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SM No. CSP9507000021

PROPOSAL AND CONTRACT DOCUMENTS

FOR THE CONSTRUCTION OF

17

Mill & Overlay approximately 4 miles of SR 430 from US 82 to the Carroll County Line, known as State Project No. SP-9507-00(002) / 109442301 in Leflore County.

Project Completion: Flexible

(STATE DELEGATED)

NOTICE

**BIDDERS MUST COMPLETE AN ONLINE REQUEST
FOR PERMISSION TO BID THIS PROJECT.**

Electronic addendum updates will be posted on www.gomdot.com

SECTION 900

OF THE CURRENT

2017 STANDARD SPECIFICATIONS

FOR ROAD AND BRIDGE CONSTRUCTION

JACKSON, MISSISSIPPI

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
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(REVISIONS TO THE ABOVE WILL BE INDICATED ON THE SECOND SHEET
OF SECTION 905 AS ADDENDA)

12/28/2023 11:43 AM

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 901 - ADVERTISEMENT

Electronic bids will be received by the Mississippi Transportation Commission at 10:00 o'clock A.M., Wednesday, January 24, 2024, from the Bid Express Service and shortly thereafter publicly read on the Sixth Floor for:

Mill & Overlay approximately 4 miles of SR 430 from US 82 to the Carroll County Line, known as State Project No. SP-9507-00(002) / 109442301 in Leflore County.

The attention of bidders is directed to the predetermined minimum wage rate set by the U. S. Department of Labor under the Fair Labor Standards Act.

The Mississippi Department of Transportation hereby notifies all bidders that it will affirmatively insure that in any contract entered into pursuant to this advertisement, disadvantaged business enterprises will be afforded full opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of race, color, sex, age, disability, religion or national origin in consideration for an award.

The specifications are on file in the offices of the Mississippi Department of Transportation.

Contractors may request permission to bid online at <http://shop.mdot.ms.gov> at no cost. Upon approval, Contractors shall be eligible to submit a bid using Bid Express at <http://bidx.com>. Specimen proposals may be viewed and downloaded online at no cost at <http://mdot.ms.gov> or purchased online at <http://shop.mdot.ms.gov> at a cost of Ten Dollars (\$10.00) per proposal plus a small convenience fee. Cash or checks will not be accepted as payment.

Bid bond, signed or countersigned by a Mississippi Agent or Qualified Nonresident Agent, with Power of Attorney attached, a Cashier's check or Certified Check for five (5%) percent of bid, payable to STATE OF MISSISSIPPI, must accompany each proposal.

The attention of bidders is directed to the provisions of Subsection 102.07 pertaining to irregular proposals and rejection of bids.

BRAD WHITE
EXECUTIVE DIRECTOR

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SUPPLEMENT TO NOTICE TO BIDDERS NO. 1

DATE: 06/08/2021

SUBJECT: Governing Specifications

Change the web address at the end of the first paragraph to the following.

<https://shop.mdot.ms.gov/default.aspx?StoreIndex=1>

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Governing Specifications

The current (2017) Edition of the Standard Specifications for Road and Bridge Construction adopted by the Mississippi Transportation Commission is made a part hereof fully and completely as if it were attached hereto, except where superseded by special provisions, or amended by revisions of the Specifications contained within this proposal. Copies of the specification book may be purchased from the MDOT Construction Division, or online at shopmdot/default.aspx?StoreIndex=1.

A reference in any contract document to controlling requirements in another portion of the contract documents shall be understood to apply equally to any revision or amendment thereof included in the contract.

In the event the plans or proposal contain references to the 2004 Edition of the Standard Specifications for Road and Bridge Construction, it is to be understood that such references shall mean the comparable provisions of the 2017 Edition of the Standard Specifications.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 3

CODE: (SP)

DATE: 01/17/2017

SUBJECT: Final Clean-Up

Immediately prior to final inspection for release of maintenance, the Contractor shall pick up, load, transport and properly dispose of all litter from the entire highway right-of-way that is within the termini of the project.

Litter shall include, but not be limited to, solid wastes such a glass, paper products, tires, wood products, metal, synthetic materials and other miscellaneous debris.

Litter removal is considered incidental to other items of work and will not be measured for separate payment.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 13

CODE: (IS)

DATE: 03/01/2017

SUBJECT: Safety Edge

Bidders are hereby advised that the Shoulder Wedge (Safety Edge) specified in Section 401, Asphalt Pavements, shall only apply to the top two (2) lifts of asphalt. Open Graded Friction Courses (OGFC) are not to be considered a lift as it pertains to safety edge. Attached is a drawing showing the safety edge. Note that the shoulder dimensions in the bottom two drawings will be less than three feet (3').



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 113

CODE: (SP)

DATE: 04/18/2017

SUBJECT: Tack Coat

Bidders are advised that in addition to the products listed on the Department's APL as referenced in Subsection 401.03.1.2 on page 256, the Contractor may use one of the following as a tack coat.

- CSS-1
- CSS-1h
- SS-1
- SS-1h

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 296

CODE: (SP)

DATE: 07/25/2017

SUBJECT: Reduced Speed Limit Signs

Bidders are advised that when the plans or contract documents require the speed limit on a project to be reduced, the Contractor shall begin work within 48 hours of installing the reduced speed limit signs. Should the Contractor not start work or have no plans to start work within 48 hours of installing the signs, the reduced speed limit signs shall be covered and existing speed limit signs uncovered.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 445

CODE: (SP)

DATE: 10/10/2017

SUBJECT: Mississippi Agent or Qualified Nonresident Agent

Bidders are hereby advised of the requirements of Subsections 102.08, 103.05.2, and 107.14.2.1 of the *2017 Standard Specifications for Road and Bridge Construction* as it refers to bonding agents. Proposal guaranties, bonds, and liability insurance policies must be signed by a **Mississippi Agent or Qualified Nonresident Agent.**

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 447

CODE: (SP)

DATE: 10/18/2017

SUBJECT: Traffic on Milled Surface in Rural Areas

Bidders are hereby advised that when the main lanes of a roadway are fine milled, traffic will be allowed to run on a milled surfaces for up to seven (7) calendar days. The Contractor will be assessed a penalty of \$5,000 per calendar day afterwards until the milled surfaces are covered with the next lift of asphalt. It shall be the Contractor's responsibility to ensure that the milling operations do not commence until such time as forecasted weather conditions are suitable enough to allow the placement of the asphalt pavement after the milling operations.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 516

CODE: (IS)

DATE: 11/28/2017

SUBJECT: Errata and Modifications to the 2017 Standard Specifications

<u>Page</u>	<u>Subsection</u>	<u>Change</u>
16	102.06	In the seventh full paragraph, change “Engineer” to “Director.”
33	105.05.1	In the sixth sentence, change “Contract Administration Engineer” to “Contract Administration Director.”
34	105.05.2.1	In subparagraph 2, change “SWPPP, ECP” to “SWPPP and the ECP”
35	105.05.2.2	In subparagraphs 2, add “ and” to the end of the sentence. In subparagraph 3, remove “, and” and add “.”.
90	109.04.2	In the last paragraph of subparagraph (a), place a period “.” at the end of the sentence.
93	109.04.2	In the last paragraph of subparagraph (g), place a period “.” at the end of the sentence. Also, in the first paragraph of subparagraph (h), place a period “.” at the end of the sentence.
97	109.07	Under ADJUSTMENT CODE, subparagraph (A1), change “HMA mixture” to “Asphalt mixtures.”
98	109.11	In the third sentence, change “Engineer” to “Director.”
219	308.04	In the last sentence of the last paragraph, change “Contractor’s decision” to “Engineer’s decision.”
300	405.02.5.9	In the first sentence of the second paragraph, change “Hot Mix Asphalt” to “Asphalt Mixtures.”
502	630.01.1	In the first paragraph, change “ <u>AASHTO</u> ” to “ <u>AASHTO’s LRFD</u> ”.
636	646.05	Change “each” to “per each” for the pay item units of payment.
640	656.02.6.2	In item 7), change “down stream” to “downstream”.
688	630.03.2	Change the subsection number from “630.03.2” to “680.03.2.”

- | | | |
|-----|---------------|---|
| 725 | 702.08.3 | In the second sentence of the first paragraph, change “hot-mix” to “asphalt.” |
| 954 | 804.02.13.1.6 | In the definition for “M” in the % Reduction formulas, change “paragraph 7.3” to “paragraph 5.3.” |

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1225

CODE: (SP)

DATE: 11/13/2018

SUBJECT: Early Notice to Proceed

Bidders are advised that if an early notice to proceed is allowed by the Department and the Contractor experiences problems or delays between the early notice to proceed date and the original notice to proceed date, this shall not be justification for any monetary compensation or an extension of contract time.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1226

CODE: (IS)

DATE: 11/16/2018

SUBJECT: Material Storage Under Bridges

Bidders are advised that Subsection 106.08 of the Standard Specifications allows the Contractor to store materials and equipment on portions of the right-of-way. However, the Contractor will not be allowed to store or stockpile materials under bridges without written permission from the Project Engineer. The Contractor shall submit a detailed request of all proposed materials to be stored under bridges to the Engineer a minimum of 14 calendar days prior to anticipated storage. This detail shall include, but not limited to, bridge location, material type, material quantity, and duration of storage. The Project Engineer and any other needed Division will review this information and determine whether to grant approval. The Contractor shall not store any material under any bridge without written approval from the Project Engineer.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1241

CODE: (IS)

DATE: 11/27/2018

SUBJECT: Fuel and Material Adjustments

Bidder's attention is brought to the last paragraph of Subsection 109.07 of the Standard Specifications which states that no fuel or material adjustment will be made after the completion of contract time. Any fuels consumed or materials incorporated into the work during the monthly estimate period falling wholly after the expiration of contract time will not be subject a fuel or material adjustment.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 1963

CODE: (SP)

DATE: 9/23/2019

SUBJECT: Guardrail Pads

Bidders are hereby advised that prior to construction of the guardrail pads, the Contractor shall coordinate with the guardrail Subcontractor to determine the guardrail pad dimensions necessary to meet MASH compliance.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2206

CODE: (IS)

DATE: 01/14/2020

SUBJECT: MASH Compliant Devices

Bidders are hereby advised that compliance associated with the requirements of meeting either the National Cooperative Highway Research Program (NCHRP) Report 350 or the Manual for Assessing Safety Hardware (MASH) for installations of certain traffic control devices and permanent safety hardware devices (guardrails, guardrail terminals, permanent portable barriers, cast-in-place barriers, all other permanent longitudinal barriers, crash cushions, cable barriers, cable barrier terminals, bridge rails, bridge rail transitions, all other terminals, sign supports, and all other breakaway hardware) as listed throughout the Standard Specifications and/or the Standard Drawings, or both, is now replaced with the requirements of meeting the 2016 version of MASH after December 31, 2019. This change applies to new permanent installations and to full replacements of existing installations.

At the preconstruction conference or prior to starting any work on the project, the Contractor shall submit a letter stating that the traffic control devices and permanent safety hardware devices as outlined within the paragraph above that are to be used on the project are certified to meet MASH 2016.

When a MASH 2016-compliant device does not exist for the new permanent installations and/or full replacement installations of permanent safety hardware devices, as listed above, a MASH 2009-compliant or a NCHRP 350-compliant device may be proposed by the Contractor for the project. A written request for such instances must be submitted by the Contractor either at the preconstruction conference or prior to starting any work on the project. The Contractor shall submit the following items to the Project Engineer: (1) a detailed list of the proposed devices and locations thereof; and (2) certification letters indicating that the proposed devices are compliant with either MASH 2009 or NCHRP 350.

When a MASH 2016-compliant device does not exist for the temporary work zone traffic control devices (Category 1, Category 2, and Category 3 devices), a MASH 2009-compliant or a NCHRP 350-compliant device may be proposed by the Contractor for the project. Temporary work zone traffic control devices (Category 1, Category 2, and Category 3 devices) that are MASH 2009-compliant or NCHRP 350-compliant that have been in use prior to December 31, 2019, and that have a remaining service life may be proposed for use throughout their normal service life on the project by the Contractor. For either of these scenarios for temporary work zone traffic control devices, a written request must be submitted by the Contractor either at the preconstruction conference or prior to starting any work on the project. The Contractor shall submit the following items to the Project Engineer: (1) a detailed list of the proposed devices and locations thereof; and (2) certification letters indicating that the proposed devices are compliant with either MASH 2009 or NCHRP 350.

Work will only be allowed to proceed after the Department has granted written concurrence(s) with the proposed request(s) as listed above.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2273

CODE: (SP)

DATE: 02/12/2020

SUBJECT: Mississippi Special Fuel Tax Law

Bidder's attention is brought to the second paragraph of Subsection 107.02 of the Standard Specifications which states that all Contractors and Subcontractors must comply with all requirements contained in the Mississippi Special Fuel Tax Law, Section 27-55-501, *et seq.* Attached are two Fact Sheets provided by the Mississippi Department of Revenue (MDOR) with additional information.

Gasoline and Dyed Diesel Used for Non-Highway Purposes

Mississippi provides a reduced rate for gasoline and dyed diesel used for non-highway purposes. The reduced rates are 6.44 cents per gallon and 5.75 cents per gallon of gasoline or dyed diesel. These fuels are generally taxed at 18 cents per gallon if for on road use.

Gasoline Used for Non-Highway Purposes

You may be entitled to a refund of 11.56 cents per gallon (making this an equivalent to a tax rate of 6.44 cents per gallon) if you desire to purchase gasoline to be used off road. The gasoline must be used for agricultural, maritime, industrial, manufacturing, domestic or non-highway purposes only.

Examples of non-highway include gasoline used in boats, golf carts, machinery used for manufacturing or farm equipment used exclusively in plowing, planting or harvesting farm products.

Refund Gasoline User

The refund is based on the amount of gallons used. Before a refund is issued, you are required to...

1. Obtain a refund gasoline user's permit and a certificate for refund booklet from the Department of Revenue;
2. Have a storage tank marked "REFUND GASOLINE"; and,
3. Purchase the gasoline from someone who holds a refund gasoline dealer's permit.

No refund will be allowed for gasoline used in motor vehicles owned or operated by a government entity or used in Mississippi government contracts.

Refund Gasoline Dealer

You must obtain a refund gasoline dealer's permit from the Department of Revenue before selling refund gasoline. At no time should the gasoline be delivered to a tank that is not properly marked. The gasoline must be dyed a distinctive mahogany color at the time of delivery.

The Department of Revenue may waive the dye requirement if the dye may cause damage to the equipment. The refund gasoline user is required to obtain the waiver from the Department of Revenue.

Dyed Diesel Used for Non-Highway Purposes

Unlike gasoline, you are not required to apply for a refund if you desire to purchase dyed diesel to be used off road. Mississippi provides a reduced rate of 5.75 cents per gallon on dyed diesel used off road. Diesel used on road is subjected to 18 cents per gallon. Dyed diesel used in motor vehicles owned or operated by a government entity or used in Mississippi government contracts will be subjected to 18 cents per gallon.

Dyed Diesel Used on the Highway

Any person who purchases, receives, acquires or uses dyed diesel for highway use will be liable to pay 18 cents per gallon and subject to a penalty in the amount of \$1000.

Identifying Dyed Diesel

Storage facilities for dyed diesel must be plainly marked "NONHIGHWAY DIESEL FUEL" or "NONHIGHWAY KEROSENE". Retailers are also required to mark all pumps or dispensing equipment.



Special Fuel Used on Government Contracts

State and Local Government Contracts

Special fuel purchased, acquired or used in performing contracts with the State of Mississippi, counties, municipalities or any political subdivision is taxed at a rate of 18 cents per gallon. Special fuel includes but is not limited to the following:

- Dyed diesel fuel;
- Kerosene;
- Undyed diesel fuel; and,
- Fuel oil.

State and local government contracts include construction, reconstruction and maintenance or repairs of projects such as roads, bridges, water systems, sewer systems, buildings, drainage canals and recreational facilities. The Department of Revenue may require contractors to remit the excise tax directly to the state in lieu of paying the tax to a distributor.

Special Fuel Direct Pay Permit

Contractors that remit the excise tax to the state will be issued a Special Fuel Direct Pay Permit. This permit relieves the distributor from collecting the tax and requires the contractor to file a monthly special fuel return. The distributor should include the contractor's permit number on all invoices that are related to tax-free sales.

The contractor is required to furnish a surety or cash bond guaranteeing the payment of the excise tax prior to receiving the Special Fuel Direct Pay Permit. The Department of Revenue may accept a contractors tax bond if the bond covers the excise tax levied on special fuel.

Special Fuel Distributors

If the contractor does not have a Special Fuel Direct Pay Permit, distributors are required to collect the 18 cents excise tax and remit the tax to the Department of Revenue. The additional 12.25 cents levied on special fuel (excluding undyed diesel) should be reported on schedules 5F and 5G of the special fuel return.

Environmental Protection Fee

Special fuel distributors are required to collect the environmental protection fee even if the contractor has a Special Fuel Direct Pay Permit. The fee is levied at 4/10^{ths} of a cent per gallon. The fee is suspended or reinstated when the trust fund has exceeded or fallen below the obligatory balance.

Penalties

Any person who knowingly and willfully purchases untaxed fuel for use in equipment utilized on a road or highway construction site in this state is guilty of a misdemeanor and, upon conviction, shall be fined not less than \$1,000 or more than \$100,000, or imprisoned in the county jail for not more than one year, or both.



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SUPPLEMENT TO NOTICE TO BIDDERS NO. 2654

DATE: **05/02/2020**

The goal is 5 percent for the Disadvantaged Business Enterprise. All Bidders are required to submit Form OCR-481 for all DBEs. Bidders are advised to check the bid tabulation link for this project on the MDOT website at:

https://mdot.ms.gov/portal/current_letting

Bid tabulations are usually posted by 3:00 pm on Letting Day.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2654

CODE: (SP)

DATE: 05/02/2020

SUBJECT: Disadvantaged Business Enterprises In Special Funded Projects

The Department has developed a Disadvantaged Business Enterprise Program that is applicable to this contract and is made a part thereof by reference, except approvals and concurrences by the Federal Highway Administration is not applicable to this contract since it is not financed in whole or in part with Federal Funds.

Copies of the program may be obtained from:

Office of Civil Rights
Mississippi Department of Transportation
P. O. Box 1850
Jackson, Mississippi 39215-1850

POLICY

It is the policy of the Mississippi Department of Transportation to provide a level playing field, to foster equal opportunity in all contracts, to improve the flexibility of the DBE Program, to reduce the burdens on small businesses, and to achieve that amount of participation that would be obtained in a non-discriminatory market place. In doing so, it is the policy of MDOT that there will be no discrimination in the award and performance of these contracts on the basis of race, color, sex, or national origin.

ASSURANCES THAT CONTRACTORS MUST TAKE:

MDOT will require that each contract which MDOT signs with a subrecipient or a Contractor, and each subcontract the Prime Contractor signs with a Subcontractor, includes the following assurances:

“The Contractor, subrecipient or Subcontractor shall not discriminate on the basis of race, color, sex or national origin in the performance of this contract. Failure by the Contractor to carry out these requirements is a material breach of this contract, which may result in the termination of this contract or such other remedy as MDOT deems appropriate.”

DEFINITIONS

For purposes of this provision the following definitions will apply:

"Disadvantaged Business" means a small business concern: (a) which is at least 51 percent owned by one or more socially and economically disadvantaged individual(s) or in the case of any publicly owned business, at least 51 percent of the stock of which is owned by one or more socially

and economically disadvantaged individual(s); and (b) whose management and daily business operations are controlled by one or more of the socially and economically disadvantaged individual(s) who own it. It is important to note that the business owners themselves must control the operations of the business. Absentee ownership or title ownership by an individual who does not take an active role in controlling the business is not consistent with eligibility as a DBE under 49 CFR Part 26.71.

CONTRACTOR'S OBLIGATION

The Contractor and all Subcontractors shall take all necessary and reasonable steps to ensure that DBE firms compete for and participate in the performance of a portion of the work in this contract and shall not discriminate on the basis of race, color, sex or national origin. Failure on the part of the Contractor to carry out the DBE requirements of this contract constitutes a breach of contract and after proper notification the Department may terminate the contract or take other appropriate action as determined by the Department.

When a contract has a zero percent (0%) DBE goal, the Contractor still has the responsibility to take all necessary and reasonable steps to ensure that DBE firms can compete for and participate in the performance of the work in the contract. In this case, all work performed by a certified DBE firm is considered to be a “race neutral” measure and the Department will receive DBE credit towards the overall State goals when the DBE firm is paid for their work. If the Prime Contractor is a certified DBE firm, the Department can receive DBE credit only for the work performed by the Prime Contractor’s work force or any work subcontracted to another DBE firm. Work performance by a non-DBE Subcontractor is not eligible for DBE credit.

CONTRACT GOAL

The goal for participation by DBEs is established for this contract in the attached Supplement. The Contractor shall exercise all necessary and reasonable steps to ensure that participation is equal to or exceeds the contract goal.

If the percentage of the contract that is proposed for DBEs is 1% or greater, shall be so stated on the last bid sheet of the proposal.

All Bidders shall submit to the Office of Civil Rights Form OCR-481, signed by the Prime Contractor and the DBE Subcontractors, no later than the 3rd business day after opening of the bids.

Form OCR-481 is available on the MDOT website at www.mdot.ms.gov under the Civil Rights tab, or by calling 601-359-7466.

The OCR-481 Form must contain the following information:

The name and address of each certified DBE Contractor/Supplier;

The Reference Number, percent of work to be completed by the DBE subcontractor and the dollar amount of each item. If a portion of an item is subcontracted, a breakdown of that item

including quantities and unit price must be attached, detailing what part of the item the DBE firm is to perform and who will perform the remainder of the item.

If the DBE Commitment shown on the last bid sheet of the proposal, does not equal or exceed the contract goal, the bidder must submit, to MDOT Contract Administration Division prior to bid opening, information to satisfy the Department that adequate good faith efforts have been made to meet the contract goal.

Failure of the lowest bidder to furnish acceptable proof of good faith efforts, submitted to MDOT Contract Administration Division prior to bid opening, shall be just cause for rejection of the proposal. Award may then be made to the next lowest responsive bidder or the **project** may be re-advertised.

GOOD FAITH EFFORTS

The following factors are illustrative of matters the Department will consider in judging whether or not the bidder has made adequate good faith effort to satisfy the contract goal.

- (1) Whether the bidder attended the pre-bid meeting that was scheduled by the Department to inform DBEs of subcontracting opportunities;
- (2) whether the bidder advertised in general circulation, trade association, and minority-focus media concerning the subcontracting opportunities;
- (3) whether the bidder provided written notice to a reasonable number of specific DBEs that their interest in the contract is being solicited;
- (4) whether the bidder followed up initial solicitations of interest by contacting DBEs to determine with certainty whether they were interested;
- (5) whether the bidder selected portions of the work to be performed by DBEs in order to increase the likelihood of meeting the contract goal;
- (6) whether the bidder provided interested DBEs with adequate information about the plans, specifications and requirements of the contract;
- (7) whether the bidder negotiated in good faith with interested DBEs and did not reject them as unqualified without sound reasons based on a thorough investigation of their capabilities; and
- (8) whether the bidder made efforts to assist interested DBEs in obtaining any required bonding or insurance.
- (9) whether the bidder has written notification to certified DBE Contractors soliciting subcontracting for items of work in the contract.
- (10) whether the bidder has a statement of why an agreement was not reached.

- (11) Proof of written notification to certified DBE Contractors by certified mail that their interest is solicited in subcontracting the work defaulted by the previous DBE or in subcontracting other items of work in the contract.

The bidder's execution of the signature portion of the proposal shall constitute execution of the following assurance:

The bidder hereby gives assurance that a good faith effort has been made to meet the contract goal for DBE participation for which this proposal is submitted.

DIRECTORY

A list of “Certified DBE Contractors” which have been certified as such by the Mississippi Department of Transportation and other Unified Certification Partners (UPC) can be found on the Mississippi Department of Transportation website at www.mdot.ms.gov. The DBE firm must be certified at the time the project is let and approved by MDOT to count towards meeting the DBE goal.

REPLACEMENT

If a DBE Subcontractor cannot perform satisfactorily, and this causes the OCR-481 commitment to fall below the contract goal, the Contractor shall take all necessary reasonable steps to replace the DBE with another certified DBE Subcontractor or submit information to satisfy the Mississippi Department of Transportation that adequate good faith efforts have been made to replace the DBE. The good faith efforts outlined previously in this document still apply. The replacement DBE must be a DBE who was on the Department's list of "Certified DBE Contractors" when the job was let, and who is still active All DBE replacements must be approved by the Department.

Under no circumstances shall the Prime or any Subcontractor perform the DBE's work (as shown on the OCR-481) without prior written approval from the Department. See "Sanctions" at the end of this document for penalties for performing DBE's work.

When a Contractor proposes to substitute/replace/terminate a DBE that was originally named on the OCR-481, the Contractor must obtain a release, in writing, from the named DBE explaining why the DBE Subcontractor cannot perform the work. A copy of the original DBE's release must be attached to the Contractor's written request to substitute/replace/terminate along with appropriate subcontract forms for the substitute/replacement/terminated Subcontractor, all of which must be submitted to the DBE Coordinator and approved, in advance, by MDOT.

PREBID MEETING

A pre-bid meeting will be held in the Commission Room on the 1st floor of the MDOT Administration Building in Jackson at 2:00 P.M. on the day preceding the date of the bid opening.

This meeting is to inform DBE firms of subcontracting and material supply opportunities. Attendance at this meeting is considered of prime importance in demonstrating good faith effort to meet the contract goal.

PARTICIPATION / DBE CREDIT

Participation shall be counted toward meeting the goal in this contract as follows:

- (1) If the Prime Contractor is a certified DBE firm, only the value of the work actually performed by the DBE Prime can be counted towards the project goal, along with any work subcontracted to a certified DBE firm.
- (2) If the Prime Contractor is not a DBE, the work subcontracted to a certified DBE Contractor will be counted toward the goal.
- (3) The Contractor may count toward the goal a portion of the total dollar value of a contract with a joint venture eligible under the standards of this provision equal to the percentage of the DBE partner in the joint venture. The joint venture must submit a Joint Venture Eligibility Form provided by the Mississippi Department of Transportation.
- (4) Expenditures to DBEs that perform a commercially useful function may be counted toward the goal. A business is considered to perform a commercially useful function when it is responsible for the execution of a distinct element of the work and carries out its responsibilities by actually performing, managing, and supervising the work involved.
- (5) The Contractor may count 100% of the expenditures for materials and supplies obtained from certified DBE suppliers and manufacturers that produce goods from raw materials or substantially alters them for resale provided the suppliers and manufacturers assume the actual and contractual responsibility for the provision of the materials and supplies. The Contractor may count 60 percent of the expenditures to suppliers that are not manufacturers, provided the supplier performs a commercially useful function in the supply process. Within 30 days after receipt of the materials, the Prime Contractor shall furnish to the DBE Coordinator invoices from the certified supplier to verify the DBE goal.
- (6) Any work that a certified DBE firm subcontracts or sub-subcontracts to a non-DBE firm will not count towards the DBE goal
- (7) Only the dollars actually paid to the DBE firm may be counted towards the DBE goal.

AWARD

Award of this contract to the low bidder will be contingent upon the following condition:

- (1) **All Bidders** must submit to the Office of Civil Rights for approval, Form OCR-481 (DBE Commitment) no later than the 3rd business day after opening of the bids, or submit information with the bid proposal to satisfy the Department and that adequate good faith

efforts have been made to meet the contract goal. For answers to questions regarding Form OCR-481, contact the MDOT Office of Civil Rights at (601) 359-7466.

- (2) Bidder must include OCR-485 information with their bid proposal listing all firms that submitted quotes for material supplies or items to be subcontracted. OCR-485 information must be signed and included with the bid proposal. If the OCR-485 information is not included as part of bid proposal, your bid will be deemed irregular.

Prior to the start of any work, the bidder must notify the Project Engineer, in writing, of the name of the designated "DBE Liaison Officer" for this project. This notification must be posted on the bulletin board at the project site.

DEFAULT

If the contract goal established by MDOT in this proposal is 1% or greater, it must be met to fulfill the terms of the contract. The Contractor may list DBE Subcontractors and items that exceed MDOT's contract goal, but should unforeseen problems arise that would prevent a DBE from completing its total commitment percentage, the Contractor will meet the terms of the contract as long as it meets or exceeds MDOT's Contract Goal. For additional information, refer to "Replacement" section of this Notice.

DBE REPORTS

- (1) OCR-481: Refer to "CONTRACT GOAL" section of this Notice to Bidders for information regarding this form.
- (2) OCR-482: At the conclusion of the project the Contractor will submit to the Project Engineer for verification of quantities and further handling Form OCR-482 whereby the Contractor certifies to the amounts of payments made to each Contractor/Supplier. The Project Engineer shall submit the completed Form OCR-482 to the DBE Coordinator (Office of Civil Rights). Final acceptance of the project is dependent upon Contract Administration Division's receipt of completed Form OCR-482 which they will receive from the Office of Civil Rights.
- (3) OCR-483: The Project Engineer/Inspector will complete Form OCR-483, the Commercially Useful Function (CUF) Performance Report, in accordance with MDOT S.O.P. No. OCR-03-05-02-483. Evaluations reported on this form are used to determine whether or not the DBE firm is performing a CUF. The Prime Contractor should take corrective action when the report contains any negative evaluations. DBE credit may be disallowed and/or other sanctions imposed if it is determined the DBE firm is not performing a CUF. This form should also be completed and returned to the DBE Coordinator (Office of Civil Rights).
- (4) OCR-484: Each month, the Prime Contractor will submit to the Project Engineer OCR-484 that certifies payments to all Subcontractors and shows all firms even if

the Prime Contractor has paid no monies to the firm during that estimate period (negative report). The Project Engineer will attach the form to the monthly estimate before forwarding to the Contract Administration Division for further processing. Failure of the Contractor to submit the OCR-484 will result in the estimate not being processed and paid.

- (5) OCR-485: Bidder must submit **signed form with bid proposal** of all firms that submitted quotes for material supplies or items to be subcontracted. If the OCR-485 information is not included as part of bid proposal, your bid will be deemed irregular.
- (6) OCR-487: Only used by Prime Contractors that are certified DBE firms. This form is used in determining the exact percentage of DBE credit for the specified project. **The low Bidder** should **return this form** to MDOT with the OCR-481 form, or can also be returned with the Permission to Subcontract Forms (CAD-720, CAD-725 and CAD-521).

DBE Forms, can be obtained from the Office of Civil Rights Division, MDOT Administration Building, 401 North West Street, Jackson, MS, or at www.mdot.ms.gov under the Civil Rights tab.

SANCTIONS

The Department has the option to enforce any of the following penalties for failure of the Prime Contractor to fulfill the DBE goal as stated on the OCR-481 form or any violations of the DBE program guidelines:

- (1) Disallow credit towards the DBE goal
- (2) Withhold progress estimate payments
- (3) Deduct from the final estimate or recover an amount equal to the unmet portion of the DBE goal which may include additional monetary penalties as outlined below based on the number of offenses and the severity of the violation as determined by MDOT.

Offense #1	10% of unmet portion of goal	or	\$5,000 lump sum payment	or	Both
Offense #2	20% of unmet portion of goal	or	\$10,000 lump sum payment	or	Both
Offense #3	40% of unmet portion of goal	or	\$20,000 lump sum payment	or	\$20,000 lump sum payment and debarment

- (4) Debar the Contractor involved from bidding on Mississippi Department of Transportation projects **for a period of up to 12 months after notification by certified email.**

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 2954

CODE: (IS)

DATE: 12/01/2020

SUBJECT: Reflective Sheeting for Signs

Bidders are hereby advised that the retroreflective sign sheeting used for signs on this project shall be as listed below and shall meet the requirements of Subsection 721.06.

Temporary Construction Signs

Temporary traffic control (orange) sign sheeting shall be a minimum Type IX Fluorescent Orange sheeting as shown in Special Provision 907-721.

Permanent Signs

Permanent signs, except signs on traffic signal poles/mast arms, shall be as follows:

- Brown background sheeting on guide signs shall be a minimum Type VIII sheeting,
- Green and blue background sheeting on guide signs shall be a minimum Type IX sheeting, and
- All white, yellow, red, fluorescent yellow, and fluorescent yellow/green sheeting shall be Type XI sheeting.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

| **SUPPLEMENT TO NOTICE TO BIDDERS NO. 3599**

| **DATE: 08/11/2021**

| After the last drawing on page 33, add the following.

STATE PROJECT NO.
MISS. _____

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

GUARDRAIL:
"W" BEAM
(WOOD POSTS)

WORKING NUMBER: CR-1
SHEET NUMBER: 6201
ISSUE DATE: MAY 21, 2017

SECTION A-A

* NOTE: UNLESS SPECIFIED OTHERWISE ON THE PLANS.

GENERAL NOTES:

- GUARDRAIL SHALL MEET THE REQUIREMENTS OF AASHTO M 188, CLASS A, TYPE 1 UNLESS OTHERWISE DESIGNATED.
- GUARDRAIL SHALL BE SINGLE Faced UNLESS OTHERWISE DESIGNATED.
- GUARDRAIL SECTIONS SHALL BE LAPPED IN THE DIRECTION OF TRAVEL AND SHALL BE AS NEAR AS POSSIBLE TO THE CENTERLINE OF THE ROADWAY. THE ONLY EXCEPTION NOTED IS THAT GUARDRAIL IS TO BE LAPPED OR APPROACHING 184°-11° ON A BRIDGE WITH 2-WAY 184°-11°.
- ALL WOOD POSTS AND BLOCKOUTS SHALL BE TREATED TIMBER IN ACCORDANCE WITH MISSISSIPPI DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS.
- FOR OTHER DETAILS OF POSTS, POST ACCESSORIES, FASTENERS & RAIL ELEMENTS, SEE AASHTO-ACC-ARTBA JOINT TASK FORCE NO. 15, TITLED "AASHTO STANDARDIZED TOWNSHIP BARRIER (UNDERWAY LATEST EDITION).

FASTENER DETAILS

GUARDRAIL BOLTS

BOLT	L (MIN)	T
"A"	1 1/2'	1"
"B"	22"	4"
"C"	33"	4"

NOTES:

- ALL GUARDRAIL BOLTS ARE 3/8" - 11 UNC THREAD PITCH.
- IF ANY BOLT EXTENDS MORE THAN 1/2" FROM THE NUT, THE BOLT SHOULD BE TRIMMED BACK.
- IF BARRIER WITH 18° X 18" WOOD POST AND 6" X 12" WOOD BLOCKOUT.

TYPICAL GUARDRAIL SECTION

ELEVATION FROM C. ROADWAY

** NOTE: OPTIONAL BEAM LENGTHS.

GUARDRAIL SPLICE DETAIL

ELEVATION: Shows 12' post spacing and 2' x 1/4" slot for guardrail bolt (TYP).

SECTION: Shows 3/21 slot and 1/4" dimensions for bolt placement.

DETAILS OF ADJUSTABLE HEIGHT BLOCKOUT ASSEMBLY

ELEVATION: Shows two extra post holes, wood blockout, and 6' x 8' wood post. Dimensions include 12" blockout height and 3/8" hole diameter.

SECTION: Shows 6' x 8' wood post and blockout assembly.

NOTES:

- ON INITIAL INSTALLATION, THE BLOCKOUT SHALL BE ATTACHED TO THE BOTTOM HOLE IN THE POST. OTHER HOLES IN THE POST AND BLOCKOUT ARE FOR FUTURE 2" HEIGHT ADJUSTMENT WHEN THE ROADWAY IS RESURFACED. HEIGHT ADJUSTMENT ONE (1) HEX NUT AND BOLT 10-22' LONG FOR SINGLE-FACED BARRIER OR BOLT 11-22' LONG FOR DOUBLE-FACED BARRIER WITH TWO (2) PLAIN ROUND WASHERS "F", ONE (1) UNDER HEAD AND ONE (1) UNDER NUT. ARE REQUIRED PER POST IN ADDITION TO THE STANDARD GUARDRAIL BOLT.
- USE PICES ARE REQUIRED ON ALL WOOD POSTS AND BLOCKOUTS.
- WOOD POSTS ARE FABRICATED FROM 6" X 8" TREATED TIMBER AND BLOCKOUTS ARE FABRICATED FROM 6" X 12" TREATED TIMBER UNLESS SPECIFIED OTHERWISE ON THE PLANS.
- ALL HOLES IN BOTH POSTS AND BLOCKOUTS ARE 3/8" IN DIAMETER.

WOOD BLOCKOUT

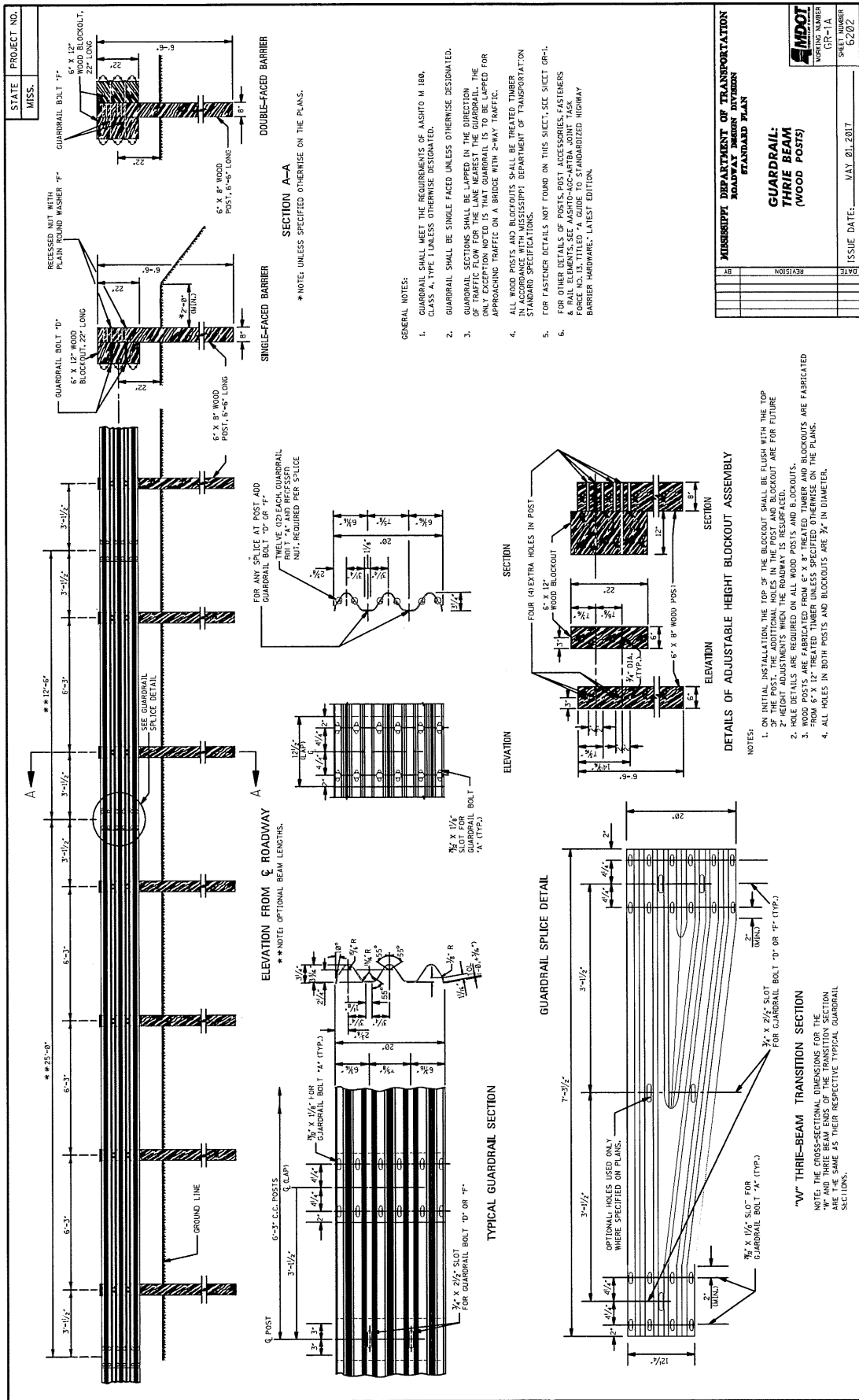
WOOD POST

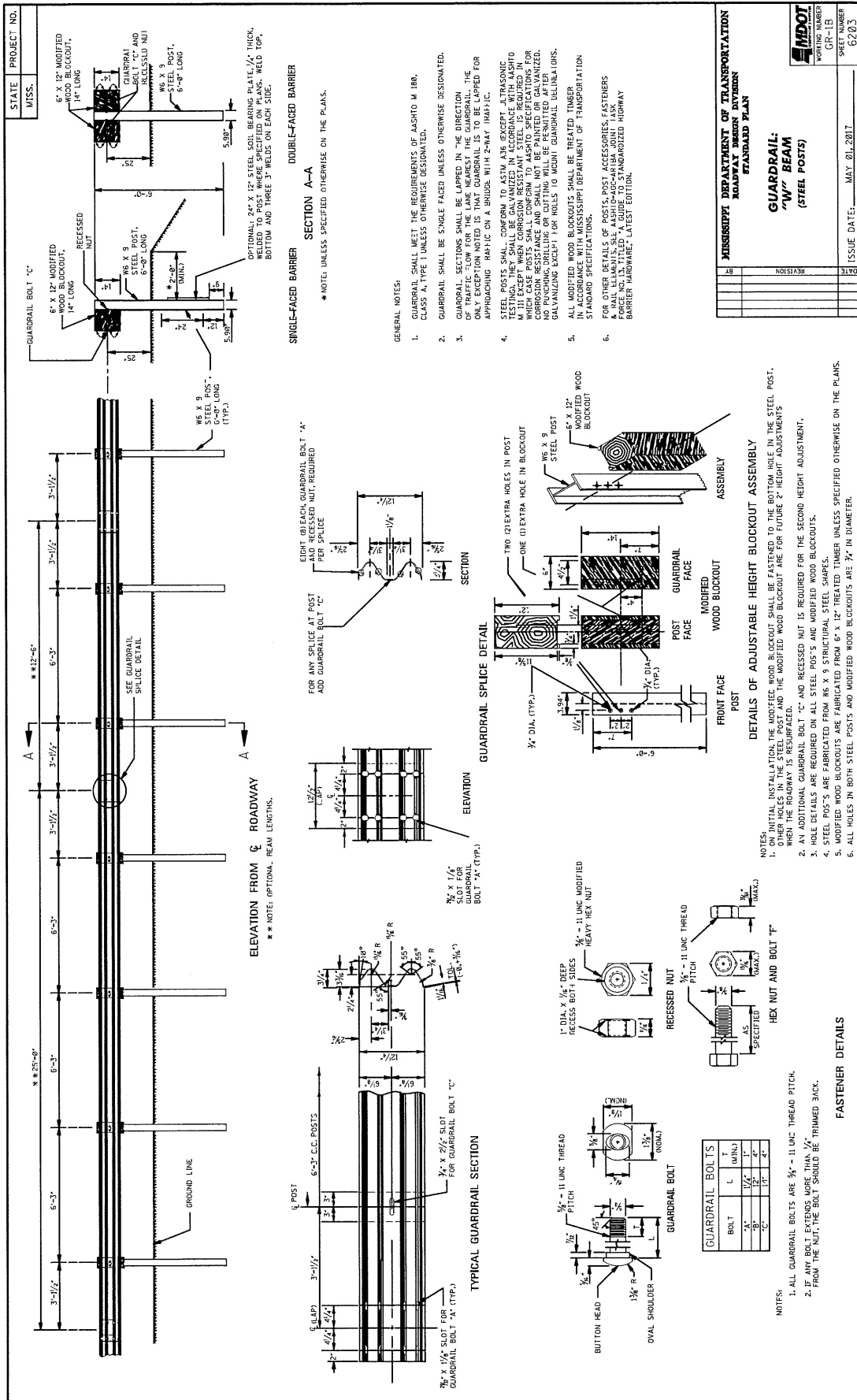
WOOD BLOCKOUT

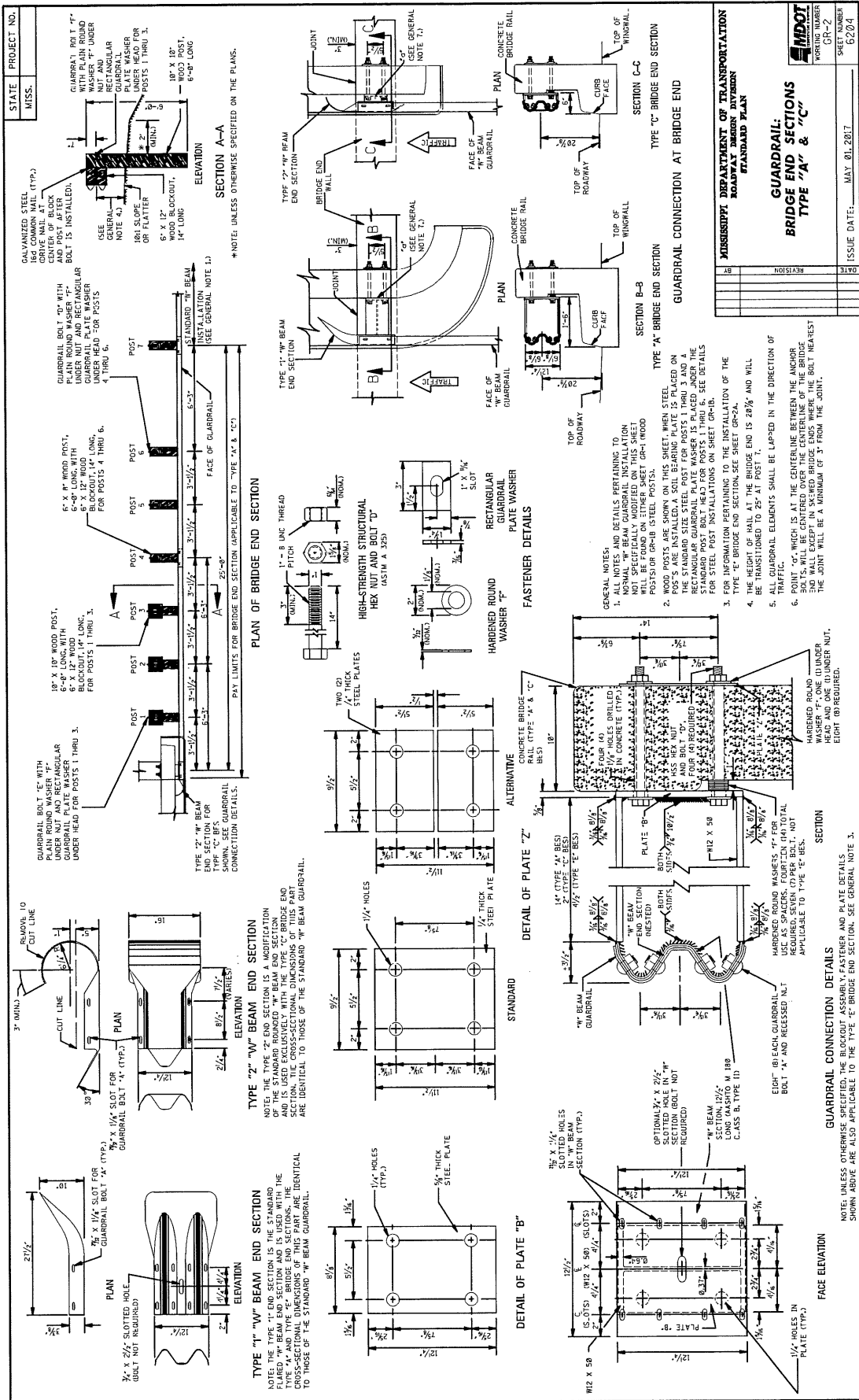
WOOD POST

WOOD BLOCKOUT

WOOD POST







STATE	MISS.
PROJECT NO.	

WORKING NUMBER	GR-2A
SHEET NUMBER	6205

ISSUE DATE:	MAY 21, 2017
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MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAYS DIVISION
 BRIDGE END SECTIONS
 TYPE "E", "F", "G"

DESIGNER	KOZLASKI
CHECKER	

TYPE "G" BRIDGE END SECTION

NOTES:
 1. BRIDGE RAILING MAY BE EITHER CONCRETE OR PIPE RAILING IN PLACE. CONNECTOR DETAILS ARE SHOWN BELOW.
 2. HARDENED ROUND WASHER "E", ONE UNDER HEAD AND ONE UNDER NUT, EIGHT (8) REQUIRED.

TYPE "F" BRIDGE END SECTION

NOTES:
 1. BRIDGE RAILING MAY BE EITHER CONCRETE OR PIPE RAILING IN PLACE. CONNECTOR DETAILS ARE SHOWN BELOW.
 2. HARDENED ROUND WASHER "E", ONE UNDER HEAD AND ONE UNDER NUT, EIGHT (8) REQUIRED.

TYPE "E" BRIDGE END SECTION

NOTES:
 1. BRIDGE RAILING MAY BE EITHER CONCRETE OR PIPE RAILING IN PLACE. CONNECTOR DETAILS ARE SHOWN BELOW.
 2. HARDENED ROUND WASHER "E", ONE UNDER HEAD AND ONE UNDER NUT, EIGHT (8) REQUIRED.

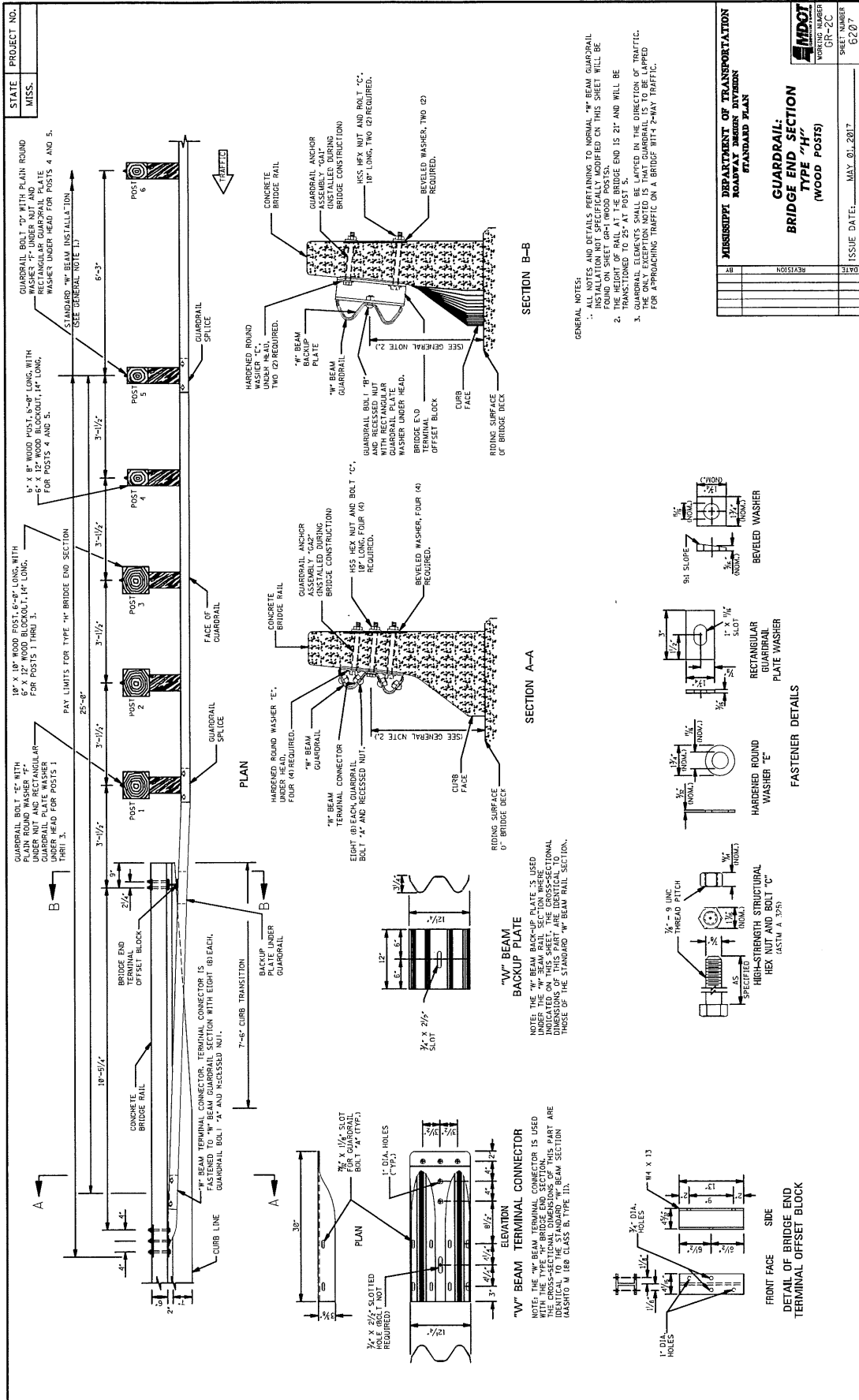
TYPE "E" BRIDGE END SECTION

NOTES:
 1. TYPE "E" BESS POSTS 1 THRU 30 TYPE "F" & "G" BESS POSTS 1 THRU 40 1 1/2" WOOD POST, 6'-8" LONG WITH 6" X 12" WOOD BLOCKOUT, 14" LONG, ATTACHED USING GUARDRAIL BOLT "E", WITH PLAIN ROUND WASHER "F" UNDER NUT AND RECTANGULAR GUARDRAIL PLATE WASHER UNDER HEAD.
 2. TYPE "E" BESS POSTS 4 THRU 81 TYPES "F" & "G" BESS POSTS 5 THRU 17: 8" X 8" WOOD POST, 6'-8" LONG WITH 6" X 12" WOOD BLOCKOUT, 14" LONG, ATTACHED USING GUARDRAIL BOLT "E", WITH PLAIN ROUND WASHER "F" UNDER NUT AND RECTANGULAR GUARDRAIL PLATE WASHER UNDER HEAD.

TYPE "E" BRIDGE END SECTION

NOTES:
 1. ALL NOTES AND DETAILS PERTAINING TO NORMAL "W" BEAM GUARDRAIL INSTALLATION NOT SPECIFICALLY MODIFIED FOR "E" BESS POSTS.
 2. WOOD POSTS ARE SHOWN ON THIS SHEET. WHEN STEEL POSTS ARE USED, A SOIL BEARING PLATE IS PLACED ON THE STANDARD SIZE STEEL POST FOR POSTS 1 THRU 3 IN THE TYPE "E" BESS AND FOR GUARDRAIL PLATE WASHER IS PLACED UNDER THE STANDARD POST BOLT HEAD FOR POSTS 4 THRU 6 IN THE TYPE "E" BESS AND FOR POSTS 7 THROUGH 17 IN THE TYPE "E" BESS. SEE DETAILS FOR STEEL POST INSTALLATION ON SHEET 6204B.
 3. GUARDRAIL ELEMENTS SHALL BE LAPPED IN THE DIRECTION OF TRAFFIC. THE ONLY EXCEPTION NOTED IS THAT GUARDRAIL IS TO BE LAPPED FOR APPROACHING TRAFFIC ON A BRIDGE WITH 2-WAY TRAFFIC.

