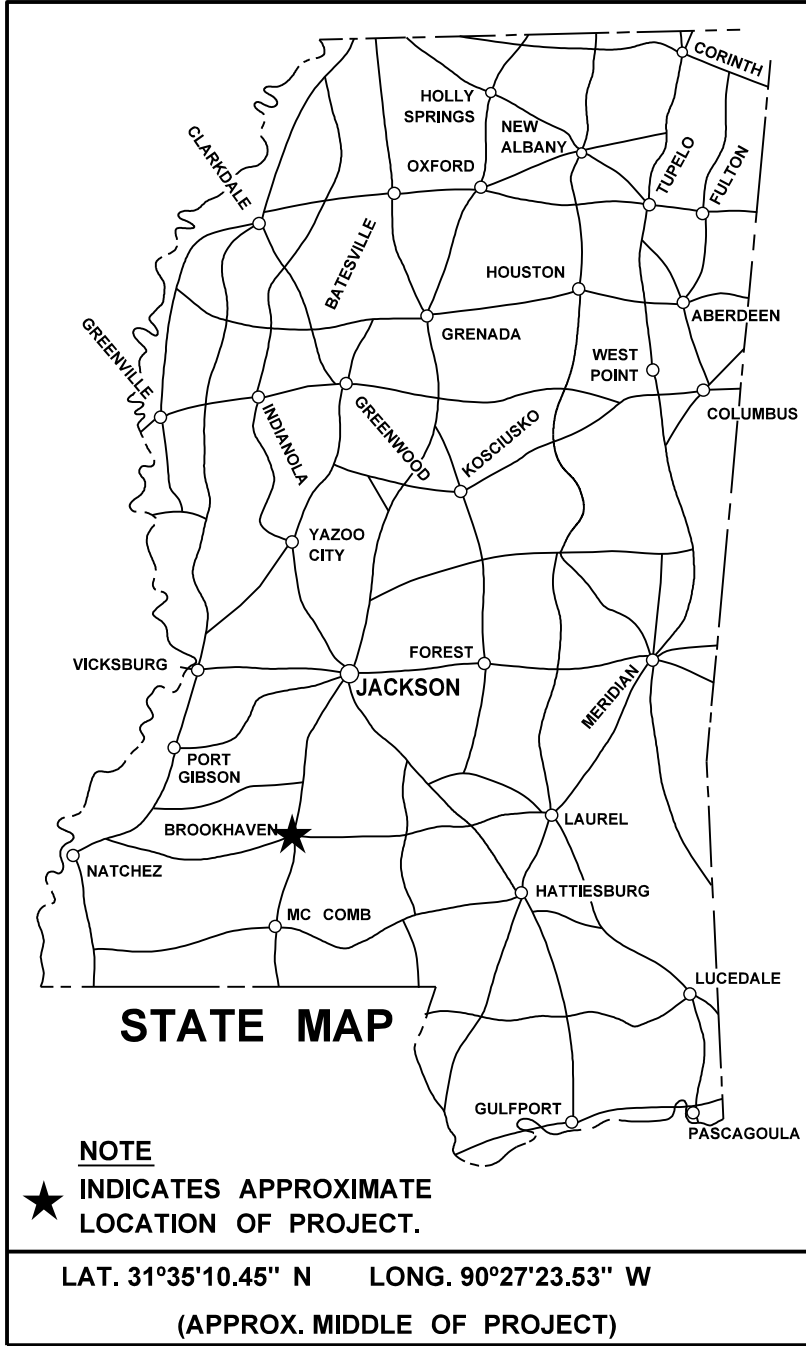


STATE	PROJECT NUMBER	SHEET NO.
MISSISSIPPI	NHPP-9515-00(006)	1



DESIGN CONTROL		
_____ MPH = V (SPEED DESIGN)		
ADT (____) = _____; ADT (____) = _____		
DHV = _____; D = _____% T = _____%		
PERMITS ACQUIRED BY MDOT		
WETLANDS AND WATERS PERMITS		
	WATERS	WETLANDS
NATIONWIDE #14	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
NATIONWIDE (OTHER)*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
GENERAL*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
INDIVIDUAL (404)*	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
STORMWATER PERMIT <input checked="" type="checkbox"/>		
Y	REQUIRED, CNOI SUBMITTED BY MDOT (DISTURBED AREA= 5 ACRES)	
S	REQUIRED, SCNOI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES)	
N	NO STORMWATER PERMIT REQUIRED (<1 ACRE)	
APPROVED BY: _____		

