		INDEX	GENERAL	
	BEGIN WITH SHEET	O.REV.		INCLUDED THIS PROJECT
_				ROAD
		S		
PI		•••••••••••••••••		
	- 6001	ARD DWGS	-	
FEDERAL	.7001	DWGS		
		T STANDARD DWGS		
			X BRIDGES	
		7 (DBL 12'×6') 24.00'	STA. 676+47.07 TH ALONG ፎ= 24	A SR19 LENG
B.O.P. STA 630+0		00 (DBL 10'×6') 20.00'	STA.820+00.00 TH ALONG Q= 20	B SR19 LENG
		2(DBL 10'×8') 23.69'	STA. 845+14.62 TH ALONG @= 23	C SR19 LENG
		9 (DBL 14'×8') 50.68' (LT. EXTENSION) 50.68' (RT. EXTENSION)	STA. 858+36.19 TH ALONG Q= 30 TH ALONG Q= 30	D SR19 LENG LENG
		3 (DBL 16'×10') 10.24' (LT. EXTENSION) 10.24' (RT. EXTENSION)	STA. 865+12.03 TH ALONG Q= 40 TH ALONG Q= 40	E SR19 LENG LENG
TO MERIDIA		(DBL 10'×8') 31.36'	STA. 43+80.63 TH ALONG @= 31	F SR19 LENG
ATE : 06/25/2020	Jeff ENGI	CHINESSON O CHINEED PROFESSION ENGINEER H POLICI ULAN DOL H 11398 OF MISSISSI	ENGINEER 15773 OF MISSISS	STATIC STATIC
BRIDGE		TRAFFIC	ADWAY	ROA

CONVENTIONAL SYMBOLS

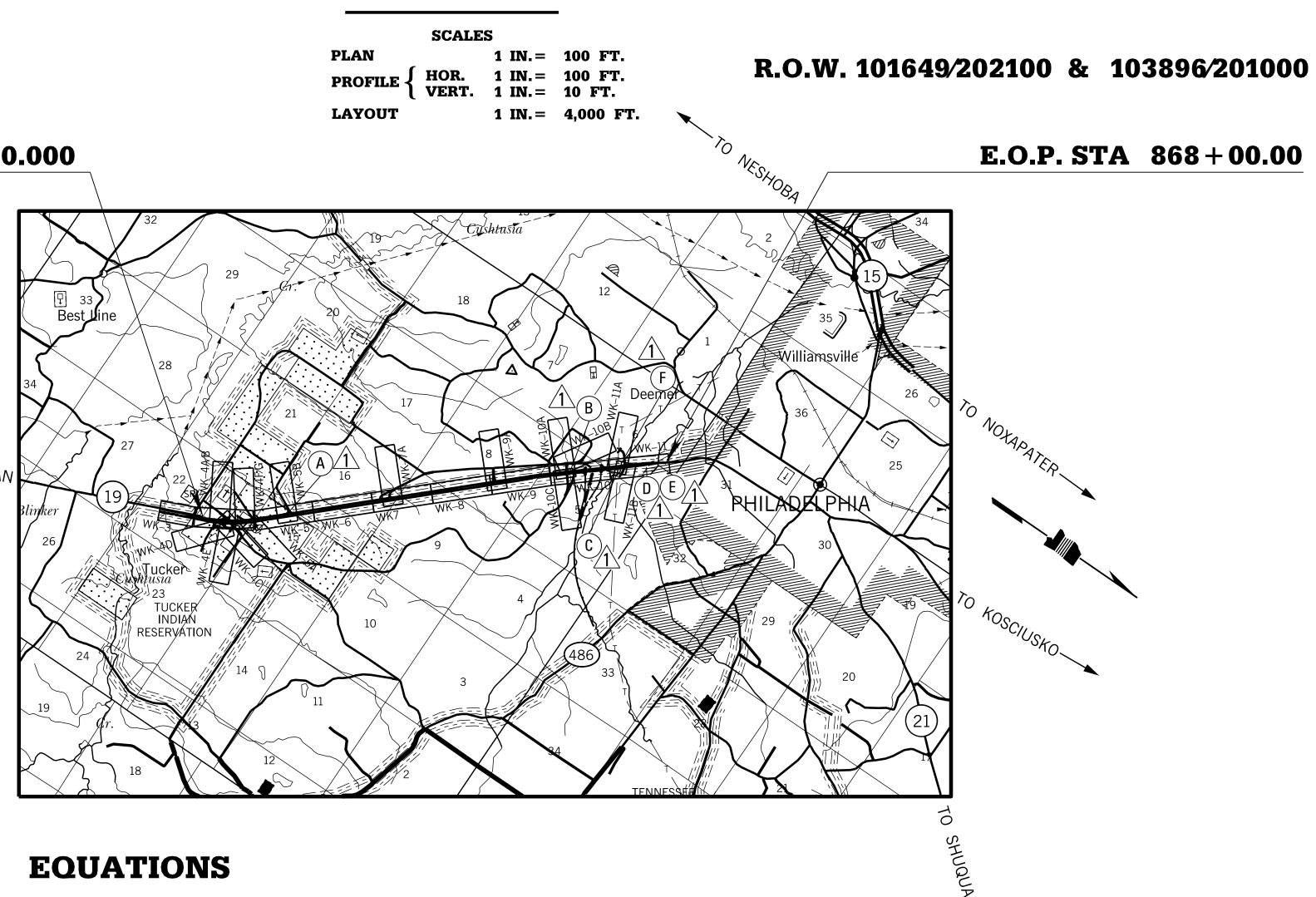
COUNTY LINE
TOWN CORPORATION LINE
SECTION LINE
EXISTING ROAD OR TRAVELED WAY
PROPOSED ROAD OR TRAVELED WAY
RAILROAD
SURVEY LINE
BRIDGES

STATE OF MISSISSIPPI

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

LAN AND PROFILE OF PROPOSED **STATE HIGHWAY** AID PROJECT NO. STP-0026-01(080)