FASTENER DETAILS



STATE	PROJECT NO.
MISS.	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION	
GUARDRAIL: BRIDGE END SECTION TYPE "H" (WOOD POSTS)	
DATE	ISSUE DATE: MAY 01, 2017
REVISION	
NO.	
BY	

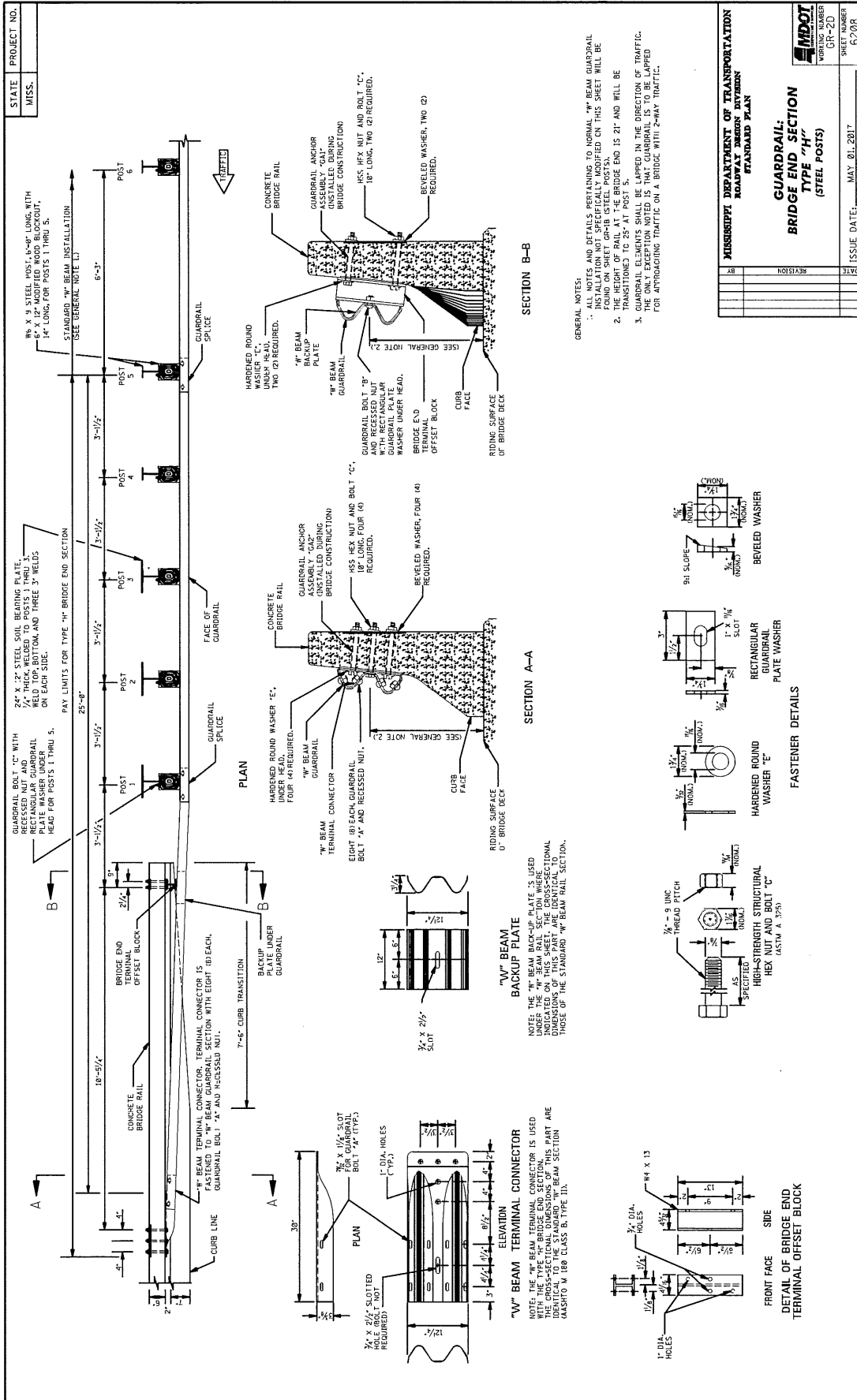
MDOT
WORKING NUMBER
GR-2C
SHEET NUMBER
6207

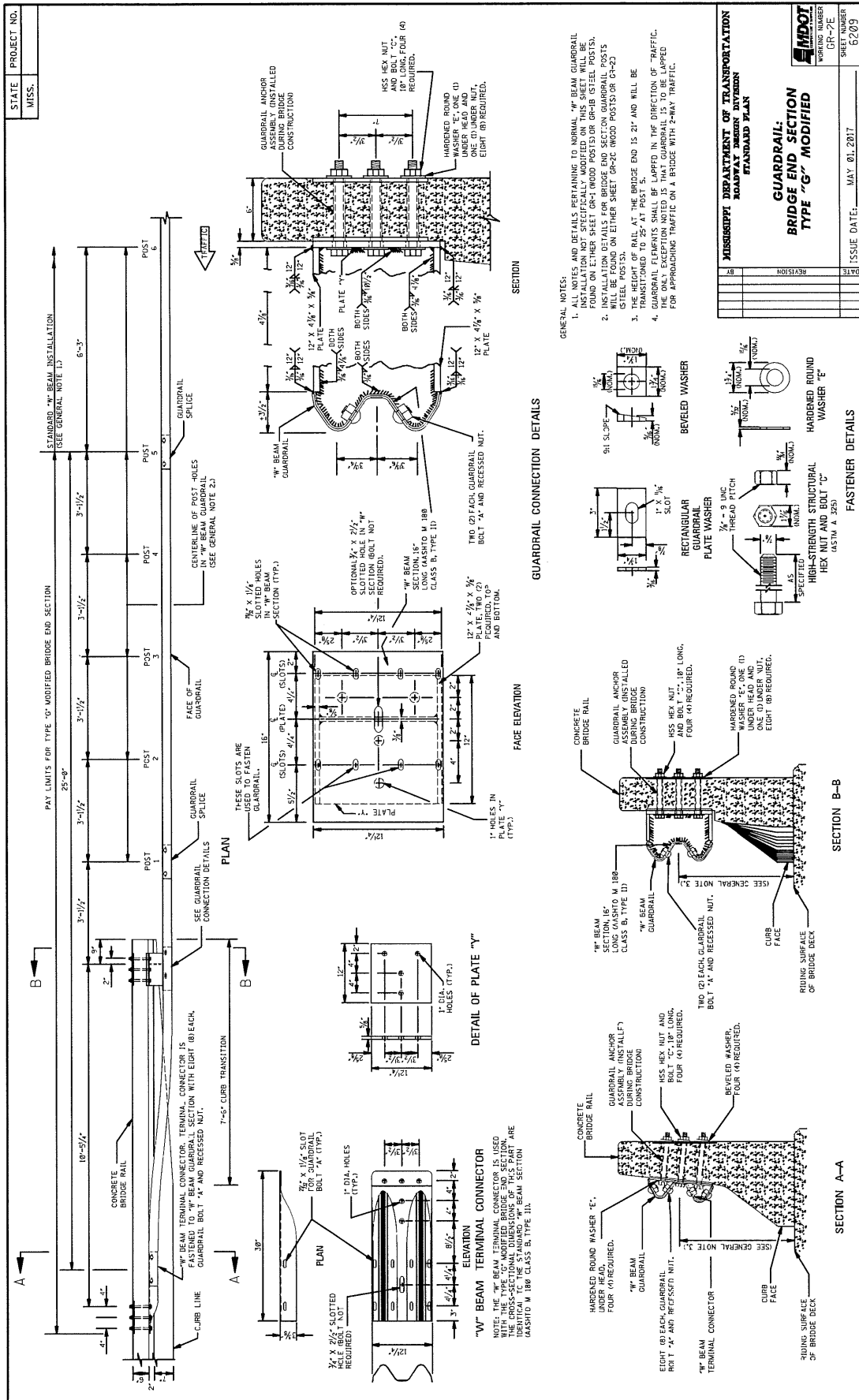
- GENERAL NOTES:
1. ALL NOTES AND DETAILS PERTAINING TO NORMAL "W" BEAM GUARDRAIL INSTALLATION NOT SPECIFICALLY MODIFIED ON THIS SHEET WILL BE APPLIED TO THIS SECTION.
 2. THE HEIGHT OF RAIL AT THE BRIDGE END IS 21" AND WILL BE TRANSTIONED TO 25" AT POST 5.
 3. GUARDRAIL ELEMENTS SHALL BE LAPPED IN THE DIRECTION OF TRAFFIC. LAPPING SHALL BE 10' MINIMUM. LAPPING SHALL BE STAPLED FOR APPROACHING TRAFFIC ON A BRIDGE WITH 2-WAY TRAFFIC.

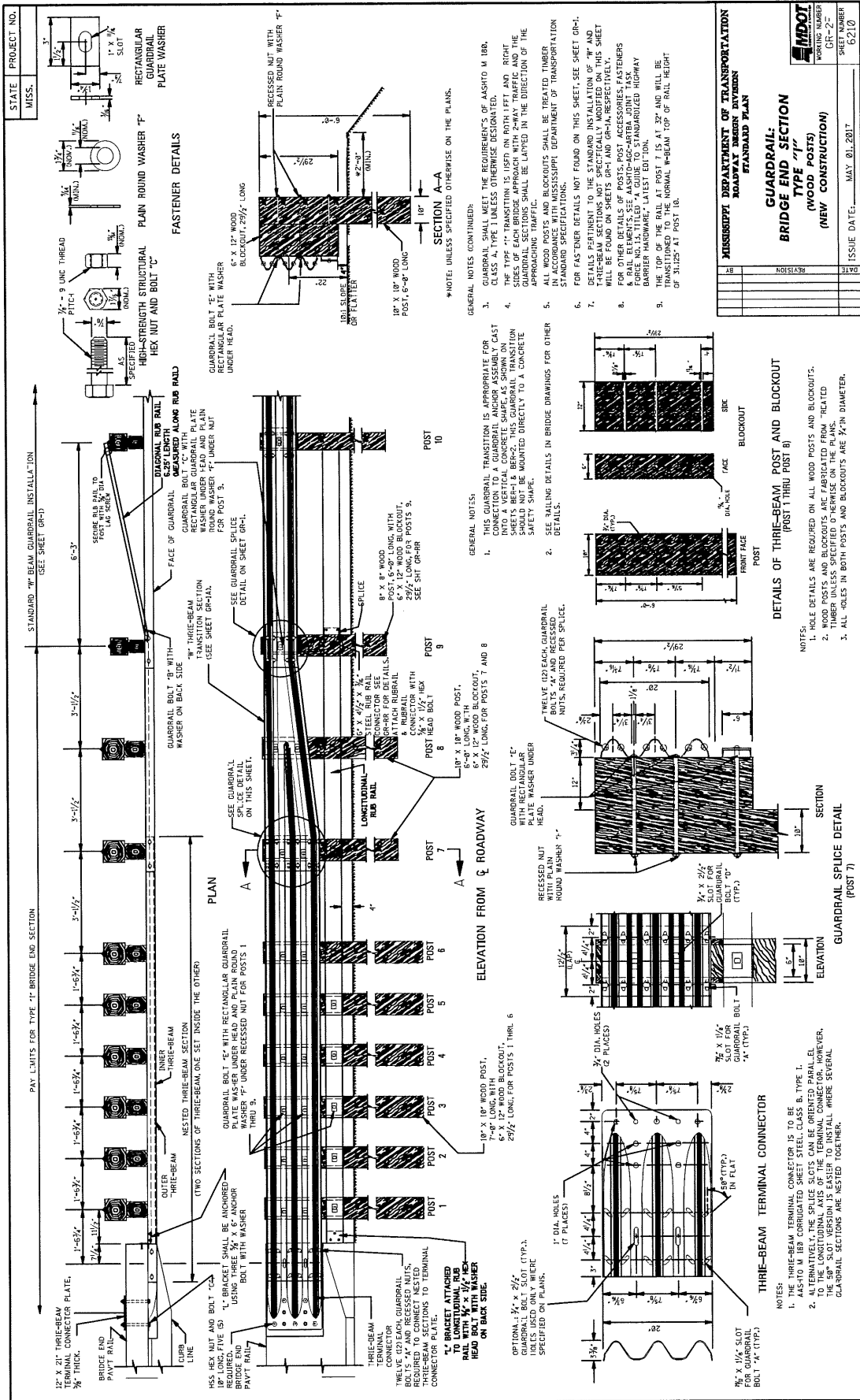
"W" BEAM BACKUP PLATE
NOTE: THE "W" BEAM BACKUP PLATE IS USED UNDER THE "W" BEAM RAIL SECTION WHERE INDICATED ON THIS SHEET. THE CROSS-SECTIONAL DIMENSIONS OF THIS PART ARE THOSE OF THE STANDARD "W" BEAM RAIL SECTION.

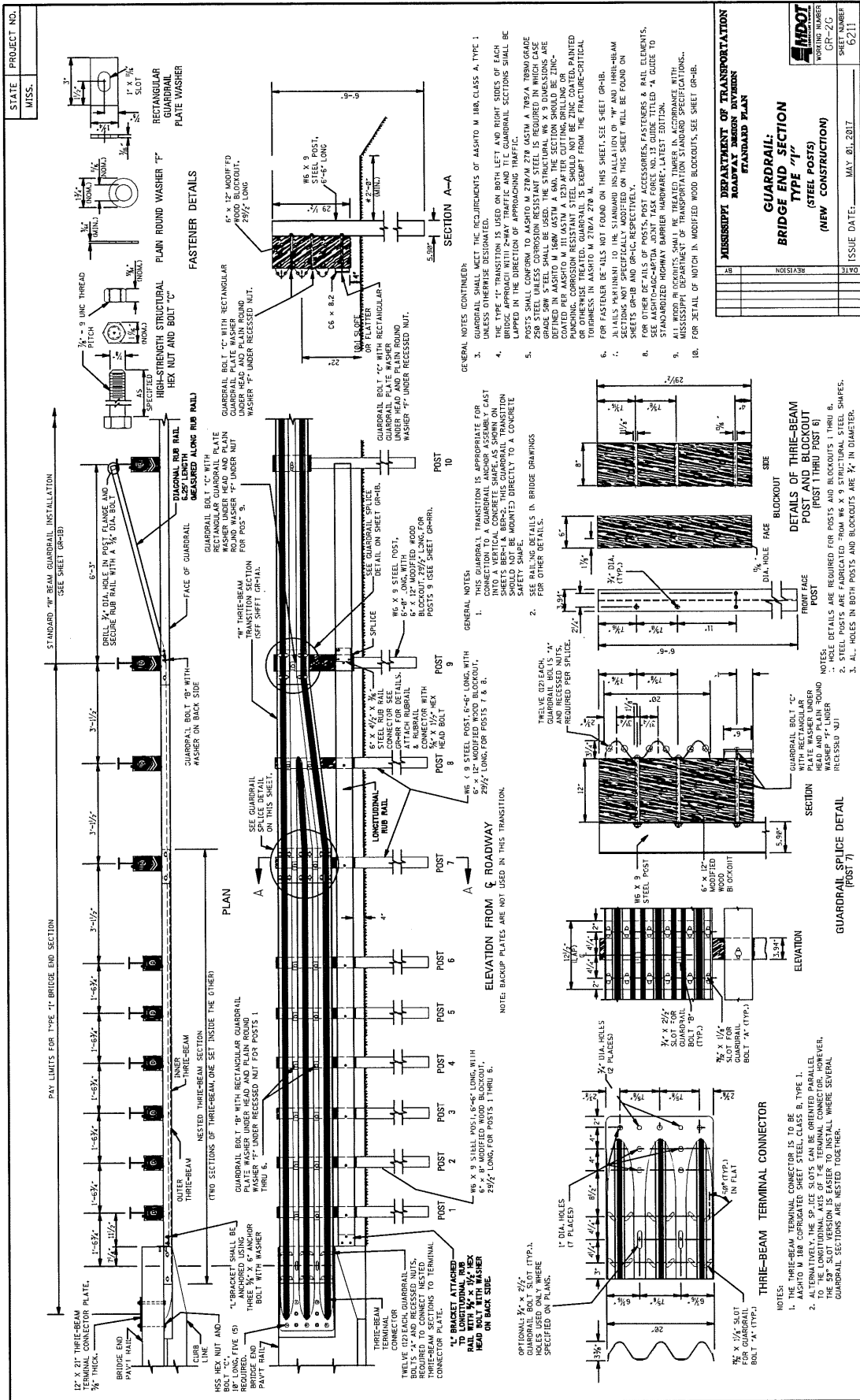
"W" BEAM TERMINAL CONNECTOR
NOTE: THE "W" BEAM TERMINAL CONNECTOR IS USED UNDER THE "W" BEAM RAIL SECTION WHERE INDICATED ON THIS SHEET. THE CROSS-SECTIONAL DIMENSIONS OF THIS PART ARE THOSE OF THE STANDARD "W" BEAM RAIL SECTION.

FASTENER DETAILS





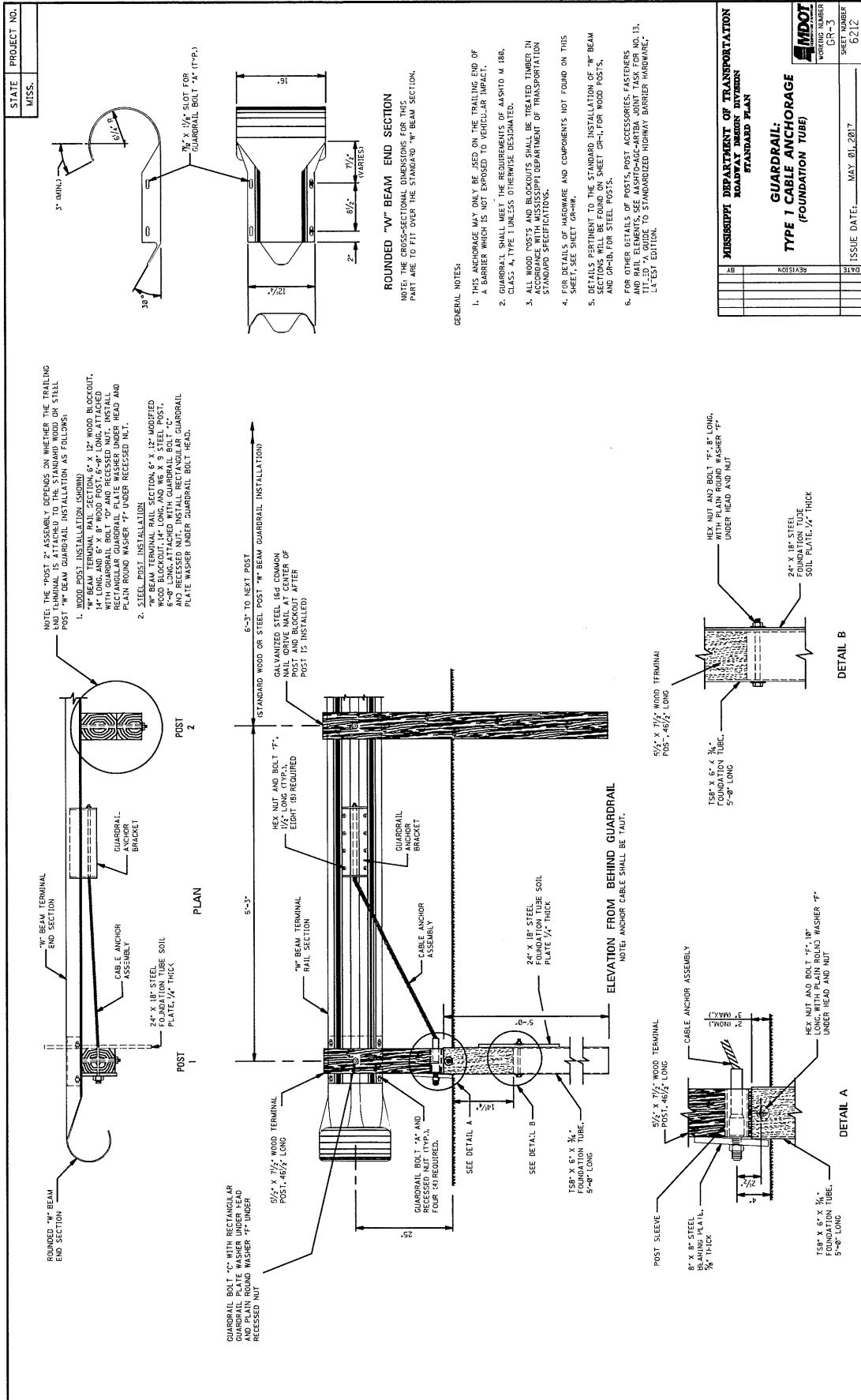




STATE	MISS.
PROJECT NO.	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
WORKING NUMBER	CR-2C
SHEET NUMBER	6211
ISSUE DATE:	MAY 01, 2017

GUARDRAIL BRIDGE END SECTION TYPE 1 (STEEL CONSTRUCTION) (NEW CONSTRUCTION)

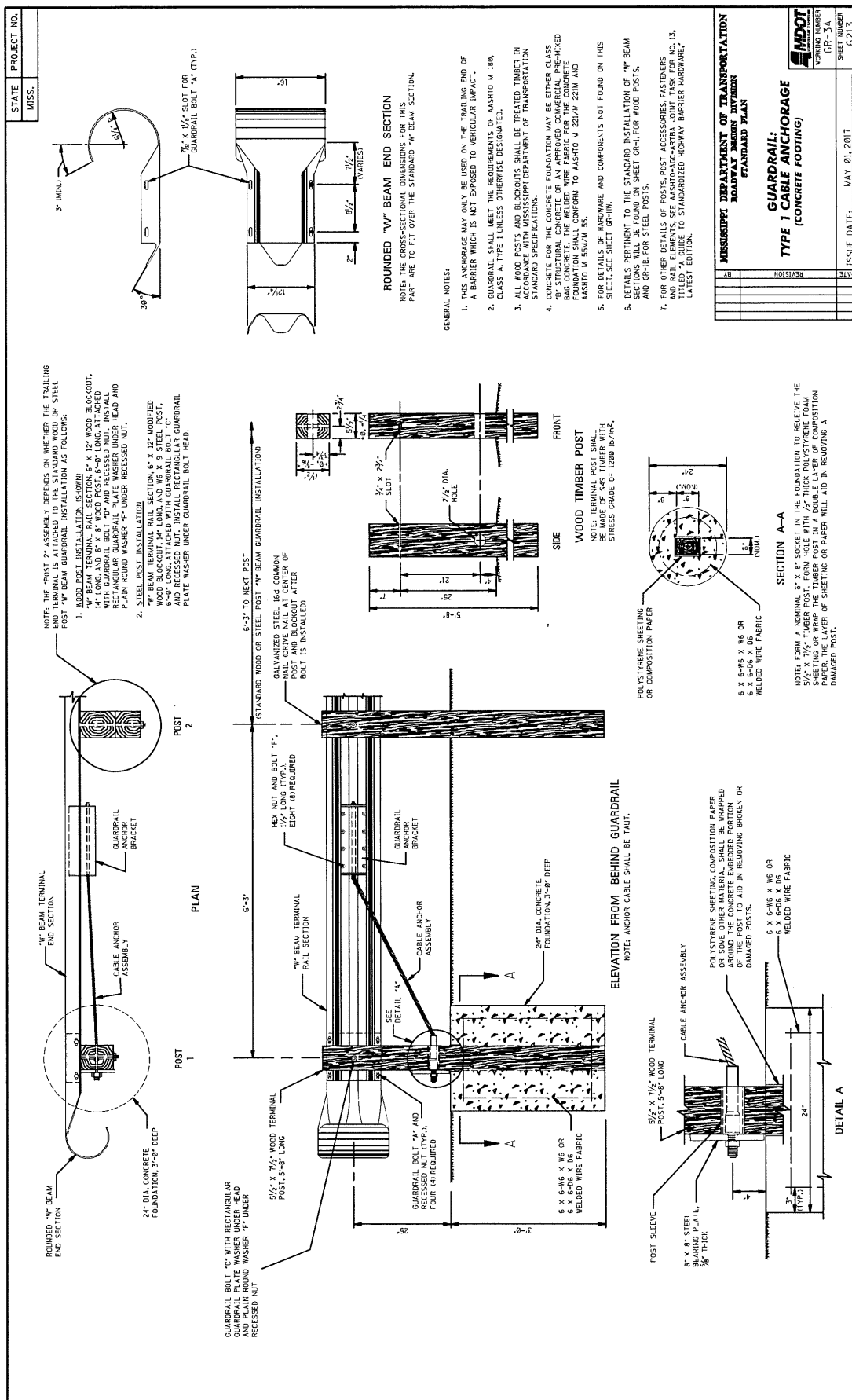


MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

GUARDRAIL:
TYPE 1 CABLE ANCHORAGE
(FOUNDATION TUBE)

DATE	ISSUE DATE: MAY 01, 2017
AS	NO. 1538
MS	
CS	
SS	
TS	
VS	
WS	
XS	
YS	
ZS	

WORKING NUMBER CR-3
 SHEET NUMBER 6212

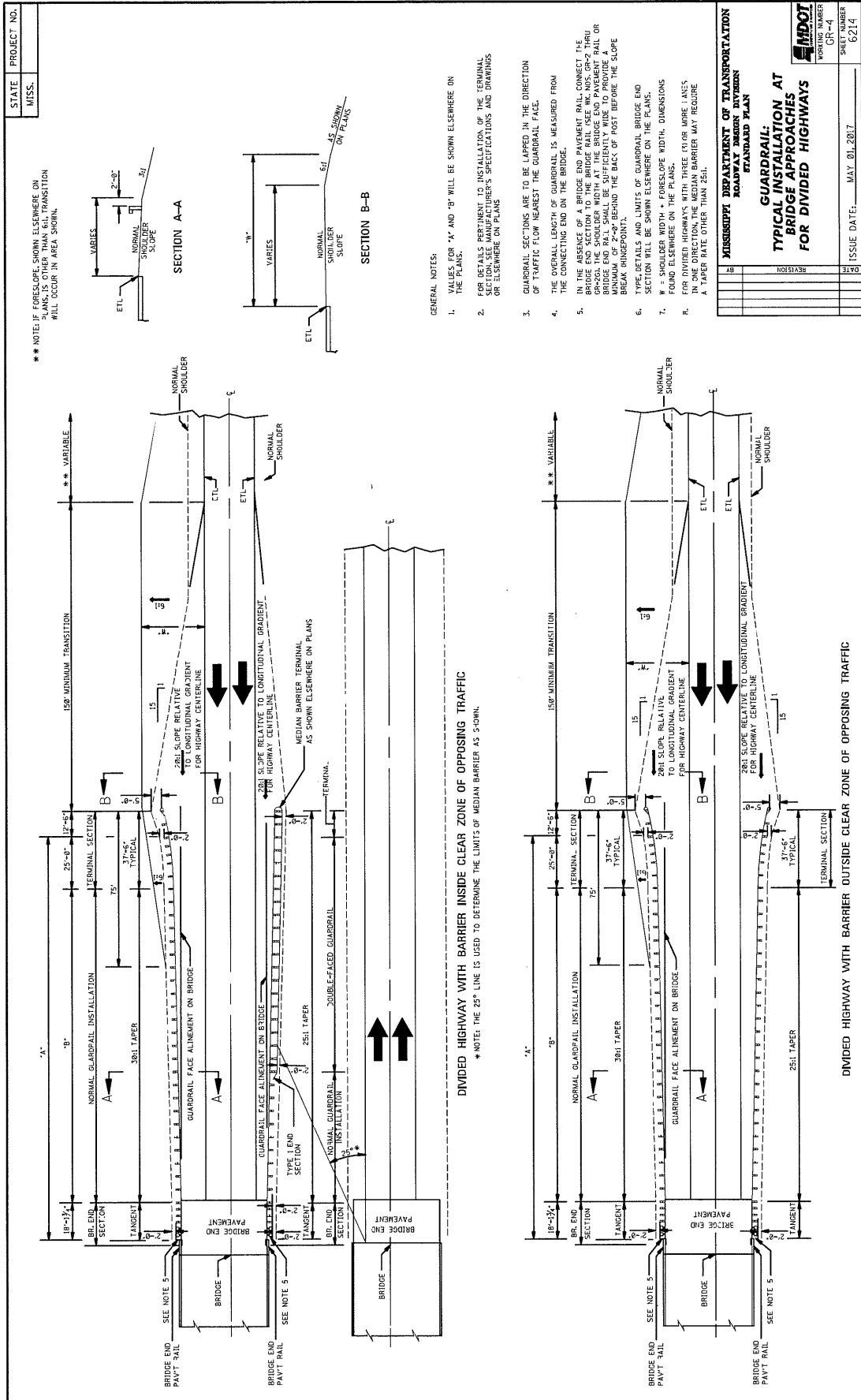


MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DIVISION
STANDARD PLAN

DATE	REVISION	DATE

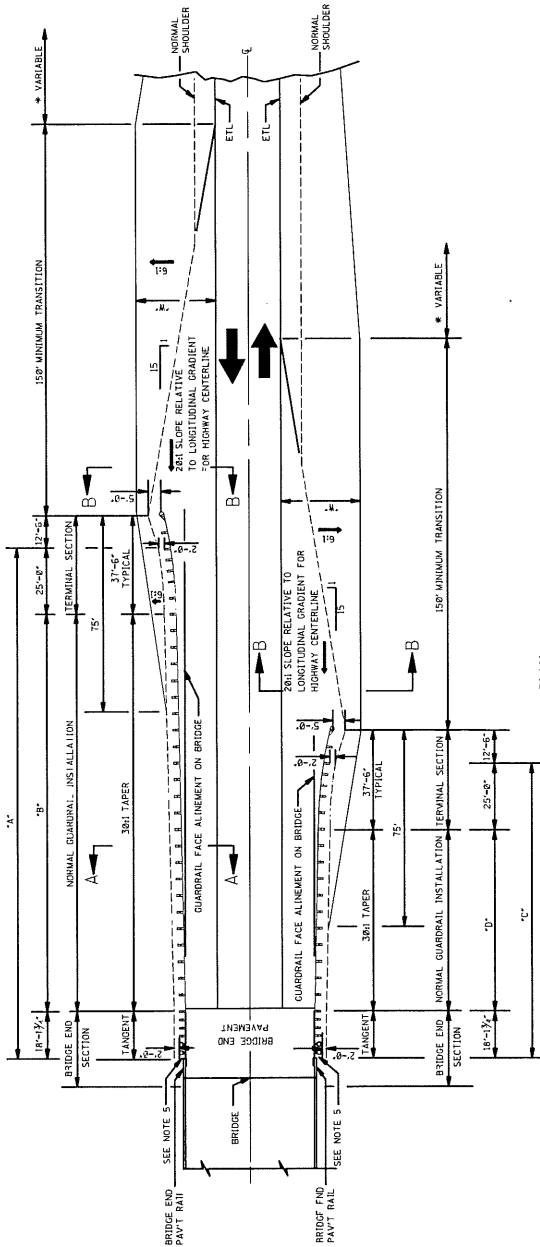
**GUARDRAIL:
TYPE 1 CABLE ANCHORAGE
(CONCRETE FOOTING)**

ISSUE DATE: MAY 01, 2012
SHEET NUMBER: 6213

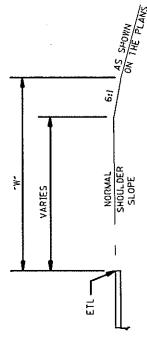


STATE	PROJECT NO.
MISS.	

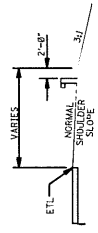
* NOTE: IF FORESLOPE SHOWN ELSEWHERE ON PLANS, IS OTHER THAN 6:1, TRANSITION WILL OCCUR IN AREA SHOWN.



PLAN



SECTION B-B



SECTION A-A

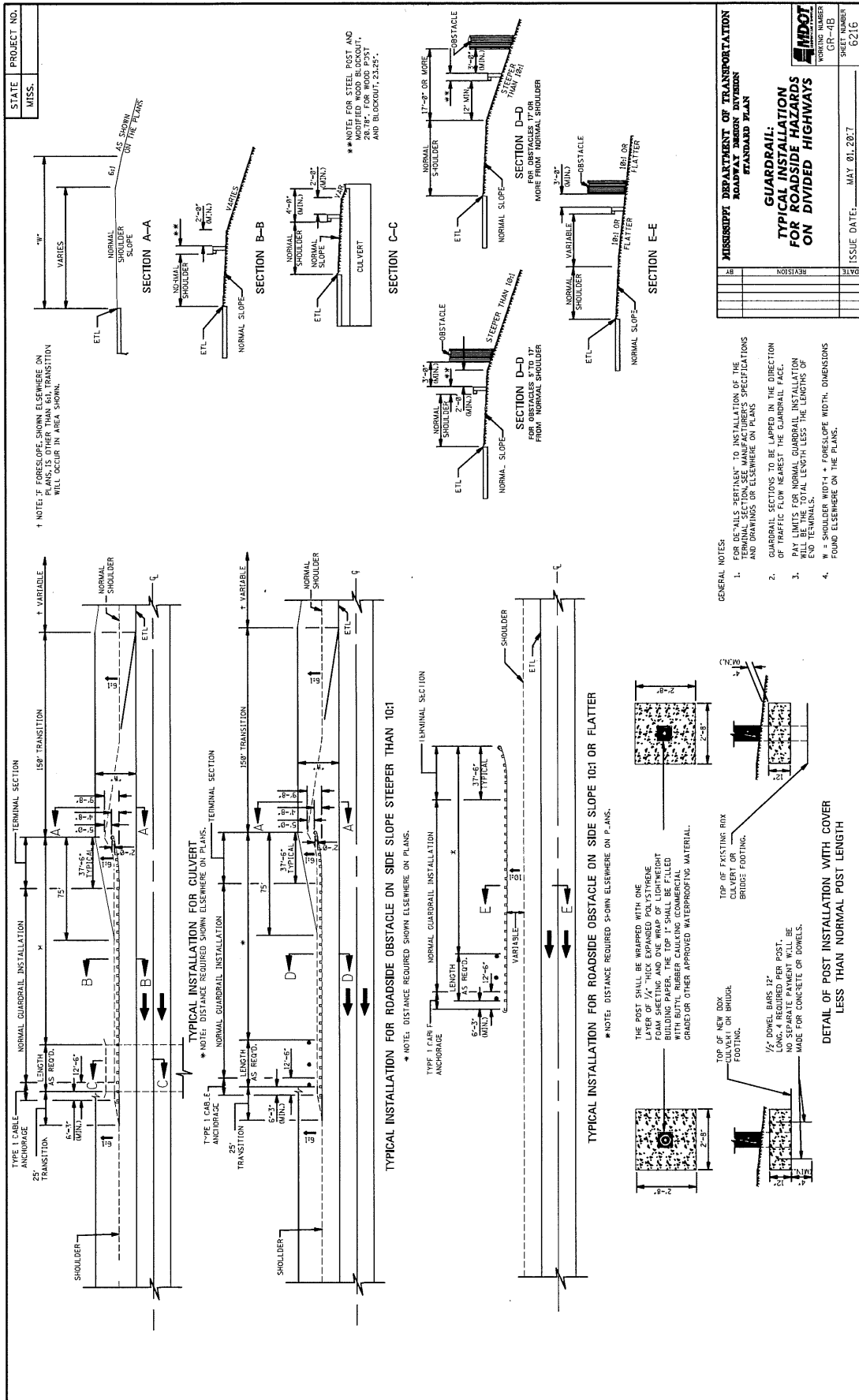
DETAIL OF GUARDRAIL SECTION LAPS



GENERAL NOTES:

- VALUES FOR "A", "B", "C" AND "D" WILL BE SHOWN ELSEWHERE ON THE PLANS.
- FOR DETAILS PERTAINING TO INSTALLATION OF THE TERMINAL SECTION, SEE MANUFACTURER'S SPECIFICATIONS AND DRAWINGS OR ELSEWHERE ON PLANS.
- GUARDRAIL SECTIONS ARE TO BE LAPPED IN THE DIRECTION OF TRAFFIC APPROACHING THE BRIDGE.
- THE OVERALL LENGTH OF GUARDRAIL IS MEASURED FROM THE CONNECTING END ON THE BRIDGE.
- IN THE ABSENCE OF A BRIDGE END PAVEMENT RAIL, CONNECT THE BRIDGE END SECTION TO THE BRIDGE RAIL (SEE W. MISS. GR-2 THRU GR-20). THE SHOULDER WIDTH AT THE BRIDGE END PAVEMENT RAIL OR BRIDGE END SECTION SHALL BE A MINIMUM OF 2'-0" BEHIND THE SLOPE BREAK (HINGEPOINT).
- TYPE DETAILS AND LIMITS OF GUARDRAIL BRIDGE END SECTION WILL BE SHOWN ELSEWHERE ON THE PLANS.
- W = SHOULDER WIDTH + FORESLOPE WIDTH. DIMENSIONS FOUND ELSEWHERE ON THE PLANS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
GUARDRAIL: TYPICAL INSTALLATION AT BRIDGE APPROACHES FOR 2-LANE, 2-WAY HIGHWAY	
WORKING NUMBER	GR-4A
SHEET NUMBER	6215
ISSUE DATE:	MAY, 01, 2017



STATE	PROJECT NO.	
MISS.	MISS.	

TYPICAL INSTALLATION FOR CULVERT
* NOTE: DISTANCE REQUIRED SHOWN ELSEWHERE ON PLANS.

TYPICAL INSTALLATION FOR ROADSIDE OBSTACLE ON SIDE SLOPE STEEPER THAN 10:1
* NOTE: DISTANCE REQUIRED SHOWN ELSEWHERE ON PLANS.

TYPICAL INSTALLATION FOR ROADSIDE OBSTACLE ON SIDE SLOPE 10:1 OR FLATTER
* NOTE: DISTANCE REQUIRED SHOWN ELSEWHERE ON P. ANS.

SECTION A-A
NORMAL SHOULDER SLOPE AS SHOWN ON THE PLANS
ETL
NORMAL SHOULDER
7"
VARIES
6H

SECTION B-B
NORMAL SHOULDER
ETL
NORMAL SLOPE
2'-0" (MIN.)

SECTION C-C
ETL
NORMAL SHOULDER
4'-0" (MIN.)
NORMAL SLOPE
2'-0" (MIN.)
CULVERT

SECTION D-D
FOR OBSTACLES 17' OR MORE FROM NORMAL SHOULDER
NORMAL SHOULDER
ETL
NORMAL SLOPE
17'-0" OR MORE
OBSTACLE
12" MIN.
STEEL POST 3" DIA. 20' MIN.
WOOD POST 4" DIA. 24" MIN.
WOOD BLOCKOUT, 24x25"

SECTION D-D
FOR OBSTACLES 17' OR MORE FROM NORMAL SHOULDER
NORMAL SHOULDER
ETL
NORMAL SLOPE
17'-0" OR MORE
OBSTACLE
12" MIN.
STEEL POST 3" DIA. 20' MIN.
WOOD POST 4" DIA. 24" MIN.
WOOD BLOCKOUT, 24x25"

SECTION E-E
NORMAL SHOULDER
ETL
NORMAL SLOPE
100'
OBSTACLE
12" MIN.
FLATTER

GENERAL NOTES:

1. ALL DETAILS PERTAINING TO INSTALLATION OF THE TERMINAL SECTION SEE MANUFACTURER'S SPECIFICATIONS AND DRAWINGS OR ELSEWHERE ON PLANS.
2. GUARDRAIL SECTIONS TO BE LAPPED IN THE DIRECTION OF TRAFFIC FLOW NEAREST THE GUARDRAIL FACE.
3. PAY LIMITS FOR NORMAL GUARDRAIL INSTALLATION SHALL BE THE TOTAL LENGTH LESS THE LENGTHS OF END TERMINALS.
4. #10 SHOULDER WIDTH * FORESLOPE WIDTH DIMENSIONS FOUND ELSEWHERE ON THE PLANS.

* NOTE: IF FORESLOPE SHOWN ELSEWHERE ON PLANS IS OTHER THAN 8:1 TRANSITION WILL OCCUR IN AREA SHOWN.

THE POST SHALL BE WRAPPED WITH ONE LAYER OF 1/4" THICK EXPANDED POLYSTYRENE INSULATION. THE POST SHALL BE SET IN A 6" DIA. BRIDGE FOOTING. THE TOP 1" SHALL BE FILLED WITH BUTYL RUBBER CAULKING (COMMERCIAL GRADE) OR OTHER APPROVED WATERPROOFING MATERIAL.

TOP OF NEW BOX CULVERT OR BRIDGE FOOTING.

1/2" DONUT BARS 12' LONG, 4 REQUIRED PER POST. NO SEPARATE PAYMENT WILL BE MADE FOR CONCRETE OR DONUTS.

DETAIL OF POST INSTALLATION WITH COVER LESS THAN NORMAL POST LENGTH

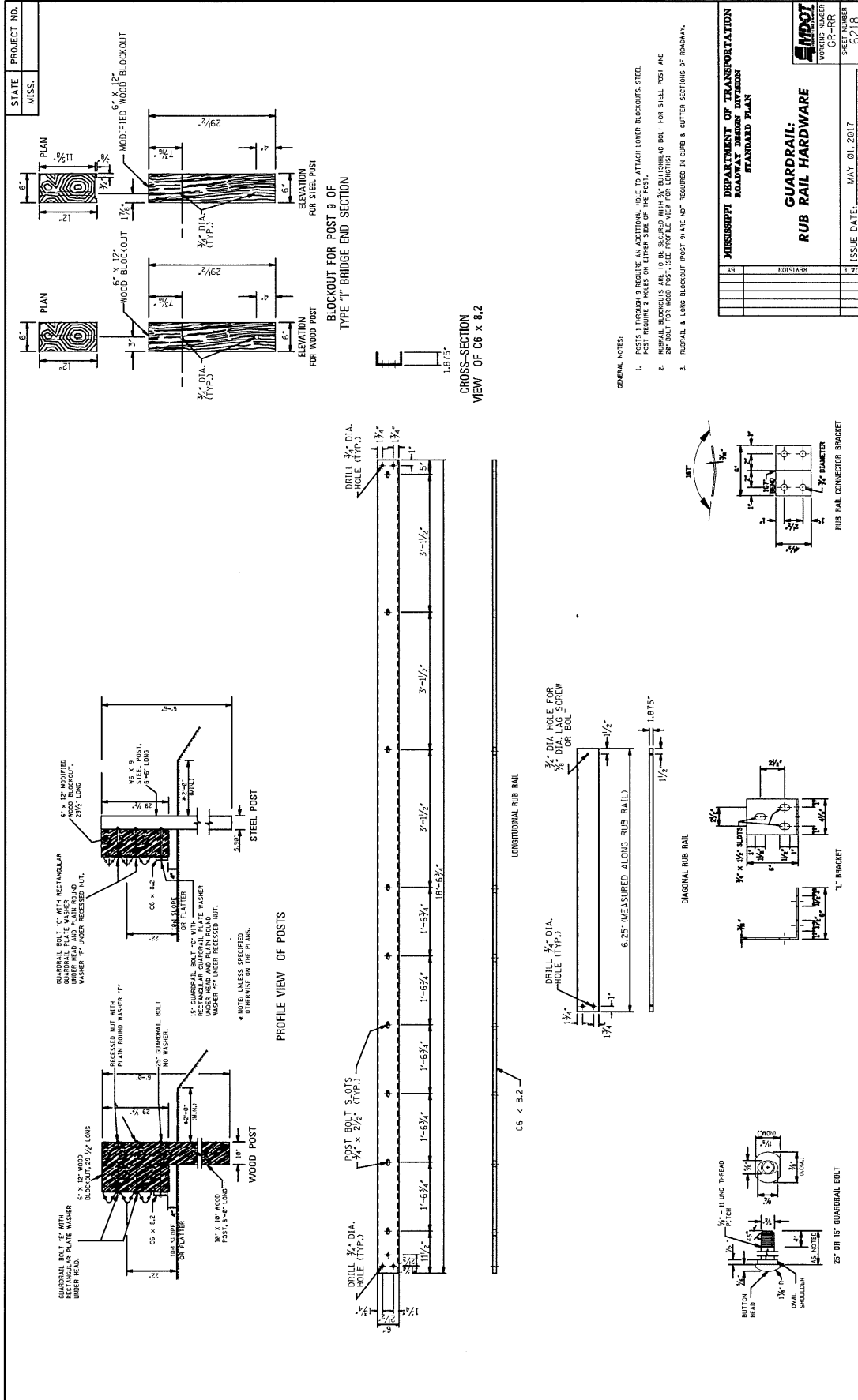
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY IMPROVEMENT DIVISION
STANDARD PLAN

GUARDRAIL: TYPICAL INSTALLATION FOR ROADSIDE HAZARDS ON 2-LANE 2-WAY HIGHWAYS

NO.	REVISION	DATE

ISSUE DATE: MAY 01, 2017
SHEET NUMBER 6217

MDOT
WORKING NUMBER GR-4C



DATE	REVISION

**MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN**

**GUARDRAIL:
RUB RAIL HARDWARE**

SHEET NUMBER: 6218
ISSUE DATE: MAY 01, 2017

STATE MISS.	PROJECT NO.	

DATE	REVISION	ISSUE DATE: MAY 01, 2017	SHEET NUMBER 6314

DETAIL OF SINGLE WHITE OR SINGLE YELLOW DELINEATOR

DETAIL OF DOUBLE WHITE OR DOUBLE YELLOW DELINEATOR ASSEMBLY

REAR VIEW OF DELINEATOR ASSEMBLY

NOTE: DELINEATORS ARE TO BE FASTENED TO U-SECTION POSTS WITH 3/4" DIA. HEX. FASTENERS OR CHERRY RIVETS OF THE COLLAR TYPE OR OTHER APPROVED EQUAL.

DELINEATORS, OBJECT MARKERS OR DISTANCE REFERENCE SIGNS SHALL BE MOUNTED TO THIS FACE AS SHOWN.

MOUNTING DETAIL

DETAIL OF TYPE 3 OBJECT MARKER

NOTE: COLORS - BLACK AND YELLOW STRIPING SHALL BE PAINTED TO THE RIGHT FOR LEFT SIDE OF BRIDGE END. SEE DETAIL BELOW.

REAR VIEW OF TYPE 3 OBJECT MARKER OR DISTANCE REFERENCE SIGN ASSEMBLY

NOTE: TYPE 3 OBJECT MARKER AND DISTANCE REFERENCE SIGNS ARE TO BE FASTENED TO U-SECTION POSTS WITH 3/4" DIA. JACK FASTENERS OR CHERRY RIVETS OF THE COLLAR TYPE OR OTHER APPROVED EQUAL.

DETAIL OF TYPE 3 OBJECT MARKER INSTALLATION

DETAIL OF TYPE 3 OBJECT MARKER WITH MOUNTABLE CURB ALONG MAIN FACILITY OR RAMP

NOTE: COLORS - BLACK AND YELLOW STRIPING SHALL BE PAINTED TO THE RIGHT FOR LEFT SIDE OF BRIDGE END. SEE DETAIL BELOW.

DELINEATOR MOUNTING ON INTERCHANGE LOOPS WITH UNMOUNTABLE CURB ON INSIDE

DELINEATOR MOUNTING ON OUTSIDE SHOULDER

NOTE: SIGN MOUNTING ON LEFT LANE SHOULDER SHALL BE 90° OPPOSITE THE RIGHT LANE STATION. IF CONDITIONS ARE SUCH THAT LANE SIGN CANNOT BE MOUNTED AT THIS LOCATION, IT SHALL BE OMITTED ENTIRELY.

DELINEATOR MOUNTING ON OUTSIDE SHOULDER WITH MOUNTABLE CURB ALONG MAIN FACILITY OR RAMP

DELINEATOR MOUNTING ON OUTSIDE SHOULDER WITH MOUNTABLE CURB ALONG MAIN FACILITY OR RAMP

DELINEATOR MOUNTING ON OUTSIDE SHOULDER WITH MOUNTABLE CURB ALONG MAIN FACILITY OR RAMP

DELINEATOR MOUNTING ON OUTSIDE SHOULDER WITH MOUNTABLE CURB ALONG MAIN FACILITY OR RAMP

DELINEATOR MOUNTING ON OUTSIDE SHOULDER WITH MOUNTABLE CURB ALONG MAIN FACILITY OR RAMP

	STATE MISS.	PROJECT NO.	
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GENERAL NOTES:

1. U.S. AND MISSISSIPPI SHIELDS DO NOT HAVE AN OUTSIDE BORDER.
2. INTERSTATE, U.S. AND MISSISSIPPI SHIELDS SHOWN ON THIS DRAWING ARE FOR INFORMATION ONLY. OTHER DRAWINGS FOR SHIELDS TO BE USED INDEPENDENTLY AS ROUTE MARKERS.
3. ON INTERSTATE SHIELDS, THE LEGEND AND BACKGROUND SHALL BE REFLECTORIZED.
4. ON U.S. AND MISSISSIPPI SHIELDS, THE BACKGROUND SHALL BE REFLECTORIZED.
5. IN SOME CASES, NUMERALS CANNOT BE REFLECTORIZED. THESE SITUATIONS ARE INDICATED BY AN "X" IN THE SPACE AVAILABLE FOR THESE SITUATIONS. THE STANDARD SERIES "0" NUMERALS MAY BE REDUCED TO 1/2" HEIGHT AS A SECOND CHOICE TO THE NEXT SMALLER HEIGHT COMMONLY AVAILABLE.

	A	B	C	D	E
12-DIGITS	24"	24"	6"	17"	12"
12-DIGITS	35"	35"	5"	18"	13"
12-DIGITS	46"	46"	4"	19"	14"
3-DIGITS	45"	36"	6"	12"	VAR.
3-DIGITS	60"	48"	12"	24"	VAR.

	12-DIGITS	12-DIGITS	12-DIGITS	24-DIGITS	24-DIGITS
A	24"	24"	35"	45"	48"
B	24"	24"	35"	45"	48"
C	5"	5"	7 1/2"	7 1/2"	10"
D	5"	5"	7 1/2"	7 1/2"	10"
E	5"	5"	7 1/2"	7 1/2"	10"
F	2"	2"	3"	3"	4"
G	7"	7"	10 1/2"	10 1/2"	14"
H	7"	7"	10 1/2"	10 1/2"	14"
I	7"	7"	10 1/2"	10 1/2"	14"
J	12"	12"	18"	18"	24"
K	12"	12"	18"	18"	24"
L	13"	13"	19 1/2"	19 1/2"	25"
M	13"	13"	19 1/2"	19 1/2"	25"
N	13"	13"	19 1/2"	19 1/2"	25"

	12-DIGITS	12-DIGITS	24-DIGITS	24-DIGITS
A	24"	30"	45"	48"
B	24"	30"	45"	48"
C	15"	24"	25 1/2"	30"
D	15"	24"	25 1/2"	30"
E	5"	5"	7 1/2"	10"
F	7 1/2"	7 1/2"	10 1/2"	14"
G	7 1/2"	7 1/2"	10 1/2"	14"
H	7 1/2"	7 1/2"	10 1/2"	14"
I	7 1/2"	7 1/2"	10 1/2"	14"
J	12"	12"	18"	24"
K	12"	12"	18"	24"
L	13"	13"	19 1/2"	25"
M	13"	13"	19 1/2"	25"
N	13"	13"	19 1/2"	25"

	A	B	C	D	E
12-DIGITS	24"	24"	6"	17"	12"
12-DIGITS	35"	35"	5"	18"	13"
12-DIGITS	46"	46"	4"	19"	14"
3-DIGITS	45"	36"	6"	12"	VAR.
3-DIGITS	60"	48"	12"	24"	VAR.

GENERAL NOTES:

1. THESE ARE "OVERLAY" SIGNS FABRICATED ON 0.063" THICK ALUMINUM (8881-T6) AND ARE TO BE RIVETED TO DIRECTIONAL SIGNS DESIGNATED ON PLANS. THESE SIGNS WILL BE ASSEMBLED AS PART OF THE MAJOR SIGNS TO WHICH THEY ARE AFFIXED.
2. LETTER SIZE: 1 1/2" SERIES "0".
COLOR: LEGEND - BLACK; BACKGROUND - HI-INTENSITY YELLOW.

