

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u> 1 </u>	DATED <u> 6/17/2021 </u>	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. _____	DATED _____	ADDENDUM NO. _____	DATED _____
ADDENDUM NO. _____	DATED _____	ADDENDUM NO. _____	DATED _____

Number	Description
1	Revised NTB No. 3375; Amendment EBSx Download Required.

TOTAL ADDENDA: 1
 (Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

 Contractor

BY _____
 Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

President	Address
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Secretary	Address
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Treasurer	Address
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The following is my (our) itemized proposal.

NH-0056-01(110)/ 108604301000

Rankin County(ies)

Revised 01/26/2016

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 3375

CODE: (SP)

DATE: 05/06/2021

SUBJECT: Scope of Work

PROJECT: NH-0056-01(110) / 108604301 -- Rankin County

The contract documents do not include an official set of construction plans but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, "Standard Drawings".

A general description of the work required on the project is to mill and overlay approximately 10 miles of existing asphalt pavement on SR 25 in Rankin County beginning at the north end of the bridge over SR 471 (BOP Station 657+01) and ending at the pavement joint 2,027 feet north of the bridge over SR 43 (EOP Station 1190+93), and mill and overlay approximately 1 miles of existing asphalt pavement on Sandhill Road beginning on the east end of 4-lane section (BOP Station 0+00) and ending on the west end of 4-lane section (EOP Station 55+14). Details of specific work are mentioned in the following sections.

Project wide work on this project is from Station 657+01 (BOP) to Station 1190+93 (EOP) on SR 25, and Station 0+00 (BOP) to Station 55+14 (EOP) on Sandhill Road.

Prior to beginning the milling and overlay operations, any failed areas in the existing pavement shall be removed full depth and repaired full depth using 12.5-mm, HT, Leveling asphalt. Other repairs may be necessary as field conditions require and as directed by the Engineer. After failures have been repaired, some corrective leveling and milling efforts will be required to correct the slope throughout the project. After corrective measures are complete, the top 2" of existing asphalt on both Mainline lanes shall be milled and overlaid using 2" of 12.5-mm, HT, asphalt. Where the cross slope is not equal to 2%, the thickness of the overlay/milling efforts shall be adjusted to correct the cross slope.

General Notes: These general notes are applicable to all sites.

Milling

Milling/paving will not begin until an **approved** asphalt mix design has been received, nor until such time that, in the opinion of the Engineer, weather conditions have been consistently suitable enough to allow placement of the asphalt pavement after the milling operations.

The reclaimed asphalt pavement (RAP) material removed by the milling operation shall become the property of the Contractor.

Where milling is required, the Contractor shall provide outlets in the existing shoulders at sufficient intervals to prevent pooling or standing water on the milled surface, the cost of which shall be absorbed in other items bid.

Milling and paving operations shall be performed such that a -2% slope from centerline is provided in normal crown roadway sections. Superelevation through curves shall be maintained as it currently exists or improved as directed. Where slope correction is required correction will be made by milling, paving, or combination thereof as directed by the Engineer.

Milling of driveway pads shall be conducted in a manner to prevent gouging or otherwise affecting the roadway pavement structure and slope. Milling of driveway pads shall not be done in simultaneous path with main line milling.

Traffic will be allowed to run on the milled surface for a maximum of five (5) days. Any surface not covered before the allowable time will result in a fine for any full or partial day exceeding five (5) days. Fine milling shall be performed in accordance with the attached drawings. This work shall be applied on all mainline tie-ins, driveway pads, county roads, and etc. Traffic will be allowed to run on all milled tie-ins not exceeding five (5) days.

Temporary pavement joints (paper joints) shall be at least three (3) paper-widths long shall be used at all milled tie-ins and shall be adequately maintained.

Paving

Prior to beginning the milling and overlay operations, any failed areas in the existing pavement shall be removed full depth (9½" and variable) and repaired with 12.5-mm, HT, Leveling asphalt. Payment for removal of failed areas shall be made under pay item 202-B: Removal of Asphalt Pavement, Failed Areas. Payment for saw cutting of failed areas shall be paid under pay item 503-C: Saw Cut, Full Depth. Milling full depth shall also be an acceptable means of removing failed areas, as directed by the Engineer, should the Contractor elect not to saw cut, thus eliminating payment under the saw cut pay item. Milling for removal of failed areas shall be paid under pay item 202-B: Removal of Asphalt Pavement, Failed Areas. Other repairs may be necessary as field conditions require and as directed by the Engineer.

If traditional excavation methods are used, the removal area shall first be saw cut full depth including concrete, where applicable, to create a neat line and prevent damage to the adjacent pavement structure. Payment for saw cuts will be made using the appropriate items. If milling techniques are used, the area will not require saw cuts but care should be exercised to create a neat removal line and to prevent damaged to the adjacent pavement structure. If saw cuts are used in conjunction with milling, payment will be made using the appropriate pay items. Payment will not be made for saw cuts that are not performed.

Grade and profile distortions due to high volume change (HVC) soils will be corrected using 12.5-mm, HT, Leveling asphalt in the locations listed in the attached tables. Most locations will require multiple lifts to obtain the desired grade. Traffic will not be allowed to cross any lane line drop-offs exceeding 2¼" per sheet TCP-14. Additionally, both lanes shall be brought up evenly the same day to prevent possible ponding. Per Sheet TCP-16, traffic is allowed to run

near a 3-inch drop-off on the shoulder during the daylight hours if drums and low shoulder signs are placed accordingly. All shoulders shall be pulled up daily prior to removing the lane closures and opening both lanes to traffic. If milling is used for slope or profile correction, joint differentials shall conform to the same provisions.

Mill and overlay NE and NW ramps at SR 471 and all ramps at Sandhill Road and SR 43 Interchanges.

Publicly maintained roads and streets should be milled and paved to the existing right-of-way. Privately owned entrances shall be paved to the shoulder line as per the included typical drawing. Pads shall be shaped horizontally and vertically to prevent excessive drop-offs. All residential pads exceeding a 2-inch drop off from the edge of pavement to the pad shall be corrected before the end of the day using paper joints to minimize damage to vehicles.

Granular Shoulder Material

Where applicable, the existing shoulders shall be raised to match the new pavement elevation by placing variable depth granular material on the existing shoulders. Placement of the granular material on the finished asphalt course shall not be permitted. The material shall be bladed, rolled, and compacted to a finished slope of four percent (4%). Placement of this material shall be performed to provide a uniform and compacted shoulder with a minimum depth and width of material placed. Shoulders with adequate shoulder material in place shall be bladed to a slope of four percent (4%). Shoulder blading will not be measured for payment and all costs shall be included in the cost of other items bid.

The Contractor shall on a daily basis, pull shoulder material up to edge of asphalt to maintain a 2-inch or less drop off. Granular material (Class 5 Group C) shall be provided around residential pads to prevent shoulder drop-offs as directed and shall be placed in a timely manner. Drop-offs exceeding 2½" shall be corrected within two (2) calendar days of placement of pad. Stabilizer aggregate shall be used as directed by the Engineer.

Removal of the existing shoulder material shall be coincident with the milling/overlaying operation to prevent the possible ponding of water. No payment will be made for blading or removal of the existing shoulder material. Any material excavated from the existing shoulder shall be used to raise the existing shoulder to match the new pavement elevation and any surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer. The cost associated with surplus material shall be absorbed in other items bid. Material which cannot be placed in adjacent areas and deemed to be excess excavation by the Engineer shall be removed under pay item 203-G: Excess Excavation.

Temporary and Permanent Pavement Markings

Temporary traffic stripe shall be required immediately after the required overlay/milling and prior to opening area to traffic. Temporary stripe shall be placed in the same location and configuration as the permanent stripe.

If temporary stripe is offset, the Contractor shall conduct operations in a manner to insure the final temporary stripe is placed at the required location of the permanent stripe. If removal of

temporary offset stripe is required in order to achieve the correct location and alignment of permanent stripe, the cost of removal will be absorbed in other items bid. Placing double temporary centerline will not be allowed.

Temporary striping shall conform to finished stripe specifications for alignment, neatness, and straightness.

All permanent striping will be thermoplastic double-drop. Edge lines shall be placed to accommodate the lane widths shown on the applicable typical sections unless prevented by field conditions. Thermoplastic pavement marking thickness shall be a minimum of 90 mils. for center lines, edge lines, lane lines, gore areas, turnouts, and County Roads. All other thermoplastic pavement markings shall be a minimum of 120 mils.

The use of short strips of traffic tape will not be allowed unless approved by the Engineer.

Permanent pavement markers shall be placed in accordance with the attached drawings and Standard Drawings. Two-way yellow markers shall be placed on two-way roads. Two-way clear markers shall be placed on County Roads as shown on attached drawings.

Rumble stripe shall be placed throughout the project according to standard specifications and per attached drawing. Payment for rumble stripe shall be made under pay item 423-A: Rumble Stripe, Ground in.

The face of all existing undisturbed curbs shall be painted with at least two (2) coats of white traffic paint with glass beads being required in the top coat. The cost associated with the painting of new or existing curb is to be included in other items bid.

Guardrail

Guard rail pads and shoulders shall be paved with 12.5-mm, HT, asphalt prior to placement of the new guard rail. Guardrail pads shall extend two feet (2') behind the guardrail post at all existing guardrail locations maintaining guardrail height requirements. Prior to placement of the guardrails and asphalt, 3" and variable depth of existing shoulder material shall be removed. Any excess material excavated from the existing shoulder shall be used to raise the existing shoulder to match the new pavement elevation and shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer. The cost associated with excess material shall be considered an absorbed item.

The existing guard rail and terminal end sections shall be removed and replaced as directed. The new guard rail shall be placed in the same location as the existing railing and height shall meet the approved departmental standards (currently 25" to center). All guard rail removed shall be delivered to Carthage Maintenance at no additional cost to the State. A 24-hour notice shall be given before delivery. Any removed metal post, hardware, concrete anchors, and wooden posts shall be disposed of by the Contractor at no additional cost to the State. All voids left by post shall be filled and compacted as directed by the Engineer prior to placing the new asphalt pad. Payment for the removal and replacement of guard rail and terminal end sections shall be made under the appropriate pay items for guard rail and terminal ends.

Guardrail lengths are based on terminal end length of 37.5'. If terminal of length other than this is used, an adjustment in w-beam length is required.

Delineators will be required on all guardrails within the project. Existing guardrail delineators shall be removed and replaced. The cost of delineator removal shall be included in the price of other items bid.