GENERAL INDEX

INCLUDED THIS PROJECT	BEGIN WITH SHEET
<input checked="" type="checkbox"/> ROADWAY	1
<input type="checkbox"/> PERMANENT SIGNS	1001
<input checked="" type="checkbox"/> TRAFFIC SIGNALS	2001
<input type="checkbox"/> ITS COMPONENTS	3001
<input type="checkbox"/> LIGHTING	4001
<input type="checkbox"/> (RESERVED)	5001
<input checked="" type="checkbox"/> ROADWAY STANDARD DWGS	6001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (LRFD)	7001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (STD. SPEC.)	7501
<input type="checkbox"/> BRIDGE	8001
<input type="checkbox"/> CROSS SECTIONS	9001

BRIDGE STRUCTURES REQ'D.

NONE

BOX BRIDGES REQ'D.

NONE

CONVENTIONAL SYMBOLS

COUNTY LINE	-----
TOWN CORPORATION LINE	-----
SECTION LINE	-----
EXISTING ROAD OR TRAVELED WAY	-----
PROPOSED ROAD OR TRAVELED WAY	-----
RAILROAD	-----
SURVEY LINE	-----
BRIDGES	-----

LENGTH OF ROADWAY	51,857,000 FT.	9.821 MI.
LENGTH OF BRIDGES	0 FT.	0 MI.
LENGTH OF PROJECT (NET)	51,857,000	9.821 MI.
LENGTH OF EXCEPTIONS	0 FT.	0 MI.
LENGTH OF PROJECT (GROSS)	51,857,000	9.821 MI.

EQUATIONS

NONE

LENGTH DATA

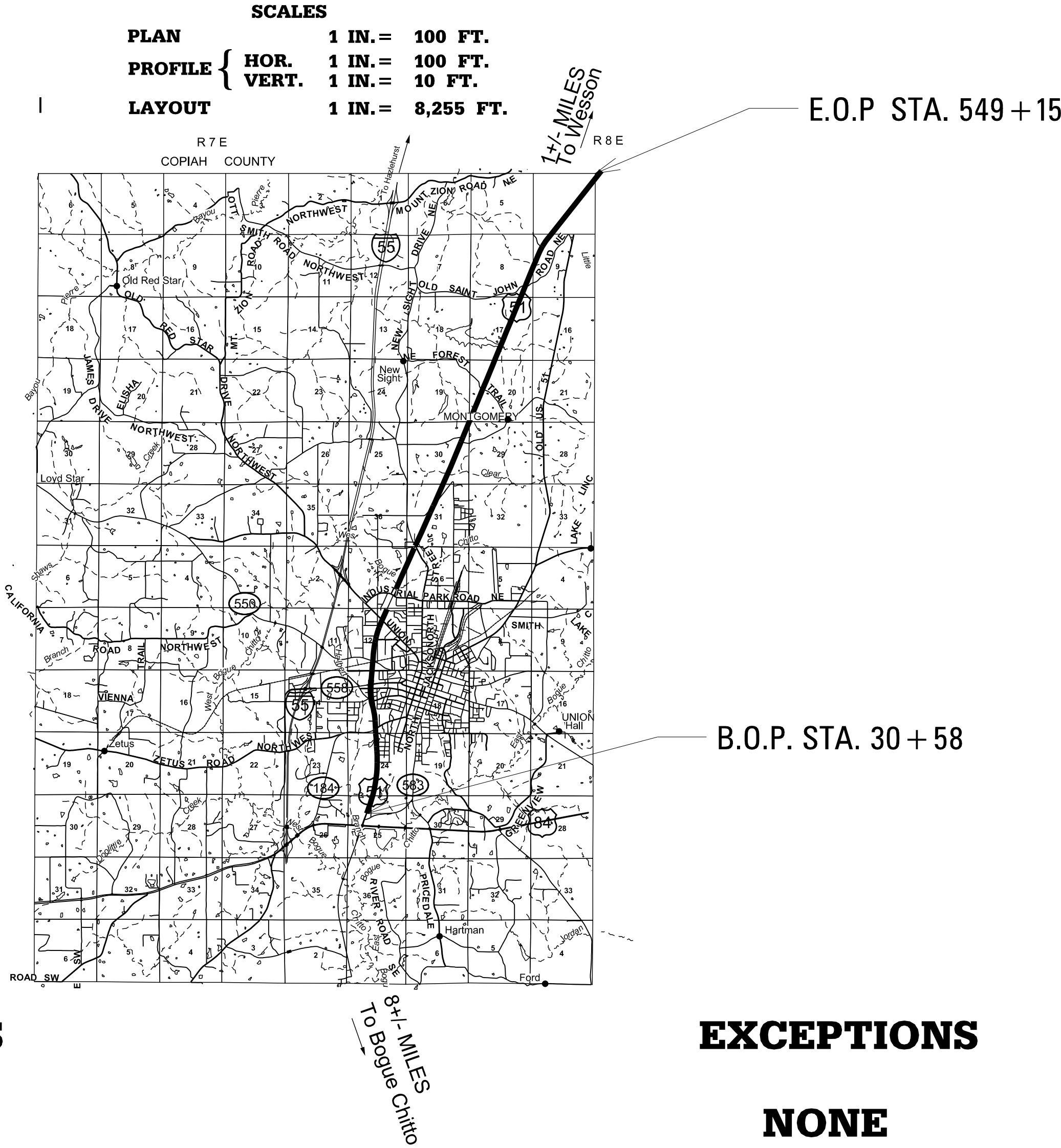
LENGTH OF ROADWAY	51,857,000 FT.	9.821 MI.
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LENGTH OF PROJECT (GROSS)	51,857,000	9.821 MI.

