

GENERAL INDEX

INCLUDED THIS PROJECT	1st O.REV.	BEGIN WITH SHEET
<input checked="" type="checkbox"/> ROADWAY		1
<input checked="" type="checkbox"/> PERMANENT SIGNS		1001
<input type="checkbox"/> TRAFFIC SIGNALS		2001
<input type="checkbox"/> ITS COMPONENTS		3001
<input type="checkbox"/> LIGHTING		4001
<input type="checkbox"/> (RESERVED)		5001
<input checked="" type="checkbox"/> ROADWAY STANDARD DWGS		6001
<input checked="" type="checkbox"/> BRIDGE STANDARD DWGS		7001
<input checked="" type="checkbox"/> 1997 BOX CULVERT STANDARD DWGS		7501
<input type="checkbox"/> BRIDGE		8001
<input checked="" type="checkbox"/> CROSS SECTIONS		9001

STATE OF MISSISSIPPI

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE OF PROPOSED STATE HIGHWAY FEDERAL AID PROJECT NO. STP-0026-01(080)

SR 19 FROM TUCKER TO PHILADELPHIA NESHOPA COUNTY

101649/302000

R.O.W. 101649/202100 & 103896/201000

SCALES

PLAN	1 IN. = 100 FT.
PROFILE	HOR. 1 IN. = 100 FT.
	VERT. 1 IN. = 10 FT.
LAYOUT	1 IN. = 4,000 FT.

