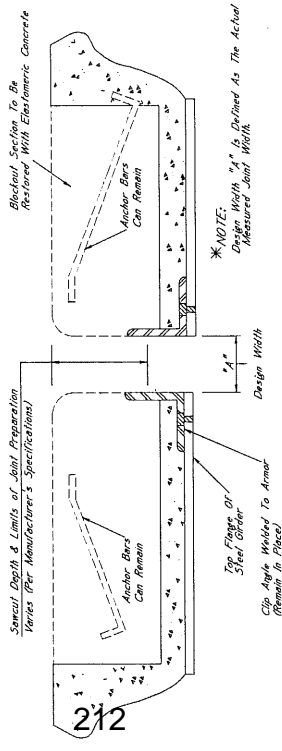
All 1" Sawcuts Shall Be Considered An Assembled Item of Work. The Contractor Shall Obtain Approval From 202-69169, The Engineer, Before Making Any Sawcuts. The Depth Of The Sawcut Shall Be No More Than 1/2" Below The Reinforcing Steel. The Sawcut Shall Be Made After The Steel Is Reinforced To The Satisfaction Of The Engineer. At No Cost To The State.

*** CONCRETE REMOVAL BLOCKOUT NOTES:**

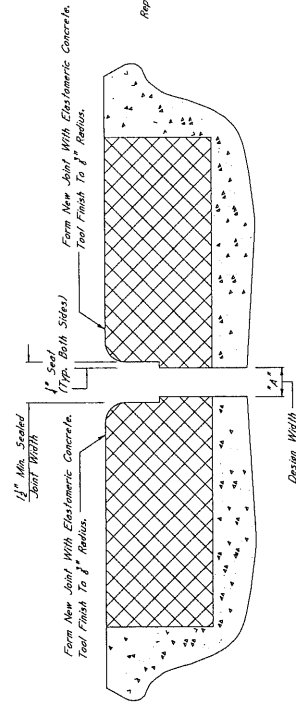
Removed Of The Concrete Blockout Area Considered An Item of Work. The Contractor Shall Obtain Approval From 202-69169, The Engineer, Before Making Any Sawcuts. The Depth Of The Sawcut Shall Be No More Than 1/2" Below The Reinforcing Steel. The Sawcut Shall Be Made After The Steel Is Reinforced To The Satisfaction Of The Engineer. At No Cost To The State.



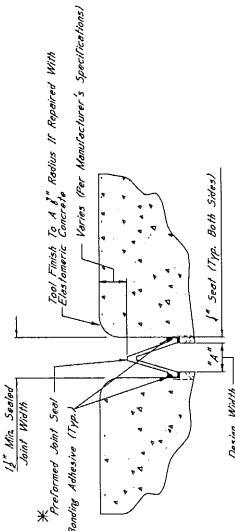
TYPICAL SECTION AT EXISTING JOINT
Showing Existing Expansion Device To Be Removed And Replaced With Preformed Joint Seal.



TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL
Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials.



TYPICAL SECTION AT SAWCUT & JOINT REPAIR
Showing Area Where Repairs Are Made After Sawcut With Elastomeric Concrete.



TYPICAL SECTION AT SAWCUT & SEALED JOINT
Showing Sealed Joint After Sawcut And Repair With Elastomeric Concrete.

*** NOTES:**

1. The Preformed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:

- A. Silicone Joint Sealing System Manufactured By R.J. Wolfson, Inc. In Adels, NY www.rjwolfson.com
- B. Wela 555 Joint Sealing System Manufactured By Wela Construction Materials www.wela.com
- C. Silcrete 555 Silicone Strip Seal Manufactured By SSI Commercial & Highway Construction Materials www.ssi.com

2. For Estimating Purposes, The R.J. Wolfson Silicone Joint Sealing System Was Used. The Contractor Shall Be Responsible To Ensure That The Manufacturer's Recommendations Are Followed For Joint Preparation, Installation Depth, And Width, Adhesive Sealing Times, And Cure Time. The Contractor Shall Provide Representative Samples At The Time Joint Sealing Begins To Ensure That The Contractor Is Properly Scheduled In Installation Of The Joint Material.

3. Joints Shall Be Sealed At Their Design Width. The Width Does Not Account For The Seal Required On Both Sides Of The Joint. The Preformed Joint Seal Type Shall Be One Of The Following: A. The Seal Width Shall Be Equal To 2" With The Maximum Design Width Being 2 1/2". In Cases Where Design Widths Are Greater Than 2 1/2", Another Seal Type Shall Be Used. B. The Seal Width Shall Be 2" With The Maximum Design Width Being 2 1/2". In Cases Where Design Widths Are Greater Than 2 1/2", Another Seal Type Shall Be Used. C. The Seal Width Shall Be 2" With The Maximum Design Width Being 2 1/2". In Cases Where Design Widths Are Greater Than 2 1/2", Another Seal Type Shall Be Used. The Contractor's Responsibility To Ensure That The Seal Width Is Appropriate For The Width Of The Joint.

*** NOTES:**

For Jersey Shape Barriers, The Minimum Required Vertical Joint Seal Dimension Within The Barrier Is 3". For Post And Beam Barriers, The Minimum Required Vertical Joint Seal Dimension Within The Barrier Is 6".

