

STATE	PROJECT NUMBER	SHEET NO.
MISSISSIPPI	SP-0914-00(039) HSIP-0914-00(039)	1

**GENERAL INDEX**

INCLUDED THIS PROJECT	BEGIN WITH SHEET
<input checked="" type="checkbox"/> ROADWAY .....	1
<input type="checkbox"/> PERMANENT SIGNS .....	1001
<input type="checkbox"/> TRAFFIC SIGNALS .....	2001
<input type="checkbox"/> ITS COMPONENTS .....	3001
<input type="checkbox"/> LIGHTING .....	4001
<input type="checkbox"/> (RESERVED) .....	5001
<input checked="" type="checkbox"/> ROADWAY STANDARD DWGS .....	6001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (LRFD) .....	7001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (STD. SPEC.) .....	7501
<input type="checkbox"/> BRIDGE .....	8001
<input type="checkbox"/> CROSS SECTIONS .....	9001

**BRIDGE STRUCTURES REQ'D.**

**BOX BRIDGES REQ'D.**

**CONVENTIONAL SYMBOLS**

COUNTY LINE .....	
TOWN CORPORATION LINE .....	
SECTION LINE .....	
EXISTING ROAD OR TRAVELED WAY .....	
PROPOSED ROAD OR TRAVELED WAY .....	
RAILROAD .....	
SURVEY LINE .....	
BRIDGES .....	