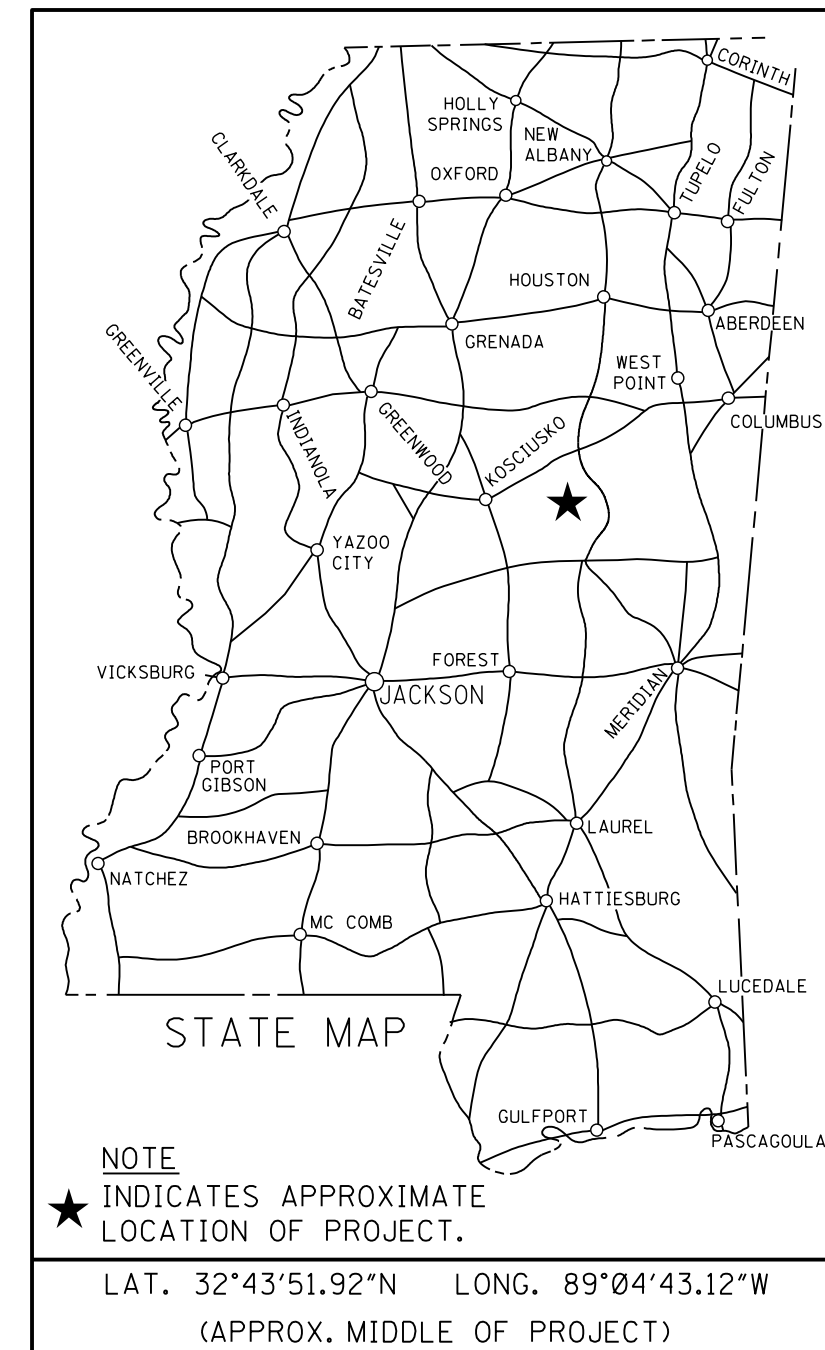
B.O.P. STA 630 + 00.000

E.O.P. STA 868 + 00.00

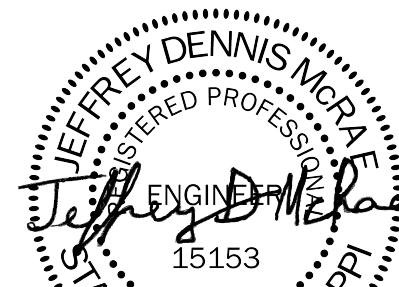
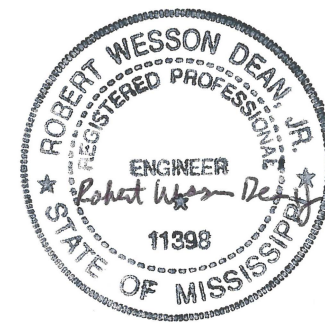
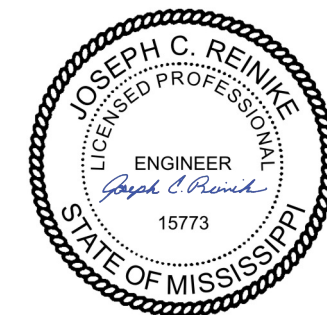
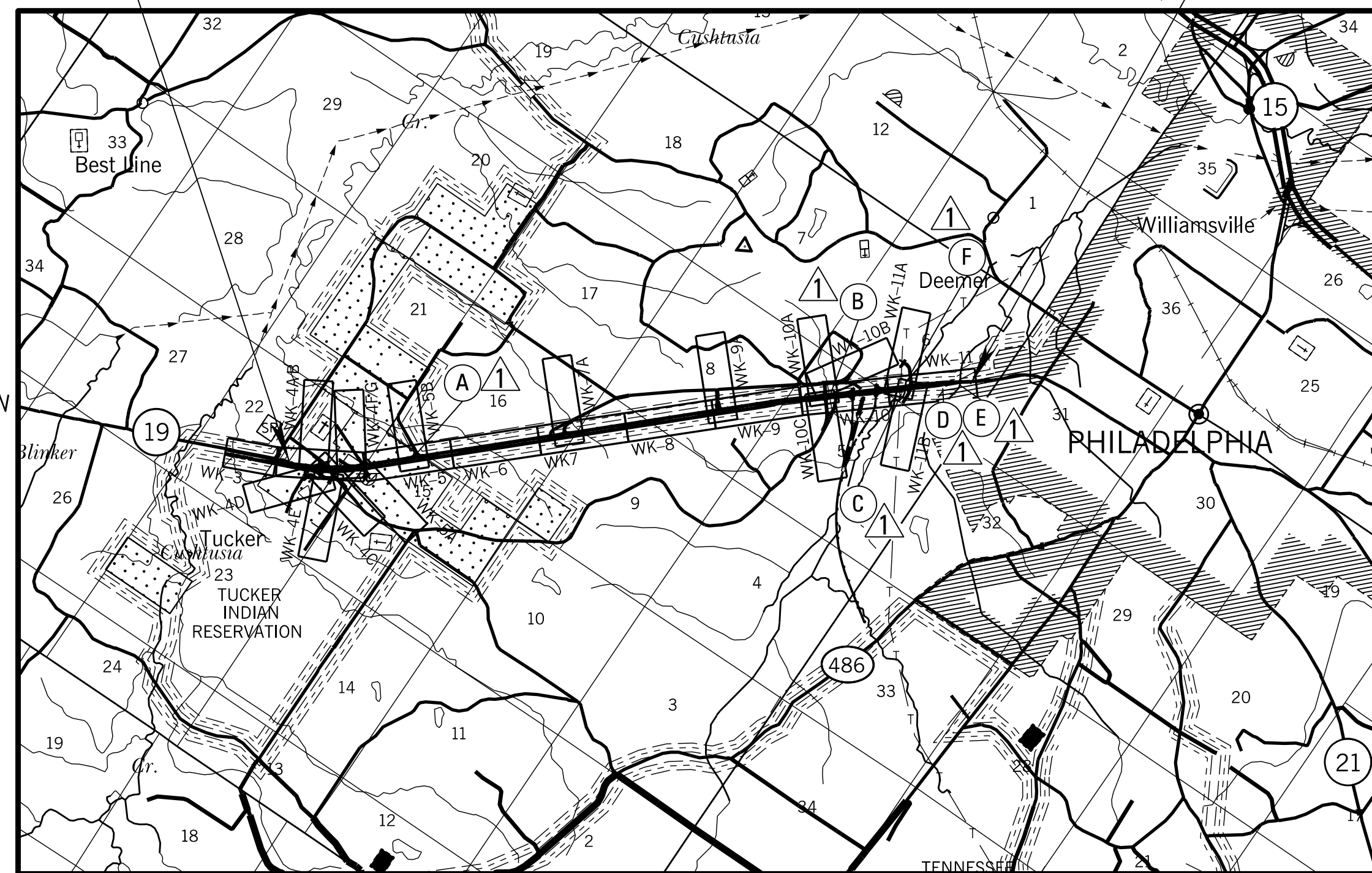
BOX BRIDGES REQ'D.

- ⚠ A SR19 STA. 676+47.07 (DBL 12'x6')
LENGTH ALONG C= 24.00'
- ⚠ B SR19 STA. 820+00.00 (DBL 10'x6')
LENGTH ALONG C= 20.00'
- ⚠ C SR19 STA. 845+14.62 (DBL 10'x8')
LENGTH ALONG C= 23.69'
- ⚠ D SR19 STA. 858+36.19 (DBL 14'x8')
LENGTH ALONG C= 30.68' (LT. EXTENSION)
LENGTH ALONG C= 30.68' (RT. EXTENSION)
- ⚠ E SR19 STA. 865+12.03 (DBL 16'x10')
LENGTH ALONG C= 40.24' (LT. EXTENSION)
LENGTH ALONG C= 40.24' (RT. EXTENSION)
- ⚠ F SR19 STA. 43+80.63 (DBL 10'x8')
LENGTH ALONG C= 31.36'

