

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Bidder acknowledges receipt of and has added to and made a part of the proposal and contract documents the following addendum (addenda):

ADDENDUM NO. <u> 1 </u>	DATED <u> 5/18/2023 </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>
ADDENDUM NO. <u> </u>	DATED <u> </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>
ADDENDUM NO. <u> </u>	DATED <u> </u>	ADDENDUM NO. <u> </u>	DATED <u> </u>

Number	Description
1	Revised Notice to Bidder No. 5045; Revised Bid Items; Amendment EBSx Download Required.

TOTAL ADDENDA: 1
 (Must agree with total addenda issued prior to opening of bids)

Respectfully Submitted,

DATE _____

 Contractor

BY _____
 Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

_____ President	_____ Address
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_____ Secretary	_____ Address
--------------------	------------------

_____ Treasurer	_____ Address
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The following is my (our) itemized proposal.

MP-7149-64(006)/ 308317301000

Simpson County(ies)

Revised 01/26/2016

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 5045

CODE: (SP)

DATE: 05/18/2023

SUBJECT: Scope of Work

PROJECT: MP-7149-64(006) / 308317301 -- Simpson County

The contract documents do not include an official set of construction plans, but may, by reference, include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings.”

The work to be accomplished using the pay items and corresponding specifications set forth in the contract is to overlay the following section of SR 149 in Simpson County from Industrial Park Drive in Magee to the intersection of SR 149 and Athens Road in Magee.

<u>Route</u>	<u>Length (Miles)</u>	<u>Width (Feet)</u>	<u>Surface Lift</u>	<u>Thickness (Inches)</u>	<u>Level Lift</u>	<u>Thickness</u>
SR 149	5.7	24'	UTAP	.75"	UTAP	0.5"

See Typical Sections TS-1 and TS-2 for more details.

Work on the Project shall consist of the following:

1. The Contractor shall erect and maintain construction signing, provide all signs, set up night time lane closures (if needed), and traffic handling devices in accordance with the Traffic Control Plan. The cost for this work is to be included in the price bid for pay item 618-A: Maintenance of Traffic. All traffic control devices on this project should comply with the latest version of the MUTCD. Fluorescent orange sheeting shall be used on all construction and traffic control signs except for those designated in the standards to be black legend and border on white background.
2. Prior to the overlay, the existing shoulders shall be clipped and surplus material shall be spread along the edge of the shoulders, fore slopes, or other adjacent areas as directed by the Engineer, and will be an absorbed item.
3. The Contractor shall fine mill at the following locations:

<u>ROUTE</u>	<u>LOCATION</u>	<u>LENGTH</u>	<u>REMARKS</u>
SR 149	BOP	125'	Tie In
SR 149	EOP	125'	Tie In
SR 149	Along Curb	Various	As directed
SR 28	10+00	125'	Tie In
SR 28	25+27	125'	Tie In

4. The Contractor shall perform pre-leveling operations by placing ½” and variable UTAP, Leveling from the BOP to the EOP as shown on TS-1 and TS-2.

The Contractor shall remove existing pavement markers prior to placing asphalt. The cost of removing these pavement markers will be included in other items bid.

The Contractor shall take due care to maintain a uniform outside edge of pavement, and shall place asphalt to establish an approximate vertical face in order for granular material to be placed directly against the surface and not on a shelf of an underlying course. A rubber tire roller shall be used in addition to a steel wheel roller in obtaining compaction in the wheel ruts on this leveling lift of asphalt.

5. The Contractor shall place the top lift of asphalt on the roadway left and right of the centerline from BOP to EOP as shown on sheets TS-1 and TS-2. The finished cross-slope shall be 2% in tangent sections and match the existing super elevation rate in horizontal curves.

Asphalt surface shall be placed on all local roads and driveway aprons (1¼” thickness). The maximum single lift thickness for local roads and driveway pads shall be 1¼”.

The section of Highway 28 extending between Highway 49 and SR 149 shall be treated as a local road for this project with the exception that the leveling and surface lifts shall be placed separately as shown on TS-1 for the mainline.

Driveway aprons shall be paved 10’ wide as directed by the Engineer. All local roads shall be paved to the normal right of way line or as directed by the Engineer.

Note: The Contractor shall be responsible for traffic control while MDOT personnel conduct density testing on the asphalt. The cost of this work shall be included in the bid price for pay item 618-A: Maintenance of Traffic.

6. The Contractor shall perform sawing and sealing of transverse joints as per the Standard Specifications with the exception that no sawing will be required in the intermediate leveling lift.
7. The Contractor shall place granular material on the shoulders to raise the existing shoulders to the new grade, bladed, shaped, and compacted to a minimum slope of 4%. Granular material will not be allowed to be placed directly on the top lift of asphalt, but must be placed directly on the gravel shoulder by means of a road widener machine approved by the Project Engineer. Light blading or mowing of the shoulders will be required prior to placement of the granular material.
8. The Contractor shall place all permanent pavement markings, including stripe and raised pavement markers, throughout the project as required by the Standard Drawings or as directed by the Engineer. Existing pavement markings located on bridges shall be removed as required prior to placing new markings.

An asphalt taper shall be placed at the temporary joints caused by the milling or overlay in order to provide for the safe movement of traffic. The taper shall be three feet (3') in length per 1-inch of depth and will be an absorbed item.

Temporary striping shall be required after milling and overlaying operations: Temporary striping shall be placed in the same locations and layout as permanent stripe. All centerline, lane lines, edge lines, and no passing stripes that have been removed during the day's operations shall be replaced with temporary stripe before work is discontinued for the day or as soon thereafter as weather conditions will permit, except that:

- Replacement of no-passing stripes may be delayed for a period not to exceed three (3) days for a two or three lane road.
- Temporary edge lines on projects requiring shoulders constructed of granular material may be delayed for a period not to exceed three (3) days.

All asphalt and concrete curbs along local roads from BOP to EOP shall be painted (two applications) with white traffic paint and traffic beads as shown on sheet DCIS-1; such costs shall be included in other items bid.

It shall be the responsibility of the Contractor to protect the roadway and all existing structures, such as bridges, culverts, signs, and curbs, from damage occurring as a result of the Contractor's operations. Damages to existing structures caused by the Contractor's operations shall be repaired or replaced at no cost to the Department.

Incidental work such as removing vegetation, shaping and compaction of shoulders, removing excess asphalt material, project clean-up, and other incidental work necessary to complete the project will not be measured for separate payment. Such costs shall be included in the price of other items bid.

It is the Contractor's responsibility to insure the drainage of surface water from milled areas. Where applicable, existing shoulder material is to remain in place to be incorporated into final sloping of the shoulders. Temporary wedges (paper joints) of full lane width asphalt shall be placed by the Contractor immediately after the fine milling process to allow the safe transition of traffic. These wedges shall be maintained in a satisfactory condition by the Contractor until the permanent asphalt is placed, cost to be absorbed.



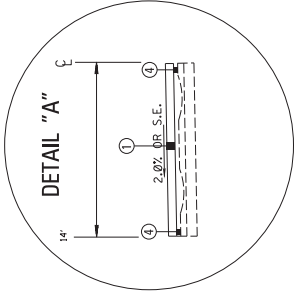
MDOT
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

DESIGNED BY:
CHECKED BY:
DATE:

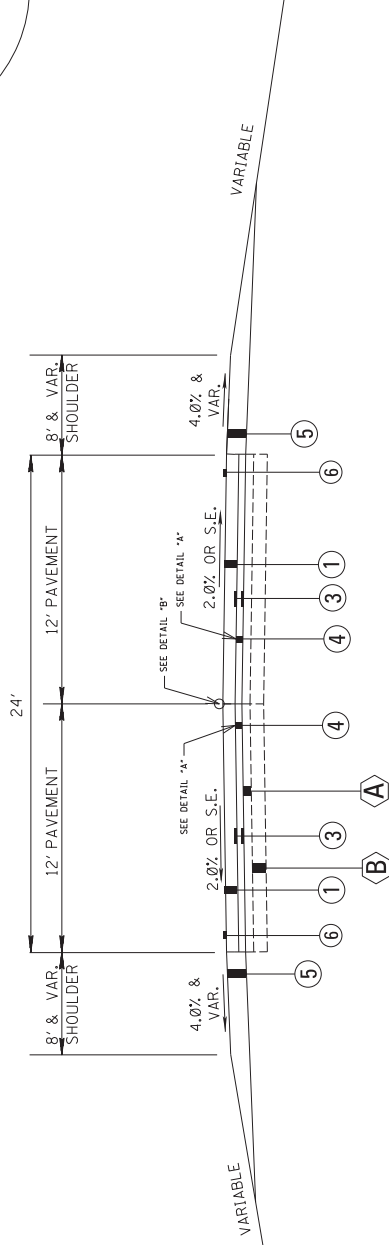
FMS CON: 308317/301000
PROJECT NO.: MP-17-49-6(406)
COUNTY: SIMPSON

Notice to Bidders No. 5045-- Cont.

WK. NO. 1
SHEET NO. 1

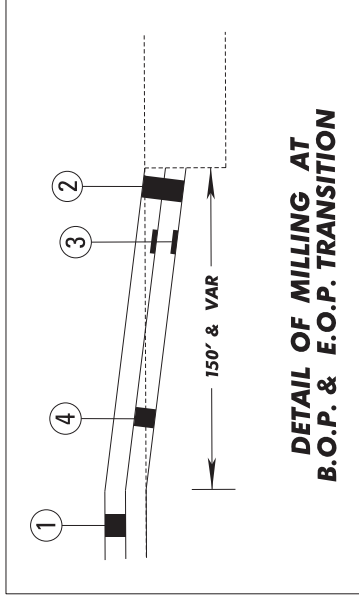


SR 149 & SR 28
C

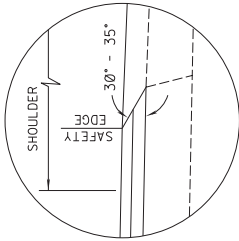


TYPICAL SECTION
B.O.P. STA. 0+00 - STA. 70+21
* STA. 137+75 - E.O.P. STA. 327+70

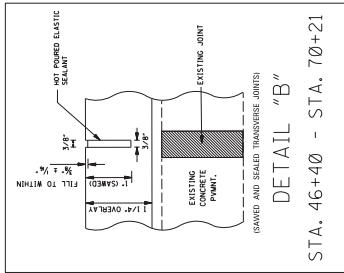
NOT TO SCALE



**DETAIL OF MILLING AT
B.O.P. & E.O.P. TRANSITION**



**SAFETY EDGE REQ'D
TOP 2 LIFTS ONLY
(NOT A PAY ITEM)
OVERLAY**



STA. 46+40 - STA. 70+21

PROPOSED

- ① 0.75" & VAR. OF ULTRA THIN ASPHALT REQ'D
- ② 1.25" & VAR. FINE MILLING WHERE REQ'D
- ③ TACK COAT REQ'D
- ④ 0.50" & VAR. OF ULTRA THIN ASPHALT, LEVELING REQ'D
- ⑤ 1.25" & VAR. OF CL. 6, CR. D GRANULAR MATERIAL REQ'D
- ⑥ RUMBLE STRIPE REQ'D (SEE SHEET RS-2 DETAIL A 12' PAVEMENT EDGE) *

EXISTING

- Ⓐ BITUMINOUS PAVEMENT IN PLACE
- Ⓑ GRANULAR SUBGRADE IN PLACE



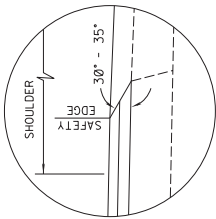
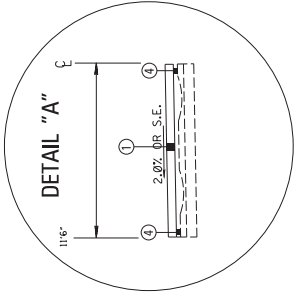
MISSISSIPPI DEPARTMENT OF TRANSPORTATION