SR 19 FROM TUCKER TO PHILADELPHIA **NESHOBA COUNTY**



PT STA. 661 + 44.778 BK = STA. 661 + 00.923 AH = +43.86

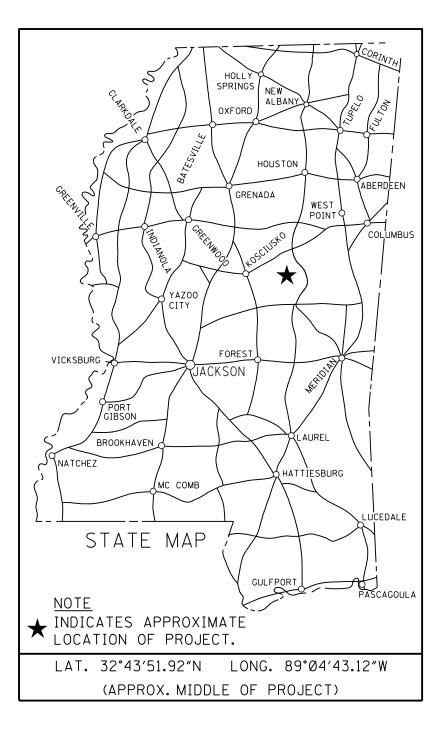
LENGTH DATA

ENGTH OF ROADWAY	23,843.86 FT.	4.52 MI.
ENGTH OF BRIDGES	FT.	Ø MI.
ENGTH OF PROJECT (NET)	23,843.86	4.52 MI.
ENGTH OF EXCEPTIONS	Ø FT.	Ø MI
ENGTH OF PROJECT (GROSS)	23,844.86	4.52 MI.

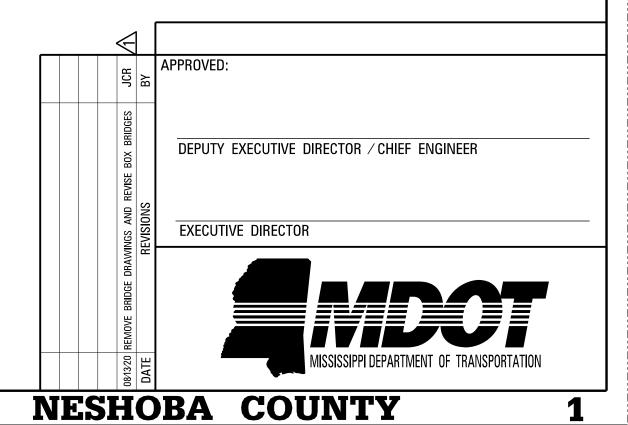
STP-0026-01(080)

P.E. SP-0026-01(080) 101649-102000

STATE	PROJECT NUMBER	SHEET NO.
MISSISSIPPI	STP-0026-01(080)	1



DESIGN C 65 MPH = V (SF		N)
ADT (<u>2020</u>) = <u>7,200</u> : A DHV = <u>1,100</u> : D =		= <u>9,700</u> ;= <u>10</u> %
PERMITS ACQUI	RED BY I	MDOT
WETLANDS AND V	VATERS PERM	ITS
	WATERS	WETLANDS
NATIONWIDE #14	Ν	Ν
NATIONWIDE (OTHER)*	Ν	Ν
GENERAL*	Υ	Υ
INDIVIDUAL (404)*	Ν	Ν
STORMWATER P	ermit	Y
Y REQUIRED, CNOI SUB (DISTURBED AR	MITTED BY M EA=5 ACRES	DOT)
S REQUIRED, SCNOI TO CONTRACTOR (1 1	BE SUBMITTE	D BY S)
N no stormwater perm	IT REQUIRED	(<1 ACRE)
APPROVED BY:		



101649/302000

E.O.P. STA 868+00.00

1st O.REV.

DESCRIPTION OF SHEET

TITLE	SHEET	(1)

DETAILED	INDEX	&	GENERAL	NOTES	(7)

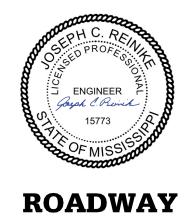
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WKG. SH. NO. NO.

FMS CON: 101649/302000		
	STATE	PROJECT NO.
	MISS.	STP-0026-01(080)
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PS & E PLANS - 07-20-2020			
FMS	CON. # 101649/302000	0	
	REVISIONS		
DATE	SHEET NO.	ΒY	
08/13/20	1, 2, 5, 16, 18-22, 27, 28, 32, & 168	JCR	

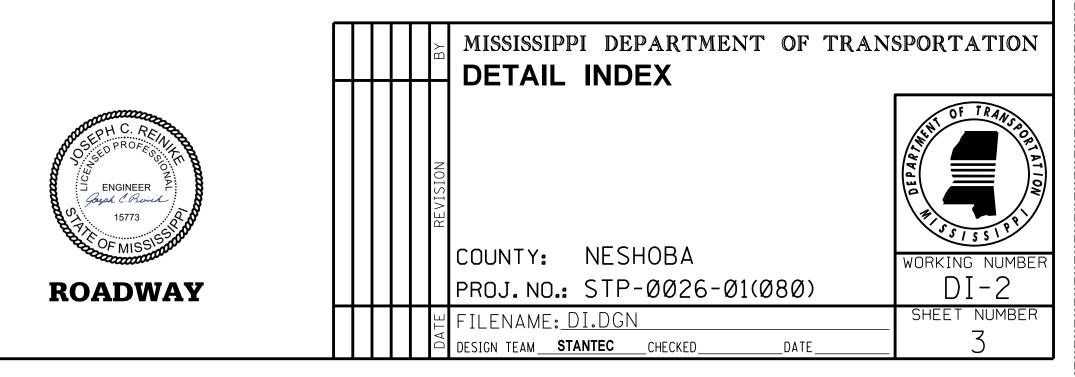


	E			
	JCR	BΥ	MISSISSIPPI DEPARTMENT OF TRAN DETAIL INDEX	SPORTATION
	REVISE SHEET NAME	REVISION		OF TRANSPORTATION HILVELIG
			COUNTY: NESHOBA	WORKING NUMBER
			PROJ.NO.: STP-0026-01(080)	DI-1
	3/20	ΤE	FILENAME: <u>DI.DGN</u>	SHEET NUMBER
	08/1	DA	DESIGN TEAM STANTEC CHECKED DATE	2