STATE	PROJECT NUMBER	SHEET NO.
MISSISSIPPI	STP-0026-01(080)	1



DESIGN CONTROL		
65 MPH = V (SPEED DESIGN)		
ADT (2020) = 7,200 ; ADT (2040) = 9,700		
DHV = 1,100 ; D = 60 % T = 10 %		
PERMITS ACQUIRED BY MDOT		
WETLANDS AND WATERS PERMITS		
	WATERS	WETLANDS
NATIONWIDE #14	<input type="checkbox"/>	<input type="checkbox"/>
NATIONWIDE (OTHER)*	<input type="checkbox"/>	<input type="checkbox"/>
GENERAL*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
INDIVIDUAL (404)*	<input type="checkbox"/>	<input type="checkbox"/>
	STORMWATER PERMIT <input checked="" type="checkbox"/>	
Y	REQUIRED, CNOI SUBMITTED BY MDOT (DISTURBED AREA = 5 ACRES)	
S	REQUIRED, SCNOI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES)	
N	NO STORMWATER PERMIT REQUIRED (<1 ACRE)	
APPROVED BY: _____		



DATE: 06/25/2020

ROADWAY

TRAFFIC

BRIDGE

CONVENTIONAL SYMBOLS

- COUNTY LINE
- TOWN CORPORATION LINE
- SECTION LINE
- EXISTING ROAD OR TRAVELED WAY
- PROPOSED ROAD OR TRAVELED WAY
- RAILROAD
- SURVEY LINE
- BRIDGES

EQUATIONS

PT STA. 661 + 44.778 BK = STA. 661 + 00.923 AH = + 43.86

LENGTH DATA

LENGTH OF ROADWAY	23,843.86 FT.	4.52 MI.
LENGTH OF BRIDGES	0 FT.	0 MI.
LENGTH OF PROJECT (NET)	23,843.86 FT.	4.52 MI.
LENGTH OF EXCEPTIONS	0 FT.	0 MI.
LENGTH OF PROJECT (GROSS)	23,844.86 FT.	4.52 MI.

DATE	REVISIONS	BY	APPROVED:
		JCR	DEPUTY EXECUTIVE DIRECTOR / CHIEF ENGINEER
			EXECUTIVE DIRECTOR

B:\17\2020\101649\MTLE.DGN 08/17/2020 10:48 AM TLE.DGN

1st O.REV.

STATE	PROJECT NO.
MISS.	STP-0026-01(080)

DESCRIPTION OF SHEET

WKG. NO. SH. NO.

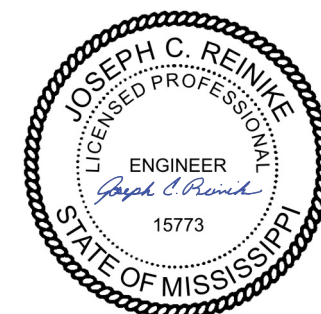
TITLE SHEET (1)			1
DETAILED INDEX & GENERAL NOTES (7)			
DETAILED INDEX	DI-1		2
DETAILED INDEX	DI-2		3
DETAILED INDEX	DI-3		4
DETAILED INDEX	DI-4		5
GENERAL NOTES	GN-1		6
GENERAL NOTES	GN-2		7
GENERAL NOTES	GN-3		8
TYPICAL SECTION SHEETS (9)			
TYPICAL SECTIONS	TS-1		9
TYPICAL SECTIONS	TS-2		10
TYPICAL SECTIONS	TS-3		11
TYPICAL SECTIONS	TS-4		12
TYPICAL SECTIONS	TS-5		13
TYPICAL SECTIONS	TS-6		14
TYPICAL SECTIONS	TS-7		15
TYPICAL SECTIONS	TS-8		16
TYPICAL SECTIONS	TS-9		17
SUMMARY OF QUANTITIES (5)			
SUMMARY OF QUANTITIES	SQ-1		18
SUMMARY OF QUANTITIES	SQ-2		19
SUMMARY OF QUANTITIES	SQ-3		20
SUMMARY OF QUANTITIES	SQ-4		21
SUMMARY OF QUANTITIES	SQ-4		21
SUMMARY OF QUANTITIES	SQ-5		22
ESTIMATED QUANTITIES (18)			
ESTIMATED QUANTITIES - REMOVAL ITEMS	EQ-1		23
ESTIMATED QUANTITIES - REMOVAL ITEMS	EQ-2		24
ESTIMATED QUANTITIES - REMOVAL OF PIPE Δ	EQ-3		25
ESTIMATED QUANTITIES - EARTHWORK ITEMS	EQ-4		26
ESTIMATED QUANTITIES - PAVED FLUMES	EQ-5		27
ESTIMATED QUANTITIES - RIPRAP	EQ-6		28
ESTIMATED QUANTITIES - PAVING ITEMS	EQ-7		29
ESTIMATED QUANTITIES - DRIVEWAY ITEMS	EQ-8		30
ESTIMATED QUANTITIES - DRIVEWAY ITEMS	EQ-9		31
ESTIMATED QUANTITIES - BOX CULVERTS, BOX BRIDGES AND JUNCTION BOXES	EQ-10		32
ESTIMATED QUANTITIES - SIDE DRAINS	EQ-11		33
ESTIMATED QUANTITIES - SIDE DRAINS	EQ-12		34
ESTIMATED QUANTITIES - DRAINAGE ITEMS - CLASS III PIPES	EQ-13		35
ESTIMATED QUANTITIES - DRAINAGE ITEMS - CLASS IV & V PIPES	EQ-14		36
ESTIMATED QUANTITIES - CONCRETE ITEMS	EQ-15		37
ESTIMATED QUANTITIES - PAVEMENT MARKING ITEMS	EQ-16		38
ESTIMATED QUANTITIES - TRAFFIC CONTROL PLAN QUANTITIES	EQ-17		39
ESTIMATED QUANTITIES - SIGNING ITEMS Δ	EQ-18		40

DESCRIPTION OF SHEET

WKG. NO. SH. NO.

