

STATE	PROJECT NUMBER	SHEET NO.
MISSISSIPPI	MP-7051-57(019)	1

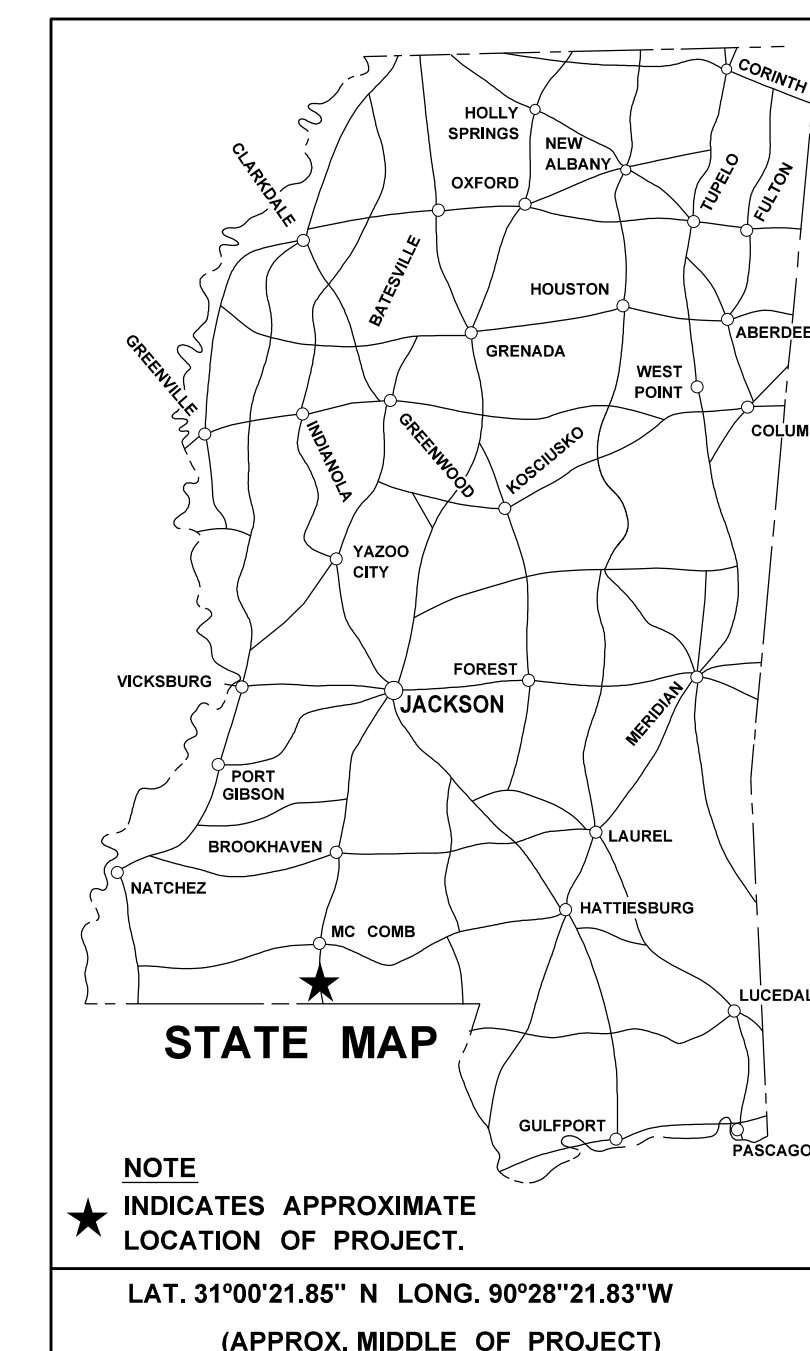
STATE OF MISSISSIPPI

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

# PLAN AND PROFILE OF PROPOSED STATE HIGHWAY STATE PROJECT NO. MP-7051-57(019)

US 51 From LA State Line to SR 48 FMS CON. NO. 307442/ 301000  
PIKE COUNTY

PLAN 1 IN. = 100 FT.  
 PROFILE { HOR. 1 IN. = 100 FT.  
 VERT. 1 IN. = 10 FT.  
 LAYOUT 1 IN. = 6,842 FT.