**STATE OF MISSISSIPPI**

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION**

**PLAN AND PROFILE OF PROPOSED STATE HIGHWAY STATE PROJECT NO. SP-0914-00(039) HSIP-0914-00(039)**

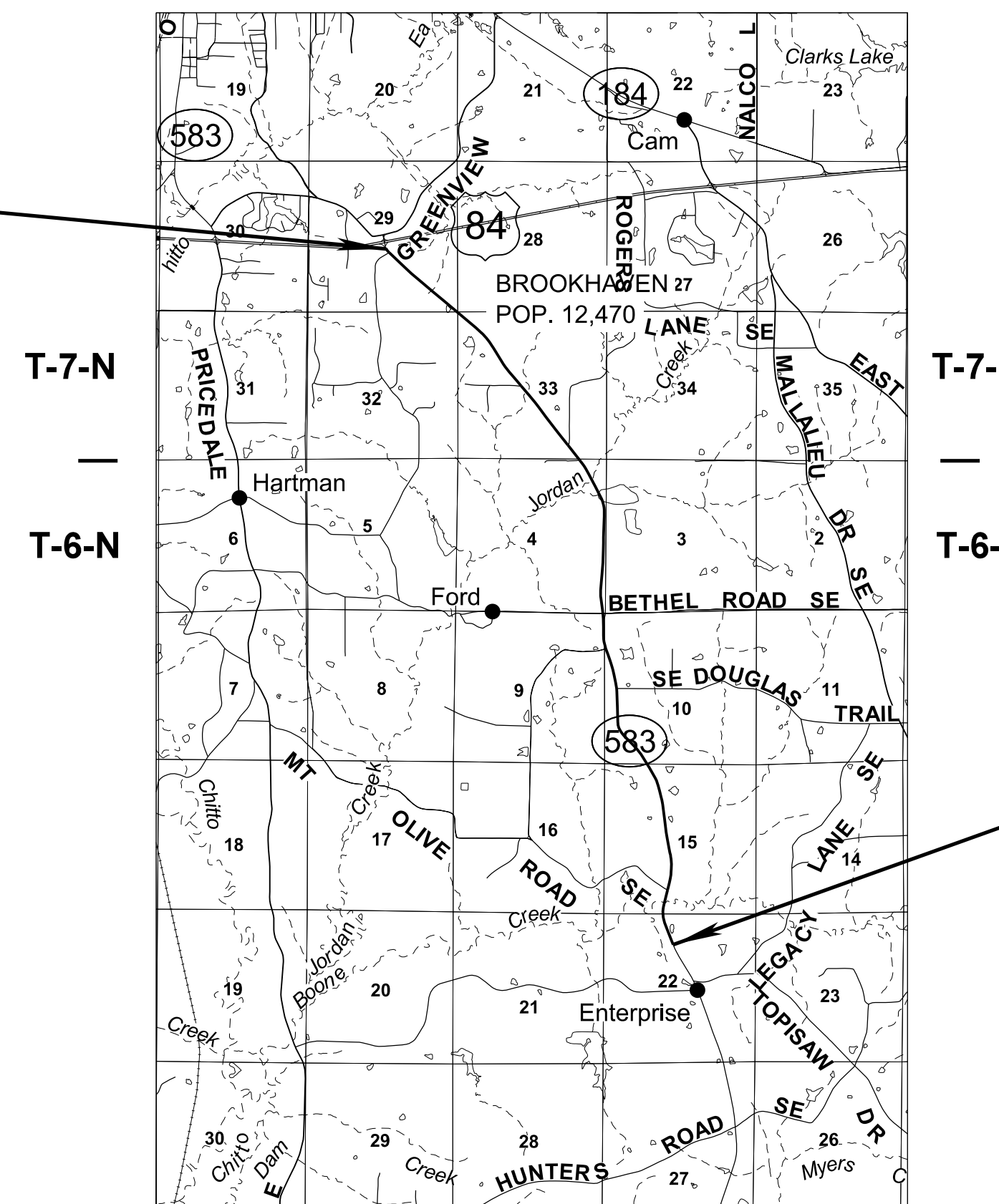
SR 583  
ENTERPRISE SCHOOL NORTH 5.4 MI TO JCT US 84  
LINCOLN COUNTY

FMS CON. NO. 108667 /301000  
108667 /301100

**SCALES**

PLAN	1 IN. = 100 FT.
PROFILE {	HOR. 1 IN. = 100 FT.
	VERT. 1 IN. = 10 FT.
R-7-E   R-8-E LAYOUT	1 IN. = 4,877 FT.

**E.O.P STA. 453 + 20**



**B.O.P. STA. 166 + 35**

**EQUATIONS**

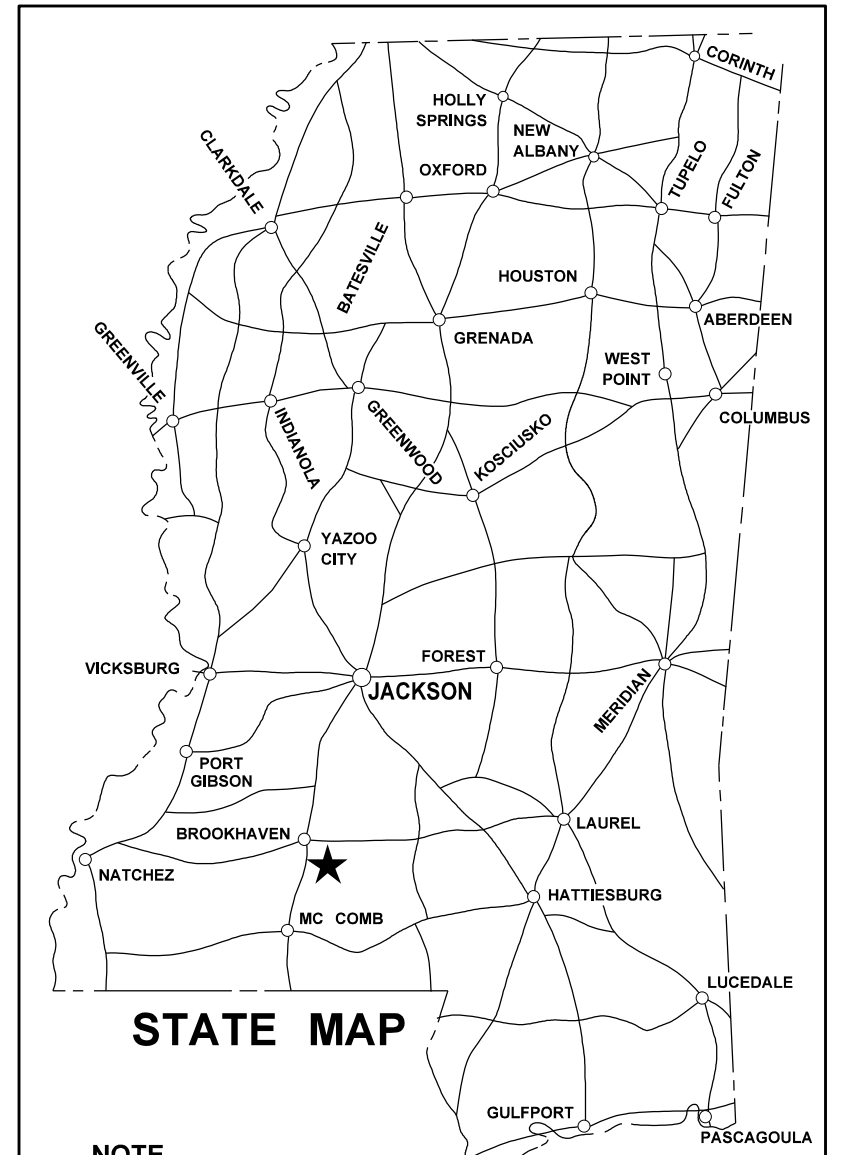
$$255+31.97 \text{ BK} = 255+26.10 \text{ AH} = 5.87$$