PLAN AND PROFILE SHEETS (35)			
PLAN AND PROFILE - MAINLINE		3	41
PLAN AND PROFILE - MAINLINE LT		4LT	42
PLAN AND PROFILE - MAINLINE RT		4RT	43
PLAN AND PROFILE - ALT1A		4A	44
PLAN AND PROFILE - CHURCH DR		4B	45
PLAN AND PROFILE - LR645R		4C	46
PLAN AND PROFILE - LR12		4D	47
PLAN AND PROFILE - SCHDRVW		4E	48
PLAN AND PROFILE - LR65420		4F	49
PLAN AND PROFILE - LR65961		4G	50
PLAN AND PROFILE - MAINLINE LT		5LT	51
PLAN AND PROFILE - MAINLINE RT		5RT	52
PLAN AND PROFILE - ELLIS SAM		5A	53
PLAN AND PROFILE - RLR67775		5B	54
PLAN AND PROFILE - MAINLINE LT		6LT	55
PLAN AND PROFILE - MAINLINE RT		6RT	56
PLAN AND PROFILE - MAINLINE LT		7LT	57
PLAN AND PROFILE - MAINLINE RT		7RT	58
PLAN AND PROFILE - RLR72900		7A	59
PLAN AND PROFILE - MAINLINE LT		8LT	60
PLAN AND PROFILE - MAINLINE RT		8RT	61
PLAN AND PROFILE - MAINLINE LT		9LT	62
PLAN AND PROFILE - MAINLINE RT		9RT	63
PLAN AND PROFILE - RLR7812G		9A	64
PLAN AND PROFILE - MAINLINE LT		10LT	65
PLAN AND PROFILE - MAINLINE RT		10RT	66
PLAN AND PROFILE - OLD19CONN		10A	67
PLAN AND PROFILE - OLD19		10B	68
PLAN AND PROFILE - TUCKER & TUCKERCONN		10C	69
PLAN AND PROFILE - SWRMP19 & SERMP19		10D	70
PLAN AND PROFILE - NWRMP19 & NeRMP19		10E	71
PLAN AND PROFILE - MAINLINE LT		11LT	72
PLAN AND PROFILE - MAINLINE RT		11RT	73
PLAN AND PROFILE - ACCESS RD1		11A	74
PLAN AND PROFILE - SR19FRNT47		11B	75
DETAIL OF INTERSECTION SHEETS (27)			
DETAIL OF INTERSECTION - ALT1A @ H19		DOI-1	76
DETAIL OF INTERSECTION - CROSSOVER @ STA. 645+00		DOI-2	77
DETAIL OF INTERSECTION - LR645R @ H19		DOI-3	78
DETAIL OF INTERSECTION - LR12 @ LR645R		DOI-4	79
DETAIL OF INTERSECTION - SCHDRVW @ LR645R		DOI-5	80
DETAIL OF INTERSECTION - LR65420 @ H19		DOI-6	81
DETAIL OF INTERSECTION - LR65961 @ H19		DOI-7	82
DETAIL OF INTERSECTION - CROSSOVER @ STA. 659+61		DOI-8	83
DETAIL OF INTERSECTION - ELLIS SAM @ H19		DOI-9	84
DETAIL OF INTERSECTION - RLR67775 @ H19		DOI-10	85
DETAIL OF INTERSECTION - CROSSOVER @ STA. 677+75		DOI-11	86
DETAIL OF INTERSECTION - CROSSOVER @ STA. 700+00		DOI-12	87
DETAIL OF INTERSECTION - RLR72900 @ H19		DOI-13	88
DETAIL OF INTERSECTION - CROSSOVER @ STA. 729+00		DOI-14	89
DETAIL OF INTERSECTION - CROSSOVER @ STA. 755+00		DOI-15	90
DETAIL OF INTERSECTION - RLR7812G @ H19		DOI-16	91

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ROADWAY



PS & E PLANS - 07-20-2020		
FMS CON. # 101649/302000		
REVISIONS		
DATE	SHEET NO.	BY
08/13/20	1, 2, 5, 16, 18-22, 27, 28, 32, & 168	JCR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION DETAIL INDEX	COUNTY: NESHOBA PROJ. NO.: STP-0026-01(080)	WORKING NUMBER DI-1 SHEET NUMBER 2
	FILENAME: DI.DGN	
	DESIGN TEAM: STANTEC	
	CHECKED: _____ DATE: _____	

STATE	PROJECT NO.
MISS.	STP-0026-01(080)

DESCRIPTION OF SHEET

WKG. NO. SH. NO.

