

STATE	PROJECT NUMBER	SHEET NO.
MISSISSIPPI	HSIP-0015-03(011)	1

GENERAL INDEX

INCLUDED THIS PROJECT	BEGIN WITH SHEET
<input checked="" type="checkbox"/> ROADWAY	1
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<input checked="" type="checkbox"/> TRAFFIC SIGNALS	2001
<input type="checkbox"/> ITS COMPONENTS	3001
<input type="checkbox"/> LIGHTING	4001
<input type="checkbox"/> (RESERVED)	5001
<input checked="" type="checkbox"/> ROADWAY STANDARD DWGS	6001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (LRFD)	7001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (STD. SPEC.)	7501
<input type="checkbox"/> BRIDGE	8001
<input checked="" type="checkbox"/> CROSS SECTIONS	9001

STATE OF MISSISSIPPI

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE OF PROPOSED STATE HIGHWAY FEDERAL AID PROJECT NO. HSIP-0015-03(011)

US 84 AT RESERVOIR / MAGNOLIA HILL ROAD AND AT SR 184 INTERSECTION IMPROVEMENTS WAYNE COUNTY

107249/301000

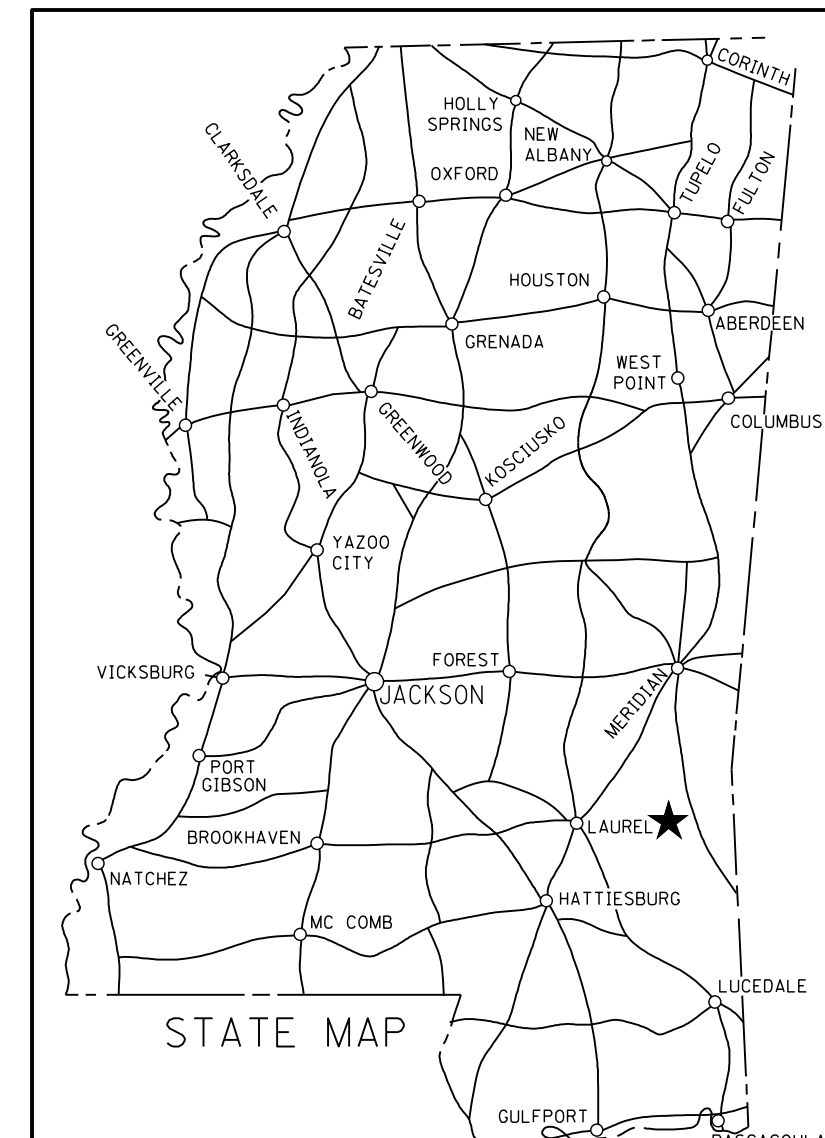
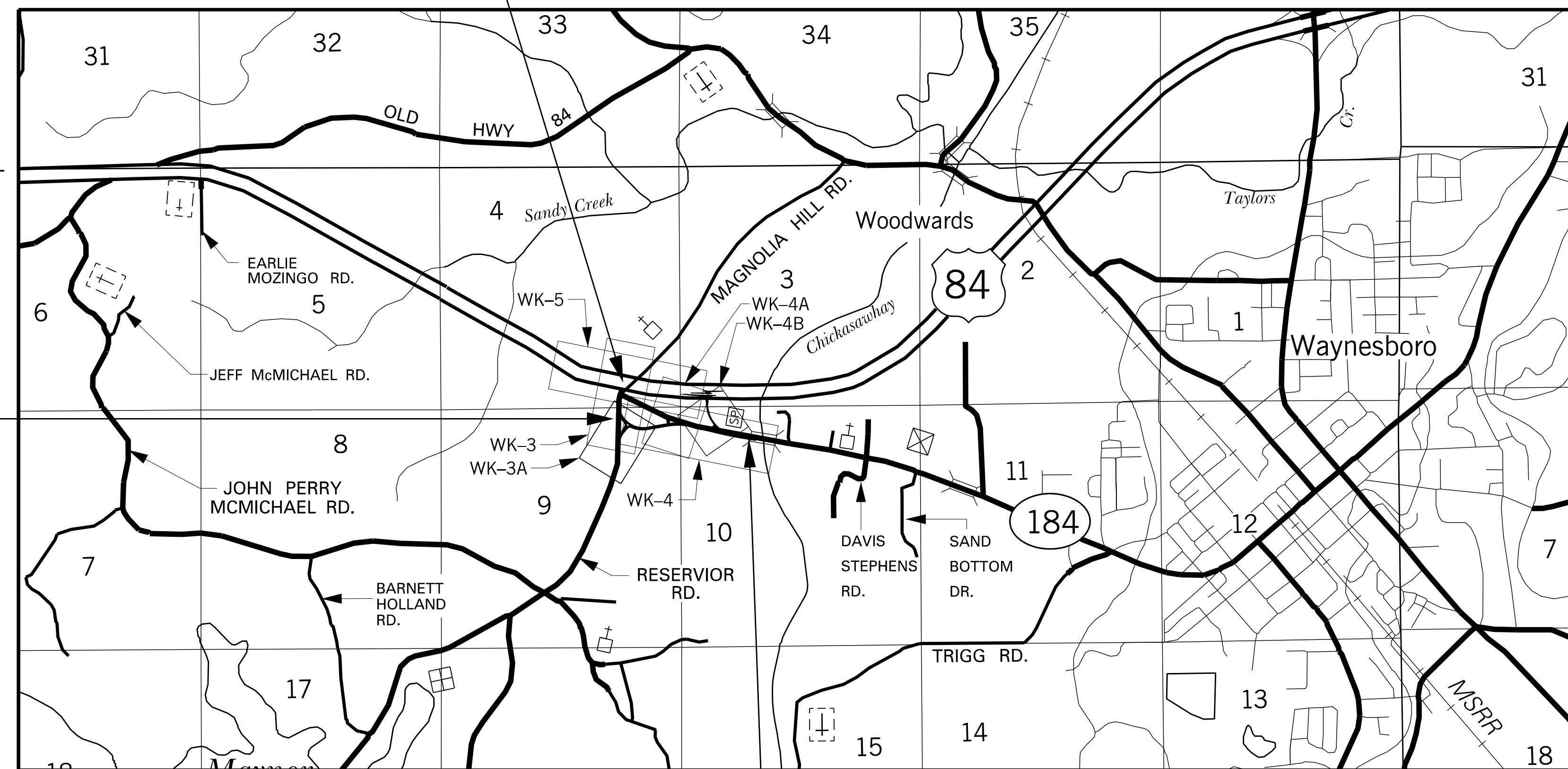
SCALES

PLAN	1 IN. = 100 FT.
PROFILE	HOR. 1 IN. = 100 FT.
	VERT. 1 IN. = 10 FT.
LAYOUT	1 IN. = 2,000 FT.