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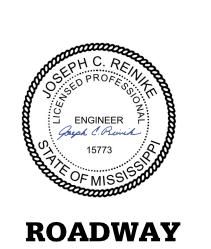
NO.	WKG.
	<u>NO.</u>

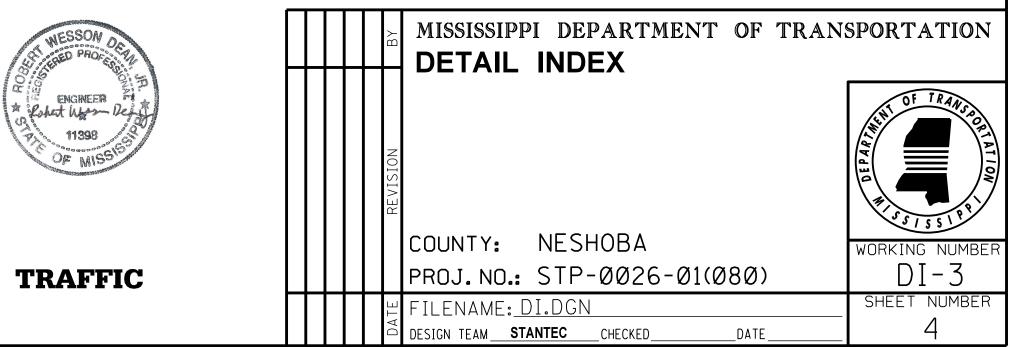
FMS CON: 101649/30200	00	
	STA	TE PROJECT NO.
	MISS	S. STP-0026-01(080)
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FMS CON: 101649/302000		
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	MISS.	STP-0026-01(080)
	WKG.	SH.
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TRAFFIC CONTROL DETAILS DRUM PLACEMENT AND SHOULDER CLOSURE	TCP-16	6366
PERMANENT BARRICADE WITH BERM	TCP-17	6367
RIGHT-OF-WAY MARKER RURAL DRIVEWAYS	RW-1 RD-1	<u>64Ø1</u> 64Ø3
TYPICAL GRADING TRANSITION BETWEEN CUTS & FILLS	GT-1	6404
SIGHT FLARE	SF-1	6405
SUPERELEVATION TRANSITION FOR LOCAL FACILITIES (V < 45 mph)	SE-1	6407
SUPERELEVATION - CASE I (ROTATION ABOUT CENTERLINE)	SE-2A	<u>64Ø8</u> 64Ø9
SUPERELEVATION - CASE II (ROTATION ABOUT EDGE OF TRAVELED WAY) SUPERELEVATION RUNOFF - CASE I (ROTATION ABOUT THE CENTERLINE)	SE-2B SE-3A	6409
SUPERELEVATION RUNOFF - CASE II (ROTATION ABOUT THE EDGE OF TRAVELED WAY)	SE-3B	6414
DRIVEWAYS, CURB & GUTTER, & SIDEWALK	SD-1	6419
MISCELLANEOUS DETAIL SHEET 1. STACKED PIPE JOINTS 2. EXCAVATION AT GRADE POINTS	MDS-1	6425
DETAILS OF PAVED FLUMES PIPE CULVERT INSTALLATION	PF-1 PI-1	<u>6426</u> 65Ø1
FLEXIBLE PIPE CULVERT INSTALLATION	PI-2	6502
CONCRETE PIPE COLLAR	PC-1	65Ø3
JUNCTION BOX FOR PIPE CULVERTS	JB-1 JB-2	6504
JUNCTION BOX TYPE 2 FOR TRAFFIC LOAD (MAXIMUM "W" = 9.3 FT.) BRANCH CONNECTIONS	BC-1	<u>65Ø6</u> 65Ø7
TYPE I MEDIAN INLET (24" PIPE AND UNDER	MI-1	6508
TYPE I MEDIAN INLET (29" TO 51" PIPE)	MI-1A	65Ø9
TYPE II MEDIAN INLET (51" PIPE AND UNDER	MI-2	6511
TYPE II MEDIAN INLET (OVER 51" PIPE) MEDIAN INLET FOR BOX CULVERTS (TYPE I & II)	MI-2A MI-3	<u>6512</u> 6513
MEDIAN INLET (FLUSH WITH FORESLOPE)	MI-4	6514
DETAILS OF GRATES FOR MEDIAN INLETS	IG-1	6516
PAVED INLET APRON AND MEDIAN DITCH PLUG	PA-1	6520
DROP INLET AND GRATE DETAILS FOR PIPE AND BOX CULVERTS FLARED END SECTION FOR CONCRETE PIPE	B-9 FE-1	<u>6527</u> 653Ø
FLARED END SECTION FOR CONCRETE ARCH PIPE	FE-1A	6531
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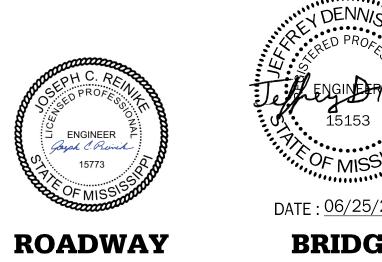
DESCRIPTION OF SHEET