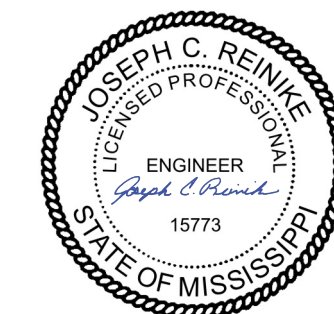
PERMANENT SIGNING SHEETS (12)		
PERMANENT SIGNING PLANS - PERMANENT SIGNS	PSP-1	1001
PERMANENT SIGNING PLANS - PERMANENT SIGNS	PSP-2	1002
PERMANENT SIGNING PLANS - PERMANENT SIGNS	PSP-3	1003
PERMANENT SIGNING PLANS - PERMANENT SIGNS	PSP-4	1004
PERMANENT SIGNING PLANS - PERMANENT SIGNS	PSP-5	1005
PERMANENT SIGNING PLANS - PERMANENT SIGNS	PSP-6	1006
PERMANENT SIGNING PLANS - PERMANENT SIGNS	PSP-7	1007
PERMANENT SIGNING PLANS - PERMANENT SIGNS	PSP-8	1008
PERMANENT SIGNING PLANS - PERMANENT SIGNS	PSP-9	1009
PERMANENT SIGNING PLANS - PERMANENT SIGN DETAILS	PSP-10	1010
PERMANENT SIGNING PLANS - PERMANENT SIGN DETAILS	PSP-11	1011
PERMANENT SIGNING PLANS - GUIDE SIGN DETAILS	GSD-1	1012
ROADWAY STANDARD DRAWINGS (90)		
CONCRETE ISLAND PAVEMENT DETAILS	CIP-1	6011
PAVEMENT MARKING DETAILS FOR 2-LANE & 4-LANE DIVIDED ROADWAYS	PM-1	6051
PAVEMENT MARKING LEGEND DETAILS	PM-5	6055
PAVEMENT MARKING LEGEND DETAILS	PM-6	6056
4-LANE TO 2-LANE TRANSITION AT INTERCHANGE	PM-8	6058
TYPICAL PAVEMENT MARKING DETAIL FOR MEDIAN CROSSEOVERS	PM-9	6059
2-WAY RAISED PAVEMENT MARKERS AT INTERSECTING ROADS (2-LANE)	PM-11	6061
2-WAY RAISED PAVEMENT MARKERS AT INTERSECTING ROADS (4-LANE)	PM-12	6062
RUMBLE STRIPES 2-LANE HIGHWAYS (ASPHALT LANES, 2-FT ASPHALT SHOULDERS)	RS-1	6064
RUMBLE STRIPES 2-LANE HIGHWAYS (ASPHALT LANES, 2-FT OR WIDER ASPHALT SHOULDERS)	RS-2	6065
TYPICAL TEMPORARY EROSION CONTROL/SEDIMENT CONTROL APPLICATIONS	ECD-1	6101
DETAILS OF SEDIMENT BARRIER APPLICATIONS	ECD-2	6102
DETAILS OF SILT FENCE INSTALLATION	ECD-3	6103
DITCH CHECK STRUCTURES, TYPICAL APPLICATIONS AND DETAILS	ECD-4	6104
TEMPORARY EROSION, SEDIMENT, AND WATER POLLUTION CONTROL MEASURES (SILT FENCE AND HAY BALE DITCH CHECKS)	ECD-5	6105
DETAILS OF EROSION CONTROL WATTLE DITCH CHECK	ECD-6	6106
DETAILS OF EROSION CONTROL SILT DIKE DITCH CHECK	ECD-7	6107
ROCK DITCH CHECK	ECD-8	6108
ROCK FILTER DAM	ECD-9	6109
ROCK DITCH CHECK WITH SUMP EXCAVATION AND ROCK FILTER DAM	ECD-10	6110
TYPICAL APPLICATIONS AND DETAILS FOR INLET CONSTRUCTION	ECD-11	6111
INLET PROTECTION DETAILS FOR SEDIMENT CONTROL STONE ON GRADES AND SAGS	ECD-12	6112
INLET PROTECTION DETAILS OF WATTLES	ECD-13	6113
INLET PROTECTION DETAILS OF MANUFACTURED INLET PROTECTION DEVICE	ECD-14	6114
INLET PROTECTION DETAILS OF SANDBAGS	ECD-15	6115
STABILIZED CONSTRUCTION ENTRANCE	ECD-16	6116
TEMPORARY STREAM DIVERSION	ECD-18	6118
TEMPORARY STREAM DIVERSION (BOX EXTENSION)	ECD-19	6119
FLOATING TURBIDITY CURTAIN	ECD-20	6120
DETAILS OF EROSION CONTROL SANDBAG DITCH CHECK	ECD-21	6121
SEDIMENT RETENTION BARRIER	ECD-22	6122
DETAILS OF TYPICAL DITCH TREATMENTS	DT-1	6123
DITCH TREATMENT INSTALLATION DETAIL FOR SOIL REINFORCING MAT	DT-1A	6124
TYPICAL TEMPORARY EROSION CONTROL MEASURES (TYPE D SILT BASIN) (135 CU. YDS. CAPACITY PER ACRE OF DRAINAGE)	BAS-D	6129
SUPER SILT FENCE	SSF-1	6130
EROSION CONTROL BLANKET	ECB-1	6131
CONCRETE MEDIAN BARRIER (PRECAST) (32")	CMB-3	6226
STANDARD ROADSIDE SIGNS	SN-3A	6304
STANDARD ROADSIDE SIGNS	SN-3B	6305
STANDARD ROADSIDE SIGN ASSEMBLY AND INSTALLATION	SN-4	6306
STANDARD ROADSIDE SIGN ASSEMBLY AND INSTALLATION	SN-4A	6307
TYPICAL INSTALLATION OF GROUND MOUNTED DIRECTIONAL SIGNS	SN-5	6309
BREAKAWAY SIGN SUPPORTS	SN-6B	6312
TYPICAL INSTALLATION AND DETAILS OF DELINEATORS AND DISTANCE REFERENCE SIGNS	SN-8	6314
TYPICAL INSTALLATION OF DELINEATORS	SN-8A	6315
TYPICAL CROSSEOVER DELINEATION	SN-8B	6316
TRAFFIC CONTROL PLAN WITH FLAGGER (ONE-LANE CLOSURE OF TWO-WAY TRAFFIC)	TCP-1	6351
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT LESS THAN 65 MPH (4-LANE: MEDIAN LANE OR OUTSIDE LANE CLOSURE) (WORK DAY ONLY)	TCP-2	6352
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT LESS THAN 65 MPH (4-LANE: MEDIAN LANE OR OUTSIDE LANE CLOSURE) (EXTENDED PERIOD)	TCP-3	6353