STATE OF MISSISSIPPI

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE OF PROPOSED STATE HIGHWAY
FEDERAL AID PROJECT NO. NHPP-9515-00(006)

US 51
US 84 NORTH TO COPIAH COUNTY LINE
LINCOLN COUNTY



EXCEPTIONS

NONE

12/11/2020 1:06 PM PLAN DIVISION ROADWAY MISSISSIPPI DEPARTMENT OF TRANSPORTATION (02)DETAIL INDEX (1).dgn

DESCRIPTION OF SHEET	WKG. NO.	SH. NO.	DESCRIPTION OF SHEET	WKG. NO.	SH. NO.
TITLE AND LAYOUT SHEET (1)		1	STANDARDS (17)		
DETAILED INDEX (1)	DI-1	2	PAVEMENT MARKING DETAILS FOR 2-LANE AND 4-LANE DIVIDED ROADWAYS	PM-1	6051
GENERAL NOTES (1)	GN-1	3	PAVEMENT MARKING DETAILS FOR 3-LANE 4-LANE AND 5-LANE UNDIVIDED ROADWAYS	PM-2	6052
GENERAL NOTES			PAVEMENT MARKING LEGEND DETAILS	PM-5	6055
			PAVEMENT MARKING LEGEND DETAILS	PM-6	6056
TYPICAL SECTION SHEETS (2)			TYPICAL PLACEMENT OF WARNING SIGNS AND PAVEMENT MARKINGS AT RAILROAD	PM-7	6057
TYPICAL SECTION MAINLINE	TS-1	4	HIGHWAY GRADE CROSSINGS	PM-8	6058
TYPICAL SECTION MAINLINE	TS-2	5	4-LANE TO 2-LANE TRANSITION AT INTERCHANGE	PM-11	6061
			2-WAY RAISED PAVEMENT MARKERS AT INTERECTING ROADS (2-LANE)	RS-1	6064
			RUMBLE STRIPES 2-LANE HIGHWAYS (ASPHALT LANES, 2-FT ASPHALT SHOULDERS)	TCP-1	6351
			TRAFFIC CONTROL PLAN WITH FLAGGER (ONE-LANE CLOSURE OF TWO-WAY TRAFFIC)		
			TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT LESS THAN 65 MPH		
			(4-LANE: MEDIAN LANE OR OUTSIDE LANE CLOSURE) (WORK DAY ONLY)	TCP-2	6352
QUANTITY SHEETS (6)			TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT LESS THAN 65 MPH		
SUMMARY OF QUANTITIES	SQ-1	6	(4-LANE: MEDIAN LANE OR OUTSIDE LANE CLOSURE) (EXTENDED PERIOD)		
SUMMARY OF QUANTITIES	SQ-2	7	SHORT DURATION CLOSING OF TWO-LANE TWO-WAY HIGHWAYS	TCP-3	6353
ESTIMATED QUANTITIES FOR ASPHALT, MILLING, & GRANULAR MATERIAL	EQ-1	8	TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULITILANE ROADS AND TWO-LANE ROADS	TCP-6	6356
ESTIMATED QUANTITIES FOR ASPHALT, MILLING, & GRANULAR MATERIAL	EQ-2	9	TRAFFIC CONTROL PLANS UNEVEN PAVEMENT DETAILS	TCP-9	6359