BOX CULVERT STANDARD DRAWINGS (39) BASIC CULVERT DRAWING - COLLAR LOCATIONS - NORMAL AND SKEWED CULVERTS CROUP I DIACRAM BASIC CULVERT DRAWING - COLLAR LOCATIONS - NORMAL AND SKEWED CULVERTS GROUP II DIACRAM BASIC CULVERT DRAWING - COLLAR LOCATIONS - NORMAL AND SKEWED CULVERTS GROUP III DIACRAM COLLAR DETAILS FCR BOX STRUCTURES BARREL DETAILS - SINGLE CELL - HEIGHT 6 FT SPANS 6-20 FT. BARREL DETAILS - SINGLE CELL - HEIGHT 6 FT SPANS 6-20 FT. BARREL DETAILS - SINGLE CELL - HEIGHT 6 FT SPANS 6-20 FT. BARREL DETAILS - SINGLE CELL - HEIGHT 6 FT SPANS 6-20 FT. WINGS WITH 31 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 15' SKEW DETAILS - HEIGHTS 6 - 12 FT SPANS 6 - 24 FT. WINCS WITH 31 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 15' SKEW DETAILS - HEICHT 6 FT SPANS 6 - 20 FT. WINGS WITH 31 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 15' SKEW DETAILS - HEICHT 6 FT SPANS 6 - 20 FT. WINGS WITH 31 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 30' SKEW DETAILS - HEIGHT 6 FT SPANS 6 - 20 FT. WINGS WITH 31 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 30' SKEW DETAILS - HEIGHTS 6 - 12 FT. SPANS 6 - 24 FT. WINGS WITH 31 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 30' SKEW DETAILS - HEIGHTS 6 - 12 FT. SPANS 6 - 24 FT. WINGS WITH 31 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 30' SKEW DETAILS - HEIGHTS 6 - 12 FT. SPANS 6 - 24 FT. WINGS WITH 31 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 30' SKEW DETAILS - HEIGHT 6 FT SPANS 6 - 20 FT. WINGS WITH 31 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 30' SKEW DETAILS - HEIGHT 6 FT SPANS 6 - 20 FT. WINGS WITH 31 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 30' SKEW DETAILS - HEIGHT 6 FT SPANS 6 - 20 FT. WINGS WITH 31 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 45' SKEW DETAILS - HEIGHT 6 FT SPANS 6 - 20 FT. WINGS WITH 31 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 45' SKEW DETAILS - HEIGHTS 6 - 12 FT SPANS 6 - 24 FT. WINGS WITH 31 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL	IBJL-1 IBJL-1 IBJL-1 ICJ-1 IBS-6 IBS-6 IBS-6 IBS-6 IWS-3W-15 IWS-6-3W-15 IWS-6-3W-15 IWS-6-3W-3Ø IWS-6-3W-3Ø IWS-6-3W-3Ø IWS-6-3W-3Ø IWS-6-3W-45	7005 7000 7000 7001 7012 7012 7013 7052 7052 7052 7052 7052 7052
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WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 15° SKEW DETAILS - HEIGHT 6 FT SPANS 6 - 20 FT. WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 15° SKEW DETAILS - HEIGHT 6 FT SPANS 6 - 20 FT. WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 30° SKEW DETAILS - HEIGHTS 6 - 12 FT SPANS 6 - 24 FT. WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 30° SKEW DETAILS - HEIGHT 6 FT SPANS 6 - 20 FT. WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 30° SKEW DETAILS - HEIGHT 6 FT SPANS 6 - 20 FT. WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 30° SKEW DETAILS - HEIGHT 6 FT SPANS 6 - 20 FT. WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 30° SKEW DETAILS - HEIGHT 6 FT HEIGHTS 6 - 12 FT SPANS 6 - 24 FT. WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 45° SKEW DETAILS - HEIGHTS 6 - 12 FT SPANS 6 - 24 FT. WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 45° SKEW DETAILS - HEIGHTS 6 - 12 FT SPANS 6 - 24 FT. WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 45° SKEW DETAILS - HEIGHTS 6 - 12 FT SPANS 6 - 24 FT.	IWS-6-3W-15 IWS-3W-3Ø IWS-6-3W-3Ø IWS-6-3W-3Ø	7054 7075 7076
WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 15° SKEW DETAILS - HEIGHT 6 FT SPANS 6 - 20 FT.WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 30° SKEW DETAILS - HEIGHTS 6 - 12 FT SPANS 6 - 24 FT.WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 30° SKEW DETAILS - HEIGHT 6 FT SPANS 6 - 20 FT.WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 30° SKEW DETAILS - HEIGHT 6 FT SPANS 6 - 20 FT.WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 30° SKEW DETAILS - HEIGHT 6 FT SPANS 6 - 20 FT.WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 45° SKEW DETAILS - HEIGHT 6 FT HEIGHTS 6 - 12 FT SPANS 6 - 24 FT.WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 45° SKEW DETAILS - HEIGHTS 6 - 12 FT SPANS 6 - 24 FT.WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 45° SKEW DETAILS - HEIGHTS 6 - 12 FT SPANS 6 - 24 FT.WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 45° SKEW DETAILS - HEIGHTS 6 - 12 FT SPANS 6 - 24 FT.WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 45° SKEW DETAILS - HEIGHTS 6 - 12 FT SPANS 6 - 24 FT.	IWS-3W-3Ø IWS-6-3W-3Ø IWS-6-3W-3Ø	7Ø75 7Ø76
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VINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 45° SKEW DETAILS - HEIGHTS 6 - 12 FT SPANS 6 - 24 FT.		7100
	IWS-3W-45	71Ø1
vings with 3:1 slope for basic culvert drawing - single cell - 45° skew details - height 6 ft	IWS-6-3W-45	71Ø2
SPANS 6 - 20 FT.		
/ <u>INGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 45° SKEW DETAILS - HEIGHT 6 FT</u> SPANS 6 - 20 FT.	IWS-6-3W-45	7103
/INGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - SINGLE CELL - 45° SKEW DETAILS - HEIGHT 6 FT SPANS 6 - 20 FT.	IWS-6-3W-45	710-
ARREL DETAILS - DOUBLE CELL - HEIGHT 6 FT SPANS 12-32 FT.	IBD-6	7115
ARREL DETAILS - DOUBLE CELL - HEIGHT 6 FT SPANS 12-32 FT.	IBD-6	7116
ARREL DETAILS - DOUBLE CELL - HEIGHT 6 FT SPANS 12-32 FT.	IBD-6	7117
<u>ARREL DETAILS - DOUBLE CELL - HEIGHT 8 FT SPANS 16-32 FT.</u> ARREL DETAILS - DOUBLE CELL - HEIGHT 8 FT SPANS 16-32 FT.	IBD-8 IBD-8	7118
ARREL DETAILS - DOUBLE CELL - HEIGHT 8 FT SPANS 16-32 FT.	IBD-8	712
INGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - ذSKEW DETAILS -	IWD-3W	713
HEIGHTS 6 - 12 FT SPANS 12 - 40 FT. /INGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - 0° SKEW DETAILS - HEIGHT 6 FT	IWD-6-3W	713
SPANS 12 - 32 FT. INGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - ذSKEW DETAILS - HEIGHT 6 FT	IWD-6-3W	7138
SPANS 12 - 32 FT. 'INGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - ذSKEW DETAILS - HEIGHT 8 FT	IWD-8-3W	7139
SPANS 16 - 32 FT.		
<u>/INGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - ذSKEW DETAILS - HEIGHT 8 FT</u> SPANS 16 - 32 FT.	IWD-8-3W	7140
<u>'INGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - 15° SKEW DETAILS -</u> HEIGHTS 6 - 12 FT SPANS 12 - 40 FT.	IWD-3W-15	7158
INGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - 15° SKEW DETAILS -	IWD-3W-15	7159
HEIGHTS 6 - 12 FT SPANS 12 - 40 FT. INGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - 15° SKEW DETAILS - HEIGHT 8 FT	IWD-8-3W-15	716
SPANS 16 - 32 FT. INGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - 15° SKEW DETAILS - HEIGHT 8 FT	IWD-8-3W-15	716
SPANS 16 - 32 FT. INGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - 15° SKEW DETAILS - HEIGHT 8 FT	IWD-8-3W-15	716
SPANS 16 - 32 FT. INGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - 45° SKEW DETAILS -	IWD-3W-45	721
HEIGHTS 6 - 12 FT SPANS 12 - 40 FT. INGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - 45° SKEW DETAILS -	IWD-3W-45	721
HEIGHTS 6 - 12 FT SPANS 12 - 40 FT. INGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - 45° SKEW DETAILS - HEIGHT 8 FT	IWD-8-3W-45	721
SPANS 16 - 32 FT. INGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - 45° SKEW DETAILS - HEIGHT 8 FT	IWD-8-3W-45	7219
SPANS 16 - 32 FT.		
VINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE CELL - 45° SKEW DETAILS - HEIGHT 8 FT SPANS 16 - 32 FT.	IWD-8-3W-45	722