Permanent Signs

Permanent signs as listed on the attached tables shall be replaced. Unless otherwise listed in the attached tables, existing posts, anchors, angles/bars, and other components shall be reused. The Contractor shall use new bolts, screws, washers, nuts, etc. of the required sizes in the installation of signs. If required as part of the sign replacement activities, all post, pipe, and I-beam lengths in these plans are estimated. Post lengths for all signs shall be verified in the field by the Contractor prior to fabrication. Installation dates shall be clearly written in bold black markings on the back bottom half off all signs with a permanent marking stick that is waterproof, fade resistant, and marks on wet or dry surfaces. If existing sign posts or footings are to be replaced, the existing posts and footings are to be removed and the area backfilled and compacted in accordance with Section 203 of the Standard Specifications. Removal of sign, post, and footing and backfilling will be paid using the removal of sign pay item.

Object markers at bridge approaches and other locations shall be replaced as shown in the attached table. Cost of removal of object markers shall be included in the cost of other items bid.

Delineators shall be required at all crossovers. Existing delineators shall be removed and replaced under pay item 630-F: Delineators, Flexible Post Mounted, Crossover, Type I Yellow. All labor, equipment, and materials associated with removal of existing delineators shall be absorbed into other items bid.

Traffic Signals

Vehicle loop detectors at Holly Bush Road intersection shall be replaced with radar detection sensors, radar units shall be installed per manufacturer's recommendations. Existing EPAC Controllers shall be replaced with new controllers and existing EPAC controllers shall be salvaged and delivered to MDOT Signal Shop (601-359-1493). It is the responsibility of the Contractor to coordinate delivery of existing EPAC controllers with MDOT personnel to MDOT signal shop. The Contractor shall also be responsible for transferring existing controller data to the new controllers. Contractor may remove existing detection loop cable, if necessary. Cable quantities may be adjusted based on radar locations per manufacturer recommendations. Removal of vehicle loop detection cable shall be absorbed in other bid items.

Traffic Control

The Contractor shall erect and maintain construction signing and provide all signs and traffic control devices necessary to safely maintain traffic around and through the work areas in accordance with the Traffic Control Plan and the MUTCD. The cost shall be included in the price bid for pay item 618-A: Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except those designated in the plans to be black

legend and border on white background. Standard roadside construction signs and barricades will be paid for using the appropriate pay items.

Roadside construction signs, barrels, etc. shall be placed in accordance with the attached drawings or as directed by the Engineer. W20-1 signs shall be placed on all public road approaches as shown or as directed.

The Contractor shall on a daily basis, remove all debris from within the roadway and a 30-foot clear zone which, in the opinion of the Engineer, is a hazard to the traveling public. This activity shall begin with the beginning of work or the beginning of the contract time, whichever comes first. No direct payment will be made for the debris removal; the cost shall be included in the prices of other items bid. Failure of the Contractor to remove the debris as prescribed herein shall be just cause for withholding the monthly progress estimate payment or suspending active operations until the debris is satisfactorily removed by the Contractor.

Potholes that may exist shall be patched in a timely manner from the date of Notice to Proceed until the date of the Final Maintenance Release. Cracks of significant depth or depressions in the existing surface which, in the opinion of the Engineer, may cause reflection cracking shall be filled with asphalt pavement immediately prior to overlay operations. Costs of patching of potholes shall be included in the cost of Maintenance of Traffic.

Miscellaneous Notes

It shall be the responsibility of the Contractor to protect existing structures such as pipes, inlets, aprons, bridges, etc. from damage which might occur during construction. The Contractor shall replace or repair, as directed by the Engineer, any structures damaged by the Contractor during the life of the contract. No payment will be made for replacement or repair of damaged items.

Any signs, mailboxes, etc. that are in conflict with construction of this project shall be removed and relocated by the Contractor as directed by the Engineer. Any costs accrued by these conflicts shall be absorbed in other items bid.

Incidental work such as removing vegetation, shaping and compacting shoulders, removing and resetting signs and/or mailboxes, removing excess asphalt material, project clean-up, and other items of incidental work necessary to complete the project will not be measured for separate payment. Cost for incidental work will be absorbed in the price of other items bid.

Existing raised pavement markers shall be removed prior to beginning the overlay operation. All costs associated with removing the existing pavement markers shall be included in the price of other items bid.

Prior to the final inspection, bridges, islands, and areas with curb shall be swept/cleaned. Care should be taken to prevent milled asphalt, asphalt debris, vegetative/granular debris, etc. from entering drainage structures or clogging other drainage ways. Disposal of material will not be measured for separate payments.

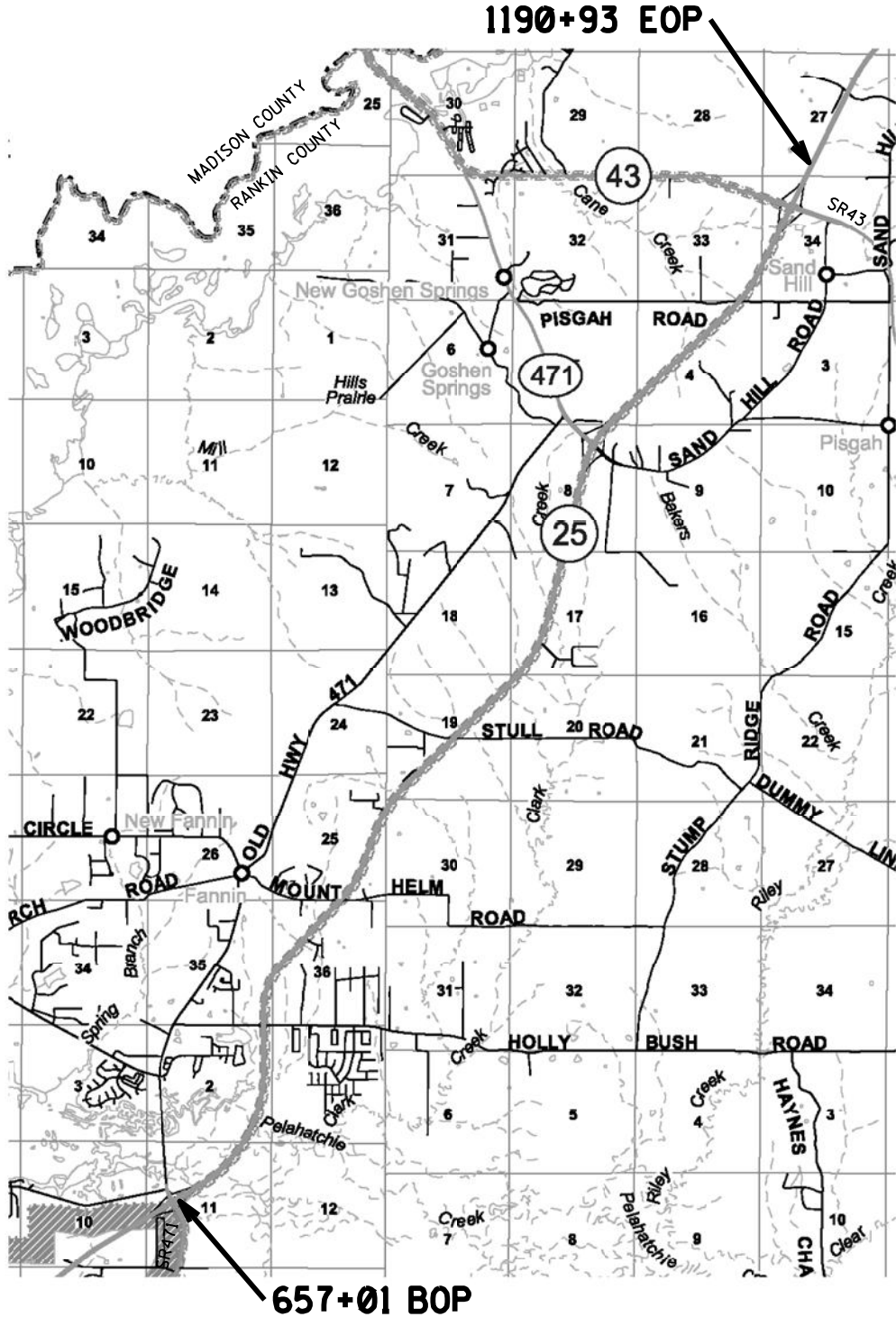
Grassing will be required at edge drain outlets and around pads. Grassing areas shall be paid under pay item 225-A: Grassing.

All existing joints on Bridges 22.2A, 22.2B, 13.3A, and 13.3 shall be removed and replaced with preformed joint seal. Appropriate bid items necessary to complete construction of new joints have been provided, except the removal of existing joints material which will be absorbed in pay item 907-808-A: Joint Repair. All material, labor, and equipment required for epoxy mortar mix shall be included in 907-808-A: Joint Repair.

Edge drain work shall be required in various locations on the project. Specific locations are listed in the tables provided. All work shall be done according to specifications and paid for using the appropriate pay items provided in the contract.

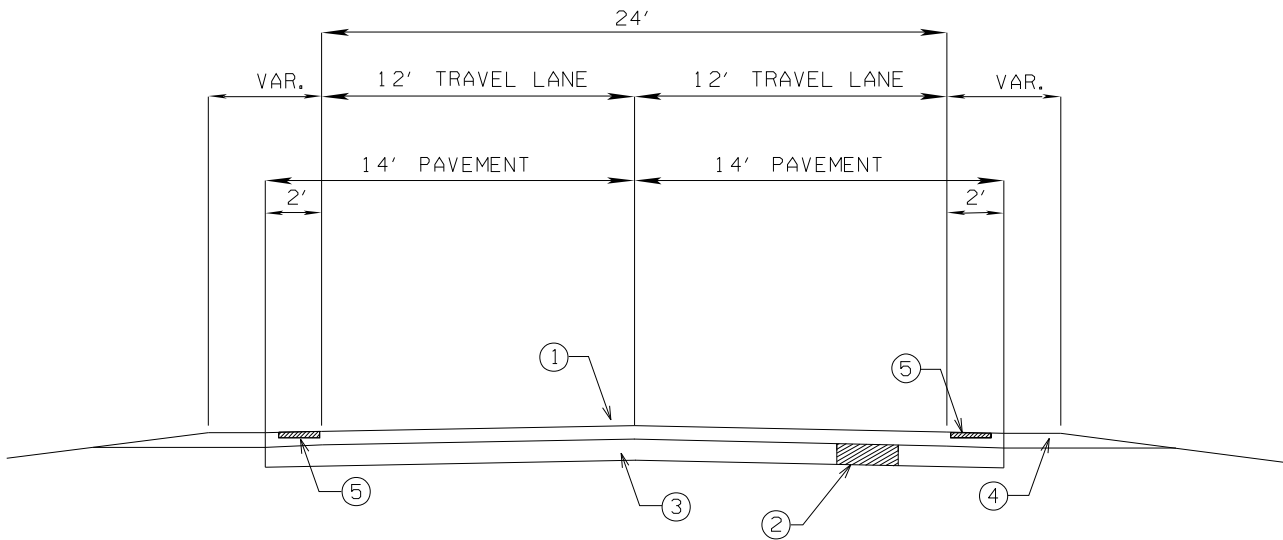
Existing guard post shall be removed and replaced according to specifications. Payment for all work, materials, and labor shall be paid for using the appropriate 606 pay items.