	A	B	C	D	E
VAR.	65"	39 1/2"	13 3/4"	13 3/4"	66"
A	12"	12"	12"	12"	12"
B	12"	12"	12"	12"	12"
C	12"	12"	12"	12"	12"
D	12"	12"	12"	12"	12"
E	12"	12"	12"	12"	12"
F	12"	12"	12"	12"	12"
G	12"	12"	12"	12"	12"
H	12"	12"	12"	12"	12"
I	12"	12"	12"	12"	12"
J	12"	12"	12"	12"	12"
K	12"	12"	12"	12"	12"
L	12"	12"	12"	12"	12"
M	12"	12"	12"	12"	12"
N	12"	12"	12"	12"	12"
O	12"	12"	12"	12"	12"
P	12"	12"	12"	12"	12"
Q	12"	12"	12"	12"	12"
R	12"	12"	12"	12"	12"
S	12"	12"	12"	12"	12"
T	12"	12"	12"	12"	12"
U	12"	12"	12"	12"	12"
V	12"	12"	12"	12"	12"
W	12"	12"	12"	12"	12"
X	12"	12"	12"	12"	12"
Y	12"	12"	12"	12"	12"
Z	12"	12"	12"	12"	12"

	A	B	C	D	E
VAR.	65"	39 1/2"	13 3/4"	13 3/4"	66"
A	12"	12"	12"	12"	12"
B	12"	12"	12"	12"	12"
C	12"	12"	12"	12"	12"
D	12"	12"	12"	12"	12"
E	12"	12"	12"	12"	12"
F	12"	12"	12"	12"	12"
G	12"	12"	12"	12"	12"
H	12"	12"	12"	12"	12"
I	12"	12"	12"	12"	12"
J	12"	12"	12"	12"	12"
K	12"	12"	12"	12"	12"
L	12"	12"	12"	12"	12"
M	12"	12"	12"	12"	12"
N	12"	12"	12"	12"	12"
O	12"	12"	12"	12"	12"
P	12"	12"	12"	12"	12"
Q	12"	12"	12"	12"	12"
R	12"	12"	12"	12"	12"
S	12"	12"	12"	12"	12"
T	12"	12"	12"	12"	12"
U	12"	12"	12"	12"	12"
V	12"	12"	12"	12"	12"
W	12"	12"	12"	12"	12"
X	12"	12"	12"	12"	12"
Y	12"	12"	12"	12"	12"
Z	12"	12"	12"	12"	12"

	A	B	C	D	E
VAR.	65"	39 1/2"	13 3/4"	13 3/4"	66"
A	12"	12"	12"	12"	12"
B	12"	12"	12"	12"	12"
C	12"	12"	12"	12"	12"
D	12"	12"	12"	12"	12"
E	12"	12"	12"	12"	12"
F	12"	12"	12"	12"	12"
G	12"	12"	12"	12"	12"
H	12"	12"	12"	12"	12"
I	12"	12"	12"	12"	12"
J	12"	12"	12"	12"	12"
K	12"	12"	12"	12"	12"
L	12"	12"	12"	12"	12"
M	12"	12"	12"	12"	12"
N	12"	12"	12"	12"	12"
O	12"	12"	12"	12"	12"
P	12"	12"	12"	12"	12"
Q	12"	12"	12"	12"	12"
R	12"	12"	12"	12"	12"
S	12"	12"	12"	12"	12"
T	12"	12"	12"	12"	12"
U	12"	12"	12"	12"	12"
V	12"	12"	12"	12"	12"
W	12"	12"	12"	12"	12"
X	12"	12"	12"	12"	12"
Y	12"	12"	12"	12"	12"
Z	12"	12"	12"	12"	12"

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

ROUTE SHIELDS AND "EXIT ONLY" PANELS

MDOT
WORKING NUMBER
SN-2
SHEET NUMBER
6302

ISSUE DATE: MAY 01, 2017

SIGN NUMBER	STATE PROJECT NO.										
	MISS.										
ALUMINUM (6061-T6) SIGN BLANK THICKNESS	0.080"	0.080"	0.100"	0.100"	0.100"	0.100"	0.100"	0.100"	0.100"	0.080"	0.080"
LEGEND	 										
LETTER & NUMERICAL SERIES	20" SERIES 'H' 4" SERIES 'B' 10" SERIES 'C' 'E' MOD. 'E' MOD. 'I' 'P' SERIES 'D'	4" SERIES 'B' 'I' 'P' SERIES 'D'	4" SERIES 'B' 'I' 'P' SERIES 'D'	4" SERIES 'B' 'I' 'P' SERIES 'D'	4" SERIES 'B' 'I' 'P' SERIES 'D'	4" SERIES 'B' 'I' 'P' SERIES 'D'	4" SERIES 'B' 'I' 'P' SERIES 'D'	4" SERIES 'B' 'I' 'P' SERIES 'D'	4" SERIES 'B' 'I' 'P' SERIES 'D'	4" SERIES 'B' 'I' 'P' SERIES 'D'	4" SERIES 'B' 'I' 'P' SERIES 'D'
WIDTH OF BORDER	1/2" WHITE	1/2" WHITE	1/2" WHITE	1/2" WHITE	1/2" WHITE	1/2" WHITE	1/2" WHITE	1/2" WHITE	1/2" WHITE	1/2" WHITE	1/2" WHITE
SIZE (WIDTH X HEIGHT)	24" X 24"	36" X 36"	12" X 24"	12" X 36"	12" X 48"	12" X 48"	12" X 36"	12" X 48"	12" X 48"	12" X 48"	12" X 48"
COLORS	WHITE	WHITE	WHITE	WHITE	WHITE	WHITE	WHITE	WHITE	WHITE	WHITE	WHITE
REFLECTORIZATION	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL
NUMBER OF POSTS	1	1	1	1	1	1	1	1	1	1	1
FOR MOUNTING	1	1	1	1	1	1	1	1	1	1	1
PUNCHING DISTANCE FROM CENTER	12" (VERT. CENTER)	18" (VERT. CENTER)	6" (VERT. CENTER)	6" (VERT. CENTER)	6" (VERT. CENTER)	6" (VERT. CENTER)	6" (VERT. CENTER)	6" (VERT. CENTER)	6" (VERT. CENTER)	6" (VERT. CENTER)	6" (VERT. CENTER)
PUNCHING DISTANCE FROM TOP EDGE	31" 21"	31" 21"	31" 21"	31" 21"	31" 21"	31" 21"	31" 21"	31" 21"	31" 21"	31" 21"	31" 21"
ALUMINUM (6061-T6) SIGN BLANK THICKNESS	0.080"	0.080"	0.080"	0.080"	0.080"	0.080"	0.080"	0.080"	0.080"	0.080"	0.080"
LEGEND	 										
LETTER & NUMERICAL SERIES	9" SERIES 'C' 7" SERIES 'C' 5" SERIES 'C' 5" SERIES 'C'	7" SERIES 'C' 5" SERIES 'C' 5" SERIES 'C'	5" SERIES 'C' 5" SERIES 'C'	5" SERIES 'C' 5" SERIES 'C'	5" SERIES 'C' 5" SERIES 'C'	5" SERIES 'C' 5" SERIES 'C'	5" SERIES 'C' 5" SERIES 'C'	5" SERIES 'C' 5" SERIES 'C'	5" SERIES 'C' 5" SERIES 'C'	5" SERIES 'C' 5" SERIES 'C'	5" SERIES 'C' 5" SERIES 'C'
WIDTH OF BORDER	1/2" WHITE	1/2" WHITE	1/2" WHITE	1/2" WHITE	1/2" WHITE	1/2" WHITE	1/2" WHITE	1/2" WHITE	1/2" WHITE	1/2" WHITE	1/2" WHITE
SIZE (WIDTH X HEIGHT)	21" X 15"	24" X 12"	30" X 15"	30" X 15"	21" X 15"	21" X 15"	21" X 15"	21" X 15"	21" X 15"	21" X 15"	21" X 15"
COLORS	BLACK	WHITE	BLACK	BLACK	BLACK	BLACK	BLACK	BLACK	BLACK	BLACK	BLACK
REFLECTORIZATION	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL
NUMBER OF HOLES TO BE PUNCHED (2" DIA.)	1	2	2	2	2	2	2	2	2	2	2
PUNCHING DISTANCE FROM EACH VERTICAL EDGE	1/2" 1 1/2"	1/2" 1 1/2"	1/2" 1 1/2"	1/2" 1 1/2"	1/2" 1 1/2"	1/2" 1 1/2"	1/2" 1 1/2"	1/2" 1 1/2"	1/2" 1 1/2"	1/2" 1 1/2"	1/2" 1 1/2"
PUNCHING DISTANCE FROM TOP EDGE	1 1/2" 1 1/2"	1 1/2" 1 1/2"	1 1/2" 1 1/2"	1 1/2" 1 1/2"	1 1/2" 1 1/2"	1 1/2" 1 1/2"	1 1/2" 1 1/2"	1 1/2" 1 1/2"	1 1/2" 1 1/2"	1 1/2" 1 1/2"	1 1/2" 1 1/2"
ALUMINUM (6061-T6) SIGN BLANK THICKNESS	0.080"	0.080"	0.080"	0.080"	0.080"	0.080"	0.080"	0.080"	0.080"	0.080"	0.080"
GENERAL NOTES:	<p>1. THE DIMENSIONS FOR THE INTERSTATE AND U.S. SHIELDS SHALL CONFORM WITH THE DIMENSIONS SPECIFIED IN THE STANDARD SPECIFICATIONS FOR ROADWAY DESIGN DIVISION CONTROL DEVICES, LATEST EDITION.</p> <p>2. ROUTE MARKERS AND SHIELDS SHOWN ON THIS SHEET ARE FOR USE INDEPENDENTLY OF INTERSTATE DIRECTIONAL (GUIDE) SIGNS. SEE OTHER DRAWINGS FOR SHIELDS TO BE USED ON INTERSTATE DIRECTIONAL (GUIDE) SIGNS.</p> <p>3. THE QUANTITIES LISTED ON THE SUMMARY OF QUANTITIES SHEET FOR THE SIGNS SHOWN ON THIS SHEET WILL BE USED AS THE BASIS FOR FINAL PAYMENT, EXCEPT WHERE SIGNS ARE MODIFIED FROM THAT SHOWN.</p>										

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**STANDARD
ROADSIDE SIGNS**

WORKING NUMBER
SN-3

SHEET NUMBER
6-303

ISSUE DATE: MAY 01, 2017

REVISION	DATE	BY	DESCRIPTION

* NORTH SOUTH WEST EAST
M5-1A M5-2A M5-2LA M5-2RA
M5-10A M5-102A M5-103A M5-104A

4. UNLESS SOME BASES NUMBERS CANNOT BE ACCOMMODATED WITHIN THE SPACE AVAILABLE FOR THESE STATIONS, THE STANDARD SERIES OF NUMBERS SHALL BE REDUCED TO SERIES 'C'.

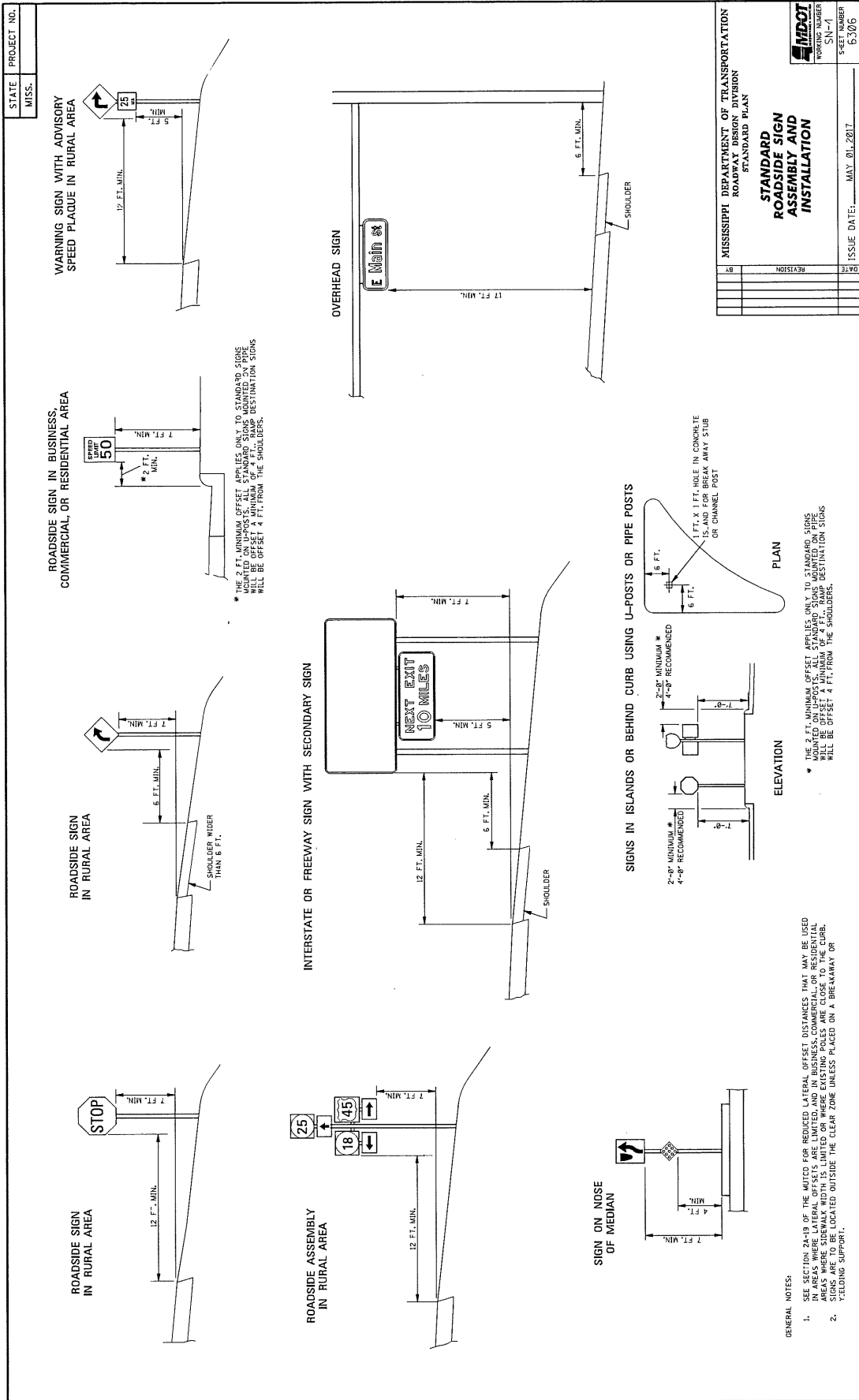
SIGN NUMBER	R1-1		R1-2		R2-1		R2-40		R3-1		R3-2		R3-4		R3-5		R4-3		
	0.100"	0.125"	0.082"	0.100"	0.125"	2.080"	0.100"	0.125"	0.088"	0.100"	0.088"	0.100"	0.088"	0.100"	0.088"	0.125"	0.100"	0.125"	
ALUMINUM (6861-16) SIGN BLANK THICKNESS																			
LEGEND																			
LETTER & NUMERICAL SERIES	12" SERIES "C" 7/8" WHITE	5" RED 3/4" WHITE	4" SERIES "E" 10" SERIES "C" 14" SERIES "E" 14" SERIES "D"	8" SERIES "E" 16" SERIES "E" 16" SERIES "C" 14" SERIES "E" 14" SERIES "D"	4" SERIES "D" 8" SERIES "D"	4" SERIES "D" 8" SERIES "D"	8" SERIES "E" 16" SERIES "E" 16" SERIES "C" 14" SERIES "E" 14" SERIES "D"	8" SERIES "E" 16" SERIES "E" 16" SERIES "C" 14" SERIES "E" 14" SERIES "D"	4" SERIES "D" 8" SERIES "D"	4" SERIES "D" 8" SERIES "D"	4" SERIES "D" 8" SERIES "D"	4" SERIES "D" 8" SERIES "D"	4" SERIES "D" 8" SERIES "D"	4" SERIES "D" 8" SERIES "D"	4" SERIES "D" 8" SERIES "D"	4" SERIES "D" 8" SERIES "D"	4" SERIES "D" 8" SERIES "D"	4" SERIES "D" 8" SERIES "D"	4" SERIES "D" 8" SERIES "D"
WIDTH OF BORDER	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE
SIZE (WIDTH X HEIGHT)	36" OCTAGON	36" TRIANGLE	24" X 30"	48" X 36"	24" X 24"	36" X 36"	24" X 24"	48" X 36"	24" X 24"	36" X 36"	24" X 24"	36" X 36"	24" X 24"	36" X 36"	24" X 24"	36" X 36"	24" X 24"	36" X 36"	24" X 24"
COLORS	WHITE RED ALL	RED WHITE ALL	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND
REFLECTORIZATION	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL
NUMBER OF POSTS	2	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1
NUMBER OF HOLES TO BE PUNCHED (IN DIA.)	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4
SPACING (IN DIA.) FROM EACH VERTICAL EDGE	15" (VERT. CENTER)	15" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)
PUNCHING DISTANCE FROM TOP EDGE	31.33"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"
ALUMINUM (6861-16) SIGN BLANK THICKNESS																			
LEGEND																			
LETTER & NUMERICAL SERIES	R-2/4"	R-3"	R-1/2"	R-1/2"	R-1/2"	R-2/4"	R-3"	R-1/2"	R-1/2"	R-1/2"	R-1/2"	R-1/2"	R-1/2"	R-1/2"	R-1/2"	R-1/2"	R-1/2"	R-1/2"	R-1/2"
WIDTH OF BORDER	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE	OUTSIDE
SIZE (WIDTH X HEIGHT)	24" X 36"	36" X 48"	36" X 48"	36" X 48"	36" X 48"	36" X 48"	36" X 48"	36" X 48"	36" X 48"	36" X 48"	36" X 48"	36" X 48"	36" X 48"	36" X 48"	36" X 48"	36" X 48"	36" X 48"	36" X 48"	36" X 48"
COLORS	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND	BLACK WHITE BACKGROUND
REFLECTORIZATION	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL	ALL
NUMBER OF POSTS	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1
NUMBER OF HOLES TO BE PUNCHED (IN DIA.)	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2
SPACING (IN DIA.) FROM EACH VERTICAL EDGE	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)	12" (VERT. CENTER)
PUNCHING DISTANCE FROM TOP EDGE	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"	31.27"

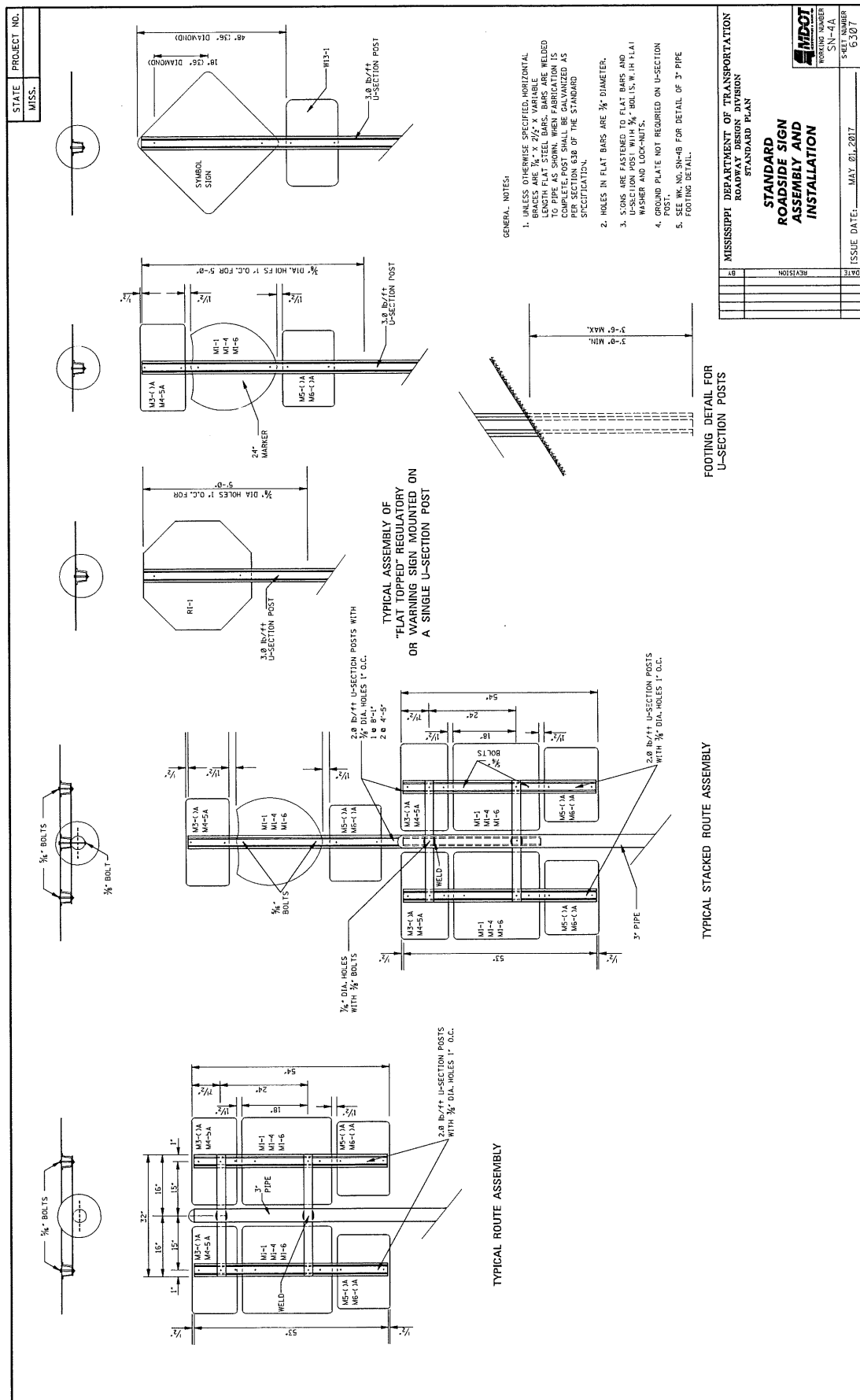
GENERAL NOTES:
 1. THE QUANTITIES LISTED ON THE SUMMARY OF QUANTITIES SHEET FOR THE PROJECT SHALL BE USED AS A GUIDE ONLY. THE FINAL QUANTITIES FOR FINAL PAYMENT, EXCEPT WHERE SHOWN, ARE MODIFIED FROM THAT SHOWN.
 2. THE ORDER LIMITS REQUIRED ON SIGNS R2-1 AND R2-40 WILL BE SHOWN ON INDIVIDUAL PLAN SHEETS.

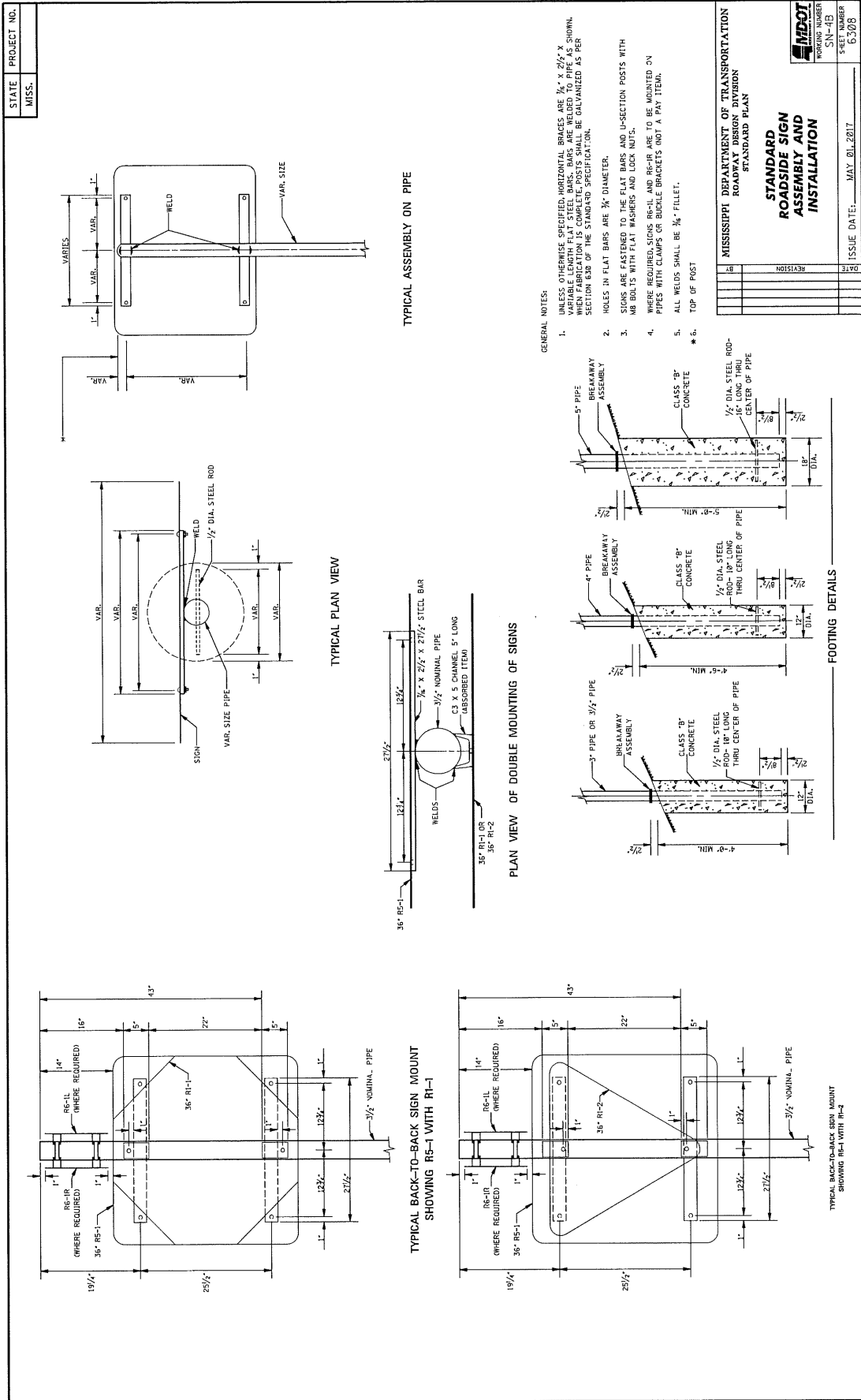
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN

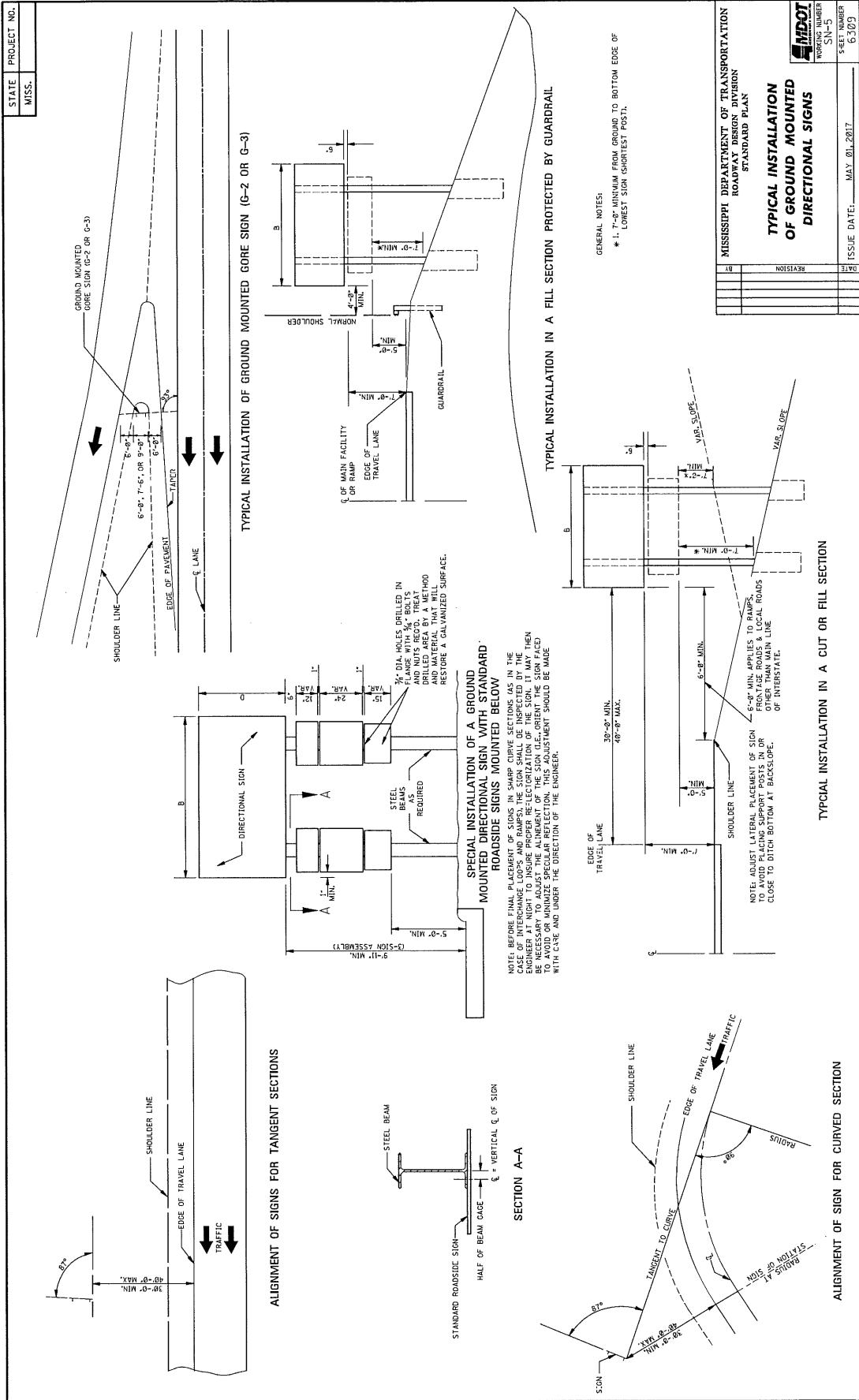
**STANDARD
 ROADSIDE SIGNS**

ISSUE DATE: MAY 01, 2017
 SHEET NUMBER 6304
 WORKING NUMBER SN-3A





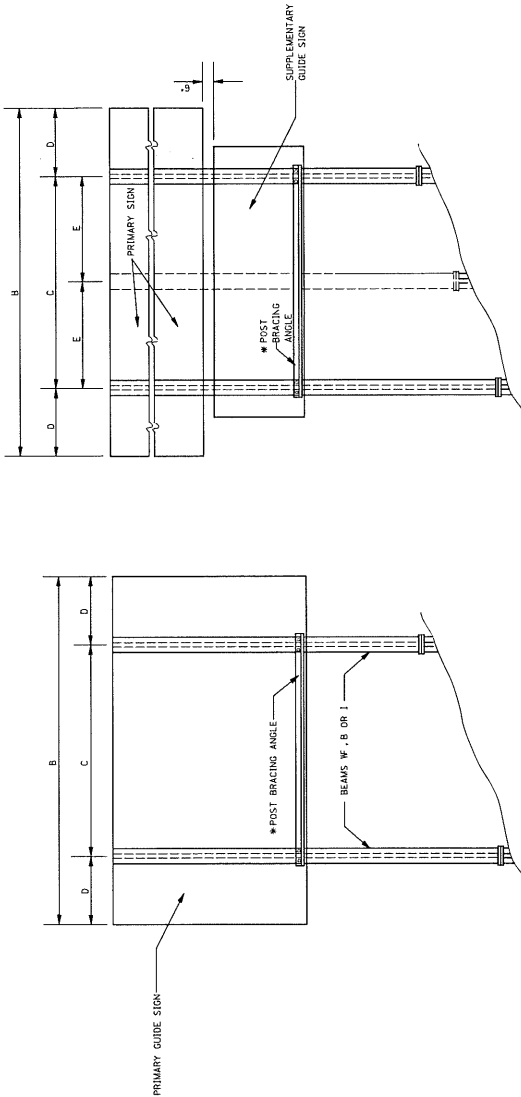




STATE	PROJECT NO.
MISS.	

GENERAL NOTES FOR WORKING SHEETS SH-6, SH-6A AND SH-6B

- 1. EDDLING**
ALL BOLTS SHALL BE CLASS "B" CONCRETE "PMS" STUDS SHALL BE "C" IN CONCRETE FOOTING AT REQUIRED GRADE AND ALIGNMENT WITH CARE SO THAT MINIMUM SHIMMING WILL BE REQUIRED.
- 2. BASE CONNECTION PROCEDURE**
ASSEMBLE POST TO STUD WITH BOLTS AND WITH A FLAT WASHER ON EACH BOLT BETWEEN PLATES. TIGHTEN BOLTS TO THE MAXIMUM POSSIBLE WITH 12" TORQUE WRENCH. REMOVE BOLTS AND CLEAN BOLT HEADS AND TO CLEAN BOLT THREADS. THEN LOOSEN EACH BOLT IN TURN AND TIGHTEN IN A SYSTEMATIC ORDER TO THE PRESCRIBED TORQUE AS SHOWN BY TABLE. BURR THREADS AT JUNCTION WITH NUT USING A CENTER PUNCH. HIGH STRENGTH BOLTS AND NUTS SHALL BE TIGHTENED TO TORQUE AS SHOWN BY TABLE ON SH-6A, 6B, 6C, 6D, 6E, 6F, 6G, 6H, 6I, 6J, 6K, 6L, 6M, 6N, 6O, 6P, 6Q, 6R, 6S, 6T, 6U, 6V, 6W, 6X, 6Y, 6Z, 6AA, 6AB, 6AC, 6AD, 6AE, 6AF, 6AG, 6AH, 6AI, 6AJ, 6AK, 6AL, 6AM, 6AN, 6AO, 6AP, 6AQ, 6AR, 6AS, 6AT, 6AU, 6AV, 6AW, 6AX, 6AY, 6AZ, 6BA, 6BB, 6BC, 6BD, 6BE, 6BF, 6BG, 6BH, 6BI, 6BJ, 6BK, 6BL, 6BM, 6BN, 6BO, 6BP, 6BQ, 6BR, 6BS, 6BT, 6BU, 6BV, 6BW, 6BX, 6BY, 6BZ, 6CA, 6CB, 6CC, 6CD, 6CE, 6CF, 6CG, 6CH, 6CI, 6CJ, 6CK, 6CL, 6CM, 6CN, 6CO, 6CP, 6CQ, 6CR, 6CS, 6CT, 6CU, 6CV, 6CW, 6CX, 6CY, 6CZ, 6DA, 6DB, 6DC, 6DD, 6DE, 6DF, 6DG, 6DH, 6DI, 6DJ, 6DK, 6DL, 6DM, 6DN, 6DO, 6DP, 6DQ, 6DR, 6DS, 6DT, 6DU, 6DV, 6DW, 6DX, 6DY, 6DZ, 6EA, 6EB, 6EC, 6ED, 6EE, 6EF, 6EG, 6EH, 6EI, 6EJ, 6EK, 6EL, 6EM, 6EN, 6EO, 6EP, 6EQ, 6ER, 6ES, 6ET, 6EU, 6EV, 6EW, 6EX, 6EY, 6EZ, 6FA, 6FB, 6FC, 6FD, 6FE, 6FF, 6FG, 6FH, 6FI, 6FJ, 6FK, 6FL, 6FM, 6FN, 6FO, 6FP, 6FQ, 6FR, 6FS, 6FT, 6FU, 6FV, 6FW, 6FX, 6FY, 6FZ, 6GA, 6GB, 6GC, 6GD, 6GE, 6GF, 6GG, 6GH, 6GI, 6GJ, 6GK, 6GL, 6GM, 6GN, 6GO, 6GP, 6GQ, 6GR, 6GS, 6GT, 6GU, 6GV, 6GW, 6GX, 6GY, 6GZ, 6HA, 6HB, 6HC, 6HD, 6HE, 6HF, 6HG, 6HH, 6HI, 6HJ, 6HK, 6HL, 6HM, 6HN, 6HO, 6HP, 6HQ, 6HR, 6HS, 6HT, 6HU, 6HV, 6HW, 6HX, 6HY, 6HZ, 6IA, 6IB, 6IC, 6ID, 6IE, 6IF, 6IG, 6IH, 6II, 6IJ, 6IK, 6IL, 6IM, 6IN, 6IO, 6IP, 6IQ, 6IR, 6IS, 6IT, 6IU, 6IV, 6IW, 6IX, 6IY, 6IZ, 6JA, 6JB, 6JC, 6JD, 6JE, 6JF, 6JG, 6JH, 6JI, 6JJ, 6JK, 6JL, 6JM, 6JN, 6JO, 6JP, 6JQ, 6JR, 6JS, 6JT, 6JU, 6JV, 6JW, 6JX, 6JY, 6JZ, 6KA, 6KB, 6KC, 6KD, 6KE, 6KF, 6KG, 6KH, 6KI, 6KJ, 6KK, 6KL, 6KM, 6KN, 6KO, 6KP, 6KQ, 6KR, 6KS, 6KT, 6KU, 6KV, 6KW, 6KX, 6KY, 6KZ, 6LA, 6LB, 6LC, 6LD, 6LE, 6LF, 6LG, 6LH, 6LI, 6LJ, 6LK, 6LL, 6LM, 6LN, 6LO, 6LP, 6LQ, 6LR, 6LS, 6LT, 6LU, 6LV, 6LW, 6LX, 6LY, 6LZ, 6MA, 6MB, 6MC, 6MD, 6ME, 6MF, 6MG, 6MH, 6MI, 6MJ, 6MK, 6ML, 6MM, 6MN, 6MO, 6MP, 6MQ, 6MR, 6MS, 6MT, 6MU, 6MV, 6MW, 6MX, 6MY, 6MZ, 6NA, 6NB, 6NC, 6ND, 6NE, 6NF, 6NG, 6NH, 6NI, 6NJ, 6NK, 6NL, 6NM, 6NN, 6NO, 6NP, 6NQ, 6NR, 6NS, 6NT, 6NU, 6NV, 6NW, 6NX, 6NY, 6NZ, 6OA, 6OB, 6OC, 6OD, 6OE, 6OF, 6OG, 6OH, 6OI, 6OJ, 6OK, 6OL, 6OM, 6ON, 6OO, 6OP, 6OQ, 6OR, 6OS, 6OT, 6OU, 6OV, 6OW, 6OX, 6OY, 6OZ, 6PA, 6PB, 6PC, 6PD, 6PE, 6PF, 6PG, 6PH, 6PI, 6PJ, 6PK, 6PL, 6PM, 6PN, 6PO, 6PP, 6PQ, 6PR, 6PS, 6PT, 6PU, 6PV, 6PW, 6PX, 6PY, 6PZ, 6QA, 6QB, 6QC, 6QD, 6QE, 6QF, 6QG, 6QH, 6QI, 6QJ, 6QK, 6QL, 6QM, 6QN, 6QO, 6QP, 6QQ, 6QR, 6QS, 6QT, 6QU, 6QV, 6QW, 6QX, 6QY, 6QZ, 6RA, 6RB, 6RC, 6RD, 6RE, 6RF, 6RG, 6RH, 6RI, 6RJ, 6RK, 6RL, 6RM, 6RN, 6RO, 6RP, 6RQ, 6RR, 6RS, 6RT, 6RU, 6RV, 6RW, 6RX, 6RY, 6RZ, 6SA, 6SB, 6SC, 6SD, 6SE, 6SF, 6SG, 6SH, 6SI, 6SJ, 6SK, 6SL, 6SM, 6SN, 6SO, 6SP, 6SQ, 6SR, 6SS, 6ST, 6SU, 6SV, 6SW, 6SX, 6SY, 6SZ, 6TA, 6TB, 6TC, 6TD, 6TE, 6TF, 6TG, 6TH, 6TI, 6TJ, 6TK, 6TL, 6TM, 6TN, 6TO, 6TP, 6TQ, 6TR, 6TS, 6TT, 6TU, 6TV, 6TW, 6TX, 6TY, 6TZ, 6UA, 6UB, 6UC, 6UD, 6UE, 6UF, 6UG, 6UH, 6UI, 6UJ, 6UK, 6UL, 6UM, 6UN, 6UO, 6UP, 6UQ, 6UR, 6US, 6UT, 6UU, 6UV, 6UW, 6UX, 6UY, 6UZ, 6VA, 6VB, 6VC, 6VD, 6VE, 6VF, 6VG, 6VH, 6VI, 6VJ, 6VK, 6VL, 6VM, 6VN, 6VO, 6VP, 6VQ, 6VR, 6VS, 6VT, 6VU, 6VV, 6VW, 6VX, 6VY, 6VZ, 6WA, 6WB, 6WC, 6WD, 6WE, 6WF, 6WG, 6WH, 6WI, 6WJ, 6WK, 6WL, 6WM, 6WN, 6WO, 6WP, 6WQ, 6WR, 6WS, 6WT, 6WU, 6WV, 6WW, 6WX, 6WY, 6WZ, 6XA, 6XB, 6XC, 6XD, 6XE, 6XF, 6XG, 6XH, 6XI, 6XJ, 6XK, 6XL, 6XM, 6XN, 6XO, 6XP, 6XQ, 6XR, 6XS, 6XT, 6XU, 6XV, 6XW, 6XX, 6XY, 6XZ, 6YA, 6YB, 6YC, 6YD, 6YE, 6YF, 6YG, 6YH, 6YI, 6YJ, 6YK, 6YL, 6YM, 6YN, 6YO, 6YP, 6YQ, 6YR, 6YS, 6YT, 6YU, 6YV, 6YW, 6YX, 6YY, 6YZ, 6ZA, 6ZB, 6ZC, 6ZD, 6ZE, 6ZF, 6ZG, 6ZH, 6ZI, 6ZJ, 6ZK, 6ZL, 6ZM, 6ZN, 6ZO, 6ZP, 6ZQ, 6ZR, 6ZS, 6ZT, 6ZU, 6ZV, 6ZW, 6ZX, 6ZY, 6ZZ
- 3. POST DESIGN**
POSTS SHALL BE VERIFIED AND APPROVED BY THE ENGINEER PRIOR TO FABRICATION. WHERE FIELD CONDITIONS REQUIRE THE POST LENGTH TO VARY MORE THAN 12", IT MAY BE NECESSARY TO CHANGE THE SIZE OR NUMBER OF POSTS. SUCH DETERMINATION WILL BE MADE BY THE ROADWAY DESIGN ENGINEER. ANY CHANGE OF SIZE OR NUMBER OF POSTS SHALL NOT BE JUSTIFICATION FOR ANY CONTRACT PRICE ADJUSTMENTS.
- 4. FABRICATOR NOTE**
IMPORTANT- ALL FRICTION FUSE BOLTS SHALL BE TIGHTENED IN SHOP BY A METHOD APPROVED BY THE BRIDGE DESIGN ENGINEER. TIGHTENING SHALL BE TO SUCH A DEGREE AS TO PROVIDE THE MINIMUM TENSION IN EACH BOLT WHEN ALL BOLTS ARE TIGHT, AS SHOWN BY TABLE SH-6A.
- 5. ALL HOLES IN FUSE PLATES AND HINGE PLATES SHALL BE DRILLED.**
- 6. ALL PLATE CUTS SHALL PREFERABLY BE SAW CUTS. FLAME CUTTING WILL BE PERMITTED PROVIDED ALL EDGES ARE GRIND, METAL PROTECTING BEYOND THE PLANE OF THE PLATE FACE WILL NOT BE ACCEPTABLE.**
- 7. WELDING FOR STEEL SIGN SUBORDINATES**
WELDING SHALL BE PERFORMED IN SHOP BY ELECTRIC ARC PROCESS.
- 8. MATERIAL SPECIFICATIONS**
THE MATERIALS USED IN THE CONSTRUCTION OF THE GROUND MOUNTED SIGN SUPPORT STRUCTURES, AS LISTED BELOW, SHALL BE OF THE QUALITY AND MANUFACTURE AS SPECIFIED AND SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE SPECIFICATIONS, EXCEPT AS OTHERWISE NOTED ON THE PLANS.



TYPICAL 2-POST INSTALLATION WITHOUT EXIT SIGN

TYPICAL 2 OR 3 POST INSTALLATION WITH SUPPLEMENTARY GUIDE SIGN AND POST BRACING ANGLE LOCATION

* NOTE: PLACE POST BRACING ANGLE AT BOTTOM OF PRIMARY OR SUPPLEMENTARY SIGN WHICHEVER IS LOWER. SEE "END ELEVATION OF POST AND FOOTING ON SH-6(A)

2 POST	2 POST (CONT'D)		3 POST	
	10'	15'	10'	15'
4'-0"	1'-0"	1'-0"	1'-0"	1'-0"
4'-5"	1'-0"	1'-0"	1'-0"	1'-0"
5'-0"	1'-0"	1'-0"	1'-0"	1'-0"
5'-5"	1'-0"	1'-0"	1'-0"	1'-0"
6'-0"	1'-0"	1'-0"	1'-0"	1'-0"
6'-5"	1'-0"	1'-0"	1'-0"	1'-0"
7'-0"	1'-0"	1'-0"	1'-0"	1'-0"
7'-5"	1'-0"	1'-0"	1'-0"	1'-0"
8'-0"	1'-0"	1'-0"	1'-0"	1'-0"
8'-5"	1'-0"	1'-0"	1'-0"	1'-0"
9'-0"	1'-0"	1'-0"	1'-0"	1'-0"
9'-5"	1'-0"	1'-0"	1'-0"	1'-0"
10'-0"	1'-0"	1'-0"	1'-0"	1'-0"
10'-5"	1'-0"	1'-0"	1'-0"	1'-0"
11'-0"	1'-0"	1'-0"	1'-0"	1'-0"
11'-5"	1'-0"	1'-0"	1'-0"	1'-0"
12'-0"	1'-0"	1'-0"	1'-0"	1'-0"
12'-5"	1'-0"	1'-0"	1'-0"	1'-0"
13'-0"	1'-0"	1'-0"	1'-0"	1'-0"
13'-5"	1'-0"	1'-0"	1'-0"	1'-0"
14'-0"	1'-0"	1'-0"	1'-0"	1'-0"
14'-5"	1'-0"	1'-0"	1'-0"	1'-0"
15'-0"	1'-0"	1'-0"	1'-0"	1'-0"
15'-5"	1'-0"	1'-0"	1'-0"	1'-0"
16'-0"	1'-0"	1'-0"	1'-0"	1'-0"
16'-5"	1'-0"	1'-0"	1'-0"	1'-0"
17'-0"	1'-0"	1'-0"	1'-0"	1'-0"
17'-5"	1'-0"	1'-0"	1'-0"	1'-0"
18'-0"	1'-0"	1'-0"	1'-0"	1'-0"
18'-5"	1'-0"	1'-0"	1'-0"	1'-0"
19'-0"	1'-0"	1'-0"	1'-0"	1'-0"
19'-5"	1'-0"	1'-0"	1'-0"	1'-0"
20'-0"	1'-0"	1'-0"	1'-0"	1'-0"
20'-5"	1'-0"	1'-0"	1'-0"	1'-0"
21'-0"	1'-0"	1'-0"	1'-0"	1'-0"
21'-5"	1'-0"	1'-0"	1'-0"	1'-0"
22'-0"	1'-0"	1'-0"	1'-0"	1'-0"
22'-5"	1'-0"	1'-0"	1'-0"	1'-0"
23'-0"	1'-0"	1'-0"	1'-0"	1'-0"
23'-5"	1'-0"	1'-0"	1'-0"	1'-0"
24'-0"	1'-0"	1'-0"	1'-0"	1'-0"
24'-5"	1'-0"	1'-0"	1'-0"	1'-0"
25'-0"	1'-0"	1'-0"	1'-0"	1'-0"
25'-5"	1'-0"	1'-0"	1'-0"	1'-0"
26'-0"	1'-0"	1'-0"	1'-0"	1'-0"
26'-5"	1'-0"	1'-0"	1'-0"	1'-0"
27'-0"	1'-0"	1'-0"	1'-0"	1'-0"
27'-5"	1'-0"	1'-0"	1'-0"	1'-0"
28'-0"	1'-0"	1'-0"	1'-0"	1'-0"
28'-5"	1'-0"	1'-0"	1'-0"	1'-0"
29'-0"	1'-0"	1'-0"	1'-0"	1'-0"
29'-5"	1'-0"	1'-0"	1'-0"	1'-0"
30'-0"	1'-0"	1'-0"	1'-0"	1'-0"
30'-5"	1'-0"	1'-0"	1'-0"	1'-0"

DESCRIPTION	MATERIALS PER ASTM DESIGNATION	GALVANIZE PER ASTM DESIGNATION
ROSTE OF STEEL PIPE	A 53 GR. B (1)	A 123
BASE CONNECTION PLATES FOR PIPES	A 36	A 123
POSTS OF STEEL W. B. AND I BEAMS INCLUDING BASE CONNECTION, FUSE AND POST BRACING ANGLES AND FLAT BARS USED IN FABRICATION AND ERECTION OF SIGN SUPPORTS	A 572 GR. 50	A 123
HIGH STRENGTH BOLTS, NUTS AND WASHERS (EXCEPT THOSE SPECIFIED OTHERWISE)	A 307	A 153
	A 307 GR. B (1)	A 153

- ① ALL STEEL SHALL BE GALVANIZED AFTER FABRICATION EXCEPT AS NOTED ON THE PLANS.
- ② PIPES MAY BE WELDED OR SEAMLESS.
- ③ BOLTS, WASHERS, AND NUTS USED FOR FASTENING ALUMINUM SIGN SHEETS AND PANELS SHALL BE ALUMINUM AS PER FOLLOWING TABLE.

DESCRIPTION	ASTM	ALLOY
BOLTS AND WASHERS	B 209	2024-T3
NUTS	B 211	2024-T3
STOP NUTS	B 211	2024-T3

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

BREAKAWAY SIGN SUPPORTS

REVISION

ISSUE DATE: MAY 01, 2017

SHEET NUMBER 6310



STATE PROJECT NO.
MISS.

MDOT
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN
**BREAKAWAY
SIGN SUPPORTS**
SN-6A
SHEET NUMBER 6311
ISSUE DATE: MAY 01, 2017

BREAKAWAY SUPPORT STUD CLEARANCE DIAGRAM

••• STUD OF BREAKAWAY SUPPORT SHALL BE 5'-0" LONG

BASE CONNECTION DETAILS FOR W SHAPED POSTS

REMOVE ALL GALVANIZING FROM CONTACT AREA OF WELDS

WELDS TO BE MADE WITH 1/2" E60XX-XF ELECTRODE

WELDED WASHER ON SUPPLEMENTARY SIGN (WHENEVER IS LOREN)

••• SEE TABLE FOR DIMENSIONS

BASE CONNECTION DETAILS FOR F SHAPED POSTS

REMOVE ALL GALVANIZING FROM CONTACT AREA OF WELDS

WELDS TO BE MADE WITH 1/2" E60XX-XF ELECTRODE

WELDED WASHER ON SUPPLEMENTARY SIGN (WHENEVER IS LOREN)

••• SEE TABLE FOR DIMENSIONS

HINGE PLATE DETAIL

NOTE: INSTALLATION SIMILAR TO DETAIL "A" USE PLATE.

••• SEE TABLE FOR DIMENSIONS

FUSE PLATE DETAIL

NOTE: SEE TABLE FOR DIMENSIONS. USE A53 BOLTS WITH FLAT HEAD AND LEVEL OR FLAT HEADERS WHERE REQUIRED UNDER NUTS.

FLANGE WELDS FOR HINGE SHALL BE CHILLED OR SUPPLEMENTED WITH NOTCHES (SEE TYPICAL DRAWING) AND REAMER.

••• SEE TABLE FOR DIMENSIONS

STIFFENER PLATE DETAIL

NOTE: SEE TABLE FOR DIMENSIONS

••• SEE TABLE FOR DIMENSIONS

DIAGRAM OF "BOLT KEEPER"

NOTE: FABRICATED FROM GALVANIZED STEEL. REQUIRED AT REAR OF BASE X AND STUB POST. DIMENSIONED AS REQUIRED FOR EACH INSTALLATION.

••• SEE TABLE FOR DIMENSIONS

SHIM DETAIL

NOTE: FABRICATED FROM GALVANIZED STEEL. REQUIRED AT REAR OF BASE X AND STUB POST. DIMENSIONED AS REQUIRED FOR EACH INSTALLATION.

••• SEE TABLE FOR DIMENSIONS

SECTION A-A

GENERAL NOTE:
1. SEE SH-4 FOR GENERAL NOTES.

SECTION B-B

FOOTING DEPTH (AS REQUIRED) (ELEMENTS ON PLANS)

FOOTING DIAMETER (SEE TABLE FOR SIZES)

BASE (AS SHOWN) (SEE TABLE FOR SIZES)

POST (SEE TABLE FOR SIZES)

CONCRETE (AS SHOWN) (SEE FOOTING NOTE)

ON SHIP

END ELEVATION OF POST AND FOOTING

SECTION E-E

NOTE: SEE TABLE FOR DIMENSIONS

SECTION F-F

NOTE: SEE TABLE FOR DIMENSIONS

POST BRACING DATA

POST SIZE	W8 X 26	W8 X 18	W8 X 15	W6 X 12
DESCRIPTION	W8 X 26	W8 X 18	W8 X 15	W6 X 12
BOLT SIZE	3/4"	3/4"	3/4"	3/4"
ANGLE LENGTH	2' x 9"	2' x 8"	2' x 8"	2' x 8"
ANGLE SIZE	L4" x 4" x 3/8"	L3 1/2" x 3 1/2" x 1/4"	L3 1/2" x 3 1/2" x 1/4"	L4" x 4" x 3/8"
SPACING OF	11'-0"	14' x 4" x 3/8"	14' x 4" x 3/8"	14' x 4" x 3/8"

NOTE: Z = DISTANCE BETWEEN EYES OF EXTERIOR POSTS.

FOOTING DATA

FOOTING DEPTH	FOOTING DIAMETER	FOOTING BARS V SIZE
4'-0"	2'-0"	#4
4'-0"	2'-0"	#5
4'-0"	2'-0"	#6
4'-0"	2'-0"	#7
4'-0"	2'-0"	#8
4'-0"	2'-0"	#9
4'-0"	2'-0"	#10
4'-0"	2'-0"	#11

••• NOTE: FOOTING DEPTH TO BE SHOWN ELSEWHERE IN THE PLANS.

FUSE PLATE DATA

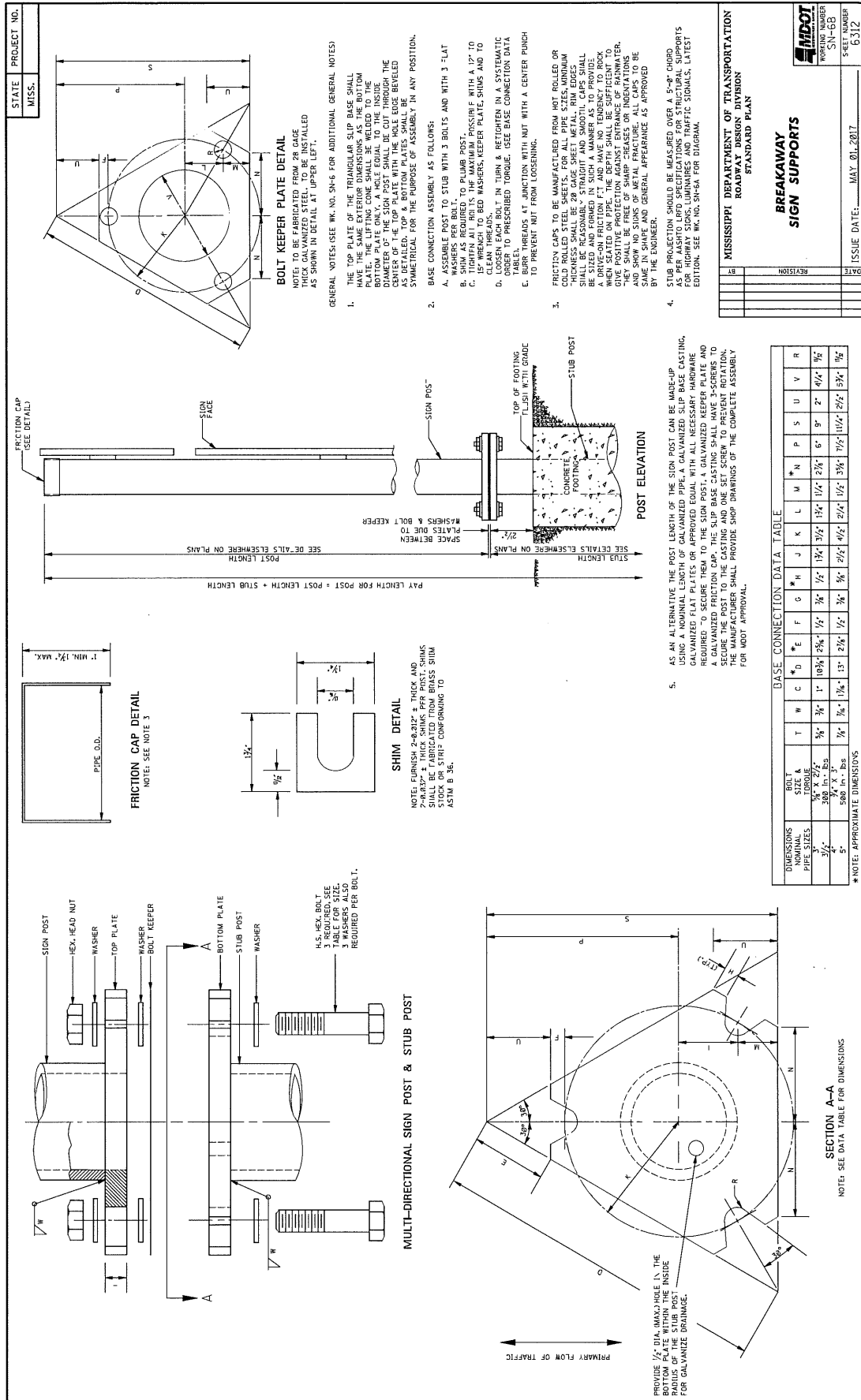
MIN. BOLT TENSION	I _x	L	N	D ₁	D ₂	BOLT SIZE (DIA. X LENGTH)	STUB LENGTH	STUB DIAMETER
12,050	1/2"	1/2"	1/2"	1/2"	1/2"	3/4" x 2'-0"	2'-0"	3"
15,200	3/4"	3/4"	3/4"	3/4"	3/4"	3/4" x 2'-0"	2'-0"	3"
25,400	1"	1"	1"	1"	1"	3/4" x 2'-0"	2'-0"	3"
25,400	1 1/4"	1 1/4"	1 1/4"	1 1/4"	1 1/4"	3/4" x 2'-0"	2'-0"	3"
38,250	1 1/2"	1 1/2"	1 1/2"	1 1/2"	1 1/2"	3/4" x 2'-0"	2'-0"	3"
38,250	1 3/4"	1 3/4"	1 3/4"	1 3/4"	1 3/4"	3/4" x 2'-0"	2'-0"	3"

••• NOTE: FOOTING DEPTH TO BE SHOWN ELSEWHERE IN THE PLANS.