ELEVATION AT END OF SPAN

NOTES ON ASSOCIATED ITEMS OF WORK:

202-69169 REMOVAL OF EXISTING JOINT MATERIAL

Description: Shall Include The Removal Of Material Associated With Armor, Sliding Plates, And Wedge Expansion Joints, As Designated In The Detail Drawings. The Contractor Shall Be Responsible For The Removal Of The Concrete Blockout Area Shall Be Shown Under This Item Of Work Unless Otherwise Directed By The Engineer.

Basis Of Payment: Removal of Armor And Sliding Plate Joint Material Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Material Will Only Be Paid For As The Length Along The Centerline Of The Joint.

907-823-8001 SAW CUT, TYPE I & 907-823-8002 SAW CUT, TYPE II

Description: The Saw Cut Depth Shall Be Equivalent To The Installation Depth Of The Joint Seal. The Saw Cut Depth Shall Be The Same As The Preformed Joint Seal Selected.

Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint.

907-823-4001 PREFORMED JOINT SEAL, TYPE I

Description: Shall Include The Manufacturer's Required Joint Preparation Including Sandblasting Both Sides Of The Joint And Blowing The Joint Free Of Debris With Compressed Air And Placement Of The New Preformed Joint Seal.

907-823-4002 PREFORMED JOINT SEAL, TYPE II

Description: Shall Include The Manufacturer's Required Joint Preparation Including Sandblasting Both Sides Of The Joint And Blowing The Joint Free Of Debris With Compressed Air And Placement Of The New Preformed Joint Seal.

ELASTOMERIC CONCRETE NOTES

907-824-9000 BRIDGE REPAIR ELASTOMERIC CONCRETE

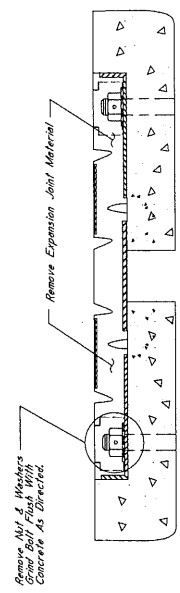
Description: Elastomeric Concrete Shall Be One Of The Following Products, Installed According To The Manufacturer's Specifications:

- A. Poly-Ton Elastomeric Concrete Manufactured By R.J. Wolfson, Inc. In Adels, NY www.rjwolfson.com
- B. Wela-Crete II By Wela Construction Materials www.wela.com
- C. Delsolite Elastomeric Concrete Manufactured By The D.S. Brown Company In North Baltimore, OH www.dsbrown.com

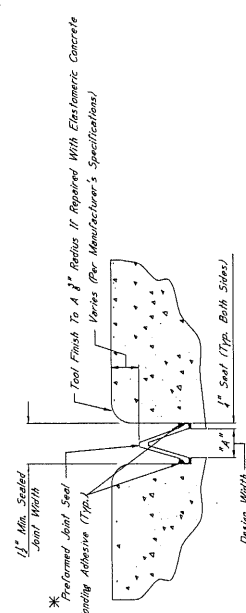
Basis Of Payment: The Accepted Quantities Will Be Paid For In Cubic Yards At The Contract Unit Price.

GENERAL NOTES:

1. Specifications: Mississippi Standard Specifications For Road And Bridge Construction.
2. No Change Of Plans Will Be Permitted Except By Written Approval Of The Director Of Structures, State Bridge Engineer. Minor Changes To Detail Drawings Or Construction Methods Will Not Be Cause For Contract Price Adjustment. Work For Which No Pay Item Is Provided In The Proposal Will Be Considered An Item of Work.

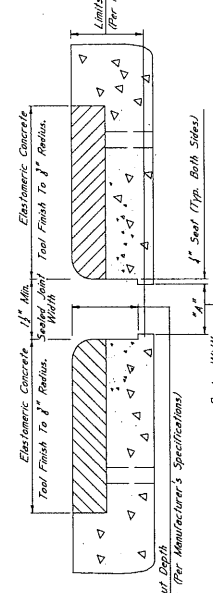


TYPICAL SECTION AT EXISTING JOINT
Showing Existing Expansion Device To Be Removed And Replaced With Preformed Joint Seal



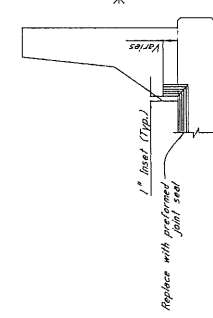
TYPICAL SECTION AT SAWCUT & SEALED JOINT
Showing Sealed Joint After Sawcut And Repair With Elastomeric Concrete

- * NOTES:
1. The Preformed Joint Seal Shall Be One Of The Following, Installed According To The Manufacturer's Specifications:
 - A. Silcoflex Joint Sealing System www.silcoflex.com
 - B. Wako SPS Joint System Manufactured By Wabson Bowman Acme Corporation In Amherst, NY www.wabson.com
 - C. Silspan 553 Silicone Strip Seal www.silspan.com



TYPICAL SECTION AT JOINT AFTER REMOVAL OF EXISTING SEAL
Showing Limits Of Joint Preparation For Application Of New Joint Seal Materials

- * NOTE:
Design Width "X" Is Defined As The Actual Measured Joint Width



ELEVATION AT END OF SPAN

NOTES ON ASSOCIATED ITEMS OF WORK
907-823-001 REMOVAL OF EXISTING JOINT MATERIAL

Description: Shall Include The Removal Of Material Associated With Armor Sliding Plates And Expansion Joints As Designated In The Detail Drawings Provided. Other Joint Types Shall Not Be Included Under This Item Of Work Unless Otherwise Directed By The Engineer.