GENERAL INDEX

INCLUDED THIS PROJECT	BEGIN WITH SHEET
<input checked="" type="checkbox"/> ROADWAY	1
<input type="checkbox"/> PERMANENT SIGNS	1001
<input type="checkbox"/> TRAFFIC SIGNALS	2001
<input type="checkbox"/> ITS COMPONENTS	3001
<input type="checkbox"/> LIGHTING	4001
<input type="checkbox"/> (RESERVED)	5001
<input checked="" type="checkbox"/> ROADWAY STANDARD DWGS	6001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (LRFD)	7001
<input type="checkbox"/> BOX CULVERT STD. DRAWINGS (STD. SPEC.)	7501
<input type="checkbox"/> BRIDGE	8001
<input type="checkbox"/> CROSS SECTIONS	9001

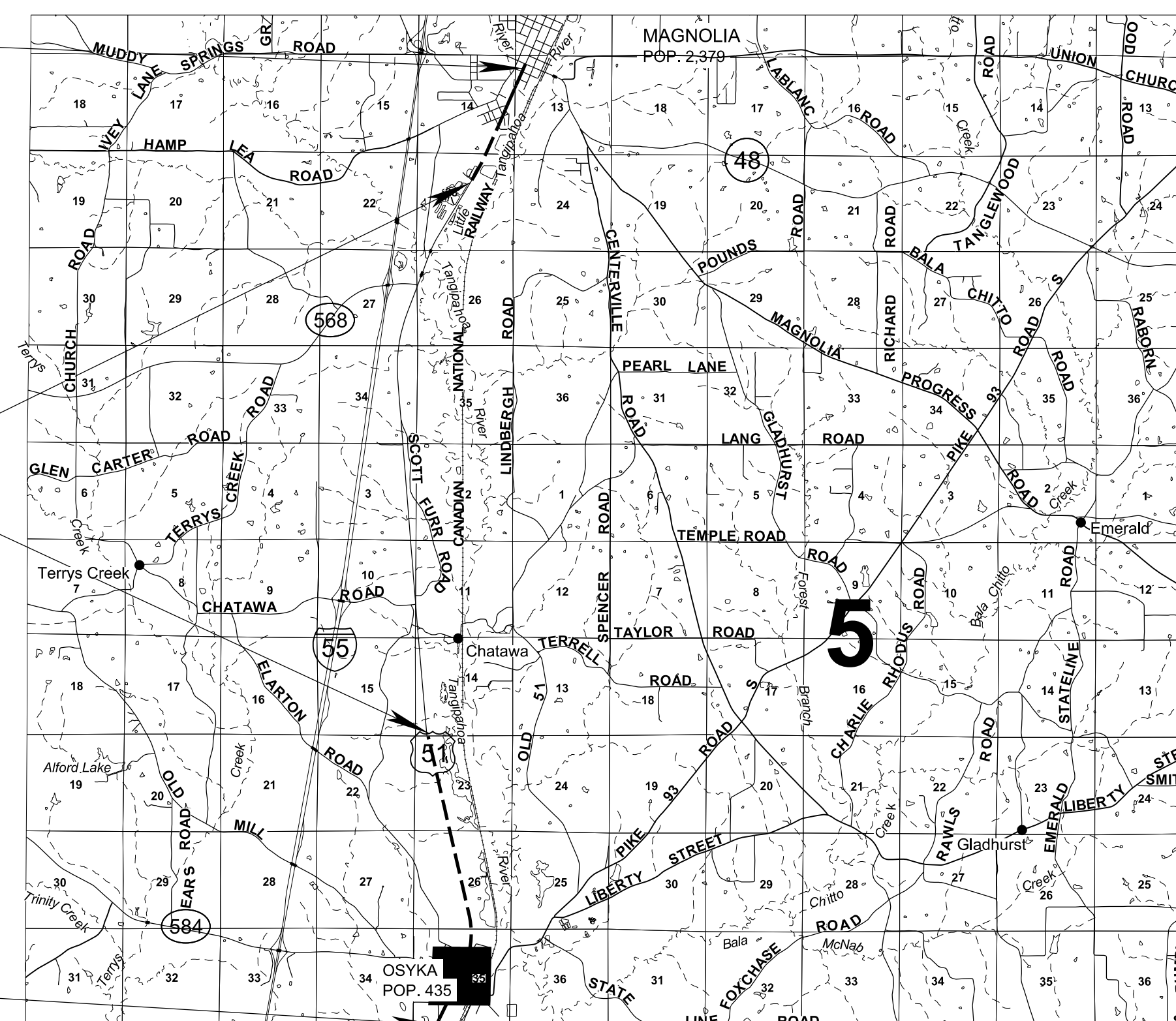
BRIDGE STRUCTURES REQ'D.