DESCRIPTION OF SHEET

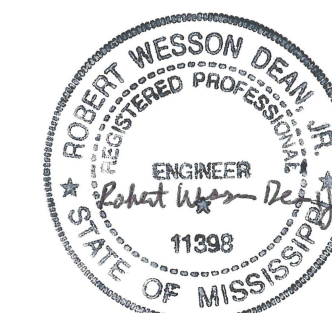
WKG. NO. SH. NO.

ROADWAY STANDARD DRAWINGS (CONT'D)		
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (EXTENDED PERIOD)	TCP-4	6354
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIAN LANE OR OUTSIDE LANE CLOSURE) (WORK DAY ONLY)	TCP-5	6355
SHORT DURATION CLOSING OF TWO-LANE TWO-WAY HIGHWAYS	TCP-6	6356
SHORT DURATION CLOSING OF DIVIDED HIGHWAYS	TCP-7	6357
HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS	TCP-8	6358
TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO-LANE ROADS	TCP-9	6359
TRAFFIC CONTROL PLAN FOR TEMPORARY CONSTRUCTION CROSSEOVER (WORK DAY ONLY)	TCP-11	6361
TRAFFIC CONTROL PLAN: UNEVEN PAVEMENT DETAILS	TCP-12	6362
TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS	TCP-13	6363
LOCATION OF R16-3 SIGNS (SPEEDING FINES DOUBLED)	TCP-15	6365
TRAFFIC CONTROL DETAILS DRUM PLACEMENT AND SHOULDER CLOSURE	TCP-16	6366
PERMANENT BARRICADE WITH BERM	TCP-17	6367
RIGHT-OF-WAY MARKER	RW-1	6401
RURAL DRIVEWAYS	RD-1	6403
TYPICAL GRADING TRANSITION BETWEEN CUTS & FILLS	GT-1	6404
SIGHT FLARE	SF-1	6405
SUPERELEVATION TRANSITION FOR LOCAL FACILITIES (V < 45 mph)	SE-1	6407
SUPERELEVATION - CASE I (ROTATION ABOUT CENTERLINE)	SE-2A	6408
SUPERELEVATION - CASE II (ROTATION ABOUT EDGE OF TRAVELED WAY)	SE-2B	6409
SUPERELEVATION RUNOFF - CASE I (ROTATION ABOUT THE CENTERLINE)	SE-3A	6413
SUPERELEVATION RUNOFF - CASE II (ROTATION ABOUT THE EDGE OF TRAVELED WAY)	SE-3B	6414
DRIVEWAYS, CURB & GUTTER, & SIDEWALK	SD-1	6419
MISCELLANEOUS DETAIL SHEET 1. STACKED PIPE JOINTS 2. EXCAVATION AT GRADE POINTS	MDS-1	6425
DETAILS OF PAVED FLUMES	PF-1	6426
PIPE CULVERT INSTALLATION	PI-1	6501
FLEXIBLE PIPE CULVERT INSTALLATION	PI-2	6502
CONCRETE PIPE COLLAR	PC-1	6503
JUNCTION BOX FOR PIPE CULVERTS	JB-1	6504
JUNCTION BOX TYPE 2 FOR TRAFFIC LOAD (MAXIMUM "W" = 9.3 FT.)	JB-2	6506
BRANCH CONNECTIONS	BC-1	6507
TYPE I MEDIAN INLET (24" PIPE AND UNDER)	MI-1	6508
TYPE I MEDIAN INLET (29" TO 51" PIPE)	MI-1A	6509
TYPE II MEDIAN INLET (51" PIPE AND UNDER)	MI-2	6511
TYPE II MEDIAN INLET (OVER 51" PIPE)	MI-2A	6512
MEDIAN INLET FOR BOX CULVERTS (TYPE I & II)	MI-3	6513
MEDIAN INLET (FLUSH WITH FORESLOPE)	MI-4	6514
DETAILS OF GRATES FOR MEDIAN INLETS	IG-1	6516
PAVED INLET APRON AND MEDIAN DITCH PLUG	PA-1	6520
DROP INLET AND GRATE DETAILS FOR PIPE AND BOX CULVERTS	B-9	6527
FLARED END SECTION FOR CONCRETE PIPE	FE-1	6530
FLARED END SECTION FOR CONCRETE ARCH PIPE	FE-1A	6531

7/6/2020 1:04:56 AM DI.DGN ROADWAY PLAN DIVISION MISSISSIPPI DEPARTMENT OF TRANSPORTATION



ROADWAY



TRAFFIC

MISSISSIPPI DEPARTMENT OF TRANSPORTATION DETAIL INDEX	
REVISION	COUNTY: NESHOBA
DATE	PROJ. NO.: STP-0026-01(080)
DESIGN TEAM	WORKING NUMBER DI-3
STANTEC	SHEET NUMBER 4
CHECKED	FILENAME: DI.DGN
DATE	DESIGN TEAM

STATE	PROJECT NO.
MISS.	STP-0026-01(080)

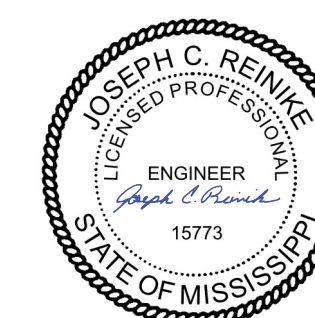
GENERAL NOTES (CONT.)