Basis Of Payment: Removal Of Armor And Sliding Plate Joint Material Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint, While Removal Of Expansion Joint Material Will Only Be Paid For As The Length Along The Centerline Of The Joint.

907-823-001 SAW CUT, TYPE I & 907-823-002 SAW CUT, TYPE II
Description: The Saw Cut Depth Shall Be Equivalent To The Installation Depth Required By The Manufacturer's Specifications. The Saw Cut Type Shall Be The Same As The Preformed Joint Seal Selected.

Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Bridge Deck On Each Side Of The Centerline Joint.

907-823-001 PREFORMED JOINT SEAL, TYPE I
907-823-002 PREFORMED JOINT SEAL, TYPE II

Description: Shall Include The Manufacturer's Required Joint Preparation Including Sandblasting Both Sides Of The Joint And Blowing The Joint Free Of All Debris. The Preformed Joint Seal Shall Be Installed In The Joint.

Basis Of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Length Of The Centerline Joint.

ELASTOMERIC CONCRETE NOTES

907-824-000T BRIDGE REPAIR ELASTOMERIC CONCRETE
Description: Elastomeric Concrete Shall Be One Of The Following Products Manufactured According To The Manufacturer's Specifications:

- A. Poly-Ton Elastomeric Concrete Manufactured By Wabson Bowman Acme Corporation In Amherst, NY www.wabson.com
- B. Wabco-Crete II By Wabson Bowman Acme Corporation In Amherst, NY www.wabson.com
- C. Poly-Ton Elastomeric Concrete Manufactured By The U.S. Brown Company In North Baltimore, OH www.usbrown.com

Basis Of Payment: The Accepted Quantities Will Be Paid For In Cubic Yards At The Contract Unit Price.

GENERAL NOTES:

1. Specifications: Message Standard Specifications For Road And Bridge Construction.
2. Approval Of The Director Of Structures, State Bridge Engineer, Must Be Obtained For Any Changes To Detail Or Construction Procedure. The Contractor Shall Be Responsible For Obtaining Such Approval. No Work For Which No Pay Item Is Provided In The Proposal Will Be Paid For Directly. And Shall Therefore Be Considered An Absorbed Item Of Work.

NOTES ON ASSOCIATED ITEMS OF WORK:
907-824-0008 BRIDGE REPAIR, ENHWALL REPAIR

Description: Shall include the Work Necessary To Remove And Replace The Damaged Enwhall As Detailed In The Detail Drawings Provided. Instead Of Limiting The Repair To The Damaged Section, The Specified Depth Of Enwhall Shall Be Removed Along The Entire Width Of The Bridge Deck.

Basis of Payment: The Accepted Quantities Will Be Paid For In Linear Feet At The Contract Unit Price Along The Width Of The Bridge Deck.

Damage Caused To Other Elements Of The Structure Or Roadway While Completing This Item Of Work Shall Be Repaired By The Contractor At No Cost To The Department.

Prior To Placing New Concrete, All Concrete Surfaces That Will Be In Contact With The New Concrete Shall Be Painted With An Approved Epoxy Binder Designed To Bond New Concrete To Old.

New Concrete Shall Be High Early Strength Bridge Concrete, As Follows:

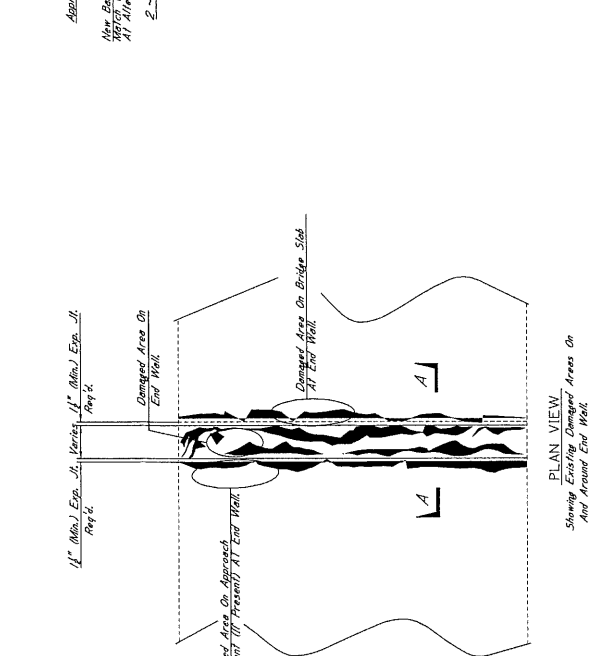
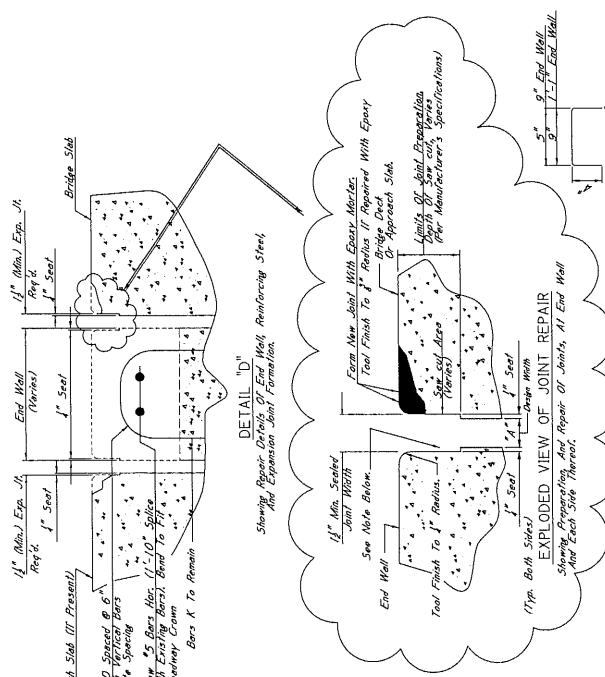
- The concrete mixture design shall be furnished by the Contractor for approval by the Materials Division. Mixture design parameters are as follows:
 Required Strength: 5200 psi prior to releasing to traffic
 Minimum Slump: 6 inches
- Non-aerated bleed accelerator may be used if the ambient temperature is 50°F or less, but shall not be used if the ambient temperature is greater than 50°F. Synthetic structural fibers shall be used. The Contractor shall submit a recommendation for the type and dosage of the fibers to the manufacturer's recommendations shall be followed for the dosage rate.
- Curing is to be continuous until 90% psi is attained. Traffic is to be used the Maturity Method per Section 907-804 to estimate the concrete compressive strength for the purpose of releasing the repair area to traffic.
- Two cylinders per test cylinder, which shall be cured in a calibrator next to the concrete placement. Two remaining cylinders shall be used to determine the 28-day compressive strength of the concrete.