DESIGNED BY: _____
 CHECKED BY: _____
 DATE: _____

FMS CON: 308317/301000
 PROJECT NO.: MP-7149-6-4(06)-5
 COUNTY: SIMPSON

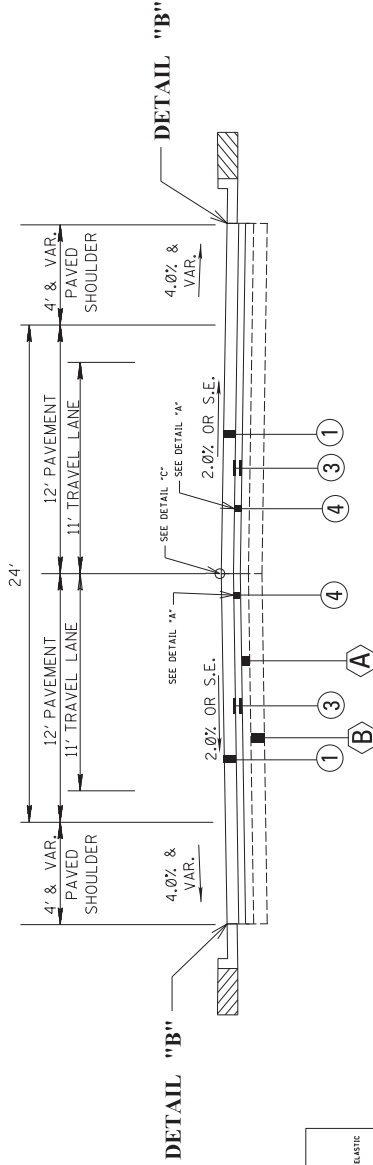
Notice to Bidders No. 5045-- Contract No. 308317-301000

WKS. NO. 152
 SHEET NO. 2



SAFETY EDGE REQ'D
 TOP 2 LIFTS ONLY
 (NOT A PAY ITEM)
 OVERLAY

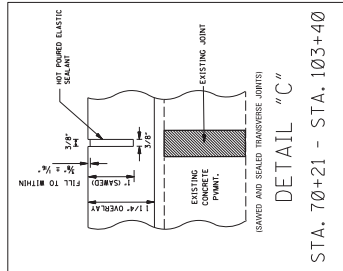
SR 149
 CL



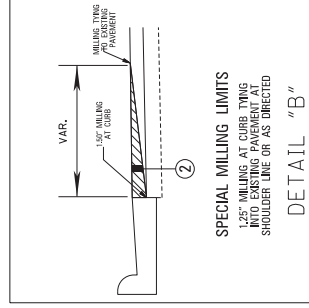
TYPICAL SECTION

STA. 70+21 - STA. 137+75

NOT TO SCALE



DETAIL "C"
 STA. 70+21 - STA. 103+40



SPECIAL MILLING LIMITS
 1/2" MILLING AT CURB
 1/2" MILLING AT SHOULDER
 1/2" MILLING AT SHOULDER LINE OR AS DIRECTED
 DETAIL "B"

EXISTING

- A BITUMINOUS PAVEMENT IN PLACE
- B GRANULAR SUBGRADE IN PLACE

PROPOSED

- 1 0.75" & VAR. OF ULTRA THIN ASPHALT REQ'D
- 2 1.25" & VAR. FINE MILLING WHERE REQ'D
- 3 TACK COAT REQ'D
- 4 0.50" & VAR. OF ULTRA THIN ASPHALT, LEVELING REQ'D
- 5 1.25" & VAR. OF CL. 6, GR. D GRANULAR MATERIAL REQ'D (NOT USED ON THIS PAGE)
- 6 RUMBLE STRIPE REQ'D (SEE SHEET RS-2 DETAIL A 12' PAVEMENT EDGE) (NOT USED ON THIS SHEET)



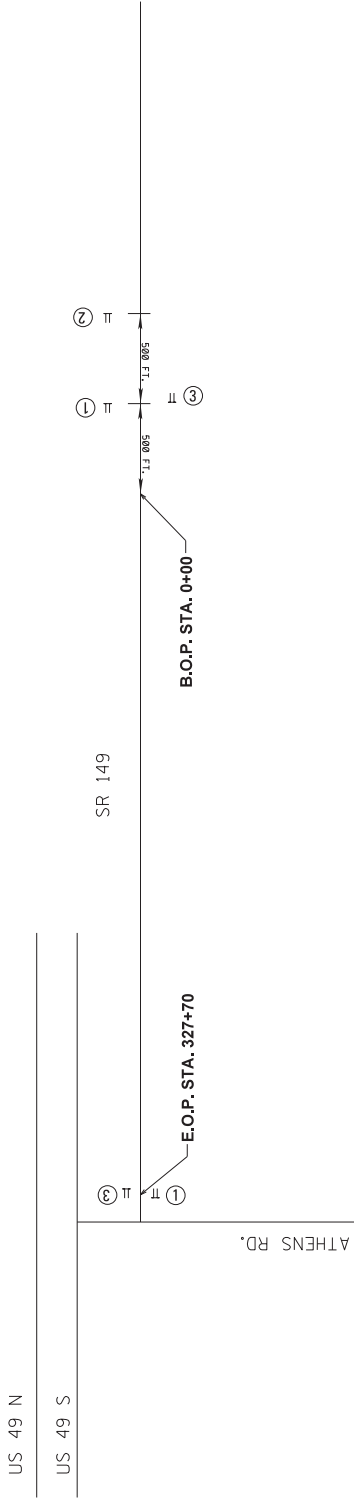
DESIGNED BY:
 CHECKED BY:
 DATE: 10/12/2022

FMS CON: 308317 / 301000
 PROJECT NO.: MP-7149-64(061)-6
 COUNTY: SIMPSON

Notice to Bidders No. 5045 - Cont.

WFO
 DES
 SHEET NO. 4

NOTE: W20-1 (48"X48") SHALL BE REQ'D ON ALL LOCAL ROADS.



**** NOT TO SCALE ****

SIGN LEGEND	
①	G20-1 (60"X24") MOUNTED ON NEXT 6 MILES TYPE III BARRICADE (6 FT. WIDE) (DOUBLE FACED)
②	W20-1 (48"X48") (60 FT.) MOUNTED ON TYPE III BARRICADE (6 FT. WIDE) (SINGLE FACE)
③	G20-2 (48"X24") END ROAD WORK MOUNTED ON TYPE III BARRICADE (6 FT. WIDE) (DOUBLE FACED)

ADDITIONAL TRAFFIC CONTROL SIGNS REQUIRED:

R4-1 (NP7)
 R4-2 (NP7)
 R4-3 (NP7)
 R4-1 (NP7)
 R4-2 (NP7)
 R4-3 (NP7)

R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE" AND W14-3 "NO PASSING ZONE" SIGNS ARE REQUIRED IN ACCORDANCE WITH SUB-SECTION 907-618 AND AS SPECIFIED IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. IF "NO PASSING ZONES" ARE 1,000' OR MORE, INSTALL ADDITIONAL "DO NOT PASS" & "NO PASSING ZONE" SIGNS ON MAXIMUM SPACING OF 750'.

ALL TRAFFIC CONTROL ITEMS SHALL BE INCLUDED IN BID FOR PAY ITEM 618-A001, MAINTENANCE OF TRAFFIC. ALL ORANGE SHEETING SHALL BE USED ON ALL CONSTRUCTION AND TRAFFIC CONTROL SIGNS EXCEPT FOR THOSE DESIGNATED ON THE PLANS TO BE BLACK LEGEND AND BORDER ON WHITE BACKGROUND.



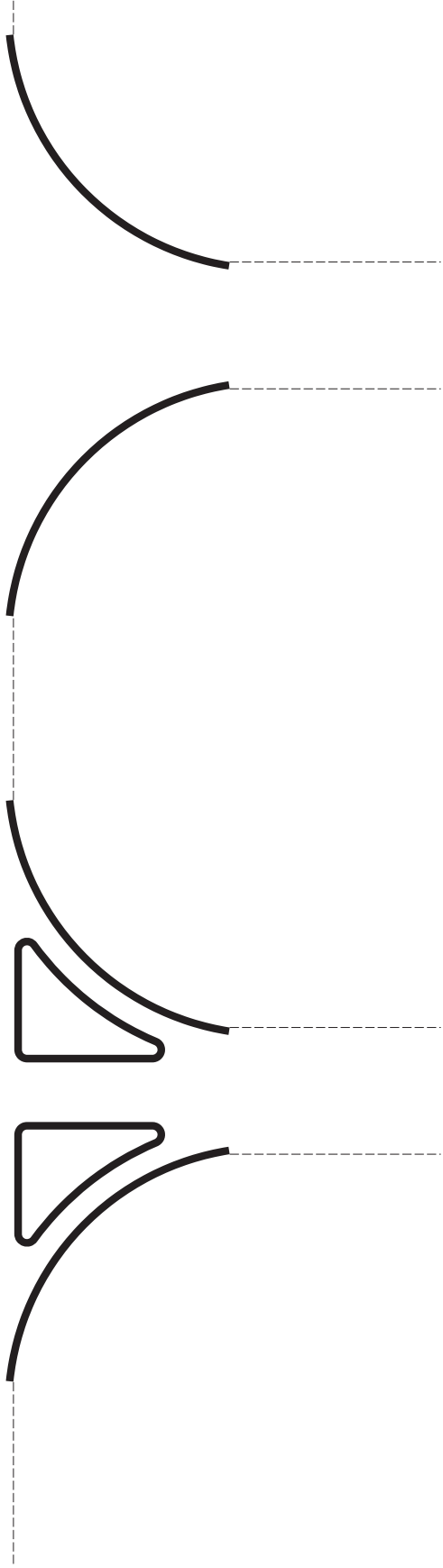
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
MDOT

DESIGNED BY:	
DETAILED BY:	
CHECKED BY:	
DATE:	

FMS CON: 308317/301000
PROJECT NO.: MP-7149-6-410061
COUNTY: SIMPSON

DETAIL FOR CURBS
 AND RAMP RAILS
 Notice to Bidders No. 5045-- Cont.

W.P. NO. DCS-1
SHEET NO. 6



** ALL ASPHALT AND CONCRETE CURBS ALONG RAMPS, LOCAL ROADS, ETC. FROM B.O.P. TO E.O.P. SHALL BE PAINTED (TWO APPLICATIONS) WITH WHITE TRAFFIC PAINT AND TRAFFIC BEADS; COST TO BE ABSORBED IN OTHER PAY ITEMS.



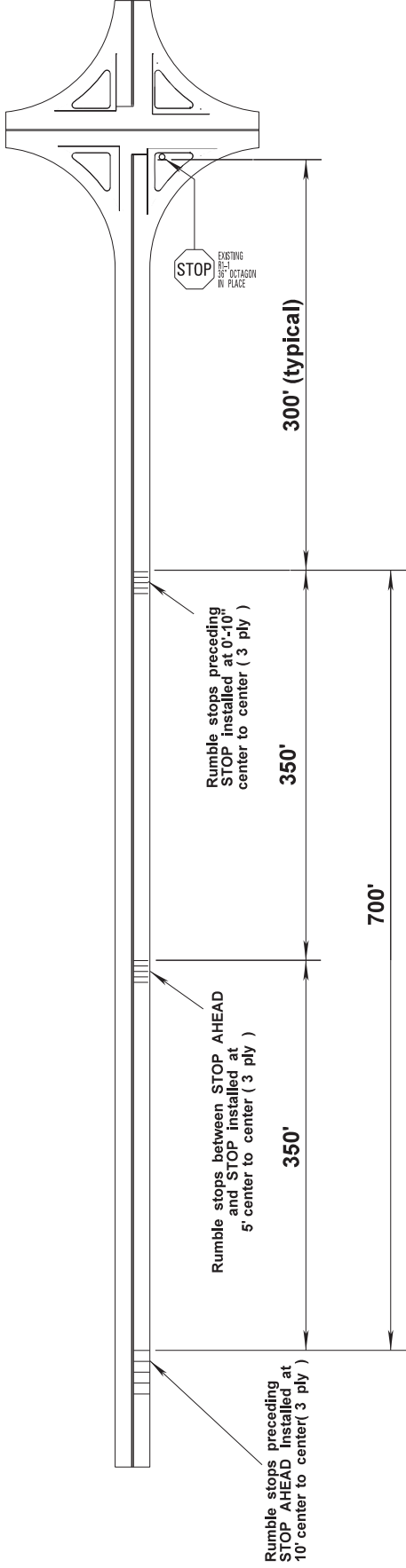
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
MDOT

DESIGNED BY:
CHECKED BY:
DATE:

FMS CON: 308317/301000
PROJECT NO.: MP-7149-6-4(061)
COUNTY: SIMPSON

Notice to Bidders No. 5045-- Cont.