SR25 OVERLAY PROJECT FROM 471 TO SR 43 RANKIN COUNTY 108604/301000



RANKIN COUNTY TYPICAL SECTION SR 25 OVERLAY

BOP 657+01 EOP 1190+93



- ① Mill & Overlay 2" asphalt pavement 12.5mm,HT
- ② Failed areas to be removed and back filled with asphalt pavement 12.5mm,HT Leveling as directed.
- ③ Existing Pavement Structure
- ④ Cl. 5, GRP. C As Required
- ⑤ Rumble Stripe Required
- ⑥ Variable Depth Profile Milling And/Or Leveling Required at Locations as Directed By The Engineer

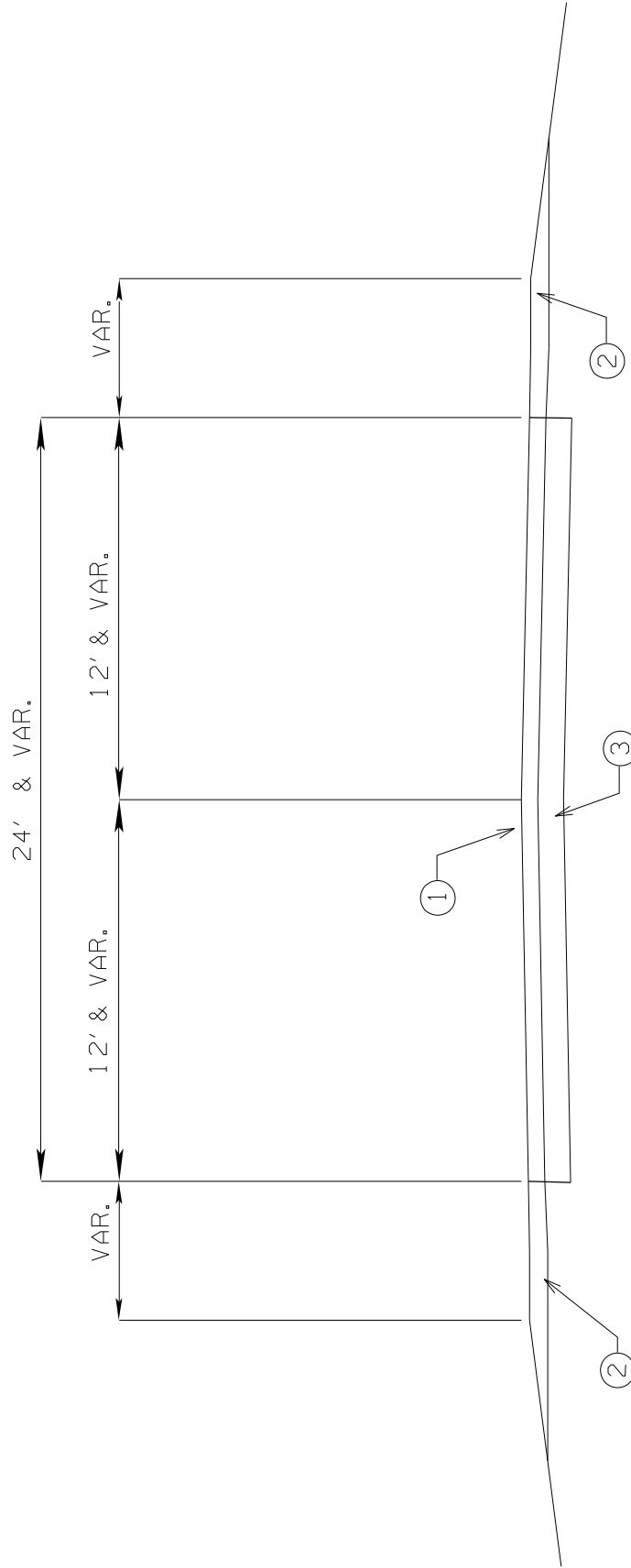
EXISTING PAVEMENT STRUCTURE

NORTHBOUND: 14" & VARIABLE ASPHALT OVER 6" CLAY GRAVEL OR 8" LIME TREATED BASE

SOUTHBOUND: 16" & VARIABLE ASPHALT OVER 6" CALY GRAVEL OR 8" LIME TREATED BASE

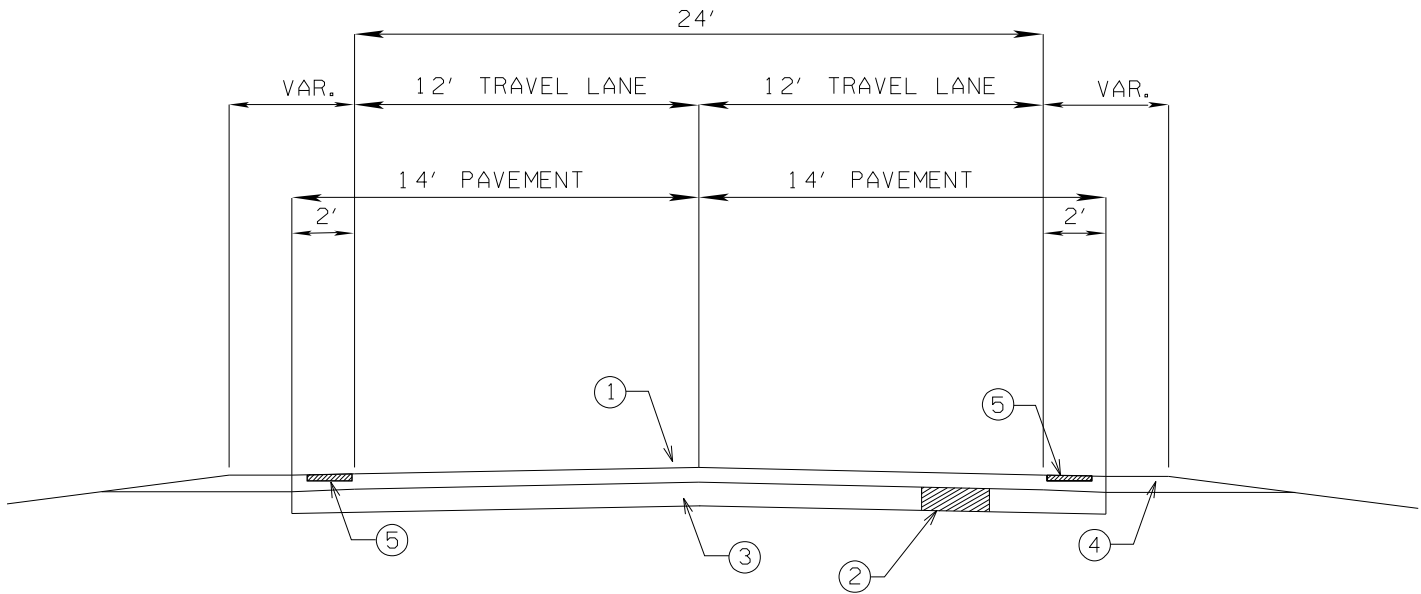
*SOME AREAS MAY CONTAIN 24" & VARIABLE ASPHALT DUE TO PREVIOUSLY PLACE LEVELING LIFTS

108604/301000
 RANKIN COUNTY
 SR 25 OVERLAY
 COUNTY ROADS



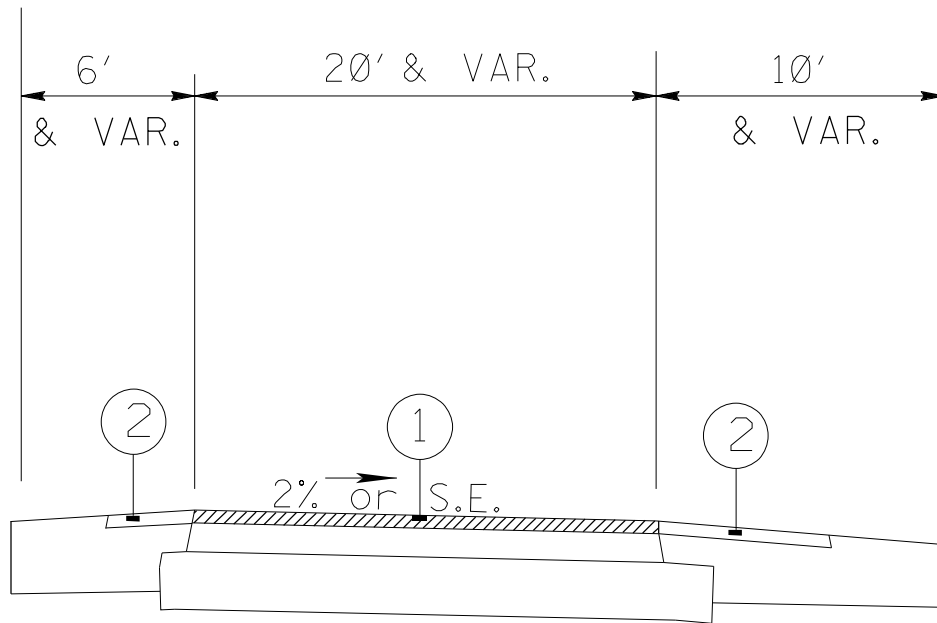
RANKIN COUNTY TYPICAL SECTION SR 25 OVERLAY

BOP 0+00 EOP 55+14
SR 471 (SANDHILL RD)



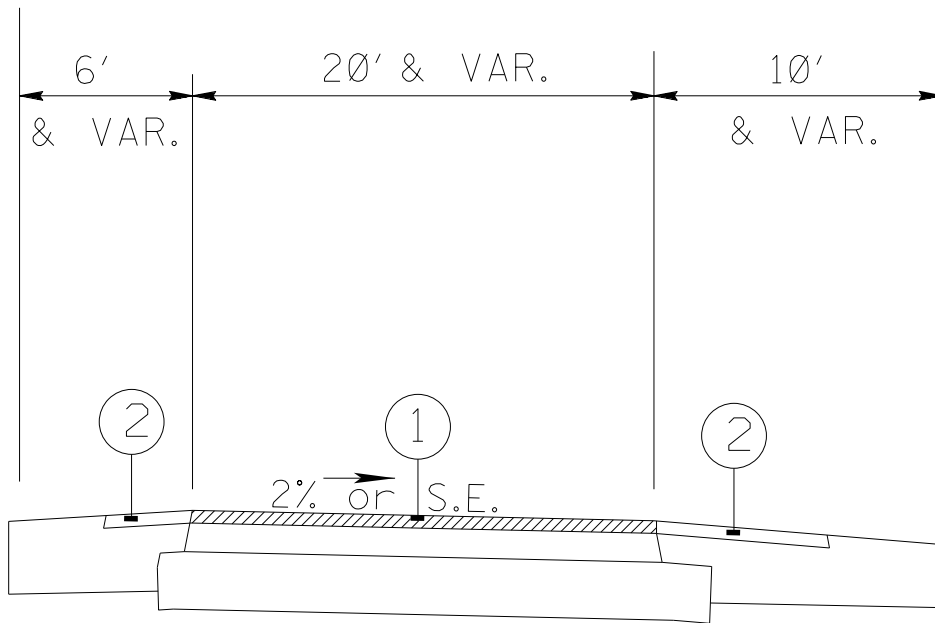
- ① Mill & Overlay 2" asphalt pavement 12.5mm,HT
- ② Failed areas to be removed and back filled with asphalt pavement 12.5mm,HT Leveling as directed.
- ③ Existing Pavement Structure
- ④ C1.5, GRP. C As Required
- ⑤ Rumble Stripe Required