BEGINNING OF PROJECT STA. 20+00.00

END OF PROJECT STA. 58+64.14

PROJECT LOCATION



NOTE
★ INDICATES APPROXIMATE LOCATION OF PROJECT.
LAT. 88° 41'26"W LONG. 31° 40'57" N
(APPROX. MIDDLE OF PROJECT)

RURAL COLLECTOR

DESIGN CONTROL
55 MPH = V (SPEED DESIGN)

ADT (2020) = 5,000 ; ADT (2040) = 6,200
DHV = 680 ; D = 55 % T = 14 %

PERMITS ACQUIRED BY MDT

WETLANDS AND WATERS PERMITS		
	WATERS	WETLANDS
NATIONWIDE #14	<input type="checkbox"/> N	<input checked="" type="checkbox"/> Y
NATIONWIDE (OTHER)*	<input type="checkbox"/> N	<input type="checkbox"/> N
GENERAL*	<input type="checkbox"/> N	<input type="checkbox"/> N
INDIVIDUAL (404)*	<input type="checkbox"/> N	<input type="checkbox"/> N

STORMWATER PERMIT	
Y	REQUIRED, SCNOI SUBMITTED BY MDT (DISTURBED AREA = 5 ACRES)
S	REQUIRED, SCNOI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES)
N	NO STORMWATER PERMIT REQUIRED (<1 ACRE)

APPROVED BY: _____

CONVENTIONAL SYMBOLS

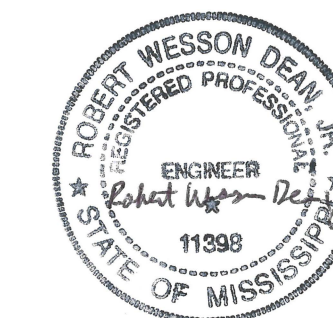
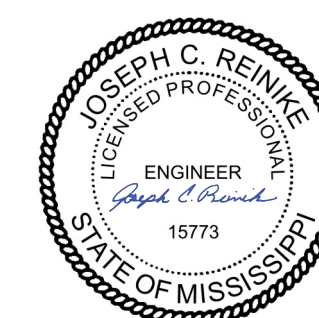
COUNTY LINE	-----
TOWN CORPORATION LINE	-----
SECTION LINE	-----
EXISTING ROAD OR TRAVELED WAY	-----
PROPOSED ROAD OR TRAVELED WAY	-----
RAILROAD	-----
SURVEY LINE	-----
BRIDGES	-----

EQUATIONS

NONE

LENGTH DATA

LENGTH OF ROADWAY	3,864.14 FT.	0.732 MI.
LENGTH OF BRIDGES	0.0 FT.	0.0 MI.
LENGTH OF PROJECT (NET)	3,864.14 FT.	0.732 MI.
LENGTH OF EXCEPTIONS	0.0 FT.	0.0 MI.
LENGTH OF PROJECT (GROSS)	3,864.14 FT.	0.732 MI.



ROADWAY

TRAFFIC

P S & E DATE: 12/15/2020

APPROVED: _____

DEPUTY EXECUTIVE DIRECTOR / CHIEF ENGINEER

EXECUTIVE DIRECTOR



3/29/2021 11:04 AM TLE.DGN

STATE	PROJECT NO.
MISS.	HSIP-0015-03(011)

DESCRIPTION OF SHEET

WKG. NO. SH. NO.

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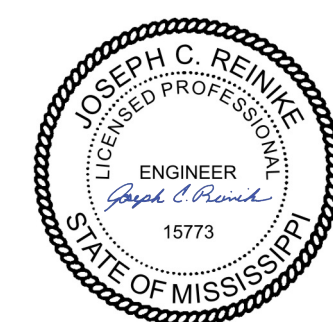
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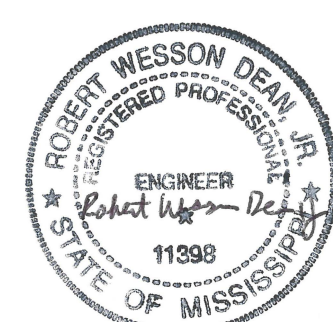
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ROADWAY PLAN DIVISION MISSISSIPPI DEPARTMENT OF TRANSPORTATION

12/15/2020 11:26 AM DI.DGN



ROADWAY



TRAFFIC



PS & E PLANS - 12-15-2020		
FMS CON. # 107249/301000		
REVISIONS		
DATE	SHEET NO.	BY

REVISION	MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
	DETAILED INDEX	
DATE	PROJ. NO.: HSIP-0015-03(011) COUNTY: WAYNE	
	FILE NAME: DI.dgn DESIGN TEAM: STANTEC CHECKED: DATE:	
	WORKING NUMBER	DI-1
	SHEET NUMBER	2



STATE	PROJECT NO.
MISS.	HSIP-0015-03(011)

DESCRIPTION OF SHEET

WKG. NO. SH. NO.

