SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO.	1	DATED	5/4/2021	ADDENDUM NO.		DATED	
ADDENDUM NO	2	DATED	5/18/2021	ADDENDUM NO.		DATED	
ADDENDUM NO		DATED		ADDENDUM NO.		DATED	

Number	Description
1	Revised Table of Contents; Added Supplement & NTB No. 2654; Added NTB No. 3318; Amendment EBSx Download Required.
2	Revised NTB No. 3299 with Attachments; Revised Bid Items; Amendment EBSx Download Required.

TOTAL ADDENDA: 2
(Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

_____
Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____
President Address

_____
Secretary Address

_____
Treasurer Address

The following is my (our) itemized proposal.

SP-0039-02(055)/ 108657301000
Rankin County(ies)

Revised 01/26/2016

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 3299 **CODE: ( )**

DATE: 04/15/2021

SUBJECT: Scope of Work

PROJECT: SP-0039-02(055) / 108657301 – Rankin County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings".

Work on this project shall consist of the following.

Mill and overlay approximately 1 mile of State Route 18 from US Highway 80 (Station 10+00) to the Bridge at Terrapin Skin Creek (Station 66+15). Details of specific work are mentioned in the following sections.

From BOP (Station 10+00) to Station EOP (66+15)

This project includes sections of 4-lane and 2-lane highway shown in the included typical sections. Work in this section will consist of repairing failed areas, replacing damaged signs, removing and replacing guardrail, and replacing existing traffic loops with radar detection systems. Prior to milling and overlay operations, failed pavement areas shall be repaired full depth as described in General Notes. Travel lanes, turn lanes, crossovers, shoulders, local roads, and interstate ramps shall be milled at a depth of 2" and variable. The Interstate ramps shall be milled and paved to the curb returns or as directed by the Engineer. Following the milling operations, the roadway shall be then overlaid with a 2"and variable of 12.5-mm, HT, asphalt. Further details on each operation can be found in the general notes for each operation.

Due to an upcoming project on US 80 and the SR 18 intersection, the left turn lane on SR 18 to US 80 Westbound shall not be milled or paved (See attached milling area detail). Temporary striping shall also be left in place in lieu of permanent thermoplastic on the Northbound and Southbound lanes according to the attached detail.

GreenField Road Turn Lane Addition

The Turn Lane from Greenfield Rd to SR 18 East shall be extended according to the attached typical sections and detail drawings. Excess excavation shall be used to remove existing material to a depth of 14". The new pavement structure consists of 6" of Crushed stone (day-lighted) and 7" of 12.5mm HT, Leveling, placed in 3 lifts (2@2.5" and 1@2"). Type V Geotextile fabric shall be placed under all areas requiring stone. This work shall be completed before milling and paving operations begin. Care shall be taken to limit the drop off to no greater than 2" if work cannot be completed in one day.

General Notes

Milling

Milling/paving shall not begin until an **approved** asphalt mix design has been received, nor until such time that, in the opinion of the Engineer, weather conditions have been consistently suitable enough to allow placement of the asphalt after the milling operations.

The Reclaimed Asphalt Pavement (RAP) material removed by the milling operation shall become the property of the Contractor with the exception of 10,000 tons or 50% of the total anticipated RAP tonnage, whichever is less, to be stockpiled at the MDOT Whitfield Maintenance Yard, 3769 Highway 468, Pearl, MS, 39208. The Contractor shall coordinate the efforts with the Maintenance Office to effectively stockpile the milled material as directed by the Engineer. Anytime that milling is being hauled to MDOT, the Contractor shall provide the necessary equipment and operator(s) at the above mentioned location to stockpile the material. All costs associated with the hauling, placing, and stockpiling the State-retained material will be absorbed in other items bid and will not be measured for separate payment.

Where milling is required, the Contractor shall provide outlets in the existing shoulders at sufficient intervals to prevent pooling or standing water on the milled surface, the cost of which shall be absorbed in other items bid.

Milling and paving operations shall be performed such that a -2% slope from centerline is provided in normal crown roadway sections. Super-Elevation through curves shall be maintained as it currently exists or improved as directed.

Milling operations shall be performed in accordance with the Contract Documents and the MDOT Standard Specifications. Variable width and length transitions may be required for ties at ramps, local roads, project limits.

Milling of driveway pads shall be conducted in a manner to prevent gouging or otherwise affecting the roadway pavement structure and slope. Milling of driveway pads shall not be performed in simultaneous path with main line milling.

Traffic will be allowed to travel on the milled surface for two (2) days. Traffic will be allowed to run on all milled local roads for two (2) days unless otherwise stated. Approved mix designs must be on hand prior to milling.

Paving

Prior to mainline milling and paving operations, failed areas in the existing pavement shall be removed and backfilled with 12.5-mm, HT, Leveling asphalt as per the attached typical sections and details. Asphalt shall be placed in multiple lifts with a maximum lift thickness of 2.5". Any granular/chemically treated/stone/etc. base or subgrade material deemed unsuitable by the Engineer shall be removed as directed and backfilled with 12.5-mm, HT, Leveling asphalt. Payment for the excavation of the granular base and subgrade will be made using pay item 203-

G: Excess Excavation. A list of the failed areas is shown in the attached tables. Pavement repairs shall be completed as a continuous operation in order to minimize traffic impacts. Lane closures shall remain in place until the failed area has been completely repaired. Lane closures may not be left unattended.

Payment for saw cuts on failed areas will be made using the appropriate pay items. If milling techniques are used, the area will not require saw cuts but care should be exercised in order to create a neat removal line and to prevent damage to the adjacent pavement structure. If saw cuts are used in conjunction with milling, payment will be made using the appropriate items. Payment will not be made for saw cuts not performed.

Publicly maintained roads and streets shall be paved to the existing right-of-way and in accordance with the attached drawings.

Privately owned entrances shall be paved to the shoulder line per the included typical drawing, unless otherwise directed. Pad dimensions shall match the existing lengths and widths, unless otherwise directed. Pads shall be shaped horizontally and vertically to prevent excessive drop-offs. Any new driveway pads deemed necessary by the Engineer shall be placed according to specifications.

Granular Shoulder Material

Any material excavated from the existing shoulder during pavement widening operations or as a result of shoulder blading shall be used on the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer and will be an absorbed item. Material which cannot be suitably placed in adjacent areas and deemed to be excess excavation by the Engineer shall be removed from the project site. Payment for removal of excess material will be made using pay item 203-G: Excess Excavation.

Granular material (crushed stone) shall be provided around driveway pads as directed to prevent shoulder drop-offs and shall be placed in a timely manner. Drop-offs exceeding 2½" shall be corrected within two (2) calendar days of the placement of the pad.

Where applicable, the existing shoulders shall be raised to match the new pavement elevation by placing variable depth granular material (crushed stone). Placement of the granular material on the finished asphalt course shall not be permitted. The existing shoulder shall be scarified to allow incorporation of the new shoulder material. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%) in normal crown sections. Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%) in normal crown sections. The cost of blading will be an absorbed item and shall be included in the price of other items bid. Crushed concrete will not be allowed.

Temporary and Permanent Pavement Markings

Temporary traffic stripe will be required immediately after the milling and/or required overlay and prior to opening area to traffic. Temporary stripe shall be placed in the same location and configuration as the permanent stripe except that it may be offset as required for milling and paving operations. If temporary stripe is offset, the Contractor shall conduct operations in a manner to insure the final temporary stripe is placed at the required location of the permanent stripe. If removal of temporary offset stripe is required in order to achieve the correct location and alignment of permanent stripe, the cost of removal will be absorbed in other items bid. Placing double temporary centerline will not be allowed.

Temporary striping shall conform to finished stripe specifications for alignment, neatness, and straightness.

The use of short strips of traffic tape will not be allowed unless approved by the Engineer.

Temporary raised pavement markers shall be placed along the centerline of the roadway in any areas expected to be dormant for more than 90 days and/or as directed by the Engineer.

All permanent striping will be double drop thermoplastic, 90-mil thickness unless otherwise specified in Subsection 626.03.1.2. Edge lines shall be placed to accommodate the lane widths shown on the attached applicable typical sections unless prevented by field conditions.

Guardrail

Guardrails shall be replaced at the locations shown on the attached table. Removal of guardrail shall consist of removal of bridge end section, w-beam/thrie beam, terminal end section, posts, and all other appurtenances. All guardrail removed shall be replaced the same day and prior to reopening the adjacent lane of traffic. Voids created by the removal of posts, concrete anchors, footings, etc. shall be backfilled and tamped in accordance with Section 203 of the Standard Specifications. Asphalt shall be extended under the guard rail and two feet (2') behind guard rail post as per the attached detail. The area to be paved shall be bladed to accommodate 3" of asphalt. All existing guardrail will be retained by the Department. The removed material shall be delivered to the MDOT Whitfield Maintenance Yard, 3769 Highway 468, Pearl, MS, 39208. The Contractor shall coordinate the delivery of the retained guard rail with MDOT maintenance personnel. Pavement around guardrail posts shall be blocked out in accordance with the attached drawing. The excavated material shall be retained and used to raise the existing shoulder to match the new pavement elevation. The cost of blading will be an absorbed item and shall be included in the price of other pay items bid. Material which cannot be placed and blended in adjacent areas and deemed to be excess excavation by the Engineer shall be removed under pay item 203-G: Excess Excavation. Object markers at bridge approaches and other locations shall be replaced as shown in the attached table. Removal of object markers shall be absorbed in the cost of other items bid.

Permanent Signs

Permanent signs as listed on the attached tables shall be replaced. Unless otherwise listed in the attached tables, existing posts, anchors, angles/bars, and other components shall be reused. The Contractor shall use new bolts, screws, washers, nuts, etc. of the required sizes in the installation of signs. If required as part of the sign replacement activities, all post, pipe, and I-beam lengths

in these plans are estimated. Post lengths for all signs shall be verified in the field by the Contractor prior to fabrication. Installation dates shall be clearly written in bold black markings on the back bottom half off all signs with a permanent marking stick that is waterproof, fade resistant, and marks on wet or dry surfaces. The removal of damaged signs shall be absorbed in the price of other items bid. All existing signs which are to be removed as a part of this project that are not in conflict with construction shall remain in place until new signs are installed unless noted or directed otherwise by the Project Engineer.

Radar Detection Systems

All existing vehicle loop assemblies in the attached table shall be replaced with radar detection systems with the exception of the West Bound Approach at US 80. The Contractor shall replace all three (3) detection loops for the WB approach (6' x 50' QUAD). The cost of loop tails shall be cost absorbed. The vehicle loop assemblies shall be replaced at this location only. Removal of existing loop assemblies shall be absorbed into other items bid. The existing EPAC Controllers shall be replaced with new controllers. Existing EPAC controllers are to be salvaged to MDOT Signal Shop (601-359-1454). Contractor shall be responsible for transferring existing controller data to the new controllers. Radar units shall be mounted per manufacturer recommendations. Contractor shall be responsible for setting up all new signal controllers and detection units to communicate with MDOT Network via existing network switch in each signal cabinet. MDOT shall provide the IP addresses. Contractor may remove existing detection loop cable, if necessary.

Traffic Control

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost shall be included in the price bid for pay item 618-A: Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black legend and border on white background.

Standard roadside construction signs, barricades, etc. shall be placed in accordance with the attached tables, drawings, and as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed. Payment for standard roadside construction signs, barricades, etc. will be made using the appropriate pay items.

On a daily basis, the Contractor shall remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal; the cost shall be included in the prices of other items bids. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor.

Temporary asphalt joints (aka paper joints) shall be employed at all locations requiring traffic to traverse an uneven, transverse, pavement joint. Paper joints shall be a minimum of nine feet (9’) in length and for the full width of the milled/paved surface. Paper joints for 1” OGFC joints shall be a minimum of three feet (3’) in length. Paper joints shall be adequately maintained.

Potholes that may exist or occur in the existing pavement shall be patched in a timely manner as required. Patching of potholes shall be considered an absorbed item.

Miscellaneous Notes

It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged by the Contractor during the life of the contract. No payment will be made for replacement or repair of damaged items.

Any signs that are in conflict with construction of this project shall be removed and relocated by the Contractor as directed by the Engineer; the cost of which shall be absorbed in other items bid.