SR 471/SR 25 RAMPS
HWY 25 RANKIN COUNTY
(UNCURBED SECTION)



- ① 2" FINE MILLING AND REPLACE WITH 2" HMA, 12.5mm Mix, HT
- ② VAR. DEPTH GRANULAR MATERIAL, CLASS 5 GROUP C REQUIRED

108604/301000
SR 471/SR 25 RAMPS
SR 25 OVERLAY
RANKIN COUNTY



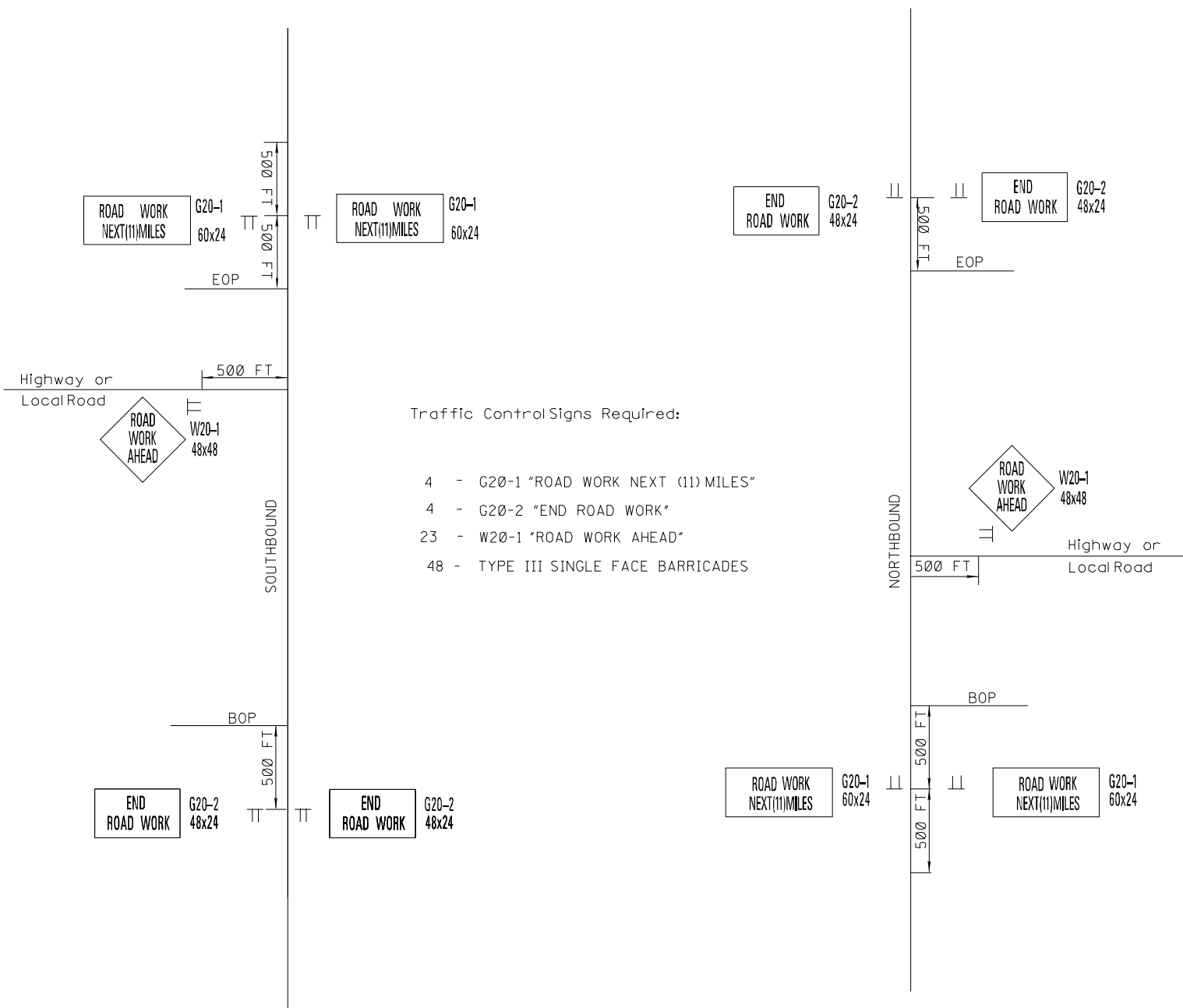
- ① 2" FINE MILLING AND REPLACE WITH 2" HMA, 12.5mm Mix, HT
- ② VAR. DEPTH GRANULAR MATERIAL, CLASS 5 GROUP C REQUIRED

-14- Notice to Bidders No. 3375 -- Cont'd.

CONSTRUCTION SIGNING DETAIL

SR 25 OVERLAY

RANKIN COUNTY



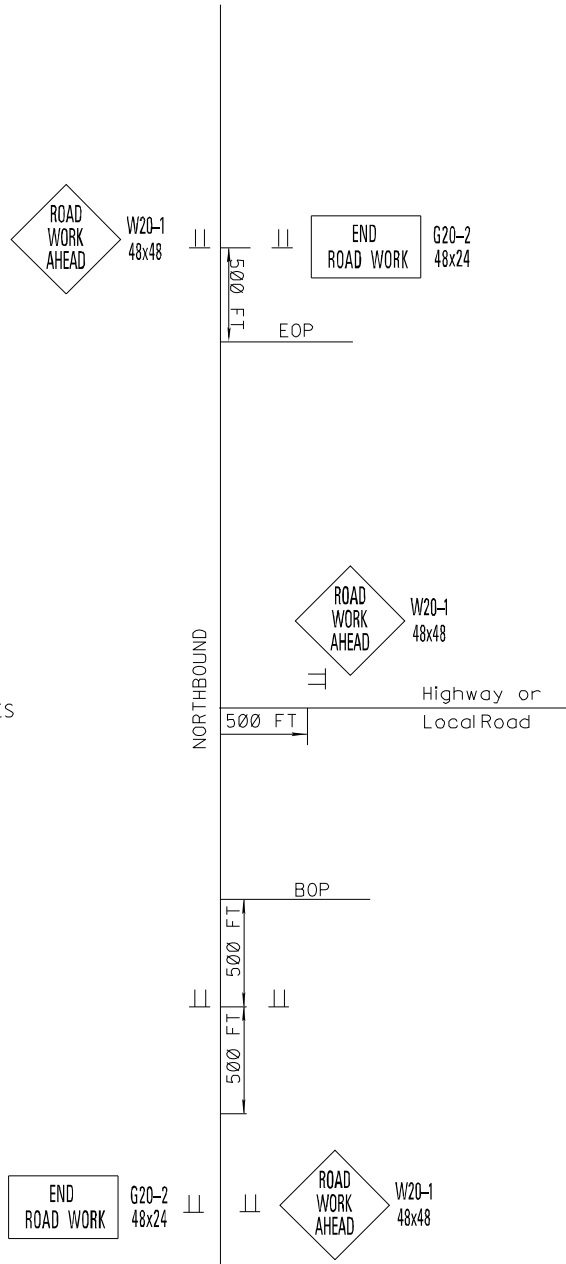
NOTES: One (1) W20-1 "ROAD WORK AHEAD" Sign is Required at each Local Road, Street, Ramps or Highway Entering the Project.

G20-1 and G20-2 signs mounted on Type III Single Faced Barricade.

CONSTRUCTION SIGNING DETAIL
SR 25 OVERLAY
RANKIN COUNTY
SR 471

Traffic Control Signs Required:

- 2 - G20-2 "END ROAD WORK"
- 8 - W20-1 "ROAD WORK AHEAD"
- 24 - TYPE III DOUBLE FACE BARRICADES



NOTES: One (1) W20-1 "ROAD WORK AHEAD" Sign is Required at each Local Road, Street, Ramps or Highway Entering the Project.
W20-1 and G20-2 signs on SR471 mainline mounted on Type III DOUBLE Faced Barricade.

STA #	Lane	12.5mm, HT, Asphalt Pavement, Leveling 403-B001			
		Length	Width		TONS
810+00 - 830+00	SB	2000	28		957.30
925+50 - 966+00	SB	4050	28		1126.72
1020+00 - 1063+00	SB	4300	28		1624.69
828+25 - 873+75	NB	4550	28		1471.17
925+75 - 965+00	NB	3925	28		1100.24
1005+25 - 1066+25	NB	6100	28		2589.85
1107+00 - 1144+25	NB	3725	28		1251.75
777+00 - 793+00	NB	1600	28		1016.32
777+00 - 793+00	SB	1600	28		1775.90
880+00 - 899+00	NB	1900	28		1402.15
880+00 - 899+00	SB	1900	28		1353.00
909+00 - 920+00	NB	1100	28		523.69
1005+00 - 1019+00	SB	1400	28		792.41
1086+00 - 1105+00	SB	1900	28		1535.82
TOTAL:					18521.01

108604/301000
SR 25 From SR 471 to SR 43
Rankin County

STA. #	6" Perforated Sewer Pipe 605-0005	6" Non- Perforated Sewer Pipe 605-P004	Filter Mat'l Type "A" 605-W001	Filter Mat'l Type "B" 605-W002	Geotextile for Sub. Drainage, Type III 605-AA001
943+00	300	40	19.14	20.82	227.79
947+00	200	40	12.76	13.88	151.86
1084+00	200	40	12.76	13.88	151.86
TOTAL:	700	120	44.66	48.58	531.51