BOX BRIDGES REQ'D.

E.O.P. STA. 540 + 35

OMITTED SECTION

B.O.P. STA. 0 + 25



CONVENTIONAL SYMBOLS

COUNTY LINE	-----
TOWN CORPORATION LINE	-----
SECTION LINE	-----
EXISTING ROAD OR TRAVELED WAY	-----
PROPOSED ROAD OR TRAVELED WAY	-----
RAILROAD	-----
SURVEY LINE	-----
BRIDGES	-----

EQUATIONS

LENGTH DATA

LENGTH OF ROADWAY	11,617 FT.	2.200 MI.
LENGTH OF BRIDGES	224 FT.	0.424 MI.
LENGTH OF PROJECT (NET)	11,841 FT.	2.242 MI.
LENGTH OF EXCEPTIONS	FT.	MI.
LENGTH OF PROJECT (GROSS)	11,841 FT.	2.242 MI.

EXCEPTIONS

58+00 TO 481+93  
(OMITTED SECTION)

DESIGN CONTROL

MPH = V (SPEED DESIGN)  
 ADT ( ) = : ADT ( ) =  
 DHV = : D = % T = %

PERMITS ACQUIRED BY MDOT

WETLANDS AND WATERS PERMITS		
	WATERS	WETLANDS
NATIONWIDE #14	<input type="checkbox"/> N	<input type="checkbox"/> N
NATIONWIDE (OTHER)*	<input type="checkbox"/> N	<input type="checkbox"/> N
GENERAL*	<input type="checkbox"/> N	<input type="checkbox"/> N
INDIVIDUAL (404)*	<input type="checkbox"/> N	<input type="checkbox"/> N
STORMWATER PERMIT <input checked="" type="checkbox"/> N		
Y	REQUIRED, CNQI SUBMITTED BY MDOT (DISTURBED AREA = 5 ACRES)	
S	REQUIRED, SCQI TO BE SUBMITTED BY CONTRACTOR (1 TO 4.99 ACRES)	
N	NO STORMWATER PERMIT REQUIRED (<1 ACRE)	
APPROVED BY: _____		

P S & E DATE: 3/12/2021

APPROVED: \_\_\_\_\_  
 DEPUTY EXECUTIVE DIRECTOR / CHIEF ENGINEER  
 EXECUTIVE DIRECTOR



3/1/2021 2:01 PM TITLE

STATE	PROJECT NO.
MISS.	MP-7051-57(019)

