

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u> 1 </u>	DATED <u> 5/12/2023 </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>
ADDENDUM NO. <u> </u>	DATED <u> </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>
ADDENDUM NO. <u> </u>	DATED <u> </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>

Number	Description
1	Revised Table of Contents; Deleted Notice to Bidder No. 447; Revised Notice to Bidder No. 3599; Amendment EBSx Download Required.

TOTAL ADDENDA: 1
 (Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

 Contractor

BY _____
 Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____	President	Address
_____	Secretary	Address
_____	Treasurer	Address

The following is my (our) itemized proposal.

SP-0038-01(040)/ 109266301000

Noxubee County(ies)

Revised 01/26/2016

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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PROJECT: SP-0038-01(040)/109266301 - Noxubee

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OCR-485
Progress Schedule

(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

05/12/2023 09:46 AM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

| **SUPPLEMENT TO NOTICE TO BIDDERS NO. 3599**

| **DATE: 08/11/2021**

| After the last drawing on page 33, add the following.

STATE: MISS.

PROJECT NO. _____

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY CONSTRUCTION DIVISION
STANDARD PLAN

**GUARDRAIL:
"W" BEAM
(WOOD POSTS)**

WORKING NUMBER: CR-1
SHEET NUMBER: 6201
ISSUE DATE: MAY 21, 2017

ELEVATION FROM ROADWAY
** NOTE: OPTIONAL BEAM LENGTHS.

SECTION A-A
* NOTE: UNLESS SPECIFIED OTHERWISE ON THE PLANS.

GENERAL NOTES:

- GUARDRAIL SHALL MEET THE REQUIREMENTS OF AASHTO M 188, CLASS A, TYPE 1 UNLESS OTHERWISE DESIGNATED.
- GUARDRAIL SHALL BE SINGLE FACED UNLESS OTHERWISE DESIGNATED.
- GUARDRAIL SECTIONS SHALL BE LAPPED IN THE DIRECTION OF TRAVEL. THE LAPPING SHALL BE AT LEAST THE GUARDRAIL WIDTH. THE GUARDRAIL SHALL BE LAPPED ON A BRIDGE WITH 2-WAY TRAFFIC.
- ALL WOOD POSTS AND BLOCKOUTS SHALL BE TREATED TIMBER IN ACCORDANCE WITH MISSISSIPPI DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS.
- FOR OTHER DETAILS OF POSTS, POST ACCESSORIES, FASTENERS & RAIL ELEMENTS, SEE ASHTO-AAC-ARTBA JOINT TASK FORCE NO. 15, TITLED "STANDARDIZED FLOWWAY BARRIER (UNDERWAY LATES" EDITION.

TYPICAL GUARDRAIL SECTION

DETAILS OF ADJUSTABLE HEIGHT BLOCKOUT ASSEMBLY

SECTION

ELEVATION

SECTION

FASTENER DETAILS

PLAIN ROUND WASHER "F"

HEX NUT AND BOLT "F"

RECESSED NUT

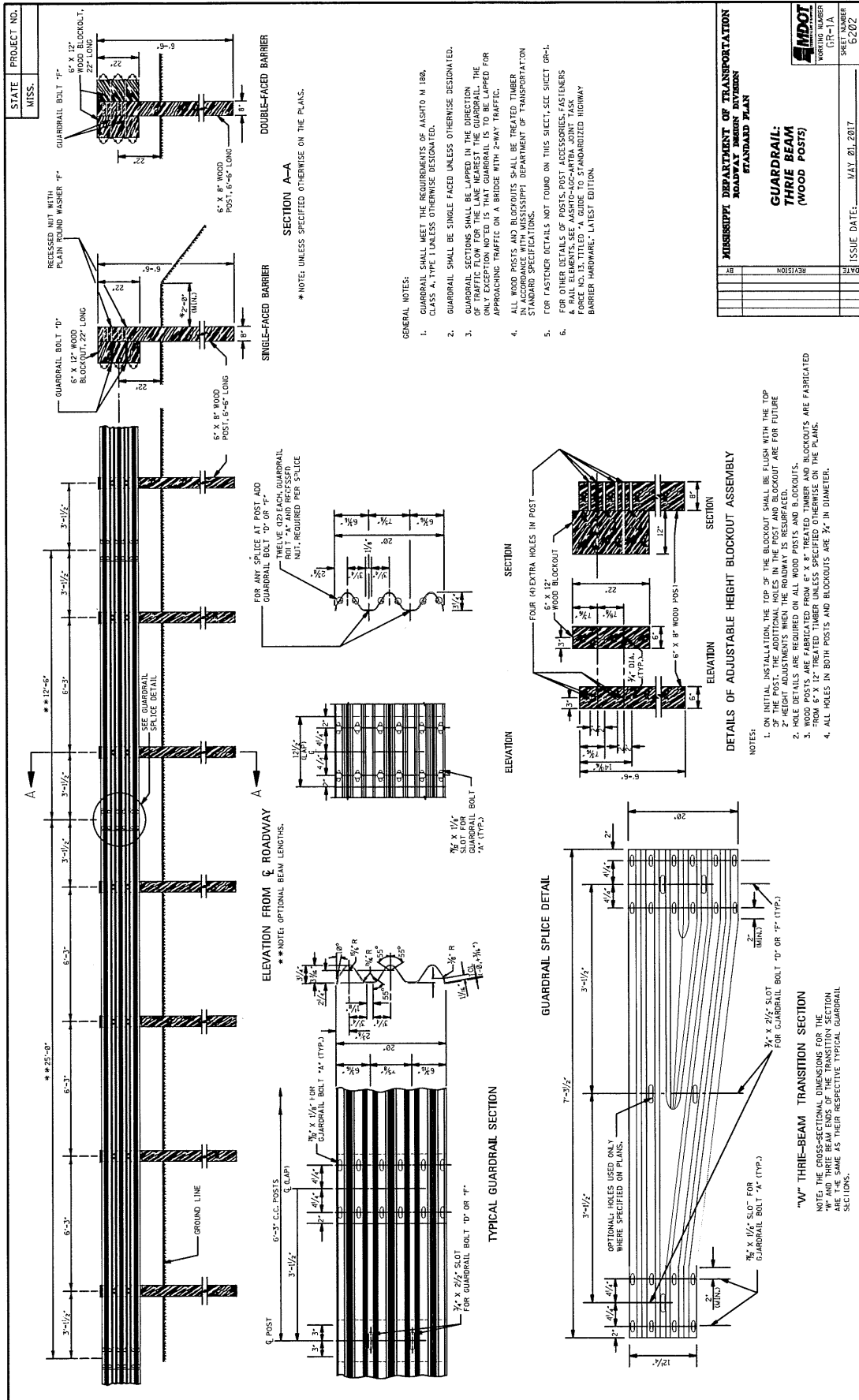
GUARDRAIL BOLT

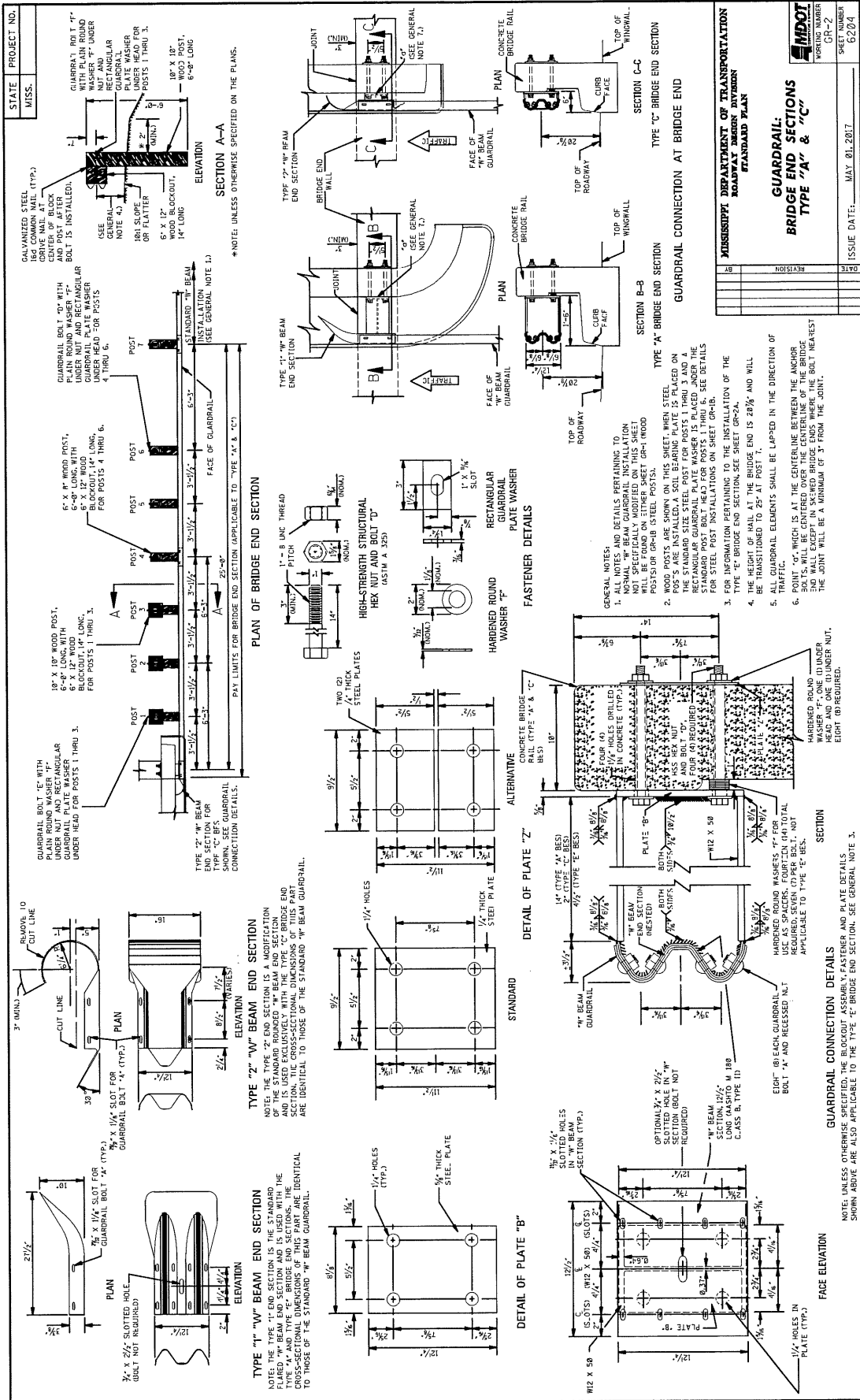
GUARDRAIL BOLTS

BOLT	L (MIN)	T
"A"	1 1/2"	1"
"B"	2 1/2"	1 1/2"
"C"	3 1/2"	2"

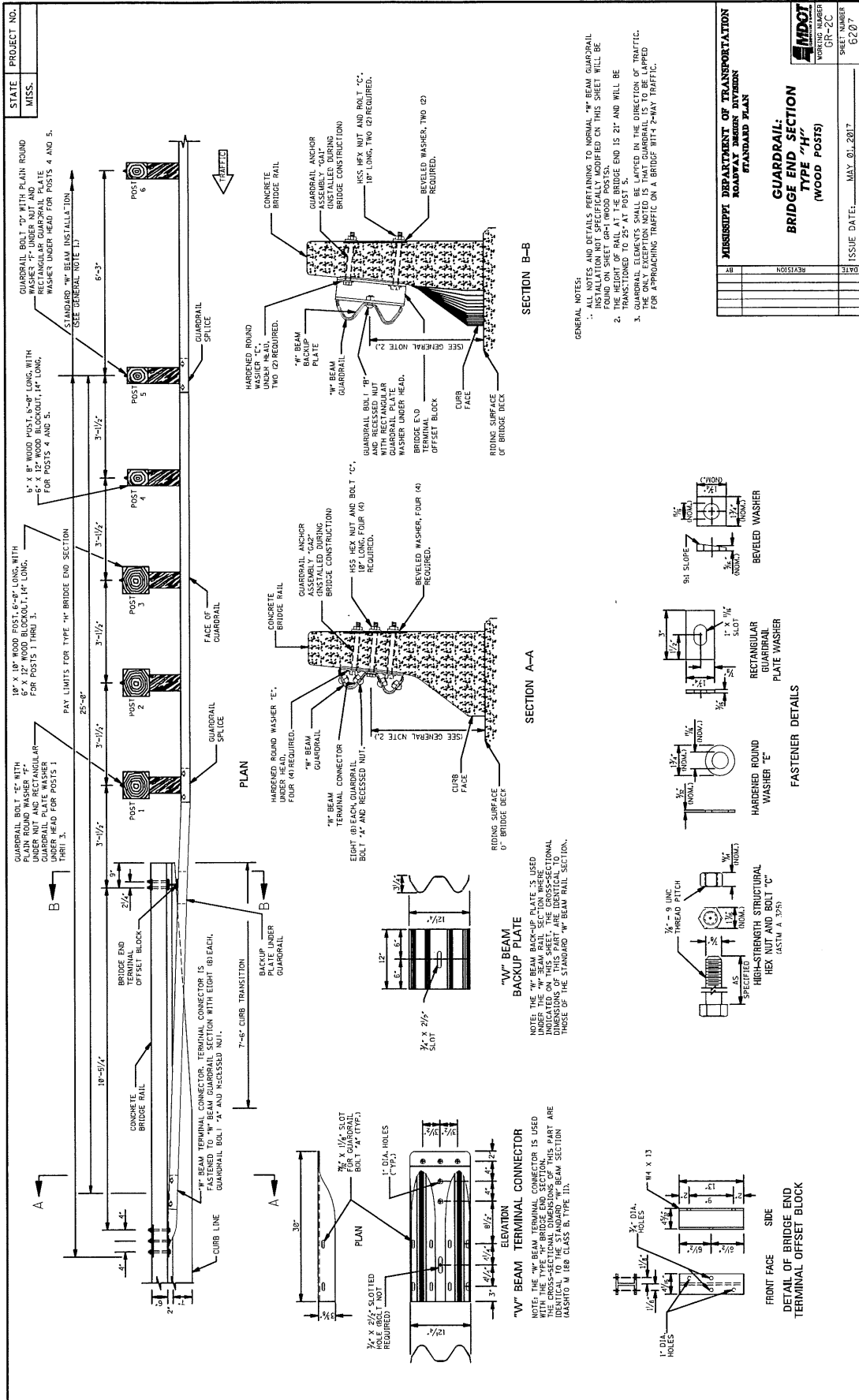
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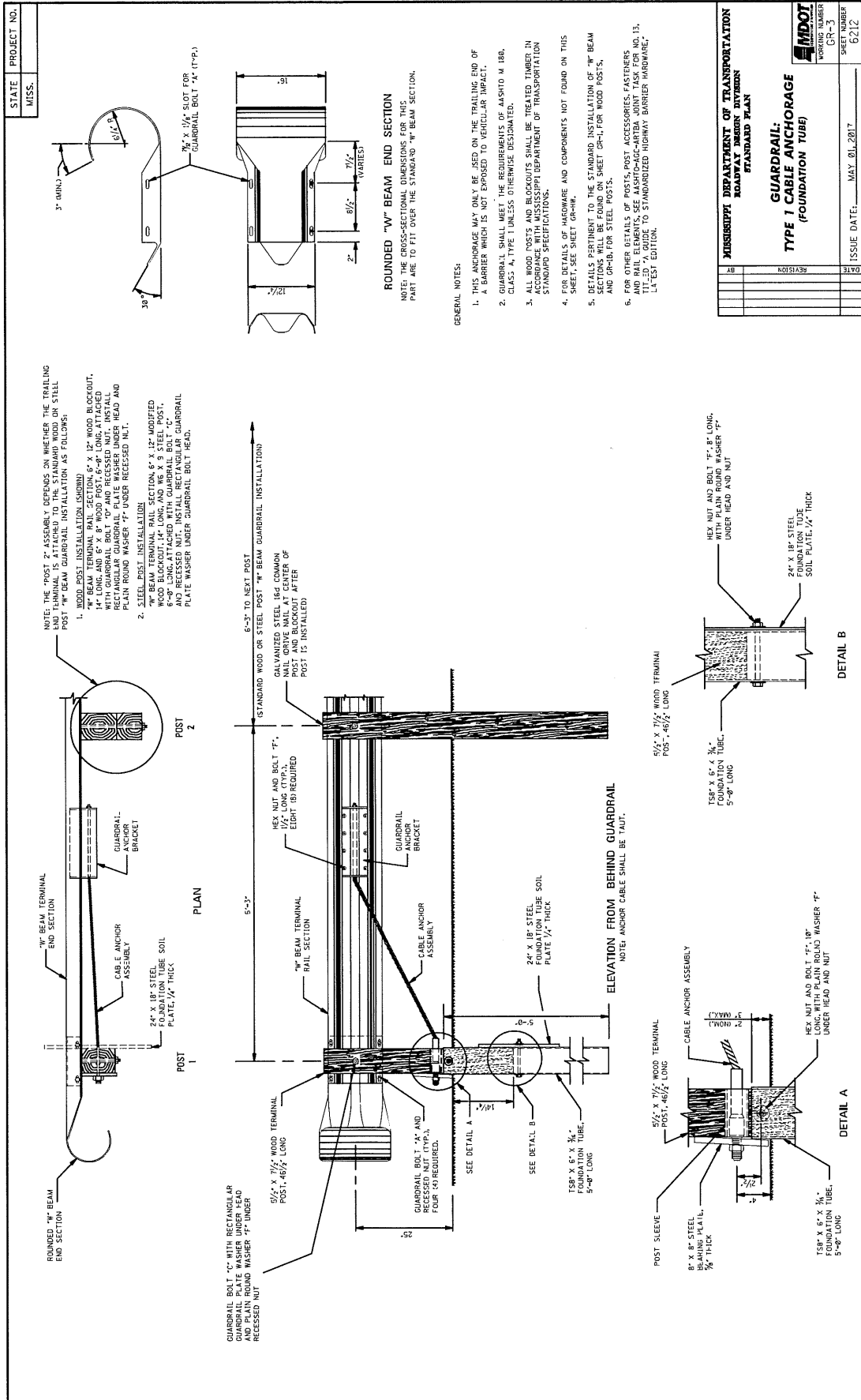
- ALL GUARDRAIL BOLTS ARE 3/8" - 11 UNC THREAD PITCH.
- IF ANY BOLT EXTENDS MORE THAN 1/2" FROM THE NUT, THE BOLT SHOULD BE TRIMMED BACK.
- 3" BARRIER WITH 18" X 18" WOOD POST AND 6" X 12" WOOD BLOCKOUT.





MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY IMPROVEMENT DIVISION STANDARD PLAN	
AB	NO. 101348
3	1
GUARDRAIL: BRIDGE END SECTIONS TYPE "A" & "C"	
WORKING NUMBER	CR-2
SHEET NUMBER	62/04
ISSUE DATE:	MAY 01, 2017

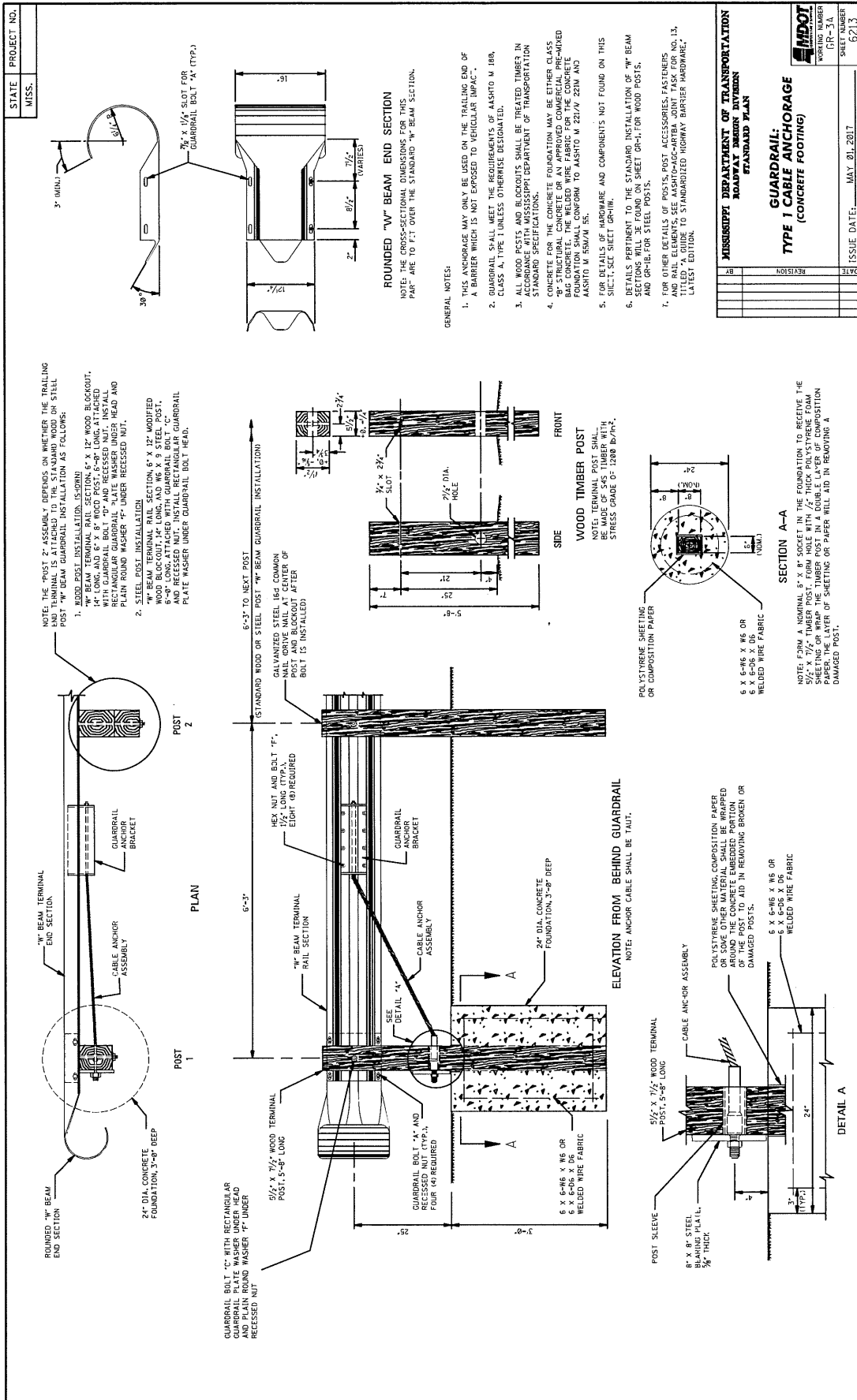




DATE	ISSUE DATE: MAY 01, 2017
SHEET NUMBER	6212
WORKING NUMBER	CR-3

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

GUARDRAIL:
TYPE 1 CABLE ANCHORAGE
(FOUNDATION TUBE)



STATE PROJECT NO.
MISS.