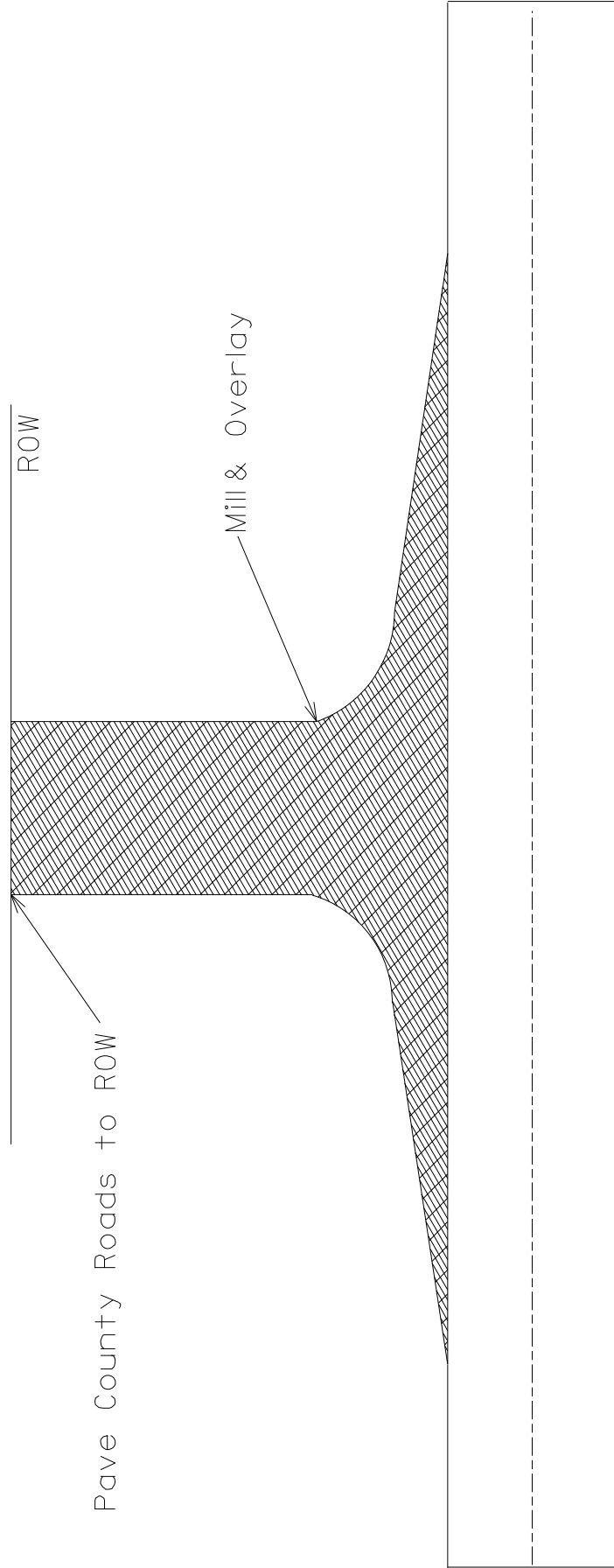
TRAFFIC SIGNAL RADAR DETECTION CHART									
Intersection	Detection Zone Location	Phase #	Detection Zone Size	STOPBAR Radar Units	Advance Radar Unit	Radar Cable (ft)	Existing Controller Type	Existing Pole Configuration	
MS 25 at Holly Bush Road	SB Left Turn Lane	1	6'X50'	1		350	Existing M34 Controller (New Controller Required)	Woodpole Spanwire	
	SB Thru Lanes	6	445' from STOPBAR		1	350			
	NB Left Turn Lane	5	6'X50'	1		240			
	NB Thru Lanes	2	445' from STOPBAR		1	240			
	WB Left Turn/Thru Lanes	3	6'X50'	1		430			
	EB Left Turn/Thru Lanes	4	6'X50'	1		170			
	Total				4	2			1780

Rankin COUNTY - 108604/301000

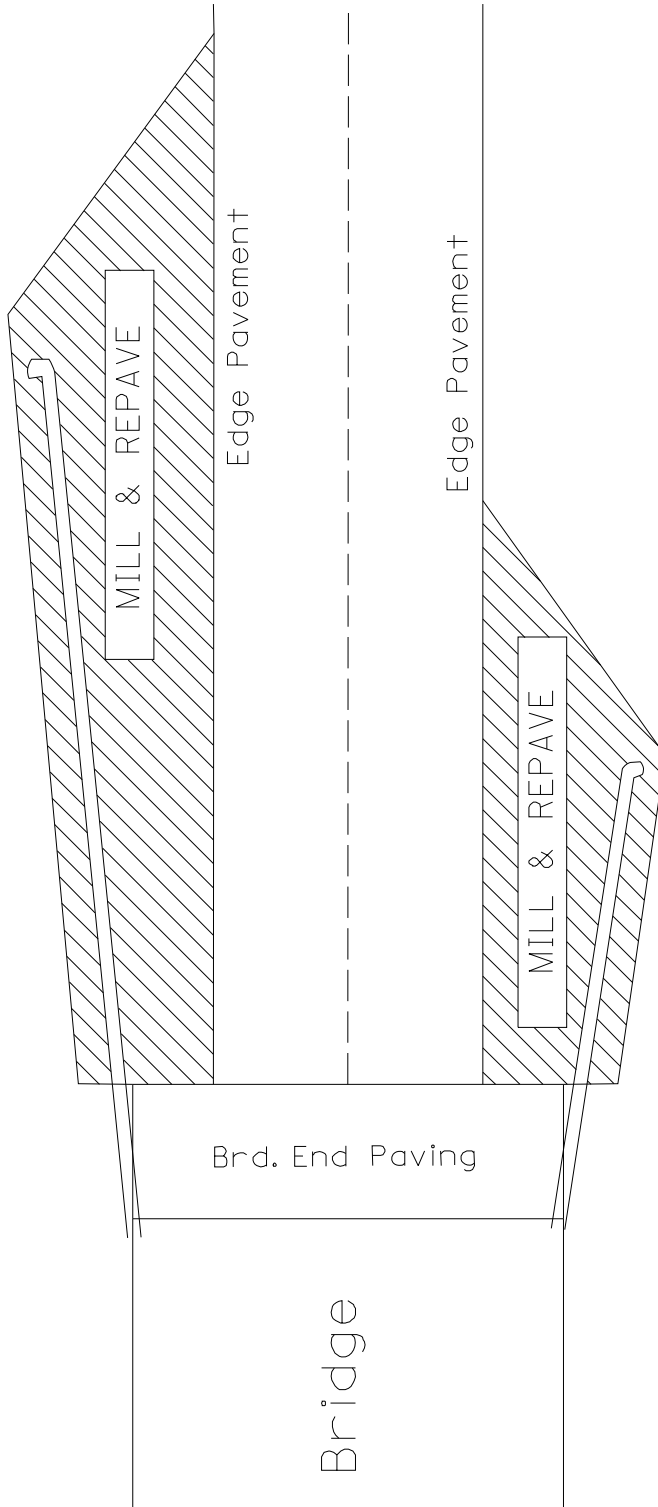
STATION	LOCATION (L/R/T)	GUARDRAIL		THIRIE BEAM TRANS. SECT. (LF)	THIRIE BEAM (LF)	FIRED TERMINAL END SECT (EA)	TANGENT TERMINAL END SECT (EA)	Cable Anchor TYPE I (EA)	BRIDGE END SECTION			DELINEATORS		Type 3 Object Markers (EA)	GUARDRAIL REMOVAL (LF)	REMARKS	
		TYPE "A" (EA)	TYPE "C" (EA)						TYPE "F" (EA)	Type "H" (EA)	WHITE (EA)	YELLOW (EA)					
657+01	Lt	150				1							7		1	206	Hwy. 25
	Rt	150				1									1	206	
701+08	Lt	150				1							7		1	206	Hwy. 25
	Rt	150				1									1	206	
705+48	Lt	150				1							7		1	206	Hwy. 25
	Rt	150				1									1	206	
1165+81	Lt	150				1							7		1	206	Hwy. 25
	Rt	150				1									1	206	
1171+95	Lt	150				1							7		1	206	Hwy. 25
	Rt	150				1									1	206	
19+17	Lt	150				1							7		1	206	Sandhill Road
	Rt	150				1									1	206	
23+00	Lt	150				1							7		1	206	Sandhill Road
	Rt	150				1									1	206	
TOTAL =		2100	0	0	0	0	14	0	0	0	0	14	49	49	14	2884	
		L.F.	EA.	LF.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	LF.	

* REMOVAL OF ALL GUARDRAIL (BRIDGE END SECTIONS, W-BEAM, TYPE-I, CABLE ANCHORAGE, TERMINAL END SECTIONS, ETC.) WILL BE PAID UNDER PAY ITEM 202-B REMOVAL OF GUARD RAIL.
 * REMOVAL OF GUARDRAIL DELINEATORS ARE CONSIDERED INCIDENTAL TO THE REMOVAL OF GUARDRAIL AND WILL NOT BE MEASURED AS A SEPARATE PAY ITEM.
 * ALL GUARDRAIL (METAL RAIL AND METAL POSTS ONLY) WILL BE RETAINED BY MDOT. WOODEN POSTS, ALL BLOCKOUTS, CONCRETE ANCHORS, ETC. WILL BE THE PROPERTY OF THE CONTRACTOR.
 * TOTAL GUARDRAIL LENGTH IS BASED ON A TERMINAL END SECTION 37.5' LONG. IF A TERMINAL END SECTION OF A DIFFERENT LENGTH IS USED, THE LENGTH OF THE W-BEAM MAY HAVE TO BE ADJUSTED.