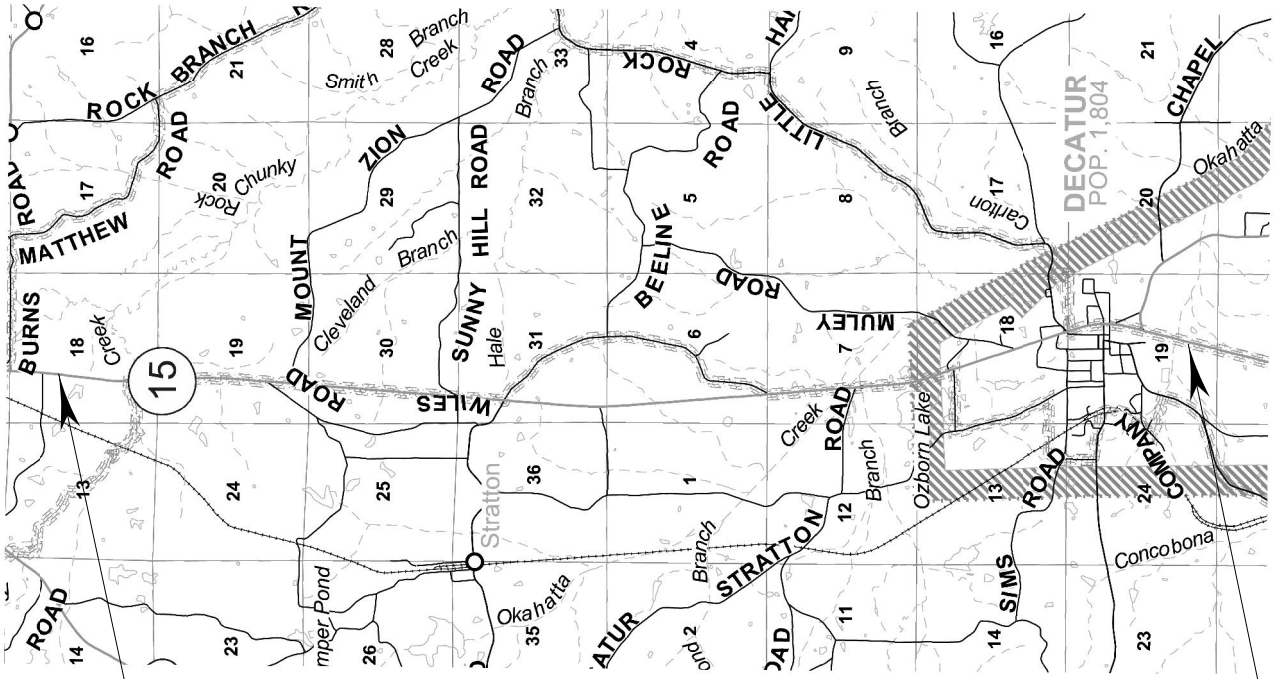
Removal of existing raised pavement markers shall be included in the prices for other items bid.

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment and will be considered included in the prices of items bid.

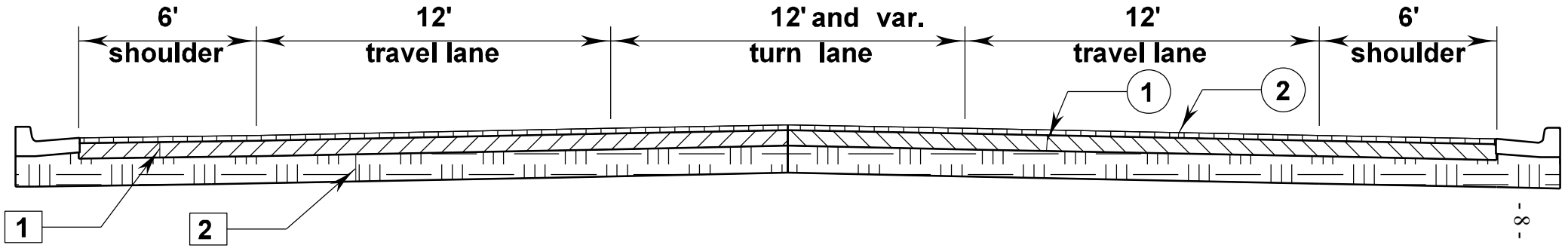
Temporary portable rumble strips, paid for under pay item 907-619-B, shall be used in accordance with detail shown. These rumble strips shall be retained by the Department upon completion of the project. The rumble strips shall be delivered to the MDOT Whitfield Maintenance Yard, 3769 Highway 468, Pearl, MS 39208.

Bridges shall be swept off and cleaned at the end of the project to remove any existing debris plus any debris accumulated from construction activities. The sweeping and cleaning of the bridges shall be absorbed into other items bid.

NEWTON COUNTY
MP-5015-51(021) 307480/301000
SR 15 FROM DECATUR TO HENRY MACK SMITH RD



Typical Section 1
Sta. 10+00 - Sta. 56+00



EXISTING

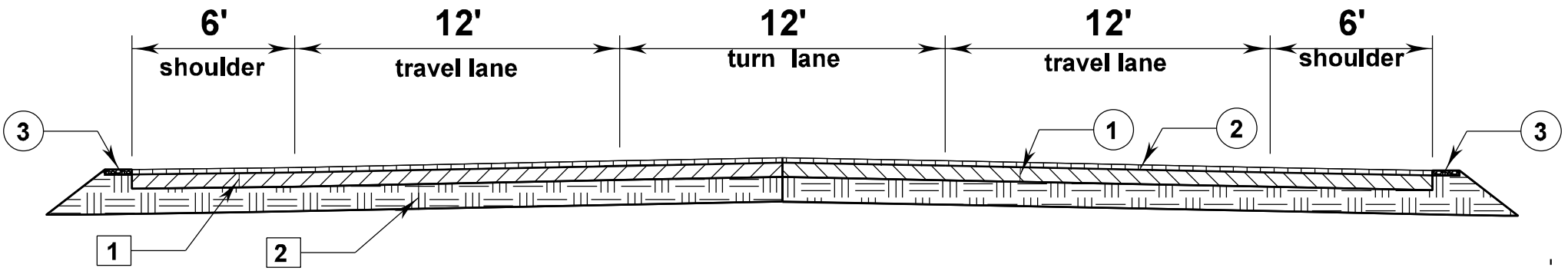
1. Existing $6\frac{1}{2}$" and variable hot mix asphalt.
2. 11" granular material.

- proposed asphalt
- existing asphalt
- granular material

PROPOSED

1. Mill existing asphalt $1\frac{1}{2}$" and variable.
2. Place 2" and variable 12.5mm, Mix, MT.
   - lift thickness to be adjusted on outside to match curb.

Typical Section 2
Sta. 56+00 - Sta. 75+58



EXISTING

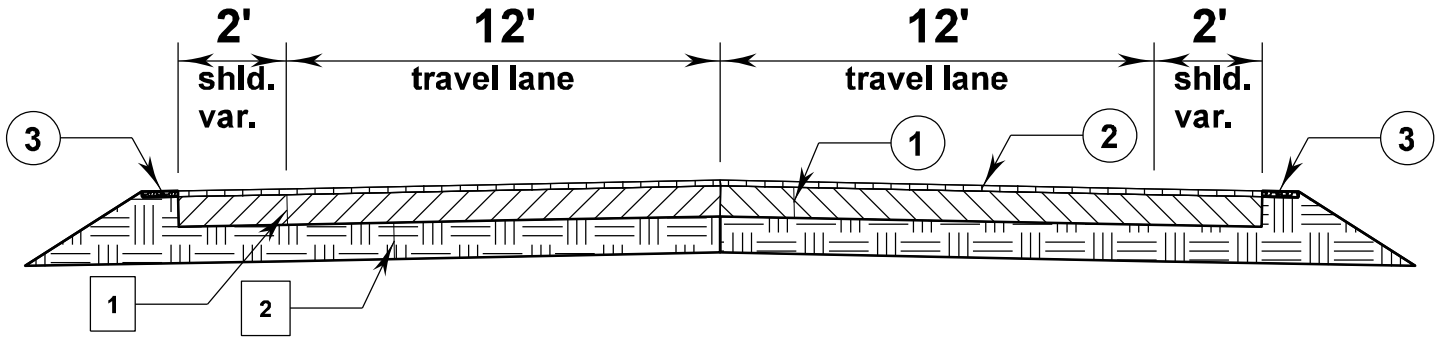
1. Existing $6\frac{1}{2}$" and variable hot mix asphalt.
2. 11" granular material.

- proposed asphalt
- existing asphalt
- granular material
- crushed stone

PROPOSED

1. Mill existing asphalt $1\frac{1}{2}$" and variable.
2. Place 2" and variable 12.5mm, Mix, MT.
3. Place variable depth crushed stone to bring shoulders to grade.

Typical Section 3
Sta. 75+58 - Sta. 81+30



EXISTING

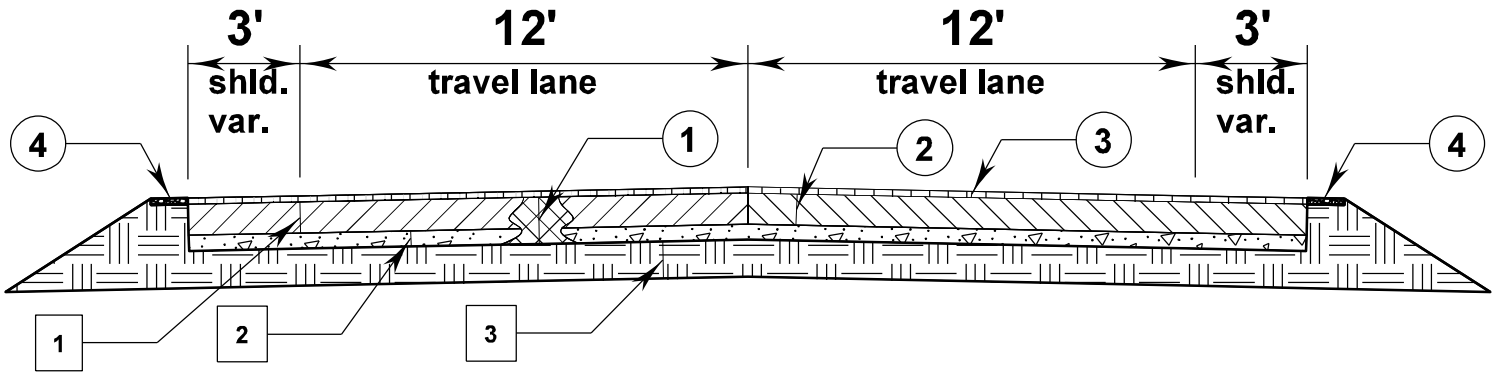
1. Existing 10" and variable hot mix asphalt.
2. 5" Granular material and variable.

- proposed asphalt
- existing asphalt
- granular material
- crushed stone

PROPOSED

1. Mill existing asphalt $1\frac{1}{2}$".
2. Place 2" and variable 12.5mm, Mix, MT.
3. Place variable depth crushed stone to bring shoulders to grade.

Typical Section 4
Sta. 81+30 - Sta. 408+38



EXISTING

1. Existing 10" and variable hot mix asphalt.
2. 8" and variable JRCP.
3. 12" clay gravel.

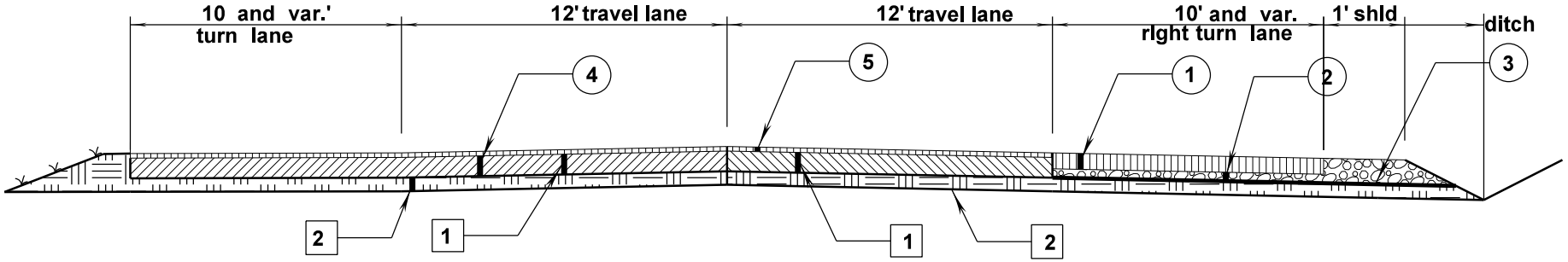
- proposed asphalt
- existing asphalt
- granular material
- failed area
- existing concrete
- crushed stone

PROPOSED

1. Repair failed areas full depth with 12.5mm, Mix, MT Leveling.
2. Mill existing asphalt $1\frac{1}{2}$".
3. Place 2" and varlable 12.5mm, Mlx, MT.
4. Place variable depth crushed stone to bring shoulders to grade.