NOTE: THE POST 2' ASSEMBLY DEPENDS ON WHETHER THE TRAILING END TERMINAL IS ATTACHED TO THE STANDARD WOOD OR STEEL POST "W" BEAM GUARDRAIL INSTALLATION AS FOLLOWS:

1. WOOD POST INSTALLATION (USING):
 1 1/2" X 12" WOOD BLOCKOUT,
 1 1/2" LONG AND 6" X 6" WOOD POST, 6'-0" LONG, ATTACHED WITH GUARDRAIL BOLT "D" AND RECESSED NUT. INSTALL WITH GUARDRAIL BOLT "D" UNDER RECESSED NUT, HEAD AND PLAIN ROUND WASHER "F" UNDER RECESSED NUT.
2. STEEL POST INSTALLATION:
 "W" BEAM TERMINAL RAIL SECTION, 6" X 12" MODIFIED WOOD BLOCKOUT, 1 1/2" LONG, AND W6 X 9 STEEL POST, 6'-0" LONG, ATTACHED WITH GUARDRAIL BOLT "C" AND PLAIN ROUND WASHER "E" UNDER RECESSED NUT. GUARDRAIL PLATE WASHER UNDER GUARDRAIL BOLT HEAD.

GUARDRAIL BOLT "C" WITH RECTANGULAR GUARDRAIL PLATE WASHER UNDER HEAD AND PLAIN ROUND WASHER "F" UNDER RECESSED NUT

24" DIA. CONCRETE FOUNDATION, 3'-0" DEEP

5 1/2" X 7 1/2" WOOD TERMINAL POST, 5'-8" LONG

GUARDRAIL BOLT "A" AND RECESSED NUT (TYP.), FOUR (4) REQUIRED

6 X 6-#6 X #6 OR 6 X 6-#6 X #6 WELDED WIRE FABRIC

24" DIA. CONCRETE FOUNDATION, 3'-0" DEEP

HEX NUT AND BOLT "F", 1 1/2" LONG (TYP.), EIGHT (8) REQUIRED

STANDARD WOOD OR STEEL POST "W" BEAM GUARDRAIL INSTALLATION

GALVANIZED STEEL (EG. COMMON NAIL DRIVE NAIL AT CENTER OF POST AND BLOCKOUT AFTER BOLT IS INSTALLED)

6'-3" TO NEXT POST

6'-3"

WELDED WIRE FABRIC

6 X 6-#6 X #6 OR 6 X 6-#6 X #6 WELDED WIRE FABRIC

24"

3"

8" X 8" STEEL BLANKING PLATE, 1/8" THICK

POST SLEEVE

5 1/2" X 7 1/2" WOOD TERMINAL POST, 5'-8" LONG

CABLE ANCHOR ASSEMBLY

POLYSTYRENE SHEETING, COMPOSITION PAPER OR SOME OTHER MATERIAL SHALL BE WRAPPED AROUND THE POST TO PROTECT THE POST TO AID IN REMOVING BROKEN OR DAMAGED POSTS.

6 X 6-#6 X #6 OR 6 X 6-#6 X #6 WELDED WIRE FABRIC

24"

3"

ROUNDED "W" BEAM END SECTION

NOTE: THE CROSS-SECTIONAL DIMENSIONS FOR THIS PART ARE TO FIT OVER THE STANDARD "W" BEAM SECTION.

GENERAL NOTES:

1. THIS ANCHORAGE MAY ONLY BE USED ON THE TRAILING END OF A BARRIER WHICH IS NOT EXPOSED TO VEHICULAR IMPACT.
2. GUARDRAIL SHALL MEET THE REQUIREMENTS OF ASHTO M 118A, CLASS A, TYPE 1 UNLESS OTHERWISE DESIGNATED.
3. ALL WOOD PILES AND BLOCKOUTS SHALL BE TREATED TIMBER IN ACCORDANCE WITH MISSISSIPPI DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS.
4. CONCRETE FOR THE CONCRETE FOUNDATION MAY BE EITHER CLASS "B" STRUCTURAL CONCRETE OR AN APPROVED COMMERCIAL PRE-MIXED CONCRETE. THE CONCRETE SHALL BE PLACED IN A 24" DIA. CONCRETE FOUNDATION SHALL CONFORM TO ASHTO M 221/V 221M AND ASHTO M 55/M 55.
5. FOR DETAILS OF HARDWARE AND COMPONENTS NOT FOUND ON THIS SHEET, SEE SHEET GR-11H.
6. DETAILS PERTAINING TO THE STANDARD INSTALLATION OF "W" BEAM SECTIONS WILL BE FOUND ON SHEET GR-11 FOR WOOD POSTS, AND GR-1B FOR STEEL POSTS.
7. FOR OTHER DETAILS OF POSTS, POST ACCESSORIES, FASTENERS AND RAIL ELEMENTS, SEE ASHTO-M 118A, JOINT TASK FOR NO. 13, LATEST EDITION. REFER TO STANDARDIZED HIGHWAY BARRIER HARDWARE, LATEST EDITION.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DIVISION
STANDARD PLAN

**GUARDRAIL:
TYPE 1 CABLE ANCHORAGE
(CONCRETE FOOTING)**

REVISION
DATE

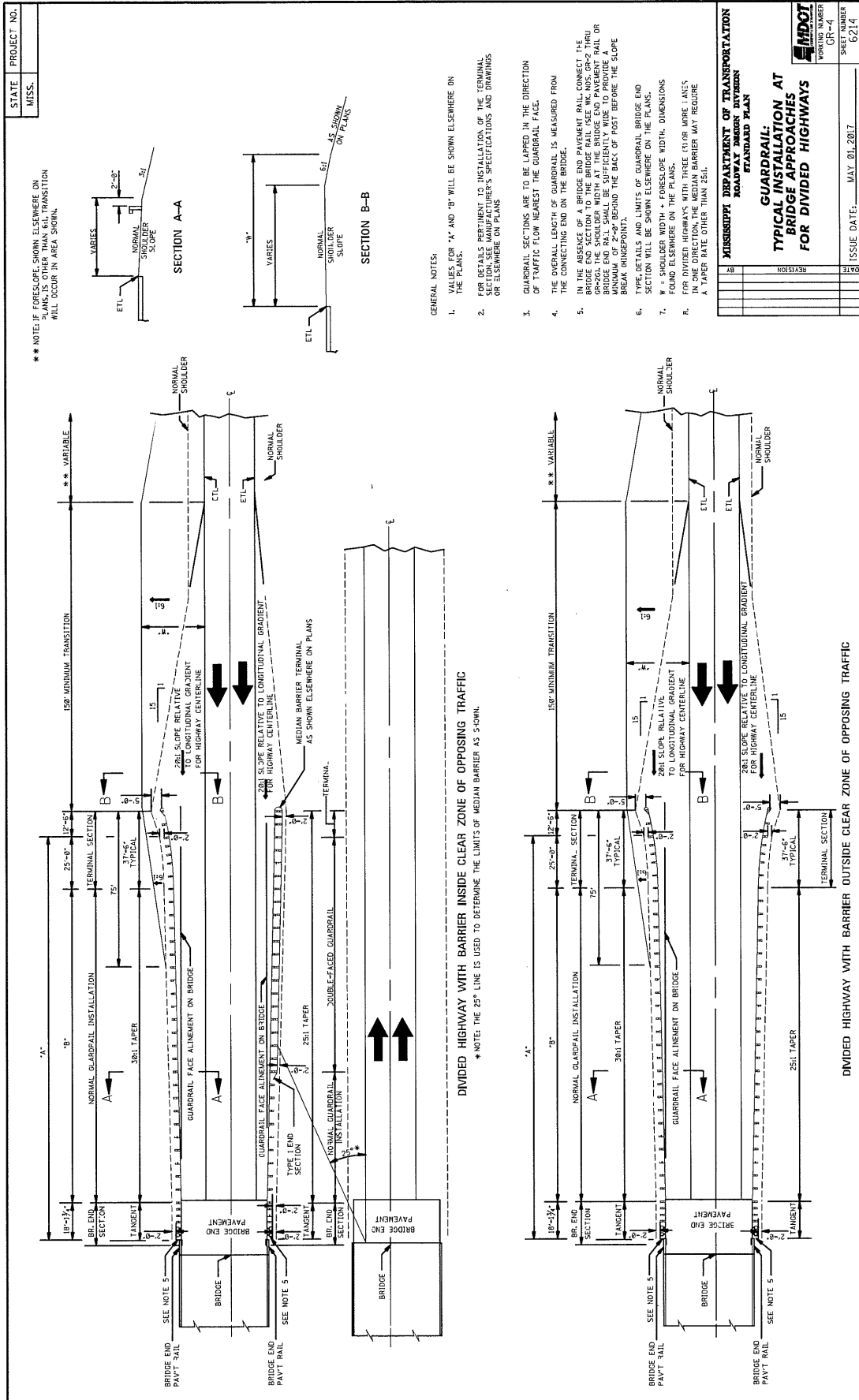
ISSUE DATE: MAY 01, 2012

SHEET NUMBER 6213

PROJECT NUMBER

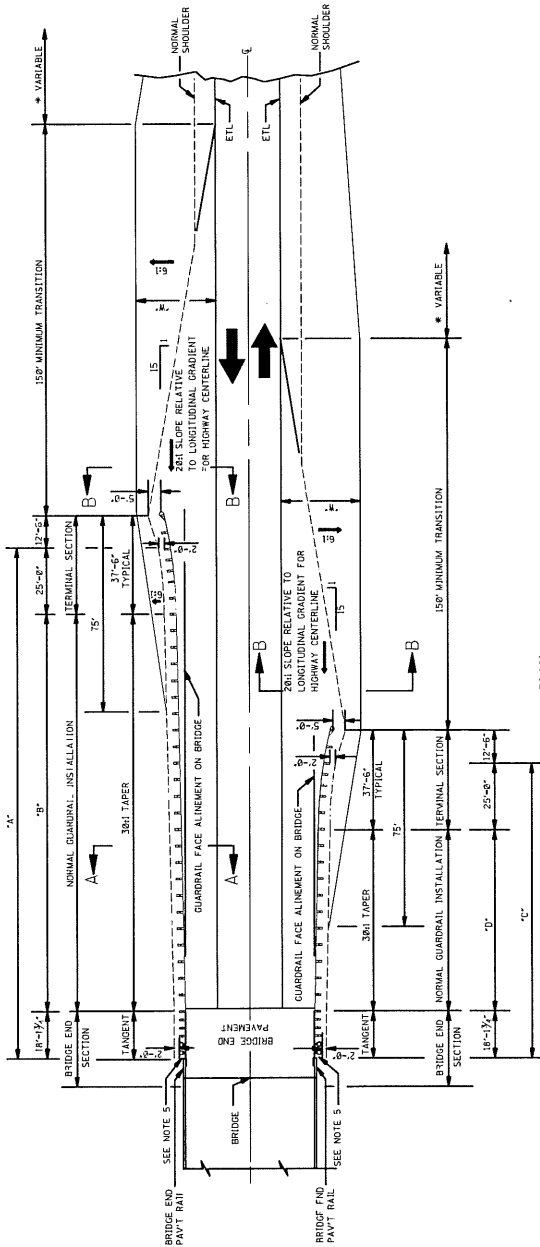
SECTION A-A

NOTE: FORM A NOMINAL 6" X 8" SOCKET IN THE FOUNDATION TO RECEIVE THE POST. THE POST SHALL BE INSTALLED WITH POLYSTYRENE SHEETING OR PAPER WRAPPED AROUND THE TIMBER POST IN A DOUBLE LAYER OF COMPOSITION PAPER. THE LAYER OF SHEETING OR PAPER WILL AID IN REMOVING A DAMAGED POST.

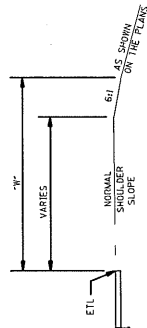


STATE	PROJECT NO.
MISS.	

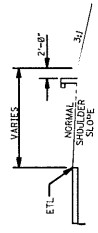
* NOTE: IF FORESLOPE SHOWN ELSEWHERE ON PLANS, IS OTHER THAN 6:1, TRANSITION WILL OCCUR IN AREA SHOWN.



PLAN



SECTION B-B



SECTION A-A

DETAIL OF GUARDRAIL SECTION LAPS



GENERAL NOTES:

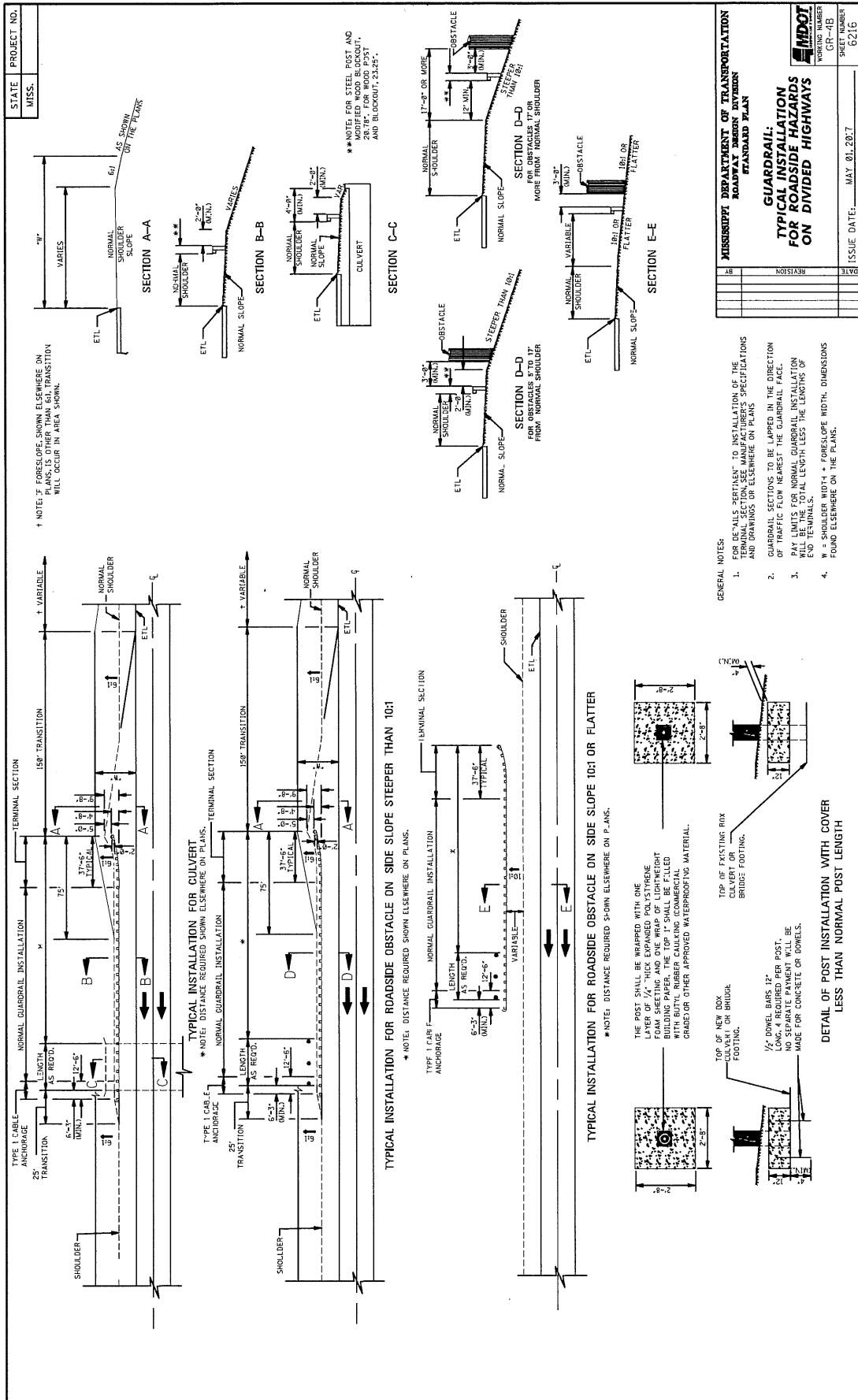
- VALUES FOR "A", "B", "C" AND "D" WILL BE SHOWN ELSEWHERE ON THE PLANS.
- FOR DETAILS PERTAINING TO INSTALLATION OF THE TERMINAL SECTION, SEE MANUFACTURER'S SPECIFICATIONS AND DRAWINGS OR ELSEWHERE ON PLANS.
- GUARDRAIL SECTIONS ARE TO BE LAPPED IN THE DIRECTION OF TRAFFIC APPROACHING THE BRIDGE.
- THE OVERALL LENGTH OF GUARDRAIL IS MEASURED FROM THE CONNECTING END ON THE BRIDGE.
- IN THE ABSENCE OF A BRIDGE END PAVEMENT RAIL, CONNECT THE BRIDGE END SECTION TO THE BRIDGE RAIL (SEE W. MISS. GR-2 THRU GR-20). THE SHOULDER WIDTH AT THE BRIDGE END PAVEMENT RAIL OR BRIDGE RAIL SHALL BE A MINIMUM OF 2'-0" BEHIND THE BACK OF POST BEFORE THE SLOPE BREAK (HINGEPOINT).
- TYPE DETAILS AND LIMITS OF GUARDRAIL BRIDGE END SECTION WILL BE SHOWN ELSEWHERE ON THE PLANS.
- W = SHOULDER WIDTH + FORESLOPE WIDTH. DIMENSIONS FOUND ELSEWHERE ON THE PLANS.