ESTIMATED QUANTITIES FOR ASPHALT, MILLING, & GRANULAR MATERIAL	EQ-3	10	TRAFFIC CONTROL PLANS UNEVEN PAVEMENT DETAILS	TCP-12	6362
ESTIMATED QUANTITIES FOR TRAFFIC CONTROL SIGNS	TCPQ-1	11	TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS	TCP-13	6363
			TEMPORARY STRIPING FOR TRAFFIC CONTROL 4-LANE AND 5-LANE UNDIVIDED ROADWAYS	TCP-14	6364
			LOCATION OF R16-3 SIGNS (SPEEDING FINES DOUBLED)	TCP-15	6365
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STA. 46+00 TO STA. 61+00	WK-4	13			
STA. 61+00 TO STA. 76+00	WK-5	14			
STA. 76+00 TO STA. 91+00	WK-6	15			
STA. 91+00 TO STA. 106+00	WK-7	16			
STA. 106+00 TO STA. 121+00	WK-8	17			
STA. 121+00 TO STA. 136+00	WK-9	18			
STA. 136+00 TO STA. 151+00	WK-10	19			
STA. 151+00 TO STA. 166+00	WK-11	20			
STA. 166+00 TO STA. 181+00	WK-12	21			
STA. 181+00 TO STA. 196+00	WK-13	22			
STA. 196+00 TO STA. 211+00	WK-14	23			
STA. 211+00 TO STA. 226+00	WK-15	24			
STA. 226+00 TO STA. 241+00	WK-16	25			
STA. 241+00 TO STA. 256+00	WK-17	26			
STA. 256+00 TO STA. 271+00	WK-18	27			
STA. 270+00 TO STA. 300+00	WK-19	28			
STA. 300+00 TO STA. 330+00	WK-20	29			
STA. 330+00 TO STA. 360+00	WK-21	30			
STA. 360+00 TO STA. 390+00	WK-22	31			
STA. 390+00 TO STA. 420+00	WK-23	32			
STA. 420+00 TO STA. 450+00	WK-24	33			
STA. 450+00 TO STA. 480+00	WK-25	34			
STA. 480+00 TO STA. 510+00	WK-26	35			
STA. 510+00 TO STA. 540+00	WK-27	36			
STA. 540+00 TO STA. 552+84	WK-28	37			
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TRAFFIC SIGNAL IMPROVEMENTS- BROOKWAY BLVD.	TSI-2	2002			
TRAFFIC SIGNAL IMPROVEMENTS - S.R. 550	TSI-3	2003			
TRAFFIC SIGNAL IMPROVEMENTS- UNION ST.	TSI-4	2004			
TRAFFIC SIGNAL IMPROVEMENTS- INDUSTRIAL PARK RD.	TSI-5	2005			
TRAFFIC SIGNAL GENERAL NOTES	TSD-1	2006			
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VIDEO/MULTI-SENSOR DETECTION INSTALLATION OR TRAFFIC SIGNALS	TSD-9V	2010			
TRAFFIC CONTROL PLAN TYPICAL SIGNAL INSTALLATION	TSD-10	2011			
SPAN WIRE DETAILS	TSD-12	2012			
TYPICAL INTERSECTION LAYOUT	TSD-14	2013			