RANKIN COUNTY
SP-0039-02(055)
108657/301000
HIGHWAY 18

Greenfield Road



EXISTING

1 - 11" and variable HMA

2 - 6" and Variable Granular Material

= Overlay with 2" 12.5mm,Mix, MT

= Daylight stone

= Geotextile fabric

= Existing asphalt

= Granular Material, CL10, GR D

PROPOSED

① Overlay 7" and variable of 12.5mm Mix, HT Leveling. (2 lifts @ 2.5", 1 lift @ 2")

② Place 6" of crushed stone to be daylighted.

③ Place geotextile fabric.

④ Mill existing asphalt 2" and varaible.

⑤ Overlay with 2" and variable of 12.5mm MIX, HT.

Newton County
MP-5015-51(021) 307480/301000

STANDARD ROADSIDE SIGNS - 0.080" THICKNESS										
STATION	SIGN NUMBER	SIZE (in. x in.)	AREA (sf)	PIPE POSTS (lf)				U POST (lf)		REMARKS
				3"	3-1/2"	4"	5"	2 lb/ft	3 lb/ft	
28+10	M2-1	21x15	2.15						12.00	RL, JCT
28+10	M1-5	30x24	5.00						12.00	RL, 503
28+10	SN7923	24x30	5.00						12.00	RL, Weight Limit
Brand St	W1-8	18x24	3.00						12.00	
Brand St	W1-8	18x24	3.00						12.00	
29+75	R3-9B	24x36	6.00						12.00	
Total this sheet =			24.150	0.00	0.00	0.00	0.00	0.00	72.00	

*Signs attached to pipe post shall be mounted on the existing post and footing, removal and/or reinstallation of existing signs shall be absorbed in other items bid.

Newton County
MP-5015-51(021) 307480/301000

STANDARD ROADSIDE SIGNS - 0.125" THICKNESS										
STATION	SIGN NUMBER	SIZE (in. x in.)	AREA (sf)	PIPE POSTS (lf)				U POST (lf)		REMARKS
				3"	3-1/2"	4"	5"	2 lb/ft	3 lb/ft	
25+50									**12.00**	Remove Turkey Creek, Reinstall North 15 on new post
61+50	S1-1	36x36	9.00						**12.00**	RL, School Crosswalk
243+00	W2-1	30x30	6.25						**12.00**	RL, Interection
Brand St	R1-1	36x36	9.00						**12.00**	STOP Sign
Total this sheet =			24.25	0.00	0.00	0.00	0.00	0.00	36.00	

*Signs attached to pipe post shall be mounted on the existing post and footing, removal and/or reinstallation of existing signs shall be absorbed in other items bid.

Removal of Concrete Failed Areas w/ variable Depth Overlay									
Location	STA		Length (ft)	Width (ft)	Saw Cuts (ft)	Removal Area (SY)	Estimated Asphalt Req. (TONS)	Estimated Excess	REMARKS
Column1	**Column2**	**lum**	**Column4**	**Column5**	**Column6**	**Column7**	**Column8**	**Column9**	**Column10**
RT &LT	139+25		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	141+23		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	143+33		10	20	50	22.222	18.750	3.70	Joint Repair
RT &LT	147+20		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	149+00		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	154+75		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	156+25		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	161+30		7	20	47	15.556	13.125	2.59	Joint Repair
RT &LT	163+75		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	168+75		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	173+65		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	187+05		10	20	50	22.222	18.750	3.70	Joint Repair
RT &LT	188+85		7	20	47	15.556	13.125	2.59	Joint Repair
RT &LT	194+60		6	20	46	13.333	11.250	2.22	Joint Repair
RT	197+25		10	10	30	11.111	9.375	1.85	Joint Repair
RT &LT	203+75		6	20	46	13.333	11.250	2.22	Joint Repair
RT	212+50		9	10	29	10.000	8.438	1.67	Joint Repair
LT	212+50		6	10	26	6.667	5.625	1.11	Joint Repair
RT &LT	221+20		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	233+22		8	20	48	17.778	15.000	2.96	Joint Repair
RT &LT	241+00		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	248+45		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	272+00		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	277+28		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	279+50		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	307+75		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	309+80		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	312+20		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	314+00		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	330+65		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	332+30		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	337+50		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	338+35		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	342+25		7	20	47	15.556	13.125	2.59	Joint Repair
RT &LT	343+90		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	347+50		8	20	48	17.778	15.000	2.96	Joint Repair
RT &LT	357+15		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	364+75		6	20	46	13.333	11.250	2.22	Joint Repair
RT &LT	392+42		6	20	46	13.333	11.250	2.22	Joint Repair
			Totals		**1756**	541.111	456.563	90.185	

Removal of Asphalt Failed Areas, All Depths								
Location	STA	Length (ft)	Width (ft)	Saw Cuts (ft)	Area (SY)	Estimated Asphalt Req. (TONS)	Estimated Excess	REMARKS
Column1	**Column2**	**Column4**	**Column5**	**Column6**	**Column7**	**Column8**	**Column9**	**Column10**
RT &LT	139+25	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	141+23	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	143+33	10	8	36	8.89	7.5	1.48	Joint Repair
RT &LT	147+20	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	149+00	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	154+75	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	156+25	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	161+30	7	8	30	6.22	5.25	1.04	Joint Repair
RT &LT	163+75	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	168+75	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	173+65	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	187+05	10	8	36	8.89	7.5	1.48	Joint Repair
RT &LT	188+85	7	8	30	6.22	5.25	1.04	Joint Repair
RT &LT	194+60	6	8	28	5.33	4.5	0.89	Joint Repair
RT	197+25	10	4	28	4.44	3.75	0.74	Joint Repair
RT &LT	203+75	6	8	28	5.33	4.5	0.89	Joint Repair
RT	212+50	9	4	26	4.00	3.375	0.67	Joint Repair
LT	212+50	6	4	20	2.67	2.25	0.44	Joint Repair
RT &LT	221+20	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	233+22	8	8	32	7.11	6	1.19	Joint Repair
RT &LT	241+00	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	248+45	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	272+00	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	277+28	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	279+50	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	307+75	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	309+80	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	312+20	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	314+00	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	330+65	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	332+30	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	337+50	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	338+35	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	342+25	7	8	30	6.22	5.3	1.04	Joint Repair
RT &LT	343+90	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	347+50	8	8	32	7.11	6.0	1.19	Joint Repair
RT &LT	357+15	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	364+75	6	8	28	5.33	4.5	0.89	Joint Repair
RT &LT	392+42	6	8	28	5.33	4.5	0.89	Joint Repair
LT,TWR	395+15	4	20	48	8.89	7.5	1.48	Trench Widening Repair
LT,TWR	395+80	4	15	38	6.67	5.6	1.11	Trench Widening Repair
LT,TWR	396+90	4	25	58	11.11	9.4	1.85	Trench Widening Repair
RT ,TWR	399+25	4	25	58	11.11	9.4	1.85	Trench Widening Repair
		Totals		**1314**	**254.222**	**214.500**	**42.370**	

*QUANITITIES TO BE USED AS DIRECTED BY THE ENGINEER.

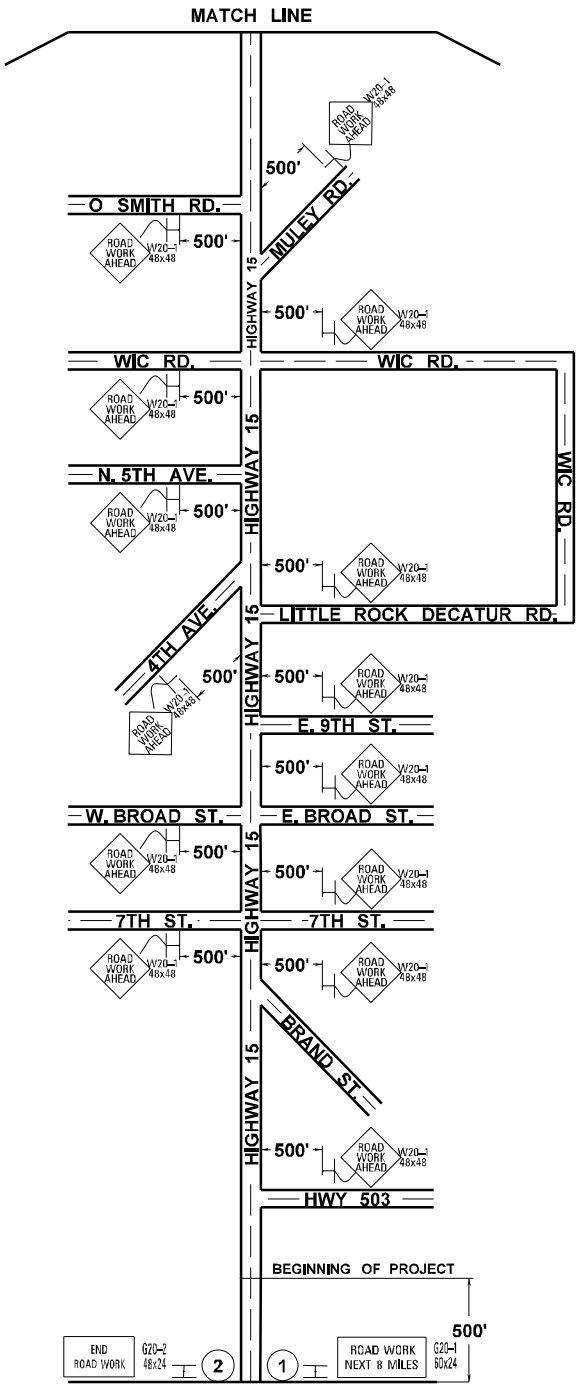
GUARD RAIL QUANTITIES

GUARDRAIL			Cable	BRIDGE END SECTION			DELINEATORS				REMOVAL ITEMS	REMARKS
STATION	(W-BEAM) (LF)	TERMINAL END SECTION	Anchor TYPE I (EA)	Type "I" (EA)	TYPE "G" (EA)	TYPE "G" Modified (EA)	WHITE	YELLOW	Type 3 Object Markers OM-3R,3L	Type 3 Object Markers OM-3R,3L 2 Markers Per Post	GUARDRAIL	
123+29	162.5	1		1			8		1		221	RL
123+29	50	1		1			6		1		108.5	LL
124+71	37.5	1		1			6		1		112.5	RL
124+71	150	1		1			9		1		225	LL
197+00	137.5	2					7			1	196	LL
197+00	150	2					8			1	208.5	RL
292+00	137.5	2					8			1	212.5	LL
292+00	137.5	2					8			1	212.5	RL
344+50	50	2					7			1	125	LL
344+50	87.5	2					8			1	162.5	RL
350+60	137.5	2					8			1	212.5	LL
350+60	137.5	2					8			1	212.5	RL
372+28	162.5	1		1			10		1		221	RL
372+28	50	1		1			6		1		108.5	LL
373+70	50	1		1			6		1		108.5	RL
373+70	162.5	1		1			8		1		221	LL
TOTAL =	**1800**	**24**	**0**	**8**	**0**	**0**	**121**	**0**	**8**	**8**	**2868**	
	L.F.	**EA.**	**EA.**	**EA.**	**EA.**	**EA.**	**EA.**	**EA.**	**EA.**	**EA.**	**LF.**	**MP-5015-51(021)**

* ALL EXISTING TERMINAL END SECTIONS WERE MEASURED AT 37.5'. W-BEAM MAY HAVE TO BE ADJUSTED BASED OFF NEW TERMINAL END LENGHTS.
* REMOVAL OF ALL GUARDRAIL (BRIDGE END SECTIONS, W-BEAM, TYPE-I CABLE ANCHORAGE, TERMINAL END SECTIONS, ETC.) WILL BE PAID UNDER PAY ITEM 202-B REMOVAL OF GUARD RAIL.
* REMOVAL OF GUARDRAIL DELINEATORS ARE CONSIDERED INCIDENTAL TO THE REMOVAL OF GUARDRAIL AND WILL NOT BE MEASURED AS A SEPARATE PAY ITEM.
*REMOVAL OF OBJECT MARKERS WILL NOT BE MEASURED AS A SEPARATE PAY ITEM AS SHALL BE ABSORBED IN OTHER ITEMS

NEWTON COUNTY
MP-5015-51(021) 307480/301000
HWY 15 DECATUR TO HENRY MACK SMITH RD

CONSTRUCTION SIGN DETAIL



SIGN LEGEND	
NUMBER	DESCRIPTION
1	ROAD WORK NEXT 8 MILES G20-1 60x24
2	END ROAD WORK G20-2 48x24
3	ROAD WORK AHEAD W20-1 48x48

TRAFFIC CONTROL SIGNS REQUIRED

2 - G20-1 "ROAD WORK NEXT MILE"

2 - G20-2 "END ROAD WORK"

4 - TYPE III DOUBLE FACED BARRICADES

25 - W20-1 "ROAD WORK AHEAD"

63 - R4-1 "DO NOT PASS"

16 - R4-2 "PASS WITH CARE"

19 - W14-3 "NO PASSING ZONE"

NOTES:

ONE (1) W20-1 "ROAD WORK AHEAD SIGN IS REQUIRED AT EACH LOCAL ROAD, STREET, OR HIGHWAY ENTERING THE PROJECT.