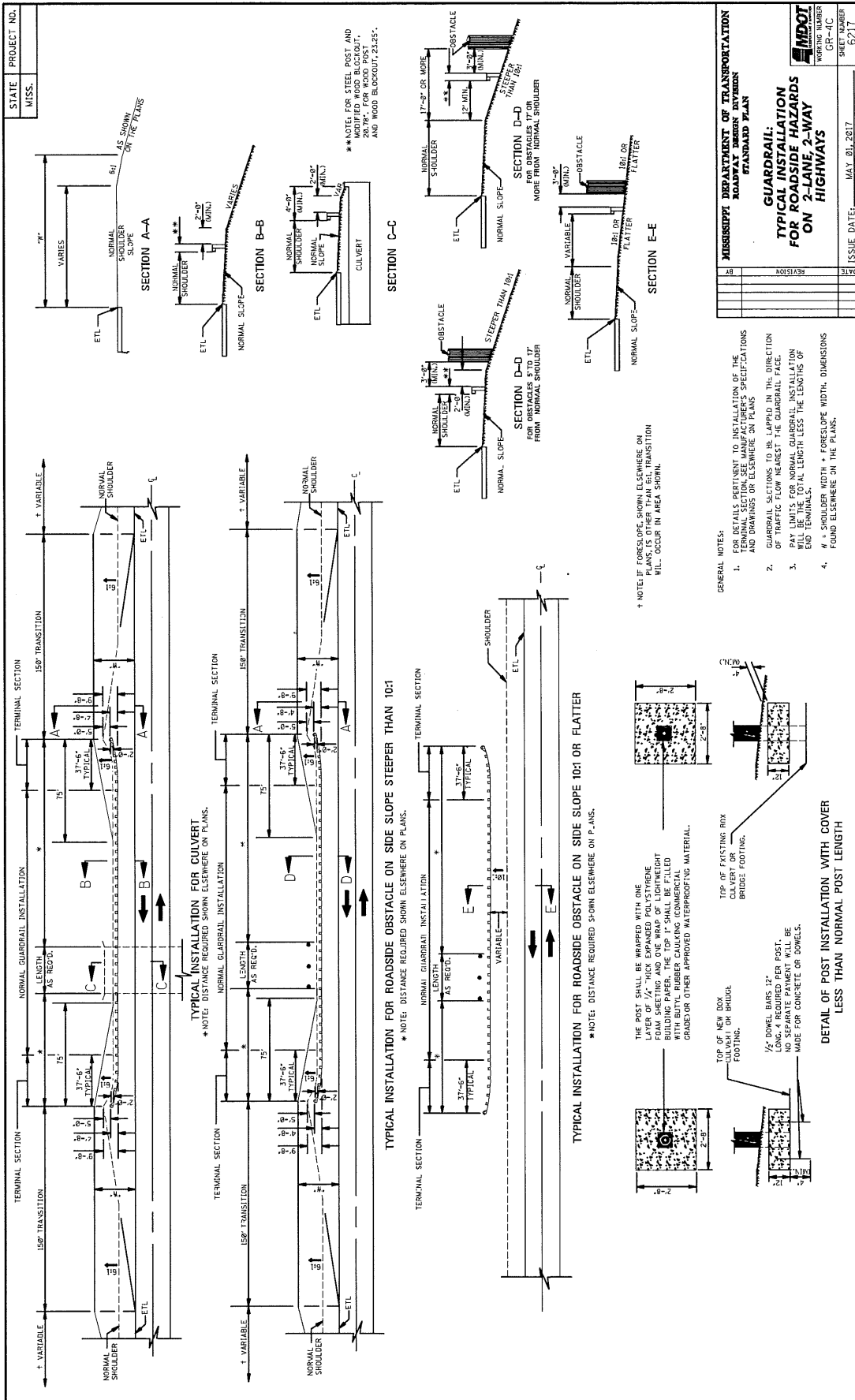
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**GUARDRAIL:
TYPICAL INSTALLATION
AT BRIDGE APPROACHES
FOR 2-LANE, 2-WAY
HIGHWAY**

WORKING NUMBER: GR-4A
SHEET NUMBER: 6215
ISSUE DATE: MAY, 01, 2017

AB	NO.	REVISION





MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 3599

CODE: (SP)

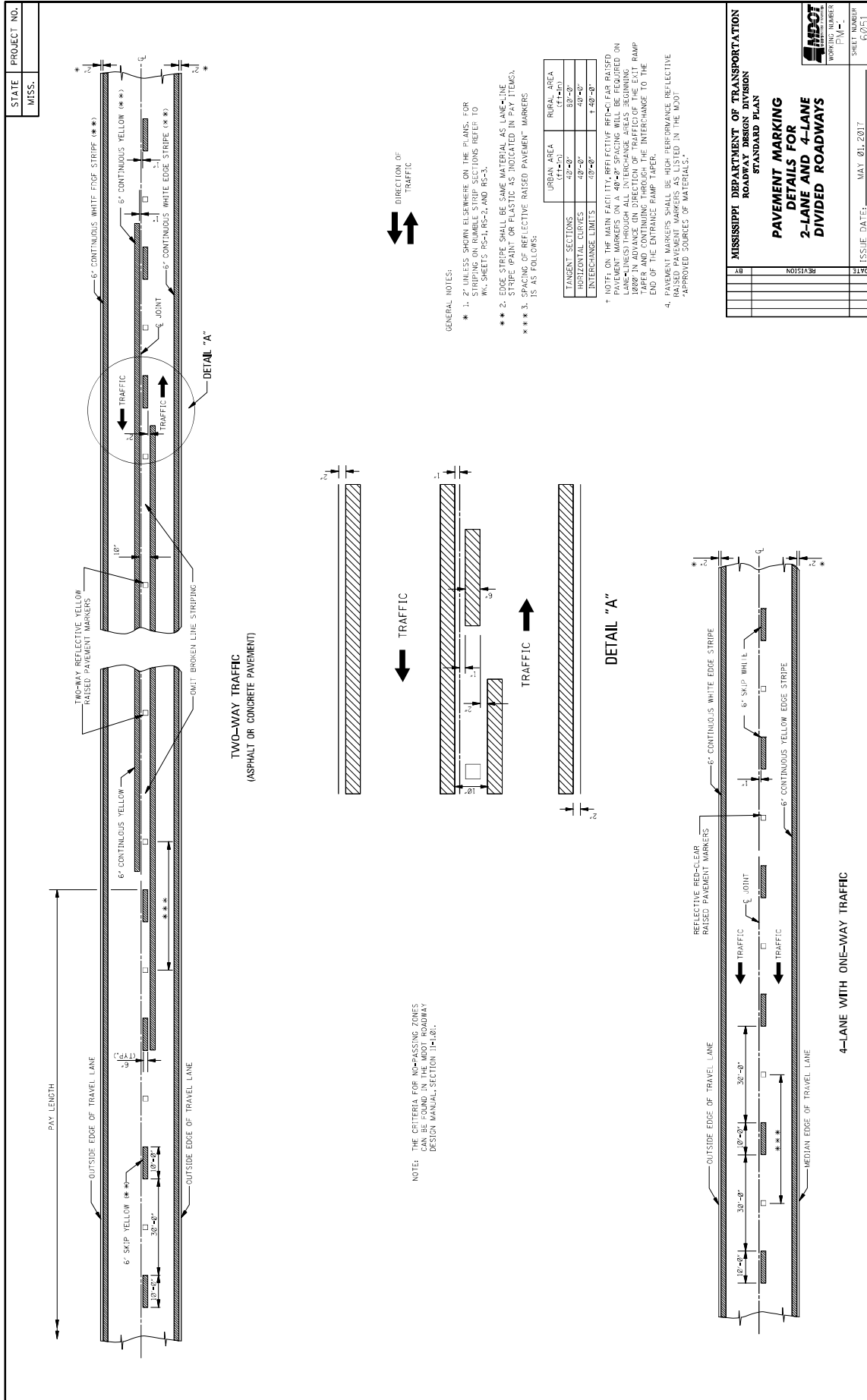
DATE: 08/11/2021

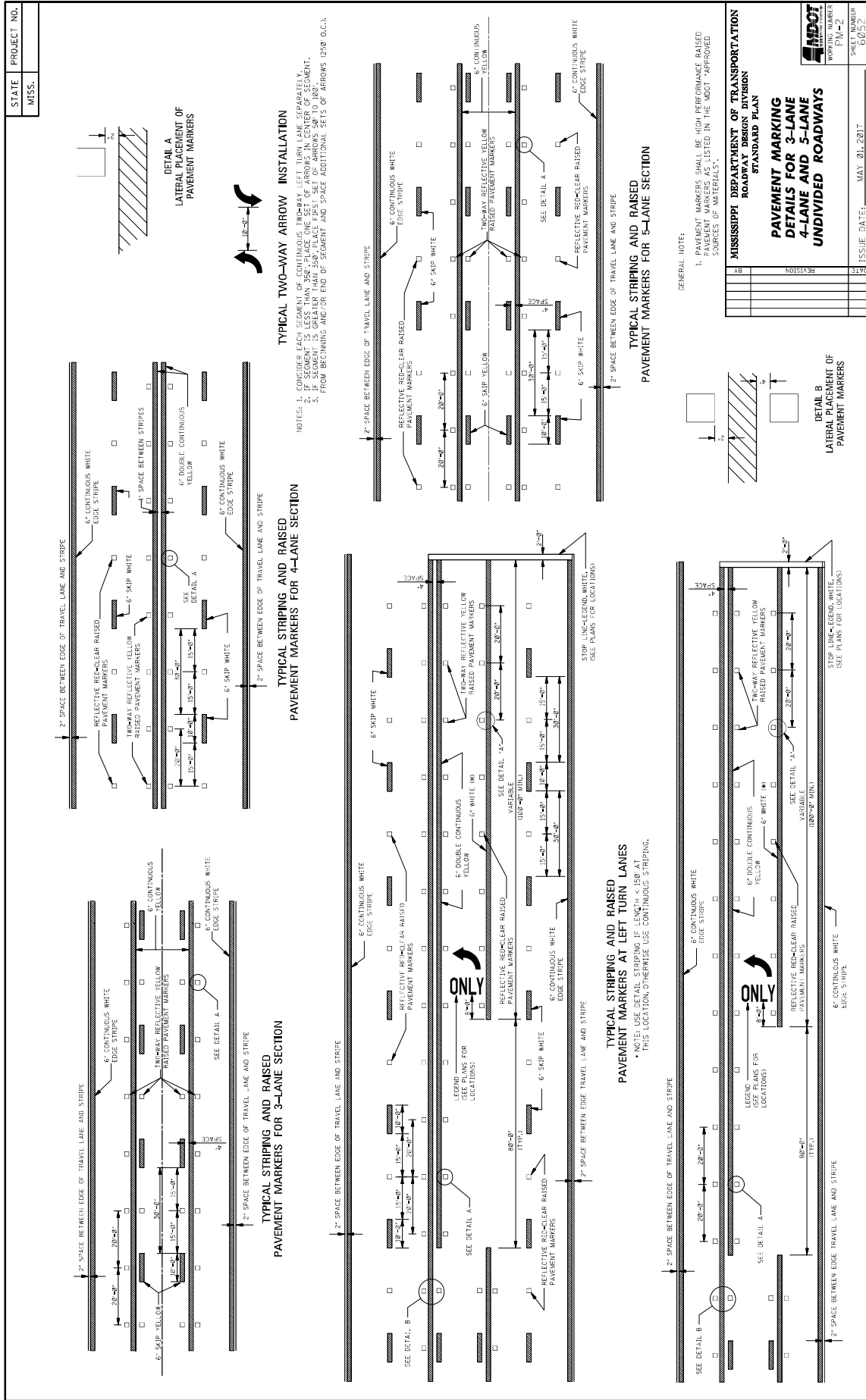
SUBJECT: Standard Drawings

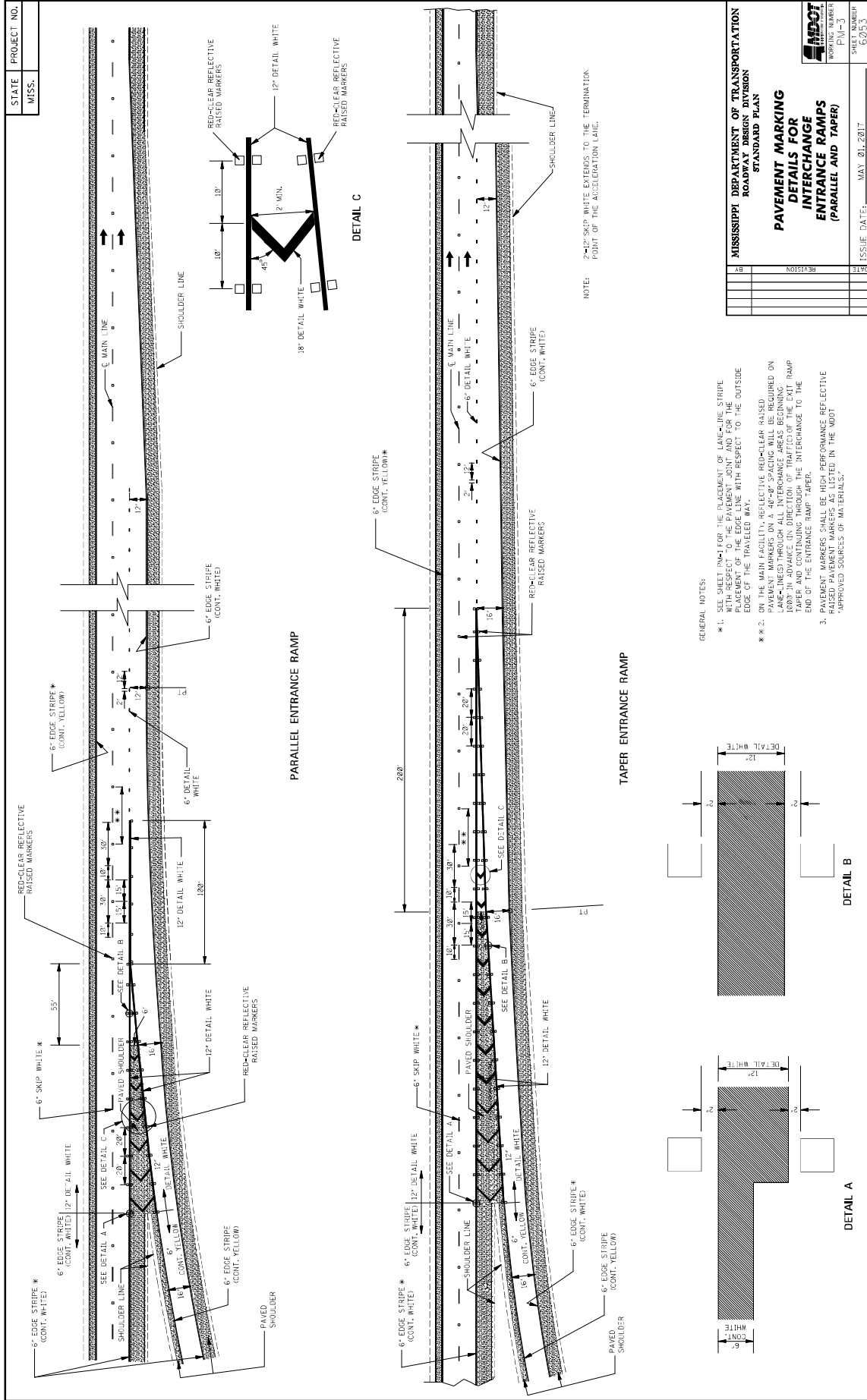
Standard Drawings attached hereto shall govern appropriate items of required work.

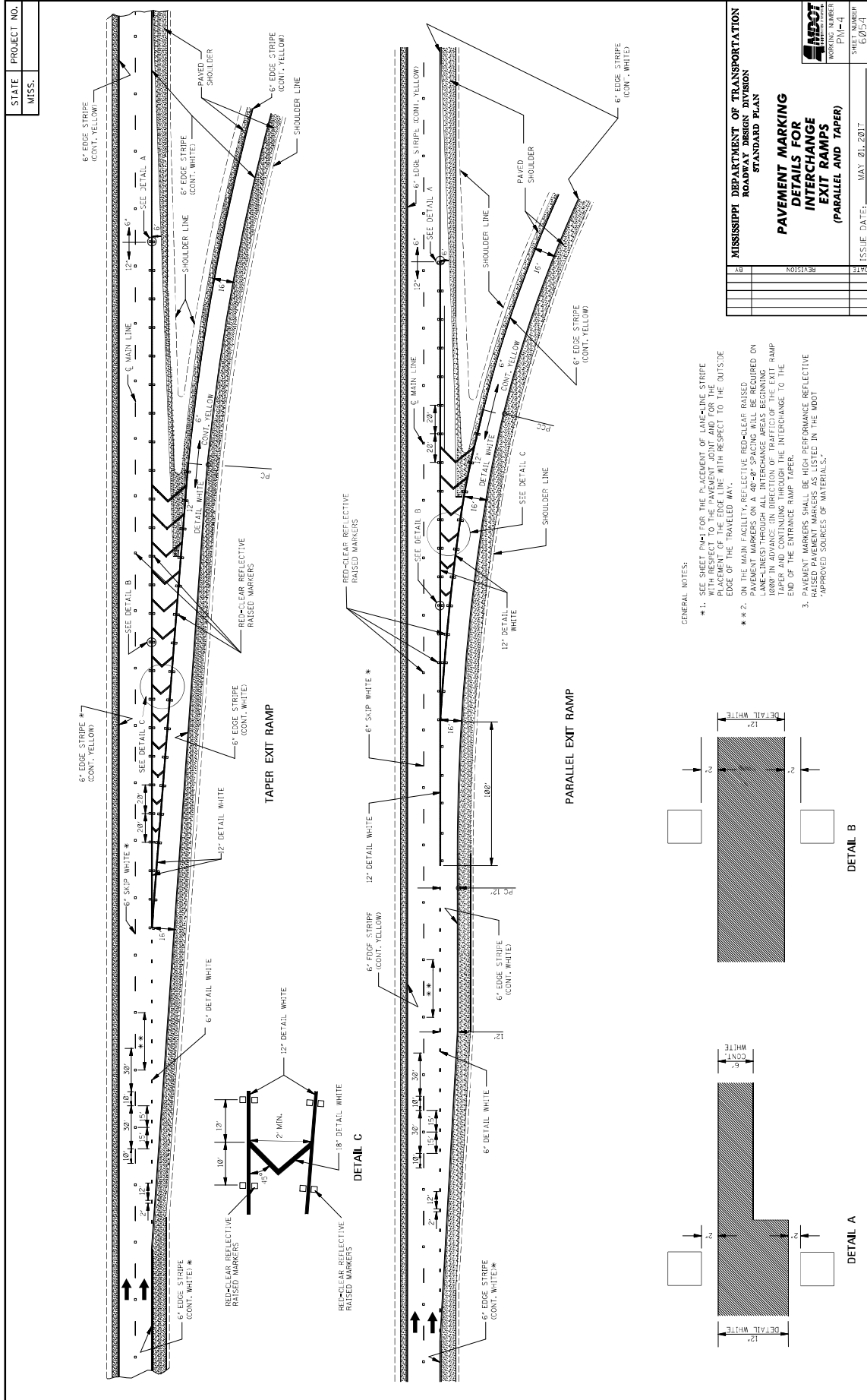
Larger copies of Standard Drawings may be purchased from:

MDOT Plans Print Shop
MDOT Shop Complex, Building C, Room 114
2567 North West Street
P.O. Box 1850
Jackson, MS 39215-1850
Telephone: (601) 359-7460
or FAX: (601) 359-7461
or e-mail: plans@mdot.state.ms.us





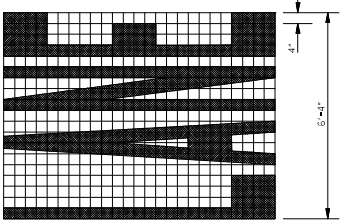




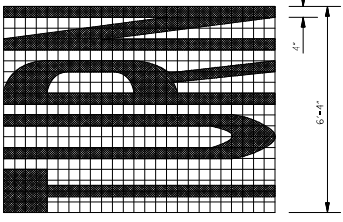
- GENERAL NOTES:
- * 1. SEE SHEET PAV-1 FOR THE PLACEMENT OF LANE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACE OF THE PAVED SHOULDER WITH RESPECT TO THE OUTSIDE EDGE OF THE FINISHED MARKING. RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-84" SPACING WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE IN DIRECTION OF TRAFFIC OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 - * 2. RAISED PAVEMENT MARKERS BE HIGH PERFORMANCE REFLECTIVE 'APPROVED SOURCES OF MATERIALS.'