The Removal Of Existing Expansion Material May Require Any Number Of The Pay Items Listed Below. Once The Expansion Device Is Removed, The Contractor Shall Place A 6" (Min.) Exp. Jt. Over Each Steel For Additional Detail On The Associated Items Of Work.

Item No.	Description
907-809-4002	REMOVAL OF EXISTING JOINT MATERIAL
907-809-4003	JOINT REPAIR WITHOUT EPXY
907-823-8000	SAW CUT
907-823-4001	PREFORMED JOINT SEAL, TYPE I
907-823-4002	PREFORMED JOINT SEAL, TYPE II

GENERAL NOTES:

- Specifications: Massachusetts Standard Specifications For Road And Bridge Construction, 2011.
- Approval: Approval Of The Director Of Structures, State Bridge Engineer. Any Changes To Detail Of Design Or Construction Procedures Will Not Be Cause For Contract Price Adjustment. See Changes Work For Which No Pay Item Is Provided In The Proposal Will Be Suffered At Contractor's Risk. Therefore Be Considered An Absorbed Item of Work.



*NOTE: Form Vertical Faces Of End Wall To Include 1" Seal Such That The Performance Joint Seal May Be Applied On This Sheet.

*NOTE: Design Width "A" Is Defined As The Actual Measured Joint Width.

*NOTE: Remove A 6" (Min.) Of End Wall Above Damaged Area To Allow Clean And Prepare According To Specifications.

*NOTE: In The Event That Bars K Are Not Present, As In Other End Wall As Shown In DETAIL "D".

For Estimating Purposes, The R.J. Wilson Silicone Joint Sealing System Was Used In The Detail Drawings. It Is The Contractor's Responsibility To Ensure That The Manufacturer's Recommendations Are Followed For Joint Preparation, Installation Details, And Widths. Allowing Slippages, And All Manufacturer Representative Shall Be Present At The Time Joint Sealing Begins To Ensure That The Contractor Is Properly Schooled In Installation Of The Joint Sealing System.

3. Joints Shall Be Sealed At Their Design Widths. Dimensions "A" Which Is Defined As The Actual Width Of The Joint Opening, Has Width Does Not Account For The 1/2" Seal Applied On Both Sides Of The Joint. Performance Joint Seal Type Shall Be Sealed For Design Widths Greater Than Or Equal To 2" With The Maximum Design Width Being 24". In Cases Where Design Widths Are Greater Than 24" And/Or Slippages State Bridge Engineers, It Is The Contractor's Responsibility To Ensure That The Size Specified Is Appropriate For The Width Of The Joint.

SECTION 905 - PROPOSAL

Date _____

Mississippi Transportation Commission
Jackson, Mississippi

Sirs: The following proposal is made on behalf of _____
_____ of _____

for constructing the following designated project(s) within the time(s) hereinafter specified.

The plans are composed of drawings and blue prints on file in the offices of the Mississippi Department of Transportation, Jackson, Mississippi.

The Specifications are the current Standard Specifications of the Mississippi Department of Transportation approved by the Federal Highway Administration, except where superseded or amended by the plans, Special Provisions and Notice(s) to Bidders attached hereto and made a part thereof.

I (We) certify that I (we) possess a copy of said Standard and any Supplemental Specifications.