BASE CONNECTION DATA

POST SIZE	A	B	C	D	E	T ₁	T ₂	W	R	F	G	H	I	J	K	L	N	D ₁	D ₂	BOLT SIZE (DIA. X LENGTH)	MIN. BOLT TENSION	STUB LENGTH	STUB DIAMETER	FOOTING DEPTH
W8 X 26	5"	2"	1 1/4"	2 1/4"	1 1/4"	3/4"	1/2"	3/4"	1/2"	3/4"	2"	1 1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	3/4" x 2'-0"	12,050	2'-0"	3"	4'-0"
W8 X 18	5"	2"	1 1/4"	2 1/4"	1 1/4"	3/4"	1/2"	3/4"	1/2"	3/4"	2"	1 1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	3/4" x 2'-0"	15,200	2'-0"	3"	4'-0"
W8 X 15	5"	2"	1 1/4"	2 1/4"	1 1/4"	3/4"	1/2"	3/4"	1/2"	3/4"	2"	1 1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	3/4" x 2'-0"	25,400	2'-0"	3"	4'-0"
W6 X 12	5"	2"	1 1/4"	2 1/4"	1 1/4"	3/4"	1/2"	3/4"	1/2"	3/4"	2"	1 1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	3/4" x 2'-0"	25,400	2'-0"	3"	4'-0"
W6 X 12	5"	2"	1 1/4"	2 1/4"	1 1/4"	3/4"	1/2"	3/4"	1/2"	3/4"	2"	1 1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	3/4" x 2'-0"	38,250	2'-0"	3"	4'-0"
W6 X 12	5"	2"	1 1/4"	2 1/4"	1 1/4"	3/4"	1/2"	3/4"	1/2"	3/4"	2"	1 1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	1/2"	3/4" x 2'-0"	38,250	2'-0"	3"	4'-0"

••• NOTE: FOOTING DEPTH TO BE SHOWN ELSEWHERE IN THE PLANS.



STATE MISS. PROJECT NO.

ISSUE DATE: MAY 01, 2017

6312

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**BREAKAWAY
SIGN SUPPORTS**

MDOT
DESIGN NUMBER
SN-66
SHEET NUMBER

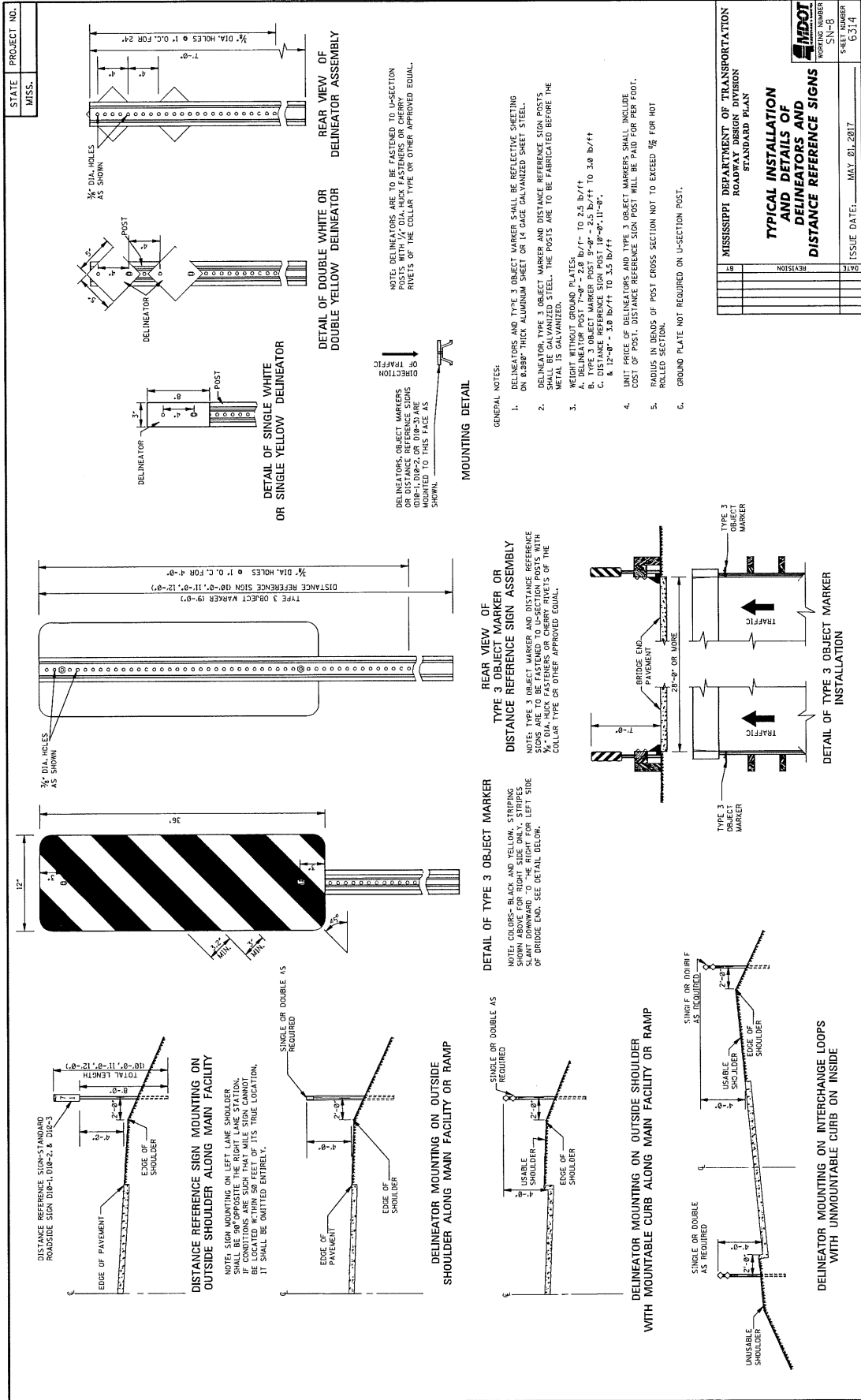
- GENERAL NOTES: (SEE MK. NO. SN-6 FOR ADDITIONAL GENERAL NOTES)
- THE TOP PLATE OF THE TRIANGULAR SLIP BASE SHALL HAVE THE SAME EXTERIOR DIMENSIONS AS THE BOTTOM PLATE. THE HOLE IN THE TOP PLATE SHALL BE THE SAME AS THE HOLE IN THE BOTTOM PLATE ONLY A HOLE EQUAL TO THE INSIDE DIAMETER OF THE SIGN POST SHALL BE CUT THROUGH THE CENTER OF THE TOP PLATE WITH THE HOLE EDGE BEVELLED TO 30°. THE SIGN POST SHALL BE CENTERED AND SYMMETRICAL FOR THE PURPOSE OF ASSEMBLY IN ANY POSITION.
 - BASE CONNECTION ASSEMBLY AS FOLLOWS:
 - ASSEMBLE POST TO STUB WITH 3 BOLTS & WITH 3 1/2" SHIMS PER BOLTS.
 - SHOW AS REQUIRED TO PLUMB POST.
 - TIGHTEN ALL BOLTS THE MAXIMUM PROSICRIF WITH A 1/2" TORQUE WRENCH. CHECK FOR CLEAN THREADS AND TO CLEAN THREADS.
 - LOOSEN EACH BOLT IN TURN & RETIGHTEN IN A SYSTEMATIC MANNER TO PRESCRIBED TORQUE. USE BASE CONNECTION DATA TABLE.
 - BURR THREADS AT JUNCTION WITH NUT WITH A CENTER PUNCH TO PREVENT NUT FROM LOOSENING.
 - FRICION CAPS TO BE MANUFACTURED FROM HOT ROLLED OR COLD ROLLED STEEL SHEETS. FOR ALL PIPE SIZES, MINIMUM THICKNESS SHALL BE 2# GAGE SHEET METAL. RIM EDGES SHALL BE BEVELLED TO 30°. CAPS SHALL BE MANUFACTURED TO BE SIZED AND FORMED IN SUCH A MANNER AS TO PROVIDE:
 - A DRIVE-ON FRICION FIT AND HAVE NO TENDENCY TO ROCK WHEN POSTING.
 - WHEN POSTING PROTECTION AGAINST ENTRANCE OF RAINWATER. THEY SHALL BE FREE OF SHARP CREASES OR INDENTATIONS AND SHOW NO SIGNS OF METAL FRACTURE. ALL CAPS TO BE MANUFACTURED IN GENERAL APPEARANCE AS APPROVED BY THE ENGINEER.
 - STUB PROJECTION SHOULD BE MEASURED OVER A 5-#4 GARD AS PER AASHTO LRFD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS; LATEST EDITION. SEE MK. NO. SN-6 FOR DIAGRAM.

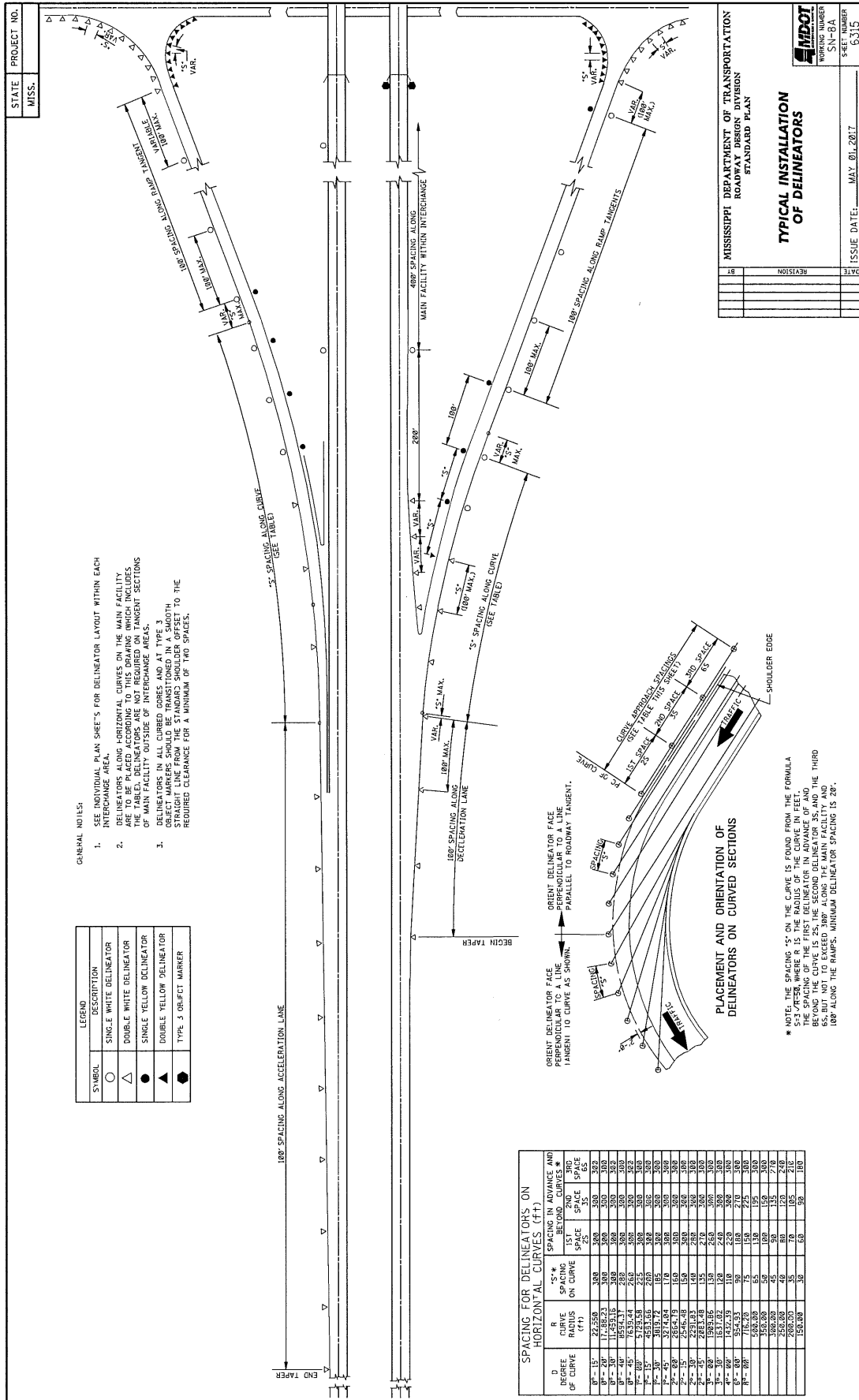
SECTION A-A

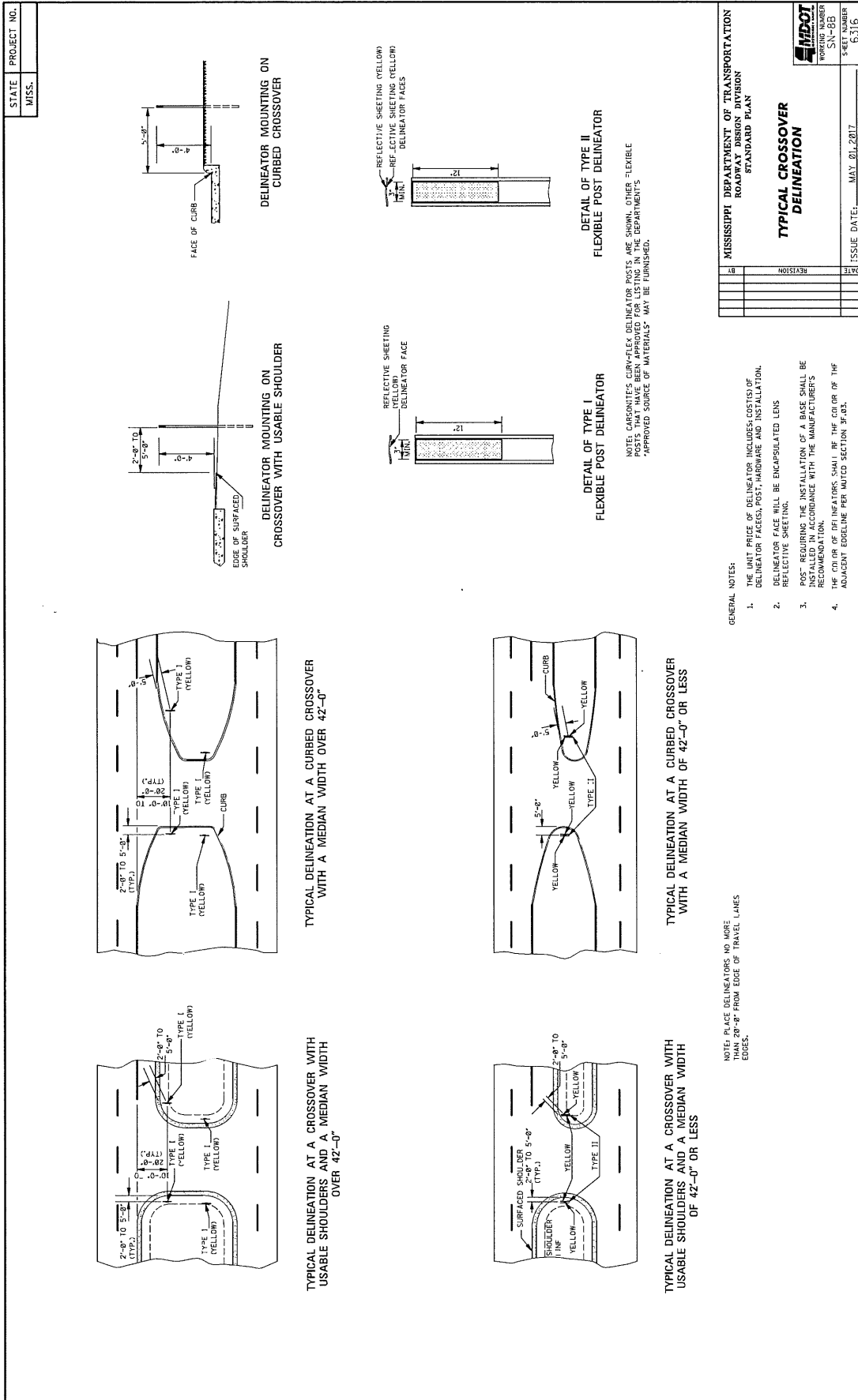
NOTE: SEE DATA TABLE FOR DIMENSIONS

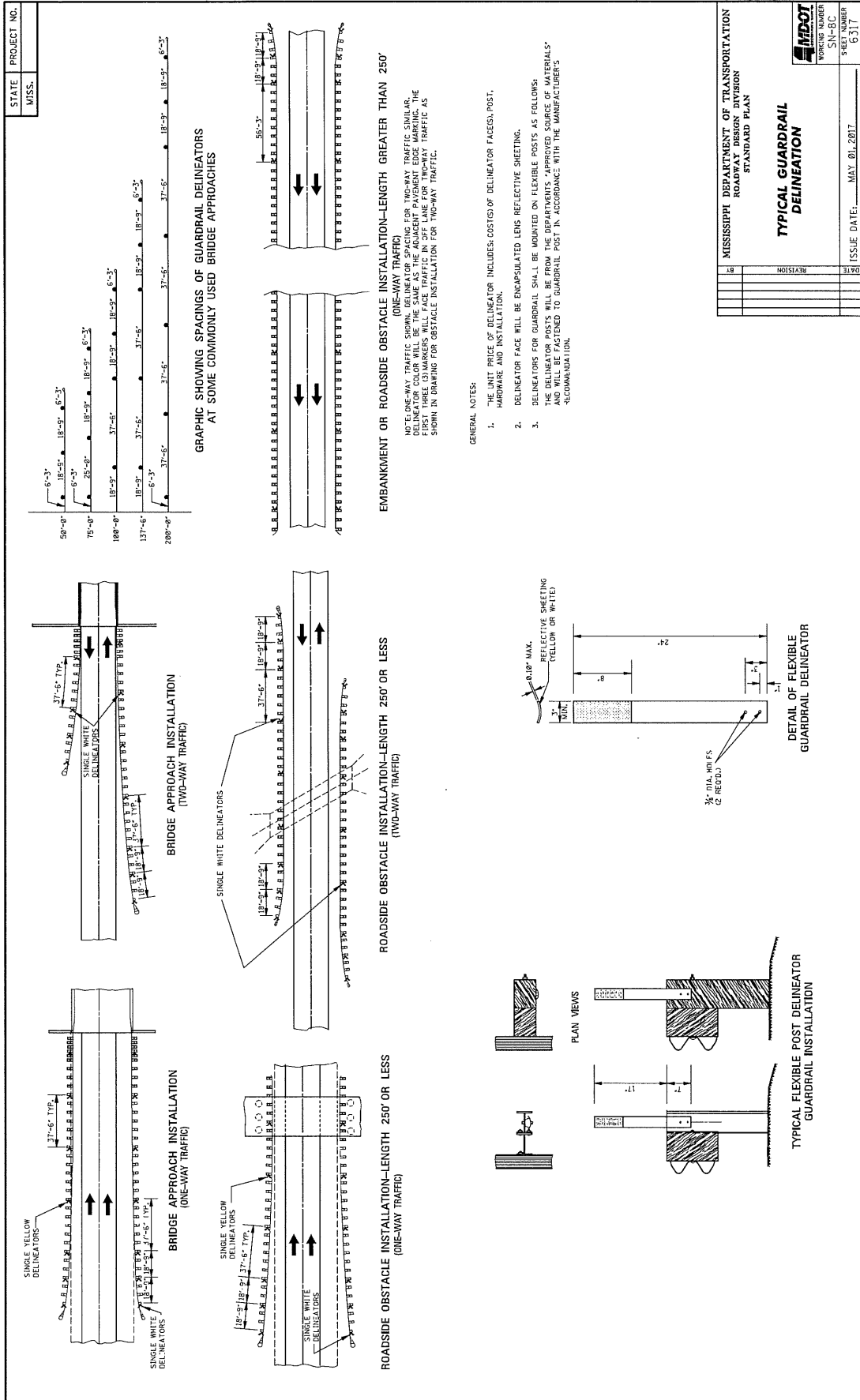
BASE CONNECTION DATA TABLE

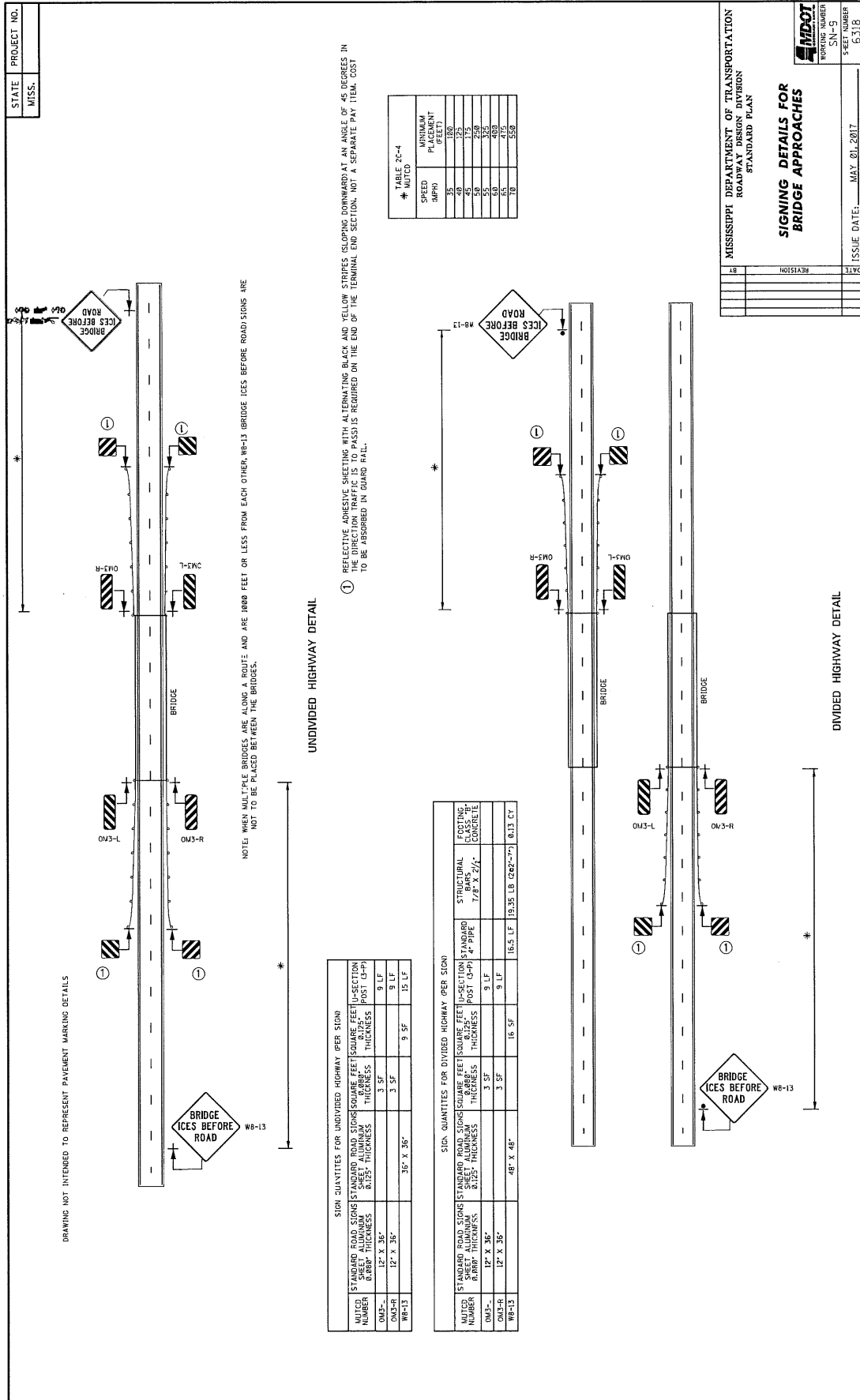
BOLT TORQUE	T	W	C	D	E	F	G	H	J	K	L	M	N	P	S	U	V	R	
300 in.-lbs	3/4"	1"	10/8"	2 3/4"	1/4"	3/4"	1 1/4"	1 1/2"	1 3/4"	2 1/4"	2 1/2"	1 1/4"	1 1/4"	2 3/4"	6"	9"	2"	4 1/2"	1/2"
500 in.-lbs	7/8"	1 1/4"	1 1/4"	1 3/4"	2 1/4"	3/4"	1 3/4"	2 1/4"	2 3/4"	3 1/4"	3 1/2"	2 1/4"	2 1/4"	3 1/4"	7 1/2"	10 1/4"	2 1/2"	5 3/4"	1/2"











MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 – NOTICE TO BIDDERS NO. 3599

CODE: (SP)

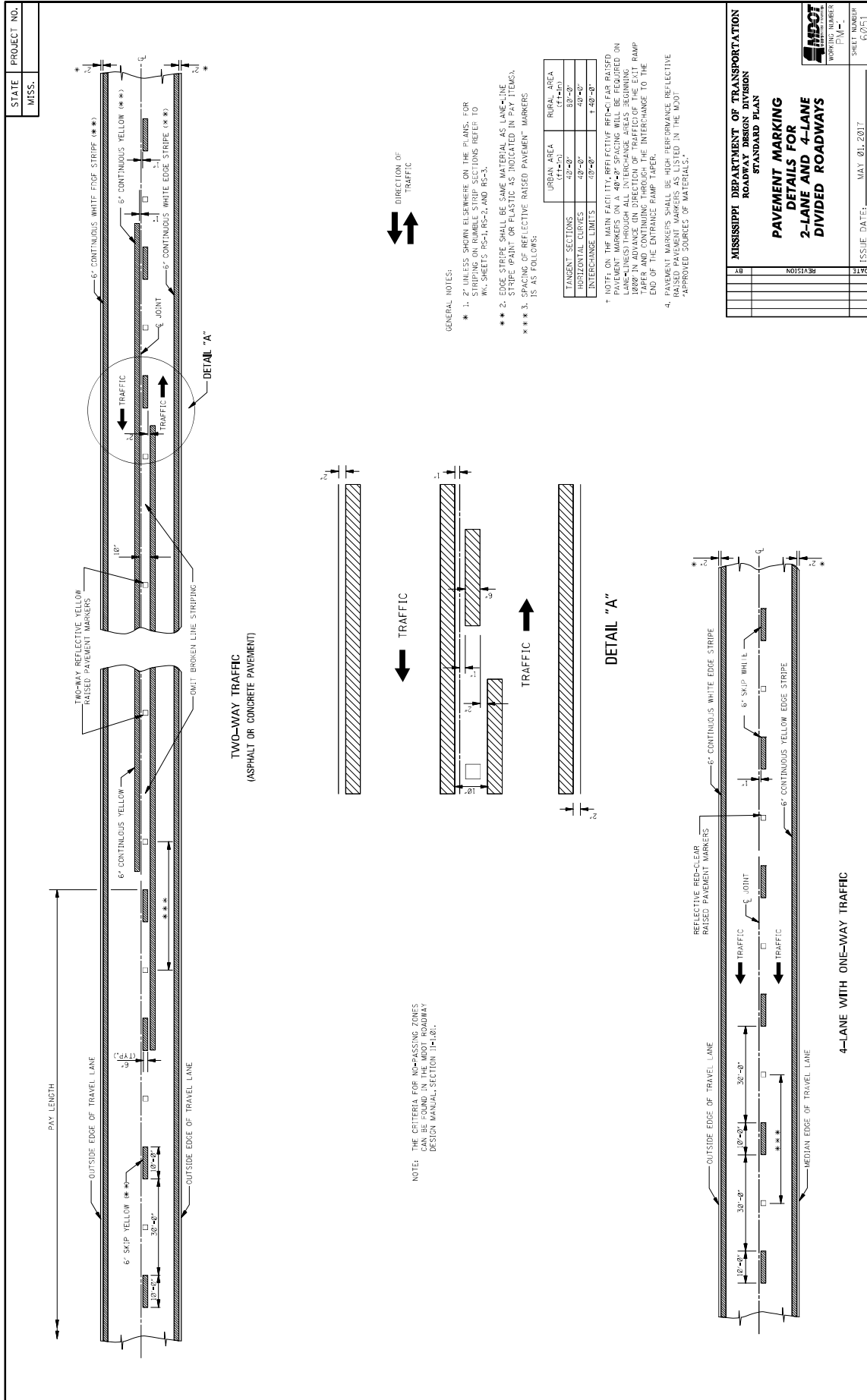
DATE: 08/11/2021

SUBJECT: Standard Drawings

Standard Drawings attached hereto shall govern appropriate items of required work.

Larger copies of Standard Drawings may be purchased from:

MDOT Plans Print Shop
MDOT Shop Complex, Building C, Room 114
2567 North West Street
P.O. Box 1850
Jackson, MS 39215-1850
Telephone: (601) 359-7460
or FAX: (601) 359-7461
or e-mail: plans@mdot.state.ms.us



MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY DESIGN DIVISION	
STANDARD PLAN	
PAVEMENT MARKING	
DETAILS FOR	
2-LANE AND 4-LANE	
DIVIDED ROADWAYS	
DATE	ISSUE DATE: MAY 01, 2017
BY	SHEET NUMBER 6001
REVISION	WORKING NUMBER P.M.-

STATE MISS.	PROJECT NO.	
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TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 3-LANE SECTION

TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION

DETAIL A
LATERAL PLACEMENT OF PAVEMENT MARKERS

DETAIL B
LATERAL PLACEMENT OF PAVEMENT MARKERS

TYPICAL TWO-WAY ARROW INSTALLATION

TYPICAL STRIPING AND RAISED PAVEMENT MARKERS AT LEFT TURN LANES

* NOTE: USE DETAIL STRIPING IF LENGTH < 150' AT THIS LOCATION, OTHERWISE USE CONTINUOUS STRIPING.

TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 3-LANE SECTION

NOTES: 1. CONSIDER EACH SEGMENT OF CONTINUOUS TWO-WAY LEFT TURN LANE SEPARATELY; 2. IF SEGMENT IS SHORTER THAN 150 FEET, USE DETAIL STRIPING; 3. IF SEGMENT IS LONGER THAN 150 FEET, USE CONTINUOUS STRIPING FROM BEGINNING AND/OR END OF SEGMENT AND SPACE ADDITIONAL SETS OF ARROWS (250' O.C.).

TYPICAL STRIPING AND RAISED PAVEMENT MARKERS FOR 4-LANE SECTION

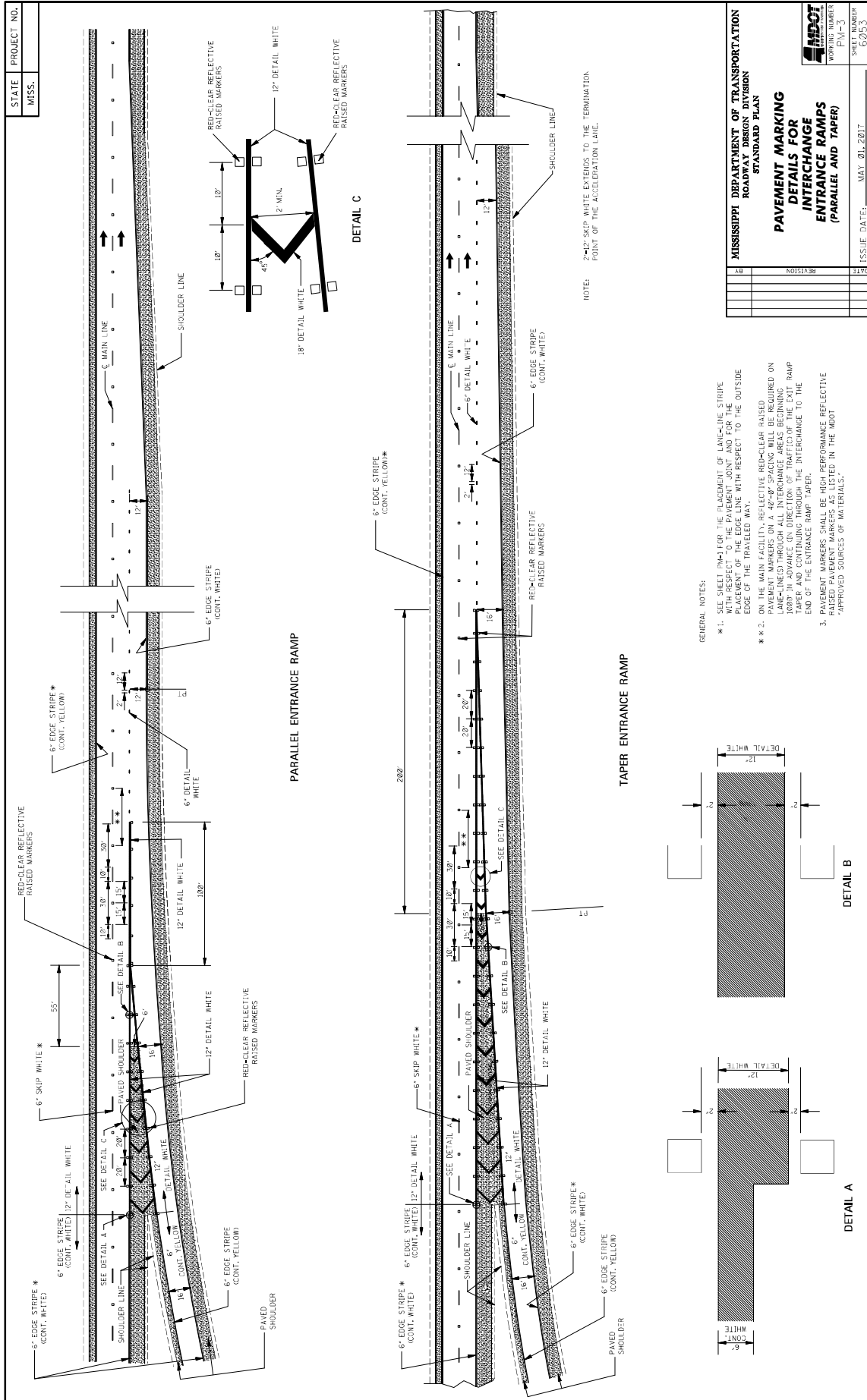
GENERAL NOTE:
1. PAVEMENT MARKERS SHALL BE HIGH PERFORMANCE RAISED PAVEMENT MARKERS AS LISTED IN THE MOST APPROVED SOURCE OF MATERIALS.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

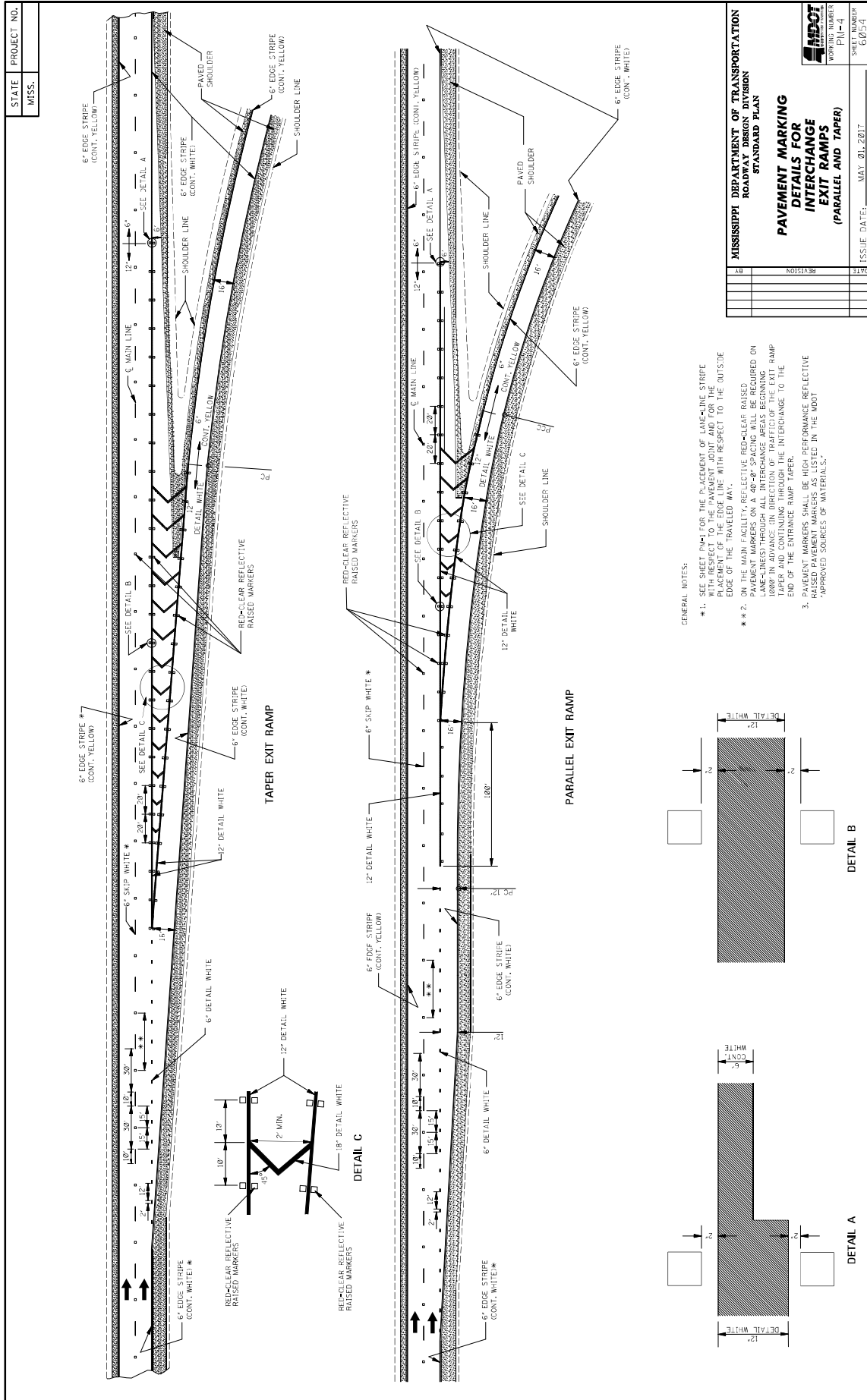
PAVEMENT MARKING
DETAILS FOR 3-LANE
4-LANE AND 5-LANE
UNDIVIDED ROADWAYS

DATE	REVISION	ISSUE DATE: MAY 01, 2017

SHEET NUMBER: 10-2
PROJECT NUMBER: 3599



MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION	
BY	REVISION
DATE	
PAVEMENT MARKING DETAILS FOR INTERCHANGE ENTRANCE RAMP (PARALLEL AND TAPER)	
SHEET NUMBER	FM-3
SHEET TOTAL	60/53
ISSUE DATE:	MAY 20, 2017



- GENERAL NOTES:
- * 1. SEE SHEET PAV-1 FOR THE PLACEMENT OF LANE-LINE STRIPE WITH RESPECT TO THE PAVEMENT JOINT AND FOR THE PLACE OF THE PAVED SHOULDER WITH RESPECT TO THE OUTSIDE EDGE OF THE PAVED SHOULDER.
 - ** 2. RAISED PAVEMENT MARKERS (REFLECTIVE BEAD-BLE BLENDED PAVEMENT MARKERS ON A 40-60" SPACING) WILL BE REQUIRED ON LANE-LINES THROUGH ALL INTERCHANGE AREAS BEGINNING 1000' IN ADVANCE IN DIRECTION OF TRAFFIC OF THE EXIT RAMP TAPER AND CONTINUING THROUGH THE INTERCHANGE TO THE END OF THE ENTRANCE RAMP TAPER.
 3. RAISED PAVEMENT MARKERS BE HIGH PERFORMANCE REFLECTIVE "APPROVED SOURCES OF MATERIALS."

MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
ROADWAY DESIGN DIVISION	
STANDARD PLAN	
PAVEMENT MARKING	
INTERCHANGE	
EXIT RAMP	
(PARALLEL AND TAPER)	
SHEET NUMBER	ISSUE DATE: MAY 01, 2017
PL-4	
WORKING NUMBER	
67294	
DATE	REVISION

STATE MISS.	PROJECT NO.										
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PAVE MARKING
8'-4" x 6'-4"

TRAFFIC
8'-4" x 6'-4"

STOP
8'-4" x 6'-4"

SIGNAL
8'-4" x 6'-4"

EXIT
8'-4" x 5'-4"

RIGHT
8'-4" x 7'-0"

YIELD
8'-4" x 7'-0"

AHEAD
8'-4" x 8'-0"

SCHOOL
8'-4" x 9'-8"

GENERAL NOTES:

- TWO HORIZONTAL GAPS (USED BY TEMPLATE CONNECTIONS) OF 1/4" SHALL BE SHOWN EXTENDING FULL WIDTH OF RESPECTIVE LETTER.
- FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

LEGEND	AREA (FT ²)
STOP	24.6
RIGHT	28.6
LEFT	19.5
YIELD	27.2
AHEAD	32.4
YIELD	26.8
EXIT	18.5
SIGNAL	32.5
SCHOOL	35.2

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**PAVEMENT MARKING
LEGEND DETAILS**

DATE	ISSUE DATE: MAY 01, 2017
BY	
REVISION	

WORKING NUMBER
PM-5

SHEET NUMBER
6035

STATE MISS.	PROJECT NO.		
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ONLY

TURN ARROW

THRU ARROW

LANE-REDUCTION ARROW

COMBINATION ARROW

1-WAY ARROW

YIELD LINE

1-WAY ARROW

GENERAL NOTES:

- TWO HORIZONTAL GAPS (CAUSED BY TEMPLATE CONNECTORS OF 1/4" OR LESS AND EXTENDING THE FULL WIDTH) ARE PERMITTED IN EACH LETTER.
- FOR OTHER DETAILS, SEE THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- DIMENSIONS OF THE YIELD LINE MAY VARY WITH APPROVAL OF THE ENGINEER. SEE MUTCD, LATEST EDITION, FOR ALLOWABLE DIMENSIONS.
- PAY QUANTITIES FOR PAVEMENT MARKING LEGENDS ARE AS FOLLOWS:

PAY QUANTITIES	
LEGEND/SYMBOL	AREA (ft ²)
ONLY	22.0
TURN ARROW	16.4
THRU ARROW	12.3
COMB. ARROW	27.5
LANE REDUCTION ARROW	40.0

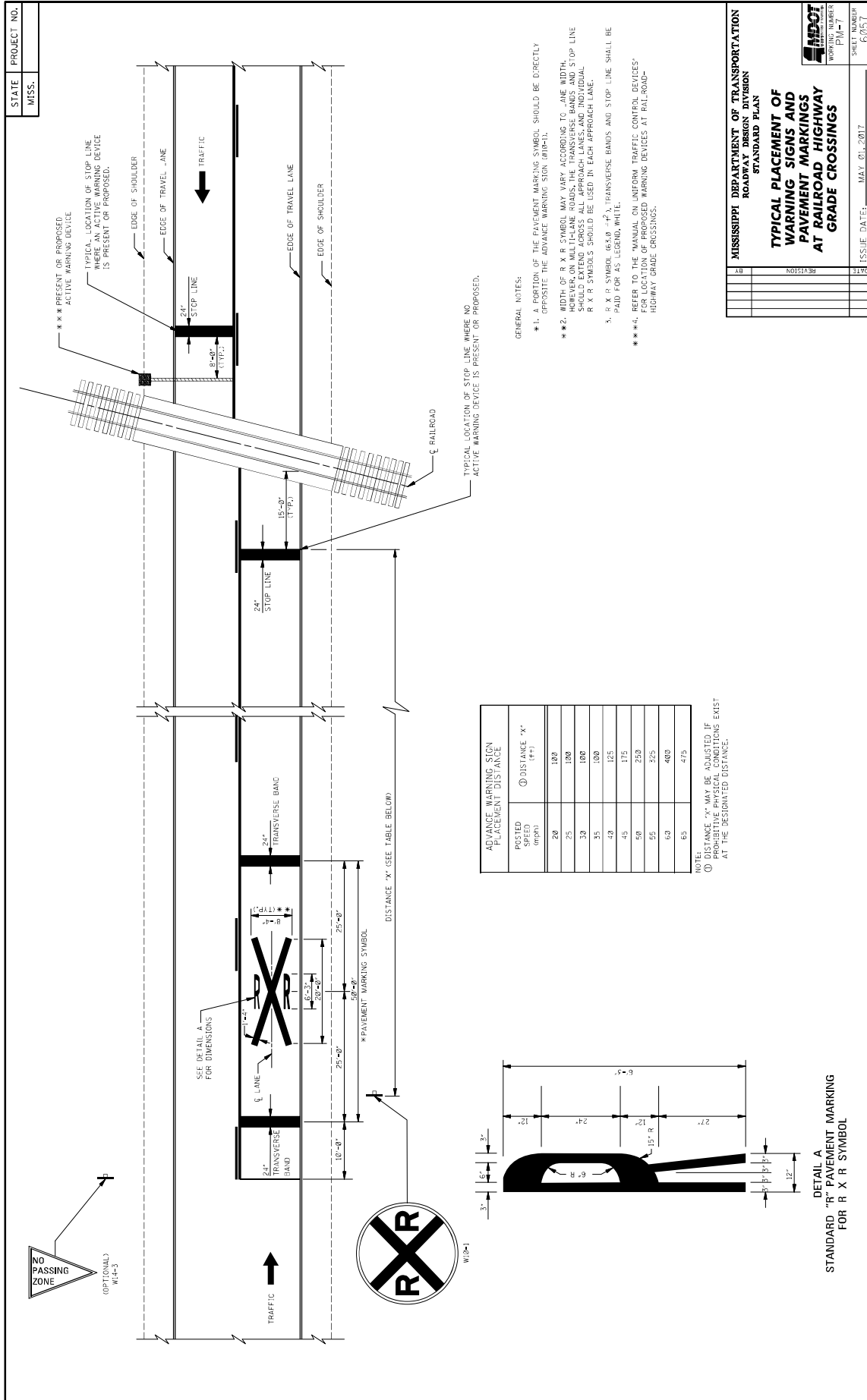
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

PAVEMENT MARKING
LEGEND DETAILS

BY	DATE
REVISION	

ISSUE DATE: MAY 01, 2017

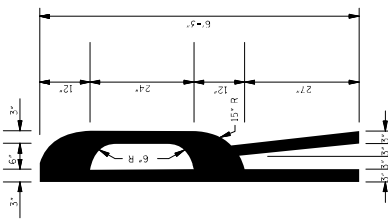
SHEET NUMBER: PM-6
SHEET TOTAL: 60/66



- GENERAL NOTES:
- *1. A PORTION OF THE PAVEMENT MARKING SYMBOL SHOULD BE DIRECTLY OPPOSITE THE ADVANCE WARNING SIGN (W10-1).
 - **2. WIDTH OF 'R' X 'R' SYMBOL MAY VARY ACCORDING TO LANE WIDTH. SYMBOL SHOULD EXTEND ACROSS ALL APPROACH LANES AND INDIVIDUAL LANE. 'R' X 'R' SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.
 - 3. 'R' X 'R' SYMBOL (65 X 8 - 1/2") TRANSVERSE BANDS AND STOP LINE SHALL BE PAID FOR AS LEGEND WHITE.
 - ***4. REFER TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR LOCATION OF PROPOSED WARNING DEVICES AT RAILROAD-HIGHWAY GRADE CROSSINGS.

POSTED SPEED (mph)	① DISTANCE 'X' (ft)
20	100
25	100
30	100
35	100
40	125
45	175
50	250
55	325
60	400
65	475

NOTE: DISTANCE 'X' MAY BE ADJUSTED IF PROHIBITIVE PHYSICAL CONDITIONS EXIST AT THE DESIGNATED DISTANCE.