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY DESIGN DIVISION	
STANDARD PLAN	
PAVEMENT MARKING	
INTERCHANGE	
EXIT RAMP	
(PARALLEL AND TAPER)	
SHEET NUMBER	ISSUE DATE: MAY 01, 2017
PL-4	
WORKING NUMBER	
62/241	

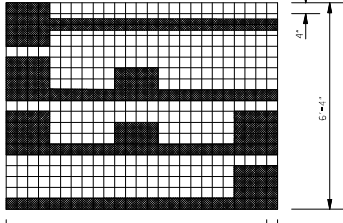
STATE MISS.	PROJECT NO.										
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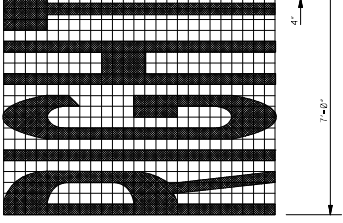
8'-4" x 6'-4" x 4"



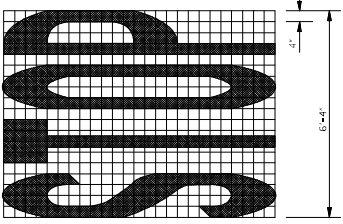
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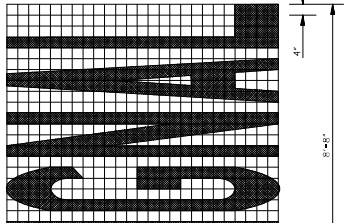
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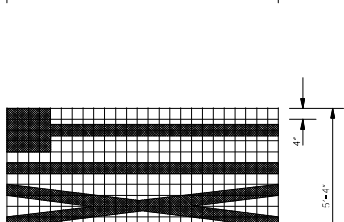
8'-4" x 7'-0" x 4"



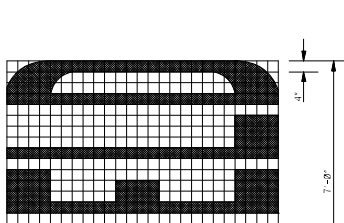
8'-4" x 6'-4" x 4"



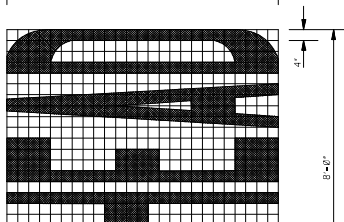
8'-4" x 6'-0" x 4"



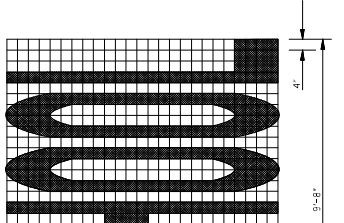
8'-4" x 5'-4" x 4"



8'-4" x 7'-0" x 4"



8'-4" x 8'-0" x 4"



8'-4" x 9'-8" x 4"

GENERAL NOTES:

- TWO HORIZONTAL GAPS (USED BY TEMPLATE CONNECTIONS) OF 1/4" SHALL BE EXTENDING FULL WIDTH OF RESPECTIVE LETTER.
- FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

LEGEND	AREA (ft ²)
STOP	24.6
RIGHT	28.6
LEFT	19.5
YIELD	27.2
AHEAD	32.4
YIELD	26.8
EXIT	18.5
SIGNAL	32.5
SCHOOL	35.2

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
LEGEND DETAILS**

DATE	REVISION	BY

ISSUE DATE: MAY 01, 2017
SHEET NUMBER: PM-5
PROJECT NUMBER: 60535

STATE MISS.	PROJECT NO.		
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PAVING ONLY

TURN ARROW

THRU ARROW

YIELD LINE

LANE-REDUCTION ARROW

1-WAY ARROW

COMBINATION ARROW

GENERAL NOTES:

- TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTIONS OF 1/4" OR LESS AND EXTENDING THE FULL WIDTH) ARE PERMITTED IN EACH LETTER.
- FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- DIMENSIONS OF THE YIELD LINE MAY VARY WITH APPROVAL OF THE ENGINEER. SEE MUTCD, LATEST EDITION, FOR ALLOWABLE DIMENSIONS.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

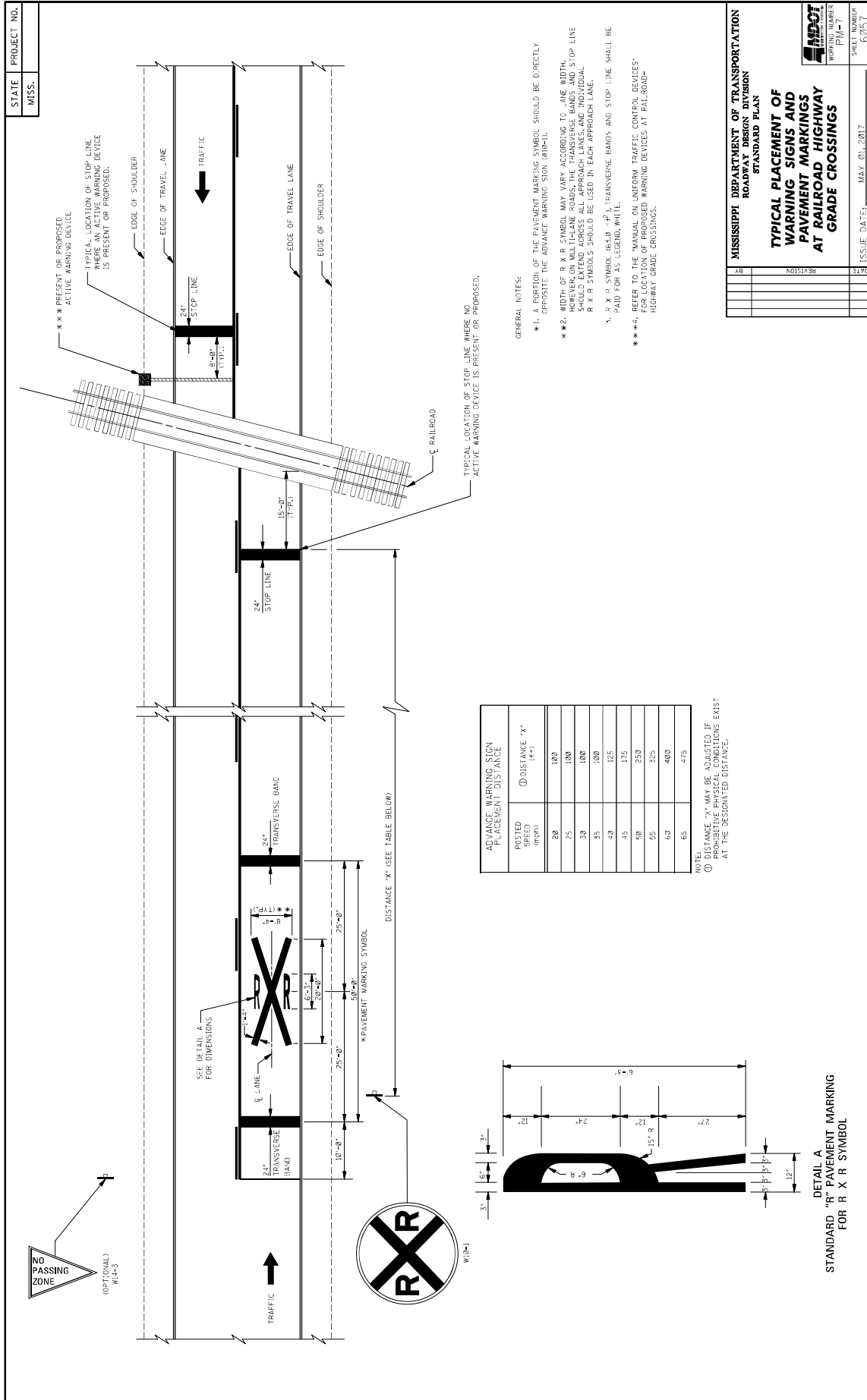
PAY QUANTITIES	
LEGEND/SYMBOL	AREA (ft ²)
ONLY	22.0
TURN ARROW	16.4
THRU ARROW	12.3
COMB. ARROW	27.5
1-WAY ARROW	24.3
LANE REDUCTION ARROW	40.0

BY	DATE

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN**

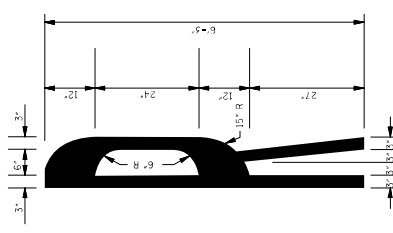
**PAVEMENT MARKING
LEGEND DETAILS**

SHEET NUMBER PM-6	ISSUE DATE: MAY 01, 2017	SHEET NUMBER 60/56			
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POSTED SPEED (mph)	① DISTANCE 'X' (ft)
20	100
25	100
30	100
35	100
40	125
45	175
50	250
55	325
60	400
65	475

NOTE: DISTANCE 'X' MAY BE SHORTER IF PROTECTIVE PHYSICAL CONDITIONS EXIST AT THE DESIGNATED DISTANCE.



DETAIL A
STANDARD "R" PAVEMENT MARKING
FOR R X R SYMBOL

- GENERAL NOTES:
- **1. A PORTION OF THE PAVEMENT MARKING SYMBOL SHOULD BE DIRECTLY OPPOSITE THE ADVANCE WARNING SIGN (W14-3).
 - **2. WIDTH OF R X R SYMBOL MAY VARY ACCORDING TO LANE WIDTH. SYMBOL SHOULD EXTEND ACROSS ALL APPROACH LANES AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.
 - **3. R X R SYMBOL (65.8 - 71.1) TRANSVERSE BANDS AND STOP LINE SHALL BE PAID FOR AS LEGEND WHITE.
 - **4. REFER TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR LOCATION OF PROPOSED WARNING DEVICES AT RAILROAD-HIGHWAY GRADE CROSSINGS.

STATE PROJECT NO.
MISS.

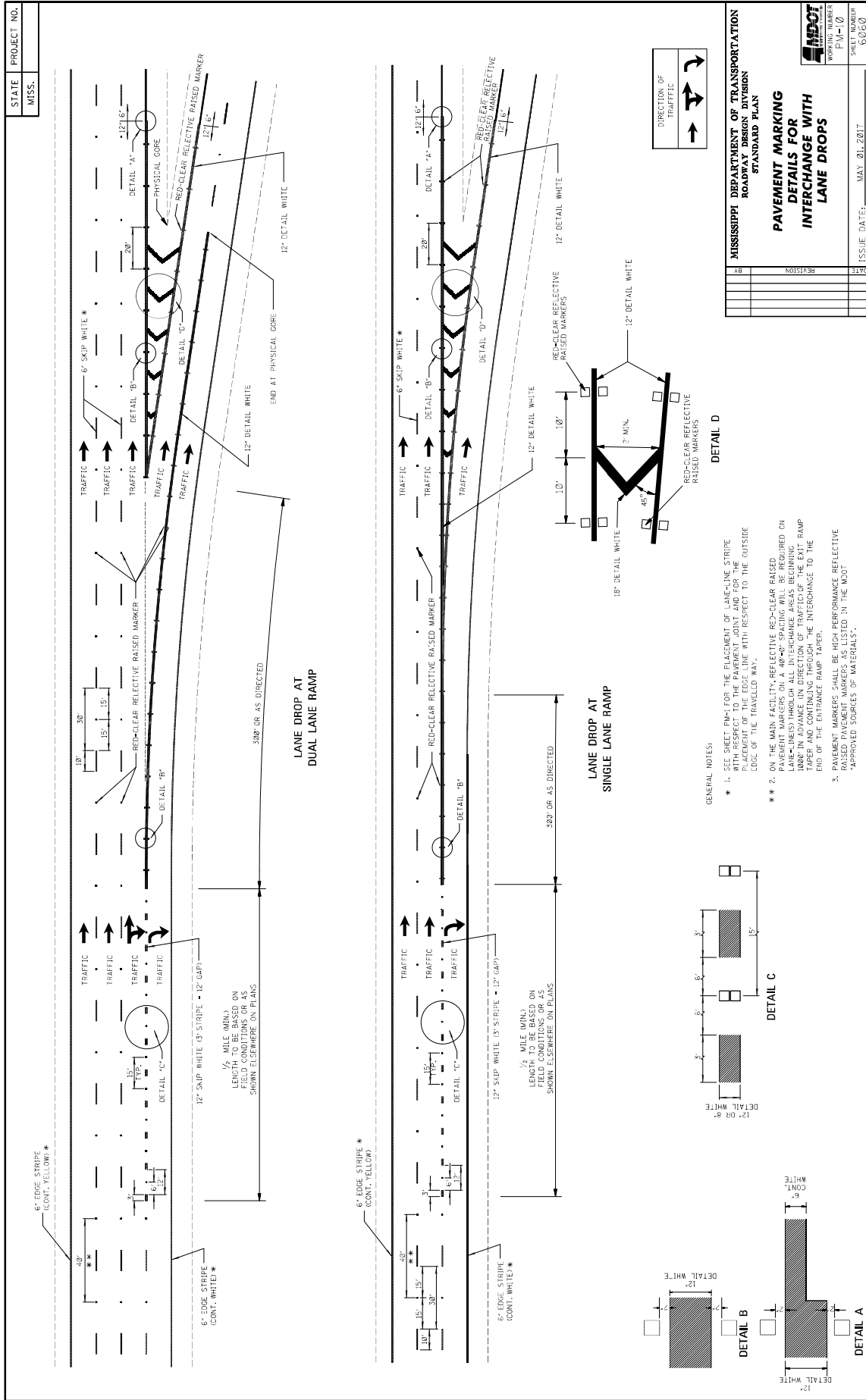
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

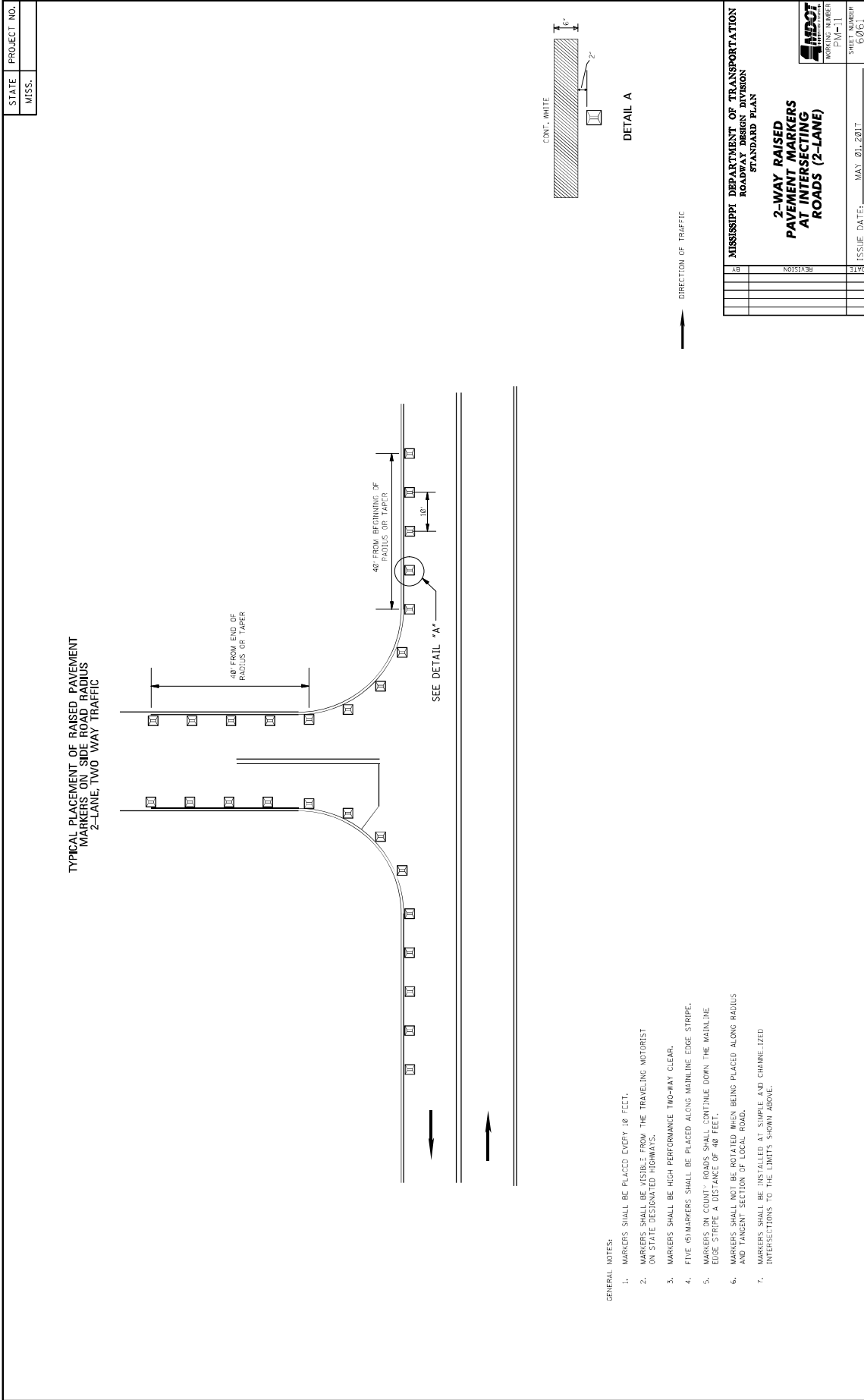
TYPICAL PLACEMENT OF WARNING SIGNS AND PAVEMENT MARKINGS AT RAILROAD HIGHWAY GRADE CROSSINGS

MDOT
WORKING NUMBER
P10-7

SHEET NUMBER
60511

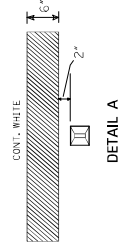
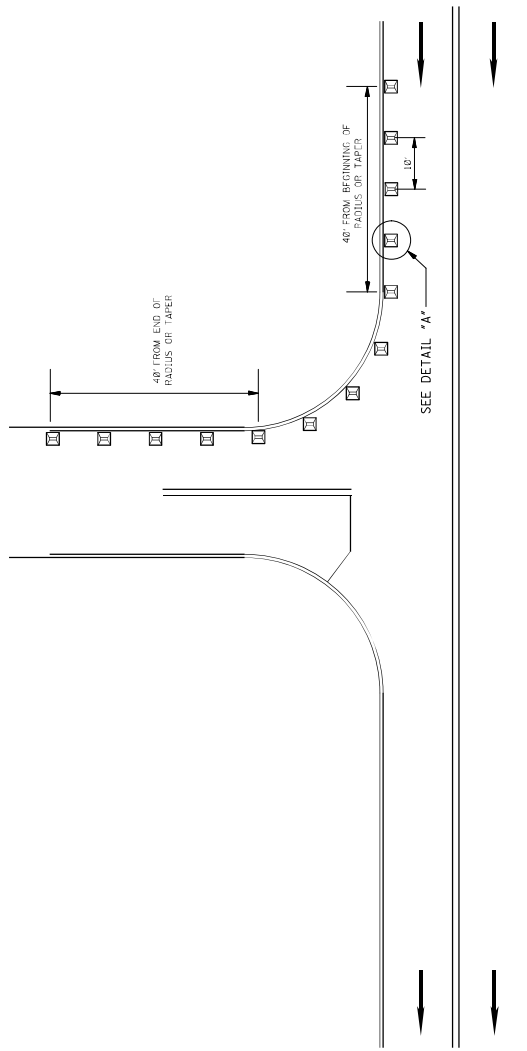
ISSUE DATE: MAY 01, 2017





STATE	PROJECT NO.
MISS.	

TYPICAL PLACEMENT OF RAISED PAVEMENT MARKERS ON SIDE ROAD RADIUS
4-LANE, TWO WAY TRAFFIC



DETAIL A

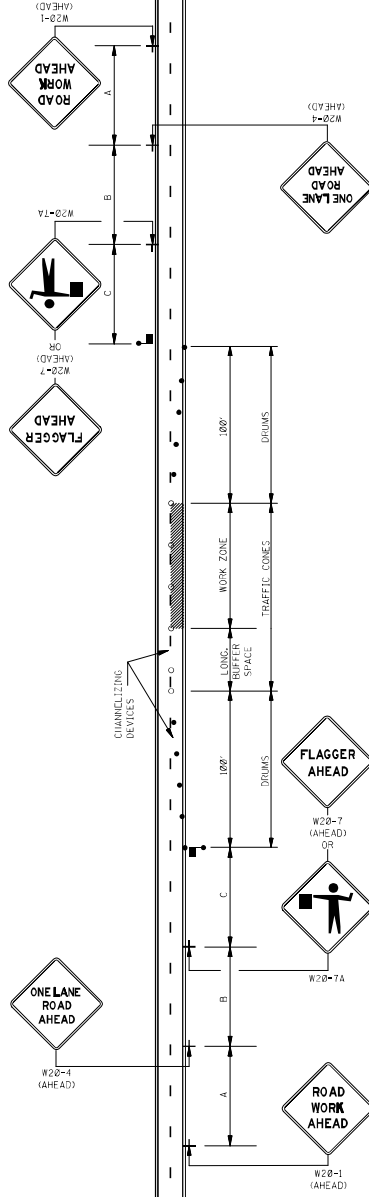
↑ DIRECTION OF TRAFFIC

GENERAL NOTES:

1. MARKERS SHALL BE FLACCO EVERY 10' FEET.
2. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
3. MARKERS SHALL BE HIGH PERFORMANCE TWO-WAY CLEAR.
4. FIVE (5) MARKERS SHALL BE PLACED ALONG MAINLINE EDGE STRIPE.
5. MARKERS ON COUNTY ROADS SHALL CONTINUE DOWN THE MAINLINE EDGE STRIPE A DISTANCE OF 40' FEET.
6. MARKERS SHALL NOT BE ROTATED WHEN BEING PLACED ALONG RADIUS AND TANGENT SECTIONS OF LOCAL ROAD.
7. MARKERS SHALL BE INSTALLED AT SIMPLE AND CHANNELIZED INTERSECTIONS TO THE LIMITS SHOWN ABOVE.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
2-WAY RAISED PAVEMENT MARKERS AT INTERSECTING ROADS (4-LANE)	
DATE	BY
REVISION	
ISSUE DATE:	MAY 01, 2017
SHEET NUMBER	69/62
WORKING NUMBER	P.M.-12

STATE PROJECT NO.
MISS.