SR 25
LOCAL ROAD MILL & OVERLAY
FROM SR 471 TO SR 43
RANKIN COUNTY
108604/301000

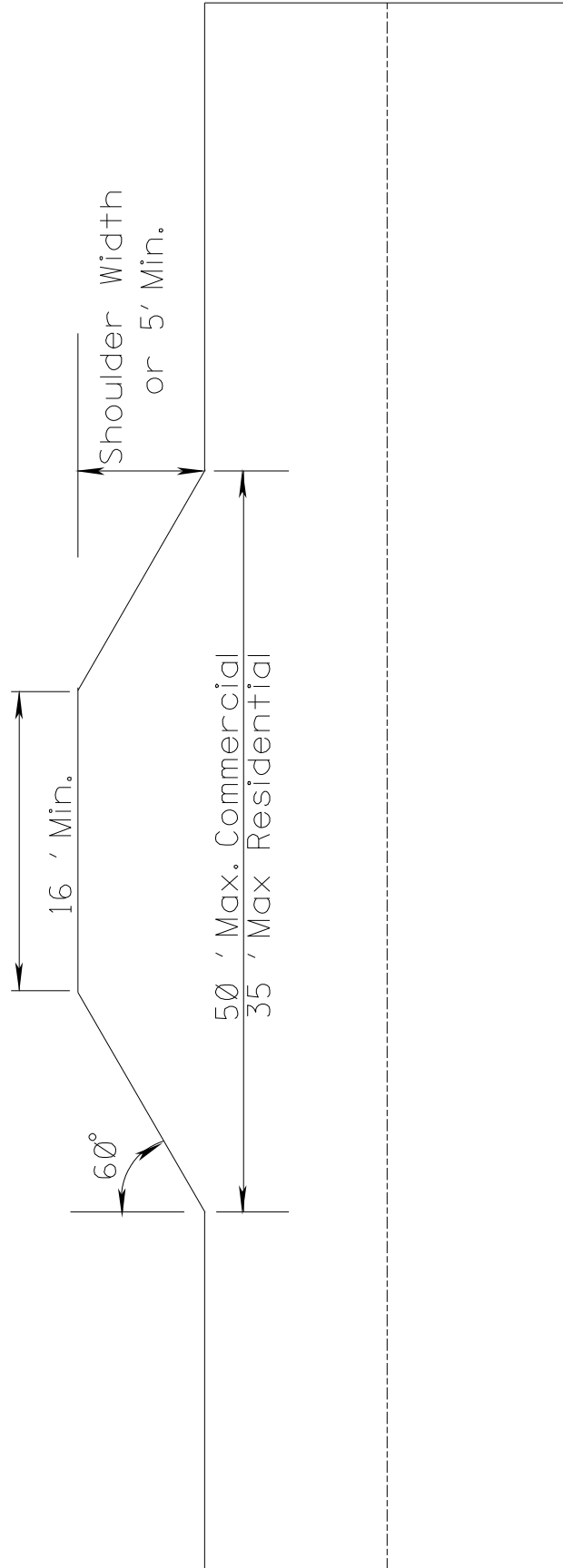


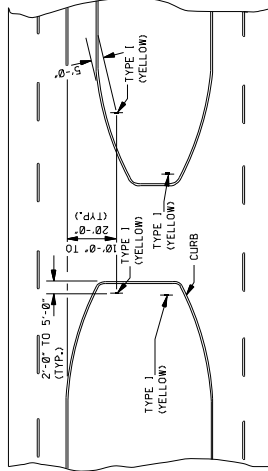
REMOVE & REPAVE BRIDGE GUARDRAIL PAD
108604/301000
RANKIN COUNTY
SR 25 OVERLAY
FROM 471 TO SR 43



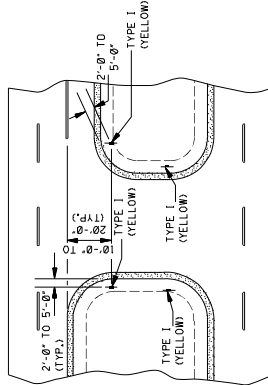
108604/301000
RANKIN COUNTY
SR 25 OVERLAY
FROM 471 TO SR 43

TYPICAL RAMP/PAD DETAIL

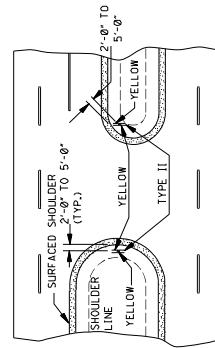




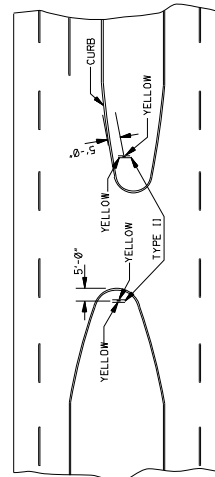
TYPICAL DELINEATION AT A CURBED CROSSOVER WITH A MEDIAN WIDTH OVER 42'-0"



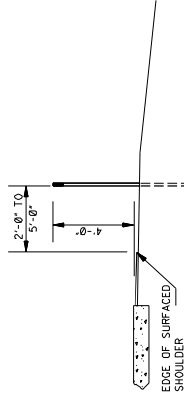
TYPICAL DELINEATION AT A CROSSOVER WITH USABLE SHOULDERS AND A MEDIAN WIDTH OVER 42'-0"



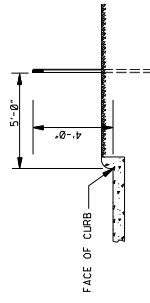
TYPICAL DELINEATION AT A CROSSOVER WITH USABLE SHOULDERS AND A MEDIAN WIDTH OF 42'-0" OR LESS



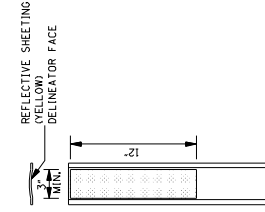
TYPICAL DELINEATION AT A CURBED CROSSOVER WITH A MEDIAN WIDTH OF 42'-0" OR LESS



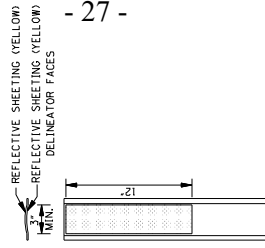
DELINEATOR MOUNTING ON CROSSOVER WITH USABLE SHOULDER



DELINEATOR MOUNTING ON CURBED CROSSOVER



DETAIL OF TYPE I FLEXIBLE POST DELINEATOR



DETAIL OF TYPE II FLEXIBLE POST DELINEATOR

NOTE: CARBONITE'S CURVE-FLEX DELINEATOR POSTS ARE SHOWN. OTHER FLEXIBLE POSTS THAT HAVE BEEN APPROVED FOR LISTING IN THE DEPARTMENT'S "APPROVED SOURCE OF MATERIALS" MAY BE FURNISHED.

NOTE: PLACE DELINEATORS NO MORE THAN 28'-0" FROM EDGE OF TRAVEL LANES EDGES.

GENERAL NOTES:

- THE UNIT PRICE OF DELINEATOR INCLUDES COST(S) OF DELINEATOR FACE(S), POST, HARDWARE AND INSTALLATION.
- DELINEATOR FACE WILL BE ENCAPSULATED LENS REFLECTIVE SHEETING.
- POST REQUIRING THE INSTALLATION OF A BASE SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION.
- THE COLOR OF DELINEATORS SHALL BE THE COLOR OF THE ADJACENT EDGELINE PER MUTCO SECTION 3F.03.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TYPICAL CROSSOVER
DELINEATION