DISTRICT 7		
PS & E PLANS-12/11/20		
FMS CON. # 107619/301000		
REVISIONS		
DATE	SHEET NO.	BY

				BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION
				REVISION	
					DETAILED INDEX
					PROJ. NO.: NHPP-9515-00(006)
					COUNTY: LINCOLN
					WORKING NUMBER
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					2
				DATE	FILE NAME: DETAIL INDEX (1).dgn
				DESIGN TEAM	CHECKED
					DATE

11/14/2020 11:22:11 AM 030 GN-001 PLAN DIVISION ROADWAY MISSISSIPPI DEPARTMENT OF TRANSPORTATION


GENERAL NOTES

- ① THE LOCATION AND SPACING OF SIGNS, SHOWN ON THE TRAFFIC CONTROL PLANS, ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.
- ② ALL TRAFFIC CONTROL DEVICES ON THIS PROJECT SHALL COMPLY WITH PART VI OF THE MUTCD (LATEST EDITION).
- ③ ALL PLASTIC DRUMS SHALL HAVE A BALLASTING COLLAR MADE FROM RECYCLED TRUCK TIRES OR OTHER SUITABLE MATERIAL.
- ④ THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING STRUCTURES SUCH AS, BUT NOT LIMITED TO, PIPES, INLETS, APRONS, AND BRIDGES FROM DAMAGE WHICH MIGHT OCCUR DURING CONSTRUCTION. THE CONTRACTOR SHALL REPLACE OR REPAIR, AS DIRECTED BY THE ENGINEER, ANY STRUCTURES DAMAGED DURING THE LIFE OF THE CONTRACT. NO PAYMENT WILL BE MADE FOR REPLACEMENT OR REPAIR OF DAMAGED ITEMS.
- ⑤ FLUORESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.
- ⑥ VEGETATIVE MATERIAL WILL BE REMOVED PRIOR TO PLACEMENT OF GRANULAR MATERIAL. THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
- ⑦ THE CONTRACTOR SHALL COVER ANY TEMPORARY TRAFFIC CONTROL SIGNS SHOWN IN THE TRAFFIC CONTROL PLAN THAT DO NOT APPLY TO THE CURRENT PHASE.
- ⑧ REMOVAL OF RAISED PAVEMENT MARKERS THAT ARE IN CONFLICT WITH REQUIRED CONSTRUCTION IS NOT CONSIDERED A SEPARATE PAY ITEM. COST TO BE ABSORBED IN OTHER ITEMS BID.
- ⑨ WHERE MILLING IS REQUIRED, THE CONTRACTOR SHALL PROVIDE OUTLETS IN THE EXISTING SHOULDERS AT SUFFICIENT INTERVALS TO PREVENT POOLING OR STANDING WATER ON THE MILLED SURFACE, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
- ⑩ TEMPORARY STRIPING SHALL CONFORM TO FINISHED STRIPE SPECIFICATIONS FOR ALIGNMENT, NEATNESS, AND STRAIGHTNESS.
- ⑪ ALL ADDENDA TO THESE PLANS WILL BE POSTED TO WWW.MDOT.MS.GOV UNDER THE PROPOSAL ADDENDA COLUMN. BIDDERS ARE ADVISED THAT HARD COPIES OF ANY ADDENDA FOR THIS PROJECT WILL NOT BE MAILED. IT IS THE BIDDER'S RESPONSIBILITY TO CHECK AND SEE IF ANY ADDENDA HAVE BEEN POSTED FOR THIS PROJECT.
- ⑫ STORAGE OF FLAMMABLE MATERIALS WILL NOT BE ALLOWED UNDER ANY BRIDGE STRUCTURES WITHOUT WRITTEN APPROVAL FROM THE PROJECT ENGINEER. SEE NOTICE TO BIDDERS ENTITLED "MATERIAL STORAGE UNDER BRIDGES" FOR MORE INFORMATION.
- ⑬ INSTALLATION DATES SHALL BE CLEARLY WRITTEN IN BOLD BLACK MARKINGS ON THE BACK BOTTOM HALF OF ALL SIGNS WITH A PERMANENT MARKING STICK THAT IS WATERPROOF, FADE RESISTANT AND MARKS ON WET OR DRY SURFACES.