- LEGEND**
- FLAGGER
 - RETROREFLECTIVE PREF-STANDING PLASTIC DRUMS
 - TRAFFIC CONES (28" HEIGHT MINIMUM)

DISTANCE BETWEEN SIGNS

ROAD TYPE	A	B	C
URBAN (35 MPH OR LESS)	100 FT.	100 FT.	100 FT.
URBAN (40 - 70 MPH)	250 FT.	250 FT.	350 FT.
RURAL	500 FT.	500 FT.	500 FT.
EXPRESSWAY / FREEWAY	1000 FT.	1500 FT.	2500 FT.

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE. FLAGGER STATIONS SHALL BE LOCATED SUCH THAT THE FLAGGER IS VISIBLY LOCATED TO THE TRAFFIC APPROACHING FROM THE STOPPING SIGHT DISTANCE COLUMN MAY BE USED AS A MINIMUM FOR THIS DISTANCE.
2. ALL CHANNELIZING DEVICES SHALL BE A MINIMUM OF 20" IN HEIGHT.
3. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 36" X 36" AND BLACK COPY ON FLUORESCENT ORANGE SHEETING.
4. WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED. ALL CHANNELIZING DEVICES SHALL BE MOVED TO THE SHOULDER EDGE.
5. ADDITIONAL FLAGGERS MAY BE NEEDED AT NIGHT. FLAGGER STATIONS SHALL BE ILLUMINATED.
6. WHEN WORK IS REQUIRED AT NIGHT, FLAGGER STATIONS SHALL BE ILLUMINATED.
7. CHANNELIZING DEVICE TYPES FOR:
 - A. APPROACH AND EXIT TAPERS= RETROREFLECTIVE PLASTIC DRUMS
 - B. ALONG LANE LINE AND WORK ZONE= TRAFFIC CONES (28" HEIGHT)
8. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

POSTED SPEED DESIGN SPEED (mph)	MAXIMUM CHANNELIZING DEVICE SPACING (FT)		STOPPING SIGHT DISTANCE (ft)
	TAPER LANE LINE & WORK ZONE	CONDITIONAL BUFFER SPACE (ft)	
50	20	80	305
45	20	90	320
50	20	100	425
55	20	110	495
60	20	120	570
65	20	130	645
70	20	140	730

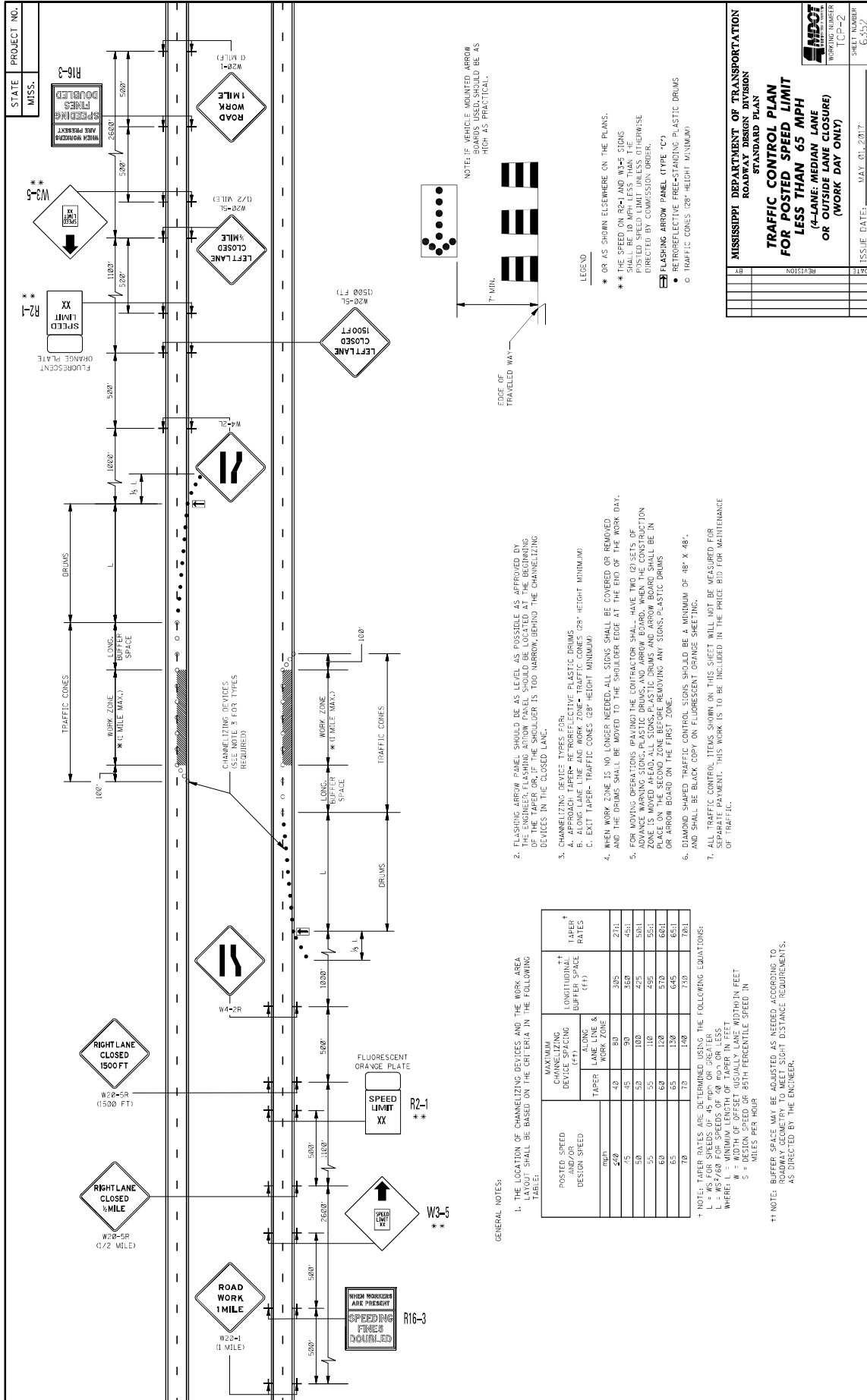
NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

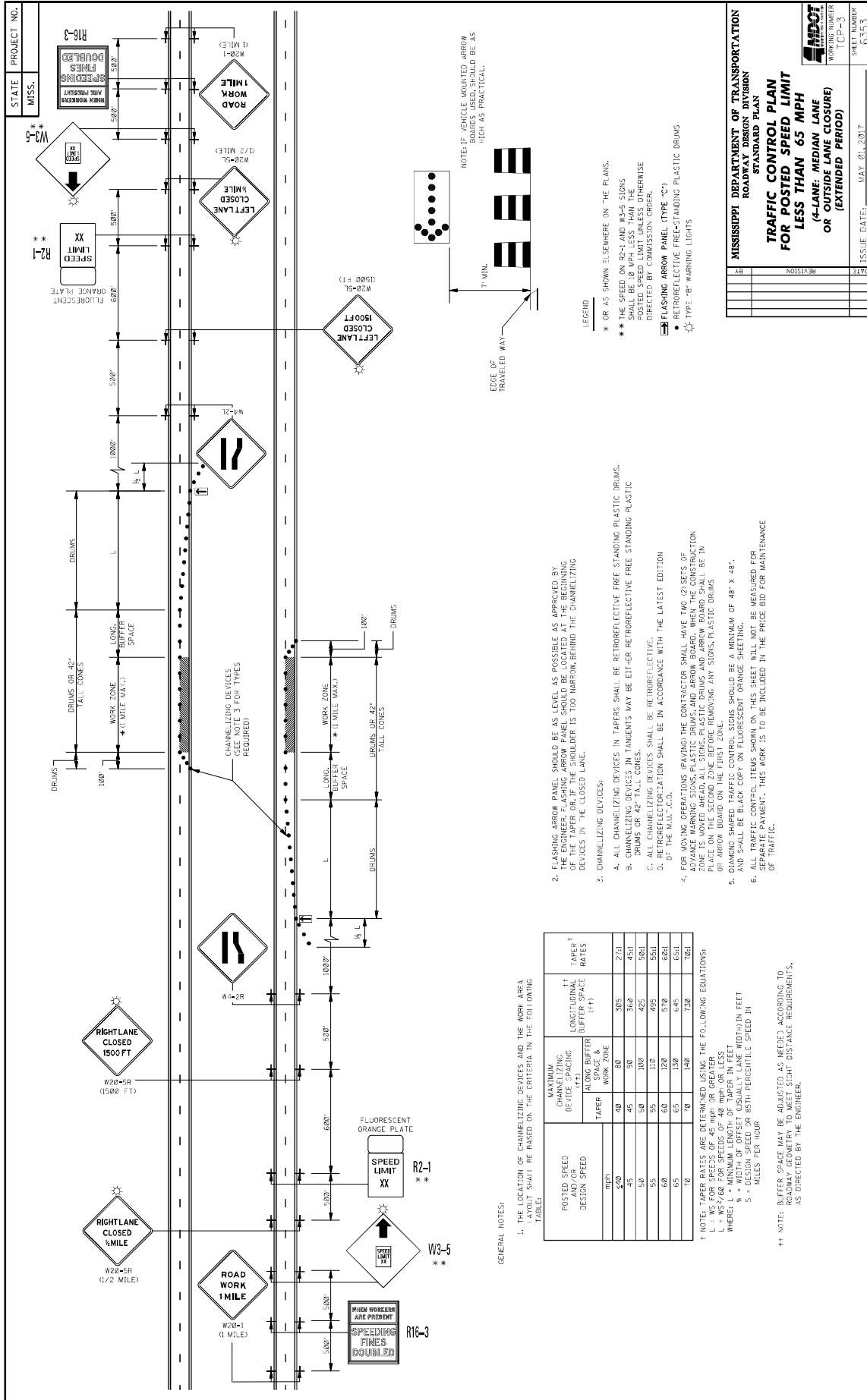
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

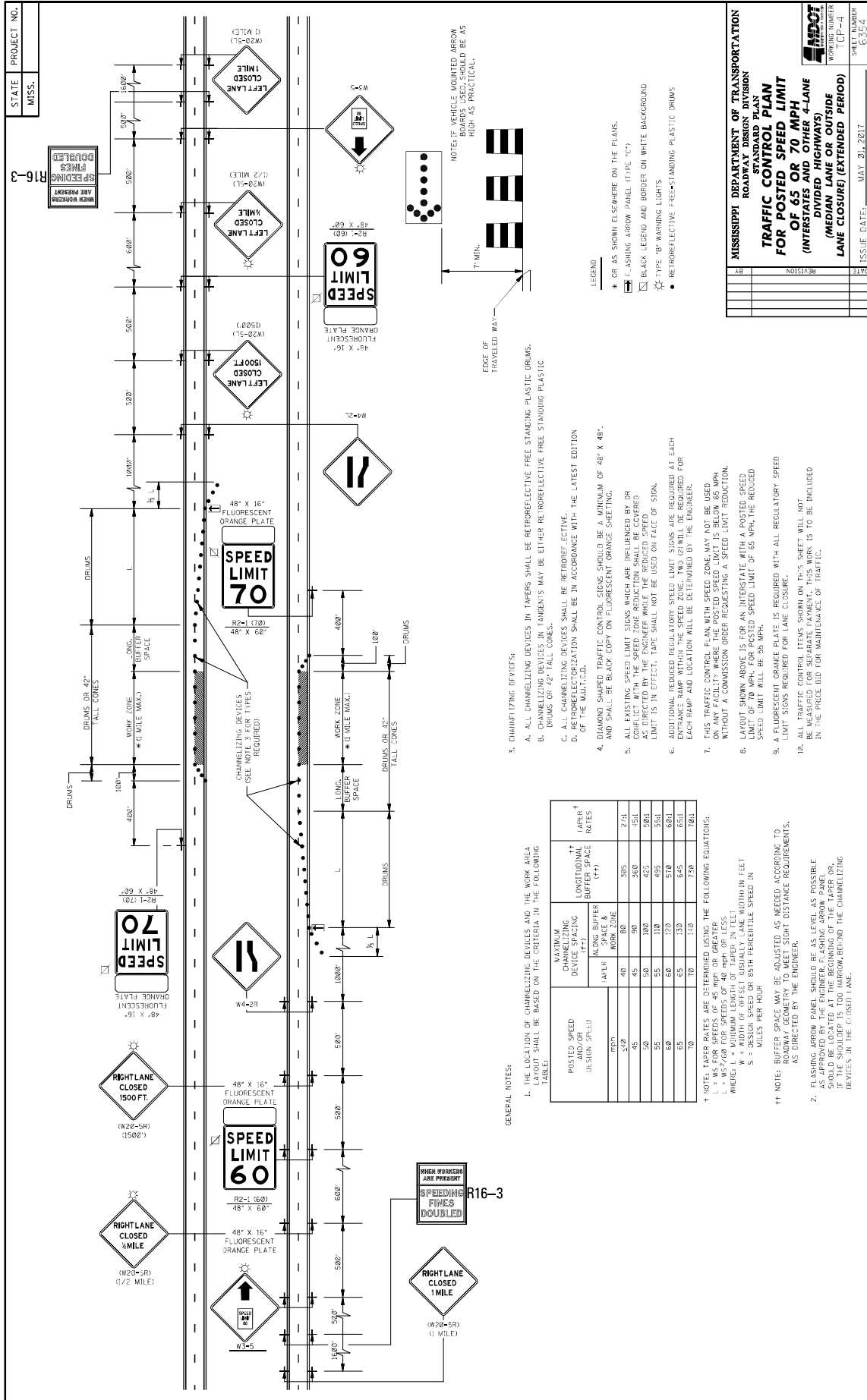
**TRAFFIC CONTROL PLAN
WITH FLAGGER
(ONE-LANE CLOSURE OF
TWO-WAY TRAFFIC)**

ISSUE DATE: MAY 01, 2017

WORKING NUMBER [CP-1]
SHEET NUMBER 6351







STATE PROJECT NO.
MISS. R16-3

WHEN WORKERS ARE WORKING SPEEDING FINES ARE DOUBLED

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
TRAFFIC CONTROL PLAN
FOR POSTED SPEED LIMIT
OF 65 OR 70 MPH
(INTERSTATES AND OTHER 4-LANE
DIVIDED HIGHWAYS)
(MEDIUM LANE OR OUTSIDE
LANE CLOSED/EXTENDED PERIOD)

ISSUE DATE: MAY 20, 2012

REV. NO. REVISION

SCALE: AS SHOWN

WORKING NUMBER: CP-44

SHEET NUMBER: 6554

- GENERAL NOTES:
1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA TAPER SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:
- | POSTED SPEED
DESIGN SPEED | MAXIMUM
CHANNELIZING
DEVICE SPACING | | LONGITUDINAL
BUFFER SPACE
(FT) | TAPER
RATES |
|------------------------------|---|-----------------|--------------------------------------|----------------|
| | ALONG
SPACE &
WORK ZONE | ACROSS
SPACE | | |
| 50 | 40 | 80 | 300 | 2/1 |
| 55 | 45 | 90 | 300 | 2/1 |
| 60 | 50 | 100 | 450 | 2/1 |
| 65 | 55 | 110 | 450 | 2/1 |
| 70 | 60 | 120 | 570 | 2/1 |
| 75 | 65 | 130 | 645 | 2/1 |
| 80 | 70 | 140 | 720 | 2/1 |
- NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 T = WS²/60 FOR SPEEDS OF 45 MPH OR GREATER
 L = WS²/60 FOR SPEEDS OF 40 MPH OR LESS
 WHERE: L = MINIMUM BUFFER SPACE (FEET)
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR
- NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.
2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR AT THE BEGINNING OF THE WORK ZONE, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. CHANNELIZING DEVICES:
 - A. ALL CHANNELIZING DEVICES IN TAPERS SHALL BE RETROREFLECTIVE FREE STANDING PLASTIC DRUMS.
 - B. CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER RETROREFLECTIVE FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.
 - C. ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE.
 - D. RETROREFLECTIVIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MUTCD-6A.
 4. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48" AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
 5. ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED WITH A BLACK LEADED AND BORDER ON WHITE BACKGROUND. LIMIT IS IN EFFECT. TAPE SHALL NOT BE USED ON FACE OF SIGN.
 6. ADDITIONAL REQUIRED REGULATORY SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP AND LEGALION WILL BE DETERMINED BY THE ENGINEER.
 7. THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
 8. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH FOR PASTED SPEED LIMIT OF 65 MPH. THE REDUCED SPEED LIMIT WILL BE 55 MPH.
 9. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS REQUIRED FOR LANE CLOSURE.
 10. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

11. THE TRAFFIC CONTROL PLAN WITH SPEED ZONE MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
12. LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH FOR PASTED SPEED LIMIT OF 65 MPH. THE REDUCED SPEED LIMIT WILL BE 55 MPH.
13. A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS REQUIRED FOR LANE CLOSURE.
14. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

NOTE: IF VEHICLE MOUNTED ARROW SHOULD BE AS HIGH AS PRACTICAL.

7' MIN.

EDGE OF TRAVELLED WAY

LEGEND

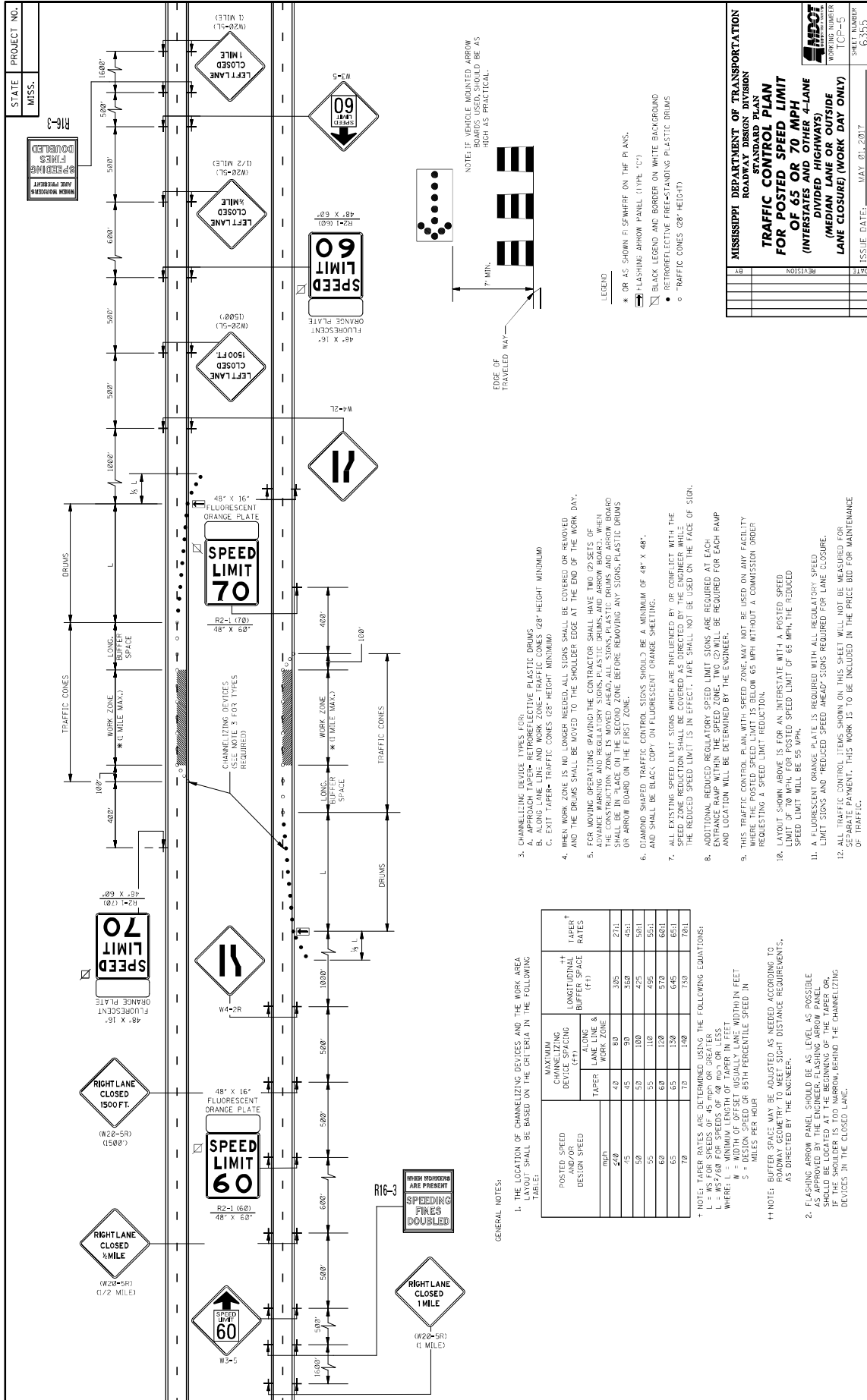
- * OR AS SHOWN ELSEWHERE ON THE PLANS.
- F. FLASHING ARROW PANEL (TYPE "C")
- BLACK LEADED AND BORDER ON WHITE BACKGROUND
- TYPE "B" MARKING LIGHTS
- RETROREFLECTIVE FREE STANDING PLASTIC DRUMS