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	DESCRIPTION OF
-	1997 BOX CULVERT STANDARD DRAWINGS (13)
-	BASIC CULVERT DRAWING - COLLAR LOCATIONS - NORMAL AND SH BASIC CULVERT DRAWING - COLLAR LOCATIONS - NORMAL AND SH COLLAR DETAILS FOR BOX STRUCTURES (SINGLE, DOUBLE, TRIPLE SKEWED COLLAR DETAILS FOR BOX STRUCTURES (SINGLE, DOUBLE, DOUBLE CELL - HEIGHT 8 FT SPANS 16 - 32 FT. DOUBLE CELL - HEIGHT 8 FT SPANS 16 - 32 FT. DOUBLE CELL - HEIGHT 10 FT SPANS 20 - 36 FT. DOUBLE CELL - HEIGHT 10 FT SPANS 20 - 36 FT. WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE C WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE C WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE C WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE C WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE C WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE C WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE C WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE C WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE C WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE C WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE C WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE C WINGS WITH 3:1 SLOPE FOR BASIC CULVERT DRAWING - DOUBLE C
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-	CROSS SECTION SHEETS (365)
-	CROSS SECTIONS
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<u>/</u> 1	TOTAL SHEETS (703)
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		<u> </u>		
WED CULVERTS GROUP I DIAGRAMS	IBJL-1	-97	75Ø1	
WED CULVERTS GROUP II DIAGRAMS	IBJL-1		7502	
QUADRUPLE) TRIPLE & QUADRUPLE)	ICJ-1- ICJS-1		75Ø4 75Ø5	
		-2W-97	7530	
		-2W-97	7531	
)-2W-97)-2W-97	7532	
LL - HEIGHTS 6 - 12 FT SPANS 12 - 40 FT.	IWD-3-	-97	7536	
LL - HEIGHTS 6 - 12 FT SPANS 12 - 40 FT. LL - HEIGHTS 6 - 12 FT SPANS 12 - 40 FT.	<u>IWD-3-</u> IWD-3-		7537 7538	
ELL CULVERTS		Ø-3W-97	7556	
ELL CULVERTS	ISK-30	Ø-3W-97	7557	
			9001 - 9365	

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IS Anna	JCR BY	MISSISSIPPI DEPARTMENT OF TRAN DETAIL INDEX	SPORTATION
ARGA ARA	BRIDGE SHEETS AND ADJUST TOTALS REVISION		OF TRANSPORTATION WILL BORNAL MILSSISSIPPI
0/2020		COUNTY: NESHOBA	WORKING NUMBER
GE	REMOVE	PROJ.NO.: STP-ØØ26-Ø1(Ø8Ø)	DI-4
	3/20 TE	FILENAME: <u>DI.DGN</u>	SHEET NUMBER
	08/13/20 DATE	DESIGN TEAM STANTEC CHECKED DATE	5