G20-1 AND G20-2 SIGNS MOUNTED ON TYPE III DOUBLE FACED BARICADE.

FIELD CONDITIONS MAY REQUIRE SOME SIGNS ON THIS DETAIL TO BE ADJUSTED.

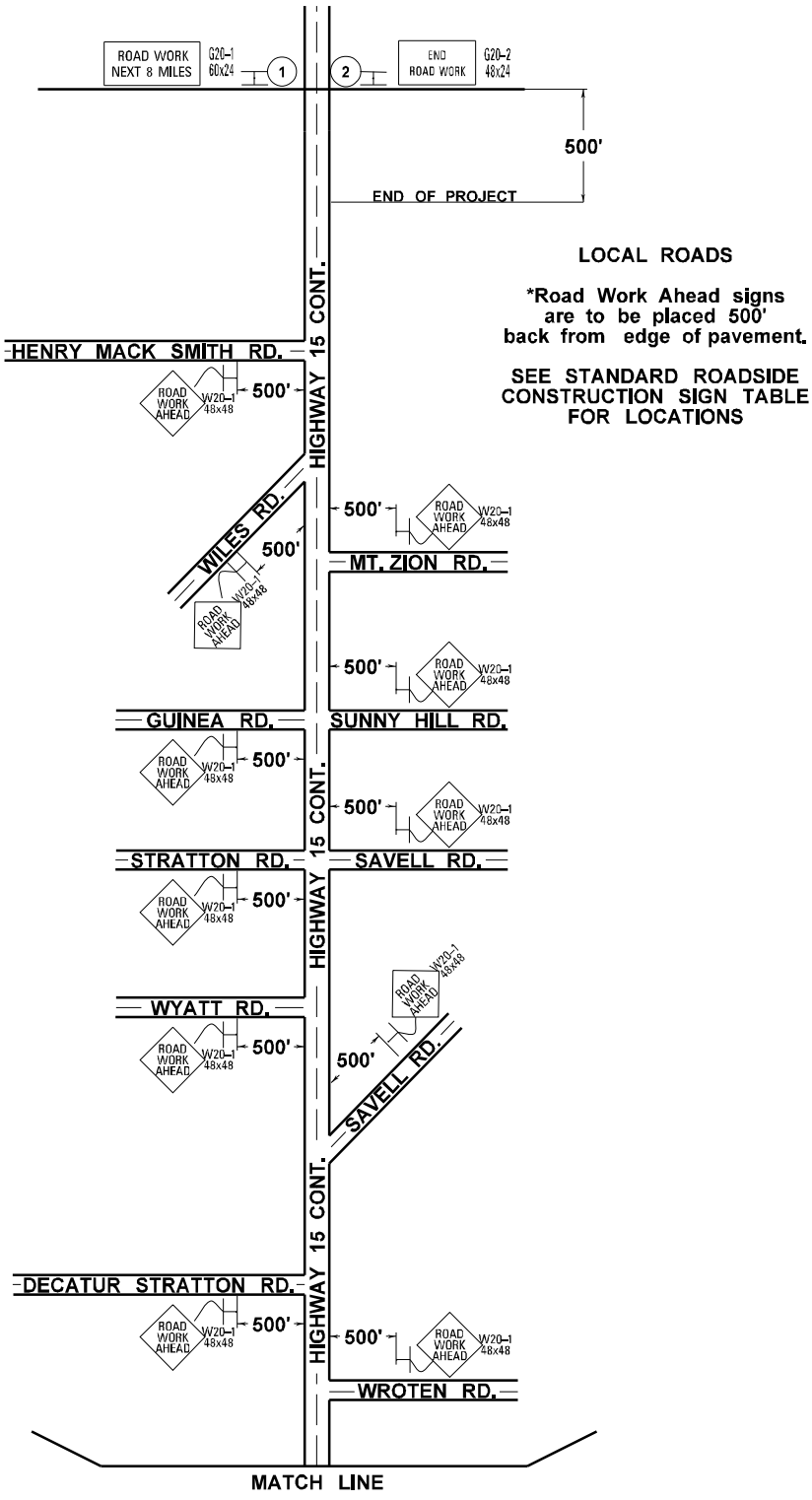
THE ABOVE SHOWN ITEMS WILL BE PAID UNDER THE APPROPRIATE PAY ITEMS.

LOCAL ROADS

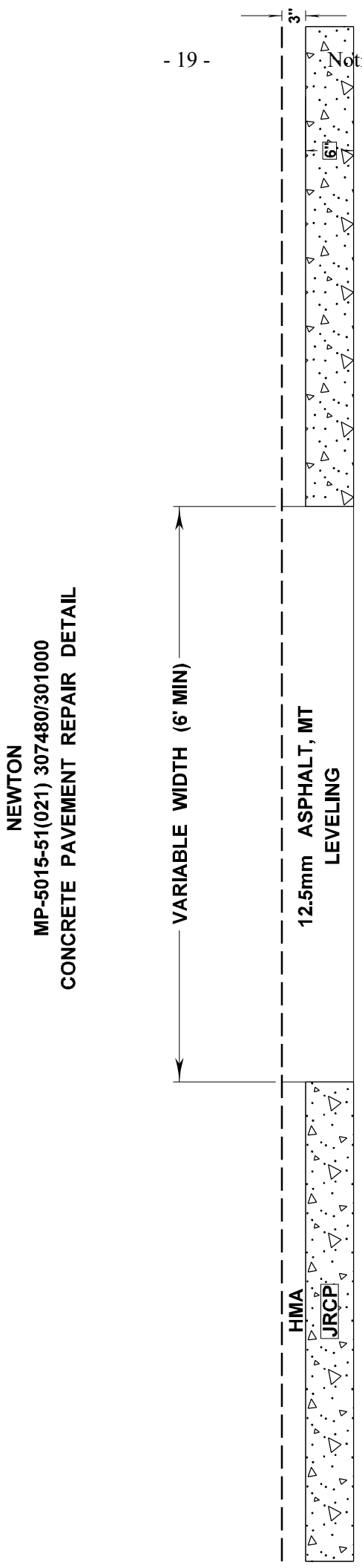
*ROAD WORK AHEAD SIGNS
ARE TO BE PLACED 500'
BACK FROM EDGE OF PAVEMENT.

SEE STANDARD ROADSIDE
CONSTRUCTION SIGN TABLE
FOR LOCATIONS

CONSTRUCTION SIGN DETAIL

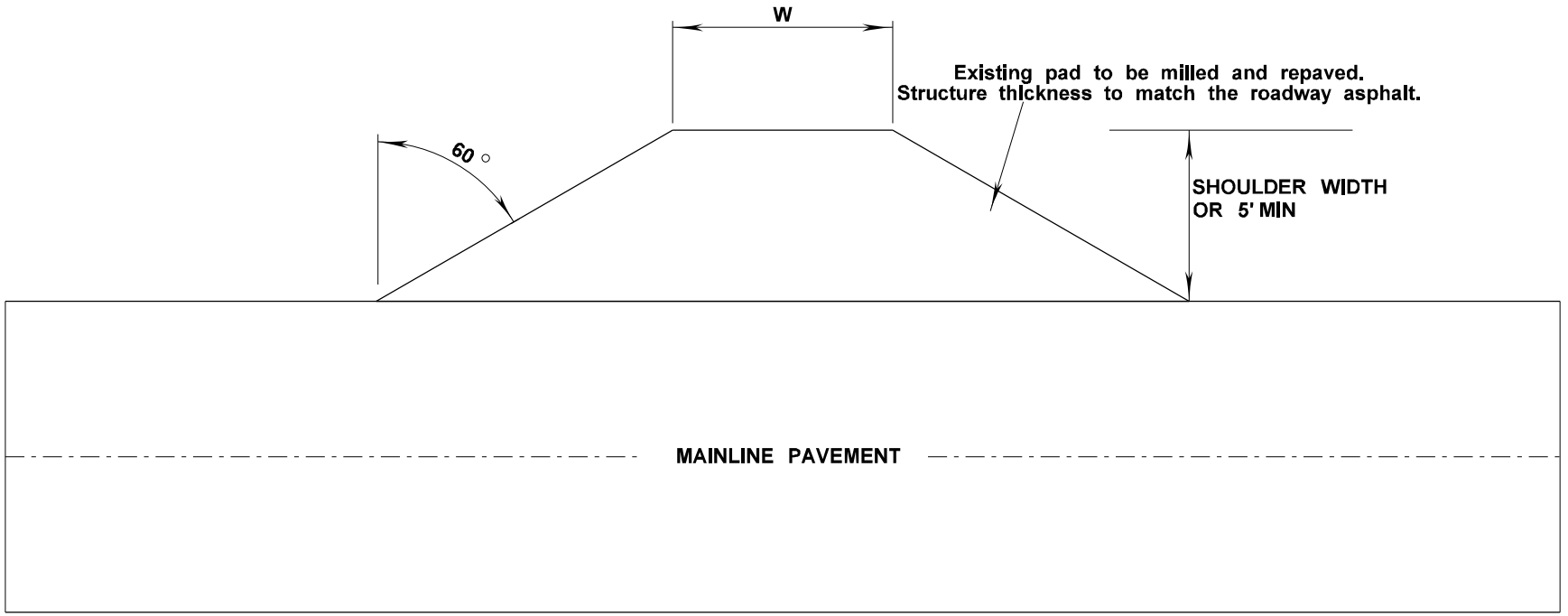


SIGN LEGEND	
NUMBER	DESCRIPTION
1	ROAD WORK NEXT 8 MILES G20-1 60x24
2	END ROAD WORK G20-2 48x24
3	ROAD WORK AHEAD W20-1 48x48


NEWTON
MP-5015-51(021) 307480/301000
CONCRETE PAVEMENT REPAIR DETAIL
3"
6"
VARIABLE WIDTH (6' MIN)
12.5mm ASPHALT, MT
LEVELING
HMA
JRCP