WV. NO. SD-5045-1
SHEET NO. 7



- NOTES:**
Install rumble stop bars as shown :
- 1 set of rumble stops approx. 250' from STOP AHEAD
 - 1 set of rumble stops approx. 300' from STOP
 - 1 set of rumble stops approx. Halfway between first and last set
 - Rumble stops to be 6" thermoplastic (120 mill/each ply, 360 mil total)
 - 5 rumble stops per set minimum
 - Installation may vary due to terrain
 - Signs should be 48" for channelized intersection, 36" for non-channelized intersection



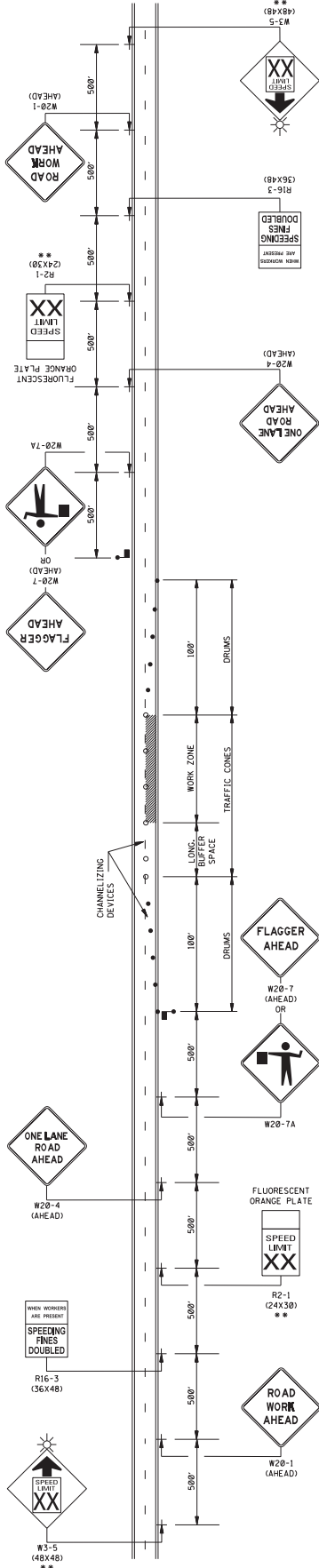
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
MDOT

DESIGNED BY:
CHECKED BY:
DATE:

FMS CON: 308317/301000
PROJECT NO.: M-17-149-6-001 -
COUNTY: SIMPSON

TRAFFIC CONTROL PLAN WITH FLAGGER
CONTRACT NO. 308317-301000-001

WY: 2017
SD: 16P-1
SHEET NO.: 8



- LEGEND
- FLAGGER
 - RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
 - TRAFFIC CONES (28" HEIGHT MINIMUM)
 - CIRCLED SIGNS AND PLASTIC SIGNS SHALL BE 18 MPH LESS THAN THE POSTED SPEED LIMIT UNLESS OTHERWISE DIRECTED BY COMMISSION ORDER.

ROAD TYPE	A	B	C
URBAN (25 MPH OR LESS)	100 FT.	100 FT.	100 FT.
URBAN (40 - 70 MPH)	350 FT.	350 FT.	350 FT.
RURAL	500 FT.	500 FT.	500 FT.
EXPRESSWAY / FREEWAY	1000 FT.	1500 FT.	2640 FT.

- ALL CHANNELIZING DEVICES SHALL BE A MINIMUM OF 28" IN HEIGHT.
- WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED AND ALL CHANNELIZING DEVICES SHALL BE MOVED TO THE SHOULDER EDGE.
- ADDITIONAL FLAGGERS MAY BE NEEDED AS DIRECTED BY THE ENGINEER.
- WHEN WORK IS REQUIRED AT NIGHT, FLAGGER STATIONS SHALL BE ILLUMINATED.
- CHANNELIZING DEVICE TYPES FOR:
 - APPROACH AND EXIT TAPERS - RETROREFLECTIVE PLASTIC DRUMS
 - ALONG LINE LINE AND WORK ZONE - TRAFFIC CONES (28" HEIGHT)
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

POSTED SPEED AND/OR DESIGN SPEED (mph)	MAXIMUM CHANNELIZING DEVICE SPACING (FT)		LONGITUDINAL BUFFER SPACE (FT)	STOPPING DISTANCE
	TAPER	LANE LINE & WORK ZONE		
25	20	50	55	155
30	20	50	65	200
35	20	60	75	250
40	20	70	120	325
45	20	80	170	425
50	20	100	230	425
55	20	110	335	495
60	20	120	415	570
65	20	130	485	645

* NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS. AS DIRECTED BY THE ENGINEER.

GENERAL NOTES:

- THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE. FLAGGER STATIONS SHALL BE LOCATED SUCH THAT STOPPING DISTANCE REQUIREMENTS ARE MET. STOPPING DISTANCE TO STOP VALUES IN STOPPING SIGHT DISTANCE COLUMN MAY BE USED AS A MINIMUM FOR THIS DISTANCE.

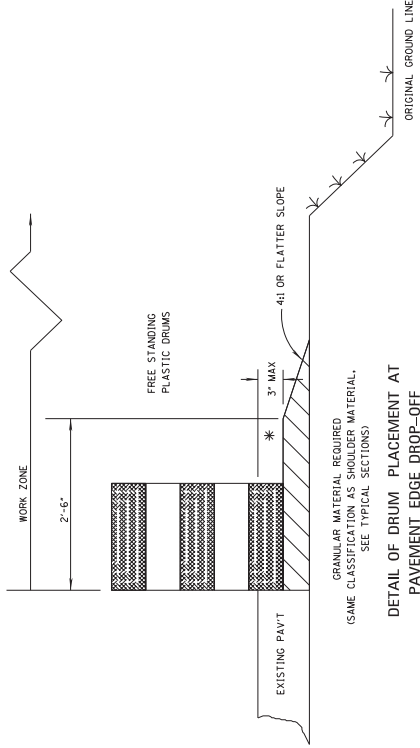


DESIGNED BY: _____
 DETAILED BY: _____
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 DATE: _____

FMS CON: 308317/301000
 PROJECT NO.: MP-7149-4(100) - 1
 COUNTY: SIMPSON

TRAFFIC CONTROL DETAILS
 DRUM PLACEMENT AND SPACING
 Office of Design & Construction

REV. NO. _____
 SDT# 16
 SHEET NO. 9



NOTES:

* A. PAVEMENT EDGE DROP-OFF

- IF LESS THAN TWO AND ONE QUARTER (2.25) INCHES-NO PROTECTION REQUIRED. PLACE A SHOULDER WORK SIGN (W21-5) 500 FEET IN ADVANCE OF WORK ZONE SHOULDER AND A LOW SHOULDER SIGN (W8-9) AT THE BEGINNING AND THROUGHOUT THE WORK ZONE @ (1.1 MILE ±0.C.).
- OF 50 MILES PER HOUR OR GREATER, CONES MAY BE USED IN PLACE OF DRUMS, PANELS, AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEEDS LESS THAN 50 MILES PER HOUR AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 50 FEET. SPACING FOR TAPERS SHOULD BE IN ACCORDANCE WITH THE MUTCD. (1 / 3" L, WHERE L IS THE TAPER LENGTH IN FEET.)
- GREATER THAN THREE (3) INCHES-POSITIVE SEPARATION OR WEDGE WITH 4:1 OR FLATTER SLOPE NEEDED. IF THERE IS EIGHT (8) FEET OR MORE DISTANCE BETWEEN THE EDGE OF TRAVEL LANE AND DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED.
- FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN THREE (3) INCHES MAY BE PROTECTED WITH DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.
- LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

B. DRUM SPACING

- TANGENTS = $2 \times S$
 - TAPERS = $L / 3$
- WHERE $L = S \times W$
 $S =$ SPEED IN MPH (POSTED OR 85 PERCENTILE)
 $W =$ WIDTH OF OFFSET IN FEET

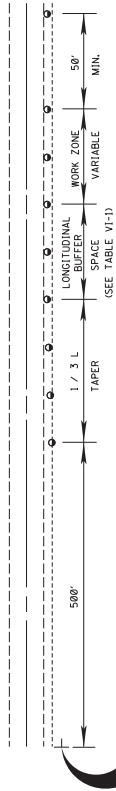
C. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

TABLE (VI-1) GUIDELINES FOR LENGTH OF LONGITUDINAL BUFFER SPACE

* SPEED (MPH)	LENGTH (FEET)
20	55
25	65
30	75
35	85
40	100
45	120
50	140
55	160
60	180
65	200
70	220
75	240
80	260
85	280
90	300
95	320
100	340

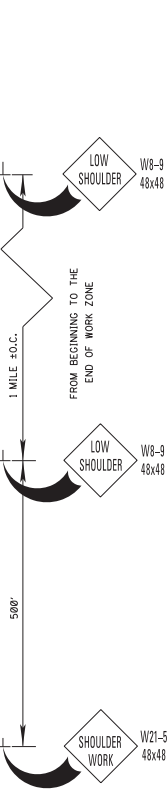
* POSTED SPEED, OFF-PEAK 85 PERCENTILE SPEED PRIOR TO WORK STARTING, OR THE ANTICIPATED OPERATING SPEED IN MPH.

PLASTIC DRUMS
 (SEE NOTE FOR SPACING)



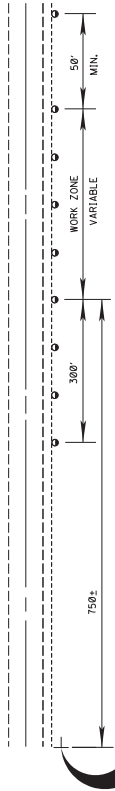
TYPICAL SHOULDER CLOSURE

- TO BE USED WITH EIGHT (8) FOOT OR GREATER WIDTH IMPROVED SHOULDER.
- TO BE USED WHEN CONSTRUCTION VEHICLES (EQUIPMENT) ENCOACHES ON OR WITHIN TWO (2) FEET OF THE SHOULDER BREAK.



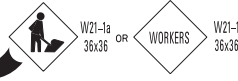
TYPICAL SHOULDER WORK #1
 (SEE NOTE A-1 THIS SHEET)

PLASTIC DRUMS
 (SEE NOTE FOR SPACING)



TYPICAL SHOULDER WORK #2

NOTE:
 WORK OUTSIDE TWO (2) FOOT AND WITHIN TEN (10) FEET OF THE SHOULDER BREAK MAY BE PROTECTED BY PLASTIC DRUMS AT THE SHOULDER EDGE, 300 FEET PRIOR TO AND 50 FEET BEYOND THE WORK AREA, OR SEE NOTE A-3 THIS SHEET.





MISSISSIPPI DEPARTMENT OF TRANSPORTATION
MDOT

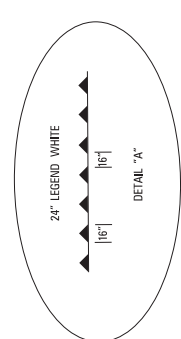
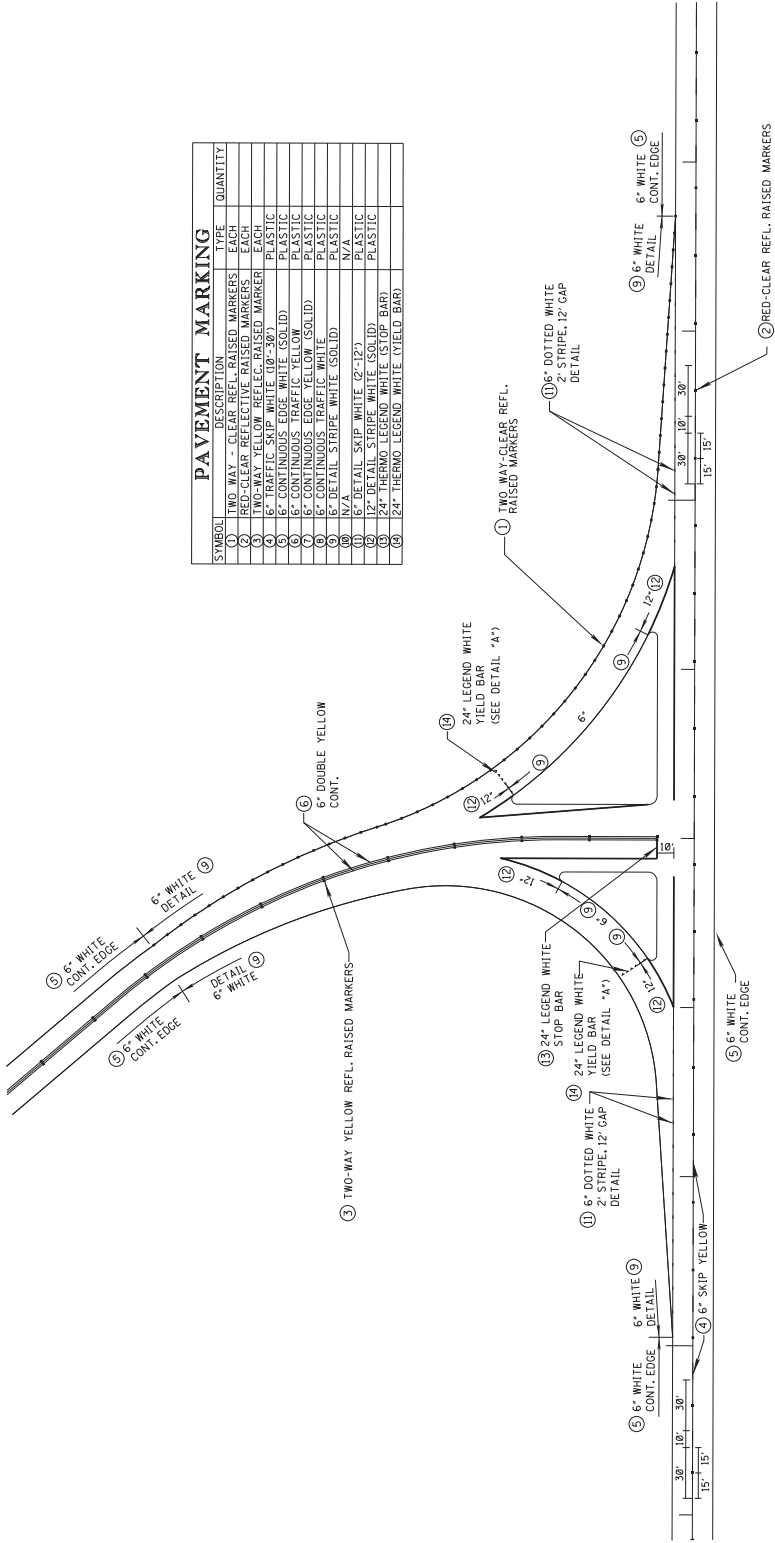
DESIGNED BY:
 CHECKED BY:
 DATE:

FMS CON: 308317/301000
 PROJECT NO.: MP-7149-6-2011
 COUNTY: SIMPSON

DETAIL OF STRIPING
 2-LAND HIGHWAY
 5045-- Cont

WYO. PAPER
 SHEET NO. 10

SYMBOL	DESCRIPTION	TYPE	QUANTITY
1	TWO WAY - CLEAR REFL. RAISED MARKERS	EACH	
2	RED-CLEAR REFLECTIVE RAISED MARKERS	EACH	
3	RED-CLEAR REFLECTIVE RAISED MARKERS	EACH	
4	6" TRAFFIC SKIP WHITE (10'-30')	PLASTIC	
5	6" CONTINUOUS EDGE WHITE (SOLID)	PLASTIC	
6	6" CONTINUOUS TRAFFIC YELLOW	PLASTIC	
7	6" CONTINUOUS TRAFFIC WHITE (SOLID)	PLASTIC	
8	6" CONTINUOUS TRAFFIC WHITE (SOLID)	PLASTIC	
9	6" DETAIL STRIPE WHITE (SOLID)	PLASTIC	
10	N/A	N/A	
11	6" DETAIL SKIP WHITE (2'-12')	PLASTIC	
12	6" DETAIL SKIP WHITE (10'-30')	PLASTIC	
13	24" THERMO LEGEND WHITE (STOP BAR)	PLASTIC	
14	24" THERMO LEGEND WHITE (YIELD BAR)	PLASTIC	





MISSISSIPPI DEPARTMENT OF TRANSPORTATION

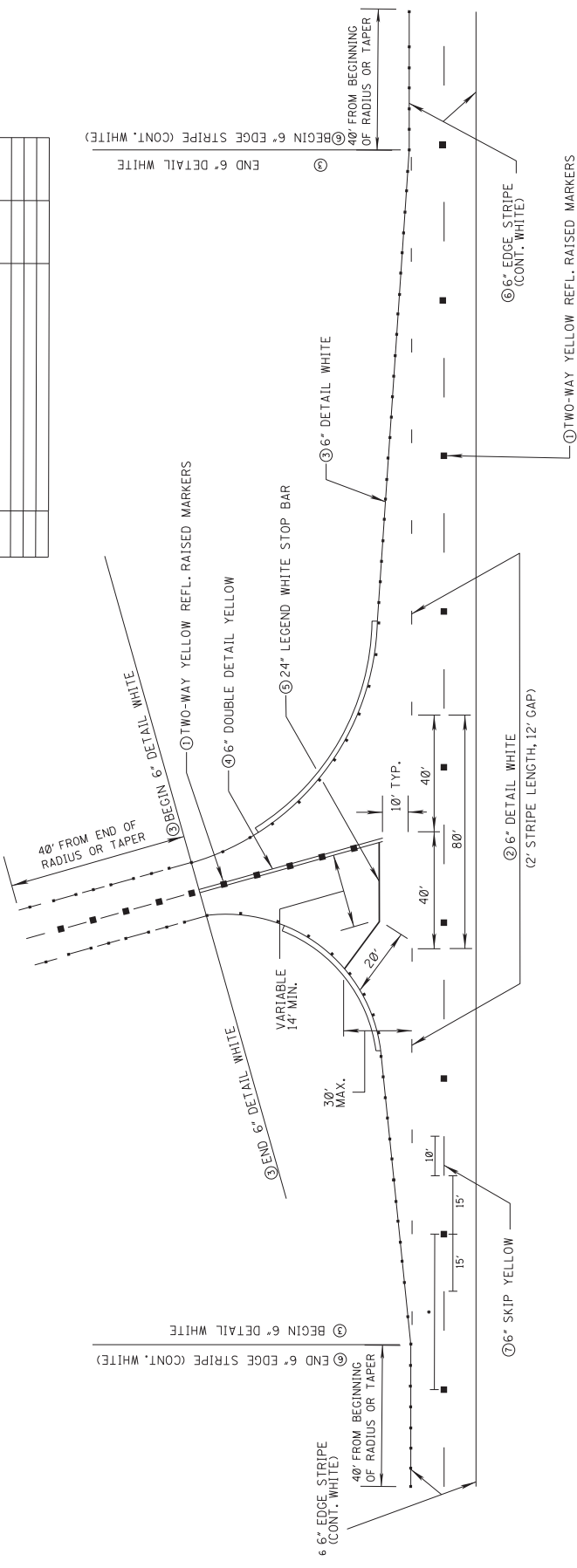
DESIGNED BY: _____
 DETAILED BY: _____
 CHECKED BY: _____
 DATE: _____

FMS CON: 308317/301000
 PROJECT NO.: MP-7149-4 (001) - 13
 COUNTY: SIMPSON

DETAIL OF STRIPING
 NOT TO SCALE
 TWO-LANE HIGHWAY

W/40' PIPES
 SHEET NO. 11

PAVEMENT MARKING			
SYMBOL	DESCRIPTION	TYPE	QUANTITY
①	TWO-WAY YELLOW-REFLEC-RAISED MARKER	PLASTIC	
②	6" DETAIL SKIP WHITE (2'-12")	PLASTIC	
③	6" DETAIL STRIPE WHITE (SOLID)	PLASTIC	
④	6" DETAIL STRIPE YELLOW (SOLID)	PLASTIC	
⑤	24" THERMO LEGEND WHITE (STOP BAR)	PLASTIC	
⑥	6" CONTINUOUS EDGE WHITE (SOLID)	PLASTIC	
⑦	6" TRAFFIC SKIP YELLOW (10'-30')	PLASTIC	



GENERAL NOTES:
 • SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

	URBAN AREA (FT - IN)	RURAL AREA (FT - IN)
TANGENT SECTIONS	40' - 0"	80' - 0"
HORIZONTAL CURVES	40' - 0"	40' - 0"
INTERCHANGE LIMITS	40' - 0"	40' - 0"



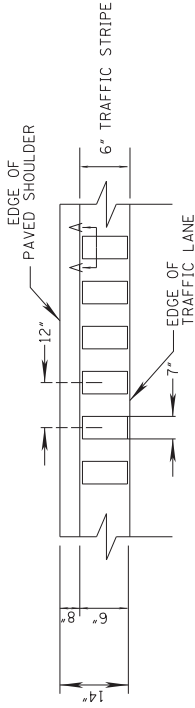
DESIGNED BY:	
DETAILED BY:	
CHECKED BY:	
DATE:	

FMS CON: 308317/301000
PROJECT NO.: MP-7149-6-1001 - 14
COUNTY: SIMPSON

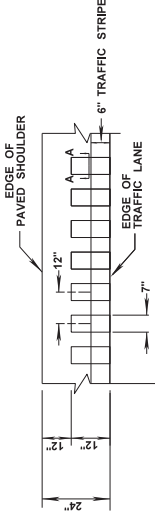
Notice to Bidders No. 5045-- Cont.

W.P. NO. R-27
SHEET NO. 12

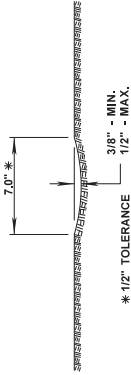
- GENERAL NOTES
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED ON LEFT AND RIGHT SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT
 - GROUND-IN RUMBLE STRIPES SHALL BE OMITTED ACROSS PRINCIPAL INTERSECTING ROADWAYS OR OTHER INTERRUPTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER
 - COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS
 - GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO MAINLINE ONLY



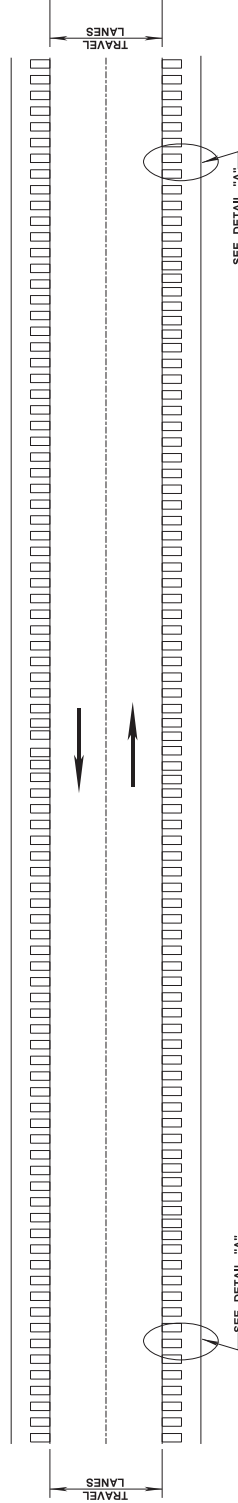
DETAIL "A"
12' PAVEMENT EDGE



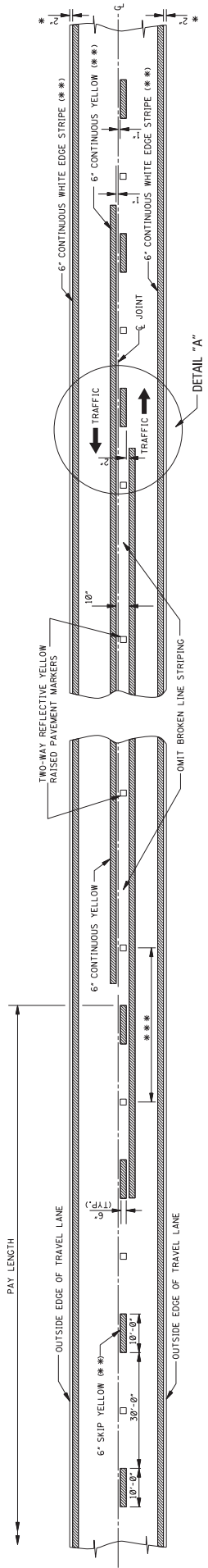
DETAIL "A"
14' PAVEMENT EDGE



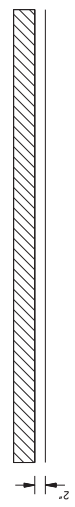
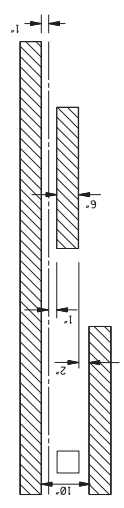
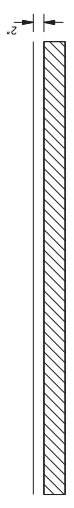
SECTION "A-A"



PLAN
NOT TO SCALE



TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



DETAIL "A"

NOTE: THE CRITERIA FOR NO-PASSING ZONES CAN BE FOUND IN THE MOOT ROADWAY DESIGN MANUAL, SECTION 11-1.01.