- ⑭ ALL POST, PIPE, AND I-BEAM LENGTHS IN THESE PLANS ARE ESTIMATES. POST LENGTHS FOR ALL SIGNS SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO FABRICATION.
- ⑮ ALL EXISTING SIGNS WHICH ARE TO BE REMOVED AS A PART OF THIS PROJECT THAT ARE NOT IN CONFLICT WITH CONSTRUCTION SHALL REMAIN IN PLACE UNTIL NEW SIGNS ARE INSTALLED UNLESS NOTED OR DIRECTED OTHERWISE BY THE PROJECT ENGINEER. ROADWAY SIGNS THAT ARE IN CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND RELOCATED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
- ⑯ DIRECT-APPLIED LEGEND, BORDER, AND/OR SHIELDS ARE TO BE USED ON ALL GUIDE SIGNS. DIGITALLY PRODUCED SIGN COPY, SHIELDS, LEGEND, SYMBOLS, OR IMAGES WILL NOT BE ALLOWED WITHOUT WRITTEN APPROVAL FROM MDOT'S PROJECT ENGINEER.
- ⑰ AFTER THE PERMANENT SIGNS HAVE BEEN INSTALLED, THE CONTRACTOR SHALL SUBMIT TO THE PROJECT ENGINEER A DIGITAL COPY OF A MICROSOFT EXCEL SPREADSHEET WITH THE FOLLOWING INVENTORY DATA CAPTURED FOR EACH SIGN: LOCATION OF SIGN (LATITUDE-LONGITUDE GPS COORDINATES), MUTCD SIGN CODE, SIZE, BACKGROUND AND LEGEND COLORS, SUPPORT TYPE (POST, PIPE, SQUARE POST, OR I-BEAM), NUMBER OF SUPPORTS, DATE OF INSTALLATION, SIGN FACE DIRECTION, ROUTE NAME OR NUMBER, DIRECTION OF VEHICLE TRAVEL, AND LEGEND ON SIGN IF APPLICABLE. EACH SIGN SHALL BE ASSIGNED A UNIQUE ID NUMBER AND A DIGITAL PHOTO OF EACH SIGN SHALL BE SUBMITTED IN BITMAP FORMAT. THE PHOTO FILENAME SHALL CORRESPOND WITH THE UNIQUE ID NUMBER.
- ⑱ ALL PERMANENT SIGNS SHALL CONFORM TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- ⑲ ALL SIGN LOCATIONS SHALL BE APPROVED BY THE PROJECT ENGINEER PRIOR TO INSTALLATION.
- ⑳ THE RETROREFLECTIVE SIGN SHEETING ON PERMANENT GROUND-MOUNTED SIGNS SHALL BE AS FOLLOWS: BROWN BACKGROUND SHEETING ON GUIDE SIGNS SHALL BE MINIMUM TYPE VIII; GREEN AND BLUE BACKGROUND SHEETING ON GUIDE SIGNS SHALL BE MINIMUM TYPE IX; ALL WHITE, YELLOW, FLUORESCENT YELLOW AND FLUORESCENT YELLOW/GREEN SHEETING SHALL BE TYPE XI. ALL SIGN SHEETING ON OVERHEAD SIGNS SHALL BE TYPE XI.
- ㉑ THE RETROREFLECTIVE SIGN SHEETING ON RIGID, TEMPORARY TRAFFIC CONTROL (ORANGE) SIGNS SHALL BE MINIMUM TYPE IX.
- ㉒ ALL EXISTING SIGNS AND SUPPORTS REMOVED UNDER THIS PROJECT SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND ARE NOT A SEPARATE PAY ITEM.
- ㉓ ALL SIDE ROAD, STOP SIGN MOUNTED STREET NAME SIGNS TO BE SALVAGED AND STORED AT THE DIRECTION OF THE PROJECT ENGINEER FOR DELIVERY TO THE CITY (NOT A SEPARATE PAY ITEM).
- ㉔ ALL SIGNS AND DELINEATORS THAT CONFLICT WITH THE CONSTRUCTION OF THIS PROJECT SHALL BE REMOVED AND RESET BY THE CONTRACTOR; COST TO BE ABSORBED IN OTHER PAY ITEMS.