19. THE CONTRACTOR SHALL COORDINATE WITH THE CONTRACTOR FROM ADJACENT PROJECT(S) IN IMPLEMENTING THE TRAFFIC CONTROL PLAN AS DIRECTED BY THE ENGINEER. ALL CONFLICTING SIGNS SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.
20. THE CONTRACTOR SHALL COVER OR REMOVE ANY TEMPORARY TRAFFIC CONTROL SIGNS SHOWN IN THE TRAFFIC CONTROL PLAN THAT DO NOT APPLY TO THE CURRENT PHASE.
21. REMOVAL OF RAISED PAVEMENT MARKERS THAT ARE IN CONFLICT WITH REQUIRED CONSTRUCTION IS NOT CONSIDERED A SEPARATE PAY ITEM. COST TO BE ABSORBED IN OTHER ITEMS BID.
22. REMOVAL OF OBJECT MARKERS IS NOT CONSIDERED A SEPARATE PAY ITEM, AND SHALL BE ABSORBED IN OTHER ITEMS BID.
23. WHERE MILLING IS REQUIRED, THE CONTRACTOR SHALL PROVIDE OUTLETS IN THE EXISTING SHOULDERS AT SUFFICIENT INTERVALS TO PREVENT POOLING OR STANDING WATER ON THE MILLED SURFACE, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
24. THE EROSION CONTROL DEVICES REFERENCED IN THESE PLANS ARE A MINIMUM REQUIREMENT. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT SILT DOES NOT LEAVE THE RIGHT OF WAY OR CONTAMINATE WATERS OF THE U. S. DURING CONSTRUCTION. THE CONTRACTOR SHALL SUBMIT AN EROSION CONTROL PLAN PRIOR TO COMMENCEMENT OF WORK AND MAINTAIN THE PLAN DURING CONSTRUCTION. ANY ADDITIONAL SILT BASINS NOT SHOWN IN THE PLANS SHALL BE INCLUDED IN THE CONTRACTOR'S EROSION CONTROL PLAN PRIOR TO SUBMITTING FOR APPROVAL.
25. PRIOR TO EARTHWORK OPERATIONS, THE EXISTING TOP 4" TOPSOIL IS TO BE STRIPPED AND STOCKPILED. AFTER THE GRADING OPERATIONS ARE COMPLETED, SAID TOPSOIL SHALL BE PLACED ON ALL AREAS THAT ARE NOT TO BE PAVED OR OTHERWISE PROTECTED, IN ACCORDANCE WITH SECTION 211 OF THE SPECIFICATIONS, OR THE VEGETATION SCHEDULE (SEE WK. SH. VS-1). EXISTING TOPSOIL AND ALL COSTS ASSOCIATED WITH STRIPPING, HAULING, STOCKPILING, AND PLACEMENT OF THE EXISTING TOPSOIL IS TO BE ABSORBED IN OTHER EARTHWORK ITEMS.
26. FOR CLEARING LIMITS ADJACENT TO THE STREAMS AT STATIONS 643+00, 662+20, 676+67, 698+40, 733+61, 761+97, 799+22, 820+00, 845+15, 865+12, SEE WORKING SHEET NUMBERS ECP-RB-3 THROUGH ECP-RB-10. THE CLEARING LIMITS SHOWN ON THESE SHEETS ARE ONLY FOR THE RIPARIAN BUFFER CLEARING. CLEARING AT OTHER LOCATIONS SHOULD STILL APPLY.

GENERAL NOTES (CONT.)

27. THE CONTRACTOR IS RESPONSIBLE FOR FIELD-VERIFICATION OF EXISTING GRADES AND MAKING ADJUSTMENTS AS NECESSARY WITH THE APPROVAL OF THE PROJECT ENGINEER.
28. TEMPORARY STRIPING SHALL CONFORM TO FINISHED STRIPE SPECIFICATIONS FOR ALIGNMENT, NEATNESS, AND STRAIGHTNESS.
29. ALL ITEMS OF WORK ASSOCIATED WITH THE INSTALLATION AND MAINTENANCE OF CONSTRUCTION ENTRANCES SHALL BE ABSORBED IN OTHER ITEMS OF WORK.
30. IF COLORS ARE USED ON PLAN/PROFILE SHEETS, THEY ARE INTENDED TO VISUALLY EASE THE LOCATION OF ELEMENTS FOR USERS OF THESE DRAWINGS. ALTHOUGH THE INTENT IS TO CATEGORIZE EVERYTHING AS EITHER EXISTING OR PROPOSED, IT IS THE END USER'S RESPONSIBILITY TO ENSURE ALL ELEMENTS ARE INTERPRETED CORRECTLY REGARDLESS OF COLOR.
31. ALL ADDENDA TO THESE PLANS WILL BE POSTED TO WWW.MDOT.MS.GOV UNDER THE PROPOSAL ADDENDA COLUMN. BIDDERS ARE ADVISED THAT HARD COPIES OF ANY ADDENDA FOR THIS PROJECT WILL NOT BE MAILED. IT IS THE BIDDER'S RESPONSIBILITY TO CHECK AND SEE IF ANY ADDENDA HAVE BEEN POSTED FOR THIS PROJECT.
32. CURB AND GUTTER VERTICAL DIMENSIONS SHOWN IN THE DETAIL DRAWINGS ARE FOR A CURB IN THE "CATCH" CONFIGURATION AND SHALL BE CONSIDERED TO BE MINIMUM DIMENSIONS. THE DIMENSIONS MAY BE MODIFIED AS NECESSARY FOR "SPILL" CURB AND GUTTER, BUT SHALL NOT BE LESS THAN THE MINIMUM SHOWN.
33. THE COST FOR REMOVAL OF ALL HEADWALLS AND WINGWALLS (PIPES, BOX CULVERTS, BOX BRIDGES) SHALL BE ABSORBED IN OTHER ITEMS BID.
34. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING AND RELOCATING MAIL BOXES AS NECESSARY TO MAINTAIN CONTINUOUS MAIL SERVICE THROUGHOUT THE LIFE OF THE PROJECT, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
35. INSTALLATION DATES SHALL BE CLEARLY WRITTEN IN BOLD BLACK MARKINGS ON THE BACK BOTTOM HALF OF ALL SIGNS WITH A PERMANENT MARKING STICK THAT IS WATERPROOF, FADE RESISTANT AND MARKS ON WET OR DRY SURFACES.
36. ALL POST, PIPE, AND I-BEAM LENGTHS IN THESE PLANS ARE ESTIMATES. POST LENGTHS FOR ALL SIGNS SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO FABRICATION.