NEWTON COUNTY
MP-5015-51(021) 307480/301000

DRIVEWAY PAD DETAIL



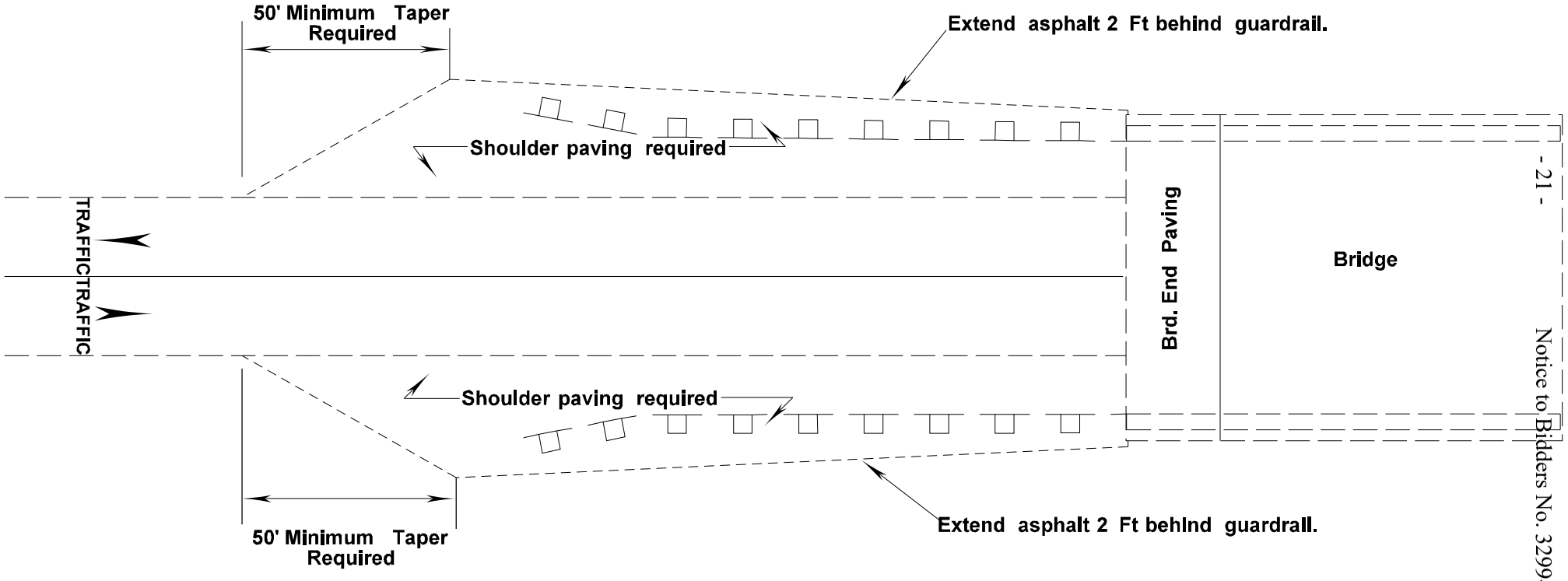
W = 16' MAX RESIDENTIAL
W = 30'-50' COMMERCIAL

NOTE:

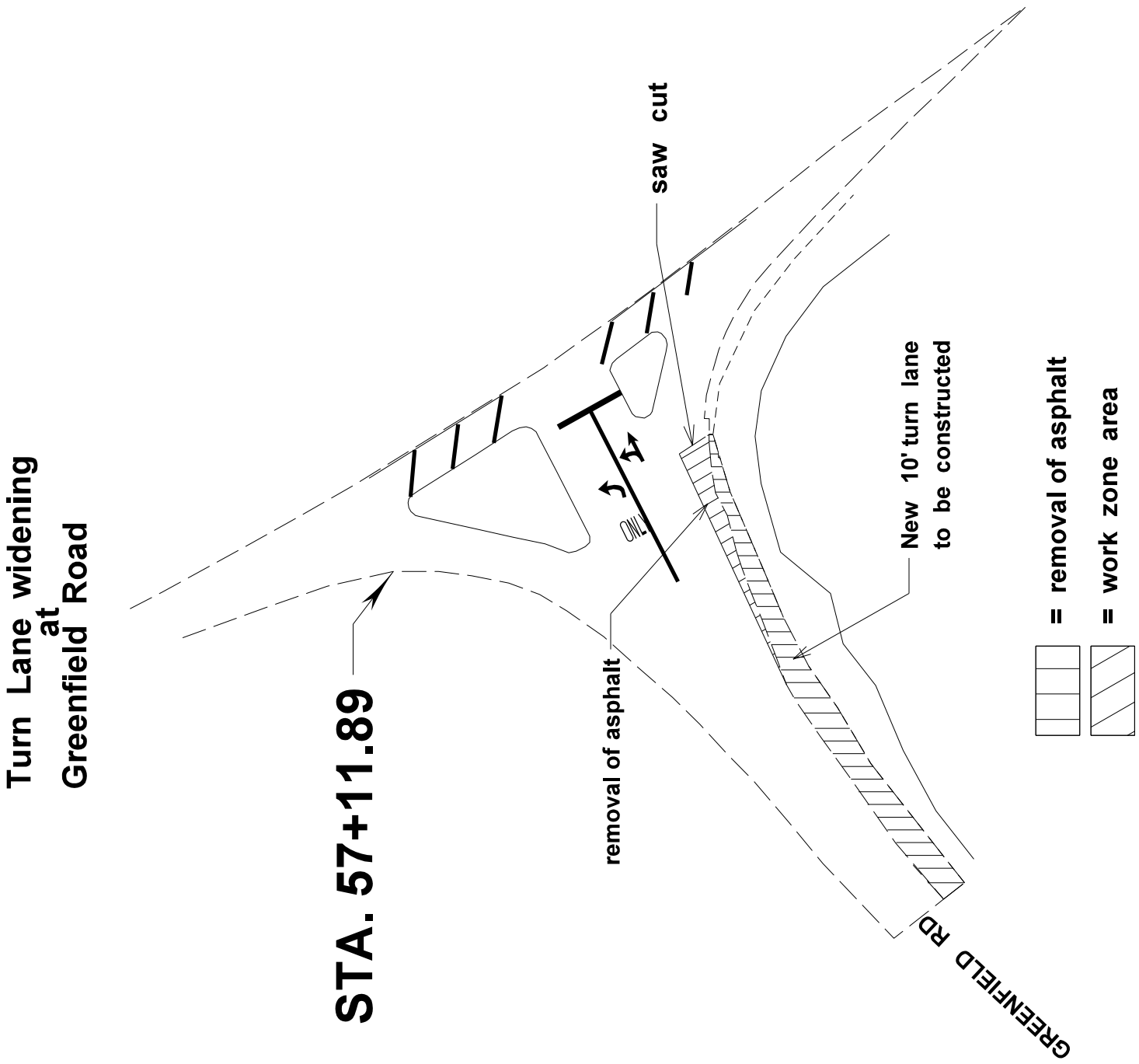
THE ASPHALT ON THE EXISTING DRIVEWAY/RAMP PADS ARE TO REMAIN IN THEIR CURRENT SIZE AND LOCATION AND MILLED/OVERLAID. IF, IN THE OPINION OF THE ENGINEER, A PAD SHOULD BE MODIFIED OR REPLACED, PAYMENT WILL BE MADE FOR THE WORK USING THE APPROPRIATE PAY ITEMS. GRANULAR MATERIAL AND/OR STABILIZER AGGREGATE SHOULD BE PLACED AROUND THE PADS AS REQUIRED.

NEWTON COUNTY
MP-5015-51(021) 307480/301000

TYPICAL DETAIL OF ADDITIONAL SHOULDER PAVING REQUIRED AT GUARDRAIL LOCATIONS



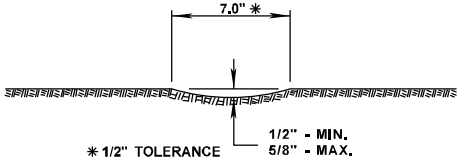
*Asphalt Thickness
See scope of work for additional details


Turn Lane widening
at
Greenfield Road
STA. 57+11.89
saw cut
removal of asphalt
CNL
New 10' turn lane
to be constructed
GREENFIELD RD
= removal of asphalt
= work zone area

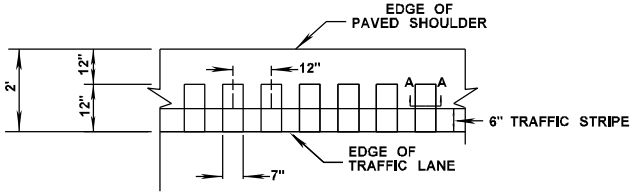

=milled area
Highway 503
mill and paving
locations.


NEWTON COUNTY
MP-5015-51(021) 307480/301000
DITCH AREA FILL
POT Sta. 343+02.817
340
POT Sta. 337+20.000
CROSS SECTIONAL VIEW OF
FILL LOCATION FROM
STA. 337+25 to STA. 341+75 RL
NOTE: WORK IN THIS AREA WILL BE DONE
ACCORDING TO SCOPE OF WORK.

NEWTON COUNTY
MP-5015-51(021) 307480/301000



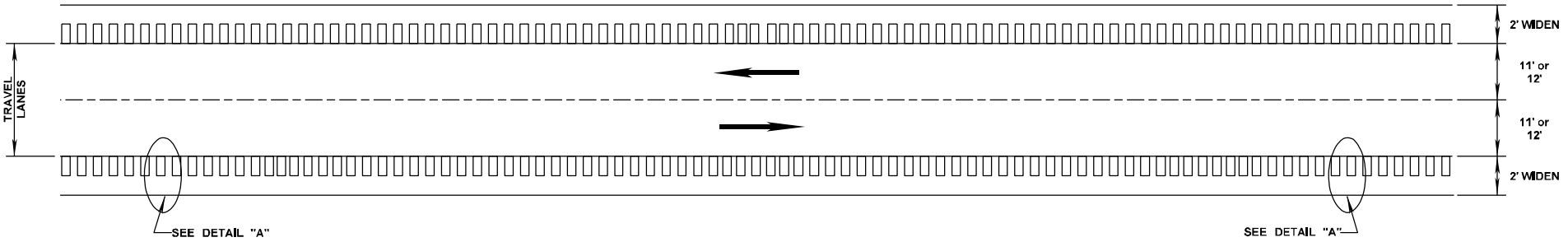
SECTION "A-A"



DETAIL "A"

GENERAL NOTES

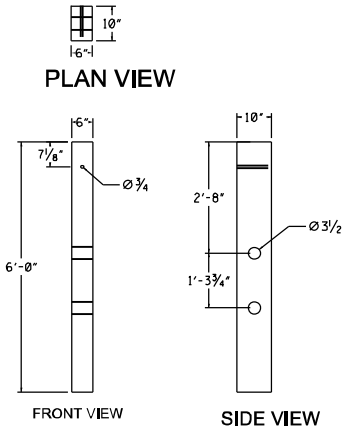
1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
2. GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
4. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.
5. DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.