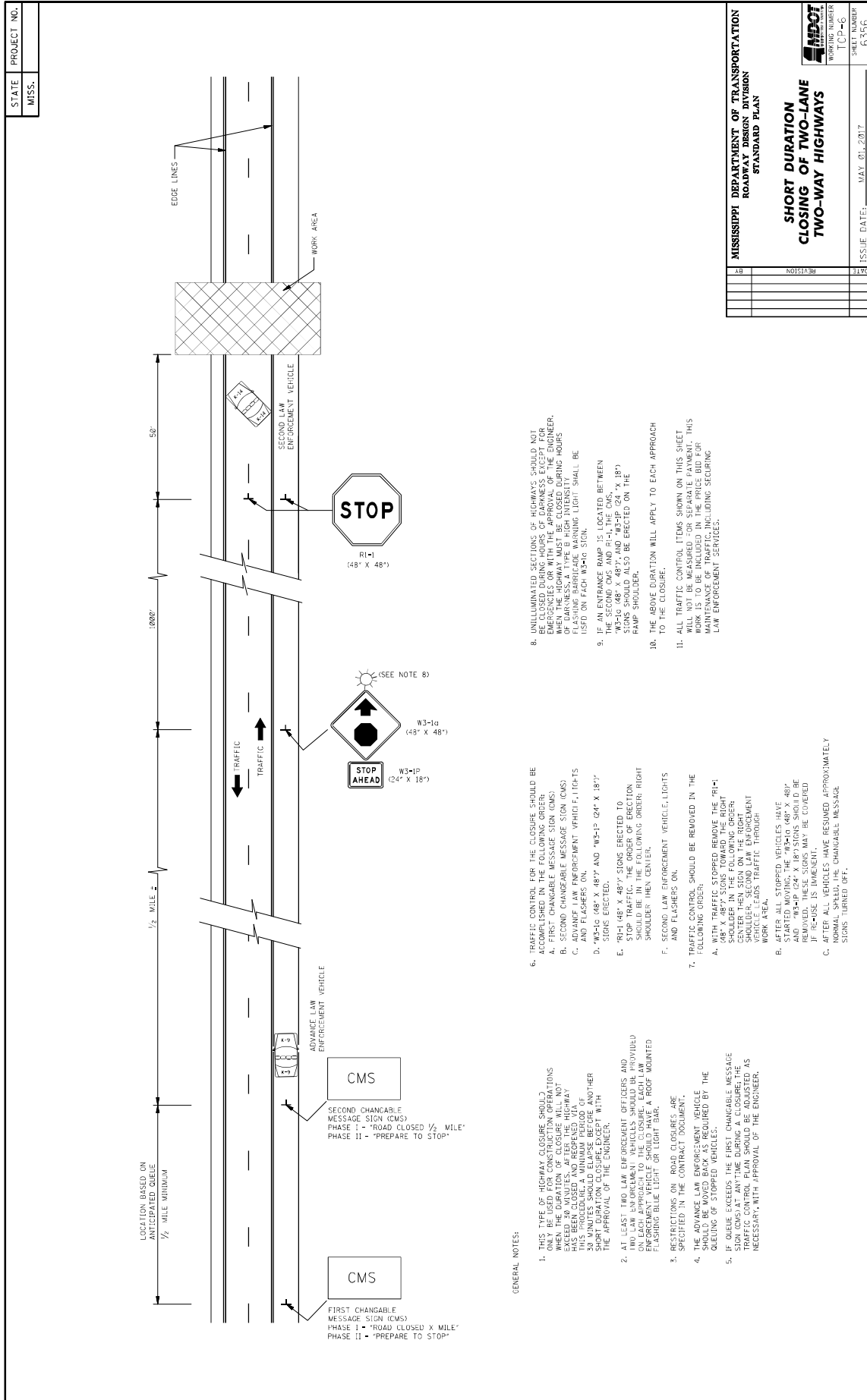
WHEN WORKERS ARE WORKING SPEEDING FINES ARE DOUBLED

R16-3

RIGHT LANE CLOSED 1 MILE

(W20-5R) (1 MILE)



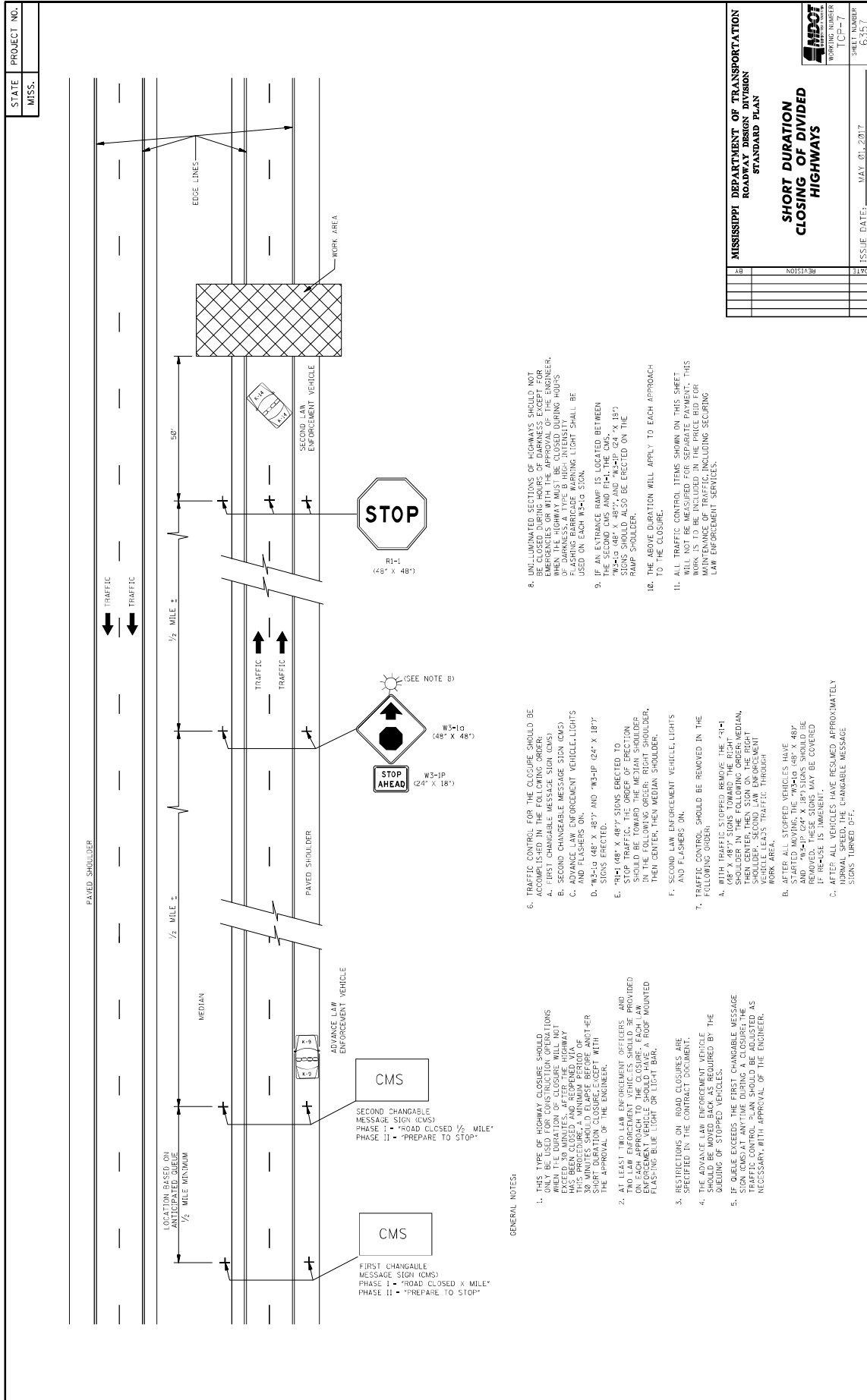


STATE	PROJECT NO.
MISS.	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
SHORT DURATION CLOSING OF TWO-LANE TWO-WAY HIGHWAYS	
ISSUE DATE:	MAY 01, 2017
SHEET NUMBER	6356
WORKING NUMBER	TC-P-6
REVISION	

GENERAL NOTES:

- THIS TYPE OF HIGHWAY CLOSURE SHOULD BE USED ONLY FOR SHORT DURATION CLOSURES WHEN THE DURATION OF CLOSURE WILL NOT EXCEED 30 MINUTES. AFTER THE HIGHWAY IS CLOSED, THE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD ELAPSE BEFORE ANOTHER ADVANCE LAW ENFORCEMENT VEHICLE WITH THE APPROVAL OF THE ENGINEER.
- AT LEAST TWO LAW ENFORCEMENT OFFICERS AND ONE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD BE ON EACH APPROACH TO THE CLOSURE. EACH LAW ENFORCEMENT VEHICLE SHOULD HAVE A ROOF MOUNTED FLASHING BLUE LIGHT ON LIGHT BAR.
- RESTRICTIONS ON ROAD CLOSURES ARE SPECIFIED IN THE CONTRACT DOCUMENT.
- THE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD BE POSITIONED TO BE TRIPPED BY THE QUEUING OF STOPPED VEHICLES.
- IF QUEUE EXCEEDS THE FIRST CHANGABLE MESSAGE SIGN (CMS) AT ANYTIME DURING A CLOSURE, THE TRAFFIC CONTROL PLAN SHOULD BE ADJUSTED AS NECESSARY, WITH APPROVAL OF THE ENGINEER.
- TRAFFIC CONTROL FOR THE CLOSURE SHOULD BE ACCOMPLISHED IN THE FOLLOWING ORDER:
 - FIRST CHANGABLE MESSAGE SIGN (CMS)
 - SECOND CHANGABLE MESSAGE SIGN (CMS)
 - ADVANCE LAW ENFORCEMENT VEHICLE (S) WITH FLASHING BLUE LIGHTS
 - "W3-1a (48" X 48") AND "W3-1P (24" X 18") SIGNS ERECTED.
 - "R1-1 (48" X 48") SIGNS ERECTED TO STOP TRAFFIC. THE ORDER OF ERECTION SHOULD BE IN THE FOLLOWING ORDER: RIGHT SHOULDER THEN CENTER.
 - SECOND LAW ENFORCEMENT VEHICLE, LIGHTS AND FLASHERS ON.
 - TRAFFIC CONTROL SHOULD BE REMOVED IN THE FOLLOWING ORDER:
 - WITH TRAFFIC STOPPED REMOVE THE "R1-1 (48" X 48") SIGNS TOWARD THE RIGHT SHOULDER THEN SIGN ON THE RIGHT SHOULDER. SECOND LAW ENFORCEMENT VEHICLE ADDS TRAFFIC THROUGH WORK AREA.
 - AFTER ALL STOPPED VEHICLES HAVE STARTED MOVING, THE "W3-1a (48" X 48") AND "W3-1P (24" X 18") SIGNS SHOULD BE REMOVED. TRAFFIC SHOULD BE COVERED IF ROAD IS WIDENED.
 - AFTER ALL VEHICLES HAVE RESUMED APPROXIMATELY NORMAL SPEED, THE CHANGABLE MESSAGE SIGNS TURNED OFF.
- UNILLUMINATED SECTIONS OF HIGHWAYS SHOULD NOT BE CLOSED DURING HOURS OF DARKNESS EXCEPT FOR EMERGENCY REPAIRS. THE ENGINEER SHALL BE RESPONSIBLE FOR THE CLOSURE. THE ORDER OF ERECTION OF DARKNESS, A TYPE B HIGH INTENSITY FLASHING FABRICATED WARNING LIGHT SHALL BE USED ON EACH APPROACH.
- IF AN ENTRANCE RAMP IS LOCATED BETWEEN THE CLOSURE AND THE RAMP, THE "W3-1a (48" X 48") AND "W3-1P (24" X 18") SIGNS SHOULD ALSO BE ERECTED ON THE RAMP SHOULDER.
- THE ABOVE DURATION WILL APPLY TO EACH APPROACH TO THE CLOSURE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE AND CONSTRUCTION INCLUDING SECURING LAW ENFORCEMENT SERVICES.

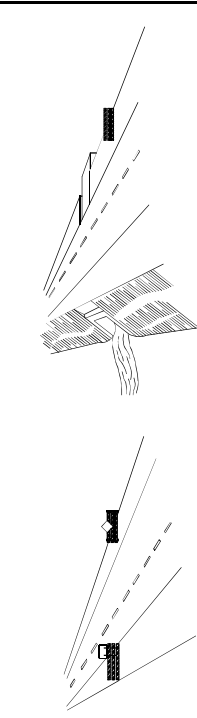


GENERAL NOTES:

- THIS TYPE OF HIGHWAY CLOSURE SHOULD BE USED ONLY WHEN THE DURATION OF CLOSURE WILL NOT EXCEED 30 MINUTES AFTER THE HIGHWAY CLOSURE. THIS PROCEDURE A MINIMUM PERIOD OF 30 MINUTES SHOULD ELAPSE BEFORE ANOTHER CLOSURE OF THE HIGHWAY WITH THE APPROVAL OF THE ENGINEER.
- AT LEAST TWO LAW ENFORCEMENT OFFICERS AND TWO LAW ENFORCEMENT VEHICLES SHOULD BE PROVIDED ON EACH APPROACH TO THE CLOSURE. EACH LAW ENFORCEMENT VEHICLE SHOULD BE EQUIPPED WITH A ROOF MOUNTED FLASHING BLUE LIGHT OR LIGHT BAR.
- RESTRICTIONS ON ROAD CLOSURES ARE SPECIFIED IN THE CONTRACT DOCUMENT.
- THE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD BE MOVED BACK AS REQUIRED BY THE QUEUEING OF STOPPED VEHICLES.
- IF QUEUE EXCEEDS THE FIRST CHANGABLE MESSAGE SIGN, THE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD BE MOVED BACK AS NECESSARY WITH APPROVAL OF THE ENGINEER.
- TRAFFIC CONTROL FOR THE CLOSURE SHOULD BE ACCOMPLISHED IN THE FOLLOWING ORDER:
 - FIRST CHANGABLE MESSAGE SIGN (CMS)
 - SECOND CHANGABLE MESSAGE SIGN (CMS)
 - ADVANCE LAW ENFORCEMENT VEHICLE AND FLASHERS ON
 - W3-1Q (48" X 48") AND W3-1P (24" X 18") SIGNS ERRECTED.
 - RI-1 (48" X 48") SIGNS ERRECTED TO STOP TRAFFIC. THE ORDER OF ERECTION SHOULD BE TOWARD THE MEDIAN SHOULDER, THEN CENTER, THEN MEDIAN SHOULDER, AND FLASHERS ON.
 - SECOND LAW ENFORCEMENT VEHICLE, LIGHTS AND FLASHERS ON.
 - TRAFFIC CONTROL SHOULD BE REMOVED IN THE FOLLOWING ORDER:
 - WITH TRAFFIC STOPPED REMOVE THE RI-1 SIGN FROM THE CENTER OF THE ROAD SHOULDER IN THE FOLLOWING ORDER: MEDIAN, THEN CENTER, THEN SIGN ON THE RIGHT SHOULDER. AFTER TRAFFIC IS REMOVED, THE VEHICLE LEADS TRAFFIC THROUGH WORK AREA.
 - AFTER ALL STOPPED VEHICLES HAVE STARTED MOVING, THE W3-1Q (48" X 48") AND W3-1P (24" X 18") SIGNS SHOULD BE REMOVED. THESE SIGNS MAY BE COVERED IF RELEASE IS IMMINENT.
 - AFTER ALL VEHICLES HAVE RESUMED APPROXIMATELY NORMAL FLOW, THE CHANGABLE MESSAGE SIGNS TURNED OFF.
- UNILLUMINATED SECTIONS OF HIGHWAYS SHOULD NOT BE CLOSED DURING HOURS OF DARKNESS EXCEPT FOR EMERGENCY REPAIRS. THE SIGNAGE FOR DARKNESS OF DARKNESS: A TYPE B HIGH-INTENSITY REFLECTIVE SIGNAGE AND FLASHING LIGHT SHALL BE USED ON EACH W3-1Q SIGN.
- IF AN ENTRANCE RAMP IS LOCATED BETWEEN THE SECOND LANE AND RAMP, THE CMS W3-1Q (48" X 48") AND W3-1P (24" X 18") SIGNS SHOULD ALSO BE ERRECTED ON THE RAMP SHOULDER.
- THE ABOVE DURATION WILL APPLY TO EACH APPROACH TO THE CLOSURE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE FOR MAINTENANCE OF TRAFFIC INCLUDING SECURING LAW ENFORCEMENT SERVICES.

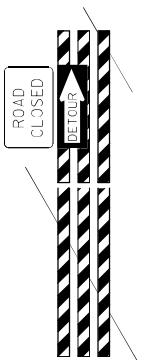
STATE PROJECT NO.	
MISS.	
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
SHORT DURATION CLOSING OF DIVIDED HIGHWAYS	
ISSUE DATE:	MAY 01, 2017
WORKING NUMBER	10P-7
SHEET NUMBER	6357

STATE PROJECT NO.
MISS.



WING BARRICADES

- WING BARRICADES ARE TYPE II BARRICADES ERECTED ON THE SHOULDER OF A ROADWAY OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
- WING BARRICADES SHOULD BE USED:
 - IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.

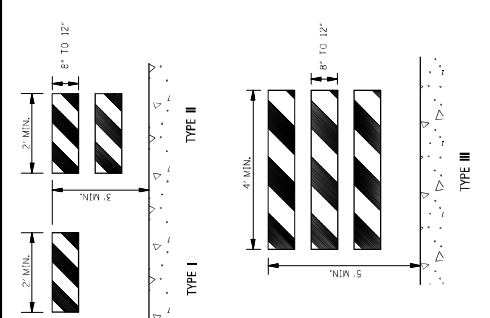


BARRICADE CLOSING A ROAD

	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF RETROREFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 4 IF FACING TRAFFIC IN TWO DIRECTIONS

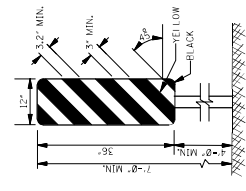
BARRICADE CHARACTERISTICS

- FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 IN² OF REFLECTIVE AREA FACING TRAFFIC.



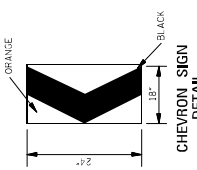
STANDARD BARRICADES

- THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION OF TRAFFIC IS TO PASS).
- RAIL STRIPE SHALL BE 6 INCHES, EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.
- DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
- FOR ADDITIONAL INFORMATION OR DETAILS, SEE METHOD, LATEST EDITION.
- BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WHEN OTHER DEVICES WHICH REQUIRE SUCCESSFULLY CRASH TESTED. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE: http://safety.fhwa.dot.gov/roadway_dept/pafety_guidance/road_hardware/cat2.cfm



TYPE 3 OBJECT MARKER (OM-3R)

- TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DETERMINED NECESSARY BY THE ENGINEER.
- THE OM-3R IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.
- THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.



CHEVRON SIGN DETAIL

- A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
- THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.
- CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 240' BEHIND THE LANE TRANSITION STRIPE.

PLASTIC DRUM STRIPING DETAIL

- PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH MARKING STRIPING OR CHANNELIZATION. THE PREDOMINANT COLOR OF DRUMS SHALL BE ORANGE WITH POLYESTER-RETROREFLECTIVE, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
- DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
- WHERE PRACTICAL PLASTIC DRUMS SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.