Evidence of my (our) authority to submit the Proposal is hereby furnished. The proposal is made without collusion on the part of any person, firm or corporation. I (We) certify that I (we) have carefully examined the Plans, the Specifications, including the Special Provisions and Notice(s) to Bidders, herein, and have personally examined the site of the work. On the basis of the Specifications, Special Provisions, Notice(s) to Bidders, and Plans, I (we) propose to furnish all necessary machinery, tools, apparatus and other means of construction and do all the work and furnish all the materials in the manner specified. I (We) understand that the quantities mentioned herein are approximate only and are subject to either increase or decrease, and hereby propose to perform any increased or decreased quantities of work at the unit prices bid, in accordance with the above.

I (We) acknowledge that this proposal will be found irregular and/or non-responsive unless a certified check, cashier's check, or Proposal Guaranty Bond in the amount as required in the Advertisement (or, by law) is submitted electronically with the proposal or is delivered to the Contract Administration Engineer prior to the bid opening time specified in the advertisement.

INSTRUCTION TO BIDDERS: Alternate and Optional Items on Bid Schedule.

1. Two or more items entered opposite a single unit quantity WITHOUT DEFINITE DESIGNATION AS "ALTERNATE ITEMS" are considered as "OPTIONAL ITEMS". Bidders may or may not indicate on bids the Optional Item proposed to be furnished or performed WITHOUT PREJUDICE IN REGARD TO IRREGULARITY OF BIDS.
2. Items classified on the bid schedule as "ALTERNATE ITEMS" and/or "ALTERNATE TYPES OF CONSTRUCTION" must be preselected and indicated on bids. However, "Alternate Types of Construction" may include Optional Items to be treated as set out in Paragraph 1, above.
3. Optional items not preselected and indicated on the bid schedule MUST be designated in accordance with Subsection 102.06 prior to or at the time of execution of the contract.
4. Optional and Alternate items designated must be used throughout the project.

I (We) further propose to perform all "force account or extra work" that may be required of me (us) on the basis provided in the Specifications and to give such work my (our) personal attention in order to see that it is economically performed.

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) shall submit electronically with our proposal or deliver prior to the bid opening time a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

President Address

Secretary Address

Treasurer Address

The following is my (our) itemized proposal.

Bridge Repairs on SR 28 (Bridge Nos. 40.9 & 43.0) & US 49 W (Bridge Nos. 255.2 & 255.2B), known as Federal Aid Project Nos. STP-9999-03(373) / 108401301 & STP-9999-03(374) / 108401302 in Copiah & Sunflower Counties.

Line no.	Item Code	Adj Code	Quantity	Units	Description[Fixed Unit Price]
Roadway Items					
0010	618-A001		1	Lump Sum	Maintenance of Traffic
0020	618-B001		2	Square Feet	Additional Construction Signs (\$10.00)
0030	619-A1002		2,160	Linear Feet	Temporary Traffic Stripe, Continuous White
0040	619-A2002		5,600	Linear Feet	Temporary Traffic Stripe, Continuous Yellow
0050	619-A6002		192	Linear Feet	Temporary Traffic Stripe, Legend
0060	619-F1001		1,685	Linear Feet	Concrete Median Barrier, Precast
0070	619-F2001		1,685	Linear Feet	Remove and Reset Concrete Median Barrier, Precast
0080	620-A001		1	Lump Sum	Mobilization
0090	626-C001		6,890	Linear Feet	6" Thermoplastic Double Drop Edge Stripe, Continuous White
0100	626-D002		6,245	Linear Feet	6" Thermoplastic Double Drop Traffic Stripe, Skip Yellow
0110	626-F002		675	Linear Feet	6" Thermoplastic Double Drop Edge Stripe, Continuous Yellow
0120	627-L001		157	Each	Two-Way Yellow Reflective High Performance Raised Markers
Bridge Items					
0130	907-808-A003	(S)	1,138	Linear Feet	Joint Repair Without Epoxy
0140	907-823-A001		569	Linear Feet	Preformed Joint Seal, Type I
0150	907-823-B001		1,138	Linear Feet	Saw Cut, Type I
0160	907-824-PP006		144	Each	Bridge Repair, Bearing Replacement
0170	907-824-PP006		25	Each	Bridge Repair, Cap Cleaning

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

CONDITIONS FOR COMBINATION BID

If a bidder elects to submit a combined bid for two or more of the contracts listed for this month's letting, the bidder must complete and execute these sheets of the proposal in each of the individual proposals to constitute a combination bid. In addition to this requirement, each individual contract shall be completed, executed and submitted in the usual specified manner.

Failure to execute this Combination Bid Proposal in each of the contracts combined will be just cause for each proposal to be received and evaluated as a separate bid.

It is understood that the Mississippi Transportation Commission not only reserves the right to reject any and all proposals, but also the right to award contracts upon the basis of lowest separate bids or combination bids most advantageous to the State.

It is further understood and agreed that the Combination Bid Proposal is for comparison of bids only and that each contract shall operate in every respect as a separate contract in accordance with its proposal and contract documents.

I (We) agree to complete each contract on or before its specified completion date.

COMBINATION BID PROPOSAL

This proposal is tendered as one part of a Combination Bid Proposal utilizing option ___* of Subsection 102.11 on the following contracts:

* Option to be shown as either (a), (b), or (c).

	<u>Project No.</u>	<u>County</u>	<u>Project No.</u>	<u>County</u>
1.	_____	_____	6.	_____
2.	_____	_____	7.	_____
3.	_____	_____	8.	_____
4.	_____	_____	9.	_____
5.	_____	_____	10.	_____

(a) If Combination A has been selected, your Combination Bid is complete.