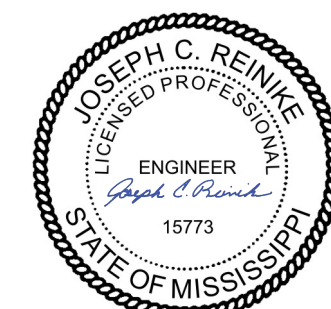
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DESCRIPTION OF SHEET

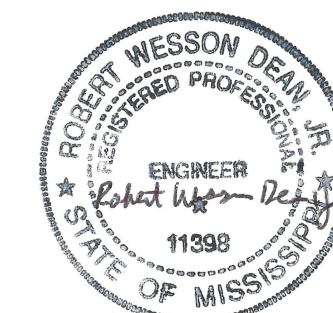
WKG. NO. SH. NO.

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CROSS SECTION SHEETS (73)		
CROSS SECTIONS		9001-9073
TOTAL SHEETS (228)		

3/31/2021 2:38 PM DI.DGN



ROADWAY



TRAFFIC

MISSISSIPPI DEPARTMENT OF TRANSPORTATION DETAILED INDEX	
PROJ. NO.: HSIP-0015-03(011)	WORKING NUMBER DI-2
COUNTY: WAYNE	SHEET NUMBER 3
FILE NAME: DI.dgn	DESIGN TEAM: STANTEC
DATE: _____	CHECKED: _____ DATE: _____

STATE	PROJECT NO.
MISS.	HSIP-0015-03(011)