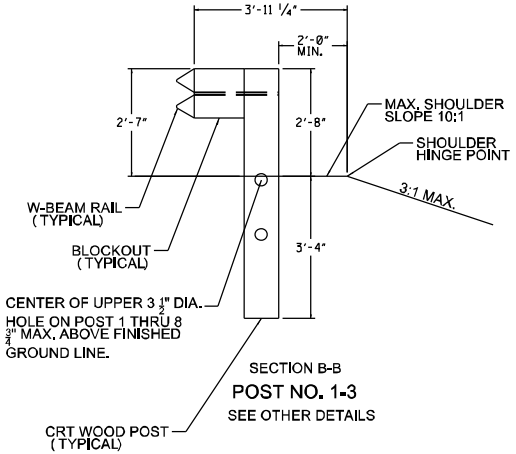
PLAN

NOT TO SCALE

NEWTON COUNTY
MP-5015-51 (021) 307480/301000
SR15 FROM DECATUR TO HENRY MACK SMTH RD



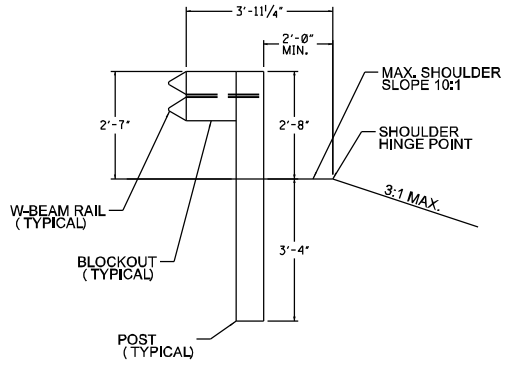
CRT WOOD POST



SECTION B-B

POST NO. 1-3

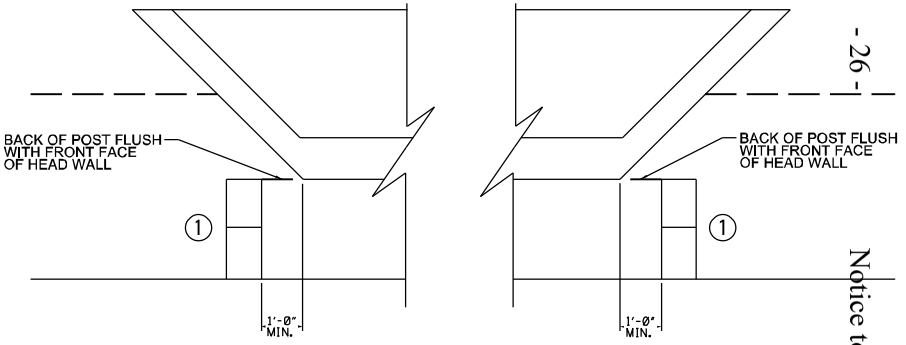
SEE OTHER DETAILS



SECTION A-A

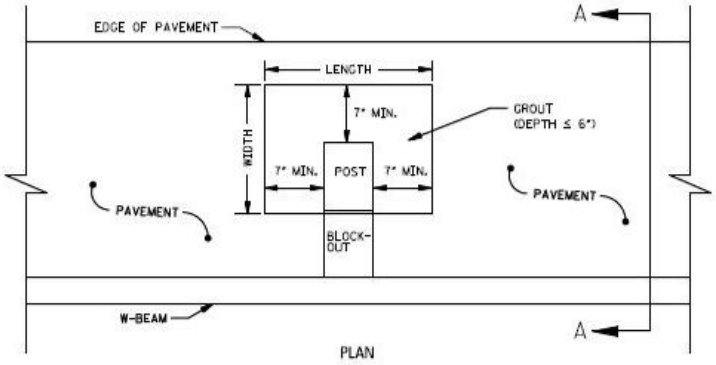
POST NO. 4-8

SEE OTHER DEALS



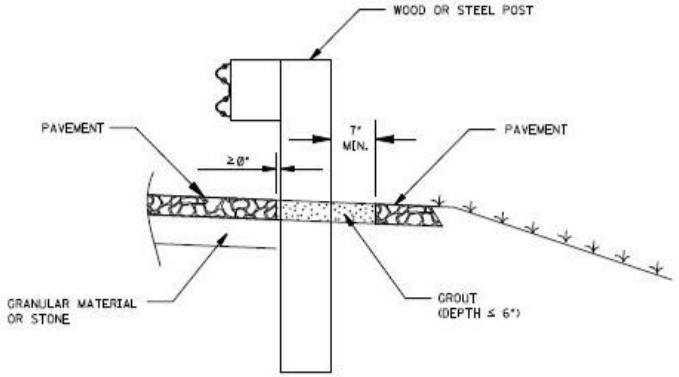
POST PLACEMENT DETAIL

Guardrail Post Installation in Paved Areas



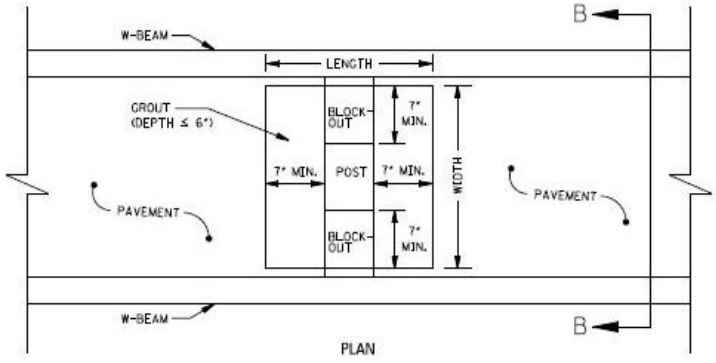
PLAN

PAVEMENT LEAVE-OUT AREA FOR GUARDRAIL POSTS
SINGLE-FACED GUARDRAIL



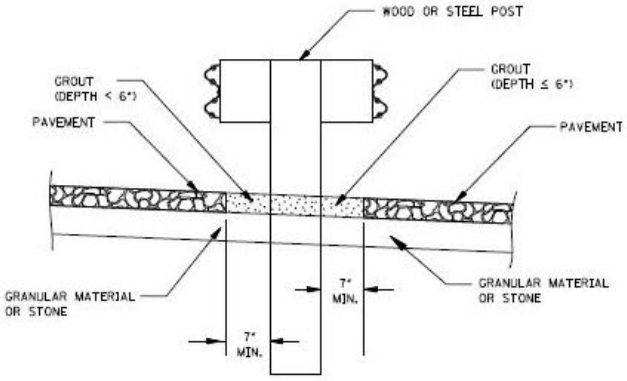
SECTION A–A

MIN. PAV'T LEAVE-OUT AREA				
POST	SINGLE-FACED		DOUBLE-FACED	
	LENGTH (IN.)	WIDTH (IN.)	LENGTH (IN.)	WIDTH (IN.)
6"x8" WOOD	20	15	20	22
8"x8" WOOD	22	15	22	22
10"x10" WOOD	24	17	24	24
W6x9 STEEL	18	13	18	20



PLAN

PAVEMENT LEAVE-OUT AREA FOR GUARDRAIL POSTS
DOUBLE-FACED GUARDRAIL



SECTION B–B

GENERAL NOTES

1. GUARDRAIL POSTS SHALL NOT BE COMPLETELY SURROUNDED BY PAVEMENT. THE AREA BEHIND AND LATERAL OF THE POST SHALL HAVE A MINIMUM 7" CLEARANCE FROM THE PAVEMENT. THIS AREA SHALL BE FILLED WITH A LOW STRENGTH GROUT WITH A MAXIMUM 28 DAY COMPRESSIVE STRENGTH OF 120 PSI.
2. GROUT SHALL BE INSTALLED AT A DEPTH EQUAL TO THE SURROUNDING PAVEMENT, UP TO A MAXIMUM OF 6". IF SURROUNDING PAVEMENT IS GREATER THAN 6", THE DIFFERNCE SHALL BE FILLED IN WITH SHOULDER GRANULAR MATERIAL.
3. COST OF GROUT SHALL BE ABSORBED IN THE COST OF OTHER ITEMS BID.
4. PAVEMENT LEAVE-OUT AREAS ARE REQUIRED FOR STEEL AND WOOD POSTS.
5. STANDARD EMBEDMENT DEPTHS STILL APPLY, MEASURED FROM THE TOP OF THE PROJECTED PAVEMENT SURFACE.

NEWTON COUNTY-HIGHWAY 15
MP-5015-51 (021) 307480/301000
(SR15 From Decatur to Henry Mack Smith Road)

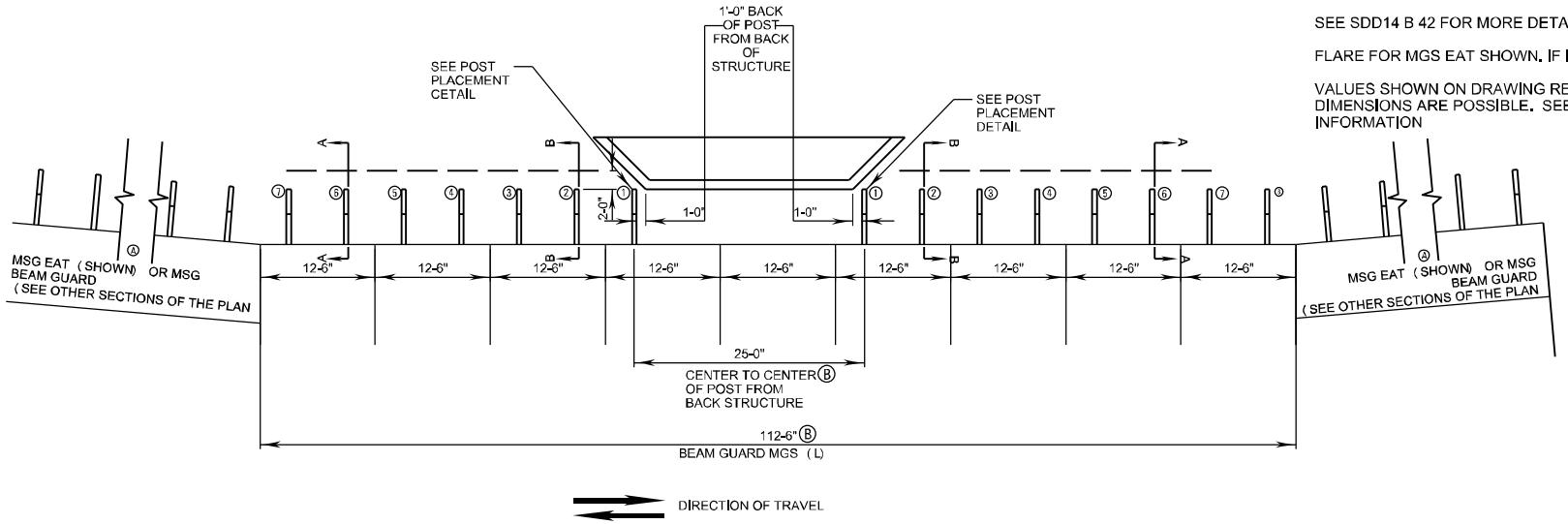
GENERAL NOTES

POSTS 1 THROUGH 3 ARE CRT POSTS.
ALL OTHER POSTS SHALL BE WOOD OR STEEL.

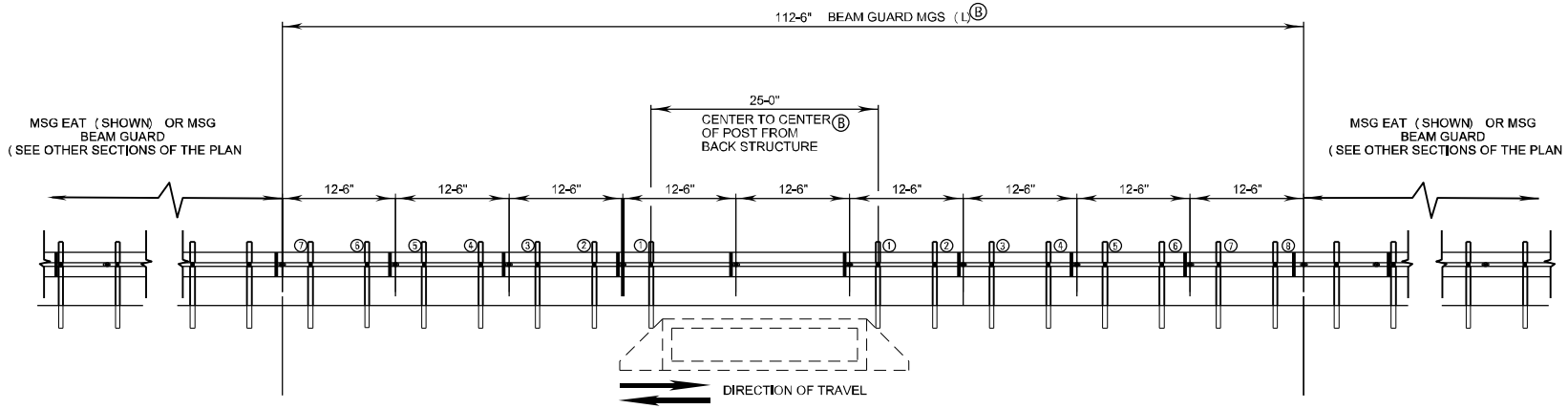
SEE SDD14 B 42 FOR MORE DETAILS

FLARE FOR MGS EAT SHOWN. IF INSTALLING MGS NO FLARE NEEDED.

VALUES SHOWN ON DRAWING REPRESENT THE MAXIMUM LENGTH. SHORTER DIMENSIONS ARE POSSIBLE. SEE OTHER SECTION OF PLAN FOR MORE INFORMATION



PLAN VIEW

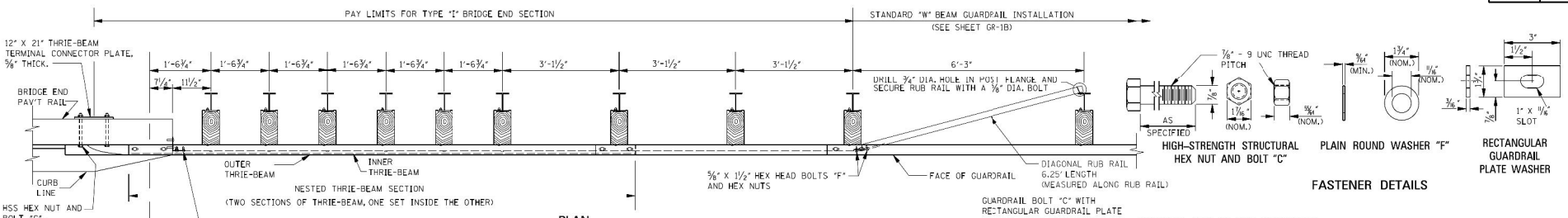


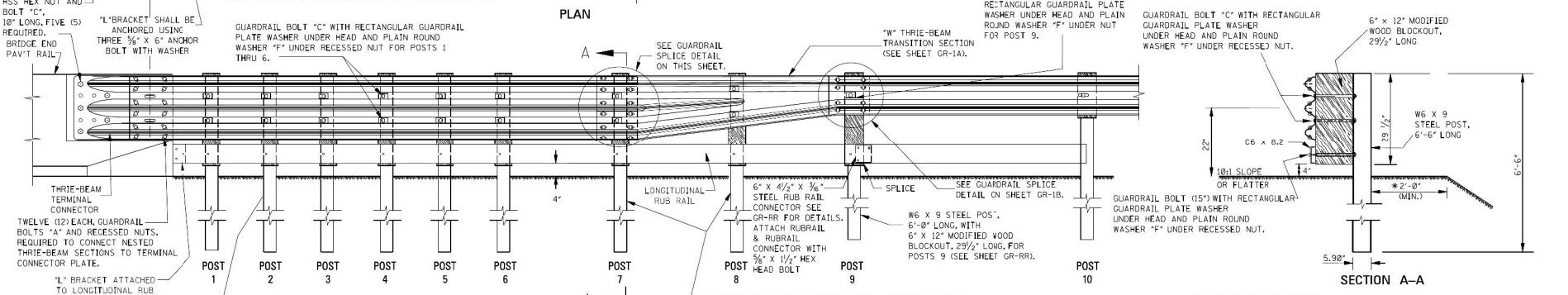
ELEVATION VIEW

GUARDRAIL SYSTEM LONG SPAN MGS (L) TWO-WAY TRAFFIC

NEWTON COUNTY
MP-5015-51(021) 307480/301000

STATE	PROJECT NO.
MISS.	