(b) If Combination B has been selected, then complete the following page.

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
1.					
2.					
3.					
4.					
5.					
6.					
7.					
8.					

For Informational Purposes Only

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
9.					
10.					

(c) If Combination C has been selected, then initial and complete ONE of the following.

_____ I (We) desire to be awarded work not to exceed a total monetary value of \$ _____.

_____ I (We) desire to be awarded work not to exceed _____ number of contracts.



**Certification with regard to the Performance of Previous
Contracts or Subcontracts subject to the Equal Opportunity
Clause and the filing of Required Reports**

The Bidder hereby certifies that he has _____, has not _____, participated in a previous contract or subcontract subject to the Equal Opportunity Clause, as required by Executive Orders 10925, 11114, or 11246, and that he has _____, has not _____, filed with the Joint Reporting Committee, the Director of the Office of Federal Contract Compliance, a Federal Government contracting or administering agency, or the former President's Committee on Equal Employment Opportunity, all reports due under the applicable filing requirements.

(COMPANY)

DATE: _____

NOTE: The above certification is required by the Equal Employment Opportunity Regulations of the Secretary of Labor (41 CFR 60-1.7 (b) (1)), and must be submitted by bidders and proposed subcontractors only in connection with contracts and subcontracts which are subject to the Equal Opportunity Clause. Contracts and Subcontracts which are exempt from the Equal Opportunity Clause are set forth in 41 CFR 60-1.5. (Generally only contracts or subcontracts of \$10,000 or under are exempt.)

Currently, Standard Form 100 (EEO-1) is the only report required by the Executive Orders or their implementing regulations.

Proposed prime Contractors and Subcontractors who have participated in a previous contract or subcontract subject to the Executive orders and have not filed the required reports should note that 41 CFR 60-1.7 (b) (1) prevents the award of contracts and subcontracts unless such Contractors submit a report covering the delinquent period or such other period specified by the Federal Highway Administration or by the Director, Office of Federal Contract Compliance, U. S. Department of Labor.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
CERTIFICATION

I, _____,
(Name of person signing bid)

individually, and in my capacity as _____ of
(Title of person signing bid)

_____ do hereby certify under
(Name of Firm, partnership, or Corporation)

penalty of perjury under the laws of the United States and the State of Mississippi that _____

_____, Bidder
(Name of Firm, Partnership, or Corporation)

on Project No. **STP-9999-03(373)/ 108401301000 & STP-9999-03(374)/ 108401302000**

in **Copiah & Sunflower** County(ies), Mississippi, has not either

directly or indirectly entered into any agreement, participated in any collusion; or otherwise taken any action in restraint of free competitive bidding in connection with this contract; nor have any of its corporate officers or principal owners.

Except as noted hereafter, it is further certified that said legal entity and its corporate officers, principal owners, managers, auditors and others in a position of administering federal funds:

- a) Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any Federal department or agency;
- b) Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, or receiving stolen property;
- c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated in (b) above; and
- d) Have not within a three-year period preceding this application/ proposal had one or more public transactions (Federal, State or local) terminated for cause or default.

Do exceptions exist and are made a part thereof? Yes / No

Any exceptions shall address to whom it applies, initiating agency and dates of such action.

Note: Exceptions will not necessarily result in denial of award but will be considered in determining bidder responsibility. Providing false information may result in criminal prosecution or administrative sanctions.

The bidder further certifies that the certification requirements contained in Section XI of Form FHWA 1273, will be or have been included in all subcontracts, material supply agreements, purchase orders, etc. except those procurement contracts for goods or services that are expected to be less than the Federal procurement small purchase threshold fixed at 10 U.S.C. 2304(g) and 41 U.S.C. 253(g) (currently \$25,000) which are excluded from the certification requirements.

The bidder further certifies, to the best of his or her knowledge and belief, that:

1) No Federal appropriated funds have been paid or will be paid to any person for influencing or attempting to influence an officer or employee of any Federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any Federal contract, the making of any Federal grant, the making of any Federal loan, the entering into of any cooperative agreement, and the extension, continuation, renewal, amendment, or modification of any Federal contract, grant, loan, or cooperative agreement.

2) If any funds other than Federal appropriated funds have been paid or will be paid to any person for influencing or attempting to influence an officer or employee of any Federal agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with this contract, Standard Form-LLL, Disclosure Form to Report Lobbying, in accordance with its instructions will be completed and submitted.

The certification contained in (1) and (2) above is a material representation of fact upon which reliance is placed and a prerequisite imposed by Section 1352, Title 31, U.S. Code prior to entering into this contract. Failure to comply shall be subject to a civil penalty of not less than \$10,000 and not more than \$100,000. The bidder shall include the language of the certification in all subcontracts exceeding \$100,000 and all subcontractors shall certify and disclose accordingly.

All of the foregoing is true and correct.

Executed on _____

Signature

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SAM.GOV Registration and DUNS Number

Bidders are advised that the Prime Contractor must maintain current registration in the **System for Award Management** (<http://www.sam.gov>) at all times during the project. A Dun and Bradstreet Data Universal Numbering System (DUNS) Number (<http://www.dnb.com>) is one of the requirements for registration in the System for Award Management.