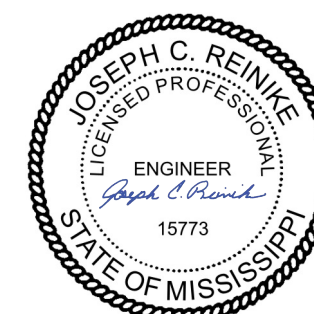
GENERAL NOTES

1. THE LOCATION AND SPACING OF SIGNS, SHOWN ON THE TRAFFIC CONTROL PLANS, ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.
2. ALL TRAFFIC CONTROL DEVICES ON THIS PROJECT SHALL COMPLY WITH THE MUTCD (LATEST EDITION).
3. ALL PLASTIC DRUMS SHALL HAVE A BALLASTING COLLAR MADE FROM RECYCLED TRUCK TIRES OR OTHER SUITABLE MATERIAL.
4. A SOIL PROFILE HAS BEEN PREPARED FOR THIS PROJECT USING SAMPLES TAKEN FROM HOLES AT THE LOCATIONS INDICATED IN THE TEST REPORTS. THIS SOIL PROFILE IS ON FILE IN THE DISTRICT AND CENTRAL CONSTRUCTION OFFICES AND IS AVAILABLE FOR EXAMINATION. THE DEPARTMENT DOES NOT GUARANTEE THAT THE MATERIALS AS SHOWN IN THE REPORTS ARE NECESSARILY TO BE FOUND OUTSIDE THE TEST HOLES.
5. 20% SHRINKAGE FACTOR USED IN THE EARTHWORK CALCULATIONS IS FOR DESIGN ESTIMATING PURPOSES ONLY.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING STRUCTURES SUCH AS, BUT NOT LIMITED TO, PIPES, INLETS, APRONS, AND BRIDGES FROM DAMAGE WHICH MIGHT OCCUR DURING CONSTRUCTION. THE CONTRACTOR SHALL REPLACE OR REPAIR, AS DIRECTED BY THE ENGINEER, ANY STRUCTURES DAMAGED DURING THE LIFE OF THE CONTRACT. NO PAYMENT WILL BE MADE FOR REPLACEMENT OR REPAIR OF DAMAGED ITEMS.
7. ALL PIPE JOINTS ARE TO BE WRAPPED COMPLETELY IN 24-INCH WIDE TYPE V GEOTEXTILE FABRIC. ALL PICKUP HOLES SHALL BE PLUGGED AND COVERED WITH TYPE V GEOTEXTILE FABRIC, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
8. VOIDS CREATED BY THE REMOVAL OF, BUT NOT LIMITED TO, POSTS, CONCRETE ANCHORS, AND FOOTINGS SHALL BE BACK-FILLED AND TAMPED IN ACCORDANCE WITH SECTION 203 OF THE MISSISSIPPI STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE COST OF WHICH WILL BE ABSORBED IN OTHER ITEMS BID.
9. UTILITIES ON THE DRAWINGS ARE SHOWN IN THEIR ORIGINAL LOCATION BASED UPON THE BEST INFORMATION AVAILABLE TO THE ENGINEER. UTILITIES THAT WERE FOUND TO BE IN CONFLICT WITH CONSTRUCTION HAVE BEEN RELOCATED. PERMITS ARE ON FILE WITH THE DEPARTMENT SHOWING THE APPROXIMATE LOCATION OF UTILITIES RELOCATED WITHIN THE RIGHT-OF-WAY. THE ENGINEER CAN NOT AND DOES NOT WARRANT THAT THIS INFORMATION IS COMPLETE OR ACCURATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING DIRECTLY WITH THE INVOLVED UTILITY OWNERS TO HAVE UNDERGROUND UTILITY LINES FIELD LOCATED IN ADVANCE OF CONSTRUCTION.
10. WORK ON STRUCTURES FOR THIS PROJECT REQUIRES EXCAVATION IN THE IMMEDIATE VICINITY OF TRAFFIC AND ADJACENT PROPERTIES. THEREFORE, THE RISK OF A FAILURE OCCURRING DURING EXCAVATION REQUIRES THAT EXTREME CAUTION BE EXERCISED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING WHAT BRACING, SHORING, OR GROUND SUPPORT SYSTEM THAT IS DEEMED NECESSARY TO PREVENT A FAILURE AND PROTECT THE PERSONS WORKING NEAR THE EXCAVATION, THE PUBLIC THAT MAY BE ABOVE THE EXCAVATION OR ANY STRUCTURES ADJACENT TO THE EXCAVATION. ALL COSTS FOR DESIGNING, DRAWING, AND CONSTRUCTING THE FACILITY SHALL BE INCLUDED IN THE PRICE BID FOR CONTRACT ITEMS.
11. SOME WORK IS REQUIRED OUTSIDE THE PROJECT LIMITS. NO ADDITIONAL COMPENSATION WILL BE MADE FOR SUCH WORK EXCEPT AS PROVIDED BY SPECIFIC PAY ITEMS INCLUDED IN THE PLANS
12. WIRE FENCE BACKING WILL BE REQUIRED FOR ALL SILT FENCE. (SEE WK. NO. ECD-3)
13. FOR LIST OF PUBLIC UTILITIES, SEE WORKING SHEET NO. 3.

GENERAL NOTES (CONT.)

14. FLUORESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.
15. THE COST OF ANY COLLARS REQUIRED TO CONNECT CONCRETE FLARED END SECTIONS TO NON-CONCRETE PIPE SECTIONS SHALL BE ABSORBED IN THE COST FOR NON-CONCRETE PIPE.
16. VEGETATIVE MATERIAL WILL BE REMOVED PRIOR TO PLACEMENT OF GRANULAR MATERIAL. THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
17. THE CONTRACTOR SHALL COORDINATE WITH THE CONTRACTOR FROM ADJACENT PROJECT(S) IN IMPLEMENTING THE TRAFFIC CONTROL PLAN AS DIRECTED BY THE ENGINEER. ALL CONFLICTING SIGNS SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.
18. THE CONTRACTOR SHALL COVER OR REMOVE ANY TEMPORARY TRAFFIC CONTROL SIGNS SHOWN IN THE TRAFFIC CONTROL PLAN THAT DO NOT APPLY TO THE CURRENT PHASE.
19. REMOVAL OF RAISED PAVEMENT MARKERS THAT ARE IN CONFLICT WITH REQUIRED CONSTRUCTION IS NOT CONSIDERED A SEPARATE PAY ITEM. COST TO BE ABSORBED IN OTHER ITEMS BID.
20. REMOVAL OF OBJECT MARKERS IS NOT CONSIDERED A SEPARATE PAY ITEM, AND SHALL BE ABSORBED IN OTHER ITEMS BID.
21. WHERE MILLING IS REQUIRED, THE CONTRACTOR SHALL PROVIDE OUTLETS IN THE EXISTING SHOULDERS AT SUFFICIENT INTERVALS TO PREVENT POOLING OR STANDING WATER ON THE MILLED SURFACE, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
22. THE EROSION CONTROL DEVICES REFERENCED IN THESE PLANS ARE A MINIMUM REQUIREMENT. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT SILT DOES NOT LEAVE THE RIGHT OF WAY OR CONTAMINATE WATERS OF THE U. S. DURING CONSTRUCTION. THE CONTRACTOR SHALL SUBMIT AN EROSION CONTROL PLAN AT THE PRE-CONSTRUCTION CONFERENCE OR PRIOR TO COMMENCEMENT OF WORK AND MAINTAIN THE PLAN DURING CONSTRUCTION. ANY ADDITIONAL SILT BASINS NOT SHOWN IN THE PLANS SHALL BE INCLUDED IN THE CONTRACTOR'S EROSION CONTROL PLAN PRIOR TO SUBMITTING FOR APPROVAL.
23. PRIOR TO EARTHWORK OPERATIONS, THE EXISTING TOP 4" TOPSOIL IS TO BE STRIPPED AND STOCKPILED. AFTER THE GRADING OPERATIONS ARE COMPLETED, SAID TOPSOIL SHALL BE PLACED ON ALL AREAS THAT ARE NOT TO BE PAVED OR OTHERWISE PROTECTED, IN ACCORDANCE WITH SECTION 211 OF THE SPECIFICATIONS, OR THE VEGETATION SCHEDULE (SEE WK. SH. VS-1). EXISTING TOPSOIL AND ALL COSTS ASSOCIATED WITH STRIPPING, HAULING, STOCKPILING, AND PLACEMENT OF THE EXISTING TOPSOIL IS TO BE PAID FOR AS UNCLASSIFIED EXCAVATION. ANY ADDITIONAL TOPSOIL TO BE PAID FOR UNDER PAY ITEM NO. 211-B001 - TOPSOIL FOR SLOPE TREATMENT, CONTRACTOR FURNISHED.

12/11/2020 1:21:05 PM GN-1.dgn



MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
GENERAL NOTES	
PROJ. NO.: HSIP-0015-03(011) COUNTY: WAYNE	
WORKING NUMBER GN-1	SHEET NUMBER 4
FILE NAME: GN-1.dgn DESIGN TEAM: STANTEC	CHECKED: _____ DATE: _____

STATE	PROJECT NO.
MISS.	HSIP-0015-03(011)

GENERAL NOTES (CONT.)