REVISION

DATE

ISSUE DATE: MAY 21, 2017

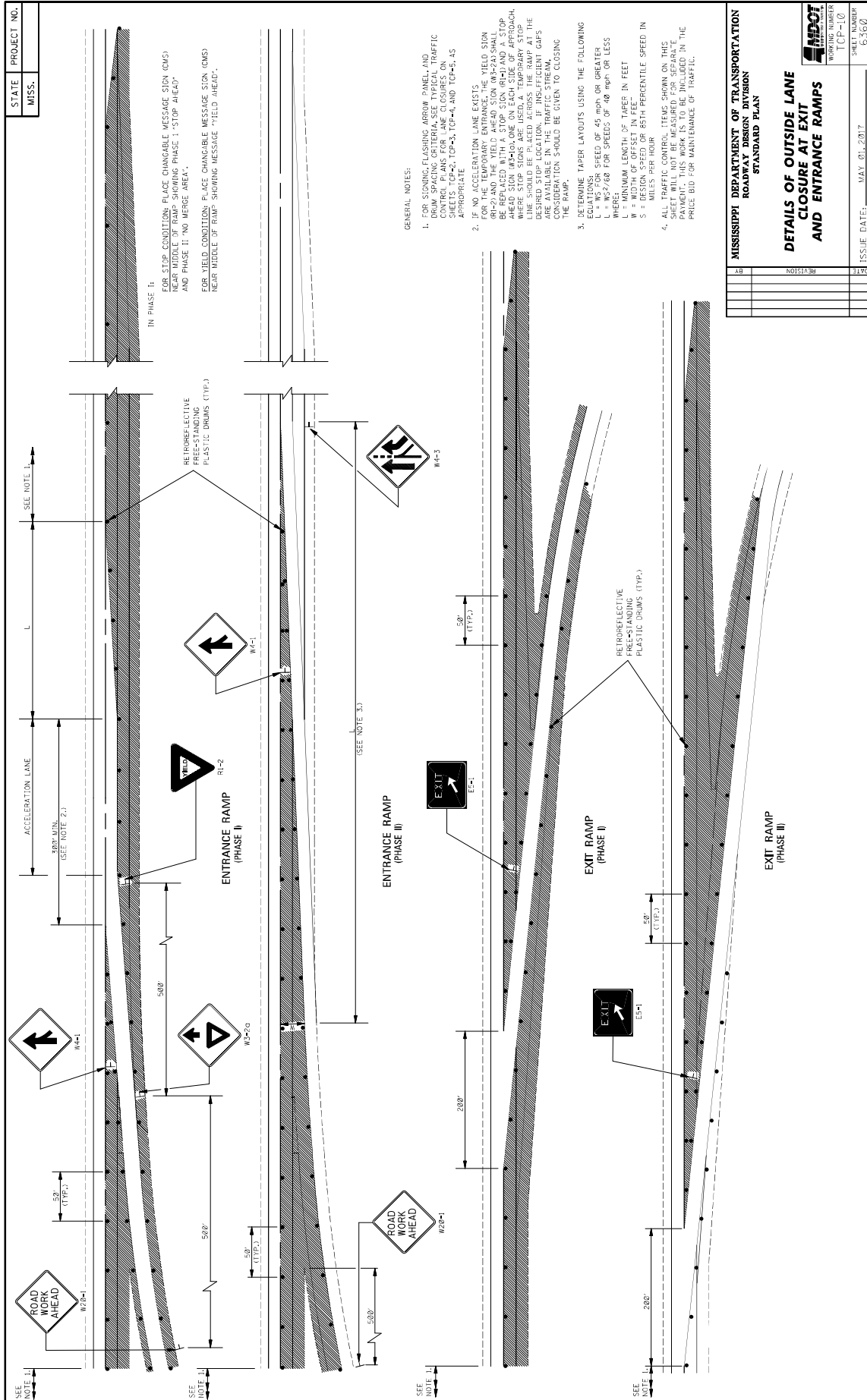
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS



WORKING NUMBER
TCP-5

SHEET NUMBER
03300



- GENERAL NOTES:
1. FOR SIGNING FLASHING ARROW PANEL AND DRUM SPACING CRITERIA, SEE TYPICAL TRAFFIC CONTROL PLANS FOR LANE CLOSURES ON RAMP ENTRIES, TPC-3, TPC-4, AND TPC-5, AS APPROPRIATE.
 2. IF NO ACCELERATION LANE EXISTS FOR THE TEMPORARY ENTRANCE, THE YIELD SIGN (R1-2) AND THE YIELD AHEAD SIGN (R3-2A) SHALL BE REPLACED WITH A STOP SIGN (R1-1) AND A STOP AHEAD SIGN (R3-2B). STOP SIGNS AND STOP AHEAD SIGNS SHOULD BE PLACED ACROSS THE RAMP AT THE DESIRED STOP LOCATION. INSUFFICIENT GAPS IN TRAFFIC SHOULD BE MAINTAINED UNTIL THE CONSIDERATION SHOULD BE GIVEN TO CLOSING THE RAMP.
 3. DETERMINE TAPER LAYOUTS USING THE FOLLOWING EQUATIONS: SPEEDS OF 45, 60, OR GREATER
 $L = W \times 2 / 60$ FOR SPEEDS OF 40, 45, OR LESS
 WHERE:
 L = MINIMUM LENGTH OF TAPER IN FEET
 W = DESIGN SPEED OF 85TH PERCENTILE SPEED IN MILES PER HOUR
 4. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. ITEMS SHOWN ARE TO BE PROVIDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

STATE PROJECT NO.
MISS.

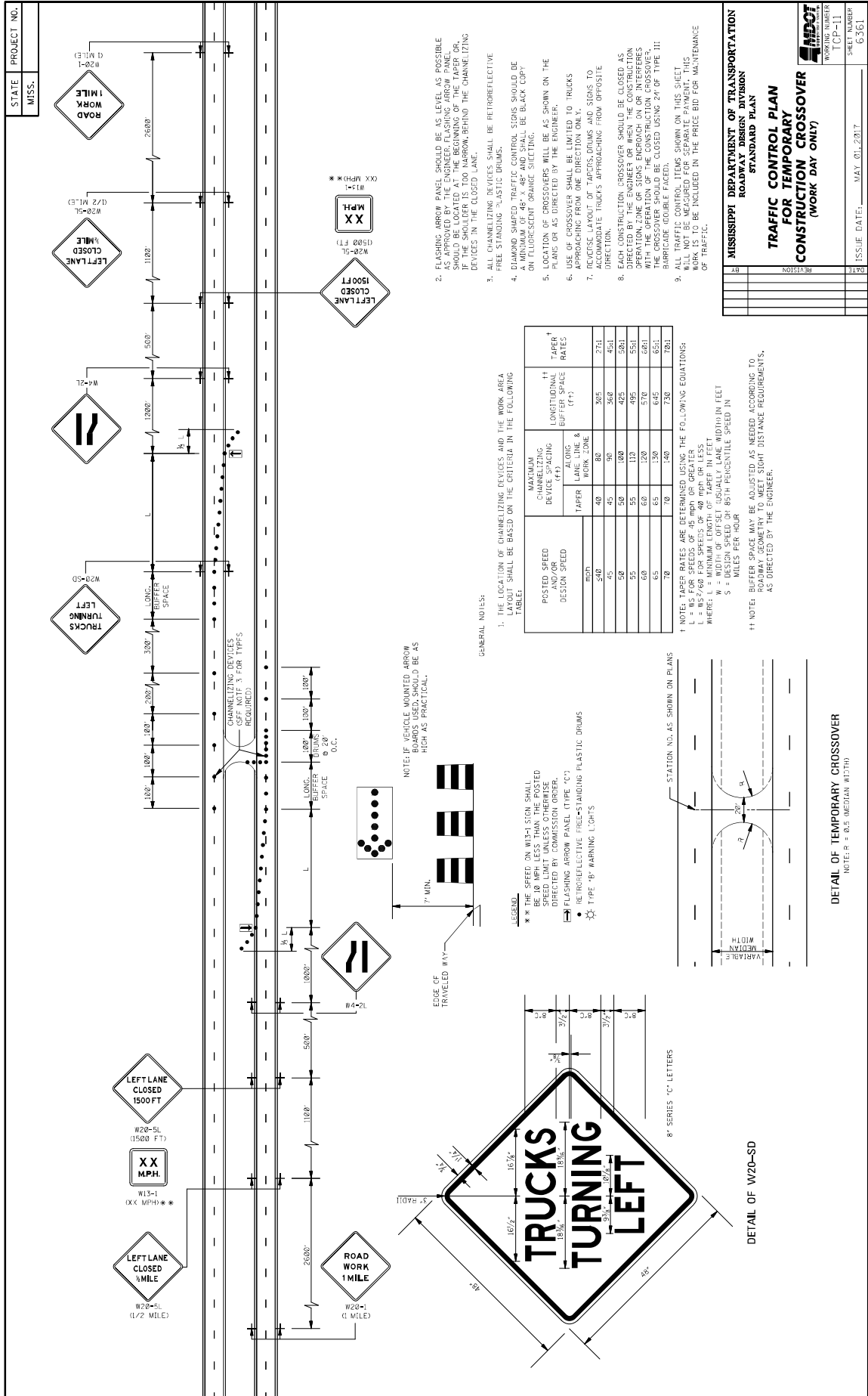
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

DETAILS OF OUTSIDE LANE CLOSURE AT EXIT AND ENTRANCE RAMP

ISSUE DATE: MAY 01, 2017

WORKING NUMBER: TPC-110
SHEET NUMBER: 63300

DATE	REVISION

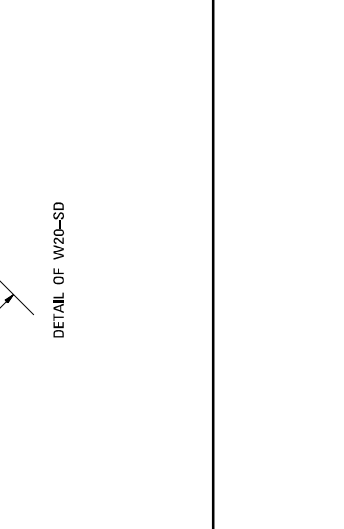
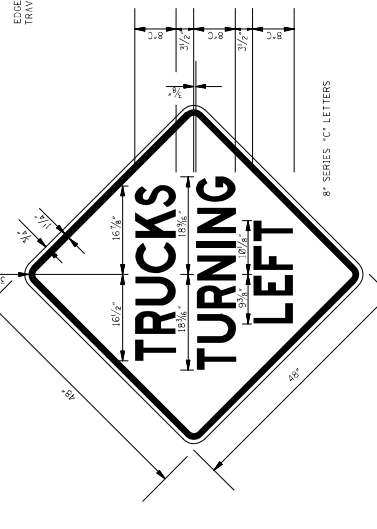


1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA TAPER SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:
2. FLASHING ARROW PANELS SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANELS SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR, IF THE SHOULDER IS TOO NARROW, BEHIND THE CHANNELIZING DEVICES IN THE CLOSED LANE.
3. ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE FREE STANDING PLASTIC DRUMS.
4. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE BLACK COPY ON FLUORESCENT ORANGE SUCTING.
5. LOCATION OF CROSSOVERS WILL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
6. USE OF CROSSOVERS SHALL BE LIMITED TO TRUCKS APPROACHING FROM ONE DIRECTION ONLY.
7. REVERSE LAYOUT OF TAPERS, DRUMS AND SIGNS TO ACCOMMODATE TRUCKS APPROACHING FROM OPPOSITE DIRECTION.
8. EACH CONSTRUCTION CROSSOVER SHOULD BE CLOSED AS SOON AS POSSIBLE AND SHOULD BE OPENED AS SOON AS POSSIBLE WITH THE OPERATION OF THE CONSTRUCTION CROSSOVER.
9. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

POSTED SPEED AND/OR DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING (FT)	LONGITUDINAL BUFFER SPACE (FT)	TAPER RATES
50	40	80	30%
55	45	90	36%
60	50	100	42%
65	55	110	49%
70	60	120	57%
75	65	130	65%
80	70	140	73%
85	75	150	81%

NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 $L = W \times S$
 WHERE: L = MINIMUM LENGTH OF TAPER IN FEET
 W = WIDTH OF OFFSET USUALLY LANE WIDTH IN FEET
 S = 85TH PERCENTILE SPEED IN MILES PER HOUR

NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.



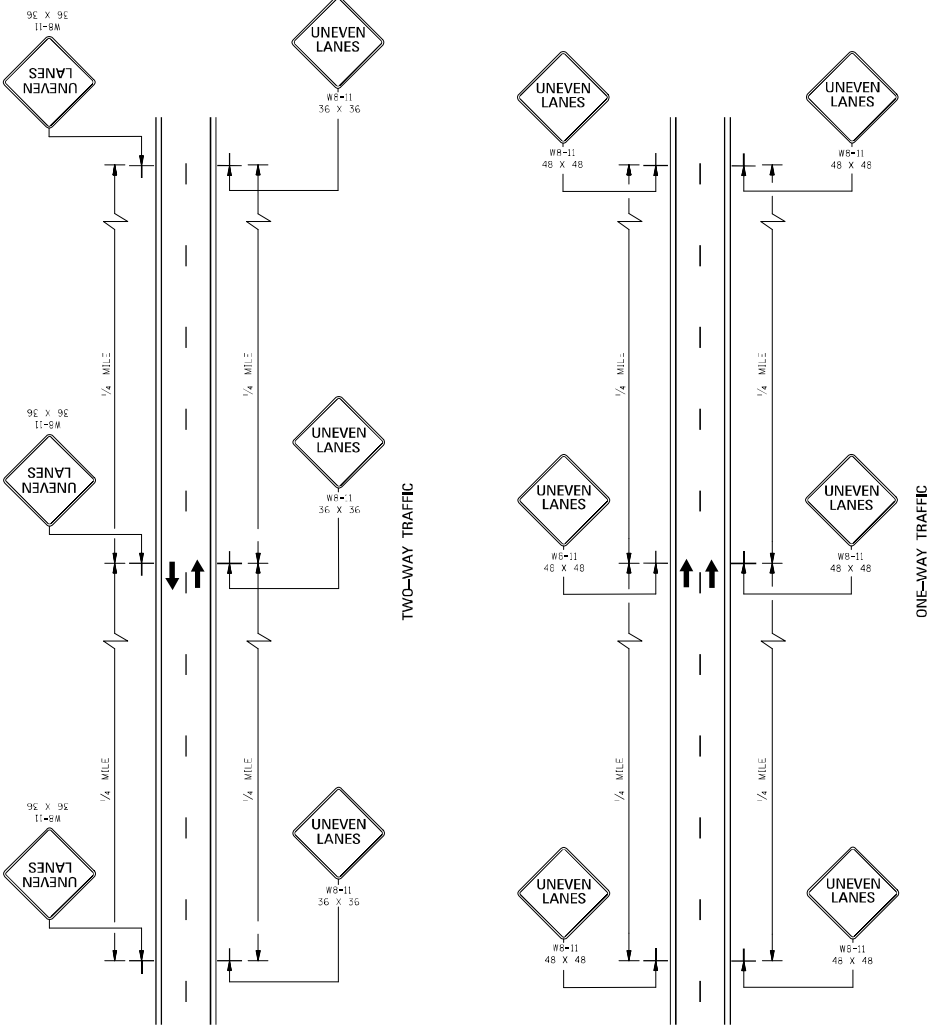
8" SERIES "C" LETTERS
 W20-5L (1/2 MILE)
 W20-1 (1/4 MILE)
 W4-2L
 W4-2R
 W20-5D
 W20-11
 W20-12
 W20-13
 W20-14
 W20-15
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MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN
 TRAFFIC CONTROL PLAN
 FOR TEMPORARY
 CONSTRUCTION CROSSOVER
 (WORK DAY ONLY)

WORKING NUMBER
 TCR-11
 SHEET NUMBER
 6361
 ISSUE DATE:
 MAY 01, 2017

STATE PROJECT NO.
 MISS.

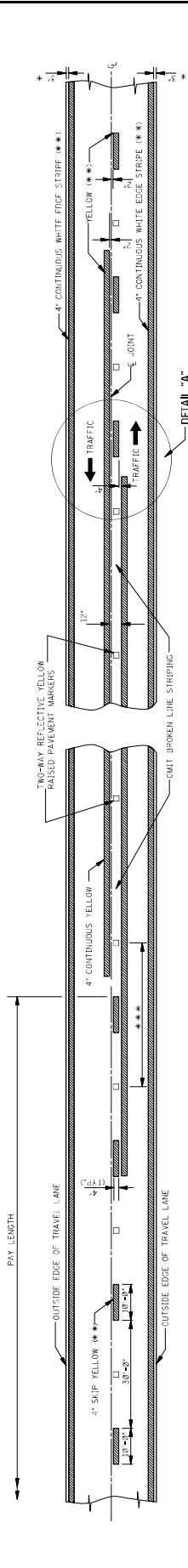
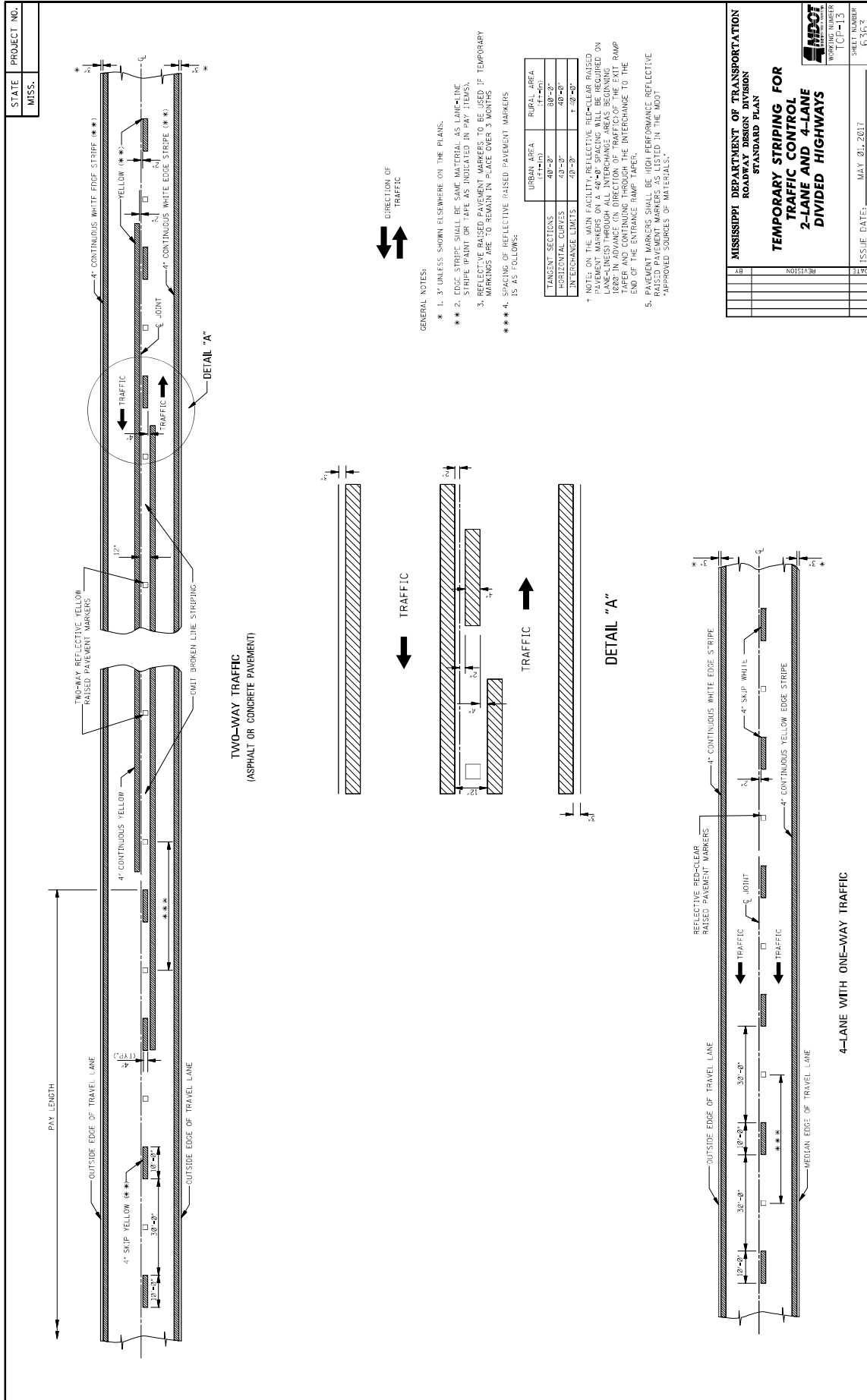
STATE	PROJECT NO.
MISS.	