GENERAL NOTES

- 1. THE LOCATION AND SPACING OF SIGNS, SHOWN ON THE TRAFFIC CONTROL PLANS, ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.
- 2. ALL TRAFFIC CONTROL DEVICES ON THIS PROJECT SHALL COMPLY WITH THE *MUTCD* (LATEST EDITION).
- 3. ALL PLASTIC DRUMS SHALL HAVE A BALLASTING COLLAR MADE FROM RECYCLED TRUCK TIRES OR OTHER SUITABLE MATERIAL.
- 4. A SOIL PROFILE HAS BEEN PREPARED FOR THIS PROJECT USING SAMPLES TAKEN FROM HOLES AT THE LOCATIONS INDICATED IN THE TEST REPORTS. THIS SOIL PROFILE IS ON FILE IN THE DISTRICT AND CENTRAL CONSTRUCTION OFFICES AND IS AVAILABLE FOR EXAMINATION. THE DEPARTMENT DOES NOT GUARANTEE THAT THE MATERIALS AS SHOWN IN THE REPORTS ARE NECESSARILY TO BE FOUND OUTSIDE THE TEST HOLES.
- 5. 25% SHRINKAGE FACTOR USED IN THE EARTHWORK CALCULATIONS IS FOR DESIGN ESTIMATING PURPOSES ONLY.
- 6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING STRUCTURES SUCH AS, BUT NOT LIMITED TO, PIPES, INLETS, APRONS, AND BRIDGES FROM DAMAGE WHICH MIGHT OCCUR DURING CONSTRUCTION. THE CONTRACTOR SHALL REPLACE OR REPAIR, AS DIRECTED BY THE ENGINEER, ANY STRUCTURES DAMAGED DURING THE LIFE OF THE CONTRACT. NO PAYMENT WILL BE MADE FOR REPLACEMENT OR REPAIR OF DAMAGED ITEMS.
- 7. THE TOP THREE FEET AND VARIABLE OF THE DESIGN SOILS (BOTH NATURAL AND EMBANKMENT) SHALL BE CONSTRUCTED OF SOIL CLASSIFIED AS A-6(10) OR BETTER, PER AASHTO DESIGNATION: M 145-91, EXCEPT AT UNDERCUT LOCATIONS DESIGNATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER TO RECEIVE CLASS B-15 BORROW EXCAVATION. EXTREME CARE SHALL BE EXERCISED IN UNDERCUT AREAS, AND THE UNDERCUT DEPTH MAY BE ADJUSTED AT CROSS DRAINS AS DIRECTED BY THE ENGINEER. FOR ADDITIONAL DETAILS THE CONTRACTOR IS REFERRED TO THE NOTICE TO BIDDERS ON DESIGN SOIL MATERIAL IN THE CONTRACT PROPOSAL DOCUMENT.
- 8. ALL PIPE JOINTS ARE TO BE WRAPPED COMPLETELY IN 24-INCH WIDE TYPE V GEOTEXTILE FABRIC. ALL PICKUP HOLES SHALL BE PLUGGED AND COVERED WITH TYPE V GEOTEXTILE FABRIC, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
- 9. VOIDS CREATED BY THE REMOVAL OF, BUT NOT LIMITED TO, POSTS, CONCRETE ANCHORS, AND FOOTINGS SHALL BE BACK-FILLED AND TAMPED IN ACCORDANCE WITH SECTION 203 OF THE MISSISSIPPI STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE COST OF WHICH WILL BE ABSORBED IN OTHER ITEMS BID.
- 10. UTILITIES ON THE DRAWINGS ARE SHOWN IN THEIR ORIGINAL LOCATION BASED UPON THE BEST INFORMATION AVAILABLE TO THE ENGINEER. UTILITIES THAT WERE FOUND TO BE IN CONFLICT WITH CONSTRUCTION HAVE BEEN RELOCATED. PERMITS ARE ON FILE WITH THE DEPARTMENT SHOWING THE APPROXIMATE LOCATION OF UTILITIES RELOCATED WITHIN THE RIGHT-OF-WAY. THE ENGINEER CAN NOT AND DOES NOT WARRANT THAT THIS INFORMATION IS COMPLETE OR ACCURATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING DIRECTLY WITH THE INVOLVED UTILITY OWNERS TO HAVE UNDERGROUND UTILITY LINES FIELD LOCATED IN ADVANCE OF CONSTRUCTION.

GENERAL NOTES (CONT.)

- 11. WORK ON STRUCTURES FOR THIS PROJECT REQUIRI PROPERTIES. THEREFORE, THE RISK OF A FAILURE OC EXERCISED. THE CONTRACTOR SHALL BE RESPONSIBL SYSTEM THAT IS DEEMED NECESSARY TO PREVENT THE PUBLIC THAT MAY BE ABOVE THE EXCAVATION DESIGNING, DRAWING, AND CONSTRUCTING THE FAC
- 12. SOME WORK IS REQUIRED OUTSIDE THE PROJECT LI EXCEPT AS PROVIDED BY SPECIFIC PAY ITEMS INCL
- 13. WIRE FENCE BACKING WILL BE REQUIRED FOR ALL
- 14. FULL COLLARS ARE TO BE USED AT ALL BOX CULV (SEE WK. NO. ICJ-1 FOR DETAILS)
- 15. LIST OF PUBLIC UTILITIES

P.0 ME	ST MISSISSIPPI CTRIC POWER A BOX 5517 RDIAN, MS 3930 —483—7361		P.O. BOX	120 .PHIA, MS 2601
-	NTRAL WATER A			
	VALLEY VIEW			
PHI	LADELPHIA, MS	39350	P.O. BOX	1731
GLE	ENN GOODMAN	OR	MERIDIAN	N, MS 39
JOI	IN WILKERSON			
601	-656-6171		CENTERP	oint en
			426 W.E	
	LADELPHIA UTIL			
	ATER LINE, SEWE	R LINE	800–371–	5417
PO	BOX 88			
PHI	LADELPHIA, MS -656-1601 OR -656-8633	39350	TENNESS	ee valli
601	–656–1601 OR		TRANSM	SSION
601	-656-8633			
CHI	ris higginboth	AM	STEVEN	R. GARRI

- 16. FLUORESCENT ORANGE SHEETING SHALL BE USED O THOSE DESIGNATED ON THE PLANS TO BE BLACK L
- 17. THE COST OF ANY COLLARS REQUIRED TO CONNECT SHALL BE ABSORBED IN THE COST FOR NON-CONCI
- 18. VEGETATIVE MATERIAL WILL BE REMOVED PRIOR TO ABSORBED IN OTHER ITEMS BID.