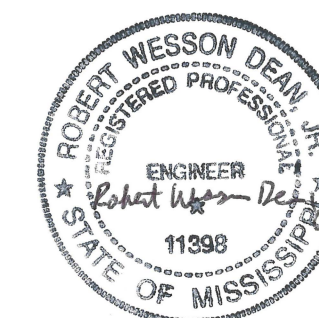
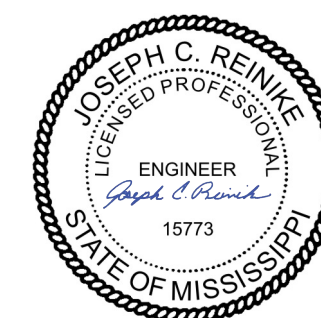
24. THE CONTRACTOR IS RESPONSIBLE FOR FIELD-VERIFICATION OF EXISTING GRADES AND MAKING ADJUSTMENTS AS NECESSARY WITH THE APPROVAL OF THE PROJECT ENGINEER.
25. TEMPORARY STRIPING SHALL CONFORM TO FINISHED STRIPE SPECIFICATIONS FOR ALIGNMENT, NEATNESS, AND STRAIGHTNESS.
26. ALL ITEMS OF WORK ASSOCIATED WITH THE INSTALLATION AND MAINTENANCE OF CONSTRUCTION ENTRANCES SHALL BE ABSORBED IN OTHER ITEMS OF WORK.
27. IF COLORS ARE USED ON PLAN / PROFILE SHEETS, THEY ARE INTENDED TO VISUALLY EASE THE LOCATION OF ELEMENTS FOR USERS OF THESE DRAWINGS. ALTHOUGH THE INTENT IS TO CATEGORIZE EVERYTHING AS EITHER EXISTING OR PROPOSED, IT IS THE END USER'S RESPONSIBILITY TO ENSURE ALL ELEMENTS ARE INTERPRETED CORRECTLY REGARDLESS OF COLOR.
28. ALL ADDENDA TO THESE PLANS WILL BE POSTED TO WWW.MDOT.MS.GOV UNDER THE PROPOSAL ADDENDA COLUMN. BIDDERS ARE ADVISED THAT HARD COPIES OF ANY ADDENDA FOR THIS PROJECT WILL NOT BE MAILED. IT IS THE BIDDER'S RESPONSIBILITY TO CHECK AND SEE IF ANY ADDENDA HAVE BEEN POSTED FOR THIS PROJECT.
29. CURB AND GUTTER VERTICAL DIMENSIONS SHOWN IN THE DETAIL DRAWINGS ARE FOR A CURB IN THE "CATCH" CONFIGURATION AND SHALL BE CONSIDERED TO BE MINIMUM DIMENSIONS. THE DIMENSIONS MAY BE MODIFIED AS NECESSARY FOR "SPILL" CURB AND GUTTER, BUT SHALL NOT BE LESS THAN THE MINIMUM SHOWN.
30. THE COST FOR REMOVAL OF ALL HEADWALLS AND WINGWALLS (PIPES, BOX CULVERTS, BOX BRIDGES) SHALL BE ABSORBED IN OTHER ITEMS BID.
31. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING AND RELOCATING MAIL BOXES AS NECESSARY TO MAINTAIN CONTINUOUS MAIL SERVICE THROUGHOUT THE LIFE OF THE PROJECT, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
32. INSTALLATION DATES SHALL BE CLEARLY WRITTEN IN BOLD BLACK MARKINGS ON THE BACK BOTTOM HALF OF ALL SIGNS WITH A PERMANENT MARKING STICK THAT IS WATERPROOF, FADE RESISTANT AND MARKS ON WET OR DRY SURFACES.
33. ALL POST, PIPE, AND I-BEAM LENGTHS IN THESE PLANS ARE ESTIMATES. POST LENGTHS FOR ALL SIGNS SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO FABRICATION.
34. ALL EXISTING SIGNS WHICH ARE TO BE REMOVED AS A PART OF THIS PROJECT THAT ARE NOT IN CONFLICT WITH CONSTRUCTION SHALL REMAIN IN PLACE UNTIL NEW SIGNS ARE INSTALLED UNLESS NOTED OR DIRECTED OTHERWISE BY THE PROJECT ENGINEER. ROADWAY SIGNS THAT ARE IN CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND RELOCATED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
35. DIRECT-APPLIED LEGEND, BORDER, AND/OR SHIELDS ARE TO BE USED ON ALL GUIDE SIGNS. DIGITALLY PRODUCED SIGN COPY, SHIELDS, LEGEND, SYMBOLS, OR IMAGES WILL NOT BE ALLOWED WITHOUT WRITTEN APPROVAL FROM MDOT'S PROJECT ENGINEER.

GENERAL NOTES (CONT.)