$$5.87$$

**LENGTH DATA**

LENGTH OF ROADWAY	28,690.87 FT.	5.434 MI.
LENGTH OF BRIDGES	FT.	MI.
LENGTH OF PROJECT (NET)	28,690.87 FT.	5.434 MI.
LENGTH OF EXCEPTIONS	FT.	MI.
LENGTH OF PROJECT (GROSS)	28,690.87 FT.	5.434 MI.

**EXCEPTIONS**



**STATE MAP**

NOTE  
★ INDICATES APPROXIMATE LOCATION OF PROJECT.

LAT. 31° 30' 36.02" N LONG. 90° 23' 47.46" W  
(APPROX. MIDDLE OF PROJECT)

**DESIGN CONTROL**

MPH = V (SPEED DESIGN)

ADT ( ) = : ADT ( ) =

DHV = : D = % T = %

**PERMITS ACQUIRED BY MDOT**

WETLANDS AND WATERS PERMITS		
	WATERS	WETLANDS
NATIONWIDE #14	<input type="checkbox"/> N	<input type="checkbox"/> N
NATIONWIDE (OTHER)*	<input type="checkbox"/> N	<input type="checkbox"/> N
GENERAL*	<input type="checkbox"/> N	<input type="checkbox"/> N
INDIVIDUAL (404)*	<input type="checkbox"/> N	<input type="checkbox"/> N
STORMWATER PERMIT <input type="checkbox"/> N		
Y	REQUIRED, CNDI SUBMITTED BY MDOT (DISTURBED AREA = 5 ACRES)	
S	REQUIRED, SCNDI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES)	
N	NO STORMWATER PERMIT REQUIRED (<1 ACRE)	
APPROVED BY: _____		

P S & E DATE: 2/24/2021

APPROVED: \_\_\_\_\_  
DEPUTY EXECUTIVE DIRECTOR / CHIEF ENGINEER

EXECUTIVE DIRECTOR \_\_\_\_\_



2/24/2021 11:21:21 AM 013 TITLE SHEET.DGN


STATE	PROJECT NO.
MISS.	SP-0914-00(039) HSIP-0914-00(039)