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CURRING DURING E FOR PLACING A FAILURE AND OR ANY STRUCT	IN THE IMMEDIATE VICINITY OF TRA EXCAVATION REQUIRES THAT EXT WHAT BRACING, SHORING, OR GROU PROTECT THE PERSONS WORKING FURES ADJACENT TO THE EXCAVAT NCLUDED IN THE PRICE BID FOR C	REME CAU JND SUPPO NEAR TH	JTION BI ORT IE EXCA\ COSTS F	E /ATION,	
MITS. NO ADDITIO UDED IN THE PLA	ONAL COMPENSATION WILL BE MA ANS.	DE FOR SI	JCH WO	RK	
SILT FENCE. (SEE	WK. NO. ECD–3)				
ERT EXTENSIONS	AND AT ALL BOX CULVERT CONST	RUCTION	JOINTS.		
TIC CIATION MS 39350	P.O. BOX 67 (9155 HWY 19)60COLLINSVILLE, MS 39325BF601-626-813825	NITY 01–503–452 RENT SONI 51–654–719	ES OR 6		
UNK 39301	DUFFEE WATER ASSOC.CHUCKY-DUFFEE ROADDUFFEE, MS601-986-521660	BARRY BUCHANAN MISSISSIPPI BAND OF CHOCTAW INDIANS 601–416–1513 RICKY COOK			
ENTERGY N ST. MS 39350	AT&T OF MS AT&T OF MS RT SITE 601–482–0194 JEREMY MALLEY OR 601–693–8801 TIM NEAL				
ALLEY AUTHORITY J RRISON, CEP	MAXX SOUTH 662–617–4554 TWENTIS MAGEE				
	CTION AND TRAFFIC CONTROL SIGN RDER ON WHITE BACKGROUND.	IS EXCEPT	FOR		
r concrete flar Rete pipe.	ED END SECTIONS TO NON-CONCF	Rete Pipe	SECTION	S	
PLACEMENT OF	GRANULAR MATERIAL. THE COST (of which	SHALL	BE	
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GENERAL NOTES (CONT.)

- 19. THE CONTRACTOR SHALL COORDINATE WITH THE CONTRACTOR FROM ADJACENT PROJECT(S) IN IMPLEMENTING THE TRAFFIC CONTROL PLAN AS DIRECTED BY THE ENGINEER. ALL CONFLICTING SIGNS SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.
- 20. THE CONTRACTOR SHALL COVER OR REMOVE ANY TEMPORARY TRAFFIC CONTROL SIGNS SHOWN IN THE TRAFFIC CONTROL PLAN THAT DO NOT APPLY TO THE CURRENT PHASE.
- 21. REMOVAL OF RAISED PAVEMENT MARKERS THAT ARE IN CONFLICT WITH REQUIRED CONSTRUCTION IS NOT CONSIDERED A SEPARATE PAY ITEM. COST TO BE ABSORBED IN OTHER ITEMS BID.
- 22. REMOVAL OF OBJECT MARKERS IS NOT CONSIDERED A SEPARATE PAY ITEM, AND SHALL BE ABSORBED IN OTHER ITEMS BID.
- 23. WHERE MILLING IS REQUIRED, THE CONTRACTOR SHALL PROVIDE OUTLETS IN THE EXISTING SHOULDERS AT SUFFICIENT INTERVALS TO PREVENT POOLING OR STANDING WATER ON THE MILLED SURFACE, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
- 24. THE EROSION CONTROL DEVICES REFERENCED IN THESE PLANS ARE A MINIMUM REQUIREMENT. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT SILT DOES NOT LEAVE THE RIGHT OF WAY OR CONTAMINATE WATERS OF THE U.S. DURING CONSTRUCTION. THE CONTRACTOR SHALL SUBMIT AN EROSION CONTROL PLAN PRIOR TO COMMENCEMENT OF WORK AND MAINTAIN THE PLAN DURING CONSTRUCTION. ANY ADDITIONAL SILT BASINS NOT SHOWN IN THE PLANS SHALL BE INCLUDED IN THE CONTRACTOR'S EROSION CONTROL PLAN PRIOR TO SUBMITTING FOR APPROVAL.
- 25. PRIOR TO EARTHWORK OPERATIONS, THE EXISTING TOP 4" TOPSOIL IS TO BE STRIPPED AND STOCKPILED. AFTER THE GRADING OPERATIONS ARE COMPLETED, SAID TOPSOIL SHALL BE PLACED ON ALL AREAS THAT ARE NOT TO BE PAVED OR OTHERWISE PROTECTED, IN ACCORDANCE WITH SECTION 211 OF THE SPECIFICATIONS, OR THE VEGETATION SCHEDULE (SEE WK. SH. VS–1). EXISTING TOPSOIL AND ALL COSTS ASSOCIATED WITH STRIPPING, HAULING, STOCKPILING, AND PLACEMENT OF THE EXISTING TOPSOIL IS TO BE ABSORBED IN OTHER EARTHWORK ITEMS.
- 26. FOR CLEARING LIMITS ADJACENT TO THE STREAMS AT STATIONS 643 + 00, 662 + 20, 676 + 67, 698 + 40, 733 + 61, 761 + 97, 799 + 22, 820 + 00, 845 + 15, 865 + 12, SEE WORKING SHEET NUMBERS ECP_RB_3 THROUGH ECP_RB_10. THE CLEARING LIMITS SHOWN ON THESE SHEETS ARE ONLY FOR THE RIPARIAN BUFFER CLEARING. CLEARING AT OTHER LOCATIONS SHOULD STILL APPLY.

GENERAL NOTES (CONT.)