DESCRIPTION OF SHEET

DESCRIPTION OF SHEET	WKG. NO.	SH. NO.
TITLE AND LAYOUT SHEET (1)		1
DETAILED INDEX (1)	DI-1	2
GENERAL NOTES (1)	CN-1	3
TYPICAL SECTION SHEETS (2)		
TYPICAL SECTION U.S. 51	TS-1	4
TYPICAL SECTION U.S. 51	TS-2	5
QUANTITY SHEETS (4)		
SUMMARY OF QUANTITIES	SQ-1	6
ESTIMATED QUANTITY MILLING, ASPHALT, GRANULAR MATERIAL	EQ-1	7
ESTIMATED QUANTITY MILLING, ASPHALT, GRANULAR MATERIAL	EQ-2	8
ESTIMATED QUANTITIES FOR TRAFFIC CONTROL SIGNING	TCPQ-1	9
PLAN & PROFILE SHEETS (5)		
STA. 0+25 TO STA. 20+00	WK3	10
STA. 20+00 TO STA. 50+00	WK4	11
STA. 50+00 TO STA. 65+59	WK5	12
STA. 481+48 TO STA. 511+00	WK6	13
STA. 511+00 TO STA. 540+73	WK7	14
SPECIAL DESIGN SHEETS (5)		
DETAIL OF CONSTRUCTION SIGNING	DCS-1	15
DETAIL OF CONSTRUCTION SIGNING	DCS-2	16
TRAFFIC CONTROL PLAN WITH FLAGGER (ONE-LANE CLOSURE OF TWO-WAY TRAFFIC)	SDTCP-1	17
TRAFFIC CONTROL DETAILS DRUM PLACEMENT AND SHOULDER CLOSURE	TCP-SC	18
DETAIL FOR CURB AND ISLAND PAINTING	DCIS-1	19
STANDARD DRAWINGS (6)		
PAVEMENT MARKING DETAILS FOR 2-LANE AND 4-LANE DIVIDED HIGHWAYS	PM-1	6051
SHORT DURATION CLOSING OF TWO-LANE TWO-WAY HIGHWAYS	TCP-6	6356
HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS	TCP-8	6358
TRAFFIC CONTROL PLAN MOBILE OPERATIONS MULTILANE ROADS AND TWO-LANE ROADS	TCP-9	6359
TRAFFIC CONTROL PLANS UNEVEN PAVEMENT DETAILS	TCP-12	6362
TEMPORARY STRIPING FOR TRAFFIC CONTROL 2-LANE AND 4-LANE DIVIDED HIGHWAYS	TCP-13	6363

TOTAL SHEETS (25)

DISTRICT 7		
PS & E PLANS-DATE 3/1/2021		
FMS CON. 307442/301000		
REVISIONS		
DATE	SHEET NO.	BY

REVISION	MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
	<b>DETAILED INDEX</b>	
	PROJ. NO.: MP-7051-57(019) COUNTY: PIKE	
	FILE NAME: \02\index.dgn DESIGN TEAM _____ CHECKED _____ DATE _____	WORKING NUMBER DI-1 SHEET NUMBER <b>2</b>

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STATE	PROJECT NO.
MISS.	MP-7051-57(019)


GENERAL NOTES

GENERAL NOTES (CONT.)