DESCRIPTION OF SHEET

DESCRIPTION OF SHEET	WKG. NO.	SH. NO.
TITLE AND LAYOUT SHEET		1
DETAILED INDEX	DI-1	2
GENERAL NOTES	GN-1	3
TYPICAL SECTION SHEETS (1)		
TYPICAL SECTION MAINLINE	TS-1	4
QUANTITY SHEETS (6)		
SUMMARY OF QUANTITIES	SQ-1	5
SUMMARY OF QUANTITIES	SQ-2	6
SUMMARY OF QUANTITIES: PROJECT TOTALS	SQ-3	7
ESTIMATED QUANTITIES FOR MILLING, ASPHALT, GRANULAR MATERIAL	EQ-1	8
ESTIMATED QUANTITIES BASE FAILURE REPAIR	EQ-2	9
ESTIMATED QUANTITIES FOR TRAFFIC CONTROL SIGNS	TCPQ-1	10
PLAN PROFILE SHEETS (10)		
STA. 163+00 - STA. 193+00	3	11
STA. 193+00 - STA. 223+00	4	12
STA. 222+00 - STA. 252+00	5	13
STA. 252+00 - STA. 282+00	6	14
STA. 282+00 - STA. 312+00	7	15
STA. 312+00 - STA. 342+00	8	16
STA. 342+00 - STA. 372+00	9	17
STA. 372+00 - STA. 402+00	10	18
STA. 402+00 - STA. 432+00	11	19
STA. 432+00 - STA. 454+00	12	20
SPECIAL DESIGN SHEETS (8)		
DETAIL OF CONSTRUCTION SIGNING	DCS-1	21
DETAIL FOR CURB AND ISLAND STRIPING	DCIS-1	22
STOP SIGN RUMBLES	SDSSR-1	23
TRAFFIC CONTROL DETAILS: DRUM PLACEMENT AND SHOULDER CLOSURE	SDTCP-16	24
DETAIL OF STRIPING NON-CANNELIZED INTERSECTIONS 2-LANE HIGHWAY	PMD-2	25
RAISED PAVEMENT MARKERS: CURVE DETAIL	RPM-1	26
DETAIL OF RUMBLE STRIPS (GROUND IN)	SDRS-1	27
TRAFFIC CONTROL PLAN WITH FLAGGER (ONE-LANE CLOSURE OF TWO-WAY TRAFFIC)	SDTCP-1	28
STANDARDS DRAWINGS (13)		
PAVEMENT MARKING DETAIL FOR 2-LANE AND 4-LANE DIVIDED HIGHWAYS	PM-1	6051
PAVEMENT MARKING LEGEND DETAILS	PM-5	6055
PAVEMENT MARKING LEGEND DETAILS	PM-6	6056
2-WAY RAISED PAVEMENT MARKERS AT INTERSECTING ROADS (2-LANE)	PM-11	6061
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (EXTENDED PERIOD)	TCP-4	6354
TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (WORK DAY ONLY)	TCP-5	6355
SHORT DURATION CLOSING OF TWO-LANE, TWO-WAY HIGHWAYS	TCP-6	6356
HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS	TCP-8	6358
TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO-LANE ROADS	TCP-9	6359
TRAFFIC CONTROL PLAN UNEVEN PAVEMENT DETAILS	TCP-12	6362
TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS	TCP-13	6363
LOCATION OF R16-3 SIGNS (SPEEDING FINES DOUBLED)	TCP-15	6365
RURAL DRIVEWAYS	RD-1	6403
TOTAL NUMBER OF SHEETS (41)		

3/1/2021 1:24:42:27 PM02DI-1-.DGN

DISTRICT 7		
PS & E PLANS-DATE: 2/24/2021		
FMS CON. # 108667/301000/301100		
REVISIONS		
DATE	SHEET NO.	BY

				MISSISSIPPI DEPARTMENT OF TRANSPORTATION			
				<b>DETAILED INDEX</b>			
				<b>PROJ. NO.: SP-0914-00(039)</b> <b>COUNTY: HSIP-0914-00(039)</b> <b>LINCOLN</b>			
				 WORKING NUMBER <b>DI-1</b>			
DATE	FILE NAME: (02)DI-1.dgn	SHEET NUMBER	DESIGN TEAM	DISTRICT 7	CHECKED	DATE	2

STATE	PROJECT NO.
MISS.	SP-0914-00(039) HSIP-0914-00(039)