- 27. THE CONTRACTOR IS RESPONSIBLE FOR FIELD-VERIFICATION NECESSARY WITH THE APPROVAL OF THE PROJECT EN
- 28. TEMPORARY STRIPING SHALL CONFORM TO FINISHED STRAIGHTNESS.
- 29. ALL ITEMS OF WORK ASSOCIATED WITH THE INSTALL BE ABSORBED IN OTHER ITEMS OF WORK.
- 30. IF COLORS ARE USED ON PLAN/PROFILE SHEETS, THEY USERS OF THESE DRAWINGS. ALTHOUGH THE INTENT IT IS THE END USER'S RESPONSIBILITY TO ENSURE AL
- 31. ALL ADDENDA TO THESE PLANS WILL BE POSTED TO BIDDERS ARE ADVISED THAT HARD COPIES OF ANY A IT IS THE BIDDER'S RESPONSIBILITY TO CHECK AND S
- 32. CURB AND GUTTER VERTICAL DIMENSIONS SHOWN IN CONFIGURATION AND SHALL BE CONSIDERED TO BE M NECESSARY FOR "SPILL" CURB AND GUTTER, BUT SHA
- 33. THE COST FOR REMOVAL OF ALL HEADWALLS AND WARD ABSORBED IN OTHER ITEMS BID.
- 34. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINT CONTINUOUS MAIL SERVICE THROUGHOUT THE LIFE OF ITEMS BID.
- 35. INSTALLATION DATES SHALL BE CLEARLY WRITTEN IN WITH A PERMANENT MARKING STICK THAT IS WATE
- 36. ALL POST, PIPE, AND I-BEAM LENGTHS IN THESE PLA IN THE FIELD BY THE CONTRACTOR PRIOR TO FABRIC



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CATION OF EXISTING GRADES AND MAKING ADJUSTMENTS AS NGINEER.	5	
STRIPE SPECIFICATIONS FOR ALIGNMENT, NEATNESS, AND		
LATION AND MAINTENANCE OF CONSTRUCTION ENTRANCES SI	HALL	
ARE INTENDED TO VISUALLY EASE THE LOCATION OF ELEMENT IS TO CATEGORIZE EVERYTHING AS EITHER EXISTING OR PRO ILL ELEMENTS ARE INTERPRETED CORRECTLY REGARDLESS OF (POSED,	
WWW.MDOT.MS.GOV UNDER THE PROPOSAL ADDENDA COLU ADDENDA FOR THIS PROJECT WILL NOT BE MAILED. SEE IF ANY ADDENDA HAVE BEEN POSTED FOR THIS PROJECT		
N THE DETAIL DRAWINGS ARE FOR A CURB IN THE "CATCH" MINIMUM DIMENSIONS. THE DIMENSIONS MAY BE MODIFIED ALL NOT BE LESS THAN THE MINIMUM SHOWN.		
VINGWALLS (PIPES, BOX CULVERTS, BOX BRIDGES) SHALL BE		
TAINING AND RELOCATING MAIL BOXES AS NECESSARY TO M F THE PROJECT, THE COST OF WHICH SHALL BE ABSORBED IN		
N BOLD BLACK MARKINGS ON THE BACK BOTTOM HALF OF A ERPROOF, FADE RESISTANT AND MARKS ON WET OR DRY SUR		NS
ANS ARE ESTIMATES. POST LENGTHS FOR ALL SIGNS SHALL B CATION.	e verif	IED

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GENERAL NOTES (CONT.)

- 37. ALL EXISTING SIGNS WHICH ARE TO BE REMOVED AS A PART OF THIS PROJECT THAT ARE NOT IN CONFLICT WITH CONSTRUCTION SHALL REMAIN IN PLACE UNTIL NEW SIGNS ARE INSTALLED UNLESS NOTED OR DIRECTED OTHERWISE BY THE PROJECT ENGINEER. ROADWAY SIGNS THAT ARE IN CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND RELOCATED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID. NON–STANDARD MDOT SIGNS SUCH AS *BUCKLE UP*, *PICK IT UP, MISSISSIPPI*, ETC. SHALL BE REMOVED IF THEY CONFLICT WITH CONSTRUCTION. THE SIGNS SHALL BE ADEQUATELY STORED AND RESET AT APPROXIMATELY THE SAME LOCATION OR AT A LOCATION DETERMINED BY THE PROJECT ENGINEER AT THE COMPLETION OF THE PROJECT.
- 38. ALL EXISTING SIGNS AND SUPPORTS REMOVED UNDER THIS PROJECT SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND ARE NOT A SEPARATE PAY ITEM.
- 39. SOME AREAS OF THE PROJECT DESIGNATED FOR CLEARING AND GRUBBING WHERE UTILITY LINES, UTILITY GUY WIRES, OR OTHER STRUCTURES ARE PRESENT MAY HAVE TO BE CLEARED BY HAND METHODS. THE CONTRACTOR SHALL TAKE PRECAUTIONS TO PREVENT DAMAGE OR DISTURBANCE TO THE RELEVANT STRUCTURES.
- 40. THE CONTRACTOR SHALL COORDINATE AND CONDUCT WORK AT LOCAL ROADS AND DRIVEWAYS IN A MANNER SUCH THAT ACCESS IS NOT INTERRUPTED UNNECESSARILY. ACCESS SHALL BE PRESERVED IN THE BEST MANNER POSSIBLE. COORDINATION AND COMMUNICATION WITH LANDOWNERS MAY BE NECESSARY TO PREVENT INTERRUPTION OF DRIVEWAY ACCESS.
- 41. TEMPORARY PAVEMENT JOINTS (PAPER JOINTS) SHALL BE EMPLOYED AT ALL LOCATIONS REQUIRING TRAFFIC TO TRAVERSE AN UNEVEN PAVEMENT JOINT. PAPER JOINTS SHALL BE A MINIMUM OF 9 FEET IN LENGTH AND SHALL BE ADEQUATELY MAINTAINED.
- 42. PAYMENT FOR SAW CUTS WILL BE MADE USING THE APPROPRIATE PAY ITEMS. IF MILLING TECHNIQUES ARE USED, THE AREA WILL NOT REQUIRE SAW CUTS, BUT CARE SHOULD BE EXERCISED TO CREATE A NEAT REMOVAL LINE AND TO PREVENT DAMAGE TO THE ADJACENT PAVEMENT STRUCTURE. IF SAW CUTS ARE USED IN CONJUNCTION WITH MILLING, PAYMENT WILL BE MADE USING THE APPROPRIATE PAY ITEMS. PAYMENT WILL NOT BE MADE FOR SAW CUTS NOT PERFORMED.



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