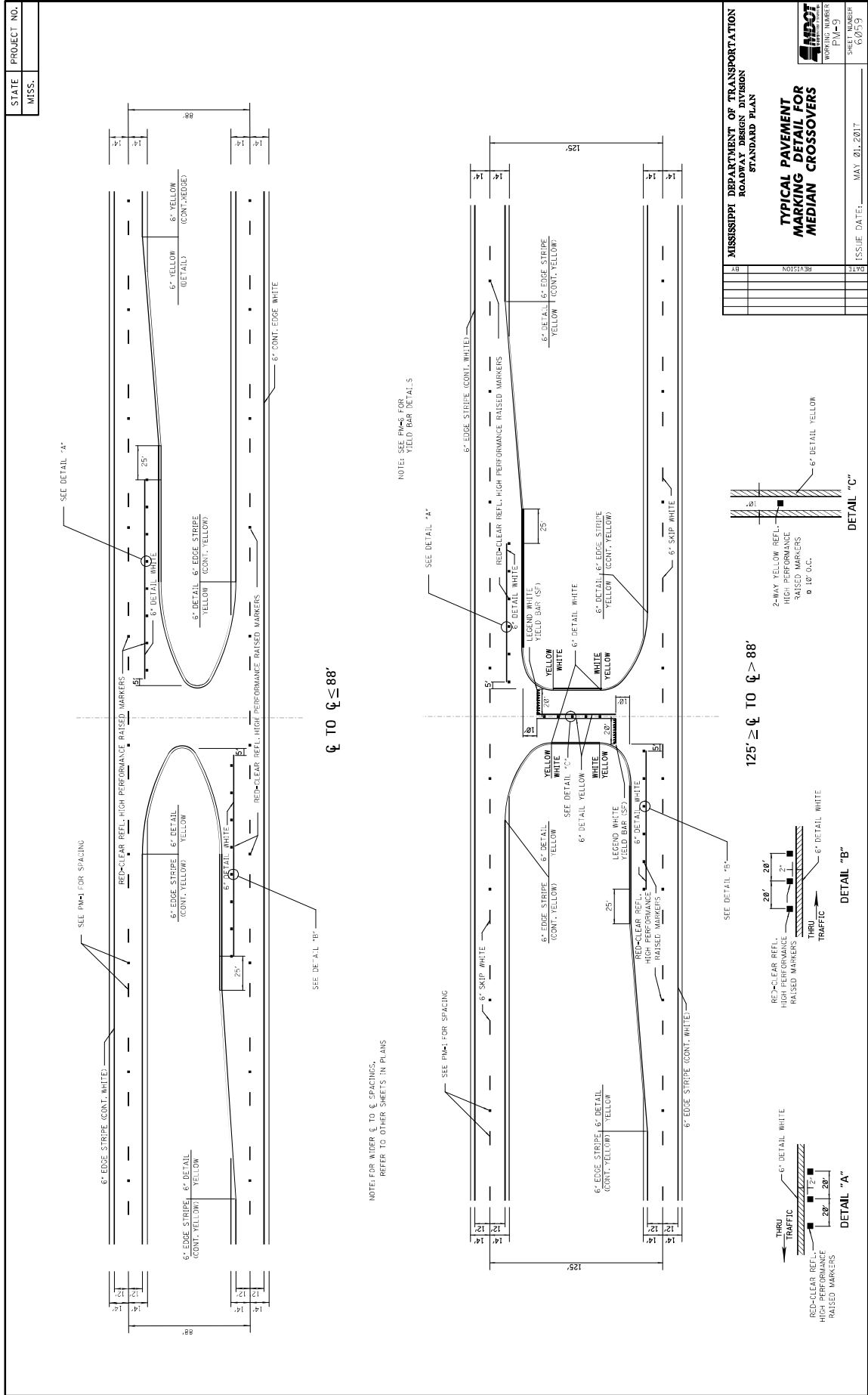
DETAIL A
STANDARD "R" PAVEMENT MARKING
FOR 'R' X 'R' SYMBOL

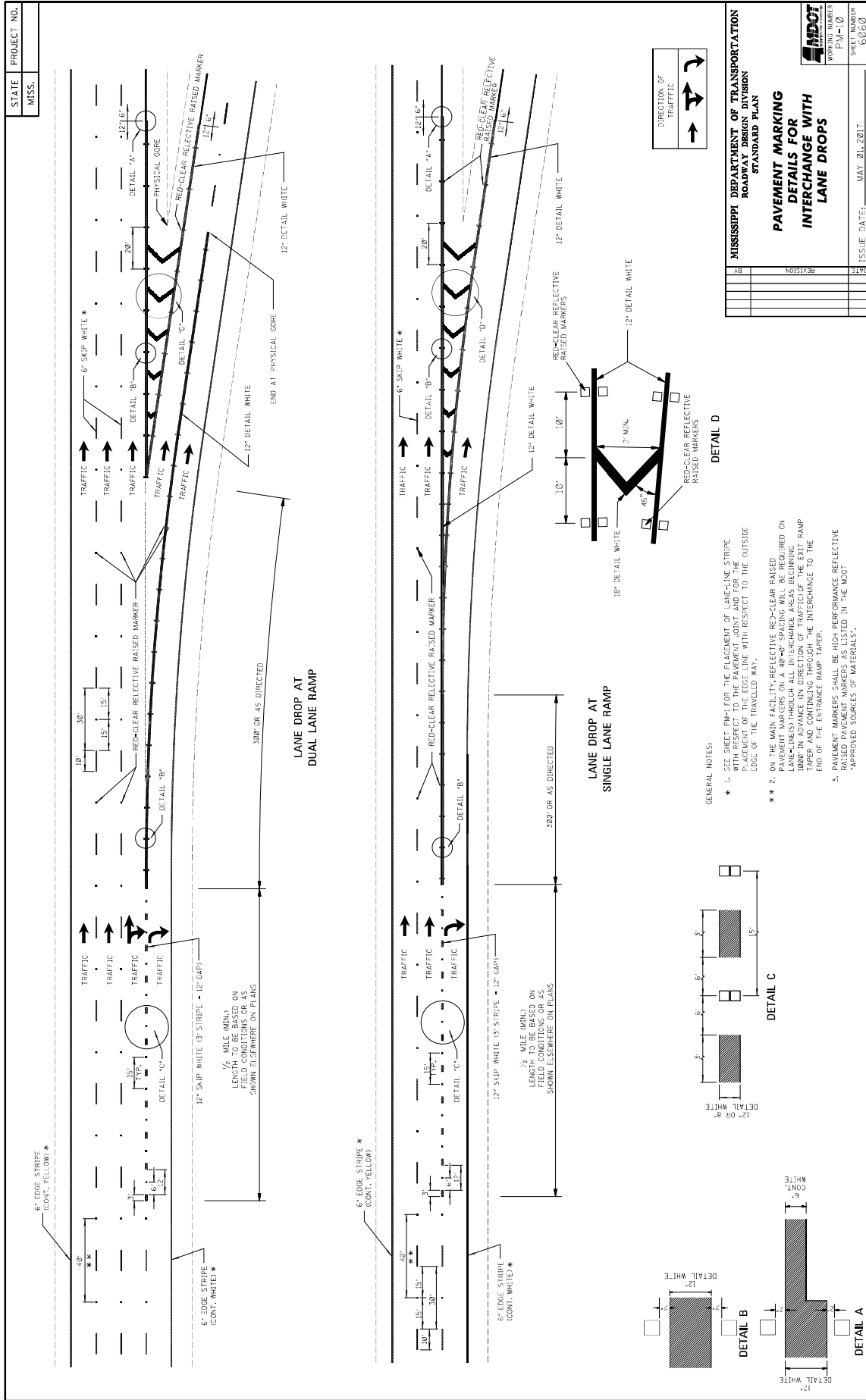
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

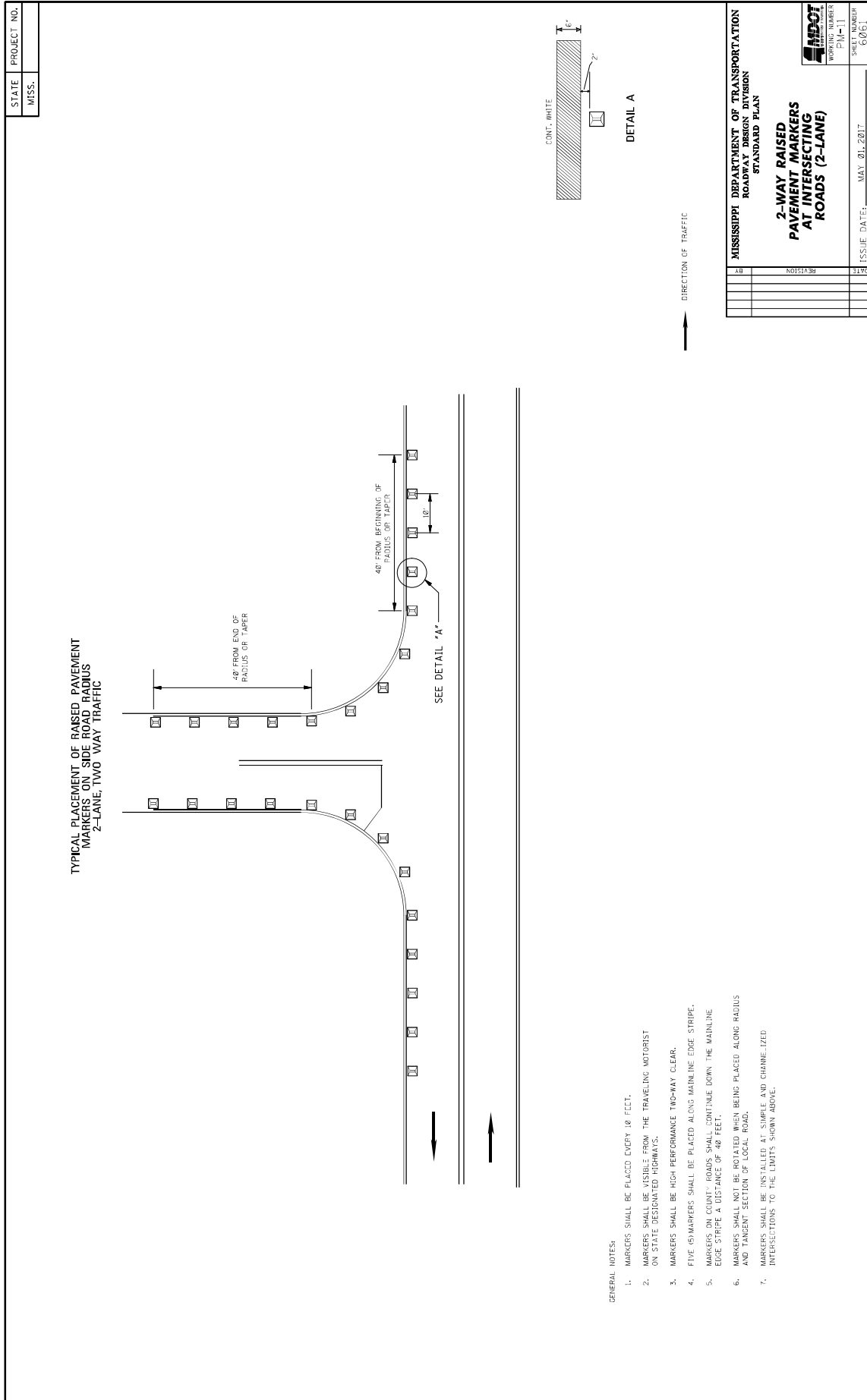
TYPICAL PLACEMENT OF WARNING SIGNS AND PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSINGS

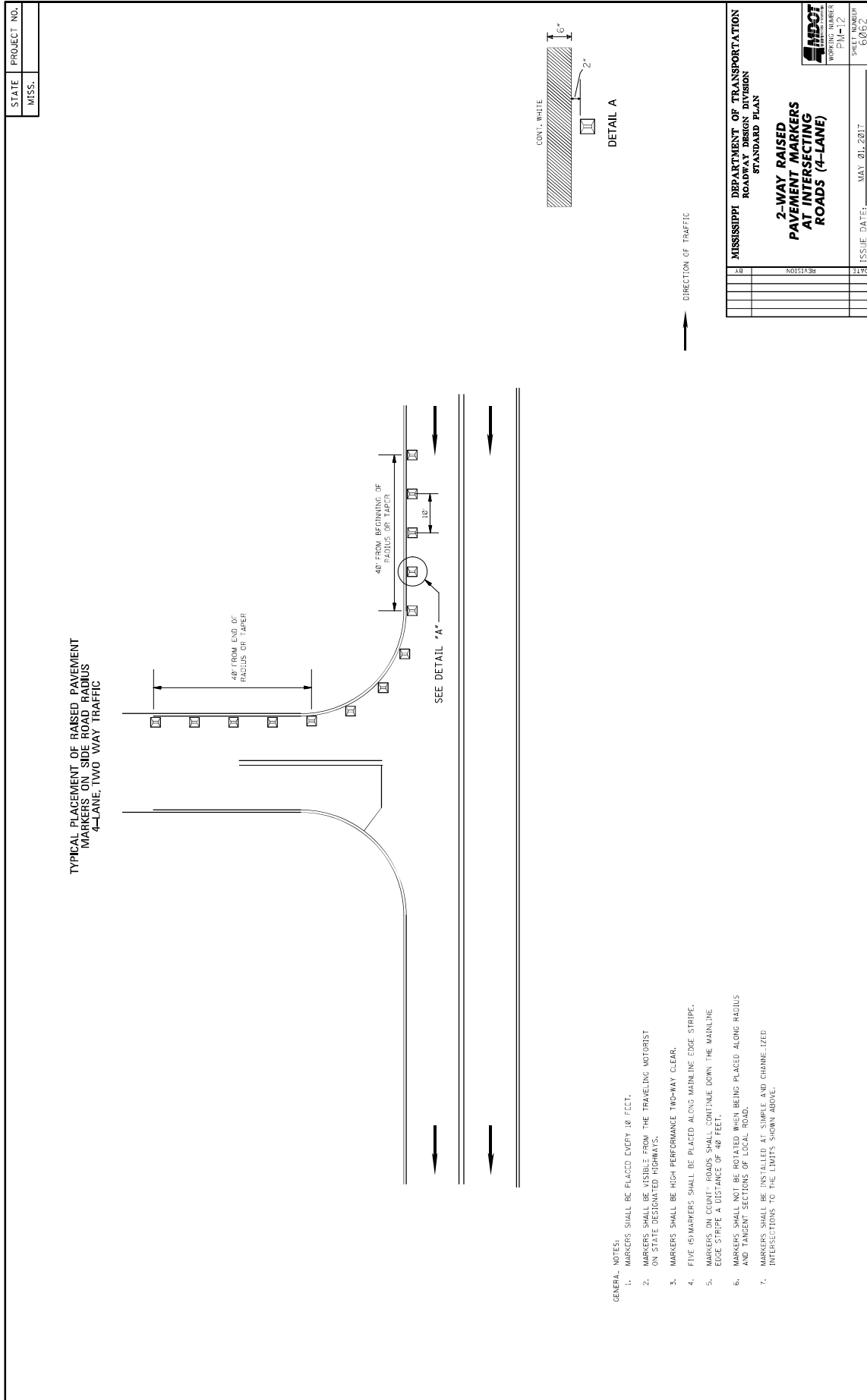
WORKING NUMBER: P10-17
SHEET NUMBER: 6031

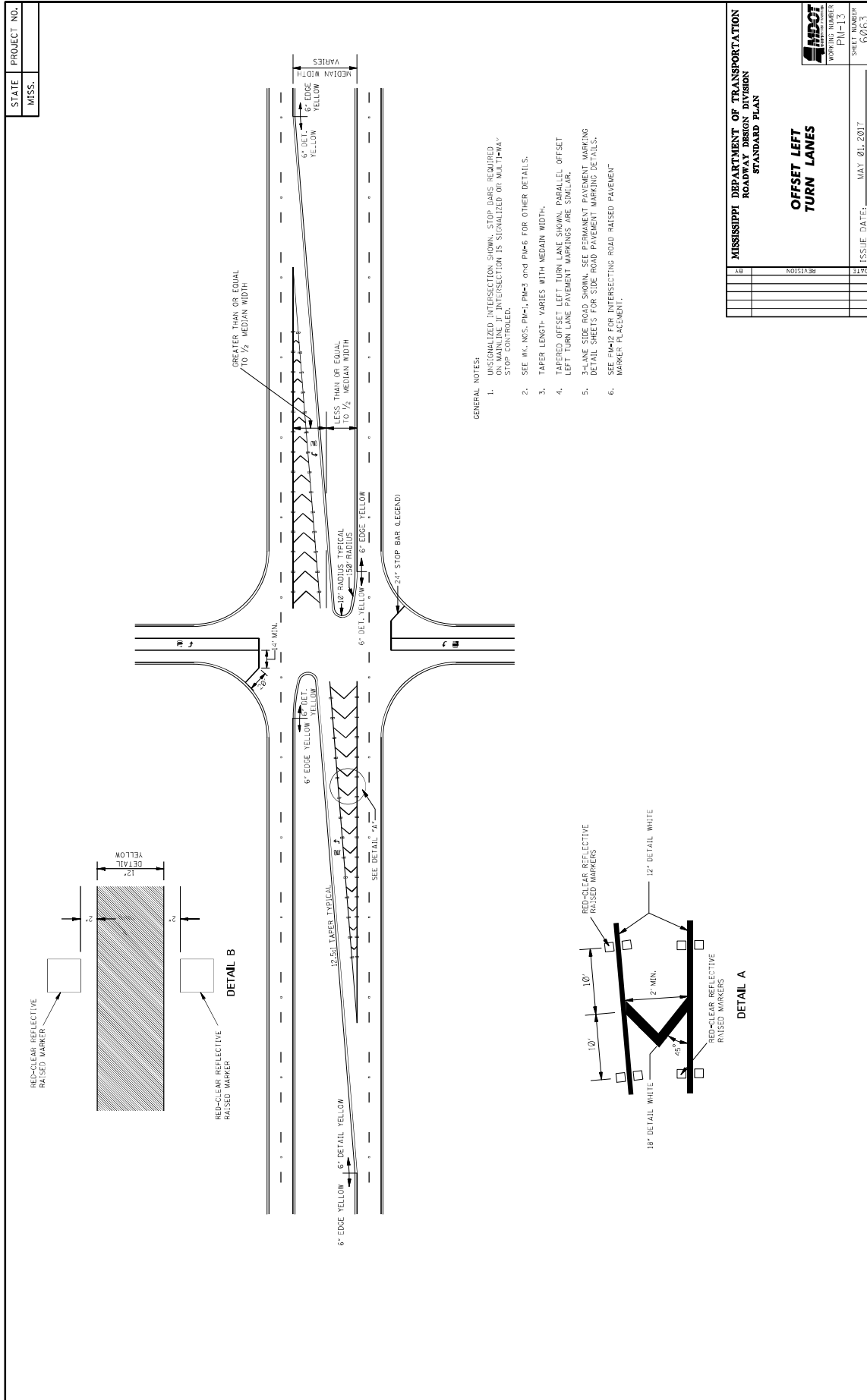
ISSUE DATE: MAY 01, 2017

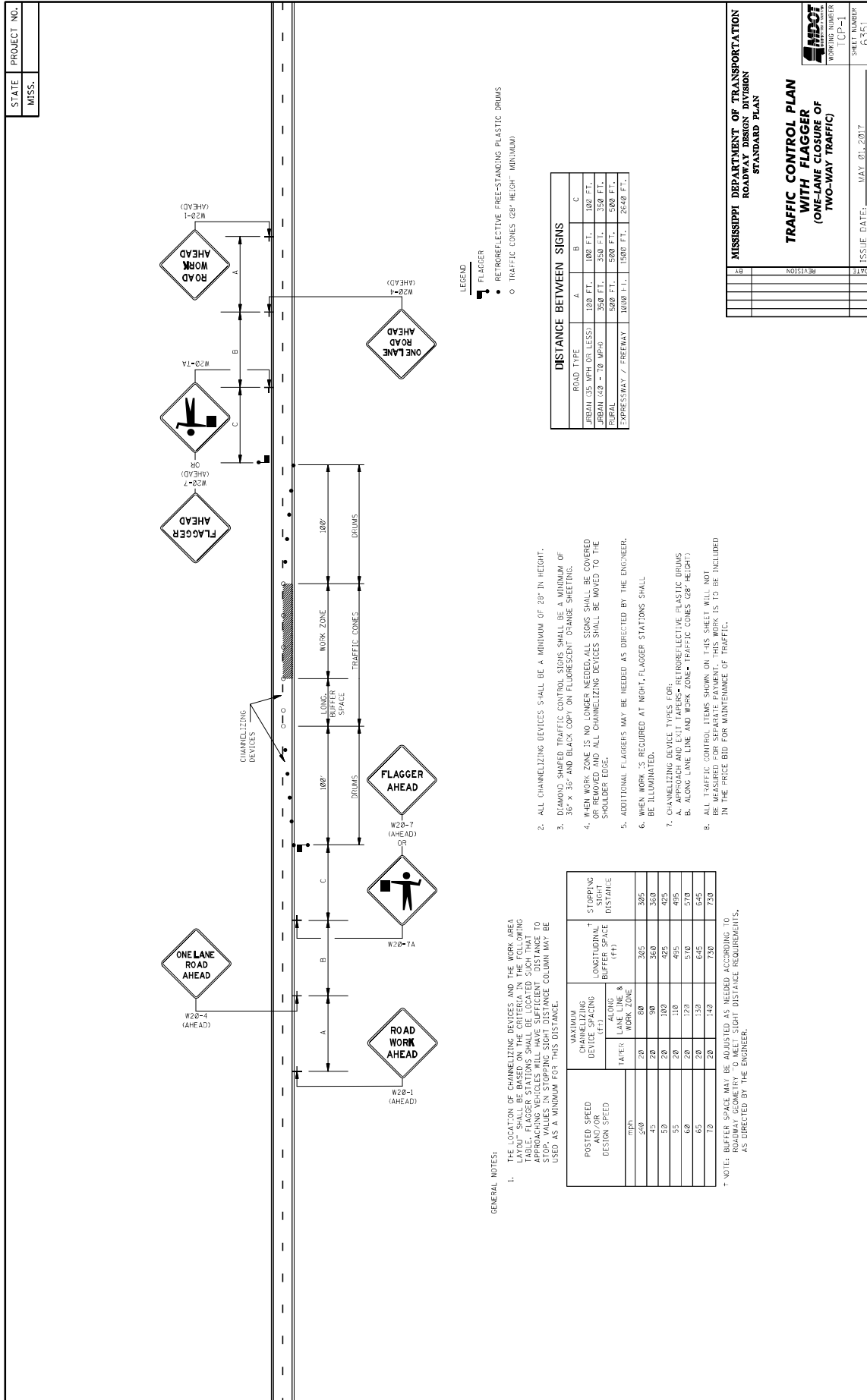












GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE. FLAGGER STATIONS SHALL BE LOCATED SUCH THAT THE FLAGGER IS VISIBLY LOCATED TO TRAFFIC. STOPPING DISTANCE VALUES IN STOPPING SIGHT DISTANCE COLUMN MAY BE USED AS A MINIMUM FOR THIS DISTANCE.

POSTED SPEED DESIGN SPEED mph	MAXIMUM CHANNELIZING DEVICE SPACING (FT)	STOPPING SIGHT DISTANCE (FT)	LONGITUDINAL BUFFER SPACE (FT)	
			PLUS LANE & WORK ZONE	MINUS WORK ZONE
50	20	80	265	305
45	20	90	260	320
50	20	100	425	425
60	20	120	570	570
70	20	140	730	730

T NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

2. ALL CHANNELIZING DEVICES SHALL BE A MINIMUM OF 20" IN HEIGHT.
3. DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHALL BE A MINIMUM OF 36" X 36" AND BLACK COPY ON FLUORESCENT ORANGE SHEETING.
4. WHEN WORK ZONE IS NO LONGER NEEDED, ALL SIGNS SHALL BE COVERED OR REMOVED. ALL CHANNELIZING DEVICES SHALL BE MOVED TO THE SHOULDER EDGE.
5. ADDITIONAL FLAGGERS MAY BE NEEDED AT NIGHT; FLAGGER STATIONS SHALL BE ILLUMINATED.
6. WHEN WORK IS REQUIRED AT NIGHT, FLAGGER STATIONS SHALL BE ILLUMINATED.
7. CHANNELIZING DEVICE TYPES FOR:
 - A. APPROACH AND EXIT TAPERS= RETROREFLECTIVE PLASTIC DRUMS
 - B. ALONG LANE LINE AND WORK ZONE= TRAFFIC CONES (28" HEIGHT)
8. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

DISTANCE BETWEEN SIGNS

ROAD TYPE	A	B	C
URBAN (35 MPH OR LESS)	100 FT.	100 FT.	100 FT.
URBAN (40 - 70 MPH)	250 FT.	250 FT.	350 FT.
RURAL	500 FT.	500 FT.	500 FT.
EXPRESSWAY / FREEWAY	1000 FT.	1500 FT.	2500 FT.

- LEGEND**
- FLAGGER
 - RETROREFLECTIVE PREF-STANDING PLASTIC DRUMS
 - TRAFFIC CONES (28" HEIGHT) MINIMUM

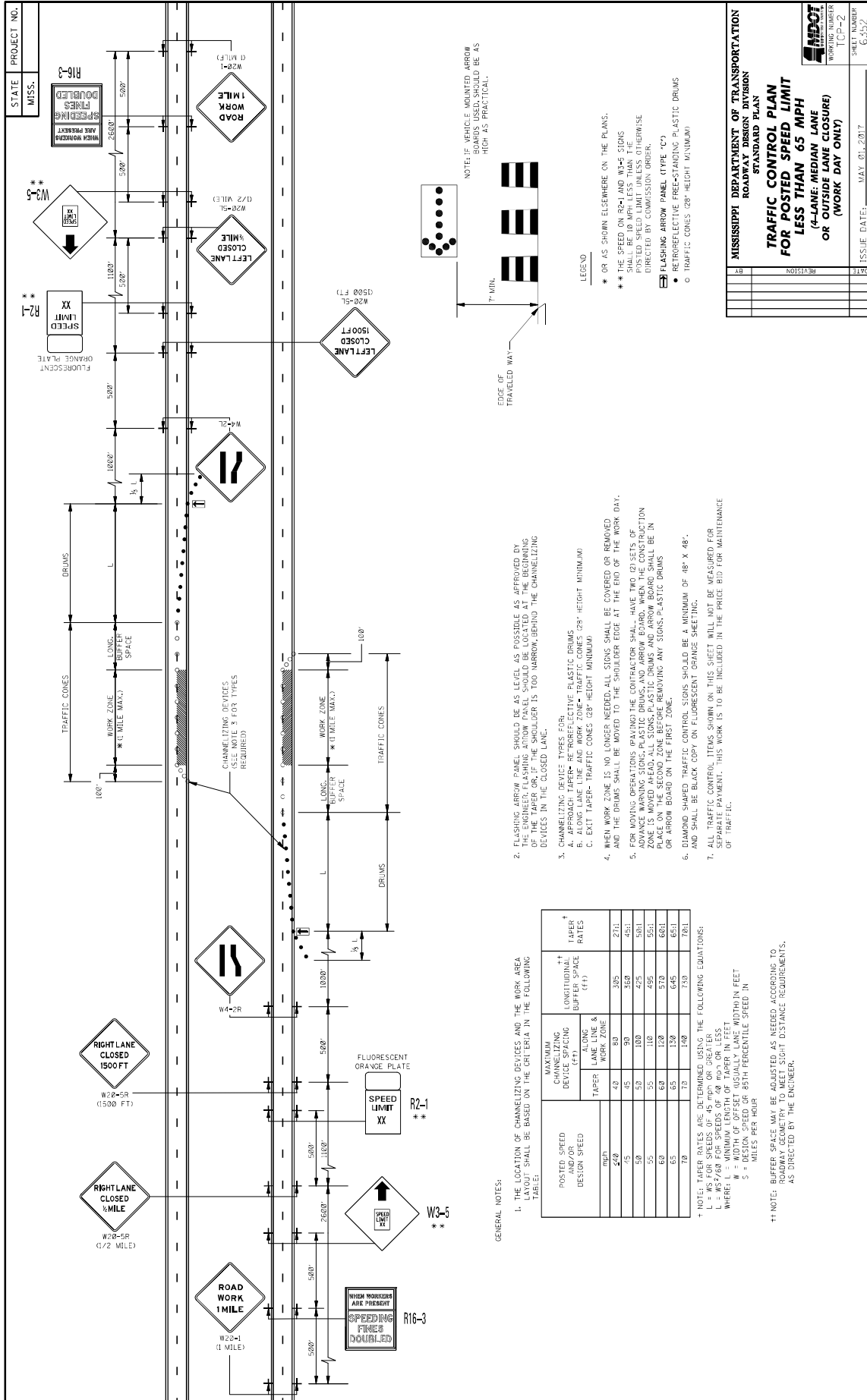
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

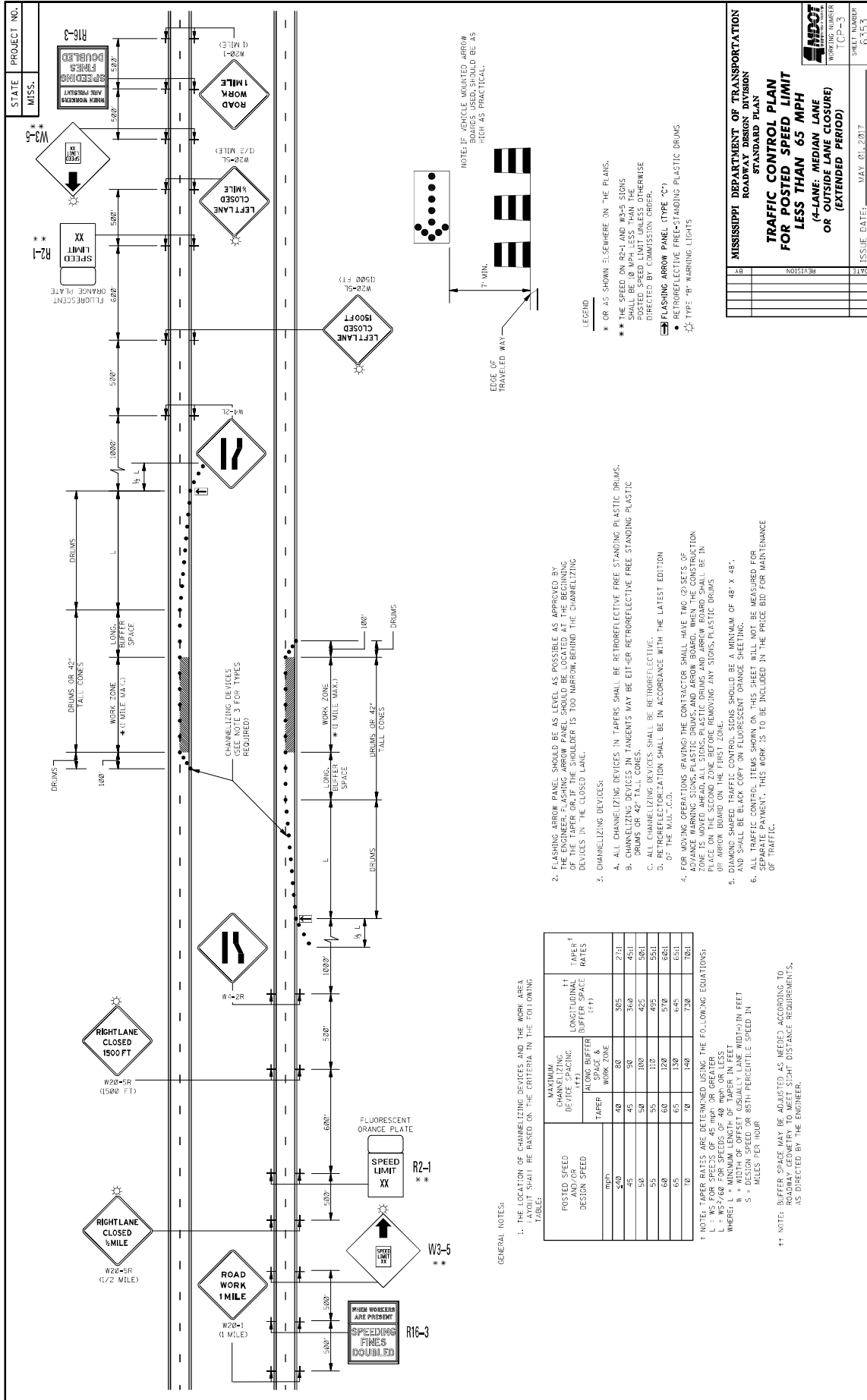
**TRAFFIC CONTROL PLAN
WITH FLAGGER
(ONE-LANE CLOSURE OF
TWO-WAY TRAFFIC)**

ISSUE DATE: MAY 01, 2017

WORKING NUMBER: [CP-1]
SHEET NUMBER: 6351

STATE PROJECT NO.
MISS.





GENERAL NOTES:

1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA LAYOUT SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

POSTED SPEED DESIGN SPEED	MAXIMUM CHANNELIZING DEVICE SPACING		LONGITUDINAL BUFFER SPACE (ft)	TAPER RATES
	ALONG BUFFER	WORK ZONE		
30	40	80	305	2:1
40	45	90	360	4:1
50	50	100	420	5:1
60	55	110	495	6:1
70	60	120	570	8:1
80	65	130	645	10:1

† NOTE: TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 L = AS FOR SPEEDS OF 45 MPH OR GREATER
 L = WS/60 FOR SPEEDS OF 40 MPH OR LESS
 WHERE: W = WIDTH OF OFFSET, USUALLY LANE WIDTH IN FEET
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

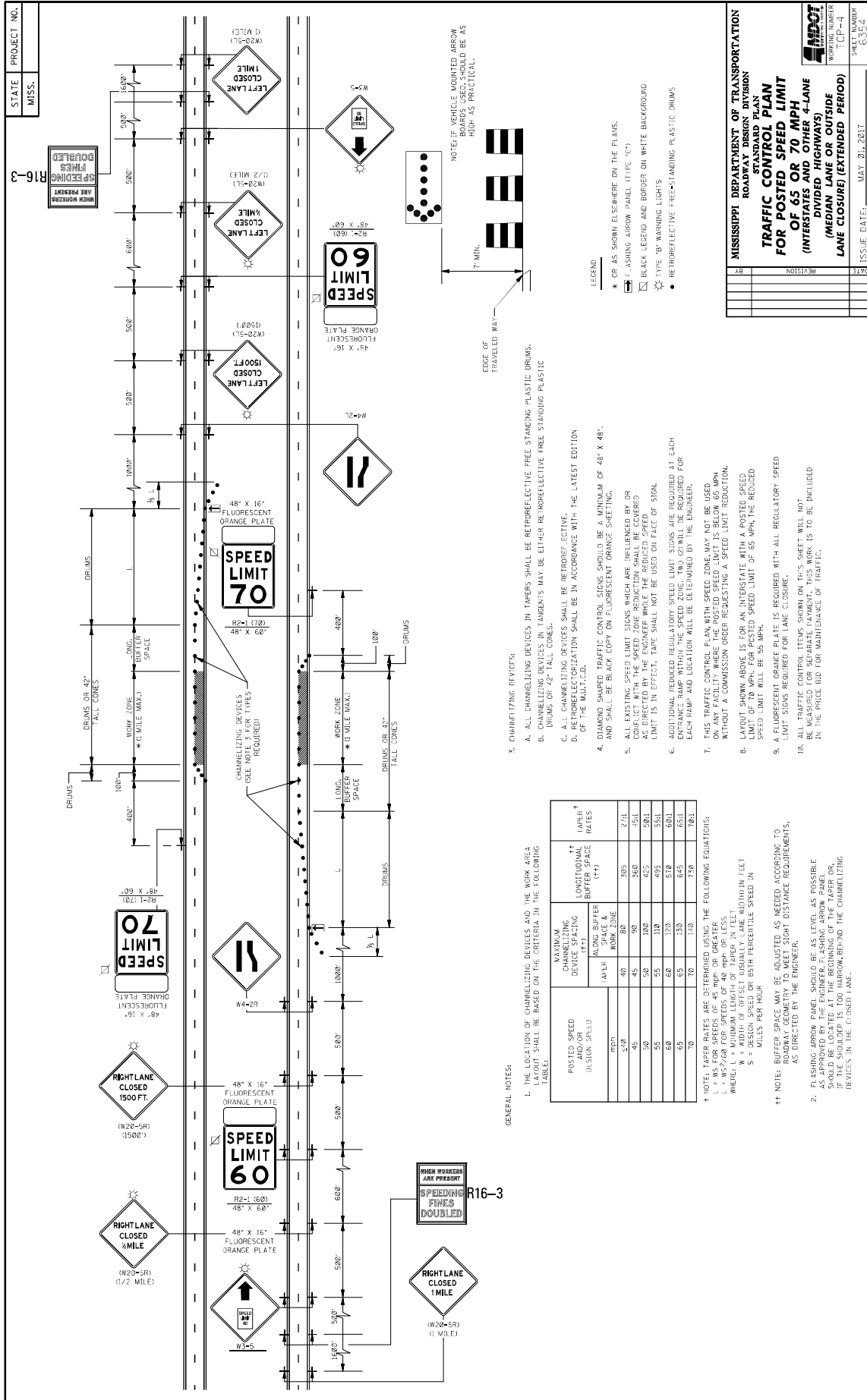
†† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NECESSARY ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

- FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE WORK ZONE. BUFFER SPACE SHOULD BE 100' AHEAD OF THE CHANNELIZING DEVICES IN THE CLOSED LANE.
- CHANNELIZING DEVICES:
 - ALL CHANNELIZING DEVICES IN TAPERS SHALL BE RETROREFLECTIVE FREE STANDING PLASTIC DRUMS.
 - DRUMS OR 42" TALL CONES.
 - ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE.
 - RETROREFLECTORIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE M.I.T.S.D.
- FOR WORKING DRUMS, TAPERS THE CONTRACTOR SHALL HAVE TWO (2) FEET OF PLACEMENT STONES PLASTIC DRUMS AND ARROW BOARD. WHEN THE CONSTRUCTION ZONE IS MOVED AHEAD, ALL SIGNS, PLASTIC DRUMS AND ARROW BOARD SHALL BE IN PLACE ON THE SECOND ZONE BEFORE REMOVING ANY SIGNS, PLASTIC DRUMS OR ARROW BOARD ON THE FIRST ZONE.
- ALL CHANNELIZING DEVICES SHOULD BE A MINIMUM OF 48" X 48" AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

- LEGEND
- * OR AS SHOWN ELSEWHERE ON THE PLANS.
 - ** THE SPEED ON R2-1 AND R2-5 SIGNS SHALL BE 10 MPH LESS THAN THE POSTED SPEED LIMIT UNLESS OTHERWISE DIRECTED BY COMMISSION ORDER.
 - ◆ FLASHING ARROW PANEL (TYPE 'C')
 - RETROREFLECTIVE FREE-STANDING PLASTIC DRUMS
 - TYPE 'B' WARNING LIGHTS

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN
**TRAFFIC CONTROL PLAN
 FOR POSTED SPEED LIMIT
 LESS THAN 65 MPH
 (4-LANE, MEDIAN LANE
 OR OUTSIDE LANE CLOSURE
 OR (EXTENDED PERIOD))**

ISSUE DATE: MAY 01, 2017
 SHEET NUMBER: 6253
 PROJECT NO.: MISS.



STATE PROJECT NO.
MISS. R16-3

WHEN WORKERS ARE WORKING SPEEDING FINES ARE DOUBLED

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION

TRAFFIC CONTROL PLAN FOR POSTED SPEED LIMIT OF 65 OR 70 MPH (INTERSTATES AND OTHER 4-LANE DIVIDED HIGHWAYS) (MEDIUM LANE OR OUTSIDE LANE CLOSED) (EXTENDED PERIOD)

ISSUE DATE: MAY 20, 2017

NO.	REVISION

SHEET NUMBER: CP-44
 PROJECT NUMBER: G5554

POSTED SPEED (DESIGN SPEED) MPH	MAXIMUM CHANNELIZING DEVICE SPACING ALONG BUFFER SPACE & WORK ZONE		LONGITUDINAL BUFFER SPACE (FT)	TAPER RATES
	FL/PL	PL/PL		
50	40	80	305	2/1
55	45	90	360	3/1
60	50	100	420	4/1
65	55	110	495	5/1
70	60	120	570	6/1
75	65	130	645	6.5/1
80	70	140	730	7/1

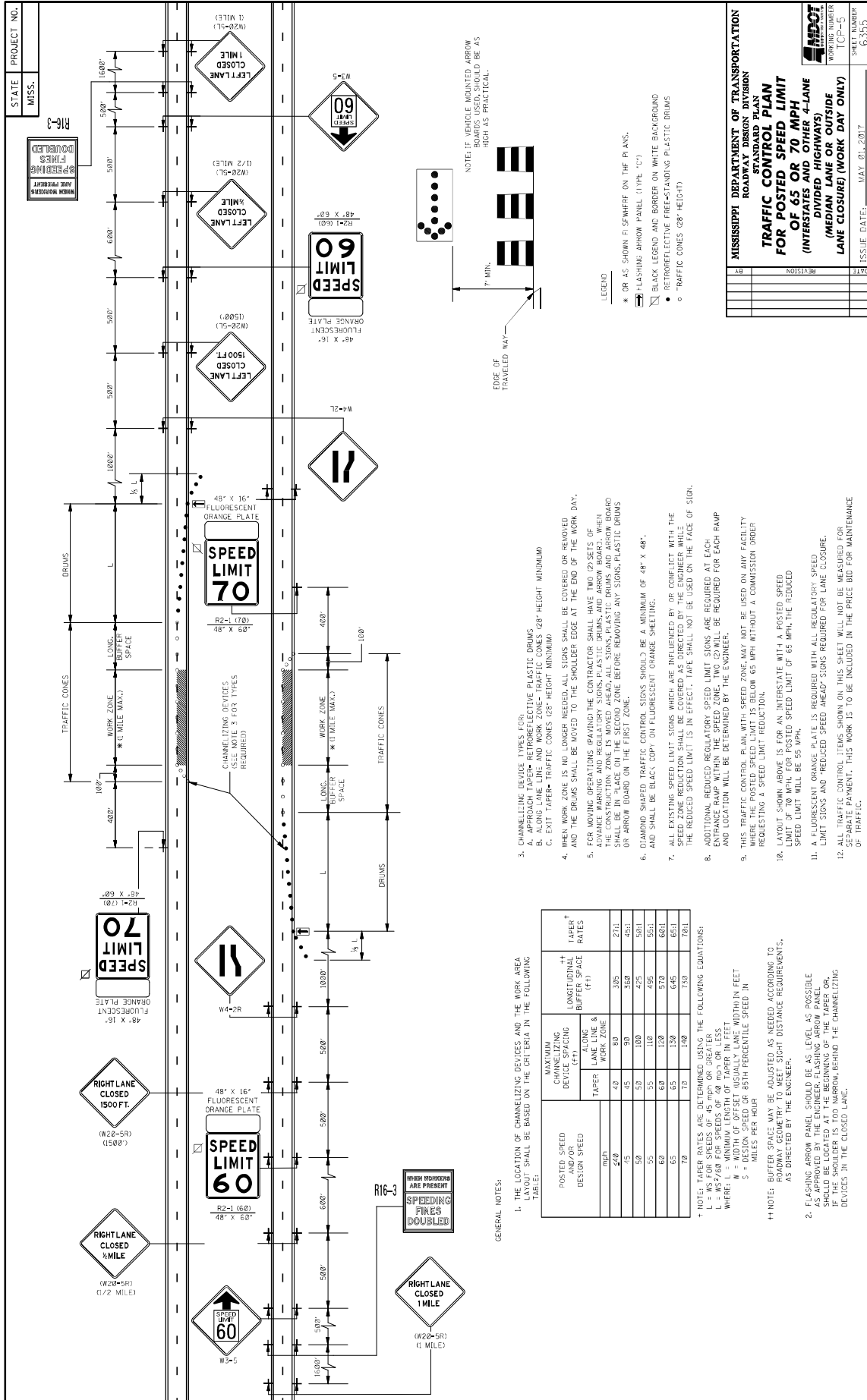
1. THE LOCATION OF CHANNELIZING DEVICES AND THE WORK AREA TAPER RATES SHALL BE BASED ON THE CRITERIA IN THE FOLLOWING TABLE:

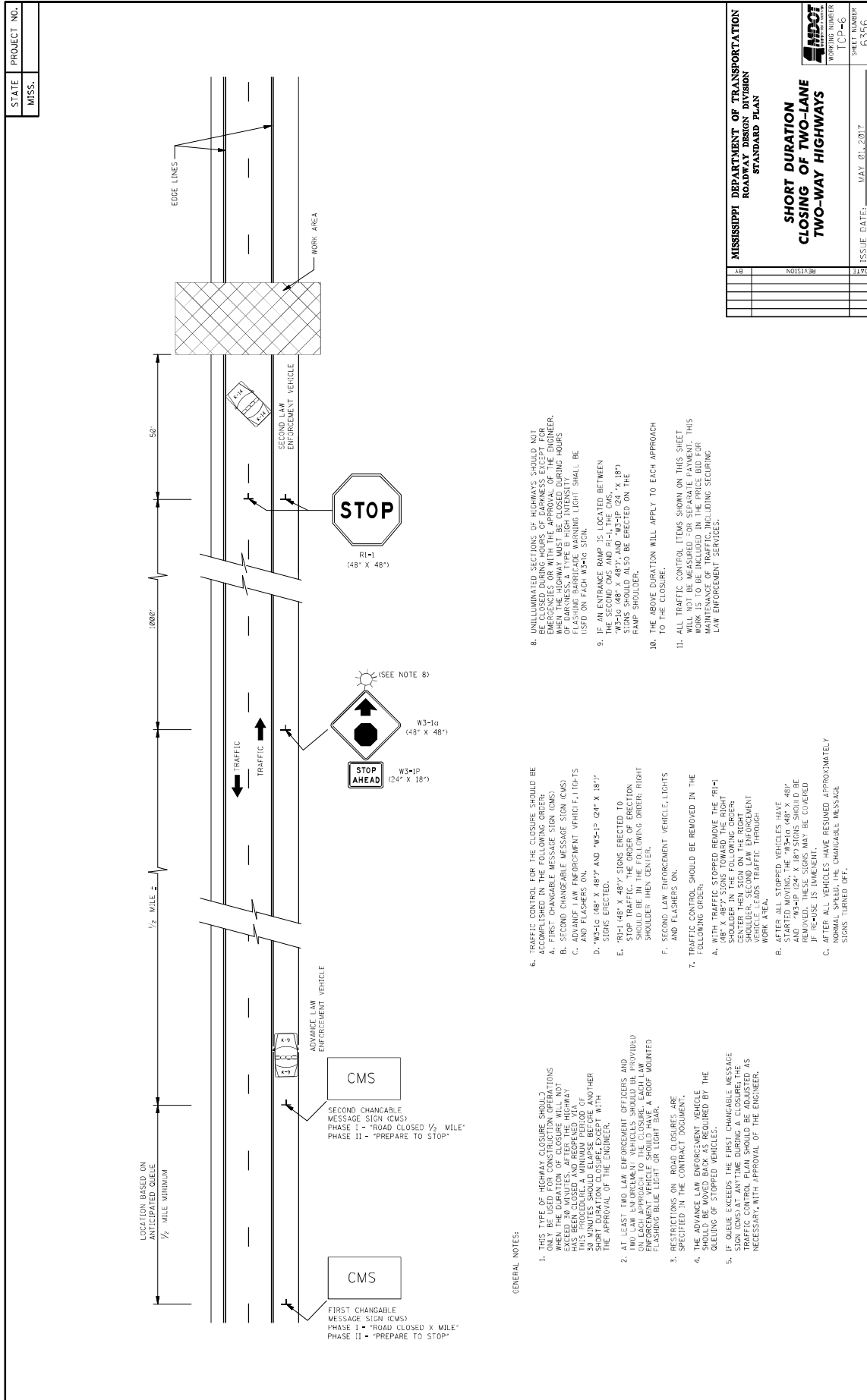
† TAPER RATES ARE DETERMINED USING THE FOLLOWING EQUATIONS:
 T = WS²/60 FOR SPEEDS OF 45 MPH OR GREATER
 L = WS/60 FOR SPEEDS OF 40 MPH OR LESS
 WHERE: L = MINIMUM BUFFER SPACE (FEET)
 S = DESIGN SPEED OR 85TH PERCENTILE SPEED IN MILES PER HOUR

†† NOTE: BUFFER SPACE MAY BE ADJUSTED AS NEEDED ACCORDING TO ROADWAY GEOMETRY TO MEET SIGHT DISTANCE REQUIREMENTS, AS DIRECTED BY THE ENGINEER.

2. FLASHING ARROW PANEL SHOULD BE AS LEVEL AS POSSIBLE AS APPROVED BY THE ENGINEER. FLASHING ARROW PANEL SHOULD BE LOCATED AT THE BEGINNING OF THE TAPER OR AT THE END OF THE TAPER. CHANNELIZING DEVICES IN THE CLOSED LANE.

- GENERAL NOTES:
- CHANNELIZING DEVICES IN TAPERS SHALL BE RETROREFLECTIVE FREE STANDING PLASTIC DRUMS.
 - CHANNELIZING DEVICES IN TANGENTS MAY BE EITHER RETROREFLECTIVE FREE STANDING PLASTIC DRUMS OR 42" TALL CONES.
 - ALL CHANNELIZING DEVICES SHALL BE RETROREFLECTIVE.
 - RETROREFLECTIVIZATION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MUTCD-6A.
 - DIAMOND SHAPED TRAFFIC CONTROL SIGNS SHOULD BE A MINIMUM OF 48" X 48" AND SHALL BE BLACK COPY ON FLUORESCENT ORANGE SHEETING.
 - ALL EXISTING SPEED LIMIT SIGNS WHICH ARE INFLUENCED BY OR CONFLICT WITH THE SPEED ZONE REDUCTION SHALL BE COVERED.
 - LIMIT IS IN EFFECT, TAPE SHALL NOT BE USED ON FACE OF SIGN.
 - ADDITIONAL REQUIRED REGULATORY SPEED LIMIT SIGNS ARE REQUIRED AT EACH ENTRANCE RAMP AND LEGALION WILL BE DETERMINED BY THE ENGINEER.
 - THIS TRAFFIC CONTROL PLAN WITH SPEED ZONE MAY NOT BE USED ON ANY FACILITY WHERE THE POSTED SPEED LIMIT IS BELOW 65 MPH WITHOUT A COMMISSION ORDER REQUESTING A SPEED LIMIT REDUCTION.
 - LAYOUT SHOWN ABOVE IS FOR AN INTERSTATE WITH A POSTED SPEED LIMIT OF 70 MPH FOR PASTED SPEED LIMIT OF 65 MPH. THE REDUCED SPEED LIMIT WILL BE 55 MPH.
 - A FLUORESCENT ORANGE PLATE IS REQUIRED WITH ALL REGULATORY SPEED LIMIT SIGNS REQUIRED FOR LANE CLOSURE.
 - ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT, THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.





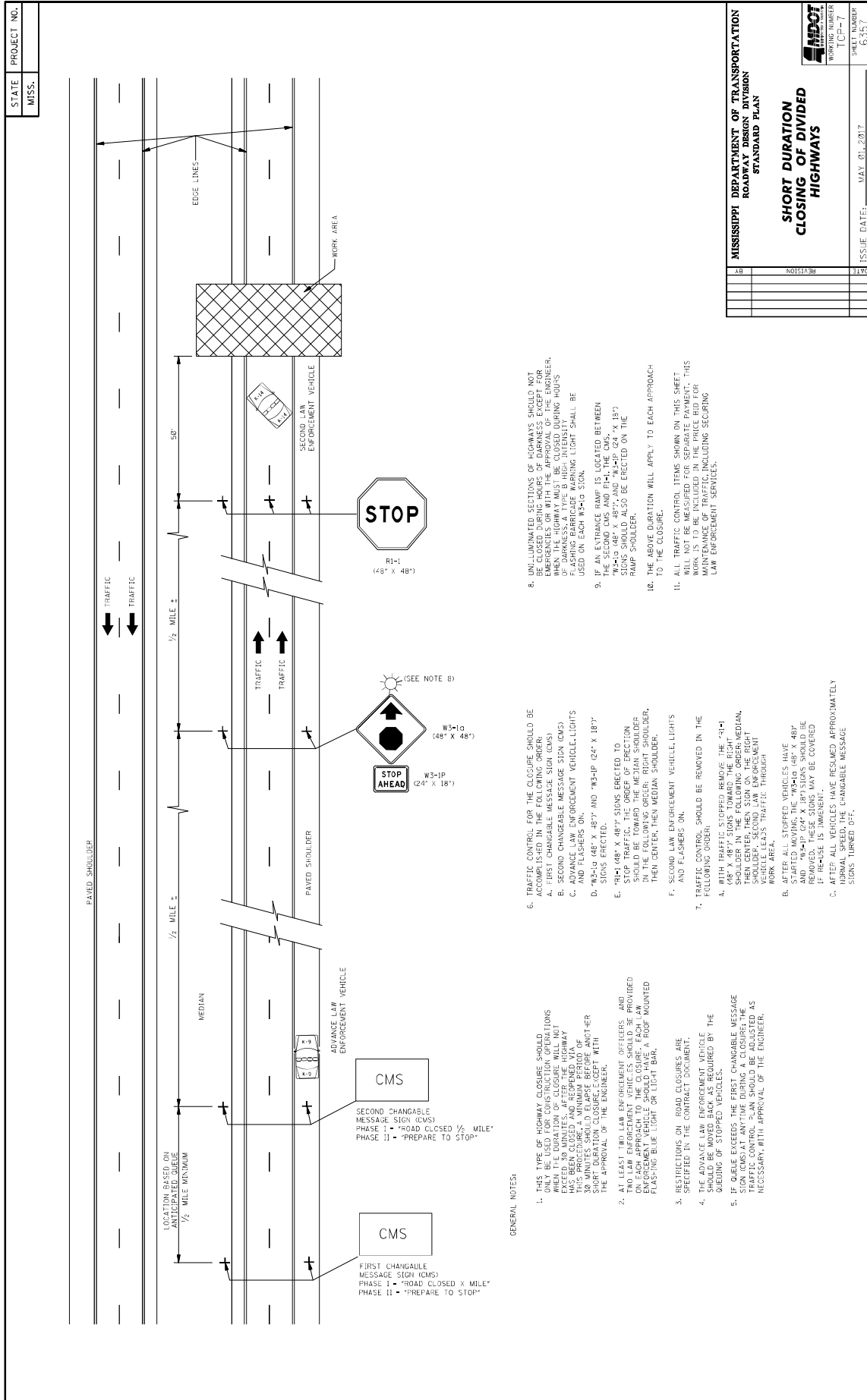
GENERAL NOTES:

1. THIS TYPE OF HIGHWAY CLOSURE SHOULD BE USED ONLY WHEN THE DURATION OF CLOSURE WILL NOT EXCEED 30 MINUTES. AFTER THE HIGHWAY IS CLOSED, A MINIMUM PERIOD OF 30 MINUTES SHOULD ELAPSE BEFORE ANOTHER CLOSURE OCCURS. CONSULT WITH THE APPROVAL OF THE ENGINEER.
2. AT LEAST TWO LAW ENFORCEMENT OFFICERS AND ONE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD BE ON EACH APPROACH TO THE CLOSURE. EACH LAW ENFORCEMENT VEHICLE SHOULD HAVE A ROOF MOUNTED FLASHING BLUE LIGHT ON LIGHT BAR.
3. RESTRICTIONS ON ROAD CLOSURES ARE SPECIFIED IN THE CONTRACT DOCUMENT.
4. THE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD BE POSITIONED BY THE QUEUING OF STOPPED VEHICLES.
5. IF QUEUE EXCEEDS THE FIRST CHANGABLE MESSAGE SIGN (CMS) AT ANYTIME DURING A CLOSURE, THE TRAFFIC CONTROL PLAN SHOULD BE ADJUSTED AS NECESSARY, WITH APPROVAL OF THE ENGINEER.
6. TRAFFIC CONTROL FOR THE CLOSURE SHOULD BE ACCOMPLISHED IN THE FOLLOWING ORDER:
 - A. FIRST CHANGABLE MESSAGE SIGN (CMS)
 - B. SECOND CHANGABLE MESSAGE SIGN (CMS)
 - C. ADVANCE LAW ENFORCEMENT VEHICLE, LIGHTS AND FLASHERS ON
 - D. "W3-1A (48" X 48") AND "W3-1P (24" X 18") SIGNS ERECTED.
 - E. "R1-1 (48" X 48") SIGNS ERECTED TO STOP TRAFFIC. THE ORDER OF ERECTION SHOULD BE IN THE FOLLOWING ORDER: RIGHT SHOULDER THEN CENTER.
 - F. SECOND LAW ENFORCEMENT VEHICLE, LIGHTS AND FLASHERS ON.
7. TRAFFIC CONTROL SHOULD BE REMOVED IN THE FOLLOWING ORDER:
 - A. WITH TRAFFIC STOPPED REMOVE THE "R1-1 (48" X 48") SIGNS TOWARD THE RIGHT SHOULDER THEN SIGN ON THE RIGHT SHOULDER. SECOND LAW ENFORCEMENT VEHICLE ADDS TRAFFIC THROUGH WORK AREA.
 - B. AFTER ALL STOPPED VEHICLES HAVE STARTED MOVING, THE "W3-1A (48" X 48") AND "W3-1P (24" X 18") SIGNS SHOULD BE REMOVED IN THE FOLLOWING ORDER: IF ROAD IS TRAVELING IN ONE DIRECTION, NORMAL SPEED; THE CHANGABLE MESSAGE SIGNS TURNED OFF.
8. UNILLUMINATED SECTIONS OF HIGHWAYS SHOULD NOT BE CLOSED DURING HOURS OF DARKNESS EXCEPT FOR EMERGENCY REPAIRS. THE APPROVAL OF THE ENGINEER, WHEN THE WORK MUST BE CLOSED DURING HOURS OF DARKNESS, A TYPE B HIGH INTENSITY FLASHING FABRICATED WARNING LIGHT SHALL BE USED ON EACH HOV-3 SIGN.
9. IF AN ENTRANCE RAMP IS LOCATED BETWEEN THE CLOSURE AND THE MAINLINE, "W3-1A (48" X 48") AND "W3-1P (24" X 18") SIGNS SHOULD ALSO BE ERECTED ON THE RAMP SHOULDER.
10. THE ABOVE DURATION WILL APPLY TO EACH APPROACH TO THE CLOSURE.
11. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE AND CONSTRUCTION INCLUDING SECURING LAW ENFORCEMENT SERVICES.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**SHORT DURATION
CLOSING OF TWO-LANE
TWO-WAY HIGHWAYS**

REVISION	SHEET NUMBER
	6356
	ISSUE DATE: MAY 01, 2017



STATE PROJECT NO.
MISS.