36. AFTER THE PERMANENT SIGNS HAVE BEEN INSTALLED, THE CONTRACTOR SHALL SUBMIT TO THE PROJECT ENGINEER A DIGITAL COPY OF A MICROSOFT EXCEL SPREADSHEET WITH THE FOLLOWING INVENTORY DATA CAPTURED FOR EACH SIGN: LOCATION OF SIGN (LATITUDE-LONGITUDE GPS COORDINATES), MUTCD SIGN CODE, SIZE, BACKGROUND AND LEGEND COLORS, SUPPORT TYPE (POST, PIPE, SQUARE POST, OR I-BEAM), NUMBER OF SUPPORTS, DATE OF INSTALLATION, SIGN FACE DIRECTION, ROUTE NAME OR NUMBER, DIRECTION OF VEHICLE TRAVEL, AND LEGEND ON SIGN IF APPLICABLE. EACH SIGN SHALL BE ASSIGNED A UNIQUE ID NUMBER AND A DIGITAL PHOTO OF EACH SIGN SHALL BE SUBMITTED IN BITMAP FORMAT. THE PHOTO FILENAME SHALL CORRESPOND WITH THE UNIQUE ID NUMBER.
37. ALL PERMANENT SIGNS SHALL CONFORM TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
38. ALL SIGN LOCATIONS SHALL BE APPROVED BY THE PROJECT ENGINEER PRIOR TO INSTALLATION.
39. THE RETROREFLECTIVE SIGN SHEETING ON PERMANENT GROUND-MOUNTED SIGNS SHALL BE AS FOLLOWS: BROWN BACKGROUND SHEETING ON GUIDE SIGNS SHALL BE MINIMUM TYPE VIII; GREEN AND BLUE BACKGROUND SHEETING ON GUIDE SIGNS SHALL BE MINIMUM TYPE IX; ALL WHITE, YELLOW, FLUORESCENT YELLOW AND FLUORESCENT YELLOW/GREEN SHEETING SHALL BE TYPE XI. ALL SIGN SHEETING ON OVERHEAD SIGNS SHALL BE TYPE XI.
40. THE RETROREFLECTIVE SIGN SHEETING ON RIGID, TEMPORARY TRAFFIC CONTROL (ORANGE) SIGNS SHALL BE MINIMUM TYPE IX.
41. ALL EXISTING SIGNS AND SUPPORTS REMOVED UNDER THIS PROJECT SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND ARE NOT A SEPARATE PAY ITEM.
42. ALL SIDE ROAD, STOP SIGN MOUNTED STREET NAME SIGNS TO BE SALVAGED AND STORED AT THE DIRECTION OF THE PROJECT ENGINEER FOR DELIVERY TO THE CITY (NOT A SEPARATE PAY ITEM).
43. THE CONTRACTOR SHALL COORDINATE AND CONDUCT WORK AT LOCAL ROADS AND DRIVEWAYS IN A MANNER SUCH THAT ACCESS IS NOT INTERRUPTED UNNECESSARILY. ACCESS SHALL BE PRESERVED IN THE BEST MANNER POSSIBLE. COORDINATION AND COMMUNICATION WITH LANDOWNERS MAY BE NECESSARY TO PREVENT INTERRUPTION OF DRIVEWAY ACCESS.
44. TEMPORARY PAVEMENT JOINTS (PAPER JOINTS) SHALL BE EMPLOYED AT ALL LOCATIONS REQUIRING TRAFFIC TO TRAVERSE AN UNEVEN PAVEMENT JOINT. PAPER JOINTS SHALL BE A MINIMUM OF 9 FEET IN LENGTH AND SHALL BE ADEQUATELY MAINTAINED.
45. ALL MILLED MATERIAL SHALL BECOME PROPERTY OF THE CONTRACTOR EXCEPT FOR 50% OR 500 TONS, WHICHEVER IS LESS. SAID MATERIAL SHALL BE DELIVERED TO MDOT MAINTENANCE. SUFFICIENT ADVANCE NOTICE SHALL BE GIVEN TO ENSURE THAT MDOT MAINTENANCE PERSONNEL WILL BE ON HAND TO DIRECT THE DELIVERY. THE CONTRACTOR SHALL ALSO PROVIDE MDOT WITH AN OPERATOR AND THE NECESSARY EQUIPMENT TO STOCKPILE THE DELIVERY. THE COST OF WHICH SHALL BE ABSORBED.
46. NO TEMPORARY CULVERT STREAM CROSSINGS WILL BE ALLOWED.

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ROADWAY PLAN DIVISION MISSISSIPPI DEPARTMENT OF TRANSPORTATION



MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
GENERAL NOTES	
PROJ. NO.: HSIP-0015-03(011) COUNTY: WAYNE	
FILE NAME: GN-1.dgn DESIGN TEAM: STANTEC CHECKED: DATE:	
REVISION BY DATE	WORKING NUMBER GN-2 SHEET NUMBER 5