- ① THE LOCATION AND SPACING OF SIGNS, SHOWN ON THE TRAFFIC CONTROL PLANS, ARE APPROXIMATE AND MAY BE ADJUSTED AS NECESSARY TO FIT FIELD CONDITIONS.
- ② ALL TRAFFIC CONTROL DEVICES ON THIS PROJECT SHALL COMPLY WITH PART VI OF THE MUTCD (LATEST EDITION).
- ③ ALL PLASTIC DRUMS SHALL HAVE A BALLASTING COLLAR MADE FROM RECYCLED TRUCK TIRES OR OTHER SUITABLE MATERIAL.
- ④ THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING STRUCTURES SUCH AS, BUT NOT LIMITED TO, PIPES, INLETS, APRONS, AND BRIDGES FROM DAMAGE WHICH MIGHT OCCUR DURING CONSTRUCTION. THE CONTRACTOR SHALL REPLACE OR REPAIR, AS DIRECTED BY THE ENGINEER, ANY STRUCTURES DAMAGED DURING THE LIFE OF THE CONTRACT. NO PAYMENT WILL BE MADE FOR REPLACEMENT OR REPAIR OF DAMAGED ITEMS.
- ⑤ FLUORESCENT ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.
- ⑥ VEGETATIVE MATERIAL WILL BE REMOVED PRIOR TO PLACEMENT OF GRANULAR MATERIAL. THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
- ⑦ THE CONTRACTOR SHALL COVER ANY TEMPORARY TRAFFIC CONTROL SIGNS SHOWN IN THE TRAFFIC CONTROL PLAN THAT DO NOT APPLY TO THE CURRENT PHASE.
- ⑧ REMOVAL OF RAISED PAVEMENT MARKERS THAT ARE IN CONFLICT WITH REQUIRED CONSTRUCTION IS NOT CONSIDERED A SEPARATE PAY ITEM. COST TO BE ABSORBED IN OTHER ITEMS BID.
- ⑨ WHERE MILLING IS REQUIRED, THE CONTRACTOR SHALL PROVIDE OUTLETS IN THE EXISTING SHOULDERS AT SUFFICIENT INTERVALS TO PREVENT POOLING OR STANDING WATER ON THE MILLED SURFACE, THE COST OF WHICH SHALL BE ABSORBED IN OTHER ITEMS BID.
- ⑩ TEMPORARY STRIPING SHALL CONFORM TO FINISHED STRIPE SPECIFICATIONS FOR ALIGNMENT, NEATNESS, AND STRAIGHTNESS.
- ⑪ ALL ADDENDA TO THESE PLANS WILL BE POSTED TO WWW.MDOT.MS.GOV UNDER THE PROPOSAL ADDENDA COLUMN. BIDDERS ARE ADVISED THAT HARD COPIES OF ANY ADDENDA FOR THIS PROJECT WILL NOT BE MAILED. IT IS THE BIDDER'S RESPONSIBILITY TO CHECK AND SEE IF ANY ADDENDA HAVE BEEN POSTED FOR THIS PROJECT.
- ⑫ STORAGE OF FLAMMABLE MATERIALS WILL NOT BE ALLOWED UNDER ANY BRIDGE STRUCTURES WITHOUT WRITTEN APPROVAL FROM THE PROJECT ENGINEER. SEE NOTICE TO BIDDERS ENTITLED "MATERIAL STORAGE UNDER BRIDGES" FOR MORE INFORMATION.
- ⑬ THE RETROREFLECTIVE SIGN SHEETING ON RIGID, TEMPORARY TRAFFIC CONTROL (ORANGE) SIGNS SHALL BE MINIMUM TYPE IX.
- ⑭ ALL SIGNS AND DELINEATORS THAT CONFLICT WITH THE CONSTRUCTION OF THIS PROJECT SHALL BE REMOVED AND RESET BY THE CONTRACTOR; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ⑮ TEMPORARY STRIPING SHALL BE REQUIRED AFTER MILLING AND OVERLAYING OPERATIONS; TEMPORARY STRIPING SHALL BE PLACED IN THE SAME LOCATIONS AND LAYOUT AS PERMANENT STRIPE. ALL CENTERLINE, LANE LINES, EDGE LINES, AND NO-PASSING STRIPES THAT HAVE BEEN REMOVED DURING THE DAY'S OPERATIONS SHALL BE REPLACED WITH TEMPORARY STRIPE BEFORE WORK IS DISCONTINUED FOR THE DAY OR AS SOON THEREAFTER AS WEATHER CONDITIONS WILL PERMIT, EXCEPT THAT:
  - (1) REPLACEMENT OF NO-PASSING STRIPES MAY BE DELAYED FOR A PERIOD NOT TO EXCEED THREE (3) DAYS FOR A TWO OR THREE LANE ROAD
  - (2) TEMPORARY EDGE LINES ON PROJECTS REQUIRING SHOULDERS CONSTRUCTED OF GRANULAR MATERIAL MAY BE DELAYED FOR A PERIOD NOT TO EXCEED THREE (3) DAYS.
- ⑯ ALL ASPHALT AND CONCRETE CURBS ALONG RAMPS, LOCAL ROADS, ETC. FROM B.O.P. TO E.O.P. SHALL BE PAINTED ( TWO APPLICATIONS ) WITH WHITE TRAFFIC PAINT AND TRAFFIC BEADS ; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ⑰ IF THE ASPHALT CURB ALONG THE LOCAL ROAD IS SEVERELY DAMAGED, THE ENTIRE ASPHALT CURB WILL BE REMOVED AS DIRECTED BY THE ENGINEER; COST TO BE ABSORBED IN OTHER PAY ITEMS.
- ⑱ ALL LOCAL ROADS SHALL BE PAVED TO THE R.O.W. LIMITS OR AS DIRECTED BY THE ENGINEER.
- ⑲ GRANULAR MATERIAL WILL NOT BE ALLOWED TO BE PLACED DIRECTLY ON THE SURFACE LIFT OF ASPHALT, BUT MUST BE PLACED DIRECTLY ON THE GRAVEL SHOULDER OR A ROAD WIDENER MACHINE USED AND APPROVED BY THE PROJECT ENGINEER.
- ⑳ ALL RAMPS AND DRIVEWAY PADS SHALL BE PAVED TO THE SHOULDER LINE, MINIMUM OR FURTHER, AS DIRECTED BY THE ENGINEER WITHIN THE R.O.W. LIMITS.
- ㉑ SOME WORK IS REQUIRED OUTSIDE THE PROJECT LIMITS. NO ADDITIONAL COMPENSATION WILL BE MADE FOR SUCH WORK EXCEPT AS PROVIDED BY SPECIFIC PAY ITEMS INCLUDED IN THE PLANS.
- ㉒ THE ASPHALT PAVING OPERATION SHALL BE COMPLETED IN THE FOLLOWING ORDER:
  - A: MILLING
  - B: SURFACE COURSE
  - C: SAW AND SEAL JOINT
- ㉓ TEMPORARY PAVEMENT JOINTS (PAPER JOINTS) SHALL BE EMPLOYED AT ALL LOCATION REQUIRING TRAFFIC TO TRAVERSE AN UNEVEN PAVEMENT JOINT. PAPER JOINTS SHALL BE A MINIMUM OF 9 FEET IN LENGTH AND SHALL BE ADEQUATELY MAINTAINED.
- ㉔ NO TEMPORARY CULVERT STREAM CROSSINGS WILL BE ALLOWED.