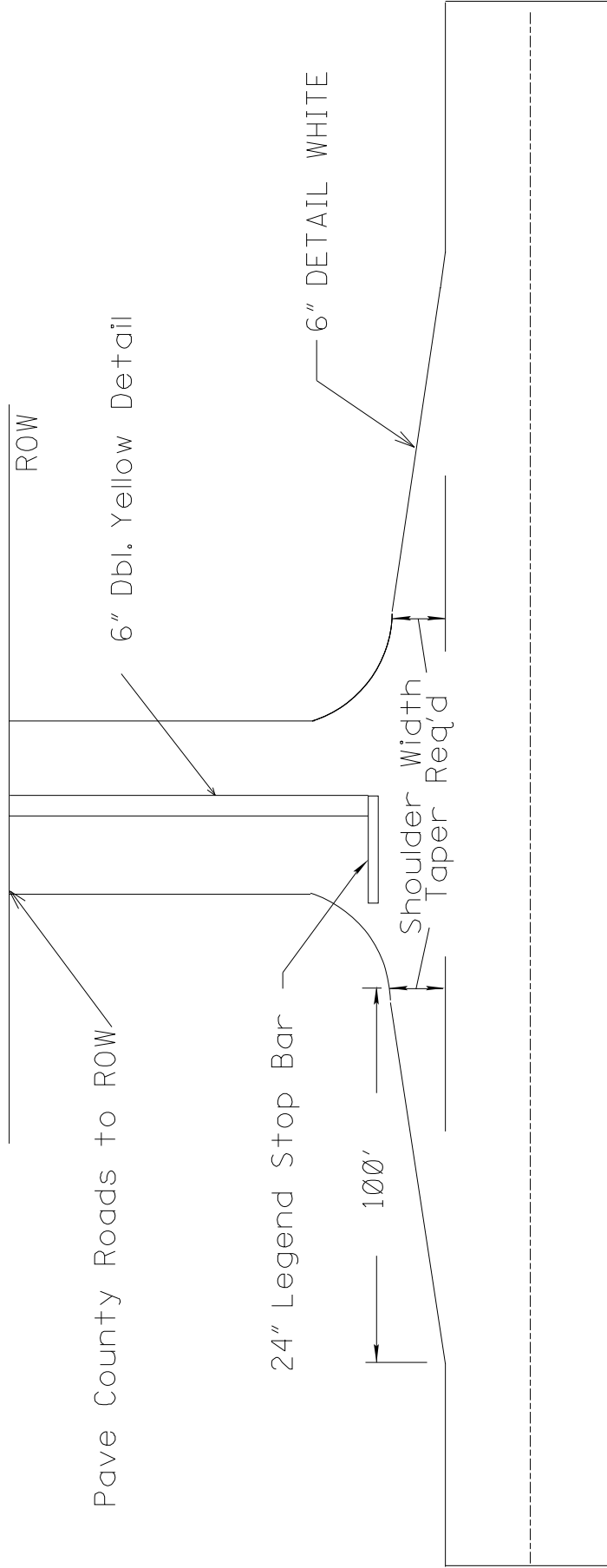
DESIGN NUMBER
SSN-88
POST NUMBER
6316

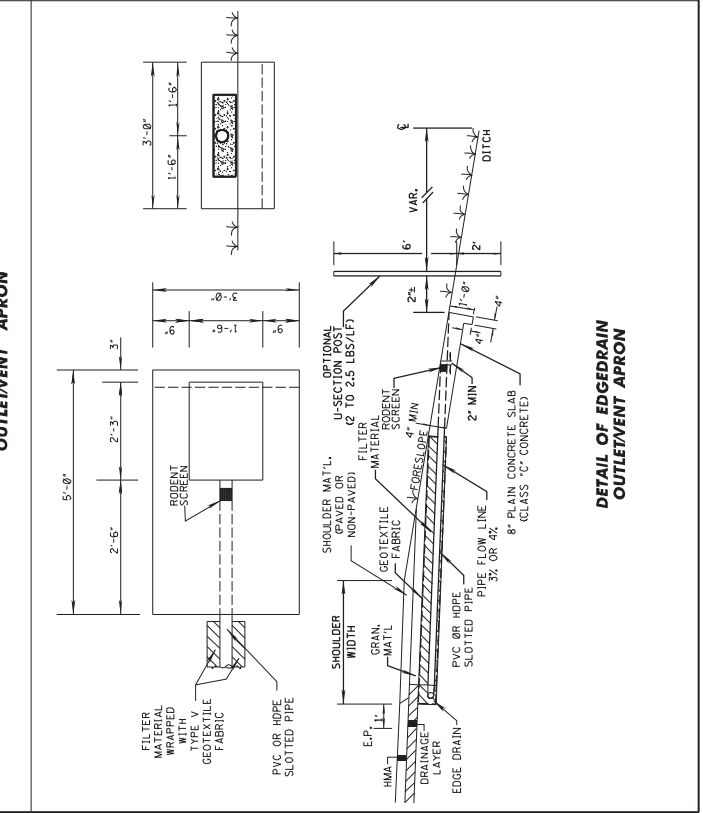
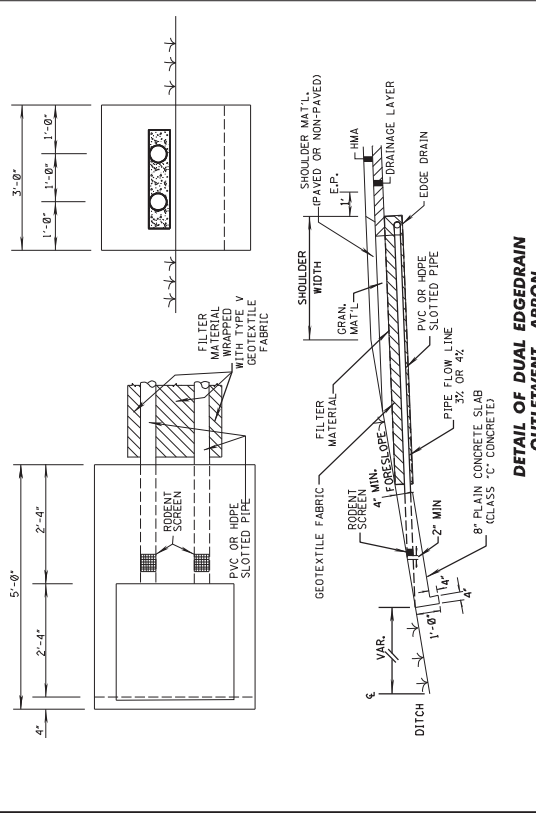
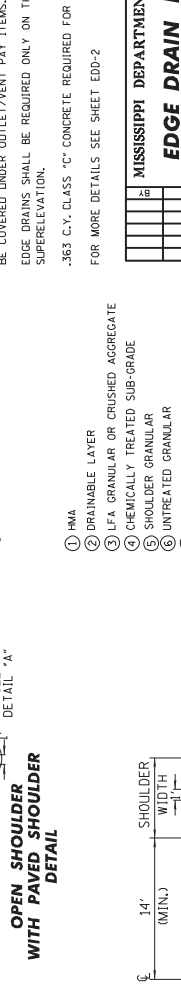
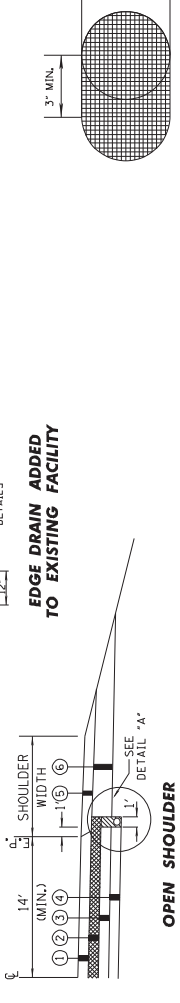
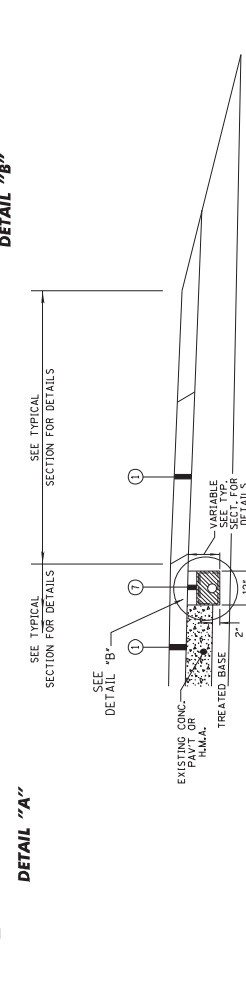
ISSUE DATE: AUGUST 01.2017

REVISION	
DATE	DESCRIPTION

108604/301000
RANKIN COUNTY
SR25 OVERLAY
FROM 471 TO SR 43

Typical Section - County Roads





DETAIL OF RODENT SCREEN

3X3 GALVANIZED HARDWARE CLOTH 0463 WIRE OR EQUAL FORMED TO FIT SNUG TO INSIDE OF PIPE. SCREEN SHALL BE WELDED TO EDGE DRAIN ABSORBER IN PAY ITEM 807-808-66G. EDGE DRAIN OUTLET/VENT, COMPLETE IN PLACE (SEE NOTE 6 EDD-2)

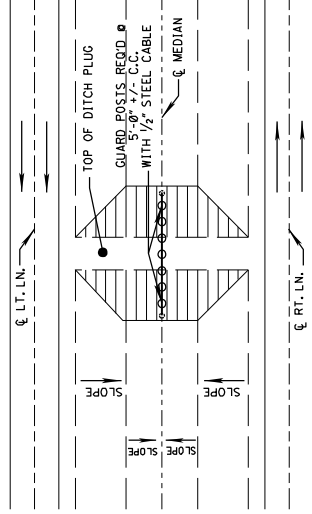
NOTES:
 URBAN EDGE DRAIN OUTLETS TO TIE INTO INLETS. ALL COSTS SHALL BE COVERED UNDER OUTLET/VENT PAY ITEMS.
 EDGE DRAINS SHALL BE REQUIRED ONLY ON THE LOW SIDE OF SUPERELEVATION.
 .363 C.Y. CLASS "C" CONCRETE REQUIRED FOR 5' X 3' APRON.
 FOR MORE DETAILS SEE SHEET EDD-2

- ① HMA
- ② DRAINABLE LAYER
- ③ UFA GRANULAR OR CRUSHED AGGREGATE
- ④ CHEMICALLY TREATED SUB-GRADE
- ⑤ SHOULDER GRANULAR
- ⑥ UNTREATED GRANULAR
- ⑦ 3" HOT MIX ASPHALT

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
EDGE DRAIN DETAIL	
CONCRETE APRON AND RODENT SCREEN	
PROJ. NO.: UPDATE	COUNTY: UPDATE
FILE NAME: DWD6.dwg	DATE: _____
DESIGN TEAM: _____	CHECKED: _____ DATE: _____
DATE: _____	BY: _____
REVISION: _____	DATE: _____
PRELIMINARY	NOT FOR CONSTRUCTION
WORKING NUMBER	EDD-1
SHEET NUMBER	_____
\$PG\$	_____

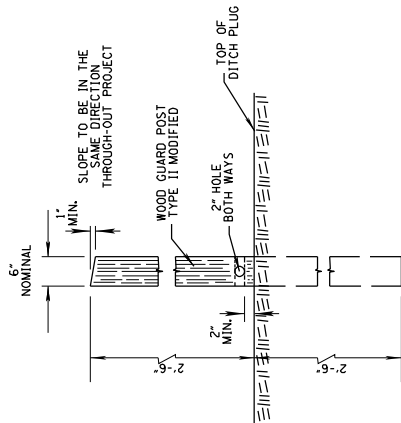
STATE	PROJECT NO.
MISS.	IM-0020-02(072)

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
GUARD POST DETAILS	
PROJECT NO. : IM-0020-02(072)	WORKING NUMBER : GP-TA-0
COUNTY : NEWTON	SHEET NUMBER : 52
FILE NAME :	DATE :
DESIGN LEAD :	DATE :
DATE :	DATE :
DATE :	DATE :
DATE :	DATE :
DATE :	DATE :
DATE :	DATE :
DATE :	DATE :

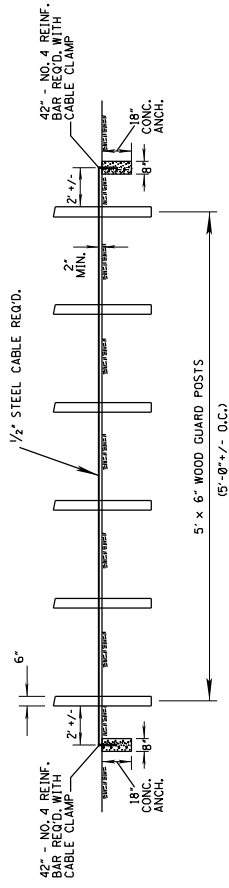


TYPICAL INSTALLATION OF GUARD POST AT DITCH PLUGS AND TRAFFIC-MADE CROSSOVERS AS SHOWN ON PLANS OR AS DIRECTED

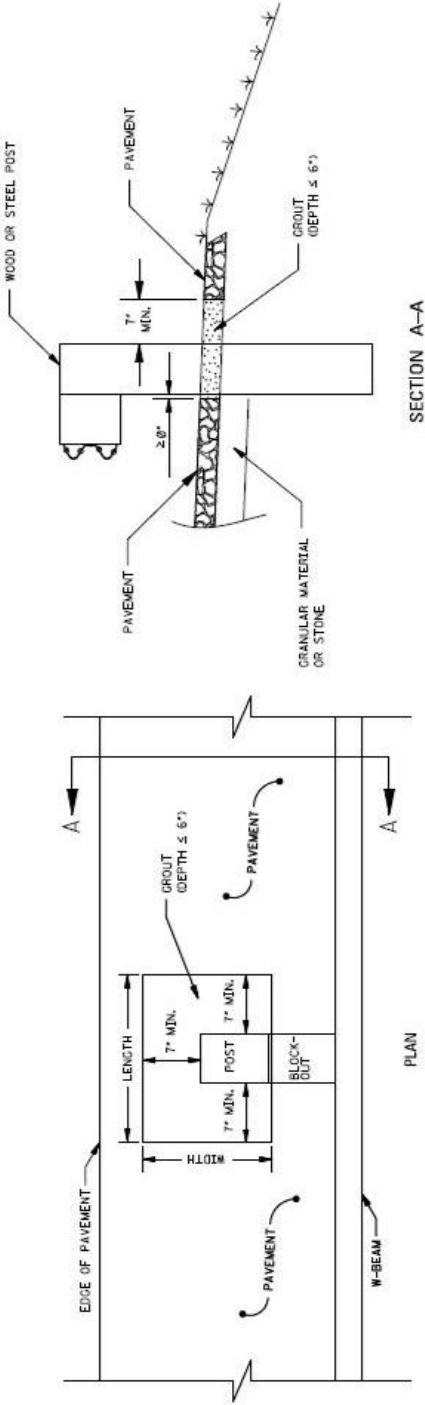
*COST OF STEEL CABLE, CONCRETE ANCHORS, CLAMPS, AND REINFORCEMENT BARS TO BE ABSORBED IN GUARD POST PAY ITEM.