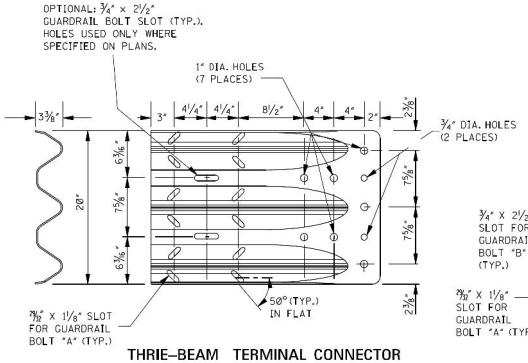


GENERAL NOTES:

1. THIS GUARDRAIL TRANSITION IS APPROPRIATE FOR CONNECTION TJ A GUARDRAIL ANCHOR ASSEMBLY CAST INTO A VERTICAL CONCRETE SHAPE, AS SHOWN ON SHEETS BER-1 & BER-2. THIS GUARDRAIL TRANSITION SHALL NOT BE MOUNTED DIRECTLY TO A CONCRETE SAFETY SHAPE.
2. SEE RAILING DETAILS IN BRIDGE DRAWINGS FOR OTHER DETAILS.

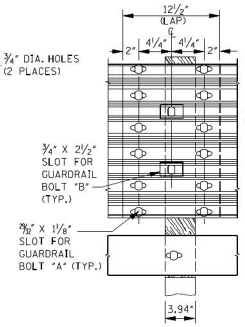
GENERAL NOTES (CONTINUED):

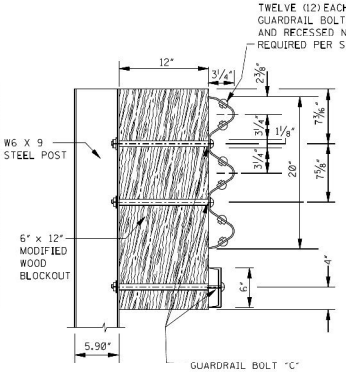
3. GUARDRAIL SHALL MEET THE REQUIREMENTS OF AASHTO M 180, CLASS A, TYPE 1 UNLESS OTHERWISE DESIGNATED.
4. THE TYPE "I" TRANSITION IS USED ON BOTH LEFT AND RIGHT SIDES OF EACH BRIDGE APPROACH WITH 2-WAY TRAFFIC AND THE GUARDRAIL SECTIONS SHALL BE LAPPED IN THE DIRECTION OF APPROACHING TRAFFIC.
5. POSTS SHALL CONFORM TO AASHTO M 270/M 270 (ASTM A 709/A 709M) GRADE 250 STEEL UNLESS CORROSION RESISTANT STEEL IS REQUIRED IN WHICH CASE GRADE 50W STEEL SHALL BE USED. THE STRUCTURAL W6 X 9 DIMENSIONS ARE DEFINED IN AASHTO M 160M (ASTM A 6M). THE SECTION SHOULD BE ZINC-COATED PER AASHTO M 111 (ASTM A 123) AFTER CUTTING, DRILLING OR PUNCHING. CORROSION RESISTANT STEEL SHOULD NOT BE ZINC COATED, PAINTED OR OTHERWISE TREATED. GUARDRAIL IS EXEMPT FROM THE FRACTURE-CRITICAL TOUGHNESS IN AASHTO M 270/A 270 M.
6. FOR FASTENER DETAILS NOT FOUND ON THIS SHEET, SEE SHEET GR-1B.
7. DETAILS PERTINENT TO THE STANDARD INSTALLATION OF "W" AND THRIE-BEAM SECTIONS NOT SPECIFICALLY MODIFIED ON THIS SHEET WILL BE FOUND ON SHEETS GR-1B AND GR-1C, RESPECTIVELY.
8. FOR OTHER DETAILS OF POSTS, POST ACCESSORIES, FASTENERS & RAIL ELEMENTS SEE AASHTO-AGC-ARTBA JOINT TASK FORCE NO. 13 GUIDE TITLED "A GUIDE TO STANDARDIZED HIGHWAY BARRIER HARDWARE", LATEST EDITION.
9. ALL WOOD BLOCKOUTS SHALL BE TREATED TIMBER IN ACCORDANCE WITH MISSISSIPPI DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS..
10. FOR DETAIL OF NOTCH IN MODIFIED WOOD BLOCKOUTS, SEE SHEET GR-1B.

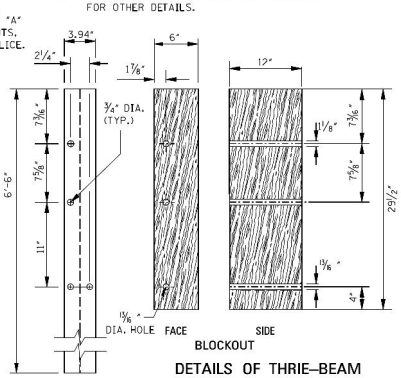


NOTES:

1. THE THRIE-BEAM TERMINAL CONNECTOR SHALL BE AASHTO M 180 CORRUGATED SHEET STEEL, CLASS B, TYPE I.
2. ALTERNATIVELY, THE SPLICE SLOTS CAN BE ORIENTED PARALLEL TO THE LONGITUDINAL AXIS OF THE TERMINAL CONNECTOR. HOWEVER, THE 50° SLOT VERSION IS EASIER TO INSTALL WHERE SEVERAL GUARDRAIL SECTIONS ARE NESTED TOGETHER.







NOTES:

1. HOLE DETAILS ARE REQUIRED FOR POSTS AND BLOCKOUTS 1 THRU 8.
2. STEEL POSTS ARE FABRICATED FROM W6 X 9 STRUCTURAL STEEL SHAPES.
3. ALL HOLES IN BOTH POSTS AND BLOCKOUTS ARE 3/4" IN DIAMETER.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

GUARDRAIL:
BRIDGE END SECTION
TYPE "I"
(STEEL POSTS)
(NEW CONSTRUCTION)

DATE	REVISION	BY

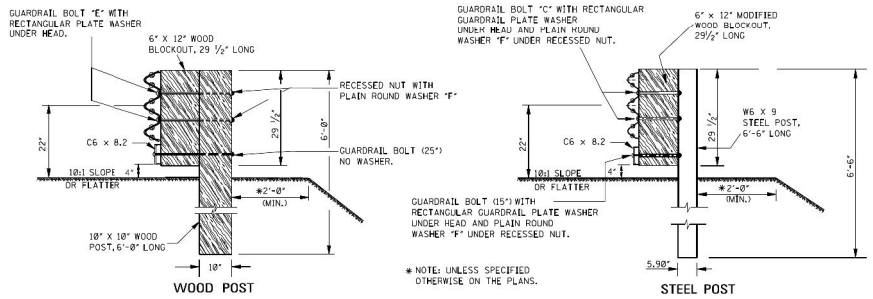
ISSUE DATE: AUGUST 01, 2017

WORKING NUMBER: GR-2G

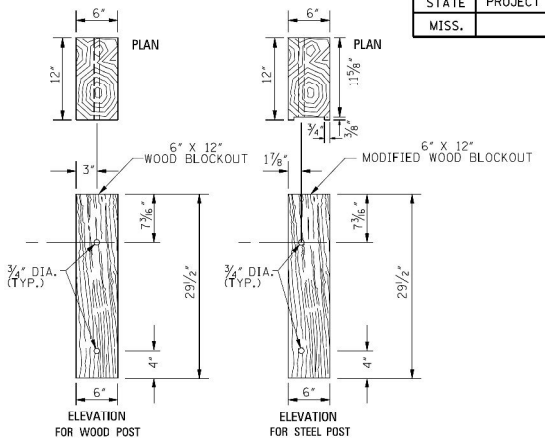
SHEET NUMBER: 6211

NEWTON COUNTY
MP-5015-51(021) 307480/301000

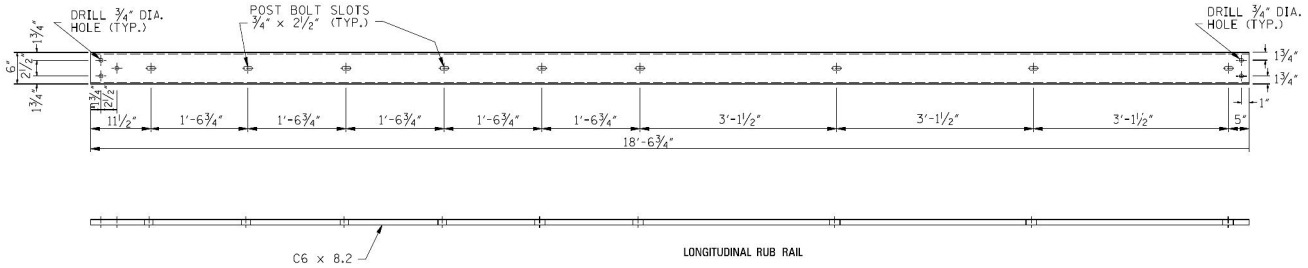
STATE	PROJECT NO.
MISS.	



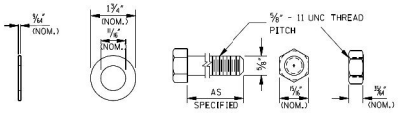
PROFILE VIEW OF POSTS

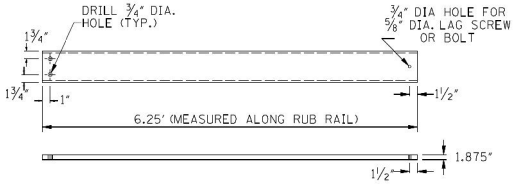


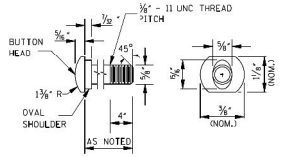
BLOCKOUT FOR POST 9 OF TYPE "I" BRIDGE END SECTION

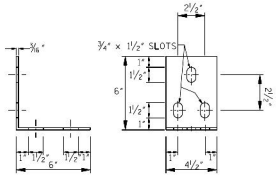


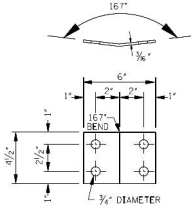
CROSS–SECTION VIEW OF C6 x 8.2











GENERAL NOTES:

1. POSTS 1 THROUGH 9 REQUIRE AN ADDITIONAL HOLE TO ATTACH LOWER BLOCKOUTS. STEEL POST REQUIRE 2 HOLES ON EITHER SIDE OF THE POST.
2. RUBRAIL BLOCKOUTS SHALL BE SECURED WITH ⅝" BUTTONHEAD BOLT FOR STEEL POST AND 20" BOLT FOR WOOD POST. (SEE PROFILE VIEW FOR LENGTHS)
3. RUBRAIL & LONG BLOCKOUT (POST 9) ARE NOT REQUIRED IN CURB & GUTTER SECTIONS OF ROAD.
4. ATTACH RUBRAIL CONNECTOR PLATE AND RUBRAIL WITH ⅝" X 1½" HEX HEAD BOLTS "F"