GENERAL NOTES (CONT.)

- ㉕ TEMPORARY STRIPING SHALL BE REQUIRED AFTER MILLING AND OVERLAYING OPERATIONS; TEMPORARY STRIPING SHALL BE PLACED IN THE SAME LOCATIONS AND LAYOUT AS PERMANENT STRIPE. ALL CENTERLINE, LANE LINES, EDGE LINES, AND NO-PASSING STRIPES THAT HAVE BEEN REMOVED DURING THE DAY'S OPERATIONS SHALL BE REPLACED WITH TEMPORARY STRIPE BEFORE WORK IS DISCONTINUED FOR THE DAY OR AS SOON THEREAFTER AS WEATHER CONDITIONS WILL PERMIT, EXCEPT THAT:
(1) REPLACEMENT OF NO-PASSING STRIPES MAY BE DELAYED FOR A PERIOD NOT TO EXCEED THREE (3) DAYS FOR A TWO OR THREE LANE ROAD
(2) TEMPORARY EDGE LINES ON PROJECTS REQUIRING SHOULDERS CONSTRUCTED OF GRANULAR MATERIAL MAY BE DELAYED FOR A PERIOD NOT TO EXCEED THREE (3) DAYS.
- ㉖ ALL ASPHALT AND CONCRETE CURBS ALONG RAMPS, LOCAL ROADS, ETC. FROM B.O.P. TO E.O.P. SHALL BE PAINTED (TWO APPLICATIONS) WITH WHITE TRAFFIC PAINT AND TRAFFIC BEADS ; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ㉗ IF THE ASPHALT CURB ALONG THE LOCAL ROAD IS SEVERELY DAMAGED, THE ENTIRE ASPHALT CURB WILL BE REMOVED AS DIRECTED BY THE ENGINEER; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ㉘ ALL LOCAL ROADS SHALL BE PAVED TO THE R.O.W. LIMITS OR AS DIRECTED BY THE ENGINEER.
- ㉙ GRANULAR MATERIAL WILL NOT BE ALLOWED TO BE PLACED DIRECTLY ON THE SURFACE LIFT OF ASPHALT, BUT MUST BE PLACED DIRECTLY ON THE GRAVEL SHOULDER OR A ROAD WIDENER MACHINE USED AND APPROVED BY THE PROJECT ENGINEER.
- ㉚ ALL RAMPS AND DRIVEWAY PADS SHALL BE PAVED TO THE SHOULDER LINE, MINIMUM OR FURTHER, AS DIRECTED BY THE ENGINEER WITHIN THE R.O.W. LIMITS.
- ㉛ ALL WORK SHALL BE DONE AT NIGHT INCLUDING WEEKEND. CONSTRUCTION OPERATIONS MAY BEGIN AT 6 P.M. AND MUST BE COMPLETED AND OUT OF THE ROADWAY BY 6 A.M. FOR THE B.O.P STA 30+58 TO STA. 270+50.
- ㉜ A CN RAILROAD CROSSING EXISTS @ STA. 155+00 +/- AND ON S.R. 550, THE CONTRACTOR IS REQUIRED TO COORDINATE WITH CN RAILROAD AT LEAST 10 WORKING DAYS PRIOR TO ANY WORK ON RAILROAD PROPERTY. THE CONTACT PERSON FOR CN RAILROAD IS JOHN DINNING @ 601-914-2658. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PAY ALL BILL ASSOCIATED WITH RAILROAD FLAGGING AND CABLE LOCATING.

				BY	MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
				REVISION	<div>GENERAL NOTES</div> <div>PROJ. NO.: NHPP-9515-00(006) COUNTY: LINCOLN</div>	
				DATE		
				DESIGN TEAM	CHECKED	DATE



WORKING NUMBER
GN-1
SHEET NUMBER
3