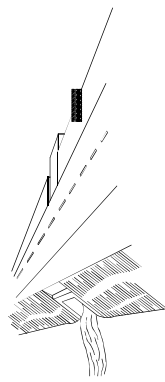
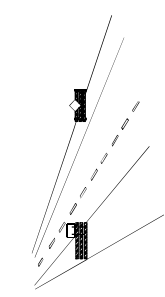
MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	
SHORT DURATION CLOSING OF DIVIDED HIGHWAYS	
AB	REVISION
3170	SHEET NUMBER
	WORKING NUMBER
	ISSUE DATE: MAY 01, 2017
	ISSUE NUMBER
	6257

- GENERAL NOTES:**
- THIS TYPE OF HIGHWAY CLOSURE SHOULD BE USED ONLY FOR SHORT DURATION CLOSURES. WHEN THE DURATION OF CLOSURE WILL NOT EXCEED 30 MINUTES AFTER THE HIGHWAY CLOSURE, THE FOLLOWING PROCEDURE SHOULD BE USED:
 - THIS PROCEDURE A MINIMUM PERIOD OF 30 MINUTES SHOULD ELAPSE BEFORE ANY OTHER CLOSURES ARE MADE AT THE SAME LOCATION WITH THE APPROVAL OF THE ENGINEER.
 - AT LEAST TWO LAW ENFORCEMENT OFFICERS AND TWO LAW ENFORCEMENT VEHICLES SHOULD BE PROVIDED ON EACH APPROACH TO THE CLOSURE. EACH LAW ENFORCEMENT VEHICLE SHOULD BE EQUIPPED WITH A ROOF MOUNTED FLASHING BLUE LIGHT OR LIGHT BAR.
 - RESTRICTIONS ON ROAD CLOSURES ARE SPECIFIED IN THE CONTRACT DOCUMENT.
 - THE ADVANCE LAW ENFORCEMENT VEHICLE SHOULD BE MOVED BACK AS REQUIRED BY THE QUEUING OF STOPPED VEHICLES.
 - IF QUEUE EXCEEDS THE FIRST CHANGABLE MESSAGE SIGN (CMS), THE MESSAGE SHOULD BE CHANGED TO "ROAD CLOSED 1/2 MILE AHEAD". TRAFFIC CONTROL PLAN SHOULD BE ADJUSTED AS NECESSARY WITH APPROVAL OF THE ENGINEER.
 - TRAFFIC CONTROL FOR THE CLOSURE SHOULD BE ACCOMPLISHED IN THE FOLLOWING ORDER:
 - FIRST CHANGABLE MESSAGE SIGN (CMS)
 - SECOND CHANGABLE MESSAGE SIGN (CMS)
 - ADVANCE LAW ENFORCEMENT VEHICLE (W3-1G) AND FLASHERS ON
 - W3-1G (48" X 48") AND W3-1P (24" X 18") SIGNS ERRECTED.
 - W3-1G (48" X 48") SIGNS ERRECTED TO STOP TRAFFIC. THE ORDER OF ERECTION SHOULD BE TOWARD THE MEDIAN SHOULDER, THEN CENTER, THEN MEDIAN SHOULDER.
 - SECOND LAW ENFORCEMENT VEHICLE, LIGHTS AND FLASHERS ON.
 - TRAFFIC CONTROL SHOULD BE REMOVED IN THE FOLLOWING ORDER:
 - WITH TRAFFIC STOPPED REMOVE THE "1/2 MILE AHEAD" SIGN FROM THE MEDIAN SHOULDER IN THE FOLLOWING ORDER: MEDIAN, THEN CENTER, THEN SIGN ON THE RIGHT SHOULDER. AFTER TRAFFIC CONTROL IS REMOVED, TRAFFIC SHOULD BE ALLOWED TO REENTER THE WORK AREA.
 - AFTER ALL STOPPED VEHICLES HAVE STARTED MOVING, THE W3-1G (48" X 48") AND W3-1P (24" X 18") SIGNS SHOULD BE REMOVED. THESE SIGNS MAY BE COVERED IF RELEASE IS IMMINENT.
 - AFTER ALL VEHICLES HAVE RESUMED APPROXIMATELY NORMAL FLOW, THE CHANGABLE MESSAGE SIGN SHOULD BE TURNED OFF.
 - UNILLUMINATED SECTIONS OF HIGHWAYS SHOULD NOT BE CLOSED DURING HOURS OF DARKNESS EXCEPT FOR EMERGENCY REPAIRS. WHEN THE HIGHWAY MUST BE CLOSED DURING HOURS OF DARKNESS, A TYPE B HIGH-INTENSITY FLUORESCENT LIGHTING SYSTEM SHALL BE USED ON EACH W3-1G SIGN.
 - IF AN ENTRANCE RAMP IS LOCATED BETWEEN THE SECOND LANE AND F1+1, THE CMS "W3-1G 48" X 48" AND "W3-1P (24" X 18") SIGNS SHOULD ALSO BE ERRECTED ON THE RAMP SHOULDER.
 - THE ABOVE DURATION WILL APPLY TO EACH APPROACH TO THE CLOSURE.
 - ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK SHALL BE INCLUDED IN THE PRICE FOR MAINTENANCE OF SERVICES INCLUDING SECURING LAW ENFORCEMENT SERVICES.

STATE PROJECT NO.
MISS.

WORKING NUMBER
ICP-5

SHEET NUMBER
03300

WING BARRICADES

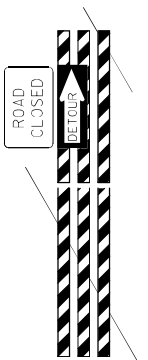
1. WING BARRICADES ARE TYPE II BARRICADES ERECTED ON THE SHOULDER OF A ROADWAY OR RESTRICTED ROADWAY. WING BARRICADES MAY BE USED AS A MOUNTING FOR THE ADVANCE WARNING SIGNS OR FLASHERS.

2. WING BARRICADES SHOULD BE USED:

A. IN ADVANCE OF A CONSTRUCTION PROJECT EVEN WHEN NO PART OF THE ROADWAY IS ACTUALLY CLOSED.

B. IN ADVANCE OF ALL BRIDGE OR CULVERT WIDENING OPERATIONS.

BARRICADE CLOSING A ROAD



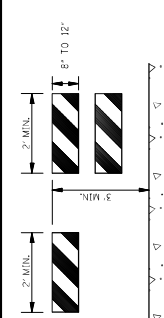
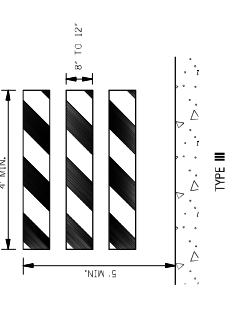
BARRICADE CHARACTERISTICS

	I	II	III
WIDTH OF RAIL **	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.	8" MIN. - 12" MAX.
LENGTH OF RAIL **	24' MIN.	24' MIN.	48' MIN.
WIDTH OF STRIPE *	6"	6"	6"
HEIGHT	36" MIN.	36" MIN.	60" MIN.
NUMBER OF RETROREFLECTORIZED RAIL FACES	2 (ONE EACH DIRECTION)	4 (TWO EACH DIRECTION)	3 IF FACING TRAFFIC IN ONE DIRECTION 4 IF FACING TRAFFIC IN TWO DIRECTIONS

* 1. FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.

** 2. BARRICADES INTENDED FOR USE ON EXPRESSWAYS, FREEWAYS AND OTHER HIGH SPEED ROADWAYS, SHALL HAVE A MINIMUM OF 270 IN² OF REFLECTIVE AREA FACING TRAFFIC.

STANDARD BARRICADES

1. THE MARKING FOR BARRICADE RAILS SHALL BE ORANGE AND WHITE (SLOPING DOWNWARD AT AN ANGLE OF 45° IN THE DIRECTION TRAFFIC IS TO PASS).

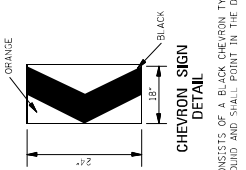
2. RAIL STRIPE SHALL BE 6 INCHES, EXCEPT THAT 4-INCH WIDE STRIPES MAY BE USED IF RAIL LENGTHS ARE LESS THAN 36 INCHES.

3. DO NOT PLACE SANDBAGS OR OTHER DEVICES TO PROVIDE MASS ON THE BOTTOM RAIL THAT WILL BLOCK VIEW OR RAIL FACE.

4. FOR ADDITIONAL INFORMATION OR DETAILS, SEE METHOD, LATEST EDITION.

5. BARRICADES ARE CLASSIFIED BY FHWA AS CATEGORY II WHEN ZONE DEVICES WHICH REQUIRE SUCCESSFULLY CRASH TESTED. A LIST OF CRASHWORTHY BARRICADES AND OTHER CATEGORY II DEVICES CAN BE FOUND ON FHWA'S WEBSITE: http://safety.fhwa.dot.gov/roadway_dept/pafety_guidance/road_hardware/cat2.cfm

CHEVRON SIGN DETAIL

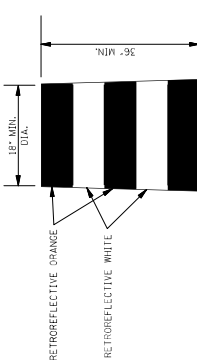


1. A CHEVRON SIGN CONSISTS OF A BLACK CHEVRON TYPE MARKING ON AN ORANGE BACKGROUND AND SHALL POINT IN THE DIRECTION OF TRAFFIC FLOW.

2. THE CHEVRON SIGN SHALL BE MOUNTED ON CRASHWORTHY SUPPORT.

3. CHEVRON SIGNS MAY BE USED TO SUPPLEMENT OTHER STANDARD DEVICES WHERE ONE OR MORE LANES ARE CLOSED FOR CONSTRUCTION OR MAINTENANCE. THEY SHOULD BE PLACED APPROXIMATELY 2' 40" BEHIND THE LANE TRANSITION STRIPE.

PLASTIC DRUM STRIPING DETAIL

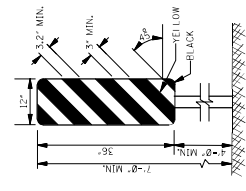


1. PLASTIC DRUMS SHALL BE ON END AND USED AS AN EXPEDIENT METHOD FOR TRAFFIC CHANNELIZATION. THE COLOR AND MARKING OF DRUMS SHALL BE CONSISTENT WITH MARKING STRIPING. THE PREFERRED COLOR OF DRUMS SHALL BE ORANGE WITH POLYESTER RETROREFLECTIVE, HORIZONTAL, CIRCUMFERENTIAL STRIPES (2 ORANGE & 2 WHITE) 6" WIDE.

2. DRUMS SHOULD NEVER BE PLACED IN THE ROADWAY WITHOUT WARNING SIGNS.

3. WHERE PRACTICAL PLASTIC DRUMS SHOULD BE PLACED NO CLOSER THAN 3'-0" FROM THE EDGE OF TRAVELED LANE.

TYPE 3 OBJECT MARKER (OM-3R)



1. TYPE 3 OBJECT MARKERS SHALL BE USED AT ALL EXPOSED BRIDGE ABUTMENTS AND AT OTHER LOCATIONS AS DEEMED NECESSARY BY THE ENGINEER.

2. THE OM-3R IS SIMILAR EXCEPT THE STRIPES SLOPE DOWNWARD FROM THE UPPER LEFT SIDE TO THE LOWER RIGHT SIDE AND SHALL BE PLACED ON THE LEFT SIDE OF THE OBJECT.

3. THE INSIDE EDGE OF THE MARKER SHALL BE IN LINE WITH THE INNER EDGE OF THE OBSTRUCTION.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

HIGHWAY SIGN AND BARRICADE DETAILS FOR CONSTRUCTION PROJECTS

REV	REVISION	DATE

ISSUE DATE: MAY 20, 2017

STATE MISS.	PROJECT NO.	
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MOBILE OPERATIONS ON MULTILANE ROAD

MOBILE OPERATIONS ON MULTILANE ROAD

NOTES FOR MULTILANE LANE OPERATION:

- VEHICLES USED FOR THESE OPERATIONS SHOULD BE MADE HIGHLY VISIBLE WITH APPROPRIATE EQUIPMENT, SUCH AS FLASHING LIGHTS, ROTATING BEACONS, FLASGS, SIGNS, OR ARROW PANELS.
- SHADOW VEHICLE 2 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK MOUNTED ATTENUATOR (TMA), AN APPROPRIATE LANE CLOSURE SIGN SHOULD BE PLACED ON SHADOW VEHICLE 2 SO AS NOT TO OBSCURE THE ARROW PANEL.
- SHADOW VEHICLE 1 SHOULD BE EQUIPPED WITH AN ARROW PANEL AND TRUCK MOUNTED ATTENUATOR (TMA).
- SHADOW VEHICLE 2 SHOULD TRAVEL AT A VARYING DISTANCE FROM THE WORK OPERATION SO AS TO PROVIDE ADEQUATE SIGHT DISTANCE FOR TRAFFIC APPROACHING FROM THE REAR.
- WHEN ADEQUATE SHOULDER WIDTH IS NOT AVAILABLE, SHADOW VEHICLE 2 SHOULD BE ELIMINATED.
- ON HIGH-SPEED ROADWAYS, A THIRD SHADOW VEHICLE SHOULD BE USED (i.e., VEHICLE 3 ON THE SHOULDER OF PRACTICALLY, VEHICLE 2 IN THE CLOSED LANE, AND VEHICLE 1 IN THE CLOSED LANE).
- ARROW PANELS SHALL BE AS A MINIMUM TYPE B, 60" X 36" IN ACCORDANCE WITH THE CRITERIA PRESENTED IN THE MUTCD.
- WORK SHOULD NORMALLY BE DONE DURING OFF-PEAK HOURS.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBTURED BY EQUIPMENT OR STRUCTURES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MOBILE OPERATIONS ON TWO-LANE ROAD

MOBILE OPERATIONS ON TWO-LANE ROAD

NOTES FOR TWO-LANE OPERATION:

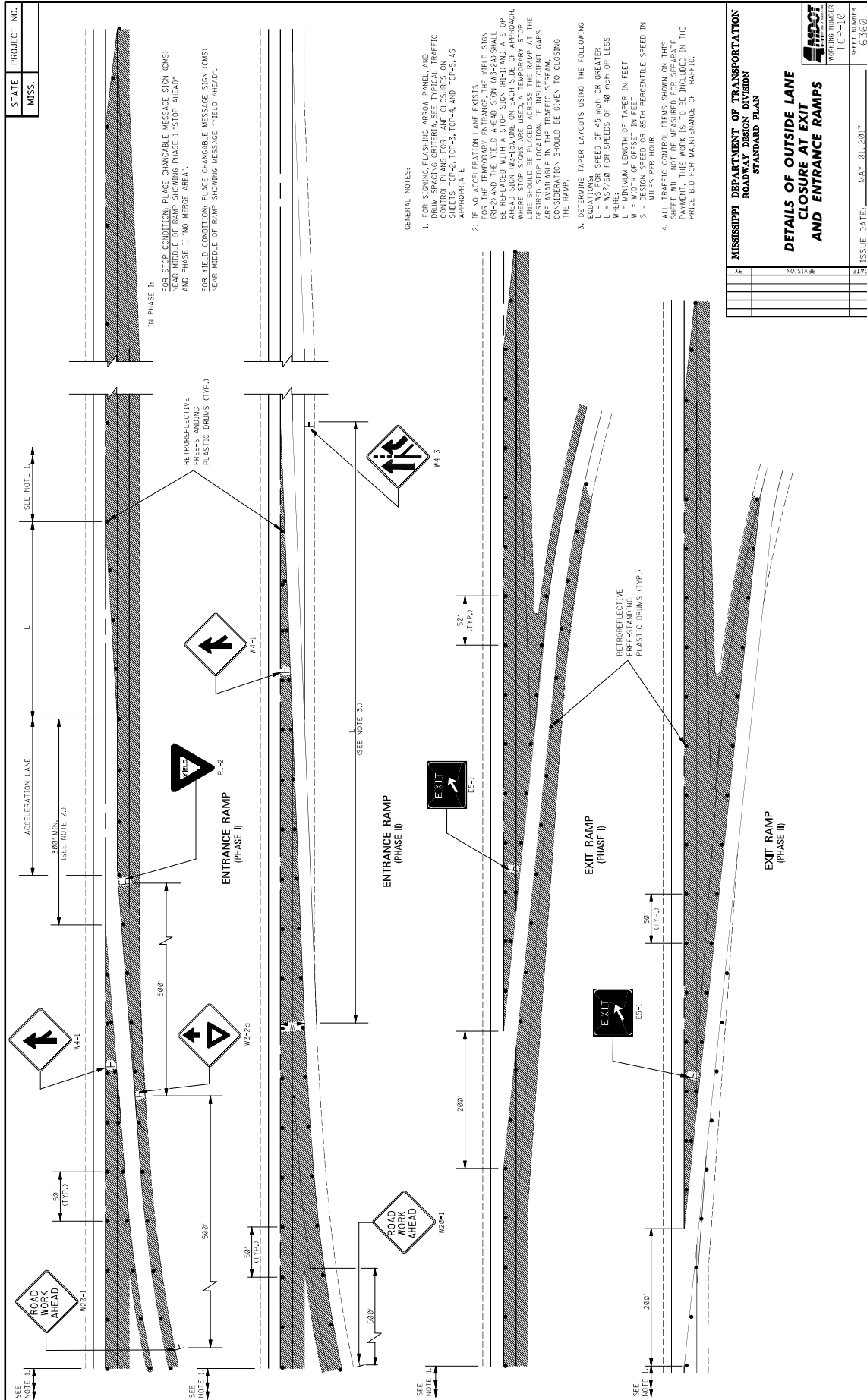
- WHERE PRACTICAL AND WHEN NEEDED, THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS. IF THIS CAN NOT BE DONE FREQUENTLY AS AN ALTERNATIVE, A "DO NOT PASS" SIGN MAY BE PLACED ON THE REAR OF THE VEHICLE BLOCKING THE LANE.
- THE DISTANCE BETWEEN THE WORK AND SHADOW VEHICLES MAY VARY ACCORDING TO TERRAIN, PAINT DRYING TIME, AND OTHER FACTORS. SHADOW VEHICLES ARE USED TO WARN TRAFFIC OF THE OPERATION AHEAD. WHENEVER ADEQUATE SHOULD MAINTAIN THE MINIMUM DISTANCE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. THE SHADOW VEHICLE SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ADDITIONAL SHADOW VEHICLES TO WARN AND REDUCE THE SPEED OF ONCOMING OR OPPOSING TRAFFIC MAY BE USED. POLICE PATROL CARS MAY BE USED FOR THIS PURPOSE.
- A TRUCK-MOUNTED ATTENUATOR (TMA) SHOULD BE USED ON THE SHADOW VEHICLE AND MAY BE USED ON THE WORK VEHICLE.
- THE WORK VEHICLE SHALL BE EQUIPPED WITH BEACONS AND THE SHADOW VEHICLE SHALL BE EQUIPPED WITH BEACONS AND LIGHTS. TRUCK MOUNTED LIGHTS MOUNTED ON THE REAR, ADJACENT TO THE SIGN, SHADOW AND WORK VEHICLES SHALL DISPLAY FLASHING OR ROTATING BEACONS BOTH FORWARD AND TO THE REAR.
- VEHICLE-MOUNTED SIGNS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGN LOCATED AT A MINIMUM HEIGHT OF 48" ABOVE THE PAVEMENT AND SHALL NOT BE OBTURED BY EQUIPMENT OR STRUCTURES. SIGN LEGENDS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
- ARROW BOARD TO BE USED IN CAUTION MODE.
- ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT. THIS WORK IS TO BE INCLUDED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

TRAFFIC CONTROL PLAN
MOBILE OPERATIONS
MULTILANE ROADS
TWO-LANE ROADS

ISSUE DATE: MAY 01, 2017

REV	REVISION	SHEET NUMBER
		CP-9
		6359



GENERAL NOTES:

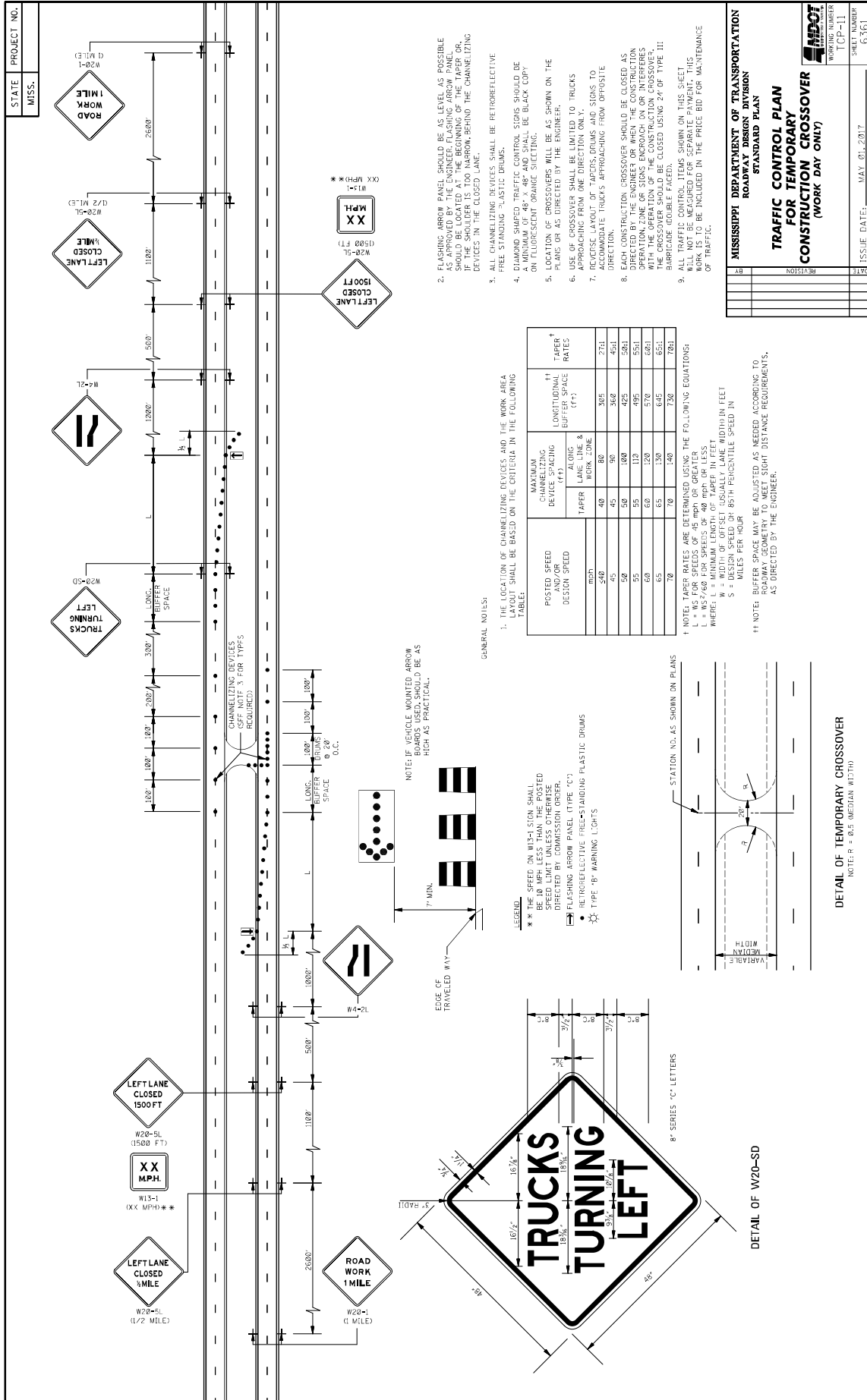
- FOR SIGNING FLASHING ARROW PANEL AND DRUM SPACING CRITERIA, SEE TYPICAL TRAFFIC CONTROL PLANS FOR LANE CLOSURES ON RAMP ENTRIES, TCS-3, TCS-4, AND TCS-5, AS APPROPRIATE.
- IF NO ACCELERATION LANE EXISTS FOR THE TEMPORARY ENTRANCE, THE YIELD SIGN (R4-2) AND THE YIELD AHEAD SIGN (R4-3) SHALL BE REPLACED WITH A STOP SIGN (R4-1) AND A STOP LINE SHOULD BE PLACED ACROSS THE RAMP AT THE DESIRED STOP LOCATION. INSUFFICIENT GAPS FOR TRAFFIC TO ENTER THE RAMP AT THE CONSIDERATION SHOULD BE GIVEN TO CLOSING THE RAMP.
- DETERMINE TAPER LAYOUTS USING THE FOLLOWING EQUATIONS: SPEEDS OF 45, 60, OR GREATER
 $L = W^2/60$ FOR SPEEDS OF 40, 45, OR LESS
 WHERE:
 L = MINIMUM LENGTH OF TAPER IN FEET
 W = DESIGN SPEED OF 85TH PERCENTILE SPEED IN MILES PER HOUR
 4. ALL TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET WILL NOT BE MEASURED FOR SEPARATE PAYMENT UNLESS OTHERWISE NOTED IN THE PRICE BID FOR MAINTENANCE OF TRAFFIC.

STATE	PROJECT NO.
MISS.	101
ISSUE DATE:	MAY 01, 2017
SHEET NUMBER	65300
WORKING NUMBER	TCP-110

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN

**DETAILS OF OUTSIDE LANE
 CLOSURE AT EXIT
 AND ENTRANCE RAMP**

NO.	REVISION	DATE
1	REVISION	01/17



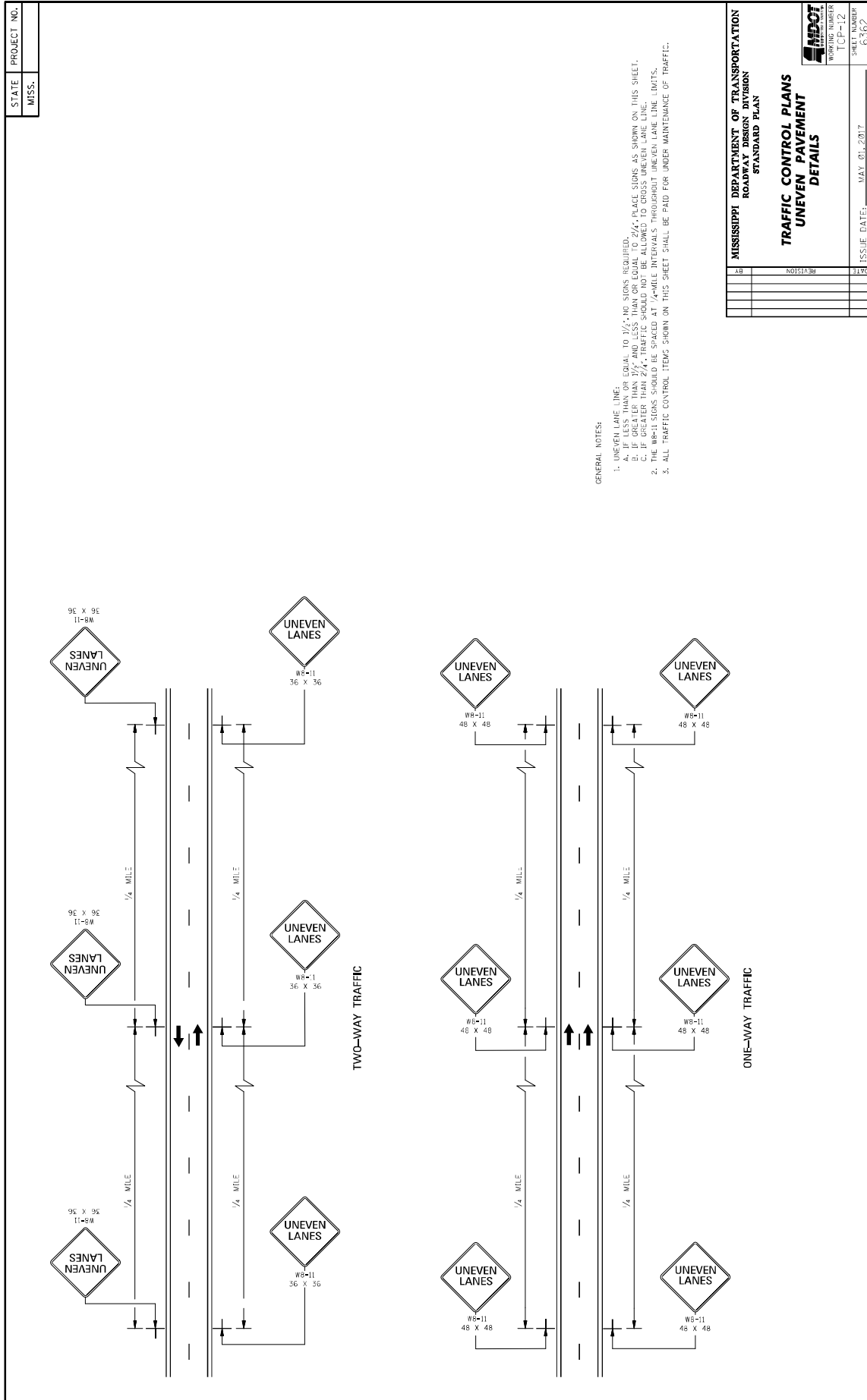
STATE PROJECT NO.
MISS.

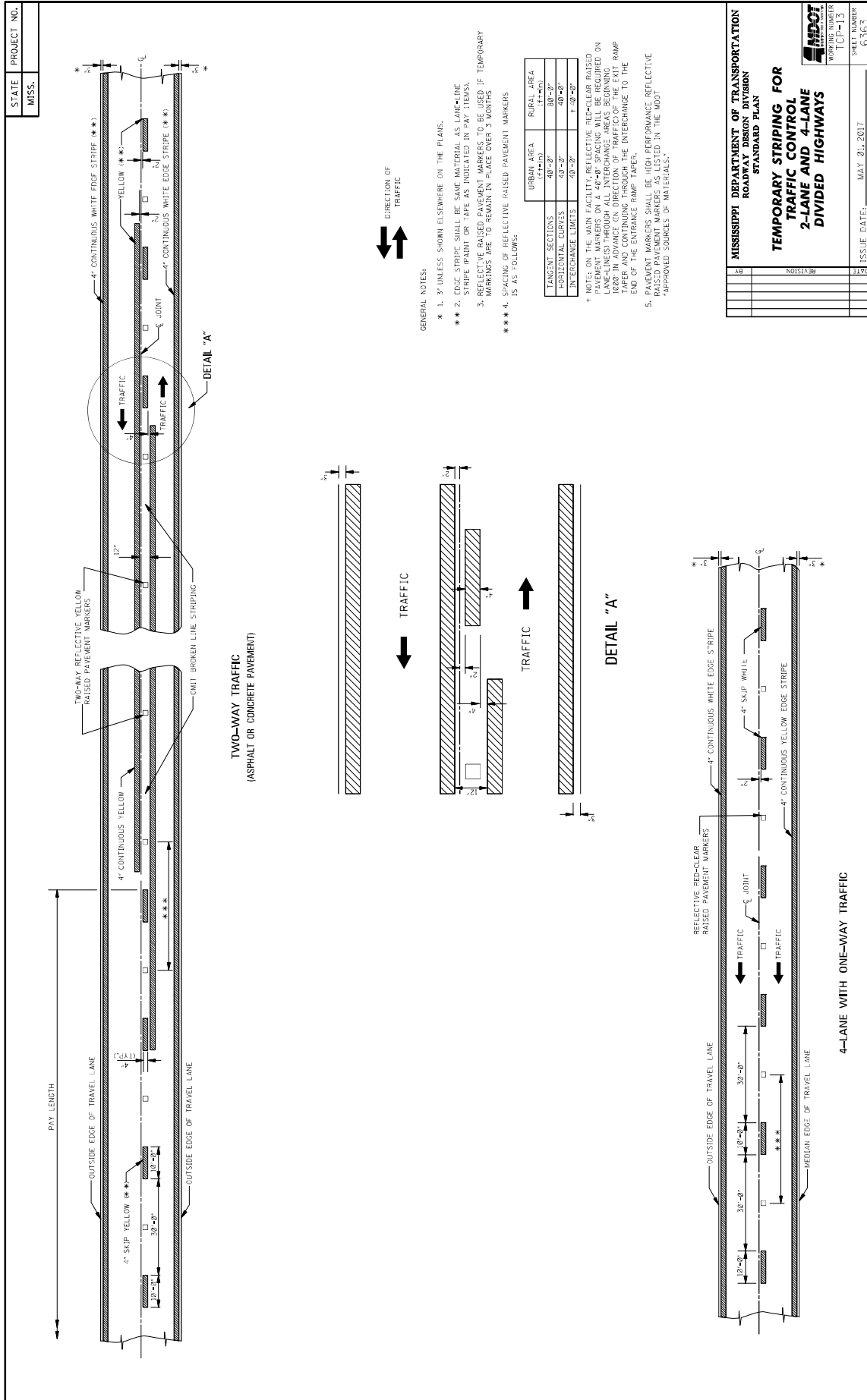
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
 ROADWAY DESIGN DIVISION
 STANDARD PLAN
**TRAFFIC CONTROL PLAN
 FOR TEMPORARY
 CONSTRUCTION CROSSOVER
 (WORK DAY ONLY)**

WORKING NUMBER
TCP-11

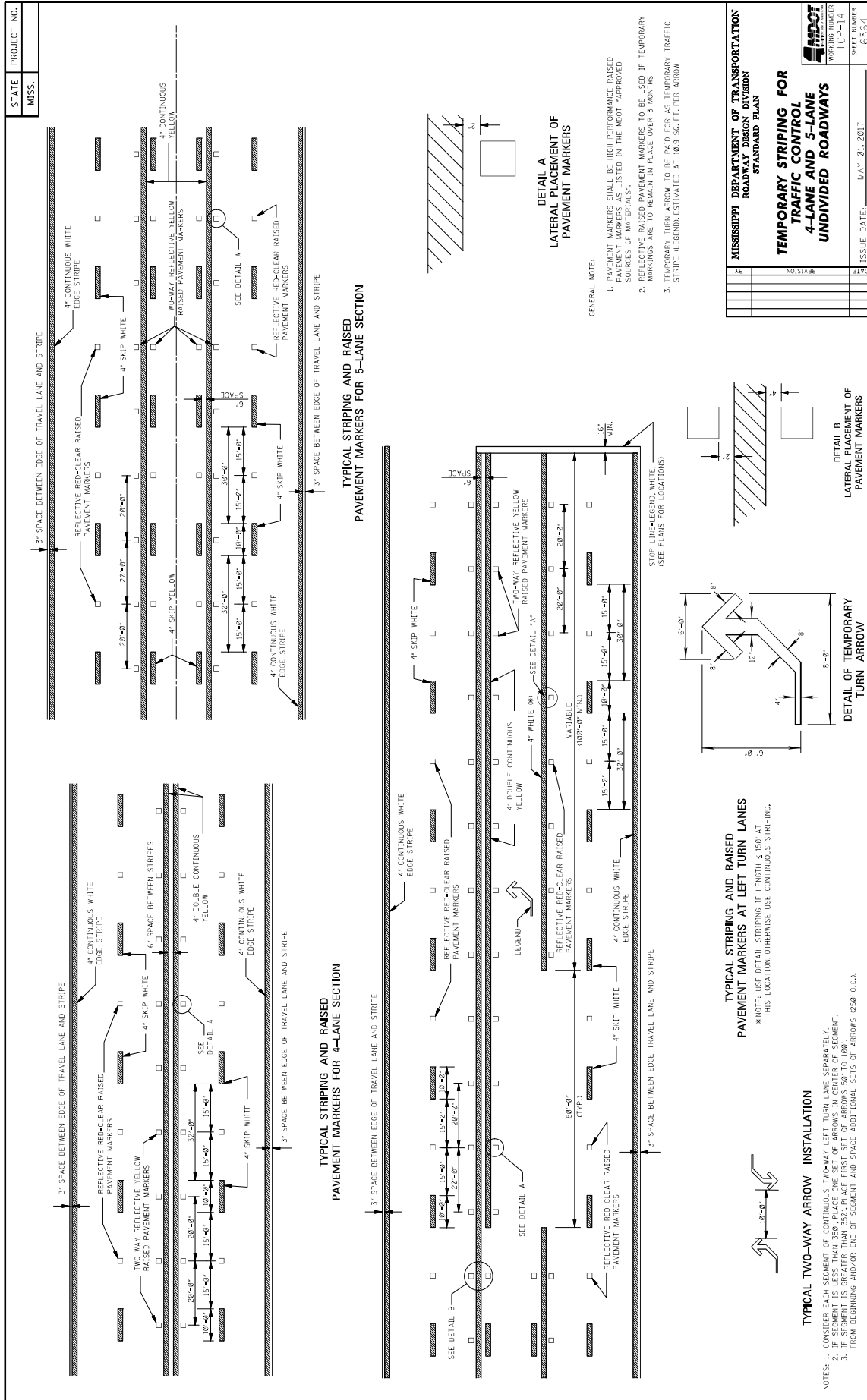
SHEET NUMBER
6361

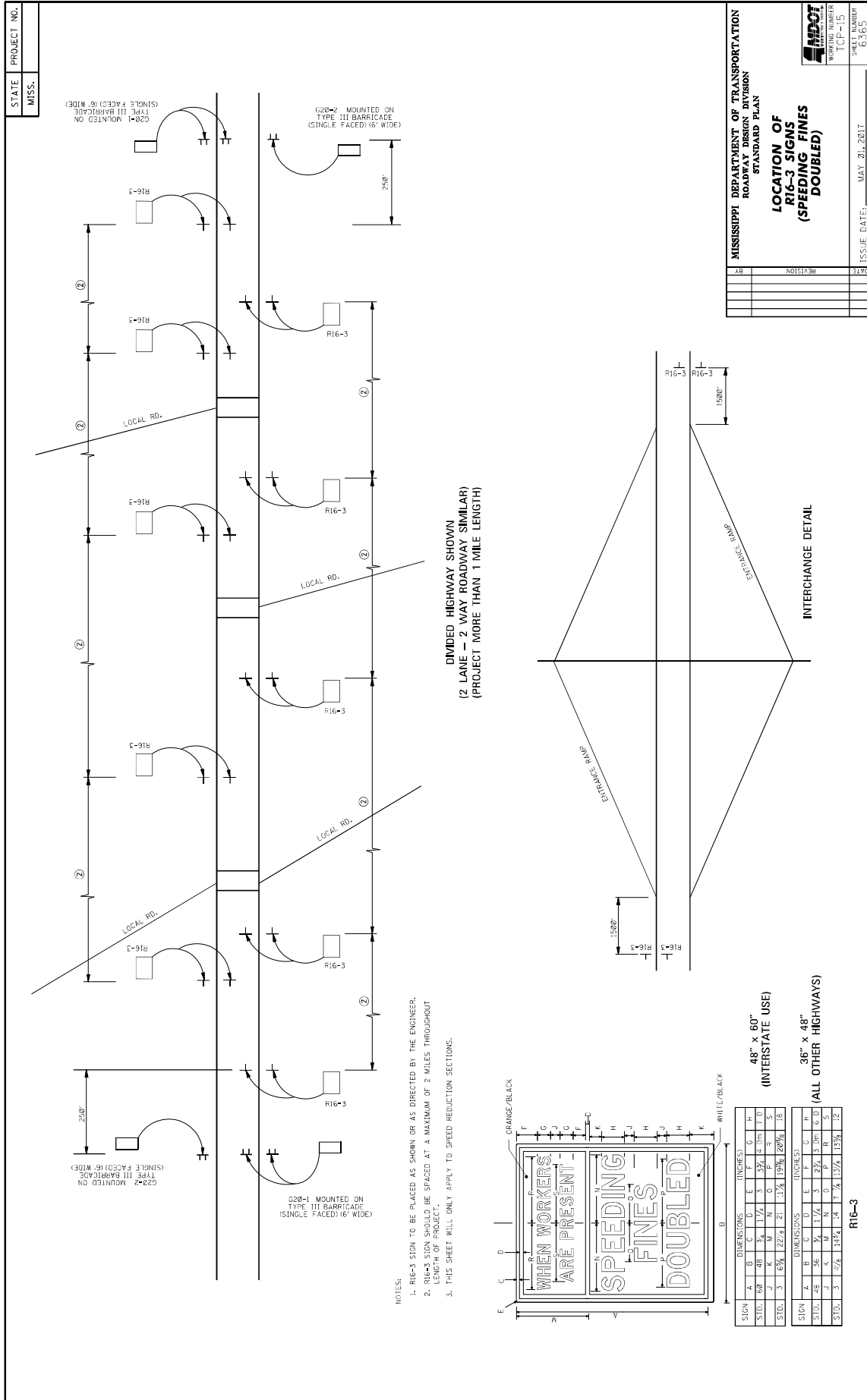
ISSUE DATE: MAY 01, 2017



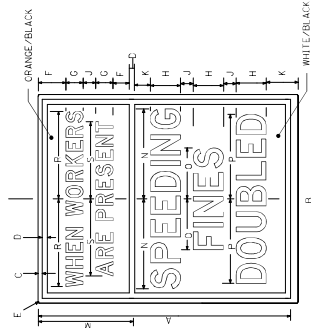


MISSISSIPPI DEPARTMENT OF TRANSPORTATION ROADWAY DESIGN DIVISION STANDARD PLAN	MISSISSIPPI DEPARTMENT OF TRANSPORTATION WORKING NUMBER [CP-13]
TEMPORARY STRIPING FOR 2-LANE AND 4-LANE DIVIDED HIGHWAYS	SHEET NUMBER 6363
REVISION	ISSUE DATE: MAY 01, 2017





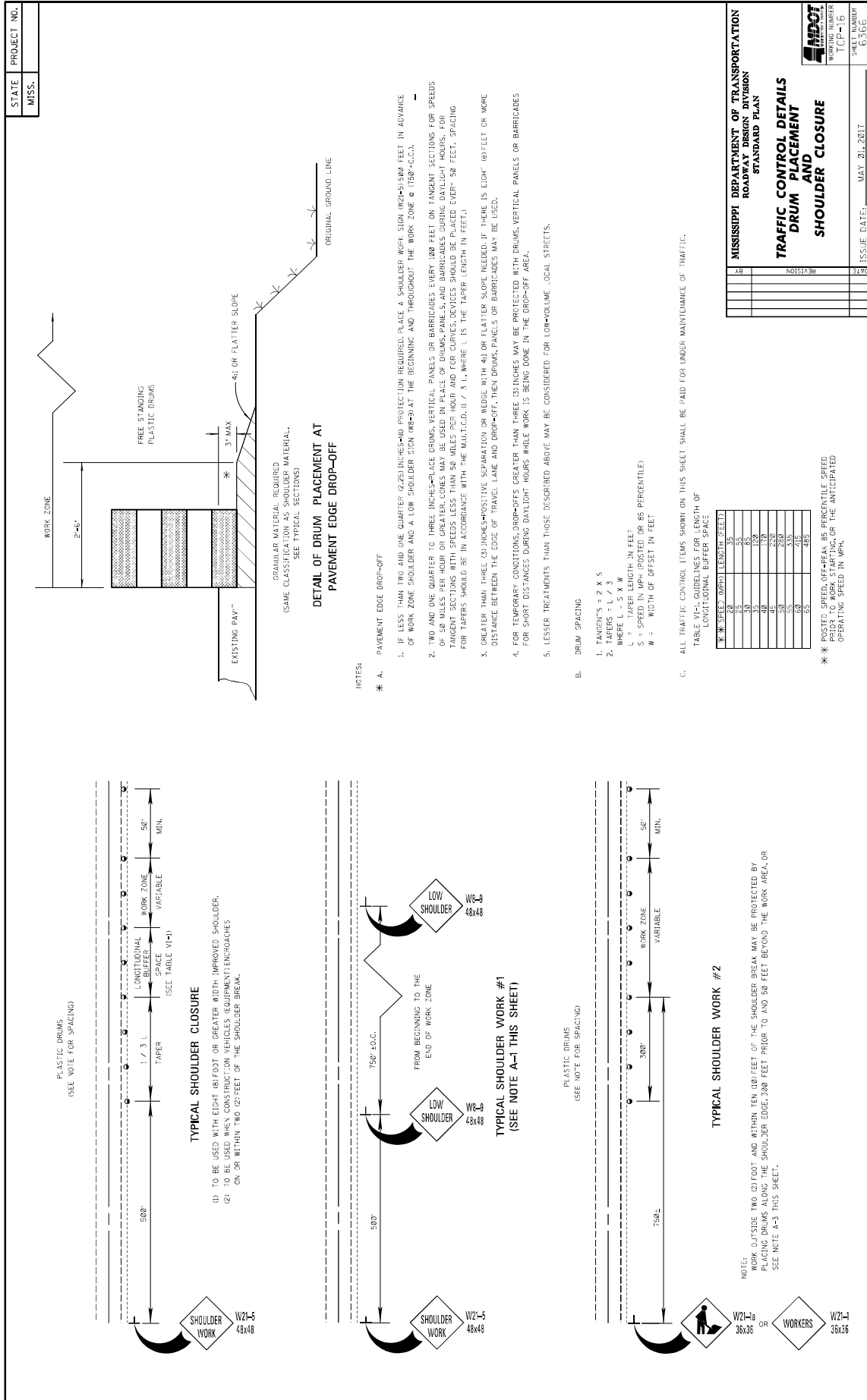
- NOTES:
1. R16-3 SIGN TO BE PLACED AS SHOWN OR AS DIRECTED BY THE ENGINEER.
 2. R16-3 SIGN SHOULD BE SPACED AT A MAXIMUM OF 2 MILES THROUGHOUT LENGTH OF PROJECT.
 3. THIS SHEET WILL ONLY APPLY TO SPEED REDUCTION SECTIONS.



SIGN		DIMENSIONS (INCHES)											
A	B	C	D	E	F	G	H	I	J	K	L	M	N
STD.	6-00	48	36	1 1/2	5	3 1/2	4	3/4	1 1/4	1 3/8	2 1/2	1 1/2	1 3/8
STD.	3	1 5/8	1 1/2	1 1/4	1 1/4	1 3/8	2 1/2	1 1/2	1 3/8	1 3/8	1 3/8	1 3/8	1 3/8

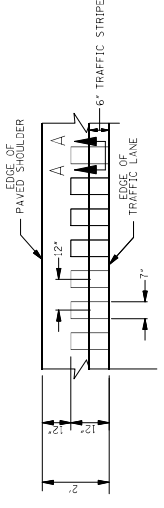
SIGN		DIMENSIONS (INCHES)											
A	B	C	D	E	F	G	H	I	J	K	L	M	N
STD.	3	36	24	1 1/2	3	2 1/4	3	3/4	1 1/4	1 3/8	2 1/2	1 1/2	1 3/8
STD.	3	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4	1 1/4

R16-3

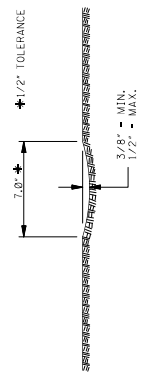


GENERAL NOTES

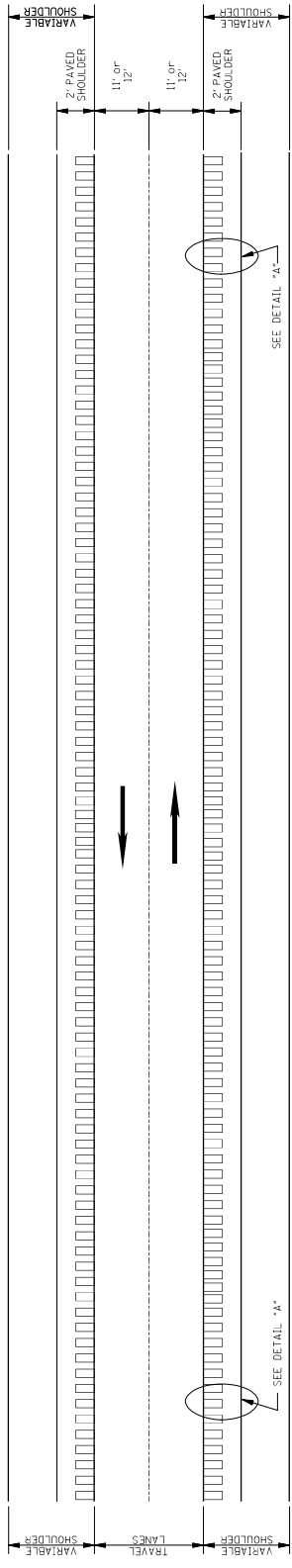
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL PAVED SHOULDERS OF ALL PAVED SHOULDERS ON THIS PROJECT.
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL EXISTING AND NEW ASPHALT ROADWAYS, INCLUDING INTERSECTIONS, NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER.
- COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS.
- GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
 - MAINLINE
 - INTERSECTING ROADWAY IF OVERLAD OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.W.
 - ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.
- DO NOT USE WHERE TRAVEL LANE IS LESS THAN 11' WIDE.



DETAIL "A"



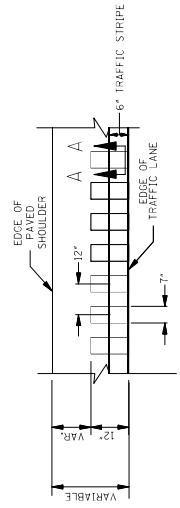
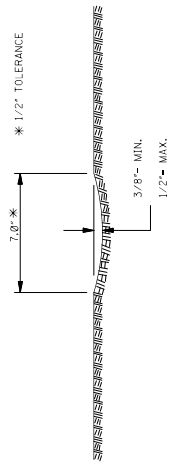
SECTION "A-A"



PLAN
NOT TO SCALE

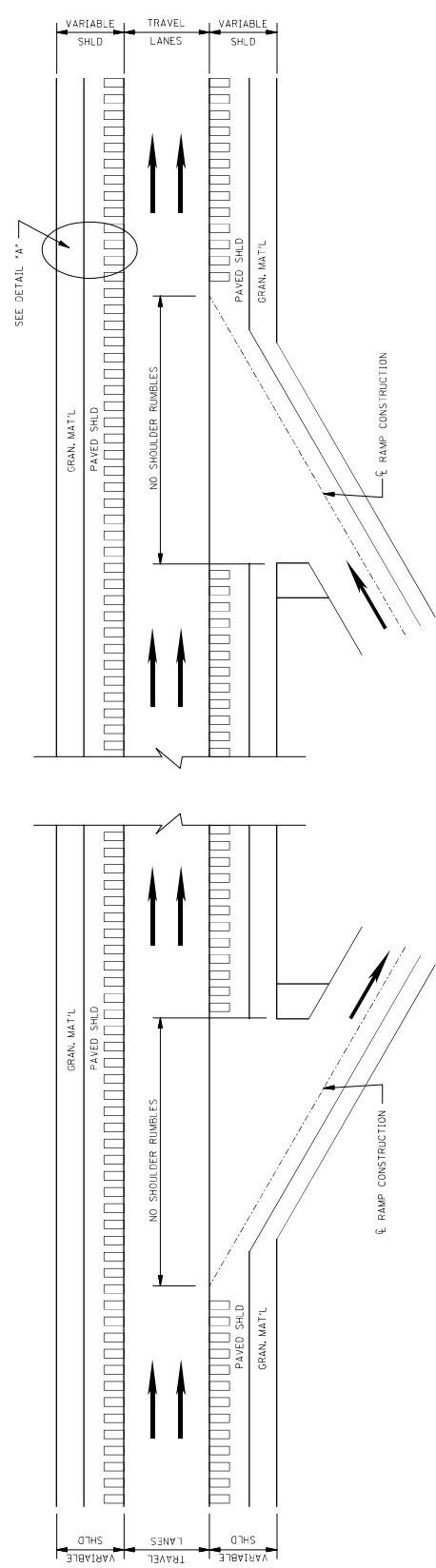
GENERAL NOTES

1. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL PAVED SHOULDERS AND ALL PAVED SHOULDERS ON THIS PROJECT.
2. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO ALL INTERSECTIONS, ROADWAYS, AND OTHER INTERSECTIONS IN NORMAL SHOULDER WIDTH AS DIRECTED BY THE ENGINEER.
3. COST TO BE PAID FOR USING APPROPRIATE PAY ITEMS.
4. GROUND-IN RUMBLE STRIPES SHALL BE APPLIED TO:
 - A. MAINLINE
 - B. INTERSECTING ROADWAY IF OVERLAP OR RECONSTRUCTED BEYOND NORMAL MAINLINE R.O.W.
 - C. ANY ROADWAY WITH EXISTING RUMBLE STRIPES PRIOR TO CONSTRUCTION.



SECTION "A-A"

DETAIL "A"



PLAN
NOT TO SCALE

DETAILS OF RUMBLE STRIPES

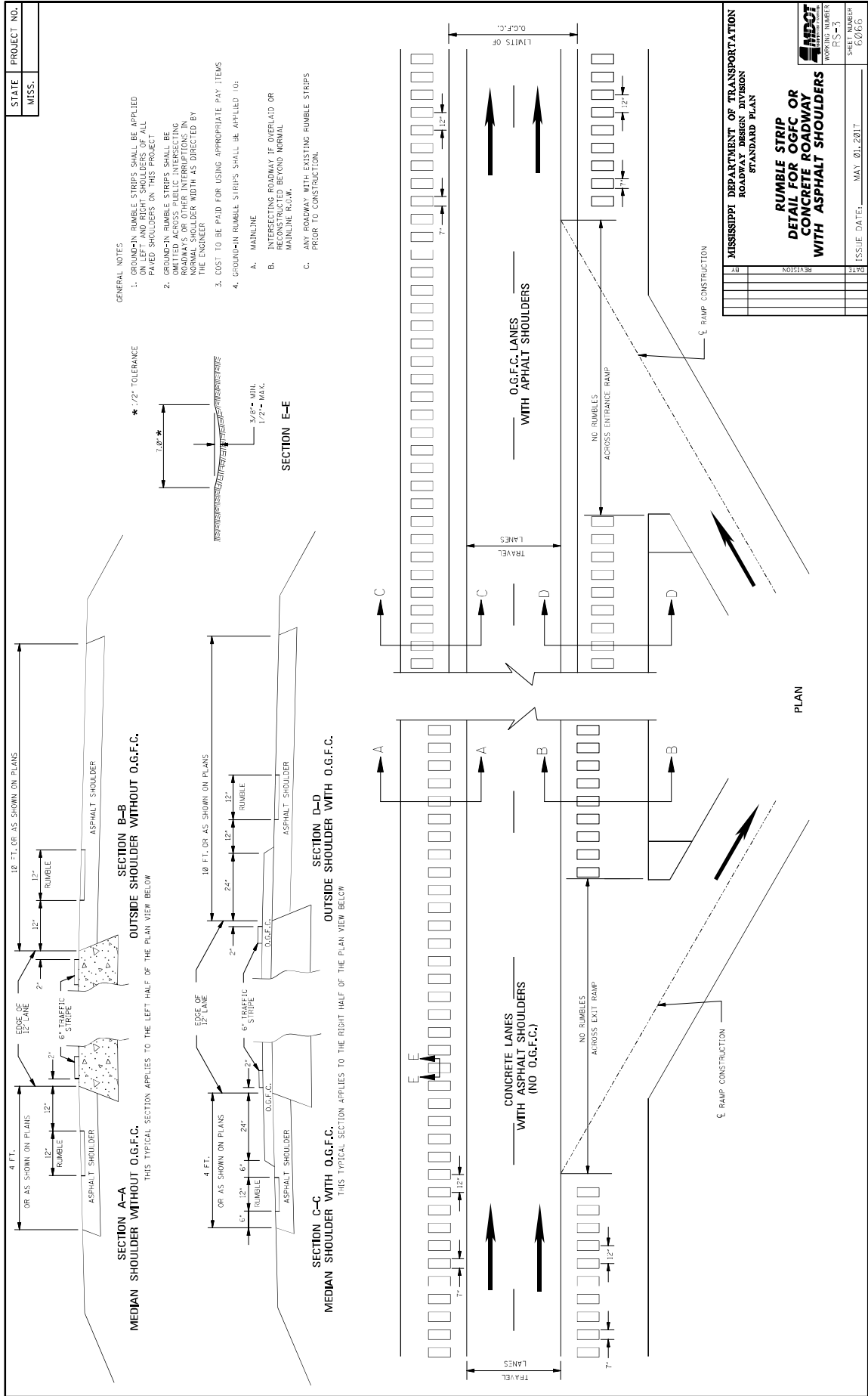
MISSISSIPPI DEPARTMENT OF TRANSPORTATION
ROADWAY DESIGN DIVISION
STANDARD PLAN

**RUMBLE STRIPES
4-LANE HIGHWAYS
(ASPHALT LANES,
2-FT OR WIDER
ASPHALT SHOULDERS)**

DATE	REVISION
05/08	05/08

ISSUE DATE: AUGUST 01, 2017

PROJECT NUMBER: 3599-2
DRAWING NUMBER: 6065



MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 3676

CODE: (SP)

DATE: 09/21/2021

SUBJECT: Asphalt Gyrotory Compactor Internal Angle Calibration

Bidders are advised that by March 1, 2022, all asphalt gyrotory compactors shall be calibrated to an internal angle of $1.16^{\circ} \pm 0.02^{\circ}$. This requirement will be reflected in updates made to MT-78, MT-80, and MT-83. This calibration requirement also extends to all QC/QA testing.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 4702

CODE: (SP)

DATE: 11/22/2022

SUBJECT: App for Traffic Control Reports

Bidders are advised that the Department has created a smart phone App for completing and submitting traffic control reports (Form CSD-762) required on this project. The Contractor who monitors traffic control activities and completes traffic control reports will be required to download and use this App when completing and submitting traffic control reports. The reports will then be readily available to all persons who need access to the forms. The App is free and is available for downloading at the following location.