GENERAL NOTES

- ① THE LOCATION AND SPACING OF SIGNS, SHOWN ON THE TRAFFIC CONTROL PLANS, ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.
- ② ALL TRAFFIC CONTROL DEVICES ON THIS PROJECT SHALL COMPLY WITH PART VI OF THE MUTCD (LATEST EDITION).
- ③ ALL PLASTIC DRUMS SHALL HAVE A BALLASTING COLLAR MADE FROM RECYCLED TRUCK TIRES OR OTHER SUITABLE MATERIAL.
- ④ THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING STRUCTURES SUCH AS, BUT NOT LIMITED TO, PIPES, INLETS, APRONS, AND BRIDGES FROM DAMAGE WHICH MIGHT OCCUR DURING CONSTRUCTION. THE CONTRACTOR SHALL REPLACE OR REPAIR, AS DIRECTED BY THE ENGINEER, ANY STRUCTURES DAMAGED DURING THE LIFE OF THE CONTRACT. NO PAYMENT WILL BE MADE FOR REPLACEMENT OR REPAIR OF DAMAGED ITEMS.
- ⑤ FLUORESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.
- ⑥ VEGETATIVE MATERIAL WILL BE REMOVED PRIOR TO PLACEMENT OF GRANULAR MATERIAL. THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
- ⑦ THE CONTRACTOR SHALL COVER ANY TEMPORARY TRAFFIC CONTROL SIGNS SHOWN IN THE TRAFFIC CONTROL PLAN THAT DO NOT APPLY TO THE CURRENT WORK OR PHASE.
- ⑧ REMOVAL OF RAISED PAVEMENT MARKERS THAT ARE IN CONFLICT WITH REQUIRED CONSTRUCTION IS NOT CONSIDERED A SEPARATE PAY ITEM. COST TO BE ABSORBED IN OTHER ITEMS BID.
- ⑨ WHERE MILLING IS REQUIRED, THE CONTRACTOR SHALL PROVIDE OUTLETS IN THE EXISTING SHOULDERS AT SUFFICIENT INTERVALS TO PREVENT POOLING OR STANDING WATER ON THE MILLED SURFACE, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
- ⑩ TEMPORARY STRIPING SHALL CONFORM TO FINISHED STRIPE SPECIFICATIONS FOR ALIGNMENT, NEATNESS, AND STRAIGHTNESS.
- ⑪ ALL ADDENDA TO THESE PLANS WILL BE POSTED TO WWW.MDOT.MS.GOV UNDER THE PROPOSAL ADDENDA COLUMN. BIDDERS ARE ADVISED THAT HARD COPIES OF ANY ADDENDA FOR THIS PROJECT WILL NOT BE MAILED. IT IS THE BIDDER'S RESPONSIBILITY TO CHECK AND SEE IF ANY ADDENDA HAVE BEEN POSTED FOR THIS PROJECT.
- ⑫ STORAGE OF FLAMMABLE MATERIALS WILL NOT BE ALLOWED UNDER ANY BRIDGE STRUCTURES WITHOUT WRITTEN APPROVAL FROM THE PROJECT ENGINEER. SEE NOTICE TO BIDDERS ENTITLED "MATERIAL STORAGE UNDER BRIDGES" FOR MORE INFORMATION.
- ⑬ THE RETROREFLECTIVE SIGN SHEETING ON RIGID, TEMPORARY TRAFFIC CONTROL (ORANGE) SIGNS SHALL BE MINIMUM TYPE IX.
- ⑭ ALL SIGNS AND DELINEATORS THAT CONFLICT WITH THE CONSTRUCTION OF THIS PROJECT SHALL BE REMOVED AND RESET BY THE CONTRACTOR; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ⑮ TEMPORARY STRIPING SHALL BE REQUIRED AFTER MILLING, PRELIMINARY LEVELING, AND OVERLAYING OPERATIONS; TEMPORARY STRIPING SHALL BE PLACED IN THE SAME LOCATIONS AND LAYOUT AS PERMANENT STRIPE. ALL CENTERLINE, LANE LINES, EDGE LINES, AND NO-PASSING STRIPES THAT HAVE BEEN REMOVED DURING THE DAY'S OPERATIONS SHALL BE REPLACED WITH TEMPORARY STRIPE BEFORE WORK IS DISCONTINUED FOR THE DAY OR AS SOON THEREAFTER AS WEATHER CONDITIONS WILL PERMIT, EXCEPT THAT:
  - (1) REPLACEMENT OF NO-PASSING STRIPES MAY BE DELAYED FOR A PERIOD NOT TO EXCEED THREE (3) DAYS FOR A TWO OR THREE LANE ROAD
  - (2) TEMPORARY EDGE LINES ON PROJECTS REQUIRING SHOULDERS CONSTRUCTED OF GRANULAR MATERIAL MAY BE DELAYED FOR A PERIOD NOT TO EXCEED THREE (3) DAYS.
- ⑯ ALL ASPHALT AND CONCRETE CURBS ALONG RAMPS, LOCAL ROADS, ETC. FROM B.O.P. TO E.O.P. SHALL BE PAINTED ( TWO APPLICATIONS ) WITH WHITE TRAFFIC PAINT AND TRAFFIC BEADS ; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ⑰ IF THE ASPHALT CURB ALONG THE LOCAL ROAD IS SEVERELY DAMAGED, THE ENTIRE ASPHALT CURB WILL BE REMOVED AS DIRECTED BY THE ENGINEER; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ⑱ ALL LOCAL ROADS SHALL BE PAVED TO THE R.O.W. LIMITS OR AS DIRECTED BY THE ENGINEER.
- ⑲ GRANULAR MATERIAL WILL NOT BE ALLOWED TO BE PLACED DIRECTLY ON THE SURFACE LIFT OF ASPHALT, BUT MUST BE PLACED DIRECTLY ON THE GRAVEL SHOULDER OR A ROAD WIDENER MACHINE USED AND APPROVED BY THE PROJECT ENGINEER.
- ⑳ ALL RAMPS AND DRIVEWAY PADS SHALL BE PAVED TO THE SHOULDER LINE, MINIMUM OR FURTHER, AS DIRECTED BY THE ENGINEER WITHIN THE R.O.W. LIMITS.