- ㉕ THE SAWCUT JOINTS SHALL BE DIRECTLY OVER THE PCC PAVEMENT JOINT AND SHALL BE ACCURATELY LOCATED BY THE METHOD EMPLOYING PINS AND STRINGLINE. THE PINS SHALL BE ACCURATELY LOCATED PRIOR TO PAVING. DETAILS OF THE CONTRACTOR'S METHOD FOR LOCATING THE SAWCUTS SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER. THE ENTIRE SAWING AND SEALING OPERATION SHALL BE COMPLETED WITHIN SEVEN (7) DAYS AFTER THE PLACEMENT OF THE FINAL WEARING COURSE.
- ㉖ THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING BRIDGE JOINT MATERIAL ON ALL BRIDGES WITHIN THE PROJECT LIMITS FROM DIRECT CONTACT WITH THERMOPLASTIC PAVEMENT MARKING, AND WITH ANY OTHER CONSTRUCTION MATERIALS THAT MIGHT DAMAGE THE BRIDGE JOINTS.

3/1/2021 2:01 PM C:\03\GN-1.dgn MISSISSIPPI DEPARTMENT OF TRANSPORTATION

		MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
		<b>GENERAL NOTES</b>	
		PROJ. NO.: <b>MP-7051-57(019)</b> COUNTY: <b>PIKE</b>	
			
		WORKING NUMBER	
		<b>GN-1</b>	
		SHEET NUMBER	
		<b>3</b>	
DATE	DESIGN TEAM	FILE NAME: C:\03\GN-1.dgn	CHECKED _____ DATE _____