GENERAL NOTES:

1. 2" UNLESS SHOWN ELSEWHERE ON THE PLANS, FOR STRIPING ON RUMBLE STRIP SECTIONS REFER TO WK. SHEETS RS-1, RS-2, AND RS-3.
2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR PLASTIC AS INDICATED IN PAY ITEMS).
3. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS:

TANGENT SECTIONS	URBAN AREA	RURAL AREA
40'-0"	40'-0"	80'-0"
80'-0"	40'-0"	40'-0"
INTERCHANGE LIMITS	40'-0"	1-40'-0"

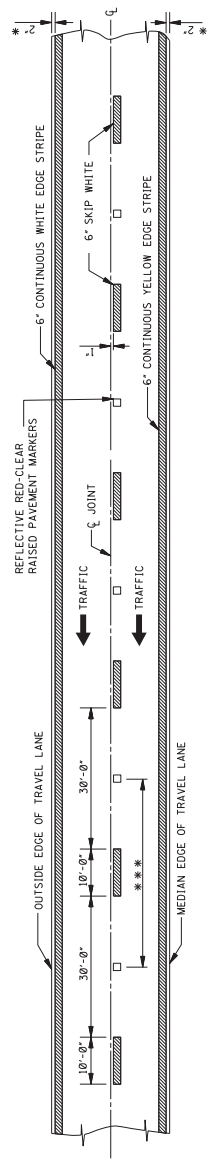
1. NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED ON ALL INTERCHANGES. REFLECTIVE RAISED PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE INTERCHANGE AREA INCLUDING 1000' TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
4. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE. RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT *APPROVED SOURCES OF MATERIALS.*

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

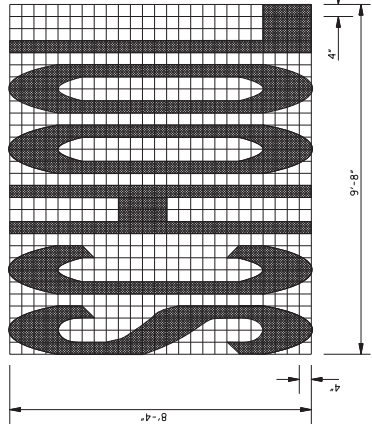
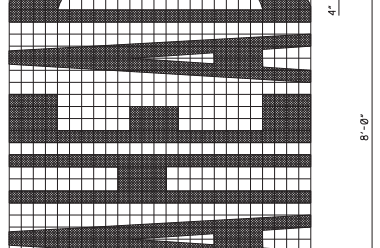
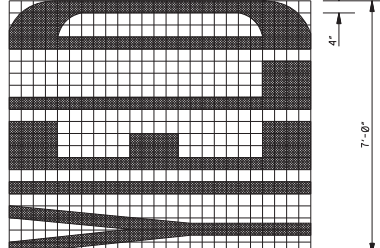
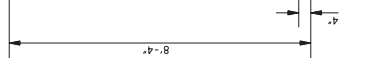
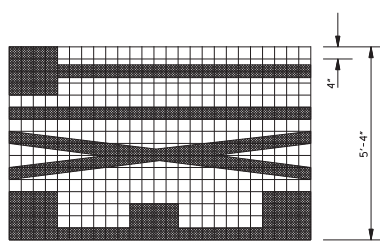
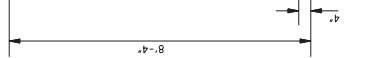
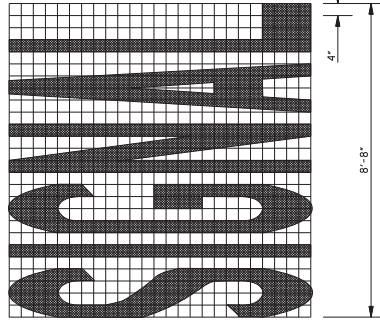
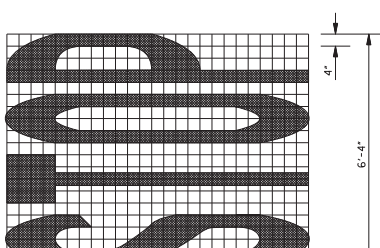
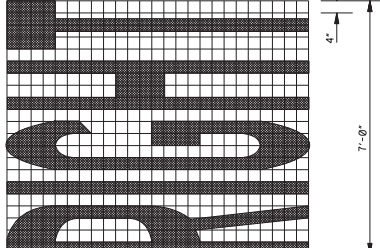
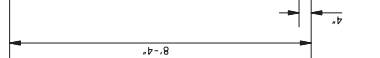
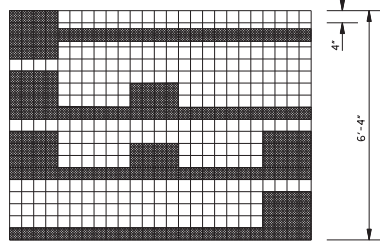
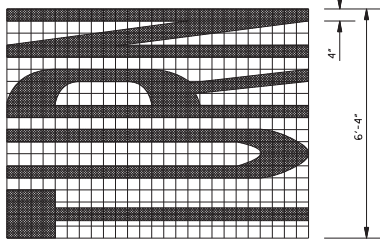
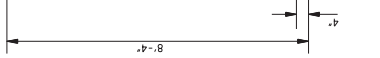
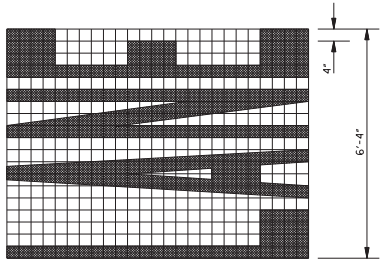
**PAVEMENT MARKING
DETAILS FOR
2-LANE AND 4-LANE
DIVIDED ROADWAYS**

ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: PM-1
SHEET NUMBER: 6/051



4-LANE WITH ONE-WAY TRAFFIC



- GENERAL NOTES:
- TWO HORIZONTAL GAPS, CAUSED BY THE CURVE OF THE LETTERS, OR LESS, AND EXTENDING THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
 - FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 - PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

PAY QUANTITIES	
LEGEND	AREA (SQ. FT.)
STOP	24.6
RIGHT	28.6
LEFT	17.3
TURN	27.3
LANE	22.7
AHEAD	32.3
YIELD	26.8
EXIT	18.5
SIGNAL	32.5
SCHOOL	35.5

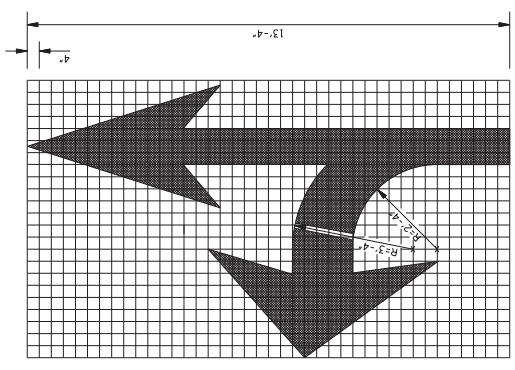
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
LEGEND DETAILS**

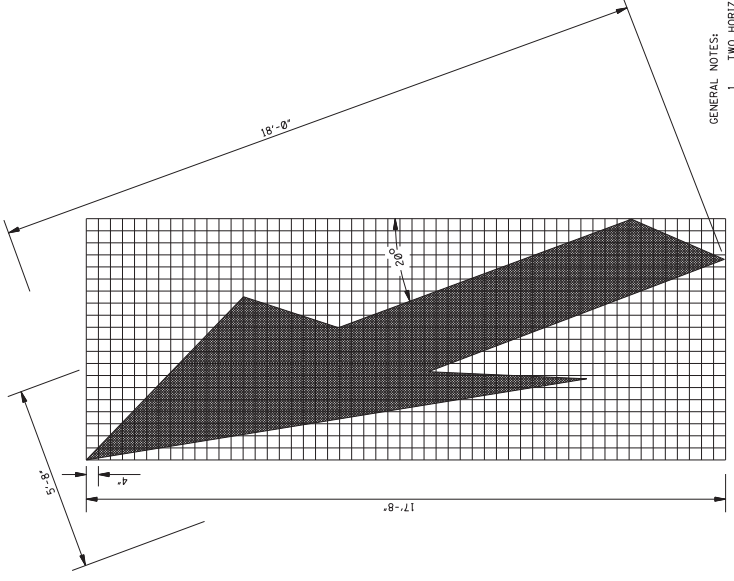
ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 5045-
DRAWING NUMBER: PM-5
SHEET NUMBER: 6055

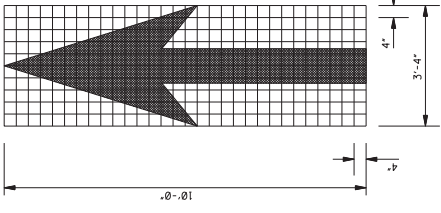
DATE	REVISION	BY



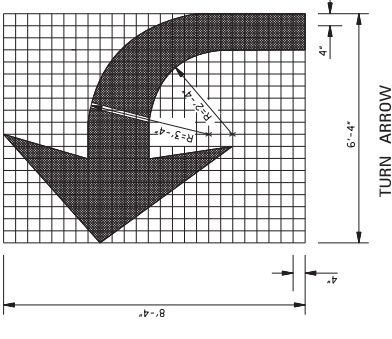
COMBINATION ARROW



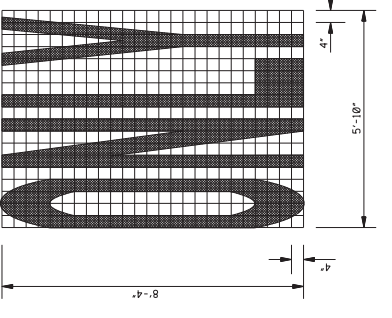
LANE-REDUCTION ARROW



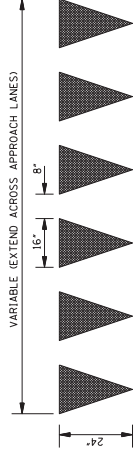
THRU ARROW



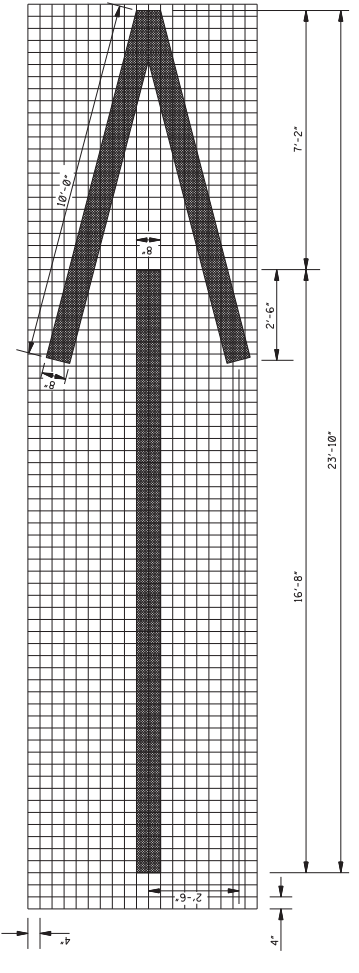
TURN ARROW



1-WAY ARROW



YIELD LINE



- GENERAL NOTES:
1. TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTORS) OF 1/2" OR LESS AND EXTENDING THE FULL WIDTH ARE PERMITTED IN EACH LETTER.
 2. FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 3. DIMENSIONS OF THE YIELD LINE MAY VARY WITH APPROVAL OF THE ENGINEER. SEE MUTCD, LATEST EDITION, FOR ALLOWABLE DIMENSIONS.
 4. PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

PAY QUANTITIES	
LEGEND/SYMBOL	AREA (FF+)
ONLY	22.0
TURN ARROW	16.4
THRU ARROW	12.3
COMB. ARROW	27.5
1-WAY ARROW	24.3
LANE REDUCTION ARROW	40.0