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

GUARDRAIL: RUB RAIL HARDWARE

DATE	REVISION	BY

ISSUE DATE: AUGUST 01, 2017

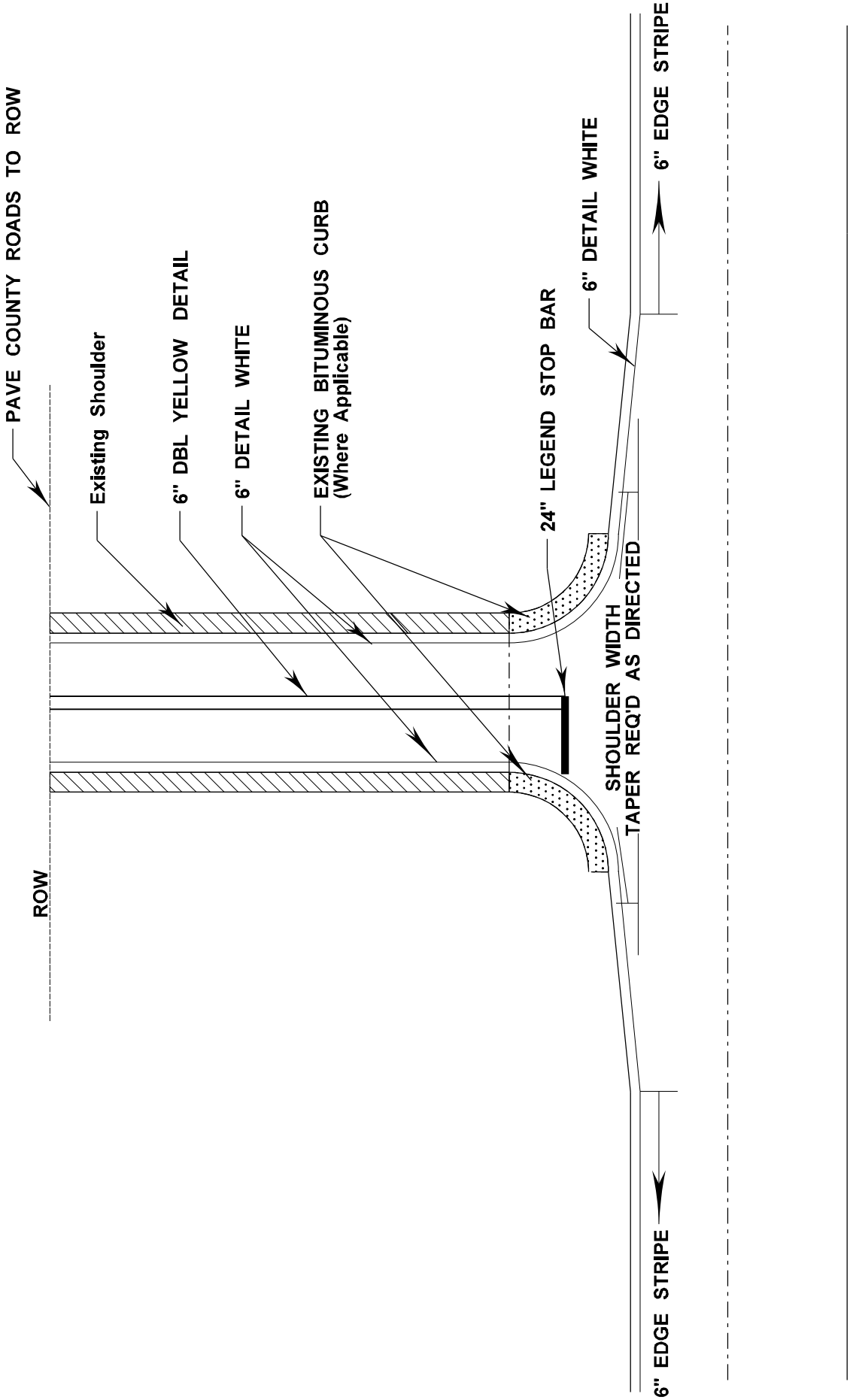
WORKING NUMBER GR-RR

SHEET NUMBER 6218

Notice to Bidders No. 3209 -- Cont'd

NEWTON COUNTY
MP-5015-51(021) 307480/301000

STRIPE DETAIL - COUNTY ROADS



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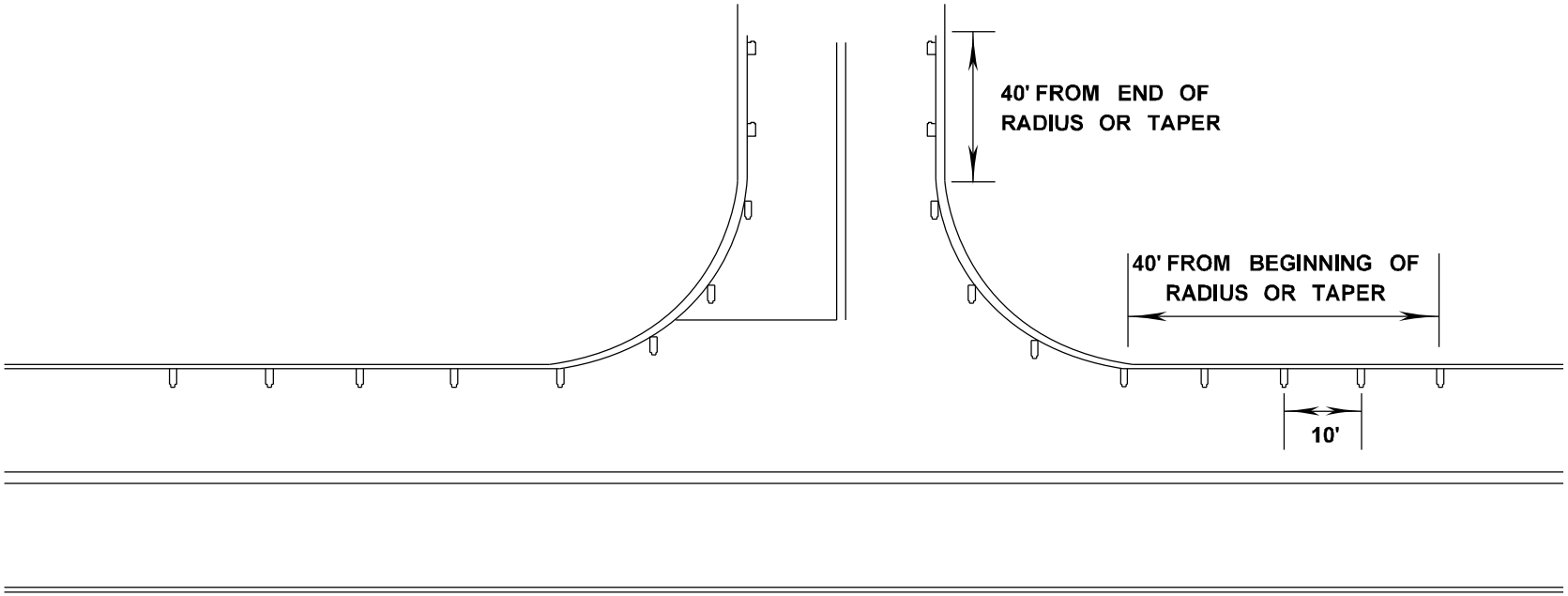
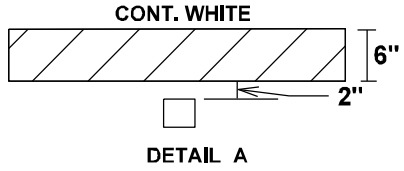
TYPICAL FOR RAISED PAVEMENT MARKERS
PLACED ON SIDE ROAD RADIUS





NOTE 1. MARKERS SHALL BE PLACED EVERY 10 FEET.

NOTE 2. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.

NOTE 3. MARKERS SHALL BE HIGH PERFORMANCE TWO WAY CLEAR.

NOTE 4. FIVE (5) MARKERS SHALL BE PLACED ALONG MAINLINE EDGE STRIPE.

NOTE 5. MARKERS FOR COUNTY ROADS SHALL CONTINUE DOWN THE EDGE STRIPE A DISTANCE OF 40 FEET.

Mill & Overlay approximately 1 mile on SR 18 from US 80 to the Terrapin Skin Creek Bridge, known as State Project No. SP-0039-02(055) / 108657301 in Rankin County.

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
			Roadway Items		
0010	202-B007		240	Square Yard	Removal of Asphalt Pavement, All Depths
0020	202-B158		1,038	Linear Feet	Removal of Guard Rail, Including Rails, Posts and Terminal Ends
0030	202-B240		1,456	Linear Feet	Removal of Traffic Stripe
0040	203-G002	(E)	190	Cubic Yard	Excess Excavation, LVM, AH
0042	209-A005		325	Square Yard	Geotextile Stabilization, Type V, Non-Woven
0050	304-D002	(GT)	550	Ton	Granular Material, Crushed Stone
0060	403-A001	(BA1)	6,639	Ton	12.5-mm, HT, Asphalt Pavement
0070	403-B001	(BA1)	120	Ton	12.5-mm, HT, Asphalt Pavement, Leveling
0080	406-D001		55,705	Square Yard	Fine Milling of Bituminous Pavement, All Depths
0090	407-A001	(A2)	5,570	Gallon	Asphalt for Tack Coat
0100	503-C010		250	Linear Feet	Saw Cut, Full Depth
0110	606-B003		563	Linear Feet	Guard Rail, Class A, Type 1, 'W' Beam, Metal Post
0120	606-D017		4	Each	Guard Rail, Bridge End Section, Type G
0130	606-D019		4	Each	Guard Rail, Bridge End Section, Type H
0140	606-E005		4	Each	Guard Rail, Terminal End Section, Flared
0150	606-E007		4	Each	Guard Rail, Terminal End Section, Non-Flared
0160	618-A001		1	Lump Sum	Maintenance of Traffic
0170	619-A1001		4	Mile	Temporary Traffic Stripe, Continuous White
0180	619-A2001		5	Mile	Temporary Traffic Stripe, Continuous Yellow
0190	619-A3001		5	Mile	Temporary Traffic Stripe, Skip White
0200	619-A5001		49,000	Linear Feet	Temporary Traffic Stripe, Detail
0210	619-A6002		6,300	Linear Feet	Temporary Traffic Stripe, Legend
0220	619-D1001		24	Square Feet	Standard Roadside Construction Signs, Less than 10 Square Feet
0230	619-D2001		218	Square Feet	Standard Roadside Construction Signs, 10 Square Feet or More
0240	619-F3001		31	Each	Delineators, Guard Rail, White
0250	619-F3002		14	Each	Delineators, Guard Rail, Yellow
0260	619-G4001		12	Linear Feet	Barricades, Type III, Double Faced
0270	619-G4005		12	Linear Feet	Barricades, Type III, Single Faced
0280	620-A001		1	Lump Sum	Mobilization
0290	626-A001		2	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip White
0300	626-B002		2	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous White
0310	626-E001		2	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
0320	626-G004		18,700	Linear Feet	Thermoplastic Double Drop Detail Stripe, White
0330	626-G005		6,000	Linear Feet	Thermoplastic Double Drop Detail Stripe, Yellow
0340	626-H001		1,800	Square Feet	Thermoplastic Double Drop Legend, White
0350	626-H002		3,200	Linear Feet	Thermoplastic Double Drop Legend, White
0360	627-K001		600	Each	Red-Clear Reflective High Performance Raised Markers
0370	627-L001		100	Each	Two-Way Yellow Reflective High Performance Raised Markers
0380	630-A001		13	Square Feet	Standard Roadside Signs, Sheet Aluminum, 0.080" Thickness
0390	630-A003		22	Square Feet	Standard Roadside Signs, Sheet Aluminum, 0.125" Thickness
0400	630-C003		66	Linear Feet	Steel U-Section Posts, 3.0 lb/ft
0410	630-G004		8	Each	Type 3 Object Markers, OM-3R or OM-3L
0420	907-619-B001		66	Linear Feet	Temporary Portable Rumble Strips
0430	907-632-D001		6	Each	Solid State Traffic Actuated Controller, Type 1
0440	907-640-A001		456	Linear Feet	Vehicle Loop Assemblies
0450	907-641-A002		18	Each	Signal Stop Bar Radar Vehicle Detection Sensor, Type 2
0460	907-641-D001		2,950	Linear Feet	Radar Vehicle Detection Cable
0470	907-899-A001		1	Lump Sum	Railway-Highway Provisions