6/25/2020 1:39 PM GN.DGN



MISSISSIPPI DEPARTMENT OF TRANSPORTATION											
GENERAL NOTES											
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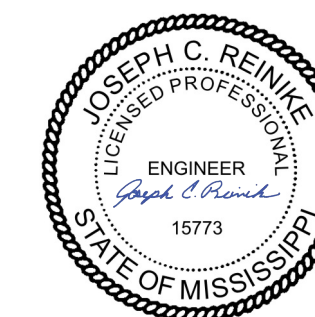



STATE	PROJECT NO.
MISS.	STP-0026-01(080)

GENERAL NOTES (CONT.)

- 37. ALL EXISTING SIGNS WHICH ARE TO BE REMOVED AS A PART OF THIS PROJECT THAT ARE NOT IN CONFLICT WITH CONSTRUCTION SHALL REMAIN IN PLACE UNTIL NEW SIGNS ARE INSTALLED UNLESS NOTED OR DIRECTED OTHERWISE BY THE PROJECT ENGINEER. ROADWAY SIGNS THAT ARE IN CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND RELOCATED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID. NON-STANDARD MDOT SIGNS SUCH AS *BUCKLE UP*, *PICK IT UP, MISSISSIPPI*, ETC. SHALL BE REMOVED IF THEY CONFLICT WITH CONSTRUCTION. THE SIGNS SHALL BE ADEQUATELY STORED AND RESET AT APPROXIMATELY THE SAME LOCATION OR AT A LOCATION DETERMINED BY THE PROJECT ENGINEER AT THE COMPLETION OF THE PROJECT.
- 38. ALL EXISTING SIGNS AND SUPPORTS REMOVED UNDER THIS PROJECT SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND ARE NOT A SEPARATE PAY ITEM.
- 39. SOME AREAS OF THE PROJECT DESIGNATED FOR CLEARING AND GRUBBING WHERE UTILITY LINES, UTILITY GUY WIRES, OR OTHER STRUCTURES ARE PRESENT MAY HAVE TO BE CLEARED BY HAND METHODS. THE CONTRACTOR SHALL TAKE PRECAUTIONS TO PREVENT DAMAGE OR DISTURBANCE TO THE RELEVANT STRUCTURES.
- 40. THE CONTRACTOR SHALL COORDINATE AND CONDUCT WORK AT LOCAL ROADS AND DRIVEWAYS IN A MANNER SUCH THAT ACCESS IS NOT INTERRUPTED UNNECESSARILY. ACCESS SHALL BE PRESERVED IN THE BEST MANNER POSSIBLE. COORDINATION AND COMMUNICATION WITH LANDOWNERS MAY BE NECESSARY TO PREVENT INTERRUPTION OF DRIVEWAY ACCESS.
- 41. TEMPORARY PAVEMENT JOINTS (PAPER JOINTS) SHALL BE EMPLOYED AT ALL LOCATIONS REQUIRING TRAFFIC TO TRAVERSE AN UNEVEN PAVEMENT JOINT. PAPER JOINTS SHALL BE A MINIMUM OF 9 FEET IN LENGTH AND SHALL BE ADEQUATELY MAINTAINED.
- 42. PAYMENT FOR SAW CUTS WILL BE MADE USING THE APPROPRIATE PAY ITEMS. IF MILLING TECHNIQUES ARE USED, THE AREA WILL NOT REQUIRE SAW CUTS, BUT CARE SHOULD BE EXERCISED TO CREATE A NEAT REMOVAL LINE AND TO PREVENT DAMAGE TO THE ADJACENT PAVEMENT STRUCTURE. IF SAW CUTS ARE USED IN CONJUNCTION WITH MILLING, PAYMENT WILL BE MADE USING THE APPROPRIATE PAY ITEMS. PAYMENT WILL NOT BE MADE FOR SAW CUTS NOT PERFORMED.

6/25/2020 1:39 PM GN.DGN



MISSISSIPPI DEPARTMENT OF TRANSPORTATION GENERAL NOTES		 WORKING NUMBER GN-3
COUNTY: NESHOPA PROJ. NO.: STP-0026-01(080)		
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