Notice to Bidders No. 5045--

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

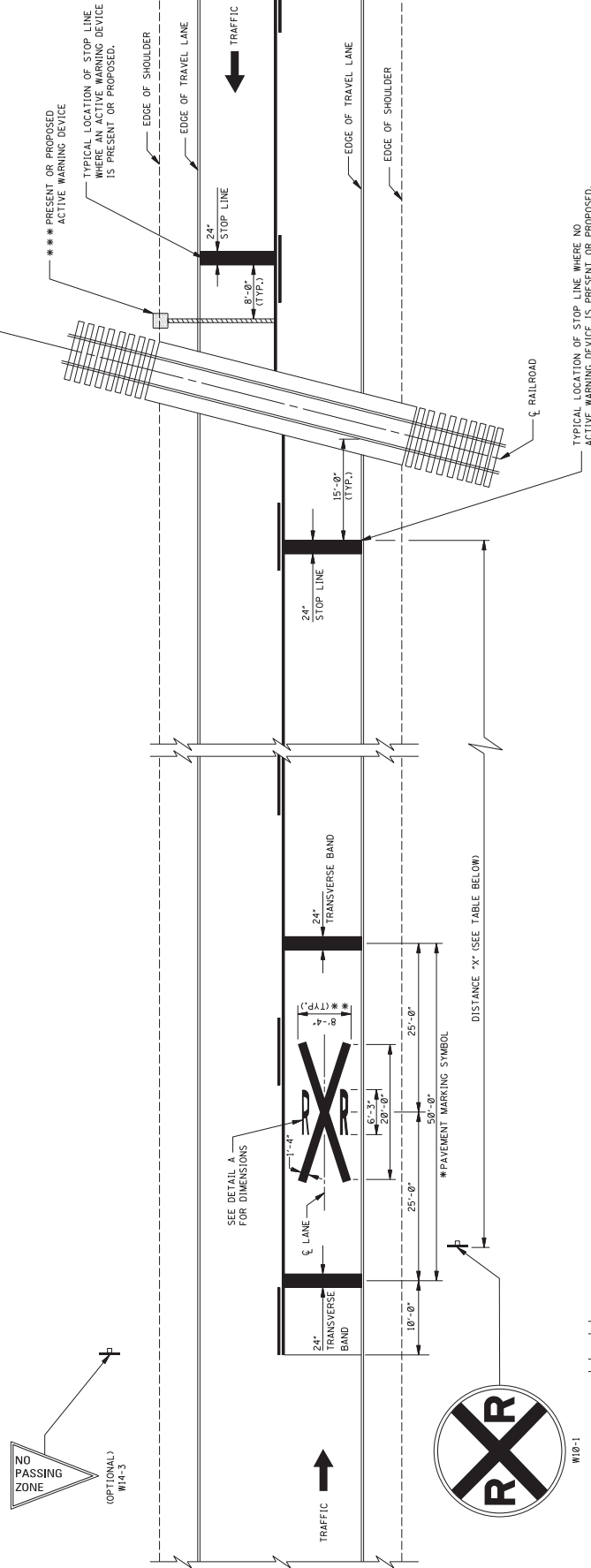
**PAVEMENT MARKING
LEGEND DETAILS**

ISSUE DATE: AUGUST 01, 2017

60556

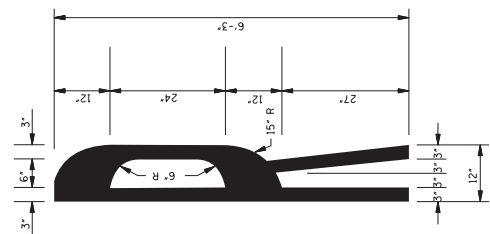


DATE	REVISION



POSTED SPEED (mph)	⓪ DISTANCE "X" (ft)
20	100
25	100
30	100
35	100
40	125
45	175
50	250
55	325
60	400
65	475

NOTE:
 ⓪ DISTANCE "X" MAY BE ADJUSTED IF PROHIBITIVE PHYSICAL CONDITIONS EXIST AT THE DESIGNATED DISTANCE.



DETAIL A
STANDARD "R" PAVEMENT MARKING FOR R X R SYMBOL

- GENERAL NOTES:
- ** 1. A PORTION OF THE PAVEMENT MARKING SYMBOL SHOULD BE DIRECTLY OPPOSITE THE ADVANCE WARNING SIGN (W10-1).
 - ** 2. WIDTH OF R X R SYMBOL MAY VARY ACCORDING TO LANE WIDTH. HOWEVER, ON MULTI-LANE ROADS, THE TRANSVERSE BANDS AND STOP LINE SHOULD BE LOCATED TO BE CENTERED IN EACH APPROACH LANE. R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.
 - 3. R X R SYMBOL (63.0 ft²), TRANSVERSE BANDS AND STOP LINE SHALL BE PAID FOR AS LEGEND, WHITE.
 - ** 3.4. REFER TO THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" FOR LOCATION OF PROPOSED WARNING DEVICES AT RAILROAD-HIGHWAY GRADE CROSSINGS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TYPICAL PLACEMENT OF WARNING SIGNS AND PAVEMENT MARKINGS AT RAILROAD HIGHWAY GRADE CROSSINGS

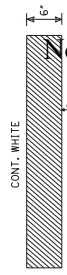
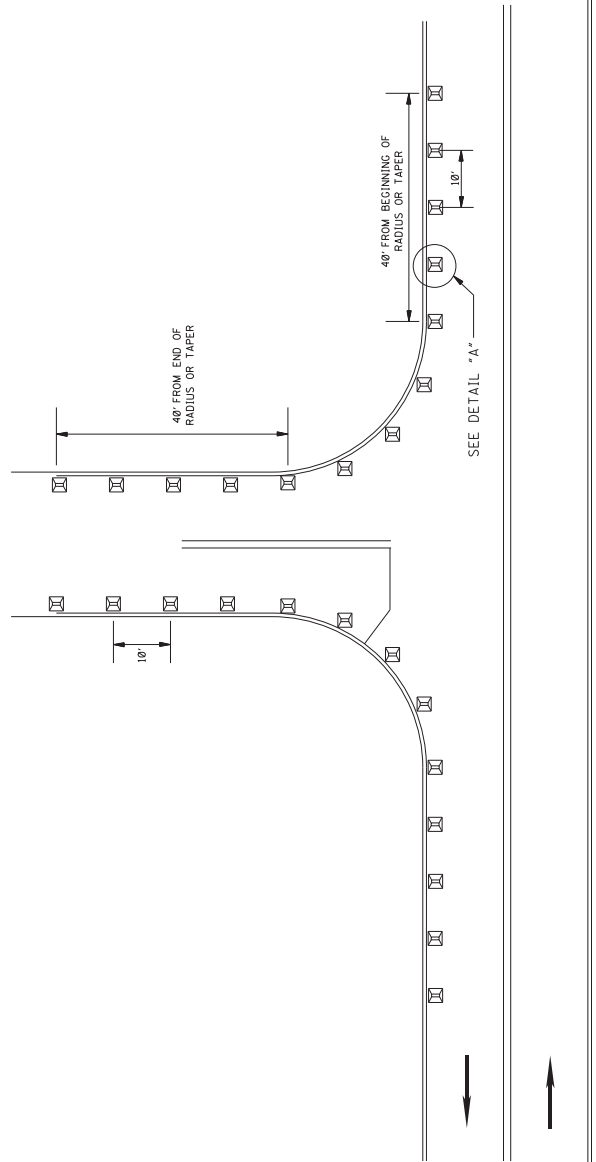
ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 5045-1

REVISION NUMBER: PM-7

DATE: 6/05/17

TYPICAL PLACEMENT OF RAISED PAVEMENT MARKERS ON SIDE ROAD RADIUS 2-LANE, TWO WAY TRAFFIC



GENERAL NOTES:

1. MARKERS SHALL BE VISIBLE FROM THE TRAVELING MOTORIST ON STATE DESIGNATED HIGHWAYS.
2. MARKERS SHALL BE HIGH PERFORMANCE TWO-WAY CLEAR.
3. MARKERS SHALL NOT BE ROTATED WHEN BEING PLACED ALONG RADIUS AND TANGENT SECTION OF LOCAL ROAD.
4. MARKERS SHALL BE INSTALLED AT SIMPLE AND CHANNELIZED INTERSECTIONS TO THE LIMITS SHOWN ABOVE.

Notice to Bidders No. 5045-

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

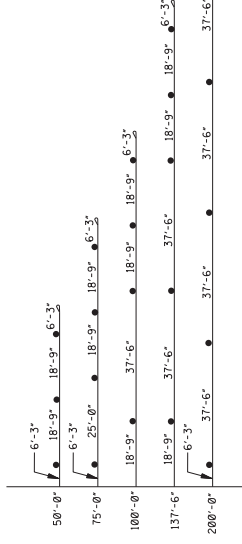
2-WAY RAISED PAVEMENT MARKERS AT INTERSECTING ROADS (2-LANE)

ISSUE DATE: AUGUST 01, 2017

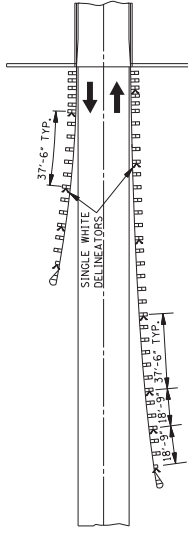
DATE	REVISION

PROJECT NUMBER: 5045-11
DRAWING NUMBER: 6061

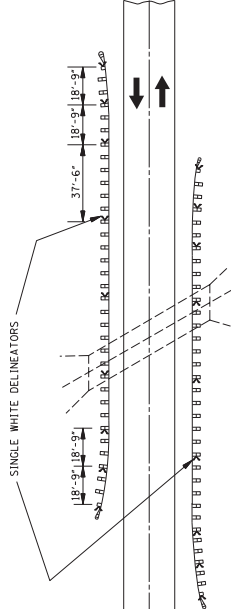
STATE PROJECT NO.
MISS.



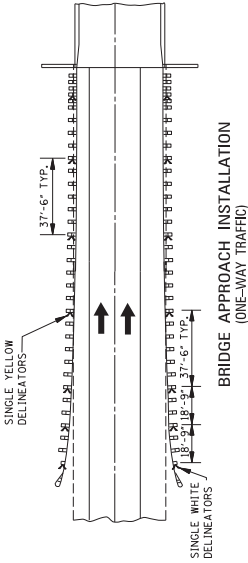
GRAPHIC SHOWING SPACINGS OF GUARDRAIL DELINEATORS AT SOME COMMONLY USED BRIDGE APPROACHES



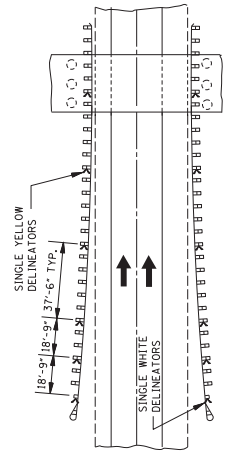
BRIDGE APPROACH INSTALLATION (TWO-WAY TRAFFIC)



ROADSIDE OBSTACLE INSTALLATION—LENGTH 250' OR LESS (TWO-WAY TRAFFIC)



BRIDGE APPROACH INSTALLATION (ONE-WAY TRAFFIC)



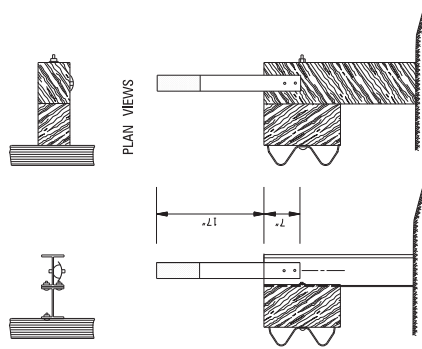
ROADSIDE OBSTACLE INSTALLATION—LENGTH 250' OR LESS (ONE-WAY TRAFFIC)

EMBANKMENT OR ROADSIDE OBSTACLE INSTALLATION—LENGTH GREATER THAN 250' (ONE-WAY TRAFFIC)

NOTE: ONE-WAY TRAFFIC SHOWN. DELINEATOR SPACING FOR TWO-WAY TRAFFIC SIMILAR. DELINEATOR COLOR WILL BE THE SAME AS THE ADJACENT PAVEMENT EDGE MARKING. THE FIRST THREE (3) MARKERS WILL FACE TRAFFIC IN OFF LANE FOR TWO-WAY TRAFFIC AS SHOWN IN DRAWING FOR OBSTACLE INSTALLATION FOR TWO-WAY TRAFFIC.

GENERAL NOTES:

1. THE UNIT PRICE OF DELINEATOR INCLUDES: COSTS OF DELINEATOR FACE(S), POST, HARDWARE AND INSTALLATION.
2. DELINEATOR FACE WILL BE ENCAPSULATED LENS REFLECTIVE SHEETING.
3. DELINEATORS FOR GUARDRAIL SHALL BE MOUNTED ON FLEXIBLE POSTS AS FOLLOWS: THE DELINEATOR POSTS WILL BE FROM THE DEPARTMENT'S "APPROVED SOURCE OF MATERIALS" AND WILL BE FASTENED TO GUARDRAIL POST IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION.