Bidders are advised that prior to the award of this contract, they **MUST** be registered in the System for Award Management.

I (We) acknowledge that this contract cannot be awarded if I (We) are not registered in the System for Award Management prior to the award of this contract. _____ (Yes / No)

I (We) have a DUNS Number . _____ (Yes / No)

DUNS Number: _____

Company Name: _____

Company e-mail address: _____

(6/2015F)

For Informational Purposes Only

SECTION 902

CONTRACT FOR STP-9999-03(373)/ 108401301000 & STP-9999-03(374)/ 108401302000

LOCATED IN THE COUNTY(IES) OF Copiah & Sunflower

STATE OF MISSISSIPPI,
COUNTY OF HINDS

This contract entered into by and between the Mississippi Transportation Commission on one hand, and the undersigned contractor, on the other witnesseth;

That, in consideration of the payment by the Mississippi Transportation Commission of the prices set out in the proposal hereto attached, to the undersigned contractor, such payment to be made in the manner and at the time of times specified in the specifications and the special provisions, if any, the undersigned contractor hereby agrees to accept the prices stated in the proposal in full compensation for the furnishing of all materials and equipment and the executing of all the work contemplated in this contract.

It is understood and agreed that the advertising according to law, the Advertisement, the instructions to bidders, the proposal for the contract, the specifications, the revisions of the specifications, the special provisions, and also the plans for the work herein contemplated, said plans showing more particularly the details of the work to be done, shall be held to be, and are hereby made a part of this contract by specific reference thereto and with like effect as if each and all of said instruments had been set out fully herein in words and figures.

It is further agreed that for the same consideration the undersigned contractor shall be responsible for all loss or damage arising out of the nature of the work aforesaid; or from the action of the elements and unforeseen obstructions or difficulties which may be encountered in the prosecution of the same and for all risks of every description connected with the work, exceptions being those specifically set out in the contract; and for faithfully completing the whole work in good and workmanlike manner according to the approved Plans, Specifications, Special Provisions, Notice(s) to Bidders and requirements of the Mississippi Department of Transportation.

It is further agreed that the work shall be done under the direct supervision and to the complete satisfaction of the Executive Director of the Mississippi Department of Transportation, or his authorized representatives, and when Federal Funds are involved subject to inspection at all times and approval by the Federal Highway Administration, or its agents as the case may be, or the agents of any other Agency whose funds are involved in accordance with those Acts of the Legislature of the State of Mississippi approved by the Governor and such rules and regulations issued pursuant thereto by the Mississippi Transportation Commission and the authorized Federal Agencies.

The Contractor agrees that all labor as outlined in the Special Provisions may be secured from list furnished by

It is agreed and understood that each and every provision of law and clause required by law to be inserted in this contract shall be deemed to be inserted herein and this contract shall be read and enforced as though it were included herein, and, if through mere mistake or otherwise any such provision is not inserted, then upon the application of either party hereto, the contract shall forthwith be physically amended to make such insertion.

The Contractor agrees that he has read each and every clause of this Contract, and fully understands the meaning of same and that he will comply with all the terms, covenants and agreements therein set forth.

Witness our signatures this the ___ day of _____, _____.

Contractor(s)

By _____

MISSISSIPPI TRANSPORTATION COMMISSION

Title _____

By _____

Signed and sealed in the presence of:
(names and addresses of witnesses)

Executive Director

Secretary to the Commission

Award authorized by the Mississippi Transportation Commission in session on the ___ day of _____, _____, Minute Book No. _____, Page No. _____.

Revised 8/06/2003

SECTION 903
PERFORMANCE AND PAYMENT BOND

CONTRACT BOND FOR: STP-9999-03(373)/ 108401301000 & STP-9999-03(374)/ 108401302000

LOCATED IN THE COUNTY(IES) OF: Copiah & Sunflower

STATE OF MISSISSIPPI,
COUNTY OF HINDS

Know all men by these presents: that we, _____

(Contractor)

Principal, a _____

residing at _____ in the State of _____

and _____

(Surety)
residing at _____ in the State of _____,

authorized to do business in the State of Mississippi, under the laws thereof, as surety, effective as of the contract date

shown below, are held and firmly bound unto the State of Mississippi in the sum of _____

_____ Dollars, lawful money of the United States of America, to be paid to it for which payment well and truly to be made, we bind ourselves, our heirs, administrators, successors, or assigns jointly and severally by these presents.

The conditions of this bond are such, that whereas the said _____

principal, has (have) entered into a contract with the Mississippi Transportation Commission, bearing the date of

_____ day of _____ A.D. _____ hereto annexed, for the construction of certain projects(s) in

the State of Mississippi as mentioned in said contract in accordance with the Contract Documents therefor, on file in the

offices of the Mississippi Department of Transportation, Jackson, Mississippi.