<https://extacctmgmt.mdot.state.ms.us/>

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 5086

CODE: (SP)

DATE: 05/02/2023

SUBJECT: Detail of Square Tube Sign Posts

Bidders are advised that the following drawings shall be used in the manufacture and installation of square tube sign posts, unless otherwise directed by the Engineer.

STATE PROJECT NO.

MMBB

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SIGN SUPPORT HARDWARE

2 1/2" SQUARE POST

PROJ. NO.: TSS-1

COUNTY: TSS-1

FILE # 155-1-0001

DATE: MAY 2003

FLAT PANEL SIGN BRACE

6061-T6 ALUMINUM ALLOY;
7/8" DEPTH x 1/8" NOMINAL
WALL THICKNESS. BRACE
CENTERED ALONG ENTIRE
LENGTH OF THE BRACE. A
MINIMUM OF TWO HORIZONTAL
BRACES MUST BE USED.

SIGN DRIVE RIVET W/WASHER

3/8" ALUMINUM SHELL / STEEL CORE RIVET
MUST BE PRE-INSTALLED AND FIT INTO
SIGN BRACE TO SECURE ALUMINUM SIGN
PLATE. USE 3/16" x 2 1/2" GRIP RANGE ALUMINUM POST
RIVETS (#46).

FLAT PANEL SIGN CLAMP

EXTRUDED PANEL SIGN CLAMP

POST CAP

SIGN POSTS

2 3/16" 10ga 8" INSERT
WHERE REQUIRED

example slip
base assembly
(detailed below)

2-1/2" SQUARE POST RECEIVER

LOCKING WEDGE FORGED
FROM SAE 1045 STEEL AND
GALVANIZED PER ASTM A153.

POST LOCKING WEDGE

MADE OF 10GA REGALVANIZED
STEEL WITH AN ID CAPABLE OF
FITTING INTO THE ID OF A
2 1/2" SQUARE POST WITHOUT
ANY FASTENERS. A
COP ALUMINUM FRICTION FIT
CAP MAY ALSO BE USED.

UNIBASE ANCHOR STUB

38" UNIBASE ANCHOR
A500 GRADE B TUBE
WELDED TO A872
GALVANIZED PER
ASTM A153.

SURFACE MOUNT BASE

A572 SLIP PLATE WELDED TO A500 GRADE B TUBE
WELDED TO A872 MOUNTING PLATE. GALVANIZED
PER ASTM A153.

SLIP PLATE DIMENSION

RECEIVER TO BE CAST
FROM DUCTILE IRON
CLASS 42
GALVANIZED PER ASTM A153.

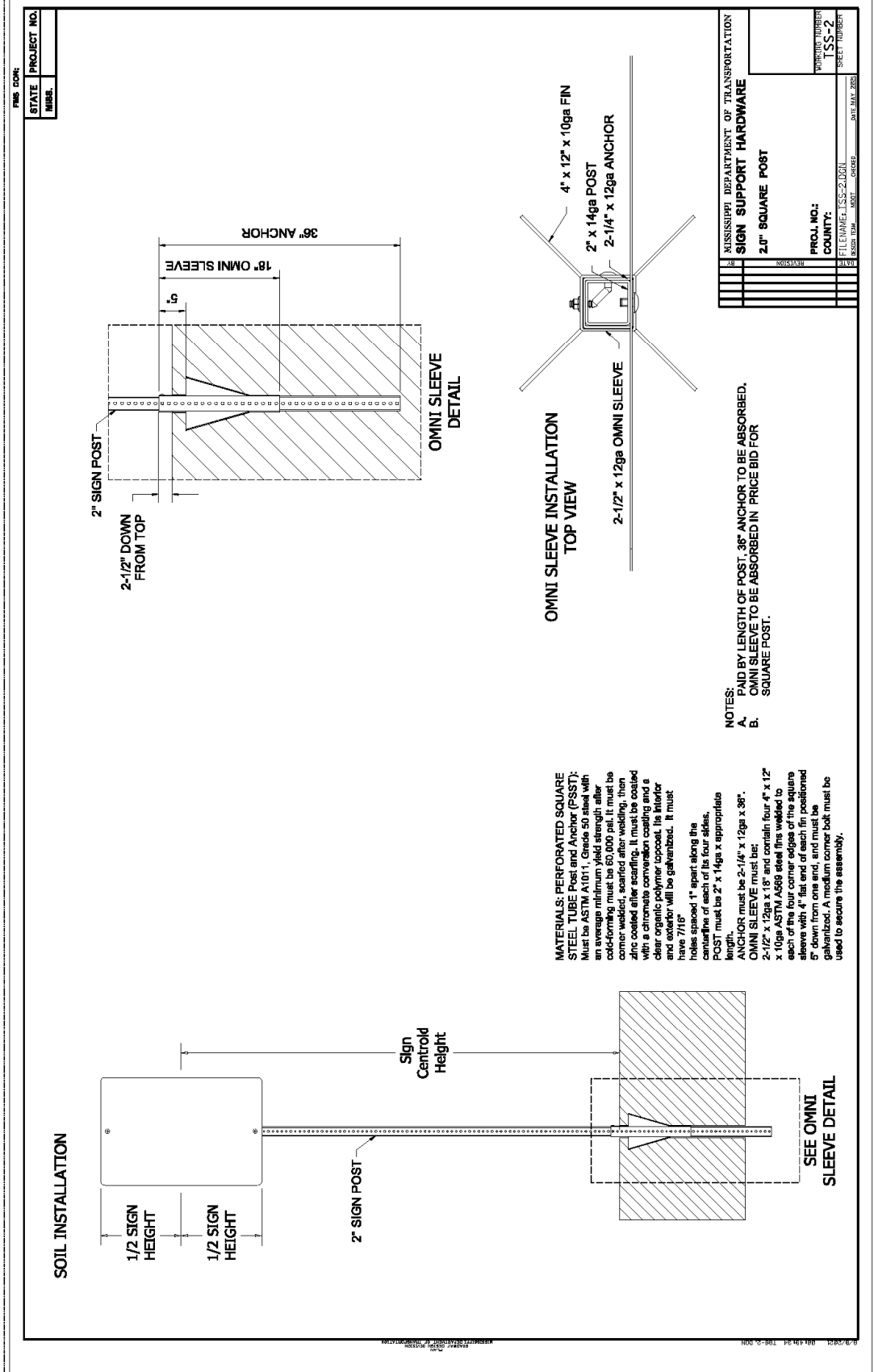
NOTES:

A. CONCRETE FOOTING MUST BE 12" DIAMETER x 36" DEEP.
BRASS SHIMS MAY BE USED BETWEEN SLIP PLATES TO
LEVEL THE UPPER SLIP PLANE.
C. SLIP BASE IS FHWA ACCEPTED, MEETING CURRENT
AASHTO & NCHRP 350 REQUIREMENTS.
D. SIGN BRACE CLAMPS ALLOW SIGN BRACE TO BE
UP, DOWN, LEFT OR RIGHT IN ORDER TO ACHIEVE
PERFECT POSITION OF SIGN PANEL.
E. USE INSTALLATION INSTRUCTION RT280U-SQ1.
F. PRODUCT MUST MEET FHWA'S MADE IN AMERICA CLAUSE.
G. PAID BY LENGTH OF POST, 38" STUB TO BE ABSORBED.

SIGN CENTROID	2-1/2" x 10ga POST	2-1/2" x 10ga & 2-3/4" x 10ga HEATED POST	Sign Bracing**
16'	24.82	43.46	18" maximum distance between braces
15'	26.48	46.36	18" maximum distance between braces
14'	28.38	49.66	18" maximum distance between braces
13'	30.56	53.48	18" maximum distance between braces
12'	33.10	57.94	18" maximum distance between braces
11'	36.12	63.20	18" maximum distance between braces

** based on two legs per sign
** Minimum of two braces per flat panel sign

NOTE: Sign area is in square feet.



FMS CON:	STATE PROJECT NO.				
MISS.					

SIGN CLAMP KIT

SIGN CLAMP KIT

MATERIALS: 2" WIDE x 3/4" TYPE 304, A2B FINISHED STAINLESS STEEL BRACKET. INCLUDES 2 STAINLESS THREADED BOLTS AND SERRATED FLANGED NUTS. ONE CLAMP REQUIRED FOR EACH BRACE TO POST JUNCTION.

SIGN POSTS

SIGN POSTS

STEEL CONFORMS TO ASTM A500, Grade C. MUST BE WELDED, SCARFED OUTSIDE AFTER WELDING, THEN ZINC FLOCCED AND COATED AFTER SCARFING. MUST BE COATED WITH A CHROMATE CONVERSION COATING & CLEAR ORGANIC POLYMER TOPCOAT. POST WEIGHT IS 6.1 LB/FT.

UNIBASE ANCHOR STUB

UNIBASE ANCHOR STUB

4-1/2" x 54" x 7 GA UNIBASE ANCHOR AS90 GRADE B TUBE WELDED TO 1" THICK A572 SLIP PLATE. GALVANIZED PER ASTM A563.

SLIP BASE ASSEMBLY

SLIP BASE ASSEMBLY

13" OMNIDIRECTIONAL SLIP BASE RECEIVER FOR 4" SQUARE POST. ALSO INCLUDES HARDWARE TO 1" THICK A572 SLIP PLATE. GALVANIZED PER ASTM A563.

4" SQUARE POST RECEIVER

4" SQUARE POST RECEIVER

13" OMNIDIRECTIONAL SLIP BASE RECEIVER FOR 4" SQUARE POST. ALSO INCLUDES HARDWARE TO 1" THICK A572 SLIP PLATE. GALVANIZED PER ASTM A563.

4" SQUARE POST RECEIVER

4" SQUARE POST RECEIVER

13" OMNIDIRECTIONAL SLIP BASE RECEIVER FOR 4" SQUARE POST. ALSO INCLUDES HARDWARE TO 1" THICK A572 SLIP PLATE. GALVANIZED PER ASTM A563.

POST ATTACHMENT HARDWARE KIT

POST ATTACHMENT HARDWARE KIT

POST ATTACHMENT HARDWARE KIT WILL CONSIST OF:
 (3) 5/8" x 1 1/4" x 5-3/4" HEX BOLTS - A325
 (3) 5/8" x 1-3/4" AR400 WASHER
 (3) 5/8" x 1-3/8" AR400 WASHER
 (3) 5/8" HEX NUTS - A563D8
 ALL NOT DIP GALVANIZED per ASTM A153 F2028.
 NOTE: 1 POST ATTACHMENT HARDWARE KIT IS USED FOR SLIP BASE TOP POST RECEIVER.
 TORQUE TO 100 FT-LBS.

SLIP BASE MATCH PLATE HARDWARE KIT

SLIP BASE MATCH PLATE HARDWARE KIT

3/4" x 10 A307 GRADE 8 DOUBLE HEADER FLANGE COATED HARDENED SLIP WASHER (8 EACH PER KIT)
 7/8" U.S.G. FLAT WASHER A307 (8 EACH PER KIT)
 2-1/4" SQUARE x 3/4" THICK TIE-BOLN COATED, HARDENED SLIP WASHER (8 EACH PER KIT)
 3/4" x 10 GRADE 8 LARGE FLANGE NUT GALVANIZED PER ASTM A563, Class 90 (8 EACH PER KIT)
 SLIP BASE MATCH PLATE HARDWARE KIT (SLIP BASE HARDWARE TORQUED IS 60 FT-LBS.)

NOTES:

- A. CONCRETE FOOTING MUST BE A MINIMUM OF 18" DIAMETER x 54" DEEP.
- B. BRASS SHIMS MAY BE USED BETWEEN SLIP PLATES TO LEVEL THE UPPER SLIP PLANE.
- C. MAXIMUM SIGN AREA PER SIGN BRACKET TO POST JUNCTION IS 10FT².

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SIGN SUPPORT HARDWARE

4" SQUARE POST (SINGLE POST)

PROJ. NO.:
COUNTY:

DRAWN BY: _____ DATE: _____
 CHECKED BY: _____ DATE: _____
 DESIGNED BY: _____ DATE: _____

WORKING DRAWING
TSS-3
SHEET NUMBER

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 5500

CODE: (SP)

DATE: 12/12/2023

SUBJECT: Contract Time

PROJECT: SP-9507-00(002) / 109442301 -- Leflore County

The completion of work to be performed by the Contractor for this project will not be a specified date but shall be when all allowable working days are assessed, or any extension thereto as provided in Subsection 108.06. It is anticipated that the Notice of Award will be issued no later than **February 13, 2024**.

The Contractor shall request a Notice to Proceed/Beginning of Contract Time date between the dates of the **March 14, 2024 to June 13, 2024**

Should the Contractor request a Notice to Proceed earlier than **March 14, 2024** and it is agreeable with the Department for an early Notice to Proceed, the requested date will become the new Notice to Proceed and Beginning of Contract Time date.

Should the Contractor not request a Notice to Proceed by **June 13, 2024**, the date for the Notice to Proceed and Beginning of Contract Time will be **June 13, 2024**.

All requests for an early Notice to Proceed shall be sent to the Project Engineer who will forward it to the Contract Administration Division.

61 Working Days have been allowed for the completion of work on this project.

The progress schedule for this project shows the Notice to Proceed and Beginning of Contract Time starting at the latest possible date. If the Contractor requests a Notice to Proceed earlier than this date, the Contractor shall submit a revised progress schedule showing the work beginning at the new Notice to Proceed and Beginning of Contract Time date.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 5501

CODE: (SP)

DATE: 09/13/2023

SUBJECT: Scope of Work

PROJECT: SP-9507-00(002) / 109442301 -- Leflore County

The contract documents do not include an official set of construction plans but may, by reference; include some Standard Drawings when so specified in a Notice to Bidders entitled, “Standard Drawings”.

The work to be accomplished using the pay items and corresponding specifications set forth in this contract is to mill and overlay Highway 430 beginning at US Highway 82 (MP: 0.000) and going south for approximately 4.2 miles to the Carroll County Line (MP:4.244) in Leflore County.

Section	Description	Termini				
		MP	Station	MP	Station	Length
1	5-Lane	0.000	0+00	0.578	30+54	3,054
2	3-Lane	0.578	30+54	1.610	85+00	5,446
3	2-Lane	1.610	85+00	4.244	225.52	14,052

Omitted Section (Bridge over Pelucia Creek), Sta 182+83 to Sta. 193+45, Length 1,062

It shall be the responsibility of the Contractor to protect the roadway and all existing structures, such as bridges and curb, from damage occurring as a result of the Contractor’s operations. Damages to existing features caused by the Contractor’s operations shall be repaired or replaced at no cost to the Department.

At bridge ends and at the end of workday, a taper of one (1) vertical inch for each three (3) horizontal feet shall be provided.

The Contractor shall make a utility location request to 811 prior to any excavation, except for trench widening or pavement removal/repair.

In order to expedite the safe movement of traffic and to protect each phase of the work as it is performed, a firm sequence of operations is essential. The work shall be begun and continually prosecuted.

The work shall consist of the following:

1. Failed areas shall be repaired using the following:
 - 202-B, Removal of Asphalt Pavement, All Depths – for pavement structure.
 - 203-G, Excess Excavation – for material below the pavement structure

- 304-F, Crushed Stone – to be used to replace unsuitable material below the 1-foot limit
- 403-A, 19-mm, ST, Asphalt Pavement
- 503-C, Saw Cut, Full Depth

NOTE: Failed areas are estimated as one foot (1’) of excavation and backfilled with one foot (1’) (maximum 3½” lifts) of 19-mm, ST, asphalt. The asphalt shall be placed per the Project Engineer’s instructions.

NOTE: Failed areas are to be backfilled the same day as excavation.

Station	Side		Width (feet)	Length (feet)	Asphalt Area (SY)	Sawcuts (LF)
	Rt.	Lt.				
85+00	x		11	175	213.9	197
85+00		x	6	10	22	6.7
107+50		x	6	100	66.7	112
156+00		x	6	20	13.3	32
165+00	x		11	50	61.1	72
168+50	x		6	10	6.7	22
175+00	x	x	22	40	97.8	84
225+25	x		6	20	13.	32
Total					479.4	573

The above areas were noted during the field inspection. Additional areas may require repair during construction.

2. The existing asphalt pavement shall be fine milled to a depth of one and one half inches (1½”). Milling operations shall be on the mainline in Sections 1 and 2 only, at CR 512, bridge ends, and EOP tie in. The Department will retain fifty (50%) percent of the milling material. The Contractor shall deliver the milling material to the MDOT Leflore County Maintenance Shop located at 6771 Highway 49 South in Greenwood. The Contractor shall provide all necessary equipment and qualified personnel to push material into a suitable stockpile.

Area	SY
Mainline	
Section 1	20,360
Section 2	21,784
CR 512	1,785
Bridge Ends	800
EOP Tie In	400
Total	45,128

Payment for fine milling of pavement will be made under pay item 406-D and shall include all cost associated with the milling operation.

NOTE: Milled surfaces shall be covered with surface asphalt within seven (7) calendar days of removal. The Contractor will be charged a fee of \$5,000.00 for each full or partial day in which the milled surface is left uncovered after the seven (7) calendar days.

NOTE: During this operation and prior to placement of the asphalt, due care shall be required to keep surface water from ponding on the roadway surface; continuous monitoring of the project may be required.

NOTE: During this operation and prior to placement of the asphalt, Contractor shall repair and maintain all potholes.

- 3. The pavement edges shall be excavated three inches (3”) deep and two feet (2’) wide for trench widening in Section 3 only. The overall pavement width shall be twenty-six feet (26’) wide.

NOTE: Prior to this excavation, sufficient granular material must be in place to provide an acceptable trench widening operation.

NOTE: Any material removed from the excavation operation of trench widening that cannot be reasonably used as part of the final shoulder shall be removed; and this material shall be moved simultaneous with the trench widening operation. Cost of this work will not be measured for separate payment, and the cost will be included in the cost of Trench Widening. Neither shall this material be allowed to remain rolled up on the existing shoulder or bladed down the existing slope.

NOTE: Due care shall be taken during this operation to blade material to the roadway and away from the ditch line. Material inadvertently bladed to the roadway vegetation shall be removed at no cost to the Department.

NOTE: Contractor shall take due care to only remove the amount of trench widening that can be replaced before the end of each day in order to not leave an open trench overnight.

- 4. Trench widening shall be constructed by placing 3” of 19-mm, ST, Trench Widening asphalt in the previous excavated trench.
- 5. One (1”) inch and variable of 9.5-mm, ST, Leveling asphalt shall be placed on Section 3 for the leveling of the mainline.

9.5-mm, ST, Leveling Asphalt		
Length (ft)	Width (ft)	Tons
12,990	26	2068.7

- 6. One and one-half inches (1½”) of 9.5-mm, ST asphalt shall be placed as a surface course on the previous leveled and milled surface.

9.5-mm, ST Asphalt	
Area	Total
Mainline	
Section 1	1,775
Section 2	1,625
Section 3	3,325
Local Roads	675
Pads	150
Total	7,550

- Granular material shall be placed on the shoulders in Section 3 as directed to raise the existing shoulders to the new surface course grade.

Granular Material, Class 5, Group D			
Length (feet)	Width Left & Right (feet)	Depth (inches)	Tons
12,9900	4	3	1,700

NOTE: Shoulders shall be bladed, shaped and compacted throughout the length of the project regardless of whether granular material is required.

NOTE: Granular material not required for the final shape of the shoulders may require removal under the pay item for excess excavation and may include small amounts of asphalt.

NOTE: Due care shall be taken during this operation to blade material to the roadway and away from the ditch line. Material inadvertently bladed to the roadway vegetation shall be removed at no cost to the Department.

- Temporary traffic stripe shall be placed daily as per Section 618 of the Standard Specifications.
- Guardrails shall be removed and replaced at the following locations:

BR #	Station	Guardrail Removal	Guardrail Installation	Terminal Section	Bridge Section Type H	Delineators, Guard Rail White
3.6	182+83	500	325	4	4	16

- Rumble strips for rumble stripe shall be placed on the outside of edge of the roadway in Section 3 only.
- Permanent pavement markings (double drop thermoplastic striping, two-way clear reflective high performance raised markers, red-clear reflective high performance raised markers and two-way yellow reflective high performance raised markers) shall be placed as required.

Existing traffic stripe on Pelucia Creek (BR# 3.6) bridge shall be removed and replaced. The length of the bridge is 1,062 feet.

- 12. All existing post mounted standard roadside signs estimated in the attached table shall be replaced. The Contractor shall deliver the removed signs to the Leflore County Maintenance Shop located at 6771 Highway 49 South in Greenwood. All existing u-channel posts shall be replaced with 2 lb./ft. square posts. All signs and hardware shall be removed from post prior to delivery. The Contractor shall verify the sign quantity prior to ordering materials. All hardware and footings required for the erection of new signs and post shall be absorbed in other items of work.

Quantity			
Pay Item No.	Description	Unit	Quantity
202-B	Removal of Sign, Including Post and Footing	EA	76
630-A	Standard Roadside Signs, Sheet Aluminum, 0.080" Thickness	SF	196
630-A	Standard Roadside Signs, Sheet Aluminum, 0.125" Thickness	SF	176
630-A	Standard Roadside Signs, Sheet Aluminum, 0.1" Thickness	SF	274
630-C	Square Tube Post, 2.0 lb/ft	LF	1125
630-C	Square Tube Post, 4.0 lb/ft	LF	75
630-G	Type 3 Object Marker, OM-3R or OM-3L	EA	6

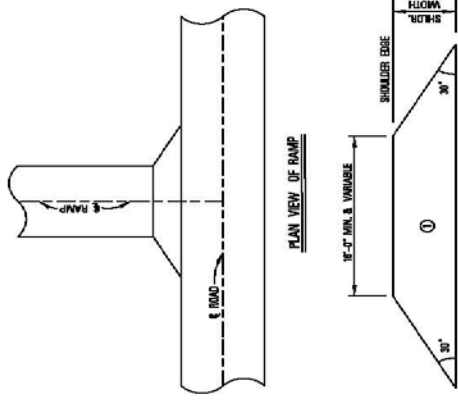
NOTE: The Contractor is to match the length, taper rate, and offset of the existing guardrail. The Contractor will be required to lay out the proposed rail for approval by the Project Engineer prior to installation.

The Contractor shall provide all signs and traffic handling devices necessary to safely maintain traffic around or through the work areas.

Incidental work such as removing vegetation, shaping and compaction of shoulder, necessary and incidental grading of roadway ditches and other incidental work that is necessary to complete the work will not be measured for separate payment and the cost will be included in the bid items provided.

The Engineer may direct the use of additional cones at County roads or intersections within lane closures and will be absorbed in pay item 618-A: Maintenance of Traffic.

STATE	PROJECT NO.
MISS.	



① 1-1/2" & VARIABLE DEPTH ASPHALT PAVEMENT REQUIRED



MISSISSIPPI DEPARTMENT OF TRANSPORTATION	
MISCELLANEOUS DETAIL	
PROJECT NUMBER	DATE
MIS-1	
FILE NAME	SCALE
REVISED BY	DATE

Station #	Lane	Sign Description	Sign Code	Size	Type 3 OM	0.08"	0.1"	0.125"	Removal U-Channel	Removal of Pipe Post & Footing	Square Tube Post 2.0lb/ft	Square Tube Post 4.0lb/ft	Notes
BOP	R	East	M3-2	24x12		2			1		15		
BOP	R	SR 430	M1-6	30x24		5							same post as "east"
BOP	R	East	M3-2	24x12		2				1		15	Route Tree on US 82
BOP	R	East	M3-2	24x12		2							Route Tree on US 82
BOP	R	Up Arrow	M6-3	21x15		2.19							Route Tree on US 82
BOP	R	Right Arrow	M6-1R	21x15		2.19							Route Tree on US 82
BOP	R	US 82	M1-4	24x24		4							Route Tree on US 82
BOP	R	SR 430	M1-6	30x24		5							Route Tree on US 82
+50	L	West	M3-4	24x12		2				1		15	
+50	L	East	M3-2	24x12		2							same post as "west"
+50	L	Left Arrow	M6-1L	21x15		2.19							same post as "west"
+50	L	Right Arrow	M6-1R	21x15		2.19							same post as "west"
+50	L	US 82	M1-4	24x24		4							same post as "west"
+50	L	US 82	M1-4	24x24		4							same post as "west"
5+50	L	48" Signal Ahead	W3-3	48x48				16	1			15	
6+00	R	Speed Limit 45	R2-1	24x30		5			1			15	
7+00	L	Rt. Lane Must Turn Rt.	R3-7	36x36			9		1			15	
7+00	R	36" Stop	R1-1	36" octagon			7.46		1			15	
7+50	R	Center Lane Turn Only	R3-9b	24x36		6			1			15	
9+00	L	Jct.	M2-1	21x15		2.19			1			15	
9+00	L	US 82	M1-4	24x24		4							same post as "jct."
14+50	R	School Crossing	S1-1	irregular				6.75	1			15	
16+00	L	Center Lane Turn Only	R3-9b	24x36		6			1			15	
16+50	R	Rt. Lane Must Turn Rt.	R3-7	36x36			9		1			15	
18+00	R	School Speed Limit 35 mph	S5-1 (mod)	24x48		8			1			15	
19+50	R	36" Stop	R1-1	36" octagon			7.46		1			15	
20+00	R	Rt. Lane Must Turn Rt.	R3-7	36x36			9		1			15	
21+50	L	Speed Limit 45	R2-1	24x30		5			1			15	
21+50	L	End School Zone	S5-2	24x30		5							same post as "45 mph"
27+00	L	36" Stop	R1-1	36" octagon			7.46		1			15	
27+00	L	Stop Ahead Symbol	W3-1a	36x36				9	1			15	
27+00	R	36" Stop	R1-1	36" octagon			7.46		1			15	
27+00	R	Stop Ahead Symbol	W3-1a	36x36				9	1			15	
29+50	L	Type 3 OM's	OM-3L & R		2				1				same post
32+00	R								1				remove damaged u-channel
35+00	L	School Speed Limit 35 mph	S5-1 (mod)	24x48		8			1			15	
36+00	L	36" Stop	R1-1	36" octagon			7.46		1			15	
37+50	L	School Crossing	S1-1	irregular				6.75	1			15	
40+50	L	36" Stop	R1-1	36" octagon			7.46		1			15	
41+00	R	36" Stop	R1-1	36" octagon			7.46		1			15	
44+00	R	Center Lane Turn Only	R3-9b	24x36		6			1			15	
44+50	L	Speed Limit 45	R2-1	24x30		5			1			15	
47+50	R	36" Stop	R1-1	36" octagon			7.46		1			15	
49+50	R	Center Lane Turn Only	R3-9b	24x36		6			1			15	
52+50	L	Center Lane Turn Only	R3-9b	24x36		6			1			15	
53+00	R	36" Stop	R1-1	36" octagon			7.46		1			15	
53+50	R	Speed Limit 45	R2-1	24x30		5			1			15	
54+00	R	Left/Right Arrow	W1-7	48x24			8		1			15	
54+00	L	36" Stop	R1-1	36" octagon			7.46		1			15	
57+00	R	Center Lane Turn Only	R3-9b	24x36		6			1			15	
61+50	L	Speed Limit 45	R2-1	24x30		5			1			15	
63+00	Both	Center Lane Turn Only	R3-9b	24x36		12			2			30	
67+50	R	Speed Limit 45	R2-1	24x30		5			1			15	
70+00	L	36" Stop	R1-1	36" octagon			7.46		1			15	
71+50	Both	Center Lane Turn Only	R3-9b	24x36		12			2			30	
73+50	L	36" Stop	R1-1	36" octagon			7.46		1			15	
77+50	L	Center Lane Turn Only	R3-9b	24x36		6			1			15	
78+50	R	Speed Limit 55	R2-1	24x30		5			1			15	
78+50	L	Speed Limit 45	R2-1	24x30		5			1			15	
79+00	L	36" Stop	R1-1	36" octagon			7.46		1			15	
80+00	R	Side Road Left	W2-2L	36x36				9	1			15	
80+50	L	36" Stop	R1-1	36" octagon			7.46		1			15	
87+00	L	Speed Zone Ahead	W3-5	36x36				9	1			15	
87+50	R	Buckle Up		24x30		5			1			15	"Buckle up, it's the law"
87+50	R	\$250 Fine		24x24		4							"\$250 fine for throwing trash on highway" -- same post as "buckle up" sign
90+50	R	36" Stop	R1-1	36" octagon			7.46		1			15	
90+50	L	Left/Right Arrow	W1-7	48x24			8					15	missing
101+00	R	36" Stop	R1-1	36" octagon			7.46		1			15	
101+00	L	Left/Right Arrow	W1-7	48x24			8					15	missing
107+00	R	Side Road Right	W2-2R	36x36				9	1			15	
109+50	R	36" Stop	R1-1	36" octagon			7.46		1			15	
109+50	L	Left/Right Arrow	W1-7	48x24			8					15	missing
112+00	R	Yield	R1-2	36" equilateral triangle		3.9			1			15	
112+00	R	Do Not Enter	R5-1	36x36			9						same post as "yield"
112+75	R	48" Stop	R1-1	48" octagon				13.25	1			15	replace existing 36" with 48" stop
112+75	R	48" Stop Ahead	W3-1a	48x48				16				15	missing
112+75	L	Left/Right Arrow	W1-7	48x24			8					15	missing

Station #	Lane	Sign Description	Sign Code	Size	Type 3 OM	0.08"	0.1"	0.125"	Removal U-Channel	Removal of Pipe Post & Footing	Square Tube Post 2.0lb/ft	Square Tube Post 4.0lb/ft	Notes
113+00	R	Yield	R1-2	36" equilateral triangle		3.9			1		15		
113+00	R	Do Not Enter	R5-1	36x36			9						same post as "yield"
117+00	L	Side Road Left	W2-2L	36x36				9			15		missing
155+00	R	Side Roads Left	W2-8L	36x36				9			15		missing
159+00	L	36" Stop	R1-1	36" octagon			7.46		1		15		
159+00	R	Left/Right Arrow	W1-7	48x24			8				15		missing
166+00	L	36" Stop	R1-1	36" octagon			7.46		1		15		
166+00	R	Left/Right Arrow	W1-7	48x24			8		1		15		missing
173+50	L	36" Stop	R1-1	36" octagon			7.46		1		15		
173+50	R	Left/Right Arrow	W1-7	48x24			8				15		missing
176+50	R	Bridge May Ice	W8-13	36x36				9	1		15		
178+00	Left	Side Roads Right	W2-8R	36x36				9	1		15		
183+00	Both	Type 3 OM's	OM-3L & R		2				2				Bridge End
193+00	Both	Type 3 OM's	OM-3L & R		2				2				Bridge End
195+50	R	Side Road Right	W2-2R	36x36				9	1		15		
198+50	L	Bridge May Ice	W8-13	36x36				9	1		15		
199+50	R	36" Stop	R1-1	36" octagon			7.46		1		15		
199+50	R	36" Stop Ahead	W3-1a	36x36				9	1		15		
199+50	L	Left/Right Arrow	W1-7	48x24			8				15		missing
204+00	L	Side Road Left	W2-2L	36x36				9	1		15		
222+00	L	Speed Limit 55	R1-2	24x30		5			1		15		
Total					6	195.75	273.66	175.75	74	2	1125	75	

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SECTION 904 - NOTICE TO BIDDERS NO. 5551

CODE: (SP)

DATE: 12/06/2023

SUBJECT: Federal Bridge Formula

Bidders are hereby advised that the latest revision of Federal Highway Administration Publication No. FHWA-HOP-06-105, **BRIDGE FORMULA WEIGHTS**, dated August 2006, is made a part of this contract when applicable.

Prior to the preconstruction conference, the Contractor shall advise the Engineer, in writing, what materials, if any, will be delivered to the jobsite via Interstate route(s).

Copies of the **BRIDGE FORMULA WEIGHTS** publication may be obtained by contacting:

Federal Highway Administration
400 7th Street, SW
Washington, DC 20590
(202) 366-2212

or

https://ops.fhwa.dot.gov/freight/publications/brdg_frm_wghts/

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-101-1

CODE: (IS)

DATE: 07/20/2023

SUBJECT: Definitions and Terms

Section 101, Definitions and Terms, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-101.01--Abbreviations. After the abbreviation API on page 1, add the following.

APL Approved Products List

Replace the abbreviation for AWPA on page 1 with the following.

AWPA American Wood Protection Association

907-101.02--Definitions. Delete the sentence after the list of holidays in Subsection 101.02 on page 6 under **holidays, legal**, and substitute the following.

When a legal holiday falls on a Saturday or Sunday, the succeeding Monday, or as proclaimed by the Governor, will be observed as a legal holiday.

Delete the definition for Notice to Proceed in Subsection 101.02 on page 8, and substitute the following.

Notice to Proceed - Written notice to the Contractor to proceed with the contract work.

Delete the definition for “Plans” in Subsection 101.02 on page 8, and substitute the following.

plans - The approved plans, profiles, typical cross-sections, working drawings and supplemental drawings, or exact reproduction thereof, that show the location, character, dimensions, and details of the work to be done. The plans may also include electronic files, referred to on the plans as Electronic Files Identified as Plans, which may include engineering models, spreadsheets, CADD files or other electronic files used to convey design intent. When the contract does not have an official set of plans, reference to the plans shall mean the contract documents.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-102-2

CODE: (IS)

DATE: 11/22/2017

SUBJECT: **Bidding Requirements and Conditions**

Section 102, Bidding Requirements and Conditions, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-102.01--Prequalification of Bidders. Delete the last sentence of the third paragraph of Subsection 102.01 on page 13, and substitute the following.

The Bidder's Certificate of Responsibility number must be on file with the Department's Contract Administration Division prior to request for permission to bid.

907-102.02--Contents of Proposal Forms. Delete the fourth paragraph in Subsection 102.02 on page 13, and substitute the following.

Prospective bidders must complete an online request for permission to be eligible to bid a project. Upon approval, the bidder will be authorized to submit a bid electronically using Bid Express at <http://bidx.com>.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-103-2

CODE: (SP)

DATE: 06/22/2017

SUBJECT: Award and Execution of Contract

Section 103, Award and Execution of Contract, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-103.01--Consideration of Proposal. Delete the second and third paragraphs of Subsection 103.01 on page 19, and substitute the following.

907-103.01.1--For Projects Constructed Without Federal Funds. Resident Contractors actually domiciled in Mississippi are to be granted preference over nonresidents in awarding of Contracts financed 100% with State funds.

In consideration of proposals that are equal to or in excess of \$50,000 and financed 100% with State funds, a nonresident bidder domiciled in a state having laws granting preference to local Contractors will be considered for such contracts on the same basis as the nonresident bidder's state awards contracts to Mississippi Contractors bidding under similar circumstances. When a nonresident Contractor submits a bid equal to or in excess of \$50,000 on a contract financed 100% with State funds, a copy of the current laws from the state of domicile and an explanation thereof pertaining to treatment of nonresident Contractors shall be attached. If no preferential treatment is provided for Contractors in the state of domicile and contracts are awarded to the lowest responsible bidder, a statement to this effect shall be attached. Should the attachment not accompany the bid when submitted, the Contractor shall have 10 days following the opening of the bids to furnish the required information to the Contract Administration Director for attachment to the bid. Failure to provide the attachment within 10 days will result in the nonresident Contractor's bid being rejected and not considered for award. As used herein, the term "resident Contractor" includes a nonresident person, firm or corporation that has been qualified to do business in this State and has maintained a permanent full-time office in the State of Mississippi for two years prior to the submission of the bid, and the subsidiaries and affiliates of such a person, firm or corporation.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-105-2

CODE: (IS)

DATE: 07/20/2023

SUBJECT: Control of Work

Section 105, Control of Work, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-105.01--Authority of the Engineer. Delete the first sentence of the second paragraph of Subsection 105.01 on page 31, and substitute the following.

The Engineer has the right to suspend the work wholly or in part and to withhold payments because of the Contractor's failure to correct conditions unsafe for workmen or the general public, for failure to carry out provisions of the Contract, or for failure to carry out orders.

907-105.02--Plans and Working Drawings. Delete the first paragraph of Subsection 105.02 on page 31, and substitute the following.

After the contract is executed by the Executive Director, the Contractor will receive, free of charge, two bound copies of the proposal and contract documents (one executed and one blank) two full scale copies of the plans, five half-scale copies of the Plans, and Electronic Files Identified as Plans. The Contractor shall have one copy of the proposal and contract documents and one half-scale copy of the plans available at all times during work activity on the project.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-108-4

CODE: (SP)

DATE: 10/07/2020

SUBJECT: Subletting of Contract

Section 108, Prosecution and Progress, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-108.01--Subletting of Contract.

907-108.01.1--General. Delete the third sentence of the tenth paragraph of Subsection 108.01.1 on the bottom of page 72.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-109-4

CODE: (IS)

DATE: 04/19/2021

SUBJECT: Measurement and Payment

Section 109, Measurement and Payment, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-109.01--Measurement of Quantities. Delete the sixth full paragraph of Subsection 109.01 on page 88, and substitute the following.

If appropriate based on the specific circumstances of the project, the Contractor may request that material specified to be measured by the cubic yard or ton be converted to the other measure. The Contractor must submit this request to the Engineer. The Engineer will provide an approval or denial in writing. The decision is in the sole discretion of the Engineer. If approved, factors for this conversion will be determined by the District Materials Engineer and agreed to by the Contractor. The conversion of the materials along with the conversion factor will be incorporated into the Contract by supplemental agreement. The supplemental agreement must be executed before such method of measurement is used.

907-109.04--Extra Work.

907-109.04.1--Supplemental Agreement. Delete the second paragraph of Subsection 109.04.1 on page 90.

907-109.06--Partial Payment.

907-109.06.2--Advancement on Materials.

Delete the next to last paragraph of Subsection 109.06.2 on page 95, and substitute the following.

Materials for which an advanced payment has been allowed must be paid for by the Contractor within 30 days of the estimate on which the advanced payment was first allowed and proof of said payment must be verified by the supplier. If proof of payment is not furnished within the allowable 30 days, the advanced payment will be deducted on subsequent current estimates until such time that proof of payment is furnished.

907-109.07--Changes in Material Costs. After the fifth paragraph of Subsection 109.07 on page 96, change the web address to the following.

https://mdot.ms.gov/portal/current_letting

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

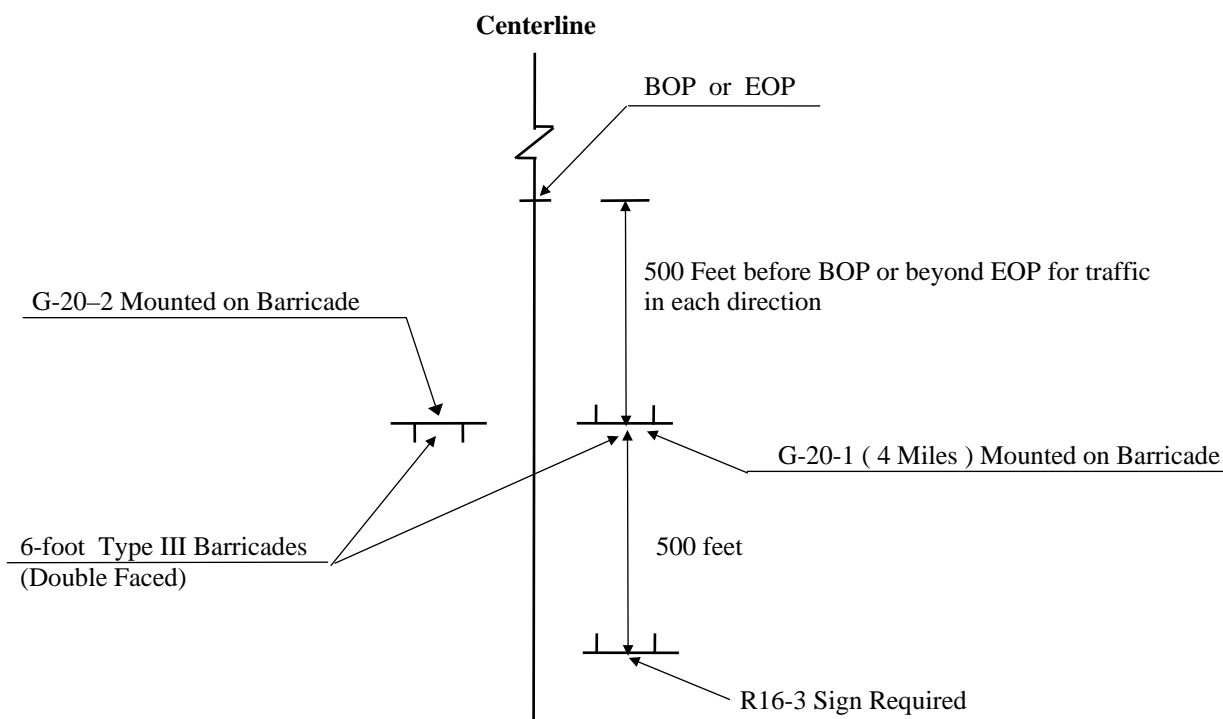
SUPPLEMENT TO SPECIAL PROVISION NO. 907-618-4

DATE: 09/13/2023

PROJECT: SP-9507-00(002) / 109442301 -- Leflore County

After the first paragraph of Subsection 907-618.01.2 on page 1, add the following.

Additional traffic control devices will be required as follows.



ADDITIONAL TRAFFIC CONTROL SIGNS REQUIRED:

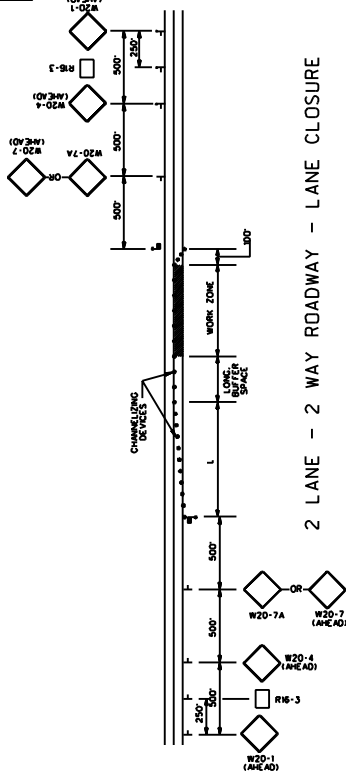
- 22 - W20-1 "AHEAD" signs required. One (1) W20-1 "AHEAD" sign is required at each local road or street entering the project.
- 12 - R4-1 "DO NOT PASS" signs required.
- 8 - R4-2 "PASS WITH CARE" signs required.
- 8 - W14-3 "NO PASSING ZONE" signs required.
- 6 - R16-3 "SPEEDING FINES DOUBLED" signs required.

R4-1 "DO NOT PASS", R4-2 "PASS WITH CARE", and W14-3 "NO PASSING ZONE" signs are required in accordance with Subsection 618.03.3, this drawing, and as specified in the Manual on Uniform Traffic Control Devices.

R16-3 signs shall be spaced in accordance with sheet titled "Location of R16-3 Signs".

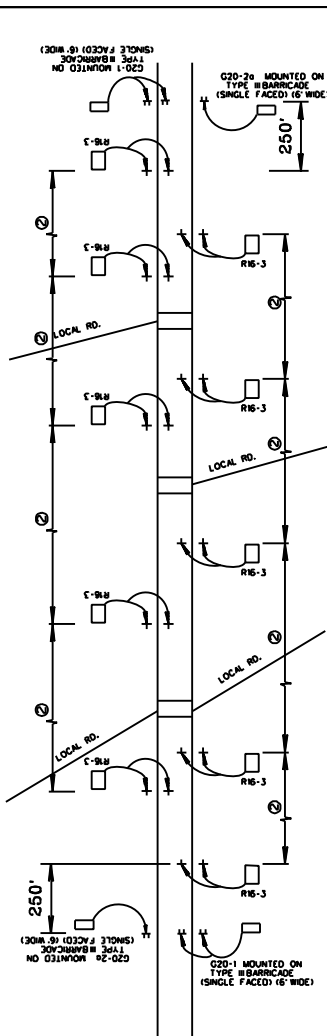
All construction signs and barricades shown on this page shall be included in the bid price for Pay Item 618-A, Maintenance of Traffic. Fluorescent orange sheeting shall be used on all construction and traffic control signs except for R16-3, R4-1 and R4-2 signs which shall be black legend and border on white background.

STATE PROJECT NO.
MISS.

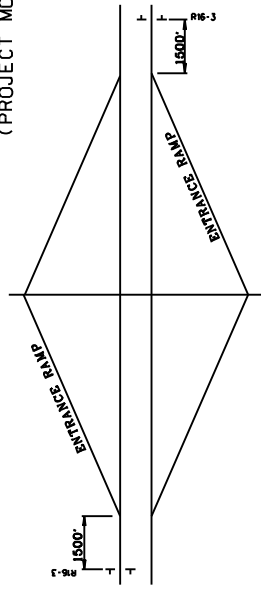


2 LANE - 2 WAY ROADWAY - LANE CLOSURE

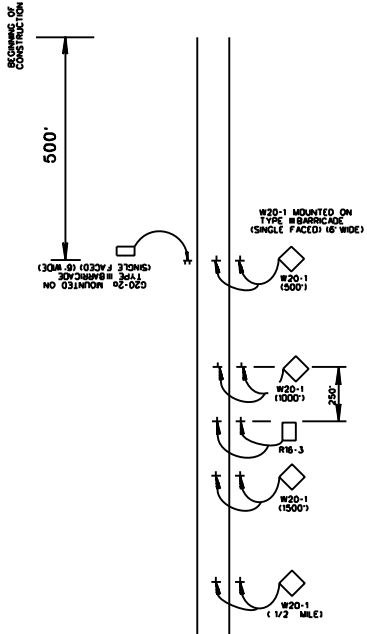
DIVIDED HIGHWAY
(PROJECTS LESS THAN 1 MILE LENGTH)



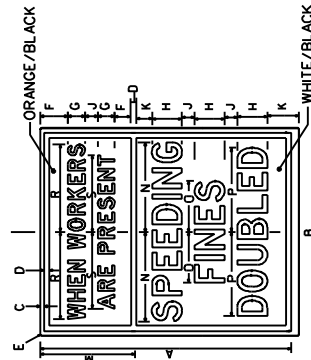
DIVIDED HIGHWAY SHOWN
(2 LANE - 2 WAY ROADWAY SIMILAR)
(PROJECT MORE THAN 1 MILE LENGTH)



INTERSTATE DETAIL



- NOTES
- 1. R16-3 SIGN TO BE PLACED AS SHOWN OR AS DIRECTED BY THE ENGINEER.
 - 2. R16-3 SIGN SHALL BE SPACED AT A MAXIMUM OF 2 MILES THROUGHOUT LENGTH OF PROJECT.



SIZE	A	B	C	D	E	F	G	H
STL 60	48	36	1 1/4	3/4	1 1/4	1 1/4	1 1/4	1 1/4
STL 72	60	48	1 1/4	3/4	1 1/4	1 1/4	1 1/4	1 1/4
STL 84	72	60	1 1/4	3/4	1 1/4	1 1/4	1 1/4	1 1/4

SIZE	A	B	C	D	E	F	G	H
STL 48	36	24	3/4	1/2	3/4	3/4	3/4	3/4
STL 60	48	36	1 1/4	3/4	1 1/4	1 1/4	1 1/4	1 1/4
STL 72	60	48	1 1/4	3/4	1 1/4	1 1/4	1 1/4	1 1/4

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

LOCATION OF R16-3 SIGNS

REVISION	BY	DATE

WORKING NUMBER

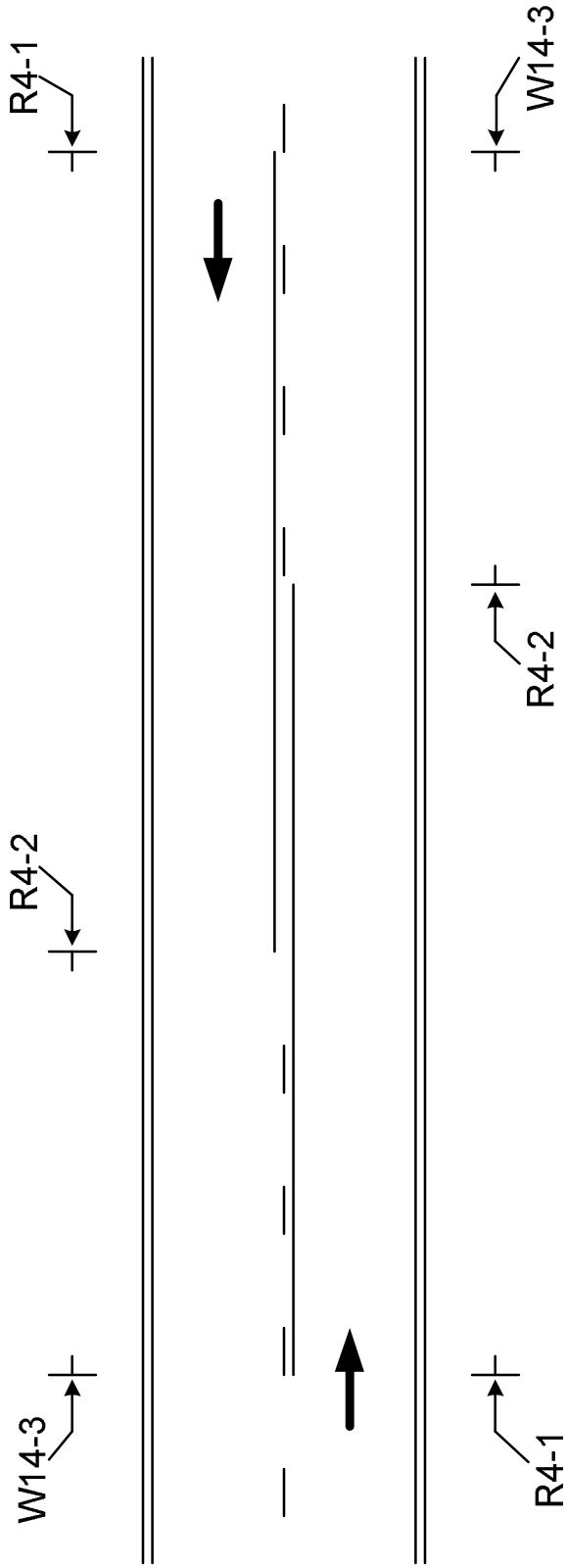
SHEET NUMBER

FILE NAME: SPEED_SIGN_DETAIL.DWG

DATE: 02/08/09

CHECKED: _____

R16-3



The W14-3, No Passing Zone sign, shall be placed on the left side of the road at the beginning of each no passing zone.

The R4-1, Do Not Pass signs, shall be placed on the right side of the road at the beginning of the no passing zone. Additional R4-1 signs shall be placed right and left in increments of 750 to 1000 feet throughout the length of the no passing zone.

The R4-2, Pass With Care sign, shall be placed on the right side of the road at the end of the no passing zone.

The R4-1, R4-2 and W14-3 signs are to be used when standard pavement markings are not in place. The signs may also be used to emphasize pavement markings.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-618-4

CODE: (SP)

DATE: 02/01/2018

SUBJECT: Additional Signing Requirements

Section 618, Maintenance of Traffic and Traffic Control Plan, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-618.01.2--Traffic Control Plan. At the end of Subsection 618.01.2 on page 441, add the following:

For compliance with the traffic control plan, the Contractor will be required to install and maintain **traffic control devices** at various locations throughout the project. Payment for these **devices** will be included in the price bid for pay item no. 618-A, Maintenance of Traffic per lump sum.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-619-6

CODE: (SP)

DATE: 03/21/2018

SUBJECT: Temporary Portable Rumble Strips

Section 619, Traffic Control for Construction Zones, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-619.02--Materials. After Subsection 619.02.15 on page 472, add the following.

907-619.02.16--Temporary Portable Rumble Strips. Temporary portable rumble strips shall be RoadQuake manufactured by PSS and meet the following requirements:

- capable of being installed without adhesives or bolts,
- have a minimum weight of 100 pounds,
- have a minimum overall length of 11 feet,
- have a minimum width of 12 inches, and
- have a maximum height of 3/4 inch.

Temporary portable rumble strips shall be installed in accordance with the attached details, or as directed by the Engineer.

907-619.03--Construction Requirements. After Subsection 619.03.11 on page 476, add the following.

907-619.03.16--Temporary Portable Rumble Strips. Temporary portable rumble strips shall be placed at locations shown on the traffic control plans, attached drawing, or as directed by the Engineer. The rumble strips shall be removed when lane closures are removed, relocated when lane closures are relocated, or as directed by the Engineer.

Prior to placement of the rumble strips, the roadway shall be cleaned to be free of dust, sand, and other materials that may cause slippage. The minimum roadway temperature at the time of installation shall be in accordance with manufacturer recommendations.

A minimum of three (3) temporary portable rumble strips shall be arranged in an array. The spacing of temporary portable rumble strips in each array shall be on 15-foot centers. One array of three (3) strips shall be used in each lane. The rumble strips shall be regularly monitored and maintained to ensure they stay in place under traffic.

907-619.04--Method of Measurement. At the end of Subsection 619.04 on page 478, add the following.