GENERAL NOTES (CONT.)

- ㉑ TEMPORARY PAVEMENT JOINTS (PAPER JOINTS) SHALL BE EMPLOYED AT ALL LOCATIONS REQUIRING TRAFFIC TO TRAVERSE AN UNEVEN PAVEMENT JOINT. PAPER JOINTS SHALL BE A MINIMUM OF 9 FEET IN LENGTH AND SHALL BE ADEQUATELY MAINTAINED.
- ㉒ NO TEMPORARY CULVERT STREAM CROSSINGS WILL BE ALLOWED.
- ㉓ SOME WORK IS REQUIRED OUTSIDE OF THE PROJECT LIMITS. NO ADDITIONAL COMPENSATION WILL BE MADE FOR SUCH WORK EXCEPT AS PROVIDED BY SPECIFIC PAY ITEMS IN THE PLANS.
- ㉔ THE ASPHALT PAVING OPERATION SHALL BE COMPLETED IN THE FOLLOWING ORDER:
  - A: MILLING
  - B: TRENCH WIDENING
  - C: PRE-LEVELING / S.E. CORRECTION
  - D: SURFACE COURSE

3/1/2021 12:42:28 PM 030 GN-1.DGN  
 REPORT PLAN DIVISION  
 MISSISSIPPI DEPARTMENT OF TRANSPORTATION

		MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
		<b>GENERAL NOTES</b>	
		PROJ. NO.: <b>SP-0914-00(039)</b> COUNTY: <b>HSIP-0914-00(039)</b> <b>LINCOLN</b>	
		WORKING NUMBER <b>GN-1</b>	
		SHEET NUMBER <b>3</b>	
DATE	FILE NAME: \03\GN-1.dgn	DESIGN TEAM	DISTRICT 7
		CHECKED	DATE