DETAIL OF FLEXIBLE GUARDRAIL DELINEATOR

TYPICAL FLEXIBLE POST DELINEATOR GUARDRAIL INSTALLATION

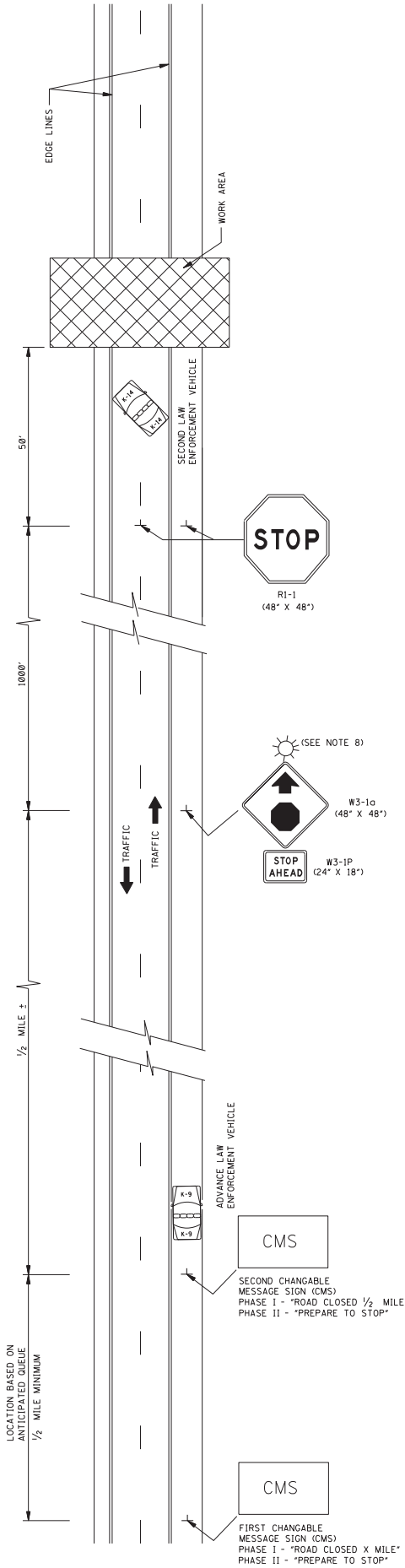
Notice to Bidders No. 5045-

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TYPICAL GUARDRAIL DELINEATION

ISSUE DATE: AUGUST 01, 2017

6317



GENERAL NOTES:

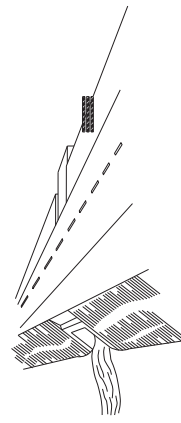
- THIS TYPE OF HIGHWAY CLOSURE SHOULD ONLY BE USED FOR CONSTRUCTION OPERATIONS THAT WILL BE COMPLETED WITHIN 30 MINUTES. AFTER THE HIGHWAY HAS BEEN CLOSED AND REOPENED VIA THE CONTRACT DOCUMENTS, ALL VEHICLES SHOULD ELIMINATE BEFORE ANOTHER SHORT DURATION CLOSURE, EXCEPT WITH THE APPROVAL OF THE ENGINEER.
- AT LEAST TWO LAW ENFORCEMENT OFFICERS AND TWO LAW ENFORCEMENT VEHICLES SHOULD BE PROVIDED ON EACH APPROACH TO THE CLOSURE. EACH LAW ENFORCEMENT VEHICLE SHOULD HAVE A ROOF MOUNTED FLASHING BLUE LIGHT OR LIGHT BAR.
- RESTRICTIONS ON ROAD CLOSURES ARE SPECIFIED IN THE CONTRACT DOCUMENT.
- THE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD BE MOVED BACK AS REQUIRED BY THE QUEUING OF STOPPED VEHICLES.
- IF QUEUE EXCEEDS THE FIRST CHANGABLE MESSAGE SIGN (CMS) AT ANY TIME DURING A CLOSURE, THE TRAFFIC CONTROL PLAN SHOULD BE ADJUSTED AS NECESSARY WITH APPROVAL OF THE ENGINEER.
- TRAFFIC CONTROL FOR THE CLOSURE SHOULD BE ACCOMPLISHED IN THE FOLLOWING ORDER:
 - FIRST CHANGABLE MESSAGE SIGN (CMS)
 - SECOND CHANGABLE MESSAGE SIGN (CMS)
 - ADVANCE LAW ENFORCEMENT VEHICLE, LIGHTS AND FLASHERS ON.
 - "W3-1a (48" X 48") AND "W3-1P (24" X 18") SIGNS ERECTED.
 - "R1-1 (48" X 48") SIGNS ERECTED TO STOP TRAFFIC. THE ORDER OF ERECTION SHOULD BE IN THE FOLLOWING ORDER: RIGHT SHOULDER THEN CENTER.
 - SECOND LAW ENFORCEMENT VEHICLE, LIGHTS AND FLASHERS ON.
- TRAFFIC CONTROL SHOULD BE REMOVED IN THE FOLLOWING ORDER:
 - WITH TRAFFIC STOPPED REMOVE THE "R1-1 SHOULDER IN THE FOLLOWING ORDER: CENTER THEN SIGN ON THE RIGHT SHOULDER. SECOND LAW ENFORCEMENT VEHICLE LEADS TRAFFIC THROUGH WORK AREA.
 - AFTER ALL STOPPED VEHICLES HAVE STARTED MOVING THE "W3-1a (48" X 48") AND "W3-1P (24" X 18") SIGNS SHOULD BE REMOVED. THESE SIGNS MAY BE COVERED IF RE-USE IS IMMINENT.
 - AFTER ALL VEHICLES HAVE RESUMED APPROXIMATELY NORMAL SPEED, THE CHANGABLE MESSAGE SIGNS TURNED OFF.
- UNILLUMINATED SECTIONS OF HIGHWAYS SHOULD NOT BE CLOSED DURING HOURS OF DARKNESS EXCEPT FOR EMERGENCIES OR WITH THE APPROVAL OF THE ENGINEER. IF DARKNESS OCCURS DURING HOURS OF DARKNESS, A TYPE B HIGH INTENSITY FLASHING BARRICADE WARNING LIGHT SHALL BE USED ON EACH W3-1a SIGN.
- IF AN ENTRANCE RAMP IS LOCATED BETWEEN THE SECOND CMS AND R1-1, THE CMS, SIGNS SHOULD ALSO BE ERECTED ON THE RAMP SHOULDER.
- THE ABOVE DURATION WILL APPLY TO EACH APPROACH TO THE CLOSURE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC, INCLUDING SECURING LAW ENFORCEMENT SERVICES.

REVISION		DATE	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

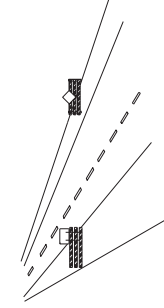
SHORT DURATION CLOSING OF TWO-LANE TWO-WAY HIGHWAYS

PROJECT NUMBER: 5045-
DRAWING NUMBER: 100-6
SHEET NUMBER: 6356
ISSUE DATE: AUGUST 01, 2017



WING BARRICADES

- WING BARRICADES ARE TYPE III BARRICADES ERECTED ON THE SHOULDER ON ONE OR BOTH SIDES OF THE PAVEMENT TO GIVE THE SENSATION OF A NARROWING OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.
 - IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.
 - IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.

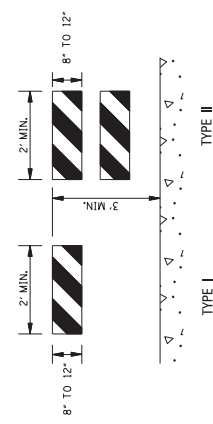


BARRICADE CLOSING A ROAD

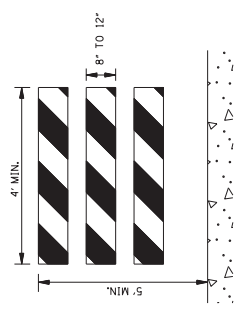
BARRICADE CHARACTERISTICS

	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24" MIN.	24" MIN.	48" MIN.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF FACTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 6 IF FACING TRAFFIC IN TWO DIRECTIONS

- * 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
- ** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS SHALL HAVE A MINIMUM OF 270 sq ft OF REFLECTIVE AREA FACING TRAFFIC.



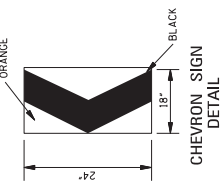
TYPE I



TYPE II

STANDARD BARRICADES

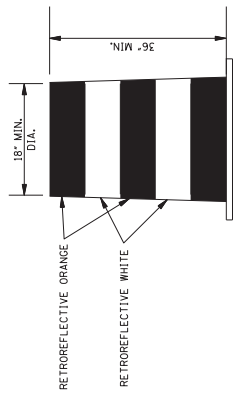
- THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).
- RAIL STRIPE SHOULD BE 6 INCHES EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.
- DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.
- FOR ADDITIONAL INFORMATION OR DETAILS: SEE MUTCD, LATEST EDITION.
- BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WORK ZONE DEVICES WHICH REQUIRE CRASHWORTHINESS ACCEPTANCE LETTERS. TO DATE, 2-IN. THICK TIMBER RAILS HAVE NOT BEEN SUCCESSFULLY CRASH TESTED. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE: http://safety.fhwa.dot.gov/roadway_dept/policy/guide/road_hardware/cat2.cfm



CHEVRON SIGN DETAIL

- A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.
- THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.
- CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2'-0" BEHIND THE LANE TRANSITION STRIPE.

PLASTIC DRUM STRIPING DETAIL



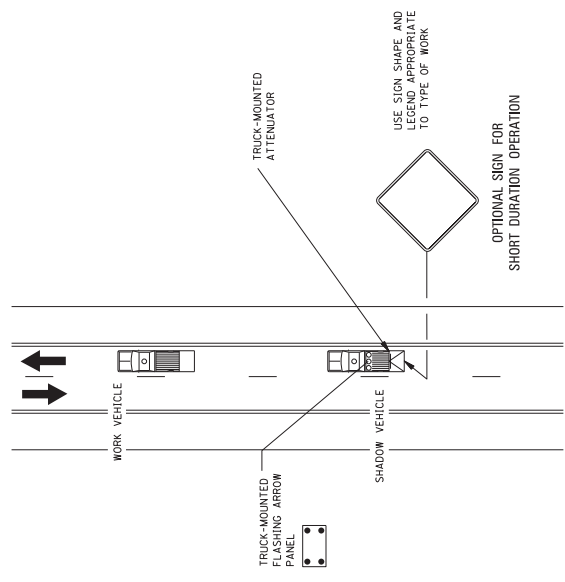
- PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR CONSISTENT STRIPING THROUGHOUT A PROJECT. PLASTIC DRUMS SHALL BE CONFORMANT WITH MARKING STANDARDS FOR BARRICADE. THE PREDOMINANT COLOR ON STRIPES SHALL BE ORANGE WITH FOUR (4) RETROREFLECTIVE, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.
- DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.
- WHERE PRACTICAL PLASTIC DRUMS SHOULD BE PLACED NO CLOSER THAN 10' FROM THE EDGE OF TRAVELED LANE.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS

PROJECT NUMBER: 5045-
SHEET NUMBER: 6358
ISSUE DATE: AUGUST 01, 2017

MOBILE OPERATIONS ON TWO-LANE ROAD

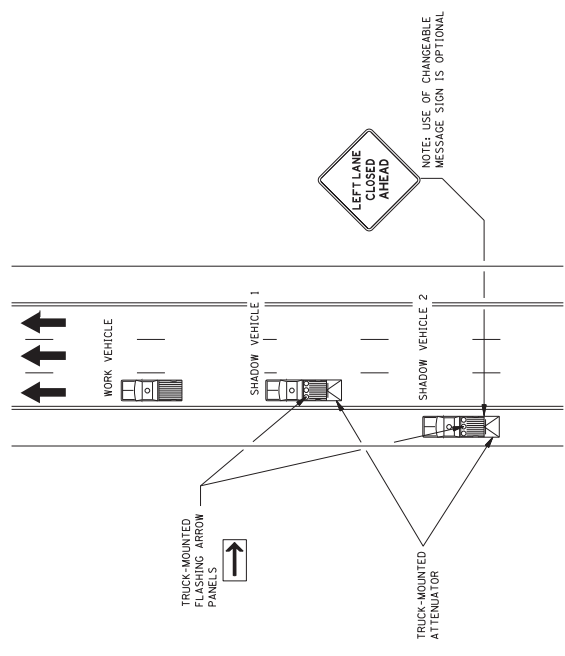


MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES FOR TWO-LANE OPERATION:

1. THESE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY, AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
2. THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO THE TYPE OF OPERATION. SHADOW VEHICLES SHOULD ALWAYS BE USED TO WARN TRAFFIC OF THE OPERATION AHEAD, WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR. THE SHADOW VEHICLE SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
3. ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
4. A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
5. THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS, AND THE SHADOW VEHICLES SHALL BE EQUIPPED WITH TWO HIGH-INTENSITY FLASHING LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN. SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
6. VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBTURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
7. ARROW BOARD TO BE USED IN CAUTION MODE.
8. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MOBILE OPERATIONS ON MULTILANE ROAD



MOBILE OPERATIONS ON MULTILANE ROAD

NOTES FOR MULTILANE LANE OPERATION:

1. VEHICLES USED FOR THESE OPERATIONS SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT, SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLAGS, SIGNS, OR ARROW PANELS.
2. SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK MOUNTED ATTENUATOR (TMA), AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON SHADOW VEHICLE 2 SO AS NOT TO OBTURE THE ARROW PANEL.
3. SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK-MOUNTED ATTENUATOR (TMA).
4. SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
5. WHEN ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, SHADOW VEHICLE 2 SHOULD BE ELIMINATED.
6. ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE SHOULD BE USED (i.e., VEHICLE 3 ON THE SHOULDER IF PRACTICAL), VEHICLE 2 IN THE CLOSED LANE, AND VEHICLE 1 IN THE CLOSED LANE.
7. ARROW PANELS SHALL BE AS A MINIMUM TYPE B, 60" X 30" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
8. WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
9. VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBTURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
10. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY DESIGN DIVISION	
STANDARD PLAN	
TRAFFIC CONTROL PLAN	
MOBILE OPERATIONS	
MULTILANE ROADS	
AND	
TWO-LANE ROADS	
DATE	ISSUE DATE:
	AUGUST 01, 2017

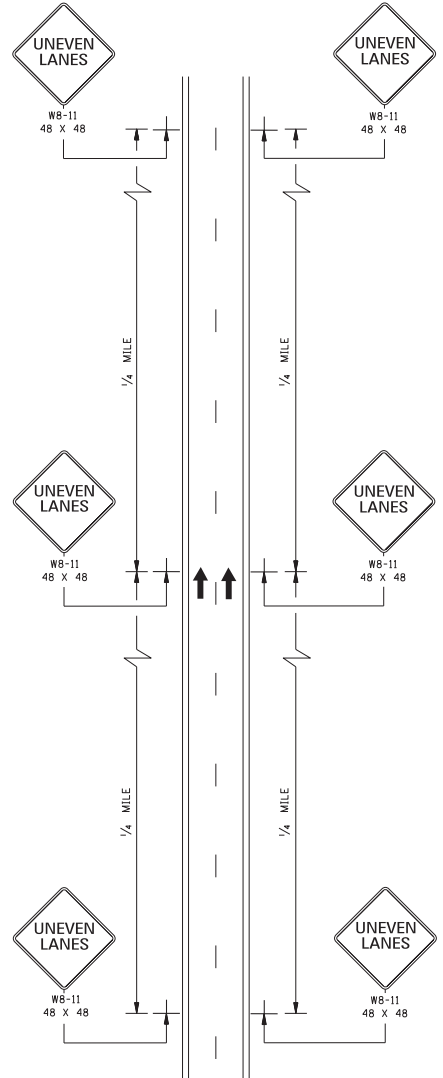
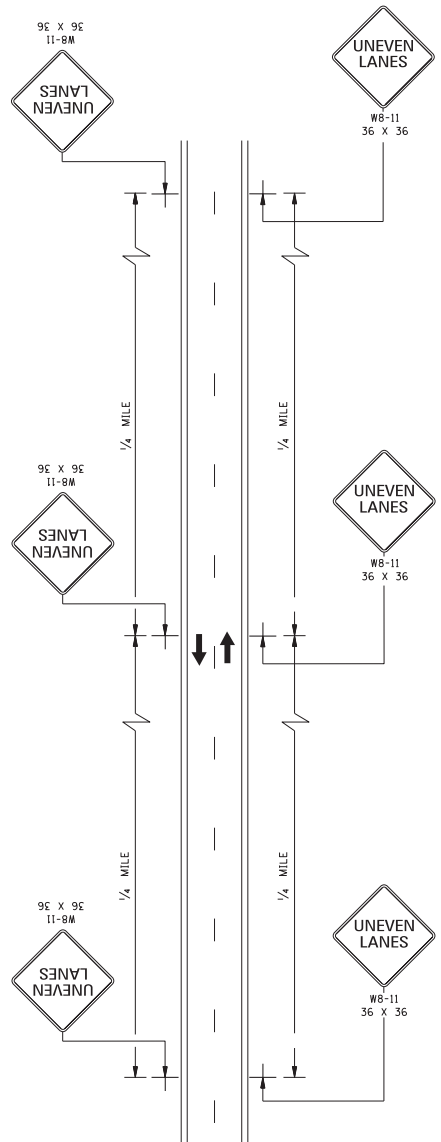
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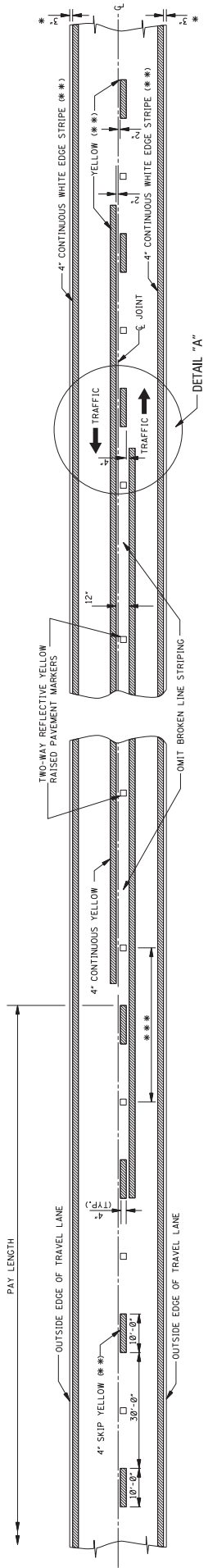
GENERAL NOTES:

1. UNEVEN LANE LINE:
 - A. WHEN EQUAL TO 1/4 MILE, NO SIGNS REQUIRED.
 - B. IF GREATER THAN 1/4 MILE AND LESS THAN OR EQUAL TO 2/4 MILE, PLACE SIGNS AS SHOWN ON THIS SHEET.
 - C. IF GREATER THAN 2/4 MILE, TRAFFIC SHOULD NOT BE ALLOWED TO CROSS UNEVEN LANE LINE.
2. THE WB-11 SIGNS SHOULD BE SPACED AT 1/4-MILE INTERVALS THROUGHOUT UNEVEN LANE LINE LIMITS.
3. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER MAINTENANCE OF TRAFFIC.

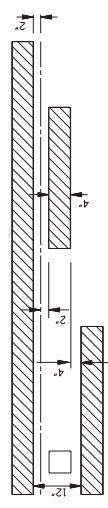
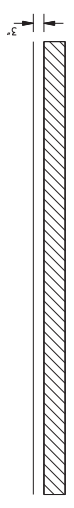
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
TRAFIC NUMBER LCP-12	PROJECT NUMBER 6362
ISSUE DATE: AUGUST 01, 2017	
REVISION	BY

**TRAFFIC CONTROL PLANS
UNEVEN PAVEMENT
DETAILS**

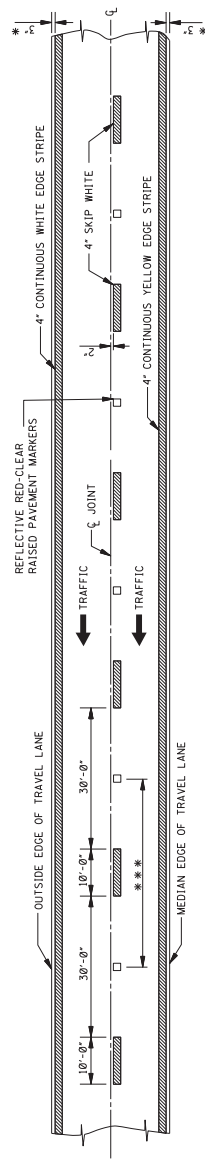




TWO-WAY TRAFFIC
(ASPHALT OR CONCRETE PAVEMENT)



DETAIL "A"



4-LANE WITH ONE-WAY TRAFFIC

- GENERAL NOTES:
- * 1. 3" UNLESS SHOWN ELSEWHERE ON THE PLANS.
 - ** 2. EDGE STRIPE SHALL BE SAME MATERIAL AS LANE-LINE STRIPE (PAINT OR TAPE AS INDICATED IN PAY ITEMS).
 - 3. REFLECTIVE RAISED PAVEMENT MARKERS TO BE USED IF TEMPORARY MARKINGS ARE TO REMAIN IN PLACE OVER 3 MONTHS
 - *** 4. SPACING OF REFLECTIVE RAISED PAVEMENT MARKERS IS AS FOLLOWS.

TANGENT SECTIONS	URBAN AREA (F+IN)	RURAL AREA (F+IN)
HORIZONTAL CURVES	40'-0"	80'-0"
INTERCHANGE LIMITS	40'-0"	+ 40'-0"

1. NOTE: ON THE MAIN FACILITY, REFLECTIVE RED-CLEAR RAISED PAVEMENT MARKERS ON A 40'-0" SPACING WILL BE REQUIRED IN LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE (IN DIRECTION OF TRAFFIC) OF THE EXIT RAMP. ON THE INTERCHANGE, REFLECTIVE RAISED PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
5. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE REFLECTIVE RAISED PAVEMENT MARKERS AS LISTED IN THE MOOT "APPROVED SOURCES OF MATERIALS."

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**TEMPORARY STRIPING FOR TRAFFIC CONTROL
2-LANE AND 4-LANE
DIVIDED HIGHWAYS**

PLAN NUMBER
CP-13

ISSUE DATE: AUGUST 01, 2017

6363

Overlay approximately 6 miles of SR 149 from Industrial Park Drive to Athens Road, known as State Project No. MP-7149-64(006) / 308317301 in Simpson County.

Line No.	Item Code	Adj Code	Quantity	Units	Description [Fixed Unit Price]
Roadway Items					
0010	304-A008	(GY)	1,060	Cubic Yard	Granular Material, LVM, Class 6, Group D
0020	406-D001		29,802	Square Yard	Fine Milling of Bituminous Pavement, All Depths
0030	407-A001	(A2)	32,655	Gallon	Asphalt for Tack Coat
0032	413-E001		5,375	Linear Feet	Sawing and Sealing Transverse Joints in Asphalt Pavement
0040	423-A001		7	Mile	Rumble Strips, Ground In
0050	618-A001		1	Lump Sum	Maintenance of Traffic
0060	618-B001		1	Square Feet	Additional Construction Signs [\$10.00]
0070	619-A1001		23	Mile	Temporary Traffic Stripe, Continuous White
0080	619-A2001		21	Mile	Temporary Traffic Stripe, Continuous Yellow
0090	619-A4002		3	Mile	Temporary Traffic Stripe, Skip Yellow
0100	619-A5001		20,450	Linear Feet	Temporary Traffic Stripe, Detail
0110	619-A6001		630	Square Feet	Temporary Traffic Stripe, Legend
0120	619-A6002		12,600	Linear Feet	Temporary Traffic Stripe, Legend
0130	620-A001		1	Lump Sum	Mobilization
0140	626-C002		11	Mile	6" Thermoplastic Double Drop Edge Stripe, Continuous White
0150	626-D001		2	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip Yellow
0160	626-E001		11	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow
0170	626-G001		200	Linear Feet	Thermoplastic Detail Stripe, Blue-ADA
0180	626-G002		8,465	Linear Feet	Thermoplastic Detail Stripe, White
0190	626-G003		3,160	Linear Feet	Thermoplastic Detail Stripe, Yellow
0200	626-H001		315	Square Feet	Thermoplastic Double Drop Legend, White
0210	626-H002		6,300	Linear Feet	Thermoplastic Double Drop Legend, White
0220	627-J001		200	Each	Two-Way Clear Reflective High Performance Raised Markers
0230	627-L001		1,797	Each	Two-Way Yellow Reflective High Performance Raised Markers
0240	627-P001		40	Each	Two-Way Blue Reflective High Performance Raised Markers
0250	907-411-A001	(BA1)	10,912	Ton	Ultra Thin Asphalt Pavement
0260	907-619-B001		66	Linear Feet	Temporary Portable Rumble Strips
0270	907-626-H001		3	Each	Thermoplastic Legend, Blue-ADA Handicap Symbol
0280	907-899-A001		1	Lump Sum	Railway-Highway Provisions