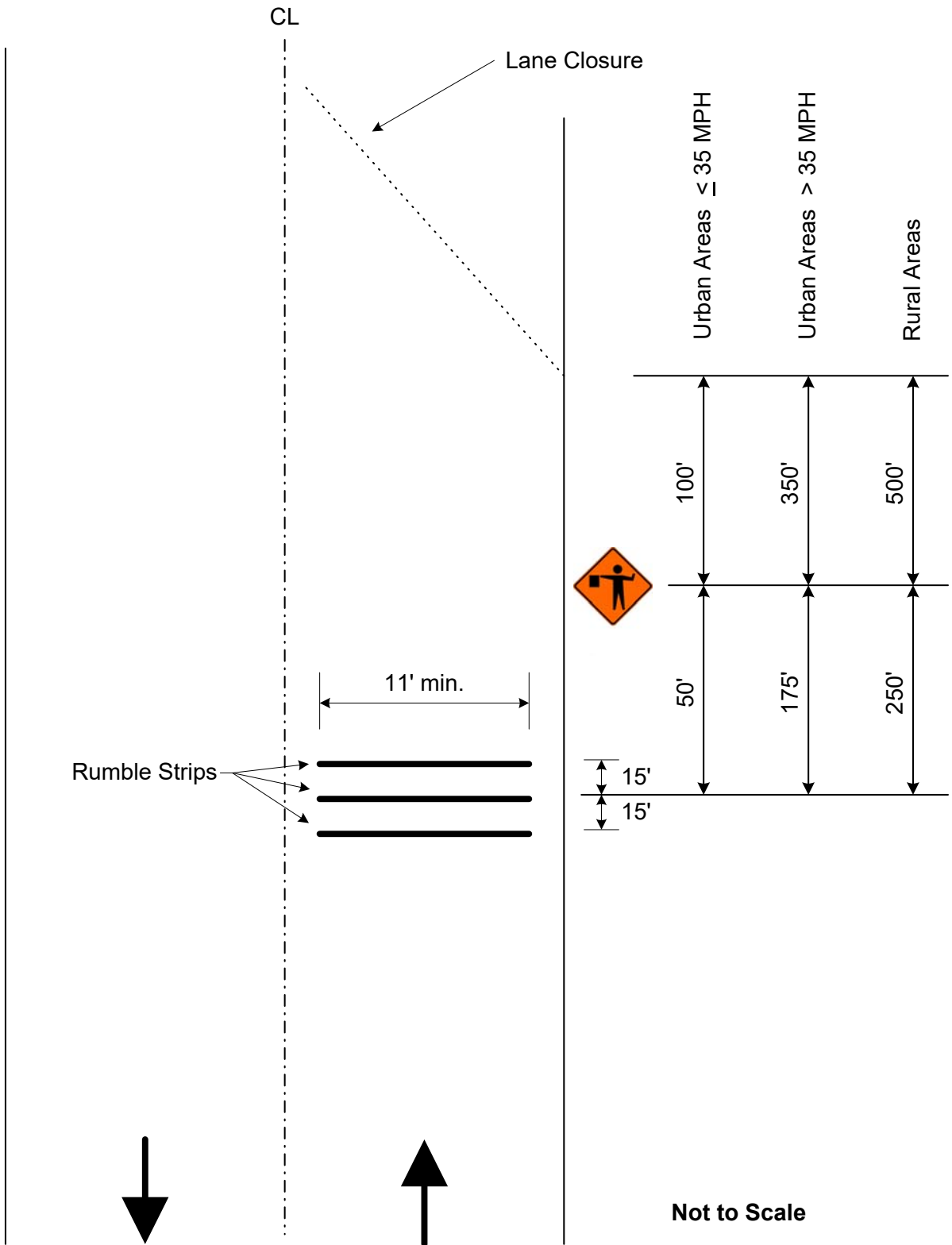
Temporary Portable Rumble Strips will be measured for payment by the linear foot only when a pay item for temporary portable rumble strips is included in the contract. Otherwise, temporary portable rumble strips will be included in the cost of pay item 618-A, Maintenance of Traffic. The quantity of temporary portable rumble strips will be the length of rumble strips approved by the Engineer to be in-place on the project at any one time.

907-619.05--Basis of Payment. After the fifth paragraph of Subsection 619.05 on page 478, add the following.

Temporary Portable Rumble Strips measured as prescribed above, will be paid for at the contract unit price per linear foot, which price shall be full compensation for cleaning the roadway surface, installing the rumble strips, maintenance and repair of the strips, cleaning and resetting of the strips, removal and replacement, and for all labor, equipment, tools, and incidentals necessary to complete the work.

After the last pay item listed on page 480, add the following.

907-619-B: Temporary Portable Rumble Strips - per linear foot



Detail of Temporary Portable Rumble Strips

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-701-3

CODE: (IS)

DATE: 05/04/2021

SUBJECT: Hydraulic Cement

Section 701, Hydraulic Cement, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-701.01--General. In the first sentence of the second paragraph of Subsection 701.01 on page 718, change “mills” to “plants.”

In the second sentence of the sixth paragraph of Subsection 701.01 on pages 718 and 719, change “shall” to “will.”

907-701.02--Portland Cement.

907-701.02.1-General.

907-701.02.1.2--Alkali Content. Delete the sentence in Subsection 701.02.1.2 on page 719, and substitute the following.

When used in portland cement concrete, the total alkali contribution from all cement types in this Subsection shall not exceed 4.0 lb. per cubic yard of concrete calculated as follows:

$$\text{lb alkali per cu Yd} = \frac{(\text{lb cement per cu Yd}) \times (\% \text{Na}_2\text{O equivalent in cement})}{100}$$

In the above calculation, the maximum cement alkali content reported on the cement mill certificate shall be used. An example calculation can be found in the Department’s *Concrete Field Manual*.

907-701.02.2--Replacement by Other Cementitious Materials. Delete the paragraph in Subsection 701.02.2 on page 719, and substitute the following.

The maximum replacement of cement by weight is 25% for fly ash or 50% for ground granulated blast furnace slag (GGBFS). Replacement contents below 20% for fly ash or 45% for GGBFS may be used, but will not be given any special considerations, such as the maximum acceptance temperature for portland cement concrete containing pozzolans in Subsection 804.02.13.1.5. Special considerations shall only apply for replacement of cement by fly ash or GGBFS.

Delete Subsection 701.02.2.1 on pages 719 and 720, and substitute the following.

907-701.02.2.1--Portland Cement Concrete Exposed to Soluble Sulfate Conditions or Seawater.

When portland cement concrete is exposed to moderate or severe soluble sulfate conditions, or to seawater, cement types and replacement of cement by Class F fly ash or GGBFS shall be as follows in Table 1. Class C fly ash shall not be used as a replacement for cement in any of the sulfate exposure conditions listed in Table 1.

Table 1- Cementitious Materials for Soluble Sulfate Conditions or Seawater

Sulfate Exposure	Water-soluble sulfate (SO ₄) in soil, % by mass	Sulfate (SO ₄) in water, ppm	Cementitious material required
Moderate and Seawater	0.10 - 0.20	150 - 1,500	Type I cement with one of the following replacements of cement by weight: 24.5 - 25.0% Class F fly ash, or 49.5 - 50.0% GGBFS or Type II ^{**} cement
Severe	0.20 - 2.00	1,500 - 10,000	Type I cement with a replacement by weight of 49.5 - 50.0% GGBFS, or Type II [*] cement with one of the following replacements of cement by weight: 24.5 - 25.0% Class F fly ash, or 49.5 - 50.0% GGBFS

* Type III cement conforming to AASHTO M85 with a maximum 8% tricalcium aluminate (C₃A) may be used in lieu of Type II cement as allowed in Subsection 701.02.1; this cement is given the designation “Type III(MS).”

** Class F fly ash or GGBFS may be added as a replacement for cement as allowed in Subsection 907-701.02.2.

Delete Subsection 701.02.2.2 on page 720, and substitute the following.

907-701.02.2.2--Portland Cement for Soil Stabilization Exposed to Soluble Sulfate Conditions or Seawater. When portland cement for use in soil stabilization is exposed to moderate or severe soluble sulfate conditions, or to seawater, cement types and replacement of cement by Class F fly ash or GGBFS shall meet the requirements of Subsection 701.02.2.1.

907-701.04--Blended Hydraulic Cement.

907-701.04.1--General. Delete Subsection 701.04.1.1 on page 720, and substitute the following.

907-701.04.1.1--Types of Blended Hydraulic Cement. Blended hydraulic cements (blended cements) shall be of the following types and conform to AASHTO M 240:

- Type II – Portland-limestone cement
- Type IP – Portland-pozzolan cement
- Type IS – Portland blast-furnace slag cement

Blended cement Types II, IP, and IS meeting the “MS” sulfate resistance requirement listed in AASHTO M 240, Table 3 shall have the “(MS)” suffix added to the type designation.

907-701.04.1.2--Alkali Content. Delete the sentence in Subsection 701.04.1.2 on page 720, and substitute the following.

All blended cement types shall be made with clinker that would result in cement meeting the requirements of Subsection 701.02.1.2 when used in the production of AASHTO M 85, Type I or Type II cement.

The blended cement manufacturer shall include the percent equivalent alkalis as Na₂O on their cement mill reports.

When calculating the total alkali contribution with blended cements, use the equivalent alkali content of the base portland cement. An example calculation for cases where blended cements are used can be found in the Department’s *Concrete Field Manual*.

907-701.04.2--Replacement by Other Cementitious Materials. Delete the paragraph in Subsection 701.04.2 on page 720, and substitute the following.

The maximum replacement of blended cement Type II by weight is 35% for fly ash or 50% for GGBFS. Replacement contents below 20% for fly ash or 45% for GGBFS may be used, but will not be given any special considerations, such as the maximum acceptance temperature for blended cement concrete containing pozzolans in Subsection 804.02.13.1.5. Special considerations shall only apply for replacement of blended cement by fly ash or GGBFS.

No additional cementitious materials, such as portland cement, blended cement, fly ash, GGBFS, or others, shall be added to or as a replacement for blended cement Types IP and IS.

Delete Subsection 701.04.2.1 on pages 720 and 721, and substitute the following.

907-701.04.2.1--Blended Cement Concrete Exposed to Soluble Sulfate Conditions or Seawater. When blended cement concrete is exposed to moderate or severe soluble sulfate conditions, or to seawater, cement types and replacement of cement by Class F fly ash or GGBFS shall be as follows in Table 2. Class C fly ash shall not be used as a replacement for cement in any of the sulfate exposure conditions listed in Table 2.

Table 2- Cementitious Materials for Soluble Sulfate Conditions or Seawater

Sulfate Exposure	Water-soluble sulfate (SO ₄) in soil, % by mass	Sulfate (SO ₄) in water, ppm	Cementitious material required
Moderate and Seawater	0.10 - 0.20	150 - 1,500	Type IL (MS) * cement, Type IL cement with one of the following replacements of cement by weight: 24.5 - 35.0% Class F fly ash, or 49.5 - 50.0% GGBFS, Type IP (MS) cement, or Type IS (MS) cement
Severe	0.20 - 2.00	1,500 - 10,000	Type IL cement with a replacement of cement by weight of 49.5 - 50.0% GGBFS, or Type IL (MS) cement with one of following replacements of cement by weight: 24.5 - 35.0% Class F fly ash, or 49.5 - 50.0% GGBFS

* Class F fly ash or GGBFS may be added as a replacement for cement as allowed in Subsection 907-701.04.2.

Delete Subsection 701.04.2.2 on page 721, and substitute the following.

907-701.04.2.2--Blended Cement for Soil Stabilization Exposed to Soluble Sulfate Conditions or Seawater. When blended cement for use in soil stabilization is exposed to moderate or severe soluble sulfate conditions, or to seawater, cement types and replacement of cement by Class F fly ash or GGBFS shall meet the requirements of Subsection 701.04.2.1.

Delete Subsection 701.04.3 on page 721.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-702-4

CODE: (IS)

DATE: 09/11/2018

SUBJECT: Bituminous Materials

Section 702, Bituminous Materials, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-702.04--Sampling. Delete the sentence in Subsection 702.04 on page 722, and substitute the following.

Sampling of bituminous materials shall be as set out in AASHTO R 66.

907-702.07--Emulsified Asphalt. Delete the last sentence in Subsection 702.07 on page 724, and substitute the following.

Asphalt for fog seal shall conform to the requirements of Subsection 907-702.12, Table V.

907-702.12--Tables. Delete Table V in Subsection 702.12 on page 729, and substitute the following.

**TABLE V
SPECIFICATION FOR FOG SEAL**

Test Requirements	LD-7		CHPF-1		Test Method
	Min.	Max.	Min.	Max.	
Viscosity, Saybolt Furol, @ 25°C, Sec.	10	100	-	100	AASHTO T 72
Storage Stability Test, 24 hr, %	-	1	-	1	AASHTO T 59
Settlement, 5 day, %	-	5	-	-	AASHTO T 59
Oil Distillate, %	-	1	-	-	AASHTO T 59
Sieve Test, % *	-	0.3	-	0.1	AASHTO T 59
Residue by Distillation, %	40	-	40	-	AASHTO T 59
Test on Residue from Distillation					
Penetration @ 25°C, 100g, 5 sec	-	20	40	90	AASHTO T 49
Softening Point, °C	65	-	-	-	ASTM D 36
Solubility in trichloroethylene, %	97.5	-	-	-	AASHTO T 44
Elastic Recovery @ 25°C, %	-	-	40	-	AASHTO T 301
Original DSR @ 82° (G*/Sinδ, 10 rad/sec)	1	-	-	-	AASHTO T 111

* The Sieve Test result is tested for reporting purposes only and may be waived if no application problems are present in the field.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-703-2

CODE: (SP)

DATE: 11/29/2022

SUBJECT: Gradation

Section 703, Aggregates, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-703.03--Coarse Aggregates for Hydraulic Cement Concrete.

907-703.03.2--Detail Requirements.

907-703.03.2.4--Gradation. In the table in Subsection 703.03.2.4 on page 734, add 100 for the percent passing by weight on the 1½-inch sieve for Size No. 67 aggregates.

Delete Note 2 under the table in Subsection 703.03.2.4 on page 734, and substitute the following.

Note ² – 100 percent shall pass the 1-inch sieve for Size 67 used in Class F and Class FX concrete.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-705-1

CODE: (IS)

DATE: 06/13/2018

SUBJECT: Stone Riprap

Section 705, Stone Blanket Protection and Filter Blanket Materials, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-705.04--Stone Riprap. Delete the last sentence of the first paragraph of Subsection 705.04 on page 750, and substitute the following.

Quality requirements for rock to be furnished under these specifications will come from a pre-approved source and be visually approved prior to use.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-707-3

CODE: (IS)

DATE: 10/27/2021

SUBJECT: Joint Materials

Section 707, Joint Materials, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-707.02--Joint Filler.

907-707.02.2--Preformed Sponge, Rubber, Cork and Closed-Cell Polypropylene Foam Joint Fillers for concrete Paving and Structural Constructions. Delete the two paragraphs of Subsection 707.02.2 on page 755, and substitute the following.

Preformed joint filler shall conform to AASHTO M 153 for sponge, rubber, and cork and tested according to ASTM D545. The type required will be indicated on the plans.

Closed-cell polypropylene foam shall conform to the requirements in ASTM D8139 and tested in accordance with ASTM D545.

907-707.02.3--Wood. Delete paragraph (b) of Subsection 707.02.3 on page 755, and substitute the following:

- (b) Dimensions shall be as shown on the plans. Dimensions shown on the plans are “dressed” sizes in accordance with Table 3 of the American Softwood Lumber Standard, SP-20. At the discretion of the Engineer, a 3/4-inch dressed board may be used in lieu of a 1-inch dressed board. A tolerance of plus or minus 1/16 inch thickness and plus or minus 1/8 inch width will be permitted. For slip-form paving a tolerance of minus 1/4 inch on each end in length will be permitted.

907-707.06--Flexible Plastic Gasket for Joining Conduit. Delete the third paragraph of Subsection 707.06 on page 756, and substitute the following.

The Department may require the performance test described in ASTM C 990.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-711-2

CODE: (IS)

DATE: 09/11/2018

SUBJECT: Plain Steel Wire

Section 711, Reinforcement and Wire Rope, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-711.02--Deformed and Plain Carbon-Steel Bars for Concrete Reinforcing.

907-711.02.3--Steel Welded and Non-Welded Wire Reinforcement, Plain and Deformed, for Concrete.

907-711.02.3.1--Plain Steel Wire. Delete the sentence in Subsection 711.02.3.1 on pages 780 and 781, and substitute the following.

Plain steel wire and plain steel welded wire shall conform to the requirements of AASHTO M 336.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-714-3

CODE: (SP)

DATE: 08/31/2021

SUBJECT: Miscellaneous Materials

Section 714, Miscellaneous Materials, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-714.01--Water.

907-714.01.1--General. Delete the last sentence of the second paragraph in Subsection 714.01.1 on page 794.

907-714.01.2--Water for Use in Concrete. Delete Subsection 714.01.2 on page 794, and substitute the following:

Water from municipal sources is permitted be used as mixing water in concrete, mortar, and grout without Department testing. Water from non-municipal water sources used in mixing of concrete, mortar, and grout which does not meet the requirements in Subsection 714.01.1 shall be tested for conformance as required in AASHTO M157, Table 1 and Table 2.

907-714.01.3--Water for Use in Chemically Stabilized Based. Delete the first sentence of first paragraph in Subsection 714.01.3 on page 794, and substitute the following:

Water used in the construction of bases that contain cement, lime, or other chemical additive shall be as set out in Subsection 714.01.1. Water from municipal sources is permitted to be used without testing for conformance to the requirements below. If water is not from a municipal source, it shall not contain impurities in excess of the following limits:

Delete Subsection 714.01.6 on page 795, and substitute the following.

907-714.01.6--Blank.

907-714.05--Fly Ash.

907-714.05.1--General. Delete the first sentence of the fifth paragraph in Subsection 714.05.1 on page 797.

907-714.13--Geotextiles.

907-714.13.11--Tables. Delete Table 1 in Subsection 714.13.11 on page 813, and substitute the following.

Table 1 - Geotextiles

Type Designation	I ¹	II ¹	III	IV	V	VI		VII		VIII	IX	Test Method
	Sediment Control	Drainage	Paving	Separation & Drainage	Separation, Stabilization & Reinforcement	Non-Woven	Woven	Non-Woven	Woven	High Strength		
Grab Strength (lb)	50	90	110	90	200	180	280	180	450	280	280	ASTM D 4632
Elongation (%)	---	50% max @ 45 lb	20% min	50% min @ break	50% min	50% min	50% max	50% min	50% max	50% min	50% min	ASTM D 4632
Seam Strength (lb)	---	---	70	---	180	160	240	160	400	240	240	ASTM D 4632
Puncture Strength (lb)	---	---	40	---	80	75	110	75	180	115	115	ASTM D 6241
Trapezoidal Tear (lb)	---	---	40	---	80	70	100	70	150	100	100	ASTM D 4533
Asphalt Retention (gal/yd ²)	---	---	---	0.2	---	---	---	---	---	---	---	ASTM D 6140
Permittivity (sec ⁻¹) min	0.05	0.05	0.5	---	0.2	0.2	0.2	0.2	0.2	0.2	0.2	ASTM D 4491
AOS Woven (mm) max	0.60	0.60	0.6	---	0.6	---	0.43	---	0.43	---	---	ASTM D 4751
AOS Non-Woven (mm) max	0.84	0.84	0.43	---	0.43	---	---	0.43	---	0.43	---	---
Tensile Strength after UV (% Retained)	70% @ 500 hr	70% @ 500 hr	50% @ 500 hr	---	50% @ 500 hr	50% @ 500 hr	50% @ 500 hr	50% @ 500 hr	50% @ 500 hr	50% @ 500 hr	50% @ 500 hr	ASTM D 4355
Melting Point °(F)	---	---	---	325	---	---	---	---	---	---	---	ASTM D 276
Minimum Ultimate Tensile Strength ³ (lb/in)	---	---	---	---	---	---	---	---	---	---	660	ASTM D 4595

Notes: 1 - All property values, with the exception of apparent opening size (AOS), represent minimum average roll values in the weakest principal direction. Values for AOS represent the maximum average roll values, 2 - Values not identified in this table should meet manufacturer certification for the use and application, 3 - Machine direction

Delete Subsection 714.15 on pages 816 and 817 and substitute the following.

907-714.15--Geogrids.

907-714.15.1--General. A geogrid is defined as a geosynthetic formed by a regular network of connected elements with apertures greater than 0.25 inch to allow interlocking with surrounding soil, rock, and other surrounding materials to function primarily as reinforcement.

Geogrid shall be manufactured from an expanded strain hardened monolithic polymer sheet composed of one or more synthetic polymers and shall be mildew resistant and inert to biological degradation and naturally encountered chemicals, alkalis and acids. The geogrid shall contain stabilizers and/or inhibitors, or a resistance finish or covering to make it resistant to deterioration from direct sunlight, ultraviolet rays, and heat.

Geogrid manufacturers shall participate in and be in compliance with the American Association of State Highway Transportation Officials (AASHTO) National Transportation Product Evaluation Program's (NTPEP) Geosynthetics audit program. Geogrid shall meet the requirements of Table II for the application and type shown on the plans and shall be selected from the Department's Approved Lists.

907-714.15.1.1--Geogrid for Retaining Walls and Reinforced Soil Slopes. Geogrid for retaining walls and reinforced soil slopes shall be creep tested in accordance with AASHTO R69 and meet Long Term Design Load, Minimum Ultimate Tensile Strength, and open area criteria listed in Table II. Manufacturers shall perform at least one long-term creep test for no less than 10,000 hours in accordance to ASTM D 5262 for each polymer or composition of polymers from which the geogrid is produced. The long-term design load that shall be reported for design use, shall be that load at which no more than 10% strain occurs over a 100-year design life of the geogrid, as calculated in accordance with AASHTO R69. Long-term design loads shall be reported unfactored, and the AASHTO strength reduction factors (Durability and Installation, and safety factors) will be considered by the Department's Geotechnical Branch on a site specific design basis.

907-714.15.1.2--Geogrid for Subgrade Stabilization. Geogrid for subgrade stabilization shall meet Minimum Ultimate Tensile Strength and open area criteria listed in Table II.

907-714.15.2--Marking, Shipment, and Storage. Each roll or container of geogrid shall be visibly labeled with the name of the manufacturer, trade name of the product, lot number, and quantity of material. In addition, each roll or container shall be clearly tagged to show the type designation that corresponds to that required by the plans. During shipment and storage the geogrid shall be protected from direct sunlight, and temperatures above 120°F or below 0°F. The geogrid shall either be wrapped and maintained in a heavy duty protective covering or stored in a safe enclosed area to protect from damage during prolonged storage.

907-714.15.3--Manufacturer Certification. The Contractor shall furnish the Engineer three copies of the manufacturer's certified test reports indicating that the geogrid furnished conforms to the requirements of the specifications and is of the same composition as the originally approved

by the Department.

907-714.15.4--Acceptance Sampling and Testing. Final acceptance of each shipment will be based upon results of tests performed by the Department on verification samples submitted from the project, as compared to the manufacturer's certified test reports. The Engineer will select one roll or container at random from each shipment for sampling. As sample extending full width of the randomly selected roll or container and being at least five (5) square yards in area will be obtained and submitted by the Engineer. All material samples shall be provided at no cost to the State.

**TABLE II
GEOGRIDS**

Physical Properties	Type Designation						Test Method
	I	II	III	IV	V	VI	
Long Term Design Load ¹ , pounds per foot, Machine Direction	250	500	750	1500	2500	3500	AASHTO R69, ASTM D5262
Minimum Ultimate Tensile Strength ² , pounds per foot, Machine Direction	500	1000	1500	3000	5000	7000	ASTM D6637
Open Area, percent	70	70	50	50	50	50	Direct Measurement

¹ Minimum design criteria requirement.

² Minimum Average Roll Value (MARV).

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-718-1

CODE: (SP)

DATE: 12/07/2021

SUBJECT: Timber and Dimension Lumber

Section 718, Timber and Dimension Lumber, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

Delete the Subsections in Section 718 on pages 836 thru 838, and substitute the following.

907-718.01--General. All timber and dimension lumber shall be Southern pine and shall conform in all respects to applicable requirements of AASHTO M 168. The Department reserves the right to sample and to test all materials at any time; all inspection, testing, and certification of materials will be performed in accordance with the requirements of the current version of the Department's *Materials Division Inspection, Testing, and Certification Manual*.

Timber and dimension lumber shall be furnished in the sizes shown on the plans or as specified. Unless otherwise specified, timber and dimension lumber shall be No. 1, or better, graded according to the latest American Lumber Standards.

Only one type of preservative shall be used for the treatment of materials for any one class of construction on a project, unless otherwise specified.

Where treated timber and dimensional lumber is to be used in non-highway construction or use, such as decking, handrails in walking trails, or in any manner where general public exposure by touch is possible, the treatment requirements will be as per project plans and/or approved by the State Materials Engineer.

907-718.02--Untreated Timber and Dimension Lumber. Untreated timber and dimension lumber shall conform to the requirements of AASHTO M 168.

907-718.03--Treated Timber and Dimension Lumber. Timber and dimension lumber to be treated shall meet the requirements herein specified and shall be treated as specified. Treated timber or dimensional lumber will not be accepted for use unless it has been inspected by an authorized representative of the Department and found to be satisfactory after treatment.

907-718.03.1--Blank.

907-718.03.2--Treatment.

907-718.03.2.1--General. All materials shall be treated in accordance with AASHTO M 133 unless otherwise directed by the Environmental Protection Agency (EPA).

907-718.03.2.2--Blank.

907-718.03.2.3--Inspection. Treated timber and dimension lumber shall be inspected by an authorized representative of the Department before being incorporated into the work. Treatment reports shall be provided to the Department for each lot of material supplied.

907-718.03.3--Blank.

907-718.03.4--Storage of Treated Material. All material treated for stock shall be stacked as compactly as possible on a well-drained surface. Material shall be supported on sills spaced as necessary, not to exceed 10 foot intervals and shall have at least one foot of air space beneath the stacks.

All materials treated with preservatives for use in buildings and applications where painting is required shall be dried after treatment. The treated wood shall be dried in accordance with American Lumber Standards.

907-718.04--Preservative. Preservatives shall be as specified in AASHTO M 133 unless otherwise directed by the Environmental Protection Agency (EPA).

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-720-2

CODE: (IS)

DATE: 09/11/2018

SUBJECT: Acceptance Procedure for Glass Beads

Section 720, Pavement Marking Materials, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-720.01--Glass Beads.

907-720.01.4--Acceptance Procedures. Delete the last sentence of the paragraph in Subsection 720.01.4 on page 841, and substitute the following.

Acceptance sampling and testing of glass beads will be in accordance with the Department's Materials Division Inspection, Testing, and Certification Manual, Section 2.9.2 -- Glass Beads.

MISSISSIPPI DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION NO. 907-721-4

CODE: (IS)

DATE: 04/19/2022

SUBJECT: Materials for Signing

Section 721, Materials for Signing, of the 2017 Edition of the Mississippi Standard Specifications for Road and Bridge Construction is hereby amended as follows.

907-721.06--Reflective Sheeting.

907-721.06.2--Performance Requirements. Delete Table 4 and Table 5 in Subsection 721.06.2 on pages 860 & 861, and substitute the following.

**MINIMUM COEFFICIENTS OF RETROREFLECTION
Candela per foot candle per square foot (cd/ft²)
Per ASTM Designation D4956**

**TABLE 4
Type IX Sheeting**

Observation Angle	Entrance Angle	White	Yellow	Green	Red	Blue	Fluorescent Yellow/Green	Fluorescent Yellow	Fluorescent Orange
0.2°	-4.0°	380	285	38	76	17	300	230	115
0.2°	+30.0°	215	162	22	43	10	170	130	65
0.5°	-4.0°	240	180	24	48	11	190	145	72
0.5°	+30.0°	135	100	14	27	6.0	110	81	41
1.0°	-4.0°	80	60	8.0	16	3.6	64	48	24
1.0°	+30.0°	45	34	4.5	9.0	2.0	36	27	14

**TABLE 5
Type XI Sheeting**

Observation Angle	Entrance Angle	White	Yellow	Green	Red	Blue	Brown	Fluorescent Yellow/Green	Fluorescent Yellow	Fluorescent Orange
0.2°	-4.0°	580	435	58	87	26	17	460	350	175
0.2°	+30.0°	220	165	22	33	10	7.0	180	130	66
0.5°	-4.0°	420	315	42	63	19	13	340	250	125
0.5°	+30.0°	150	110	15	23	7.0	5.0	120	90	45
1.0°	-4.0°	120	90	12	18	5.0	4.0	96	72	36
1.0°	+30.0°	45	34	5.0	7.0	2.0	1.0	36	27	14

After Subsection 721.10 on page 864, add the following.

907-721.11--Digital Applied Printing. The following addresses the requirements for digitally printed finished retroreflective traffic control signs on flat sheet aluminum and digitally printed traffic sign faces intended to be applied to a sign substrate.

907-721.11.1--Digitally Printed Ink Systems. Traffic signs must be produced using components, and processes that comply with the retroreflective sheeting manufacturer’s recommendations.

Digital printed ink systems used to print traffic signs must meet and comply with daytime and nighttime chromaticity (color standards) as recognized in ASTM D4956 “Standard Specification for Retroreflective Sheeting for Traffic Control.”

Digital printed ink systems must meet 70% of the initial retroreflectivity specifications of each respective reflective film color as found in ASTM D4956 “Standard Specification for Retroreflective Sheeting for Traffic Control.”

Prior to fabrication and preferably at the preconstruction meeting, the Contractor shall advise the Project Engineer in writing as to which signs on the project will be digitally printed and which ones will be screen printed. The Contractor shall submit to the Project Engineer certifications for all digitally printed signs, which will be forwarded to the State Traffic Engineer for review.

907-721.11.2--Protective Overlay Film. Permanent traffic signs printed with digital ink systems will be fabricated with a full sign protective overlay film designed to provide a smooth surface needed for retroreflectivity, and to protect the sign from fading and UV degradation. The overlamine shall comply with the retroreflective sheeting manufacturer’s recommendations to ensure proper adhesion and transparency and will also meet the reflective film durability as identified in Table 1.

**Table 1
Retroreflective Film Minimum Durability Requirements**

ASTM D4956 Type	Full Sign Replacement Term (years)	Sheeting Replacement Term (years)
IV	7	10
VIII	7	10
IX	7	12
XI	7	12

Temporary signs used in work zones printed with black ink only will not require a protective overlay film as long as the finished sign is warranted for a minimum outdoor durability of three years by the sheeting manufacturer.

907-721.11.3--Inspection. During fabrication, the Contractor shall provide sufficient testing and quality control throughout fabrication to insure good workmanship. Once the material has been received, it may be subject to random testing to ensure compliance with all requirements. If any test samples do not conform to the requirements, the entire order may be returned at the vendor’s expense.

907-721.11.4--Traffic Sign Performance Warranty Provisions. Based on the ASTM Type of sheeting specified, traffic control signs shall be warranted for the duration shown in Table 1. The Contractor shall supply a copy of the warranty document with complete details of terms and conditions upon request of the Department.

907-721.11.5--Certified Digital Sign Fabricator. Sign fabricators using digital imaging methods to produce regulated traffic signs must be certified by the reflective sheeting manufacturer whose materials are used to produce the delivered signs.

Certified sign fabricators must undergo an audit process by the sheeting manufacturer to ensure they have the proper equipment, manufacturing capabilities, manufacturing application processes and the materials required to fulfill the sheeting manufacturer's warranty obligations. Sign fabricators must recertify annually with reflective sheeting manufacturers or utilize a 3rd party certifier approved by the reflective sheeting manufacturer.

The Contractor shall submit proof of Sign Fabricator Certification as issued by the retroreflective sign sheeting manufacturer to the Project Engineer upon delivery of the signs, or with the Shop Drawings.

SECTION 905 - PROPOSAL

Date _____

Mississippi Transportation Commission
Jackson, Mississippi

Sirs: The following proposal is made on behalf of _____
_____ of _____

for constructing the following designated project(s) within the time(s) hereinafter specified.

The plans are composed of drawings and blue prints on file in the offices of the Mississippi Department of Transportation, Jackson, Mississippi.

The Specifications are the current Standard Specifications of the Mississippi Department of Transportation approved by the Federal Highway Administration, except where superseded or amended by the plans, Special Provisions and Notice(s) to Bidders attached hereto and made a part thereof.

I (We) certify that I (we) possess a copy of said Standard and any Supplemental Specifications.

Evidence of my (our) authority to submit the Proposal is hereby furnished. The proposal is made without collusion on the part of any person, firm or corporation. I (We) certify that I (we) have carefully examined the Plans, the Specifications, including the Special Provisions and Notice(s) to Bidders, herein, and have personally examined the site of the work. On the basis of the Specifications, Special Provisions, Notice(s) to Bidders, and Plans, I (we) propose to furnish all necessary machinery, tools, apparatus and other means of construction and do all the work and furnish all the materials in the manner specified. I (We) understand that the quantities mentioned herein are approximate only and are subject to either increase or decrease, and hereby propose to perform any increased or decreased quantities of work at the unit prices bid, in accordance with the above.

I (We) acknowledge that this proposal will be found irregular and/or non-responsive unless a certified check, cashier's check, or Proposal Guaranty Bond in the amount as required in the Advertisement (or, by law) is submitted electronically with the proposal or is delivered to the Contract Administration Engineer prior to the bid opening time specified in the advertisement.

INSTRUCTION TO BIDDERS: Alternate and Optional Items on Bid Schedule.

1. Two or more items entered opposite a single unit quantity WITHOUT DEFINITE DESIGNATION AS "ALTERNATE ITEMS" are considered as "OPTIONAL ITEMS". Bidders may or may not indicate on bids the Optional Item proposed to be furnished or performed WITHOUT PREJUDICE IN REGARD TO IRREGULARITY OF BIDS.
2. Items classified on the bid schedule as "ALTERNATE ITEMS" and/or "ALTERNATE TYPES OF CONSTRUCTION" must be preselected and indicated on bids. However, "Alternate Types of Construction" may include Optional Items to be treated as set out in Paragraph 1, above.
3. Optional items not preselected and indicated on the bid schedule MUST be designated in accordance with Subsection 102.06 prior to or at the time of execution of the contract.
4. Optional and Alternate items designated must be used throughout the project.

I (We) further propose to perform all "force account or extra work" that may be required of me (us) on the basis provided in the Specifications and to give such work my (our) personal attention in order to see that it is economically performed.

I (We) further propose to execute the attached contract agreement (Section 902) as soon as the work is awarded to me (us), and to begin and complete the work within the time limit(s) provided for in the Specifications and Advertisement. I (We) also propose to execute the attached contract bond (Section 903) in an amount not less than one hundred (100) percent of the total of my (our) part, but also to guarantee the excellence of both workmanship and materials until the work is finally accepted.

I (We) shall submit electronically with our proposal or deliver prior to the bid opening time a certified check, cashier's check or bid bond for **five percent (5%) of total bid** and hereby agree that in case of my (our) failure to execute the contract and furnish bond within Ten (10) days after notice of award, the amount of this check (bid bond) will be forfeited to the State of Mississippi as liquidated damages arising out of my (our) failure to execute the contract as proposed. It is understood that in case I am (we are) not awarded the work, the check will be returned as provided in the Specifications.

SECTION 905 -- PROPOSAL (CONTINUED)

I (We) hereby certify by digital signature and electronic submission via Bid Express of the Section 905 proposal below, that all certifications, disclosures and affidavits incorporated herein are deemed to be duly executed in the aggregate, fully enforceable and binding upon delivery of the bid proposal. I (We) further acknowledge that this certification shall not extend to the bid bond or alternate security which must be separately executed for the benefit of the Commission. This signature does not cure deficiencies in any required certifications, disclosures and/or affidavits. I (We) also acknowledge the right of the Commission to require full and final execution on any certification, disclosure or affidavit contained in the proposal at the Commission's election upon award. Failure to so execute at the Commission's request within the time allowed in the Standard Specifications for execution of all contract documents will result in forfeiture of the bid bond or alternate security.

Respectfully Submitted,

DATE _____

Contractor

BY _____
Signature

TITLE _____

ADDRESS _____

CITY, STATE, ZIP _____

PHONE _____

FAX _____

E-MAIL _____

(To be filled in if a corporation)

Our corporation is chartered under the Laws of the State of _____ and the names, titles and business addresses of the executives are as follows:

President Address

Secretary Address

Treasurer Address

The following is my (our) itemized proposal.

Mill & Overlay approximately 4 miles of SR 430 from US 82 to the Carroll County Line, known as State Project No. SP-9507-00(002) / 109442301 in Leflore County.

Line no.	Item Code	Adj Code	Quantity	Units	Description[Fixed Unit Price]
Roadway Items					
0010	202-B007		480	Square Yard	Removal of Asphalt Pavement, All Depths
0020	202-B158		500	Linear Feet	Removal of Guard Rail, Including Rails, Posts and Terminal Ends
0030	202-B215		76	Each	Removal of Sign Including Post & Footing
0040	202-B240		3,455	Linear Feet	Removal of Traffic Stripe
0050	203-G001	(E)	20	Cubic Yard	Excess Excavation, FM, AH
0060	304-B004	(GT)	1,700	Ton	Granular Material, Class 5, Group D
0070	403-A006	(BA1)	350	Ton	19-mm, ST, Asphalt Pavement
0080	403-A015	(BA1)	7,550	Ton	9.5-mm, ST, Asphalt Pavement
0090	403-B012	(BA1)	2,069	Ton	9.5-mm, ST, Asphalt Pavement, Leveling
0100	403-C003	(BA1)	1,000	Ton	19-mm, ST, Asphalt Pavement, Trench Widening
0110	406-D001		45,128	Square Yard	Fine Milling of Bituminous Pavement, All Depths
0120	407-A001	(A2)	13,290	Gallon	Asphalt for Tack Coat
0130	423-A001		6	Mile	Rumble Strips, Ground In
0140	503-C010		573	Linear Feet	Saw Cut, Full Depth
0150	606-B002		325	Linear Feet	Guard Rail, Class A, Type 1, 'W' Beam
0160	606-D019		4	Each	Guard Rail, Bridge End Section, Type H
0170	606-E005		4	Each	Guard Rail, Terminal End Section, Flared
0180	618-A001		1	Lump Sum	Maintenance of Traffic
0190	618-B001		1	Square Feet	Additional Construction Signs (\$10.00)
0200	619-A1001		19	Mile	Temporary Traffic Stripe, Continuous White
0210	619-A2001		10	Mile	Temporary Traffic Stripe, Continuous Yellow
0220	619-A3001		2	Mile	Temporary Traffic Stripe, Skip White
0230	619-A4002		9	Mile	Temporary Traffic Stripe, Skip Yellow
0240	619-A5001		7,100	Linear Feet	Temporary Traffic Stripe, Detail
0250	619-A6002		1,785	Linear Feet	Temporary Traffic Stripe, Legend
0260	620-A001		1	Lump Sum	Mobilization
0270	626-A001		1	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip White
0280	626-B002		10	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous White
0290	626-D001		5	Mile	6" Thermoplastic Double Drop Traffic Stripe, Skip Yellow
0300	626-E001		5	Mile	6" Thermoplastic Double Drop Traffic Stripe, Continuous Yellow
0310	626-G004		3,276	Linear Feet	Thermoplastic Double Drop Detail Stripe, White
0320	626-G005		3,810	Linear Feet	Thermoplastic Double Drop Detail Stripe, Yellow
0330	626-H001		1,628	Square Feet	Thermoplastic Double Drop Legend, White
0340	626-H002		1,785	Linear Feet	Thermoplastic Double Drop Legend, White
0350	627-J001		494	Each	Two-Way Clear Reflective High Performance Raised Markers

Line no.	Item Code	Adj Code	Quantity	Units	Description	Fixed Unit Price
0360	627-K001		158	Each	Red-Clear Reflective High Performance Raised Markers	
0370	627-L001		1,350	Each	Two-Way Yellow Reflective High Performance Raised Markers	
0380	630-A001		196	Square Feet	Standard Roadside Signs, Sheet Aluminum, 0.080" Thickness	
0390	630-A003		176	Square Feet	Standard Roadside Signs, Sheet Aluminum, 0.125" Thickness	
0400	630-A005		274	Square Feet	Standard Roadside Signs, Sheet Aluminum, 0.1" Thickness	
0410	630-C001		75	Linear Feet	Square Tube Posts, 4.0 lb/ft	
0420	630-C005		1,125	Linear Feet	Square Tube Posts, 2.0 lb/ft	
0430	630-F006		16	Each	Delineators, Guard Rail, White	
0440	630-G004		6	Each	Type 3 Object Markers, OM-3R or OM-3L	
0450	907-619-B001		66	Linear Feet	Temporary Portable Rumble Strips	
ALTERNATE GROUP AA NUMBER 1						
0460	304-F001	(GT)	40	Ton	3/4" and Down Crushed Stone Base	
ALTERNATE GROUP AA NUMBER 2						
0470	304-F002	(GT)	40	Ton	Size 610 Crushed Stone Base	
ALTERNATE GROUP AA NUMBER 3						
0480	304-F003	(GT)	40	Ton	Size 825B Crushed Stone Base	

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

CONDITIONS FOR COMBINATION BID

If a bidder elects to submit a combined bid for two or more of the contracts listed for this month's letting, the bidder must complete and execute these sheets of the proposal in each of the individual proposals to constitute a combination bid. In addition to this requirement, each individual contract shall be completed, executed and submitted in the usual specified manner.

Failure to execute this Combination Bid Proposal in each of the contracts combined will be just cause for each proposal to be received and evaluated as a separate bid.

It is understood that the Mississippi Transportation Commission not only reserves the right to reject any and all proposals, but also the right to award contracts upon the basis of lowest separate bids or combination bids most advantageous to the State.

It is further understood and agreed that the Combination Bid Proposal is for comparison of bids only and that each contract shall operate in every respect as a separate contract in accordance with its proposal and contract documents.

I (We) agree to complete each contract on or before its specified completion date.

COMBINATION BID PROPOSAL

This proposal is tendered as one part of a Combination Bid Proposal utilizing option ___* of Subsection 102.11 on the following contracts:

* Option to be shown as either (a), (b), or (c).

	<u>Project No.</u>	<u>County</u>	<u>Project No.</u>	<u>County</u>
1.	_____	_____	6.	_____
2.	_____	_____	7.	_____
3.	_____	_____	8.	_____
4.	_____	_____	9.	_____
5.	_____	_____	10.	_____

(a) If Combination A has been selected, your Combination Bid is complete.

(b) If Combination B has been selected, then complete the following page.

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
1.					
2.					
3.					
4.					
5.					
6.					
7.					
8.					

For Informational Purposes Only

SECTION 905 - COMBINATION BID PROPOSAL (Continued)

Project Number	Pay Item Number	Unit	Unit Price Reduction	Total Item Reduction	Total Contract Reduction
9.					
10.					

(c) If Combination C has been selected, then initial and complete ONE of the following.

_____ I (We) desire to be awarded work not to exceed a total monetary value of \$ _____.

_____ I (We) desire to be awarded work not to exceed _____ number of contracts.



TO: EXECUTIVE DIRECTOR, MISSISSIPPI DEPARTMENT OF TRANSPORTATION
JACKSON, MISSISSIPPI

CERTIFICATE

If awarded this contract, I (we) contemplate that portions of the contract will be sublet. I (we) certify that those subcontracts which are equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

I (we) agree that this notification of intent DOES NOT constitute APPROVAL of the subcontracts.

_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)
_____	_____
(Individual or Firm)	(Address)

NOTE: Failure to complete the above DOES NOT preclude subsequent subcontracts. Subsequent subcontracts, if any, equal to or in excess of fifty thousand dollars (\$50,000.00) will be in accordance with regulations promulgated and adopted by the Mississippi State Board of Contractors on September 8, 2011.

Contractor _____

MISSISSIPPI DEPARTMENT OF TRANSPORTATION
CERTIFICATION

I, _____,
(Name of person signing bid)

individually, and in my capacity as _____ of
(Title of person signing bid)

(Name of Firm, partnership, or Corporation)

do hereby certify under penalty of perjury under the laws of the United States and the State of Mississippi

that _____, Bidder
(Name of Firm, Partnership, or Corporation)

on Project No. **SP-9507-00(002)/ 109442301000**

in **Leflore** _____ County(ies), Mississippi, has not either directly or indirectly entered into any agreement, participated in any collusion; or otherwise taken any action in restraint of free competitive bidding in connection with this contract; nor have any of its corporate officers or principal owners.

Except as noted hereafter, it is further certified that said legal entity and its corporate officers, principal owners, managers, auditors and others in a position of administering federal funds are not currently under suspension, debarment, voluntary exclusion or determination of ineligibility; nor have a debarment pending; nor been suspended, debarred, voluntarily excluded or determined ineligible within the past three years by the Mississippi Transportation Commission, the State of Mississippi, any other State or a federal agency; nor been indicted, convicted or had a civil judgment rendered by a court of competent jurisdiction in any matter involving fraud or official misconduct within the past three years.

Do exceptions exist and are made a part thereof? Yes / No

Any exceptions shall address to whom it applies, initiating agency and dates of such action.

Note: Exceptions will not necessarily result in denial of award but will be considered in determining bidder responsibility. Providing false information may result in criminal prosecution or administrative sanctions.

All of the foregoing is true and correct.

(1/2016 S)

SECTION 902

CONTRACT FOR _____
LOCATED IN THE COUNTY(IES) OF _____

STATE OF MISSISSIPPI
COUNTY OF HINDS

This Contract is entered into by and between the Mississippi Transportation Commission (the "Commission") and the undersigned contractor (the "Contractor"), as follows:

As consideration for this Contract, the Commission agrees to pay the Contractor the amount(s) set out in the Proposal attached hereto. Said payment will be made in the manner and at the time(s) specified in the Specifications and/or Special Provisions, if any. In exchange for said consideration, the Contractor hereby agrees to accept the prices stated in the Proposal as full compensation for the furnishing of all labor, materials and equipment, and the execution of the scope of work identified for this referenced Project as contemplated in this Contract, and as more fully outlined in the Contract Documents (the "Work"). The Contract Documents consist of the Advertisement, the Notice to Bidders, the Proposal, the Specifications, the Special Provisions, and the approved Plans, all of which are hereby made a part of this Contract and incorporated herein by reference.

The Contractor shall be responsible for all loss or damage arising out of, or in any way in connection with the Work, or from any unforeseen obstructions or difficulties that may be encountered in the prosecution of the Work, and for all risks of every description connected with the Work, with the exception of any items specifically excluded in the Contract Documents. The Contractor shall fully and faithfully complete the Work in a good and workmanlike manner, according to the Contract Documents and any Supplemental Agreements thereto.

The Contractor further agrees that the Work shall be done under the direct supervision of, and to the complete satisfaction of, the Executive Director of the Mississippi Department of Transportation, or his authorized representative(s), and, when federal funds are involved, subject to the inspection and approval of the Federal Highway Administration, or its agents, and/or the agents of any other state or federal agency whose funds are involved. Further, the Work shall be done in accordance with any applicable state and federal laws, and any such rules and regulations issued by the Commission and/or any relevant Federal Agency.

The Contractor agrees that all labor as outlined in the Contract Documents may be secured from a list furnished by the Manager of the Win Job Center nearest the project location, or any successor thereto.

It is agreed and understood that each and every provision of law and clause required by law to be inserted into this Contract shall be deemed to be inserted herein, and this Contract shall be read and enforced as though it were included herein. If through mere mistake or otherwise, any such provision is not inserted, then upon the application of either party hereto, the Contract shall be physically amended to make such insertion.

The Contractor agrees that he has read each and every clause of the Contract Documents, and fully understands the meaning of same, and hereby acknowledges that he will comply with all terms, covenants and agreements therein.

Witness our signatures, this the ____ day of _____, 20__.

Contractor

By: _____
Title: _____

Signed and sealed in the presence of: (name and address of witness)

MISSISSIPPI TRANSPORTATION COMMISSION

Executive Director

Secretary to the Commission

Award authorized by the Mississippi Transportation Commission in session on the ____ day of _____, _____, Minute Book No. _____, Page No. _____.

**SECTION 903
PERFORMANCE BOND**

PERFORMANCE BOND FOR THE FOLLOWING CONTRACT:

Project No.: _____

For the construction of: _____

Contract date: _____ Contract amount: _____

FOR OWNER: MISSISSIPPI TRANSPORTATION COMMISSION, 401 N. WEST STREET, JACKSON, MISSISSIPPI 39201.

CONTRACTOR (full legal name, contact person, phone number and address):

SURETY (legal name, phone number, principal place of business and address *for notice purposes*):

Second Surety (if applicable):

The Contractor and Surety, jointly and severally, bind themselves, their heirs, executors, administrators, successors and assigns, to the Owner for the performance of the Contract, which is incorporated herein by reference, and subject to the following terms:

1. If the Contractor fully and faithfully performs the Contract, the Surety and the Contractor shall have no obligation under this Bond.
2. The Surety's obligation under this Bond shall arise after:
 - (a) the Owner first provides notice to the Contractor and the Surety that termination is imminent, pursuant to the current edition of the Mississippi Standard Specifications for Road and Bridge Construction, which is a part of the Contract; and
 - (b) the Owner declares a Contractor Default, terminates the Contract, and notifies the Surety.
3. The Surety shall promptly and at the Surety's expense, take one of the following actions:
 - (a) Arrange for the Contractor, with the consent of the Owner, to perform and complete the Contract; or
 - (b) Undertake to perform and complete the Contract itself, through its agents or independent contractors.
4. If the Surety does not proceed as provided in Paragraph 3, within 20 calendar days as set forth in Section 108.08 of the current edition of the Mississippi Standard Specifications for Road and Bridge Construction, then the Surety shall be deemed to be in default on this Bond, and the Owner shall be entitled to enforce any remedy available to it under the Contract and applicable law.
5. Subject to the commitment by the Owner to pay the Balance of the Contract Price, the Surety is obligated, without duplication, for

- (a) the responsibilities of the Contractor for correction of defective work and completion of the Contract;
 - (b) additional legal, design professional and delay costs resulting from the Contractor's Default, and resulting from the actions or failure to act of the Surety under Paragraph 3; and
 - (c) liquidated damages, or if no liquidated damages are specified in the Contract, actual damages caused by delayed performance or non-performance of the Contractor.
6. The Surety hereby waives notice of any change, including changes of time, to the Contract or to related subcontracts, purchase orders and other obligations.
 7. The penal sum of the Bond shall be subject to increase or decrease based on any subsequent Supplemental Agreements and/or final contract quantities.
 8. Notice to the Surety, the Owner or the Contractor shall be mailed or delivered to the address listed for notice purposes on the first page of this Bond.

CONTRACTOR AS PRINCIPAL

Company: _____

Signature: _____

Name: _____

Title: _____

Address: _____

SURETY

Company: _____

Signature: _____

MS Insurance ID # _____

Name: _____

Title: _____

Address: _____

SURETY (if applicable)

Company: _____

Signature: _____

MS Insurance ID # _____

Name: _____

Title: _____

Address: _____

**SECTION 903
PAYMENT BOND**

PAYMENT BOND FOR THE FOLLOWING CONTRACT:

Project No.: _____

For the construction of: _____

Contract date: _____ Contract amount: _____

FOR OWNER: MISSISSIPPI TRANSPORTATION COMMISSION, 401 N. WEST STREET, JACKSON, MISSISSIPPI 39201.

CONTRACTOR (full legal name, contact person, phone number and address):

SURETY (legal name, phone number, principal place of business and address *for notice purposes*):

Second Surety (if applicable):

The Contractor and Surety, jointly and severally, bind themselves, their heirs, executors, administrators, successors and assigns, to the Owner for payment of labor, materials and equipment furnished for use in the performance of the Contract, which is incorporated herein by reference, subject to the following terms:

1. If the Contractor promptly makes payment of all sums due to any and all subcontractors, suppliers and/or laborers, and defends, indemnifies and holds harmless the Owner from claims, demands, liens or suits by any person or entity seeking payment for labor, materials or equipment furnished for use in the performance of the Contract, then the Surety and the Contractor shall have no obligation under this Bond.
2. The Owner shall provide notice to the Surety of any claims, demands, liens or suits against the Owner or the Owner's property that it receives from any person or entity ("Claimants") seeking payment for labor, materials or equipment furnished for use in the performance of the Contract.
3. Upon notice of any claims, demands, liens or suits provided by the Owner or Contractor or given to the Surety by a Claimant, the Surety shall promptly and at the Surety's expense, defend, indemnify and hold harmless the Owner against said claim, demand, lien or suit and shall take the following additional actions:
 - (a) Send an answer to the Claimant, with a copy to the Owner, within sixty (60) days after receipt of the Claim, stating the amounts that are undisputed and the basis for challenging any amounts that are disputed; and
 - (b) Pay or arrange for payment of any undisputed amounts.
4. The Surety shall not be liable to the Owner, Claimants or others for obligations of the Contractor that are unrelated to the Contract. The Owner shall not be liable for the payment of any costs or expenses of any Claimant under this Bond, and shall have no obligation under this Bond to make payments to, or give notice on behalf of, Claimants or otherwise have any obligations to Claimants under this Bond.

5. The Surety hereby waives notice of any change, including changes of time, to the Contract or to related subcontracts, purchase orders and other obligations.
6. The penal sum of the Bond shall be subject to increase or decrease based on any subsequent Supplemental Agreements and/or final contract quantities.

CONTRACTOR AS PRINCIPAL

Company: _____
Signature: _____
Name: _____
Title: _____
Address: _____

SURETY

Company: _____
Signature: _____ MS Insurance ID # _____
Name: _____
Title: _____
Address: _____

SURETY (if applicable)

Company: _____
Signature: _____ MS Insurance ID # _____
Name: _____
Title: _____
Address: _____



BID BOND

KNOW ALL MEN BY THESE PRESENTS, that we _____
Contractor

Address

City, State ZIP

As principal, hereinafter called the Principal, and _____
Surety

a corporation duly organized under the laws of the state of _____

as Surety, hereinafter called the Surety, are held and firmly bound unto State of Mississippi, Jackson, Mississippi

As Obligee, hereinafter called Obligee, in the sum of **Five Per Cent (5%) of Amount Bid**

Dollars(\$ _____)

for the payment of which sum will and truly to be made, the said Principal and said Surety, bind ourselves, our heirs, executors, administrators, successors and assigns, jointly and severally, firmly by these presents.

WHEREAS, the Principal has submitted a bid for **Mill & Overlay approximately 4 miles of SR 430 from US 82 to the Carroll County Line, known as State Project No. SP-9507-00(002) / 109442301 in Leflore County.**

NOW THEREFORE, the condition of this obligation is such that if the aforesaid Principal shall be awarded the contract, the said Principal will, within the time required, enter into a formal contract and give a good and sufficient bond to secure the performance of the terms and conditions of the contract, then this obligation to be void; otherwise the Principal and Surety will pay unto the Obligee the difference in money between the amount of the bid of the said Principal and the amount for which the Obligee legally contracts with another party to perform the work if the latter amount be in excess of the former, but in no event shall liability hereunder exceed the penal sum hereof.

Signed and sealed this _____ day of _____, 20__

(Principal) (Seal)

(Witness) (Name) By: _____ (Title)

(Surety) (Seal)

(Witness) (Attorney-in-Fact) By: _____

(MS Agent)

Mississippi Insurance ID Number