Now therefore, if the above bounden _____

in all things shall stand to and abide by and well and truly observe, do keep and perform all and singular the terms, covenants, conditions, guarantees and agreements in said contract, contained on his (their) part to be observed, done, kept and performed and each of them, at the time and in the manner and form and furnish all of the material and equipment specified in said contract in strict accordance with the terms of said contract which said plans, specifications and special provisions are included in and form a part of said contract and shall maintain the said work contemplated until its final completion and acceptance as specified in Subsection 109.11 of the approved specifications, and save harmless said Mississippi Transportation Commission from any loss or damage arising out of or occasioned by the negligence, wrongful or criminal act, overcharge, fraud, or any other loss or damage whatsoever, on the part of said principal (s), his (their) agents, servants, or employees in the performance of said work or in any manner connected therewith, and shall be liable and responsible in a civil action instituted by the State at the instance of the Mississippi Transportation Commission or any officer of the State authorized in such cases, for double any amount in money or property, the State may lose or be overcharged or otherwise defrauded of, by reason of wrongful or criminal act, if any, of the Contractor(s), his (their) agents or employees, and shall promptly pay the said agents, servants and employees and all persons furnishing labor, material, equipment or supplies therefor, including premiums incurred, for Surety Bonds, Liability Insurance, and Workmen's Compensation Insurance; with the additional obligation that such Contractor shall promptly make payment of all taxes, licenses, assessments, contributions, damages,

any liquidated damages which may arise prior to any termination of said principal's contract, any liquidated damages which may arise after termination of the said principal's contract due to default on the part of said principal, penalties and interest thereon, when and as the same may be due this state, or any county, municipality, board, department, commission or political subdivision: in the course of the performance of said work and in accordance with Sections 31-5-51 et seq. Mississippi Code of 1972, and other State statutes applicable thereto, and shall carry out to the letter and to the satisfaction of the Executive Director of the Mississippi Department of Transportation, all, each and every one of the stipulations, obligations, conditions, covenants and agreements and terms of said contract in accordance with the terms thereof and all of the expense and cost and attorney's fee that may be incurred in the enforcement of the performance of said contract, or in the enforcement of the conditions and obligations of this bond, then this obligation shall be null and void, otherwise to be and remain in full force and virtue.

_____	_____
(Contractors) Principal	Surety
By _____	By _____
	(Signature) Attorney in Fact
	Address _____

Title _____	_____
(Contractor's Seal)	(Printed) MS Agent

	(Signature) MS Agent
	Address _____

	(Surety Seal)

	Mississippi Insurance ID Number



BID BOND

KNOW ALL MEN BY THESE PRESENTS, that we _____
Contractor

Address

City, State ZIP

As principal, hereinafter called the Principal, and _____
Surety

a corporation duly organized under the laws of the state of _____

as Surety, hereinafter called the Surety, are held and firmly bound unto State of Mississippi, Jackson, Mississippi

As Obligee, hereinafter called Obligee, in the sum of **Five Per Cent (5%) of Amount Bid**

Dollars(\$ _____)

for the payment of which sum will and truly to be made, the said Principal and said Surety, bind ourselves, our heirs, executors, administrators, successors and assigns, jointly and severally, firmly by these presents.

WHEREAS, the Principal has submitted a bid for **Bridge Repairs on SR 28 (Bridge Nos. 40.9 & 43.0) & US 49 W (Bridge Nos. 255.2 & 255.2B), known as Federal Aid Project Nos. STP-9999-03(373) / 108401301 & STP-9999-03 (374) / 108401302 in Copiah & Sunflower Counties.**

NOW THEREFORE, the condition of this obligation is such that if the aforesaid Principal shall be awarded the contract, the said Principal will, within the time required, enter into a formal contract and give a good and sufficient bond to secure the performance of the terms and conditions of the contract, then this obligation to be void; otherwise the Principal and Surety will pay unto the Obligee the difference in money between the amount of the bid of the said Principal and the amount for which the Obligee legally contracts with another party to perform the work if the latter amount be in excess of the former, but in no event shall liability hereunder exceed the penal sum hereof.

Signed and sealed this _____ day of _____, 20__

(Witness)

(Principal) (Seal)

By: _____
(Name) (Title)

(Surety) (Seal)

By: _____
(Attorney-in-Fact)

(Witness)

(MS Agent)

Mississippi Insurance ID Number

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
OFFICE OF CIVIL RIGHTS
JACKSON, MISSISSIPPI

LIST OF FIRMS SUBMITTING QUOTES

I/we received quotes from the following firms on:

Letting Date: **September 22, 2020**

Project No: STP-9999-03(373)/ 108401301000 & STP-9999-03(374)/ 108401302000

County: Copiah & Sunflower

Disadvantaged Business Enterprise (DBE) Regulations as stated in 49 CFR 26.11 require the Mississippi Department of Transportation (MDOT) to create and maintain a comprehensive list of all firms quoting/bidding subcontracts on prime contracts and quoting/bidding subcontracts on federally-funded transportation projects. For every firm, we require the following information:

Firm Name: _____
Contact Name/Title: _____
Firm Mailing Address: _____
Phone Number: _____
_____ DBE Firm _____ Non-DBE Firm

Firm Name: _____
Contact Name/Title: _____
Firm Mailing Address: _____
Phone Number: _____
_____ DBE Firm _____ Non-DBE Firm

Firm Name: _____
Contact Name/Title: _____
Firm Mailing Address: _____
Phone Number: _____
_____ DBE Firm _____ Non-DBE Firm

Firm Name: _____
Contact Name/Title: _____
Firm Mailing Address: _____
Phone Number: _____
_____ DBE Firm _____ Non-DBE Firm

Firm Name: _____
Contact Name/Title: _____
Firm Mailing Address: _____
Phone Number: _____
_____ DBE Firm _____ Non-DBE Firm