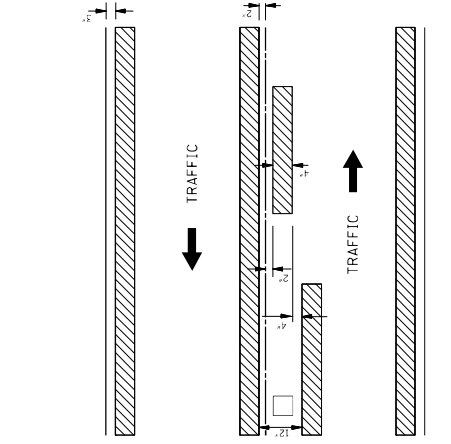
GENERAL NOTES:

- UNEVEN LANE LINE.
 - IF LESS THAN OR EQUAL TO 1/2", NO SIGNS REQUIRED.
 - IF MORE THAN 1/2" AND LESS THAN 1", PLACE SIGNS AS SHOWN ON THIS SHEET.
 - IF GREATER THAN 1", TRAFFIC SHOULD NOT BE ALLOWED TO CROSS UNEVEN LANE LINE.
 - IF GREATER THAN 2 1/4", TRAFFIC SHOULD NOT BE ALLOWED TO CROSS UNEVEN LANE LINE.
- THE WB-11 SIGNS SHOULD BE SPACED AT 1/4-MILE INTERVALS THROUGHOUT UNEVEN LANE LINE LIMITS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

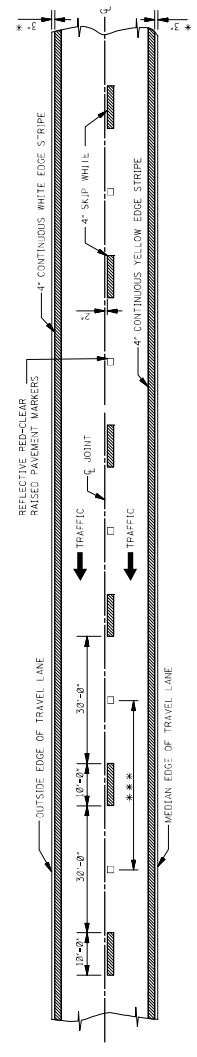
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TRAFFIC CONTROL PLANS UNEVEN PAVEMENT DETAILS	
WORKING NUMBER TCP-12	SHEET NUMBER 6262
ISSUE DATE: MAY 01, 2017	
REVISION	
BY	



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



DETAIL "A"

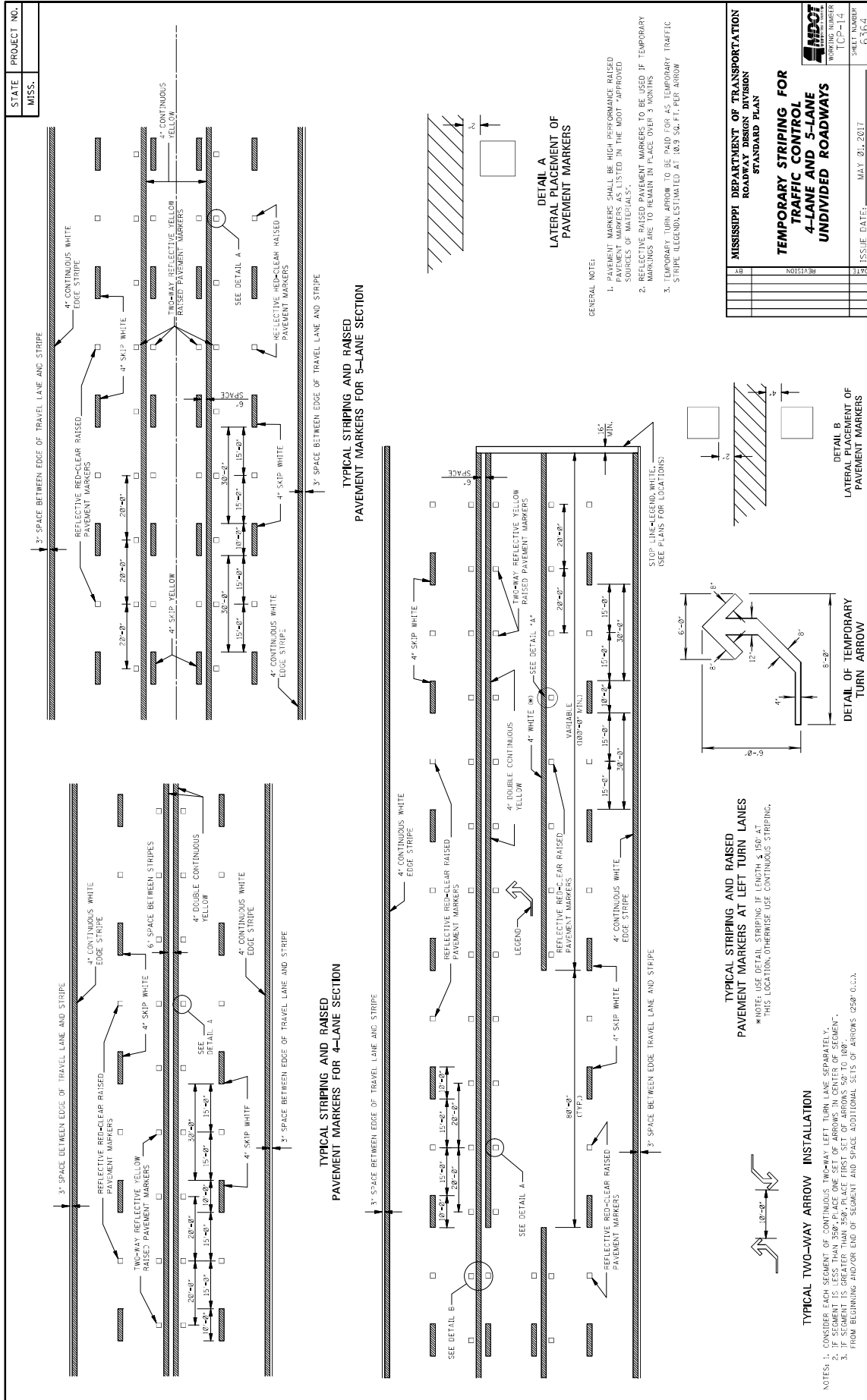


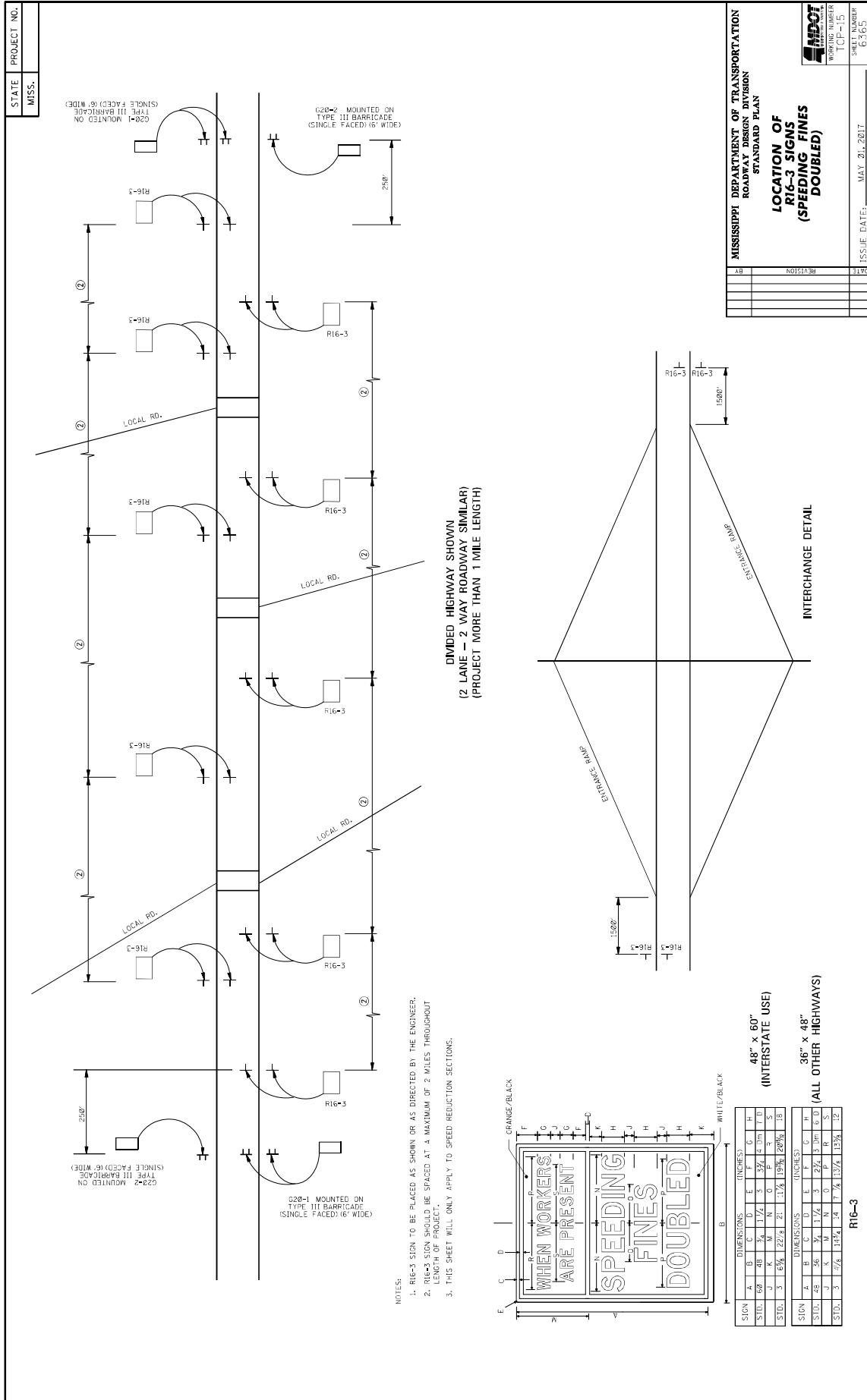
4-LANE WITH ONE-WAY TRAFFIC

DETAIL "A"

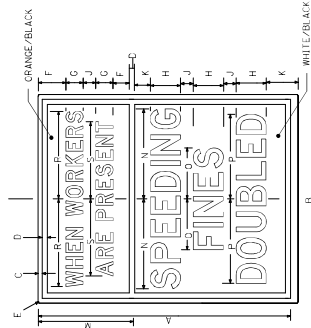
GENERAL NOTES:

- * 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
- ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
- 3. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS.
- *** 4. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:





- NOTES:
1. R16-3 SIGN TO BE PLACED AS SHOWN OR AS DIRECTED BY THE ENGINEER.
 2. R16-3 SIGN SHOULD BE SPACED AT A MAXIMUM OF 2 MILES THROUGHOUT LENGTH OF PROJECT.
 3. THIS SHEET WILL ONLY APPLY TO SPEED REDUCTION SECTIONS.



SIGN		DIMENSIONS (INCHES)											
TYPE	48" X 60"	A	B	C	D	E	F	G	H	I	J	K	L
STD.	6-00	48	36	1 1/2	5	3 1/2	4	3/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4
STD.	3	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4
TYPE	36" X 48"	M	N	O	P	Q	R	S	T	U	V	W	X
STD.	3	36	24	1 1/2	5	3 1/2	4	3/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4
STD.	3	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

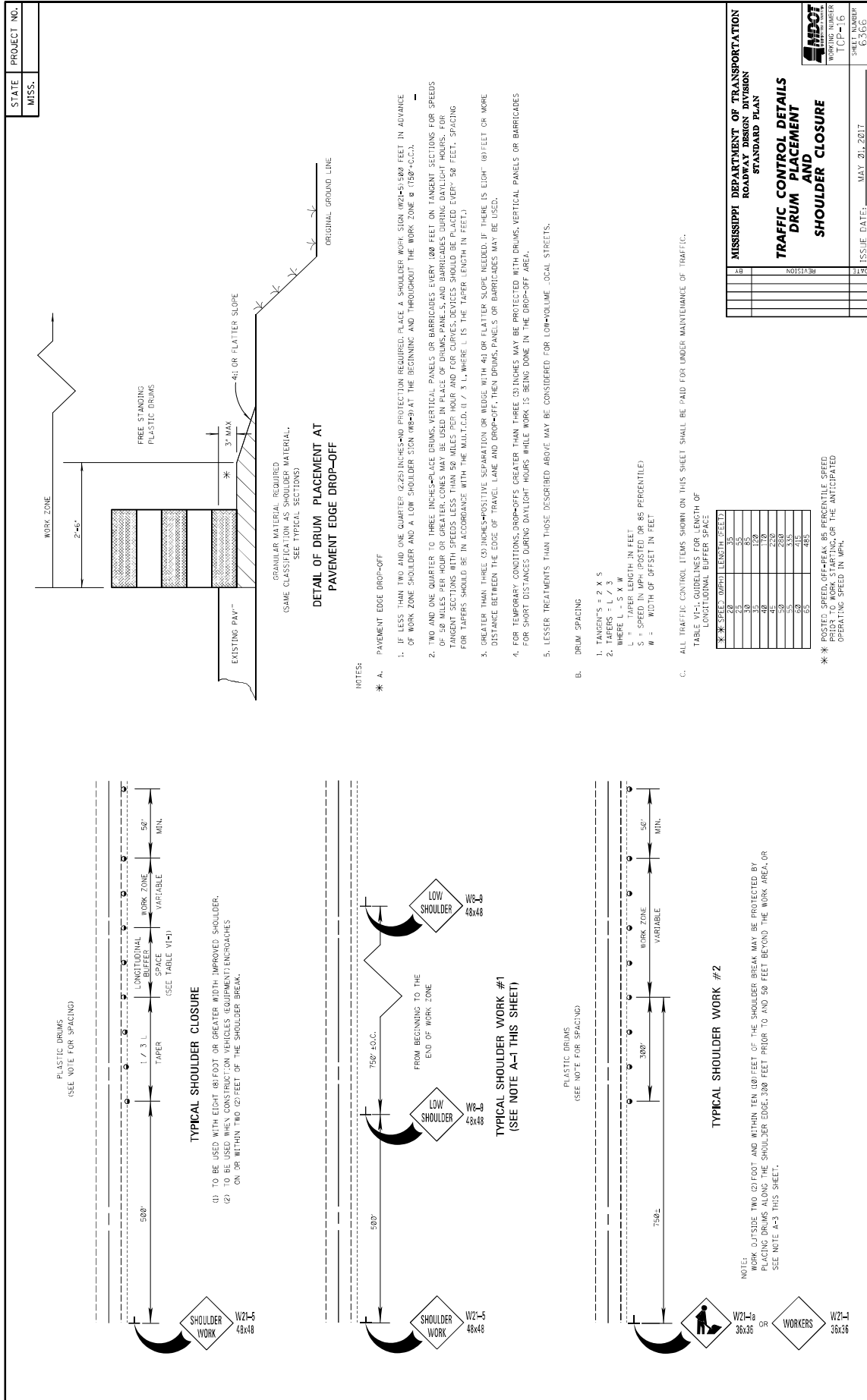
LOCATION OF R16-3 SIGNS (SPEEDING FINES DOUBLED)

ISSUE DATE: MAY 21, 2017

WORKING NUMBER: ICF-15

SHEET NUMBER: 6-163

STATE PROJECT NO.
MISS.



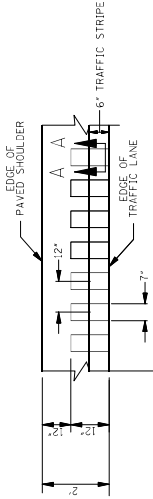
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TRAFFIC CONTROL DETAILS
DRUM PLACEMENT
SHOULDER CLOSURE**

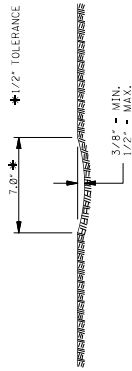
ISSUE DATE: MAY 20, 2017

GENERAL NOTES

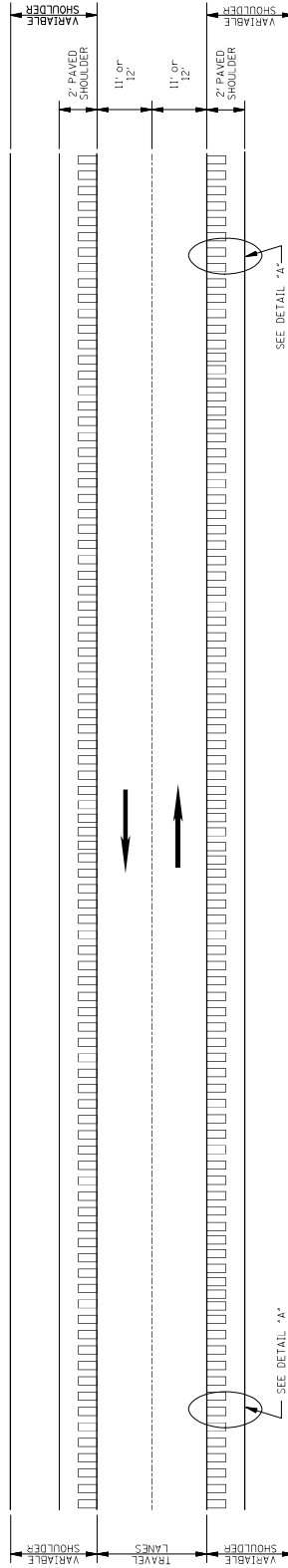
1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL PAVED SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT.
2. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL EXISTING AND NEW ROADS, INCLUDING OTHER INTERSECTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER.
3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS.
4. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
 - A. MAINLINE
 - B. INTERSECTING ROADWAY IF OVERLAP OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.W.
 - C. ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.
5. DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.



SECTION "A-A"



SECTION "A-A"



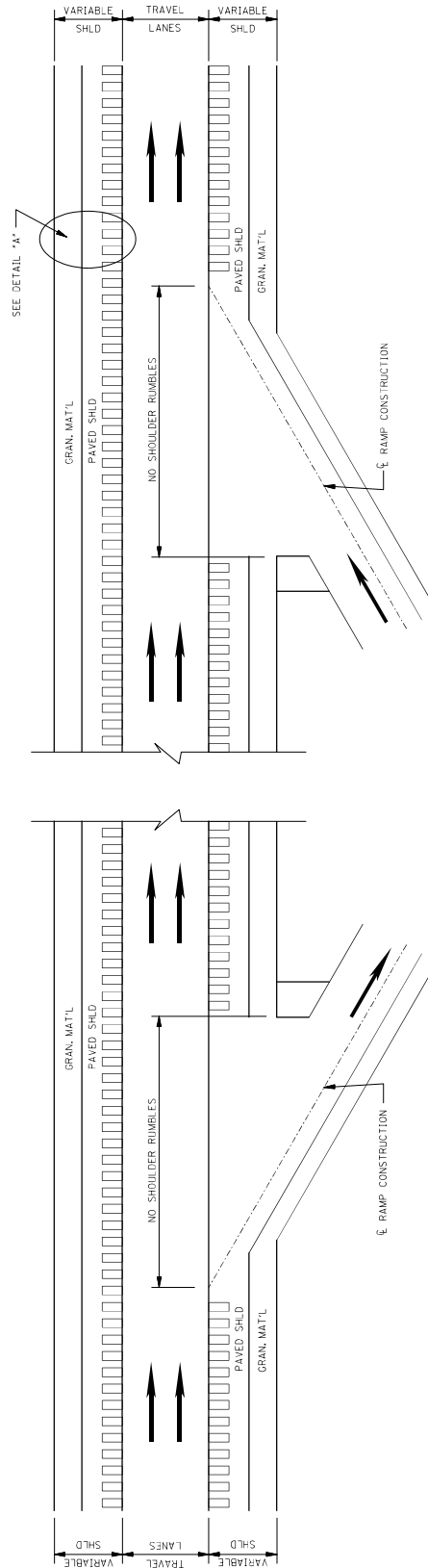
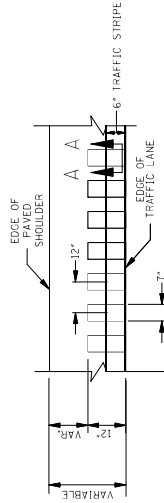
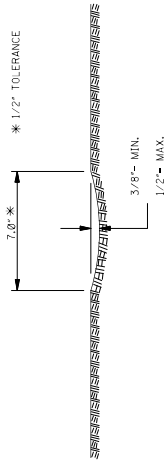
PLAN

NOT TO SCALE

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY DESIGN DIVISION	
STANDARD PLAN	
RUMBLE STRIPES 2-LANE HIGHWAYS (ASPHALT LANES, 2-FT ASPHALT SHOULDERS)	
DATE	ISSUE DATE: AUGUST 01, 2017
REVISION	
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GENERAL NOTES

1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL PAVED SHOULDERS AND ALL PAVED SHOULDERS ON THIS PROJECT.
2. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL INTERSECTIONS, ROADWAYS, AND OTHER INTERSECTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER.
3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS.
4. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
 - A. MAINLINE
 - B. INTERSECTING ROADWAY IF OVERLAP OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.W.
 - C. ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.



MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**RUMBLE STRIPES
4-LANE HIGHWAYS
(ASPHALT LANES,
2-FT OR WIDER
ASPHALT SHOULDERS)**

ISSUE NUMBER: 6065
ISSUE DATE: AUGUST 01, 2017

DATE	REVISION	LOCATION
05/08	05/08	