ELEVATION OF WOOD GUARD POST (TYPE II MODIFIED)



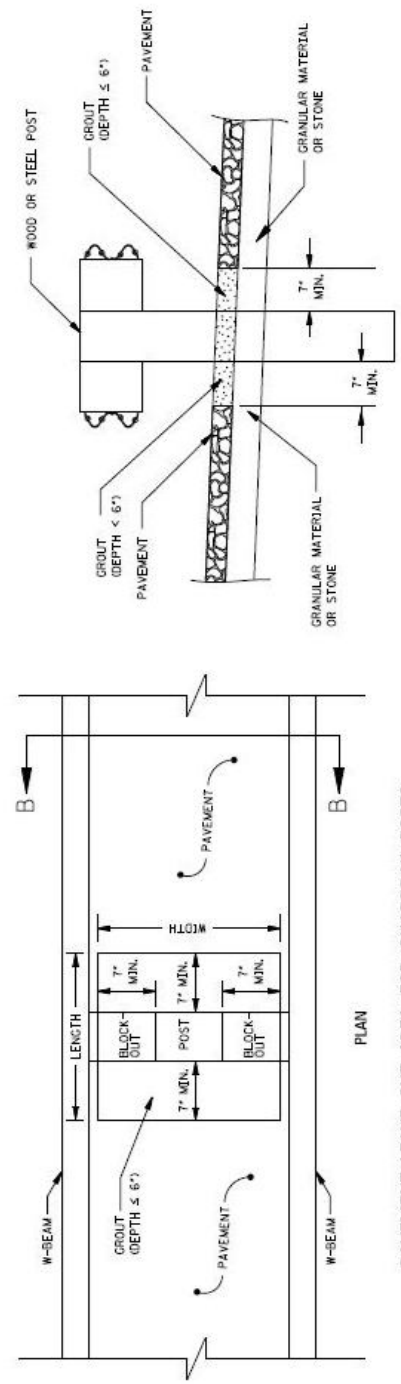
Guardrail Post Installation in Paved Areas



SECTION A-A

POST	MIN. PAV'T LEAVE-OUT AREA	
	SINGLE-FACED LENGTH (IN.)	DOUBLE-FACED LENGTH (IN.)
6" x 8" WOOD	28	28
6" x 8" WOOD	28	28
10" x 10" WOOD	24	24
#6 x 3 STEEL	18	18

PAVEMENT LEAVE-OUT AREA FOR GUARDRAIL POSTS
SINGLE-FACED GUARDRAIL



SECTION B-B

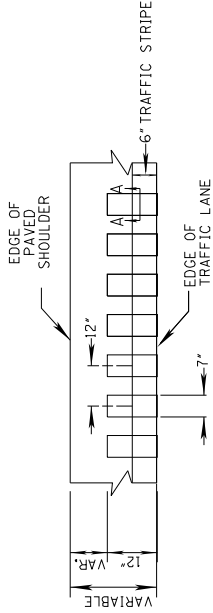
PAVEMENT LEAVE-OUT AREA FOR GUARDRAIL POSTS
DOUBLE-FACED GUARDRAIL

GENERAL NOTES

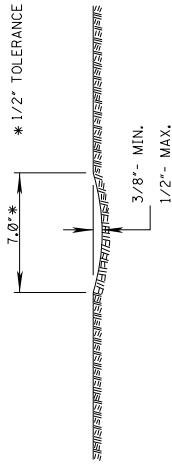
1. GUARDRAIL POSTS SHALL NOT BE COMPLETELY SURROUNDED BY PAVEMENT. THE AREA BEHIND AND LATERAL OF THE POST SHALL HAVE A MINIMUM 7" CLEARANCE FROM THE PAVEMENT. THIS AREA SHALL BE FILLED WITH A LOW STRENGTH GROUT WITH A MAXIMUM 28 DAY COMPRESSIVE STRENGTH OF 120 PSI.
2. GROUT SHALL BE INSTALLED AT A DEPTH EQUAL TO THE SURROUNDING PAVEMENT UP TO A MAXIMUM OF 6". IF SURROUNDING PAVEMENT IS GREATER THAN 6", THE DIFFERENCE SHALL BE FILLED IN WITH SHOULDER GRANULAR MATERIAL.
3. COST OF GROUT SHALL BE ABSORBED IN THE COST OF OTHER ITEMS BID.
4. PAVEMENT LEAVE-OUT AREAS ARE REQUIRED FOR STEEL AND WOOD POSTS.
5. STANDARD EMBEDMENT DEPTHS STILL APPLY, MEASURED FROM THE TOP OF THE PROJECTED PAVEMENT SURFACE.

STATE	PROJECT NO.
MISS.	

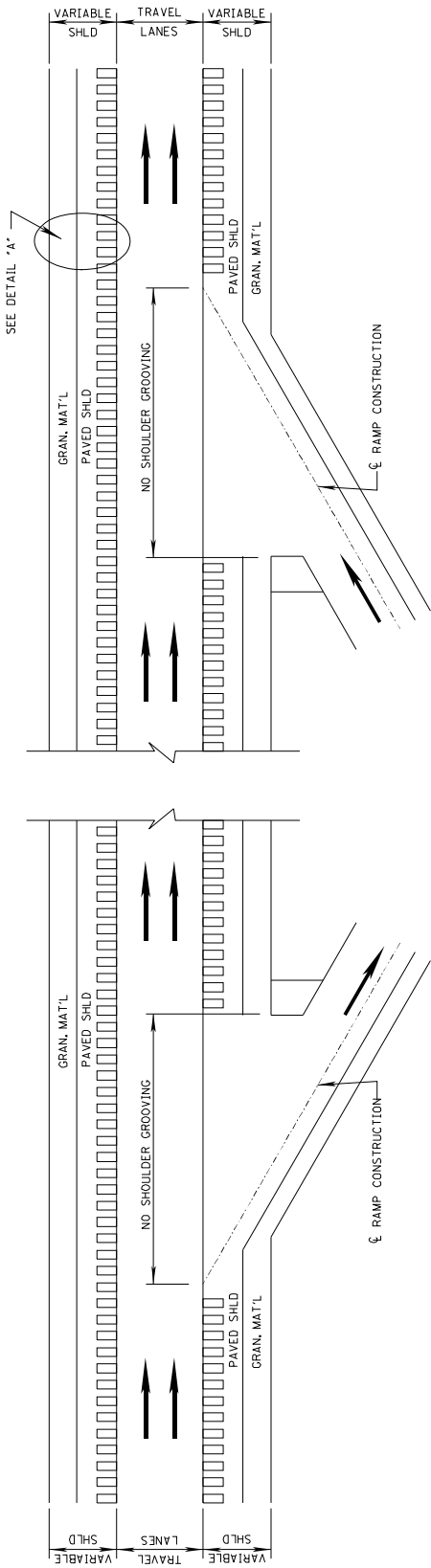
- GENERAL NOTES**
1. ALL RUMBLE STRIPES SHALL BE GROUND-IN. RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 2. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY.



DETAIL "A"



SECTION "A-A"



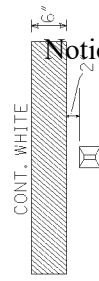
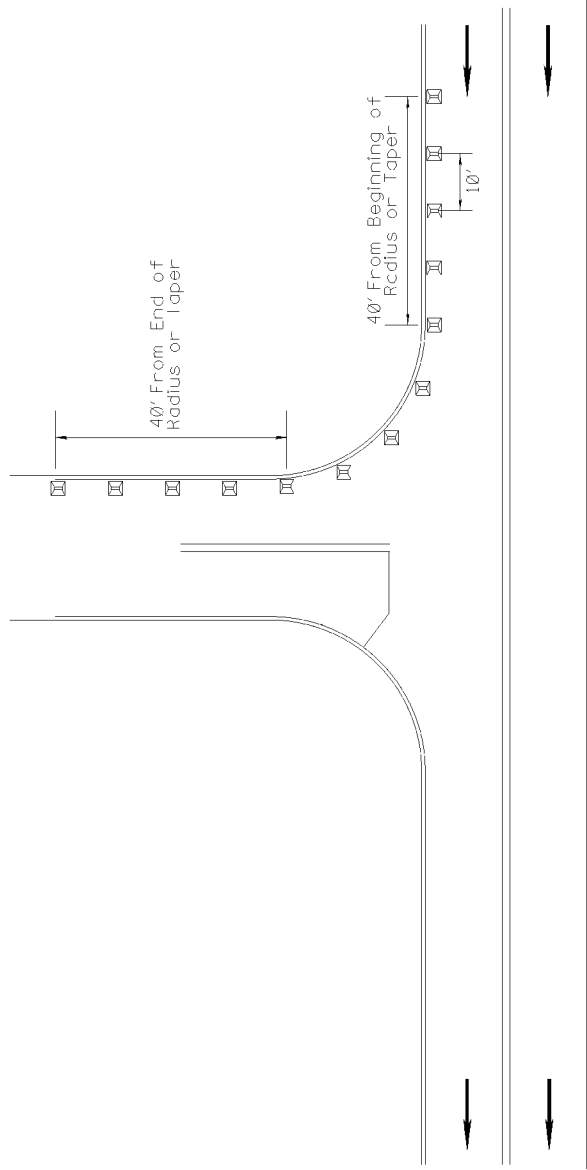
PLAN

NOT TO SCALE
DETAILS OF
RUMBLE STRIPS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
RUMBLE STRIPES	
4-LANE HIGHWAYS	
(ASPH. LANES, 2-FT ASPH. SHLD)	
PROJECT NO:	FILE NAME:
COUNTY:	DESIGN NUMBER:
DATE: 05/01/14	CHECKED:

STATE	PROJECT NO.
MISS.	

TYPICAL FOR RAISED PAVEMENT MARKERS PLACED ON SIDE ROAD RADII 4-LANE TRAFFIC



DETAIL A

→ DIRECTION OF TRAFFIC

- NOTE 1: MARKERS SHALL BE PLACED EVERY 10 FEET.
- NOTE 2: MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
- NOTE 3: MARKERS SHALL BE HIGH PERFORMANCE TWO WAY CLEAR.
- NOTE 4: FIVE (5) MARKERS SHALL BE PLACED ALONG MAINLINE EDGE STRIPE.
- NOTE 5: MARKERS FOR COUNTY ROADS SHALL CONTINUE DOWN THE EDGE STRIPE A DISTANCE OF 40 FEET.
- NOTE 6: MARKERS SHALL NOT BE ROTATED WHEN BEING PLACED ALONG RADIUS OF LOCAL ROAD.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
4 - AVL	PRELIMINARY
NOT FOR CONSTRUCTION	
2-WAY CLEAR RAISED PAVEMENT MARKERS PLACED ON SIDE ROADS	
PROJECT NO.	DRAWING NUMBER
COUNTY	CHANGING NUMBER
FILENAME: SPS\SIDERADII.DGN	SHEET NUMBER
DESIGNER	CHECKED
DATE	

Notice to Bidders No 337